

**ASHLAND PLANNING COMMISSION
REGULAR MEETING
February 9, 2021
AGENDA**

I. **CALL TO ORDER:** 7:00 PM

II. **ANNOUNCEMENTS**

III. **CONSENT AGENDA**

A. Approval of Minutes

1. January 12, 2021 Regular Meeting.

IV. **PUBLIC FORUM**

V. **UNFINISHED BUSINESS**

A. Approval of Findings for PA-T2-2020-00023, 196 & 200 Clear Creek Drive.

VI. **TYPE II PUBLIC HEARINGS**

A. PLANNING ACTION: PA-T2-2020-00025

SUBJECT PROPERTY: Tax Lot #600 on the newly constructed Independent Way

APPLICANT/OWNER: Rogue Planning & Development Services/IPCO Development Corporation.

DESCRIPTION: A request for Site Design Review approval for the construction of two new commercial/industrial buildings on Tax Lot #600 adjacent to Independent Way, the newly installed public street between Washington Street and Tolman Creek Road. Both buildings would be part of the IPCO Development Corporation service building complex, and would share driveway accesses, parking areas and landscaped areas. The first building is proposed to be 10,919 square feet and would be constructed adjacent to Independent Way. The second proposed building would be 17,859 square feet and would be near the south property line. The application includes a request for an Exception to the Site Development and Design Standards (AMC 18.4.2.040.B.3.a) which call for a ten-foot wide landscape buffer between the building and the street. **COMPREHENSIVE PLAN DESIGNATION:** Employment; **ZONING:** E-1; **ASSESSOR'S MAP #:** 391E14BA; **TAX LOT:** 600.

VII. **ADJOURNMENT**

**CITY OF
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

**CITY OF
ASHLAND**
ASHLAND PLANNING COMMISSION
REGULAR MEETING
MINUTES - *Draft*
January 12, 2021

I. CALL TO ORDER:

Chair Haywood Norton called the meeting to order at 7:01 p.m.

Commissioners Present:

Michael Dawkins
Alan Harper
Haywood Norton
Kerry KenCairn
Roger Pearce
Lynn Thompson

Staff Present:

Bill Molnar, Community Development Director
Derek Severson, Senior Planner
April Lucas, Development Services Coordinator
Brandon Goldman, Senior Planner

Absent Members:

Lisa Verner

Council Liaison:

Stef Seffinger, absent

II. ANNOUNCEMENTS

Community Development Director Bill Molnar issued the following announcements:

- The January Study Session has been canceled and instead the commission will have a joint meeting with the Housing & Human Services Commission to discuss the Housing Capacity Analysis. The virtual meeting will be held on January 28 from 4:30 p.m. to 6:30 p.m.
- Pro Tem City Manager Adam Hanks has updated the City Council on KDA's presentation to the Planning Commission regarding potential opportunities for housing on commercial and employment zone lands. The City Council expressed interest on this topic and staff is working to determine potential next steps.
- City staff is looking at a local building codes amendment that would require new residential construction to incorporate fire resistant materials.

III. CONSENT AGENDA

A. Approval of Minutes

1. December 8, 2020 Regular Meeting.
2. December 22, 2020 Study Session.

The minutes of December 8, 2020 and December 22, 2020 were approved as presented.

IV. PUBLIC FORUM – None

V. TYPE II PUBLIC HEARINGS

A. PLANNING ACTION: PA-T2-2020-00023

SUBJECT PROPERTY: 196 & 200 Clear Creek Drive

OWNER/APPLICANT: Rogue Planning & Development Services, LLC/Bryan & Stephanie DeBoer

DESCRIPTION: The Planning Commission will re-open the public hearing to consider proposed modifications to a request for Site Design Review approval to allow the construction of an 11,220 square-foot, two-story mixed-use building for the properties at 196 and 200 Clear Creek Drive. The proposed building would consist of 1,268 square feet of office space, 8,052 square feet of warehouse space, and a single 1,584 square foot residential unit on the second floor. The application also includes a request for a property line adjustment to allow the consolidation of the two lots. Since the initial public hearing in December, requests for a Variance and Major Modification of the New Addition

subdivision approval (PA-2000-096) to allow a driveway to be installed from Clear Creek Drive have been removed from the proposal. COMPREHENSIVE PLAN DESIGNATION: Employment; ZONING: E-1; MAP: 39 1E 09AB; TAX LOT #: 6604 & 6605.

Chair Norton read aloud the rules for electronic public hearings.

Commissioners Harper/Pearce m/s to reopen the public hearing. Voice Vote: all AYES. Motion passed 6-0.

Ex Parte Contact

No ex parte contact was reported.

Staff Report

Senior Planner Derek Severson presented the staff report and stated at the commission's December 8, 2020 meeting the Planning Commission denied the applicant's request for a Major Modification and Variance and determined it was not merited due to the existing alley access for the property. The applicants have since modified their proposal to remove the Variance/Major Modification components and are requesting the commission revisit their decision in light of the proposed changes.

Mr. Severson reviewed the modifications to the proposal, which include:

- Removal of the driveway curb cut on Clear Creek Dr., eliminating the need for the Major Modification and Variance.
- Three additional off-street parking spaces have been provided on-site, with access to all parking taken from the alley.
- The use of parking management strategies to reduce required off-street parking has been adjusted, and now requests an approximate 8.89 percentage reduction using two on-street credits along Clear Creek Dr.
- The screening wall along the Clear Creek Dr. frontage has been extended to enclose the parking area.
- The building length has been reduced by 4 ft., and the steepness of the ramp at the rear of the building has been reduced.
- The building area has been reduced from 11,220 sq.ft. to 10,956 sq.ft.
- The overhead garage door on the east side of the structure has been shifted to the north approx. 6 ft.
- The pedestrian entrance to the Warehouse B space has been moved to the east side of the garage door, eliminating a section of walkway.
- The trash and recycling enclosure has been relocated slightly to accommodate a hinged gate rather than a sliding gate.

Ms. Severson stated staff is supportive of approving the application with the proposed modifications.

Questions of Staff

None

Applicant's Presentation

Amy Gunter, Rogue Development Services/Explained they have developed a plan that works with the elimination of the vehicle access off Clear Creek Drive and reviewed the proposed changes. Ms. Gunter stated they have some concerns regarding deliveries for the end users of this development and noted this is something that will need to get worked out. She also commented that the approval condition regarding the alley improvement, which states that "Any damage to the alley or sidewalk from utility installation or construction vehicles shall be repaired under permit from the Public Works Department, inspected and approved" is overly broad given the existing conditions of the alley. She stated they have reached out to the Public Works department regarding this concern. Ms. Gunter stated the concerns expressed by the commission at the last hearing have been addressed in the amended proposal and stated the end result will be a very nice addition to the employment zone.

Carlos Delgado, Carlos Delgado Architects/Commented that the rearrangement of the landscaping and parking area complies with the parking lot standards, even though this area may not be deemed a parking lot since it is only 6 spaces. He stated they have a phased plan if future intensity of the parking area is needed.

Questions of the Applicant

The applicant was asked to clarify the location for the exception for the reduction in buffer. Ms. Gunter clarified this is the parking space along the east property line. There is a tree at the corner where the area narrows and then widens back out.

The applicant was asked about the existing condition of the alley and who will be responsible for improvements. Ms. Gunter stated the alley as well as the sidewalk are in poor condition. She stated the alley is a public right-of-way and is the city's responsibility; however, it was installed 20 years ago and has not been well maintained. Mr. Delgado noted along with this site there are two adjacent properties also developing and the alley's condition will worsen. He noted that in the past the Public Works department has approved deferring maintenance until the construction work is completed and is hoping that will be the case here as well. Ms. Gunter stated they do not have any specific language to suggest for the alley condition and are comfortable working this out with the Public Works Department.

Comment was made that there is a language in the draft findings that states "full screening and security fencing will allow for uses in Warehouse Unit A that need additional safety measures while also allowing for adaptive re-use by a business requiring screened outdoor storage (subject to a Conditional use Permit in E-1" and it questioned what will be going into this space that might require additional safety measures. Ms. Gunter explained this was included to provide as much flexibility as possible for the future tenants and to avoid the need to have to go through another approval process.

Public Testimony - None

Chair Norton closed the public hearing and the record at 7:45 p.m.

Deliberation and Decision

Commissioner Thompson stated she is supportive of the revised proposal but requested clarification from staff on the parking credit percentages. Mr. Severson stated staff used the 12.07 figure to calculate the reduction before rounding up; however, this method could be modified to round up first. Commissioners Thompson and Pearce voiced support for rounding up first (to 13) to determine the percentage listed in the Findings.

Commissioners Thompson/Dawkins m/s for PA-T2-2020-00023 to be approved with the conditions as set forth in the staff report, subject to modification of the condition relating to the alley improvements to clarify that the applicant will consult with the Public Works Department and be responsible for any damage caused by the applicant. DISCUSSION: Commissioner Harper commended the applicant for modifying their proposal and coming back to the Planning Commission, but stated this could have been avoided if this detail was worked out at the staff level before bringing it before the group. Commissioner Pearce requested clarification regarding the alley improvement condition and whether the motion modifies what is listed in the Findings. Commissioner Thompson stated she is supportive of modifying her motion to remove reference to this condition if they feel it is clear that this condition only applies to damage created by the applicant. The commission voiced agreement with this statement. **Commissioners Thompson/Dawkins m/s to approve this change to the motion. Roll Call Vote: Commissioners Dawkins, Pearce, Thompson, KenCairn, Harper, and Norton, YES. Motion passed 6-0.**

Commissioner Pearce commented that the standards regarding parking strategies are unclear and recommended this be revisited at a later date.

VI. ADJOURNMENT

Meeting adjourned at 7:55 p.m.

*Submitted by,
April Lucas, Development Services Coordinator*

196-200 Clear Creek Drive

New Addition Subdivision, Lots 5-6

Planning Commission Hearing

January 12th, 2021



196-200 Clear Creek Drive CITY OF ASHLAND

The Original Proposal

- **Site Design Review approval** to allow the construction of an 11,220 square-foot, two-story mixed-use building for the properties at 196 and 200 Clear Creek Drive. The proposed building would consist of 1,268 square feet of office space, 8,052 square feet of warehouse space, and a single 1,584 square foot residential unit on the second floor.
- **Property line adjustment** to allow the consolidation of the two lots.
- **Variance/Major Modification** of the New Addition subdivision approval (PA-2000-096) to allow a driveway to be installed from Clear Creek Drive where the “Vehicular Access and Circulation Standards” in AMC 18.4.3.080.C.5 and the subdivision approval both prohibit driveway access from Clear Creek Drive because alley access is available.

196-200 Clear Creek Drive CITY OF ASHLAND

- Following the close of the initial public hearing on December 8th, the Commission voted to deny the application on the basis that a Major Modification and Variance were not merited and the applicant indicated that the project could not be redesigned without access from Clear Creek Drive. Findings for that decision have not yet been adopted.
- The applicant has subsequently modified the application proposal to remove the Major Modification and Variance components, has provided a 60-day extension, and is requesting that the Commission revisit their decision in light of the proposed modifications.

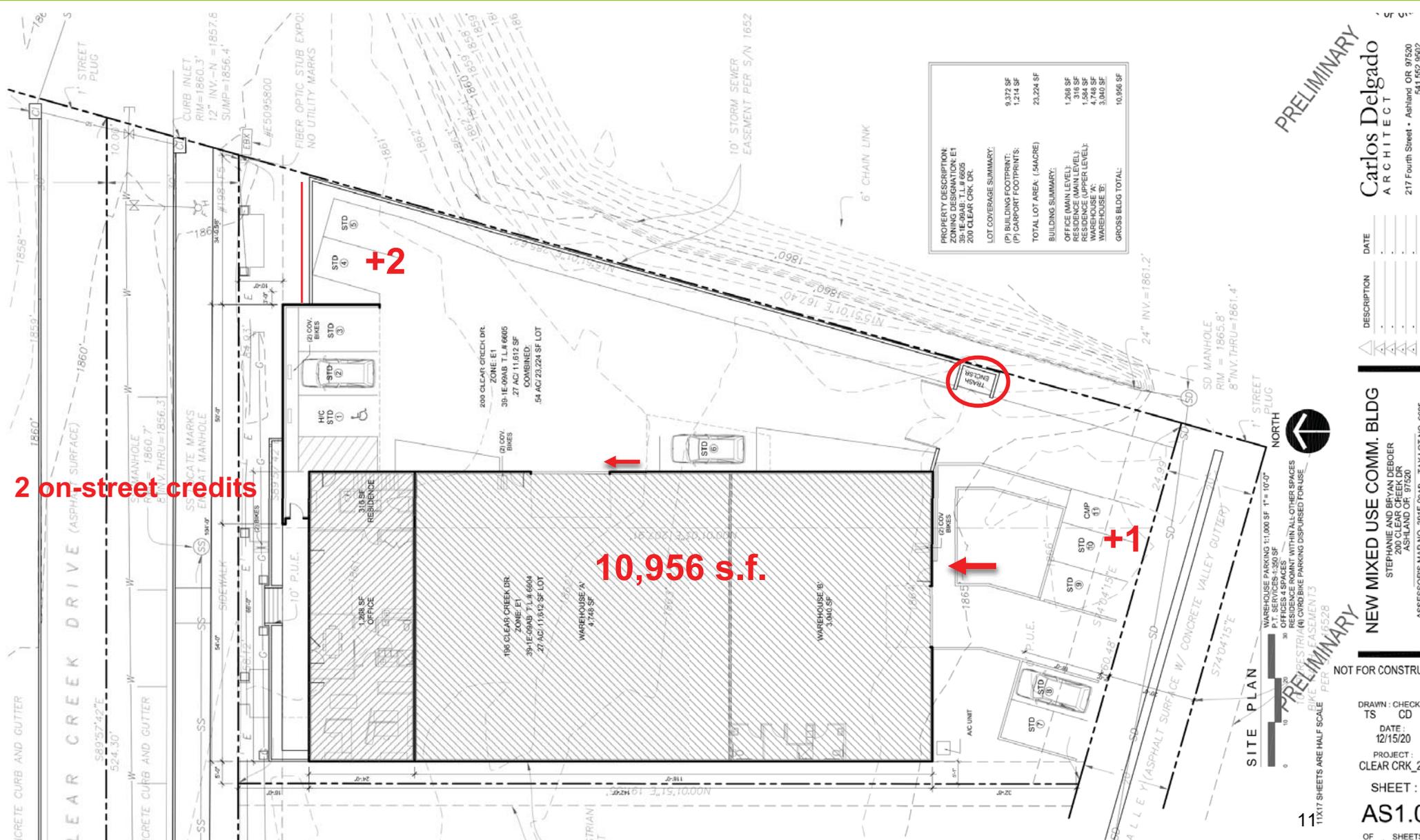
196-200 Clear Creek Drive CITY OF ASHLAND

Proposed Modifications

- **The driveway curb cut from Clear Creek Drive has been removed, eliminating the need for the Major Modification and Variance components of the original request.**
- **Three additional off-street parking spaces have been provided on-site, with access to all parking to be from the alley. The use of parking management strategies to reduce required off-street parking has been adjusted, and now requests an approximate 8.89 percent reduction using just two on-street parking credits along the Clear Creek Drive frontage.**
- The screening wall along the Clear Creek Drive frontage has been extended to enclose the parking area.
- The building length has been reduced by four feet, and the steepness of the ramp at the rear of the building has been reduced by reducing the overall length of the building.
- The building area has been reduced from 11,220 square feet to 10,956 square feet.
- The overhead garage door on the East side of the structure has been shifted to the north by approximately six feet.
- The pedestrian entrance door to the “Warehouse B” space, on the south elevation, has been moved to the east side of the garage door, eliminating a section of walkway.
- The trash and recycling enclosure at the rear of the property has been relocated *slightly* to accommodate a hinged gate rather than a sliding gate.

196-200 Clear Creek Drive CITY OF ASHLAND

Revised Site Plan



PRELIMINARY

Carlos Delgado
ARCHITECT
217 Fourth Street • Ashland OR 97520
info@CarlosDelgadoArchitect.com

DESCRIPTION	DATE

NEW MIXED USE COMM. BLDG
STEPHANIE AND BRYAN DEBOER
200 CLEAR CREEK DR
ASHLAND OR 97520
ASSESSOR'S MAP NO. 391E 09AB TAX LOT NO. 6605

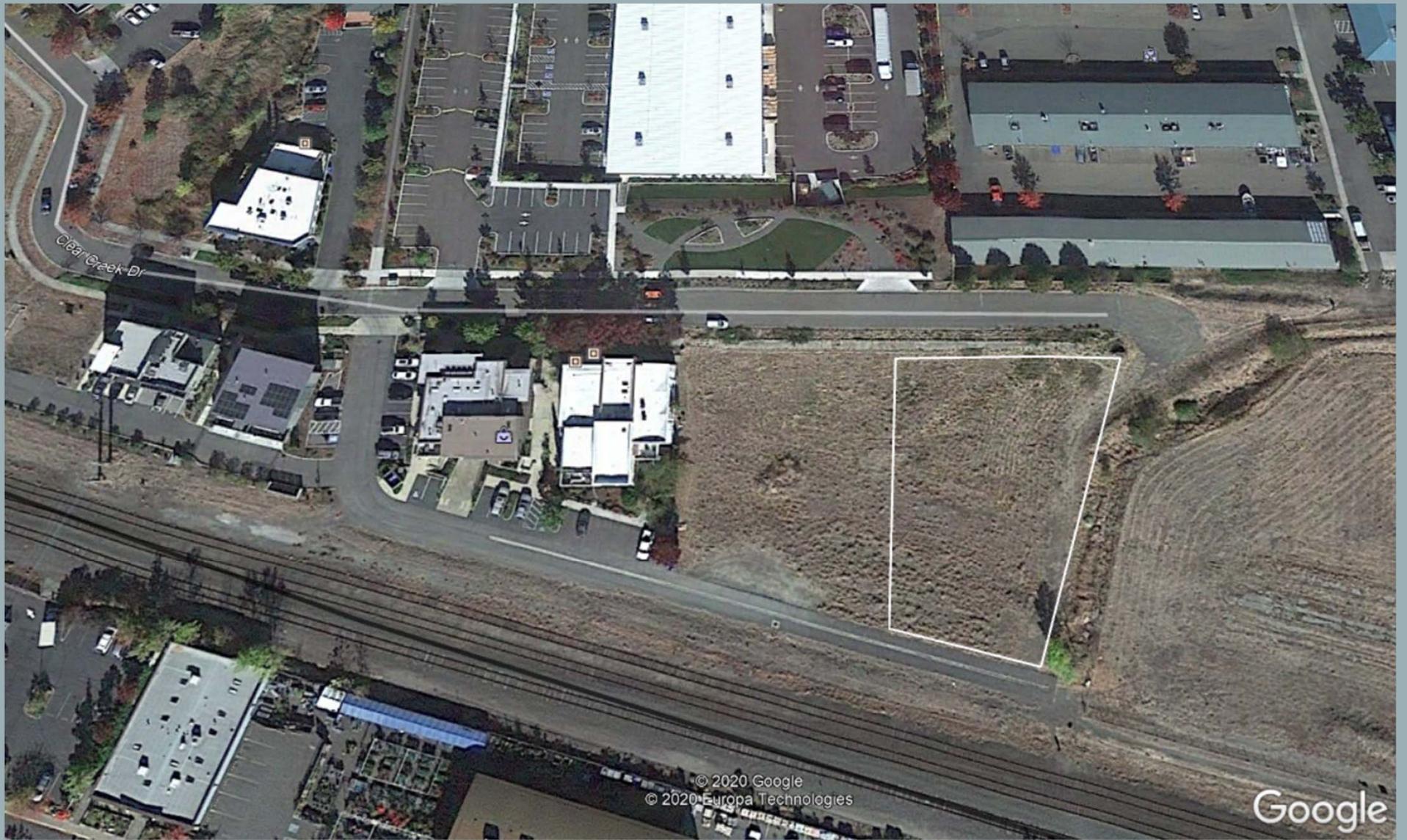
NOT FOR CONSTRUCTION

DRAWN: CHECKED:
TS CD
DATE:
12/15/20
PROJECT:
CLEAR CRK 20
SHEET:
AS1.0
OF SHEETS

CLEAR CREEK DRIVE

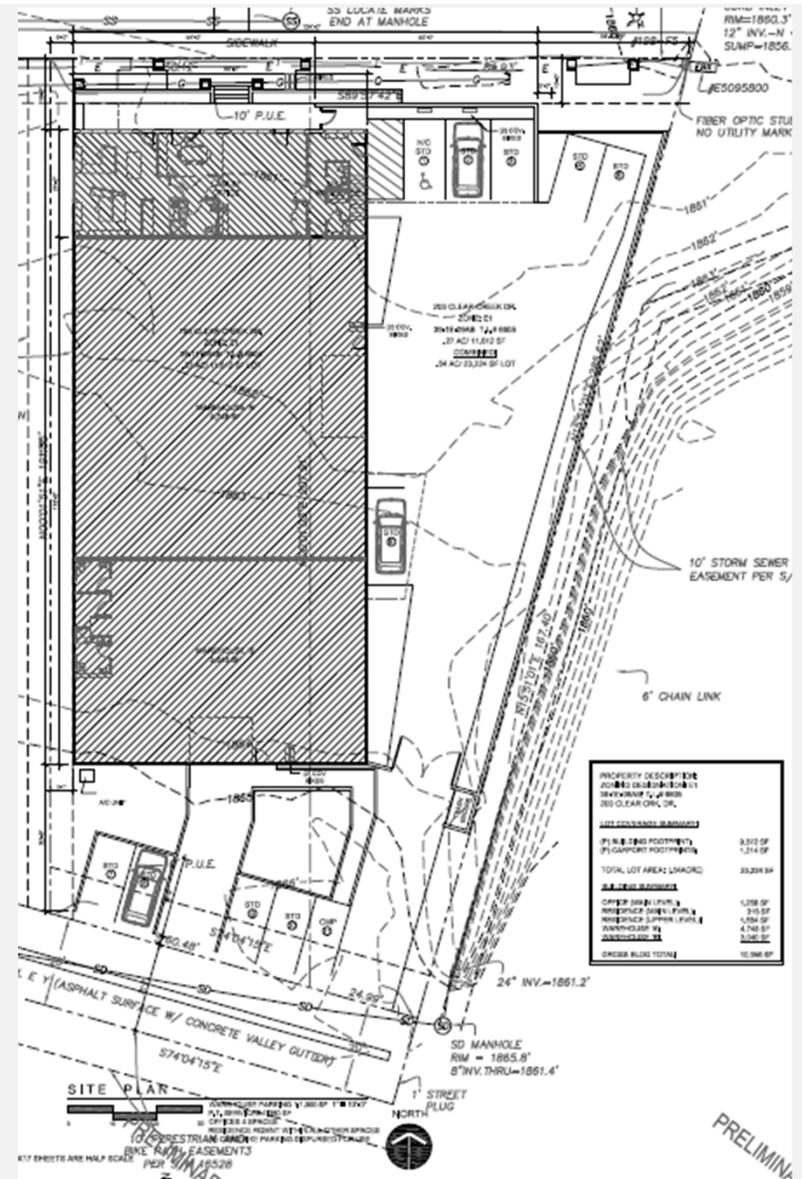
SITE DESIGN REVIEW



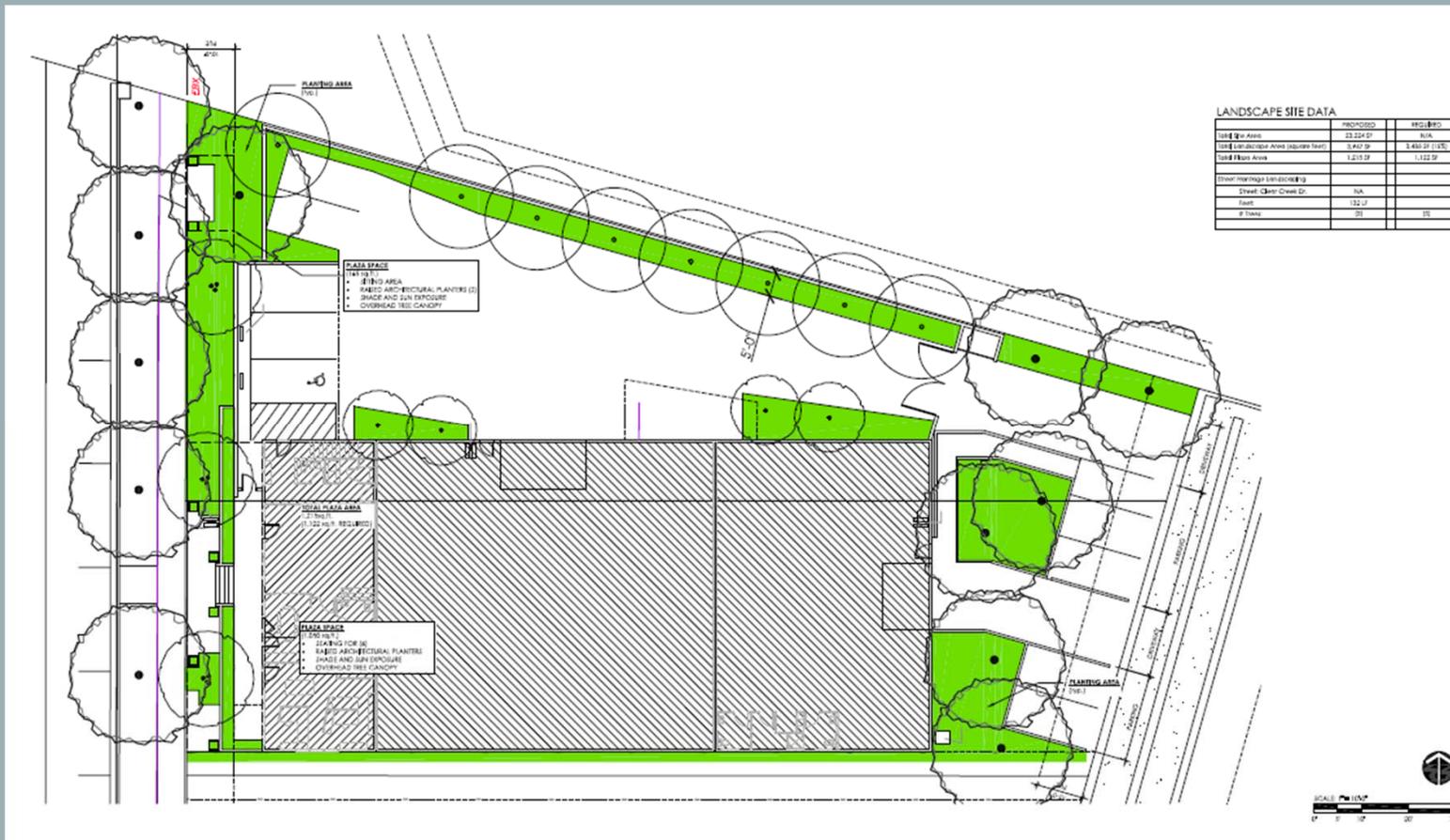


SITE PLAN MODIFICATIONS

- The building reduced in length by four feet
- The building area reduced from 11,220 to 10,956 square feet
- Removed driveway curb cut from Clear Creek Drive
- The screening wall along the frontage extended to enclose the parking area
- Additional on-site parking accessed only from the alley
- Requested parking management strategy to allow for on-street parking of two spaces.
- The pedestrian entrance door on Warehouse B moved to the east side of the garage door eliminating a walkway
- The trash/recycle area at the rear of the property was relocated slightly to accommodate a hinged gate

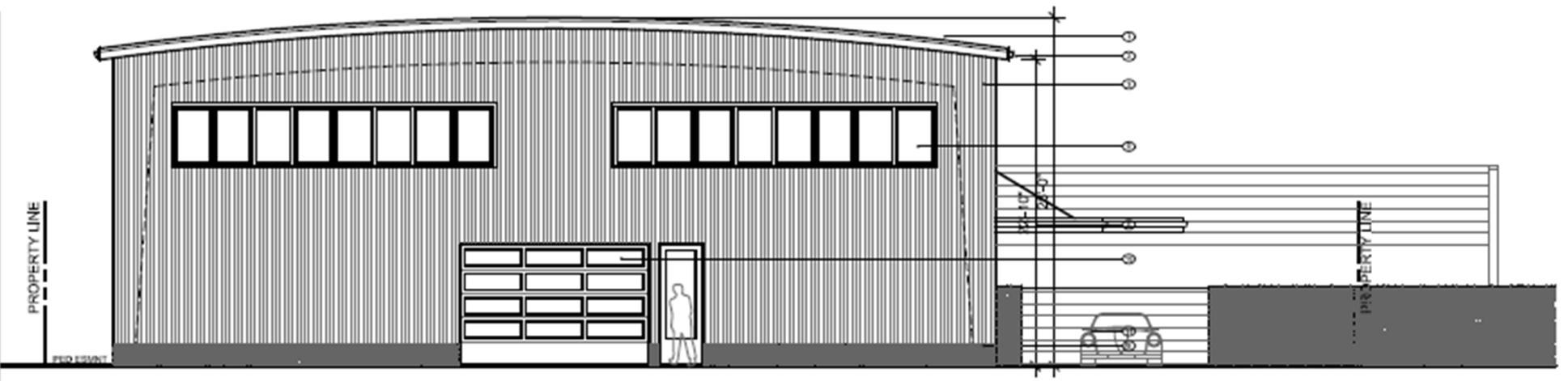


CONCEPTUAL LANDSCAPE PLAN TO DEMONSTRATE COMPLIANCE WITH AREA REQUIREMENTS

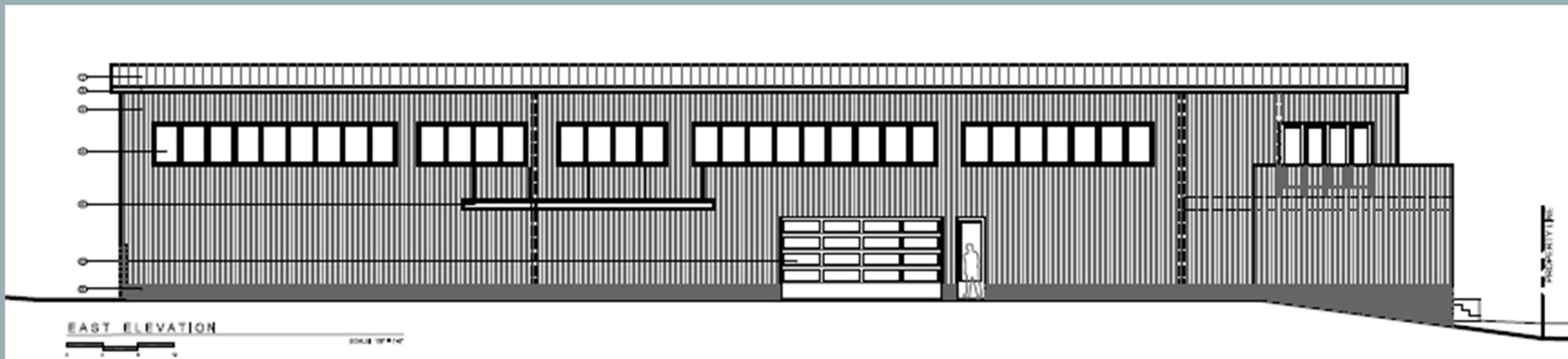
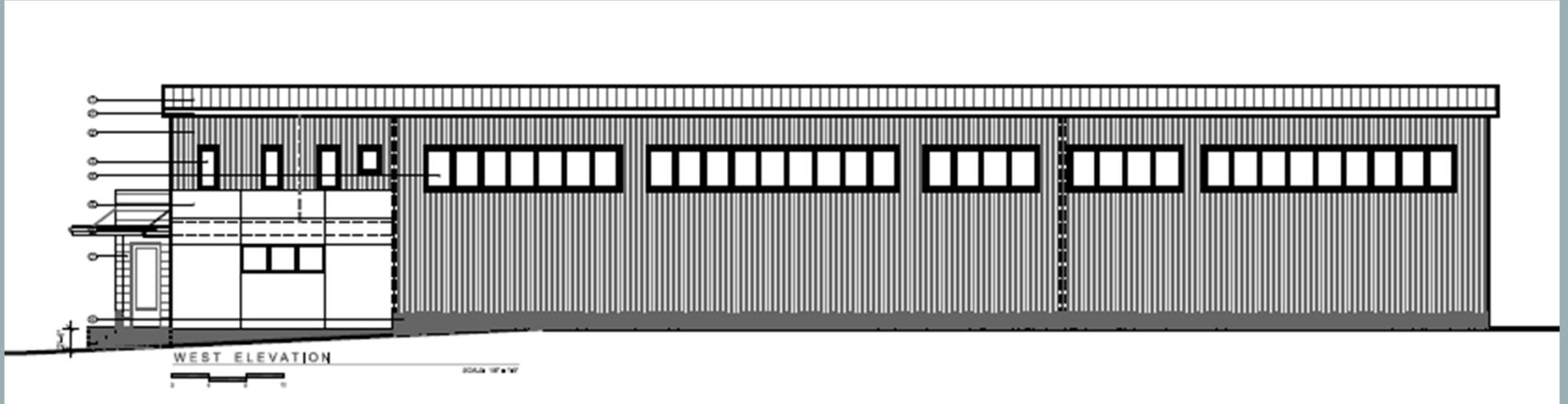




NORTH ELEVATION - CLEAR CRK FACADE
SCALE: 3/16" = 1'-0"



SOUTH ELEVATION - ALLEY FACADE
SCALE: 3/16" = 1'-0"



ALLEY IMPROVEMENT CONDITION

9.d. Any damage to the alley or sidewalk from utility installation or construction vehicles shall be repaired under permit from the Public Works Department, inspected and approved.

FINDINGS

**PA-T2-2020-00023
196 & 200 Clear Creek Dr.**

DRAFT - BEFORE THE PLANNING COMMISSION - DRAFT

February 9, 2021

IN THE MATTER OF PLANNING ACTION #PA-T2-2020-00023, A REQUEST FOR)
SITE DESIGN REVIEW APPROVAL TO CONSTRUCT AN 10,956 SQUARE FOOT,)
TWO-STORY, MIXED-USE BUILDING FOR THE PROPERTIES AT 196 AND 200)
CLEAR CREEK DRIVE. THE PROPOSED BUILDING WOULD CONSIST OF 1,268)
SQUARE FEET OF OFFICE SPACE, 7,788 SQUARE FEET OF WAREHOUSE SPACE)
AND A SINGLE 1,584 SQUARE FOOT RESIDENTIAL UNIT. THE APPLICATION)
ALSO REQUESTS A PROPERTY LINE ADJUSTMENT TO ALLOW FOR THE CON-)
SOLIDATION OF THE TWO LOTS. (REQUESTS FOR A VARIANCE AND MAJOR)
MODIFICATION OF THE ORIGINAL SUBDIVISION APPROVAL [PA #2000-096])
TO ALLOW A DRIVEWAY FROM CLEAR CREEK DRIVE WHERE “VEHICULAR)
ACCESS AND CIRCULATION STANDARDS” IN AMC 18.4.3.080.C.5 AND THE)
SUBDIVISION APPROVAL BOTH PROHIBIT DRIVEWAY ACCESS BECAUSE)
ALLEY ACCESS IS AVAILABLE WERE WITHDRAWN BY THE APPLICANT)
DURING THE PUBLIC HEARING PROCESS.))

DRAFT
FINDINGS,
CONCLUSIONS,
& ORDERS

APPLICANT/OWNERS: Rogue Planning & Development Services, LLC/
Bryan & Stephanie DeBoer

RECITALS:

- 1) Tax lots #6604 and #6605 of Map 39 1E 09AB are the vacant Lots #5 and #6 of the ‘New Addition’ subdivision on Clear Creek Drive, and are zoned Employment (E-1) and are within both the Residential, Detail Site Review and Wildfire Lands overlay zones.
- 2) The applicant is requesting Site Design Review approval to allow the construction of a 10, 956 square-foot, two-story mixed-use building for the properties at 196 and 200 Clear Creek Drive. The proposed building would consist of 1,268 square feet of office space, 7,788 square feet of warehouse space, and a single 1,584 square foot residential unit on the second floor. The application also includes a request for a property line adjustment to allow the consolidation of the two lots. Additional requests for a Major Modification of the New Addition subdivision approval (PA-2000-096) and for a Variance to allow a driveway to be installed from Clear Creek Drive where the “Vehicular Access and Circulation Standards” in AMC 18.4.3.080.C.5 and the original subdivision approval both prohibit driveway access from Clear Creek Drive because alley access is available were withdrawn by the applicant during the public hearing process. The proposal is outlined in plans on file at the Department of Community Development.
- 3) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:
 - A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions,*

density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
 - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

4) The approval criteria for a Property Line Adjustment are detailed in **AMC 18.5.3.120.B** as follows:

- 1. **Parcel Creation.** No additional parcel or lot is created by the lot line adjustment.
- 2. **Lot Standards.** Except as allowed for nonconforming lots, pursuant to chapter 18.1.4, or as required by an overlay zone in part 18.3, all lots and parcels conform to the lot standards of the applicable zoning district, including lot area, dimensions, setbacks, and coverage, per part 18.2. If a lot does not conform to the lot's standards of the applicable zoning district, it shall not be made less conforming by the property line adjustment. As applicable, all lots and parcels shall identify a buildable area free of building restrictions for physical constraints (i.e., flood plain, greater than 35 percent slope, water resource protection zones).
- 3. **Access Standards.** All lots and parcels conform to the standards in section 18.4.3.080 Vehicle Area Design. Lots and parcels that do not conform to the access standards shall not be made less conforming by the property line adjustment.

5) On April 15, 2020 Governor Kate Brown issued Executive Order #20-16 “*Keep Government Working: Ordering Necessary Measures to Ensure Safe Public Meetings and Continued Operations by Local Government During Coronavirus (COVID-19) Outbreak.*” The Governor’s Order required that public bodies hold public meetings by telephone, video, or through some other electronic or virtual means, whenever possible; that the public body make available a method by which the public can listen

to or virtually attend the public meeting or hearing at the time it occurs; that the public body does not have to provide a physical space for the public to attend the meeting or hearing; that requirements that oral public testimony be taken during hearings be suspended, and that public bodies instead provide a means for submitting written testimony by e-mail or other electronic methods that the public body can consider in a timely manner. The subsequently adopted House Bill #4212 further authorized governing bodies in Oregon to conduct all public meetings using telephone or video conferencing technology or through other electronic or virtual means.

8) The Planning Commission, following proper public notice, held an electronic public hearing on December 8, 2020. In keeping with Executive Order #20-16 and subsequent House Bill #4212, this meeting was broadcast live on local television channel 9 and on Charter Communications channels 180 & 181, and was live-streamed over the internet on RVTV Prime at <http://www.rvtv.sou.edu>. A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report were made available on-line seven days prior to the hearing. Those wishing to provide written testimony were able to submit it via e-mail in advance of the hearing, as detailed the mailed and posted notices, and all written testimony received by the established deadlines was made available for Commissioners to review before the hearing and was included in the meeting minutes. In addition, those wishing to participate during the hearing could arrange to provide oral testimony by making arrangements to do so in advance of the meeting.

Following the closing of the initial public hearing and the record, the Planning Commission considered the materials received and testimony presented and denied the project, noting that a requested Major Modification of the original subdivision approval and an associated Variance to allow a new driveway from Clear Creek Drive were not merited and that the applicant had indicated during the hearing process that the project could not be redesigned without a new driveway access.

Subsequent to this decision, but before the written findings formalizing the denial were adopted, the applicant submitted a revised proposal modifying their application by removing the request for the new driveway, eliminating the Major Modification and Variance, granting an extension of time, and asking that the Planning Commission reopen the public hearing to review the application as modified.

The Planning Commission, following proper public notice, reopened the electronic public hearing on January 12, 2021 at which time written testimony submitted in advance of the hearing was considered and new oral testimony was presented. Following the closing of the public hearing and the record, the Planning Commission considered the materials received and testimony presented and approved the project, subject to a number of conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the application materials, staff report, public testimony and exhibits received.

2.2 The Planning Commission finds that the amended proposal for Site Design Review and Property Line Adjustment meets all applicable criteria for Site Design Review described in AMC 18.5.2.050 and for a Property Line Adjustment described in AMC 18.5.3.120.B.

2.3 The Planning Commission concludes that the proposal satisfies all applicable criteria for Site Design Review approval.

The first approval criterion for Site Design Review approval addresses the requirements of the underlying zone, requiring that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”* The Planning Commission finds that the building and yard setbacks and other applicable standards have been evaluated to ensure consistency with the applicable provisions of part 18.2, and all regulations of the underlying E-1 zoning will be satisfied.

The second approval criterion deals with overlay zones, and requires that, *“The proposal complies with applicable overlay zone requirements (part 18.3).”* The Planning Commission finds that the property is within the Detail Site Review, Residential and Wildfire Lands overlay zones.

The Detail Site Review overlay requires that the application address the Detail Site Review Standards in AMC 18.4.2.040.C. Where proposed buildings are greater than 10,000 square feet in gross floor area – as is the case here – or contain more than 100 feet of building frontage, the Additional Standards for Large Scale Projects in AMC 18.4.2.040.D must also be addressed. Compliance with these standards is discussed with regard to part 18.4 below, under the next criterion.

Within the Residential (R) overlay zone, the requirements of AMC 18.3.13.010.C come into play where residential units are proposed, and require: 1) For mixed-use developments, if there is one building on a site, ground floor residential uses shall occupy not more than 35 percent of the gross floor area of the ground floor. Where more than one building is located on a site, not more than 50 percent of the total lot area shall be designated for residential uses. At least 65 percent of the gross floor area of the ground floor shall be designated for permitted uses and uses permitted with special use standards, not including

residential uses; 2) Residential densities shall not exceed 15 dwelling units per acre. For the purpose of density calculations, units of less than 500 square feet of gross habitable floor area shall count as 0.75 of a unit; and 3) Residential uses shall be subject to the same setback, landscaping, and design standards as for permitted uses in the E-1 District. The Planning Commission finds that: 1) 316 square feet of the 9,372 square foot ground floor is dedicated to the foyer and stairway for the residential unit, with the remainder of the ground floor dedicated to permitted or special permitted uses in the zone other than residential. This equates to only approximately 3.3 percent of the ground floor (316 square feet/9,372 square feet = 0.0337), with the remaining 96.7 percent to be dedicated to permitted or special permitted uses within the E-1 zone; 2) At the allowed residential density of 15 dwelling units per acre, the property has an allowed residential density of 7.997 dwelling units (0.533 acres x 15 dwelling units/acre = 7.997 dwelling units), and the single residential unit proposed does not exceed the allowed density; and 3) The proposal is being considered in light of the same setback, landscaping and design standards as any E-1 project.

The Planning Commission further finds that the subject property is located within the Wildfire Lands overlay zone, and as such a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 will need to be provided for the review of the Fire Marshal prior to bringing combustible materials onto the property, and any new landscaping proposed will need to comply with these standards and shall not include plants listed on the 'Prohibited Flammable Plant List' per Resolution #2018-028. The applicant asserts that the proposed landscape plan complies with the applicable Wildfire Lands requirements, and does not use plants from the prohibited plant list. A condition has been included below to require a final Fire Prevention and Control Plan and plant list be provided for the review and approval of the Fire Marshal prior to the issuance of a building permit or to bringing any combustibles onto the site.

Based on the foregoing, the Planning Commission finds that this criterion is satisfied.

The third criterion addresses the Site Development and Design Standards, requiring that *“The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.”*

The application discusses the Basic Site Development and Design Standards for Non-Residential Development, noting that the proposed building's primary orientation is towards Clear Creek Drive, with parking placed behind the building and with no automobile parking or circulation between the building and the street. The building's façade is 104-feet of the 141-foot wide frontage, and the building façade occupies a large majority - nearly 74 percent - of the lot frontage. The building entrances are located within 20-feet of the street right-of-way. The entrances are clearly visible, and an eight-foot door with transom window, lighting, pedestrian covering, and material changes is provided to emphasize the entrance.

Along the west side of the property, there is a five-foot pedestrian easement, and a corresponding five-foot easement is in place on the property to the west to provide a ten-foot wide pedestrian connection from Clear Creek Drive to the alley. The applicant proposes to improve this easement with compacted gravel to provide a walking surface.

A public sidewalk and parkrow planting strip were installed with the subdivision in 2000, and city standard street trees are proposed to be installed along the frontage. A new pedestrian plaza area, with hardscape surface treatments between the sidewalk and the building, will provide pedestrian access to the street-facing business entrances directly accessible from the public sidewalk.

Nearly 17 percent of the site is to be provided as a landscape area to comply with the applicable standards of the zone, and a common recycle and refuse area is provided near the rear of the property, screened from view, and accessible from the alley. All artificial lighting is noted as complying with the standards of AMC 18.4.4.050, and there are no residential zones in the immediate vicinity of the project site.

The application discusses the Detail Site Review Standards, noting that the proposed structure and the pedestrian plaza area provided combine to comply with the Floor Area Ratio (FAR) standard calling for a minimum 0.50 FAR. The proposed structure is 10,956 square feet and there is 1,215 square feet of plaza area proposed for a total of 12,171 square feet, which is more than the required 11,761 square feet needed to meet a 0.50 FAR.

The building frontage is 104-feet in length, and distinctive offsets and material changes are provided in the façade to break-up the massing and scale of the structure. All of the front façade walls are within 30-feet of the public street, and more than 20 percent of the wall area facing the street is in windows or doorways. Large windows are proposed on either side of the commercial business entrance, and there is a cantilevered overhang to protect pedestrians from the elements.

More than 15 percent of the exterior walls have substantial changes in relief. There is a substantial base; changes in façade materials with the use of vertical standing seam metal siding, split-face concrete block, horizontal, wooden plank siding, and hardboard. There are bronze-colored, aluminum storefront style windows and doors.

Landscape buffers are proposed between the surface parking spaces adjacent to the alley and the west property line, and a landscape buffer of six-feet is proposed along the east side of the parking area. The parking area and the interior area of the site are proposed to be screened from the adjacent railroad property with a six-foot-tall solid panel and CMU screen wall.

The property is within the Detail Site Review overlay zone, and the proposed building to be more than 10,000 square feet in area and as such is subject to Additional Standards for Large Scale Projects. The application explains that the building is oriented towards Clear Creek Drive, and that the building's mass along the street is divided into two separate masses. The structure has a human scale incorporated through the changes in setback and orientation in materials, and a sheltering roof is proposed to provide pedestrian shelter while adding a horizontal element to the front façade. The pattern of the windows and the doors is distinct and relates to the spaces within the structure.

The application materials note that the building requires 1,096 square feet of plaza space, and 1,215 square feet of plaza space are proposed between the sidewalk and building. This space will incorporate sitting areas, space for eating, a mixture of sunlight and shade areas under the marquee and near the plaza area trees, and the plaza area surface will include colored and/or scored concrete.

With regard to required off-street parking and parking management strategies, the Planning Commission finds that the project as proposed requires 12.074 off-street parking spaces, which is rounded to 13 off-street parking spaces by code.

OFFICE: 1,268 square feet of office at one space per 500 square feet requires	1,268/500 = 2.536 spaces
WAREHOUSE: 7,788 square feet of warehouse at one space per 1,000 square feet requires	7,788/1000 = 7.788 spaces
RESIDENTIAL: A two-bedroom residential unit requires	1.75 spaces
TOTAL OFF-STREET PARKING REQUIRED:	12.074 spaces

The Planning Commission finds that 11 off-street parking spaces are proposed, with five spaces accessed directly from the public alley at the rear of the property. A driveway extends from the alley into the property where six additional off-street parking spaces are provided – three are within an enclosed carport, two are surface spaces adjacent to the carport, and a sixth space is provided parallel to the building under a canopy. One of the 11 spaces to be provided is shown as a required accessible parking space.

The Planning Commission finds that vehicular access to the site is to be from the public alley, and the proposed internal parking and maneuvering area is to be screened with a block wall and an electric gate that fully screens the site from the public rights-of-way of the street and the alley, and from the adjacent properties to the east. The application explains that full screening and security fencing will allow for uses in Warehouse Unit A that need additional safety measures while also allowing for adaptive re-use by a business requiring screened outdoor storage (subject to a Conditional Use Permit in E-1).

The Planning Commission further finds that application proposes to utilize Parking Management Strategies as provided in **AMC 18.4.3.060**, requesting an On-Street Parking Credit to reduce required off-street parking by two spaces as there are four on-street parking spaces available along the frontage of the property. The combined reduction in required parking requested is approximately 15.4 percent (**11 spaces provided/13 spaces required = 0.846; 1.0 – 0.846 = 0.154**). The Planning Commission finds that the requested reduction is a reasonable application of the discretion provided to the Commission in the Parking Management Strategies section.

The Commission further finds that bicycle parking is proposed to be dispersed around the site to provide the most convenient parking for the various uses. A standard “U-rack” is proposed behind the sidewalk at the front of the building, visible from Clear Creek Drive. Within the covered carport, and additional two spaces are proposed. And each warehouse unit will include additional spaces as proposed. The application emphasizes that all proposed bicycle parking areas will comply with the bicycle parking standards, and will be located in a manner that provides adequate commercial customer and residential bicycle parking. Conditions requiring that adequate bicycle parking be illustrated in the building permit submittals and inspected on site before occupancy have been included below.

The Planning Commission finds that parking to be provided will comply with the dimensional, surfacing and back-up space requirements, and notes that the five spaces adjacent to the alley are to be buffered

from the structure with landscape planters which will function as stormwater swales to capture and treat surface run-off. The three parking spaces within the carport are to be covered, which will reduce the micro-climatic impacts of the parking spaces. The application materials note that parking lot shade trees and landscape buffers complying with the standards for parking area design will be provided.

Based on the foregoing, the Planning Commission concludes that the proposal complies with the applicable Site Development and Design Standards of part 18.4.

The fourth approval criterion addresses city facilities, specifically requiring that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.”*

The Planning Commission notes that the application asserts that adequate city facilities exist to serve the proposed development, and that final development plans based on the current proposal will be submitted to the City of Ashland Public Works, Engineering, Planning, Building, Electric and Fire Departments for review and approval prior to the issuance of a building permit. Specific facilities are discussed as follows:

- **Water:** The application notes that the subject properties are currently served by an eight-inch water main in adjacent Clear Creek Drive right-of-way, and that the new water services and meters proposed will be installed off of this main.
- **Sanitary Sewer** – The application explains that the subject properties are currently served by an eight-inch sanitary sewer main in the adjacent Clear Creek Drive right-of-way, further explaining that the existing main ends short of the end of the current street improvements but will be extended from the existing terminus to serve the proposed development.
- **Electric:** An existing electrical box was installed with the subdivision at the northeast corner of the property, and services will be extended to serve the project. The Electric Department has preliminarily approved the electric service plan for the project.
- **Urban Storm Drainage:** The application explains that the subject properties are currently served by a 12-inch storm drainage sewer main in Clear Creek Drive, and that because the proposed development will create more than 2,500 square feet of impervious surface, the project civil engineer has proposed a stormwater drainage facility plan which complies with the requirements of the DEQ “MS4 General Permit Phase 2” and which follows the guidance and requirements set forth in the current Rogue Valley Stormwater Quality Design Manual.
- **Adequate Transportation:** Clear Creek Drive is considered to be a Commercial Neighborhood Collector, and is ultimately intended to provide a commercial corridor connecting Oak Street to North Mountain Avenue. The improvements envisioned for a Commercial Neighborhood Collector are illustrated in AMC 18.4.6.040.G.3 and include a 28- to 36-foot paved curb-to-curb width within a 55- to 63-foot right-of-way, seven-foot landscaped parkrow, and an eight- to ten-foot sidewalk. The existing right-of-way way here is 60 feet wide with an approximate curb-to-curb width of 28 feet which will accommodate on-street parking on one side, and was improved with pavement, curbs, gutters, park rows and sidewalks to city standards at the time of the subdivision approval, however the park row planting strip here lacks street trees or street lights,

and a condition of approval has been included below to require that street trees be selected from the Recommended Street Tree Guide, planted according to standards with irrigation, and inspected, and that street lights be installed along the frontage, prior to the issuance of a certificate of occupancy.

At the rear of the property, an alley is in place. Typical alley standards envision a 12-foot paved width within a 16-foot right-of-way, however here the alley has a 16-foot paved width within a 20-foot right-of-way, and includes a central ‘valley gutter’ for drainage. With the approval of the subdivision under PA #2000-096, there was also an easement for a 10-foot pedestrian and bike path easement south of the alley, on the railroad property. With the subdivision approval, the Planning Commission at the time found that *“all necessary public facilities, utilities and services are available to serve the six tax lots. Public facilities are located within the right-of-way of Clear Creek Drive and within the public alley adjacent to the project. The Commission finds that the multi-use path adjacent to the alley is required in lieu of increasing the Clear Creek Drive right-of-way to accommodate bicycle lanes. As a result, the multi-use path is a required street improvement that must be either installed, or planned and bonded for as part of the subdivision.”*

The application includes a preliminary grading, utility and erosion control plan prepared by Registered Professional Engineer Mark Dew of Dew Engineering. These plans identify facilities available in the adjacent rights-of-way along with proposed connections; meter placement; and storm water control, detention and treatment systems. The Planning Commission finds that the site’s utilities will be extended to and through the subject property from public utility easements and street right-of-way adjacent to the site, and that based on the conceptual plans, adequate key city facilities are available within the adjacent rights-of-way and will be extended by the applicant to serve the proposed development. Conditions have been included below to require that final electric service, utility and civil plans be provided for the review and approval of the Staff Advisor and city departments in conjunction with the building permit submittals, and that civil infrastructure be installed by the applicants, inspected and approved prior to occupancy. With these conditions, the Planning Commission finds that adequate capacity of city facilities, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property. The Commission concludes that this criterion has been satisfied.

The final criterion for Site Design Review approval addresses “Exception to the Site Development and Design Standards.” The Planning Commission notes that the application materials include a request for an Exception to the Site Development and Design Standards to allow an approximately 18-foot section of the landscape buffer along the east property line to be reduced to three feet in width where the “Parking Lot Landscaping and Screening” standards in AMC 18.4.4.030.F.2.a call for a five-foot landscaped strip to screen parking abutting a property line. The application materials indicate that in addition to the three feet of surface buffering proposed with landscaping, a six- to eight-foot solid panel fence and masonry wall is proposed to provide additional screening at the property line, while on the adjacent property there is a ten-foot wide stormwater drainage easement with an open ditch. The application materials argue that the combination of landscape screening and construction of a solid wall provides equal screening while better providing for public safety by creating a physical barrier as

protection from the drop off into the open ditch on the adjacent, undeveloped property. The application further asserts that the proposal will not have a negative impact on the livability of the adjacent employment-zoned property which does not have a Residential overlay, concluding that the exception requested is minimal as it is limited to a two-foot reduction in buffer width for only 18-feet of a 258-foot length of the driveway surface. While the Planning Commission recognizes the benefit of the proposed wall installation as a barrier preventing falls into the adjacent open ditch, the Commission also finds that the angled property line which has 143 feet of frontage along Clear Creek Drive but reduces this width to approximately 85 feet along the alley poses a demonstrable difficulty in responding to the standard, that approval of the Exception for this relatively minimal 18-foot segment of the property line will not substantially negatively impact the adjacent property, and that the combination of the reduced buffer and the proposed wall will adequately serve to reduce development impacts on adjacent uses.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

2.5 The Planning Commission finds that the proposal satisfies all applicable standards specific to a Property Line Adjustment.

The first criterion for a Property Line Adjustment is that, *“No additional parcel or lot is created by the lot line adjustment.”* In this instance, the proposal would consolidate the two contiguous lots being considered and would not create an additional parcel or lot.

The second criteria for a Property Line Adjustment is that, *“Except as allowed for nonconforming lots, pursuant to chapter 18.1.4, or as required by an overlay zone in part 18.3, all lots and parcels conform to the lot standards of the applicable zoning district, including lot area, dimensions, setbacks, and coverage, per part 18.2. If a lot does not conform to the lot’s standards of the applicable zoning district, it shall not be made less conforming by the property line adjustment. As applicable, all lots and parcels shall identify a buildable area free of building restrictions for physical constraints (i.e., flood plain, greater than 35 percent slope, water resource protection zones).”* The Planning Commission finds that with the requested adjustment, the property will conform to the applicable lot standards of the E-1 zoning district as discussed elsewhere in this document.

The final criterion for a Property Line Adjustment is that, *“All lots and parcels conform to the standards in section 18.4.3.080 Vehicle Area Design. Lots and parcels that do not conform to the access standards shall not be made less conforming by the property line adjustment.”* The Planning Commission finds that the proposal complies with the applicable Vehicle Area Design requirements.

Based on the foregoing, The Planning Commission concludes that, as detailed above, the proposal is consistent with the applicable standards and criteria for a Property Line Adjustment.

SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review approval to construct an 10,956 square foot, two-story mixed-use

building and for a Property Line Adjustment to allow the consolidation of two lots is supported by evidence contained within the whole record.

The building proposed will be an attractive addition to the Clear Creek Drive streetscape, and Commission concerns identified during the initial public hearing with the requested Variance/Major Modification to allow a new driveway off of Clear Creek Drive and the requested reductions in off-street parking have been well addressed in the amended proposal now being considered, which no longer includes a new driveway and which limits the reduction in required parking to only two off-street parking credits.

As amended, the Commission concludes that the proposal merits approval with the conditions detailed below. Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2020-00023. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2020-00023 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein, including but not limited to that there shall be no driveway curb cut from Clear Creek Drive.
2. That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this Site Design Review approval shall be submitted and approved prior to issuance of a building permit.
3. That any new addresses shall be assigned by City of Ashland Engineering Department.
4. That permits shall be obtained from the Ashland Public Works Department prior to any work in the public right of way.
5. That the windows on the ground floor shall not be tinted so as to prevent views from into the interior of the building
6. That the front entrances adjacent to Clear Creek Drive shall remain functional and open to the public during all business hours.
7. That a sign permit shall be obtained prior to installation of any new signage. Signage shall meet the requirements of Chapter 18.4.7.
8. That the building permit submittal shall include:
 - a. Identification of all easements, including public and private utility easements, public pedestrian access easements, and fire apparatus access easements.
 - b. Solar setback calculations demonstrating that all new construction complies with Solar Setback Standard B in the formula $[(\text{Height} - 16) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$ and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and the height(s) from natural grade.
 - c. Lot coverage calculations including all building footprints; driveways, parking, and circulation areas; and any other areas other than natural landscaping. Lot coverage shall be limited to no more than 85 percent as required in AMC 18.2.6.030.

- d. Final electric service, utility and civil engineering plans including grading, erosion control and drainage. All civil infrastructure shall be installed by the applicants, inspected and approved prior to final inspection/occupancy approval.
- e. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer mains and services, manholes and clean-outs, and storm drainage pipes and catch basins, along with any backflow prevention measures required by the Water Department. Any required private or public utility easements shall be delineated on the civil plans.
- f. The final electric design and distribution plan shall include load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment with the Final Plan application. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department.
- g. That storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals. The storm drainage plan shall detail the location and final engineering for all storm drainage improvements associated with the project, and shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions. The storm drainage plan shall demonstrate that post-development peak flows are less than or equal to the pre-development peak flow for the site as a whole, and that storm water quality mitigation has been addressed through the final design.
- h. Final site lighting details.
- i. A final size- and species-specific landscaping plan including irrigation details satisfying the Water Conserving Landscaping Guideline in AMC 18.4.4.030.I. New landscaping shall comply with the General Fuel Modification Area requirements and shall not include plants listed on the Prohibited Flammable Plant List adopted by Resolution #2018-028. All landscaping shall be installed according to the approved plan, and tied into the existing irrigation system, inspected and approved prior to the issuance of a certificate of occupancy.
- j. That the requirements of the Ashland Fire Department relating to approved addressing; fire apparatus access; a firefighter access pathway; fire flow; hydrant installation, spacing and clearance; applicable fire sprinklers and alarm monitoring; fire department connection; key box; extinguishers; limitations on obstructions to fire access; and wildfire hazard area requirements shall be satisfactorily addressed in the permit submittals.
- k. A Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 shall be provided prior to bringing combustible materials onto the property, and any new landscaping proposed shall comply with these standards and shall not include plants listed on the 'Prohibited Flammable Plant List' adopted with Resolution #2018-028.

1. The building permit submittals shall verify that the bicycle parking, spacing and coverage requirements are met in accordance with 18.4.3.070.I. Inverted U-racks shall be used for the bicycle parking, and all bicycle parking shall be installed in accordance with design and rack standards in 18.4.3.070.I and J, inspected and approved by the Staff Advisor prior to the issuance of the certificate of occupancy. If bicycle parking is to be provided within the proposed buildings, final interior dimensions of the dedicated bicycle parking areas shall be detailed on the building permit plans to insure adequate space has been provided. A bicycle parking space located inside of a building for employee bike parking shall be a minimum of six feet long by three feet wide by four feet high, shall be accessible without moving another bicycle, and shall be clearly marked as reserved for bicycle parking only.

9. That prior to the final inspection approval or issuance of a certificate of occupancy:
 - a. That street trees, one per 30 feet of street frontage, shall be installed in the Clear Creek Drive frontage, inspected and approved by the Staff Advisor. All street trees shall be chosen from the adopted Street Tree List and shall be installed in accordance with the specifications noted in Section E of the Site Design and Use Standards. The street trees shall be irrigated.
 - b. That all landscaping in the new landscaped areas, and the irrigation system, shall be installed according to the approved plan, inspected and approved by the Staff Advisor.
 - c. Civil improvements including but not limited to utility installations shall be completed according to approved plans, inspected and approved. Public improvements including but not limited to street trees and street lighting shall be installed to City of Ashland standards under permit from the Public Works Department and in accordance with the approved plan, inspected and approved.
 - d. Any damage to the alley or sidewalk from utility installation or construction vehicles shall be repaired under permit from the Public Works Department, inspected and approved.
 - e. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
 - f. That the bicycle parking facilities shall be installed according to the approved plan, inspected and approved by the Staff Advisor.
 - g. That the required 22-foot clear back-up area shall be provided behind parking spaces along the alley, as illustrated on the applicant's Sheet AS1.0 dated December 15, 2020. The required back-up area may include the full, improved width of the alley.

Haywood Norton, *Chair*
Planning Commission Approval

February 9, 2021
Date

**TYPE II
PUBLIC HEARING**

**PA-T2-2020-00025
Independent Way, Tax Lot #600**



PLANNING ACTION: PA-T2-2020-00025

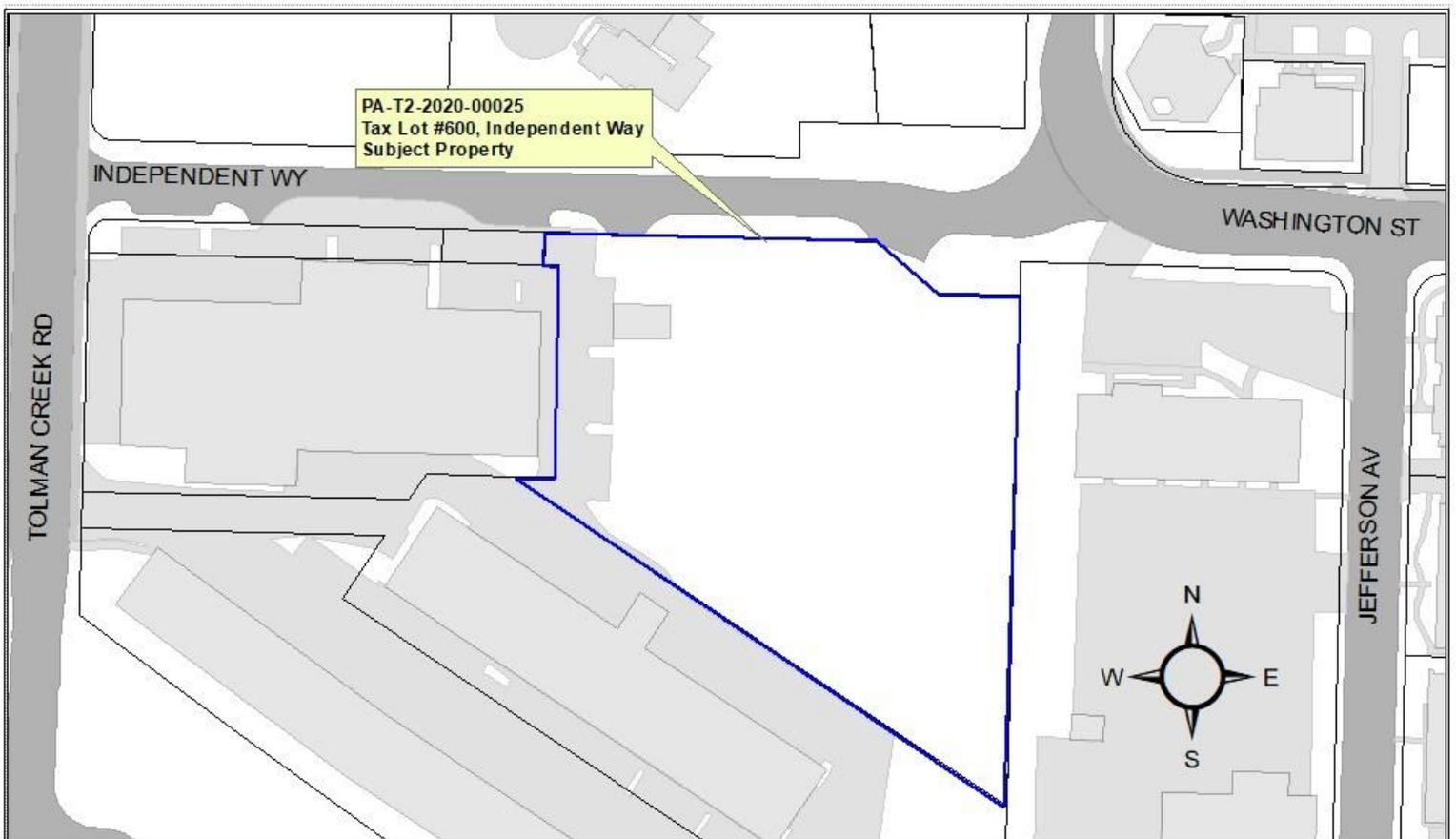
SUBJECT PROPERTY: Tax Lot #600 on the newly constructed Independent Way

APPLICANT/OWNER: Rogue Planning & Development Services/IPCO Development Corporation

DESCRIPTION: A request for Site Design Review approval for the construction of two new commercial/industrial buildings on Tax Lot #600 adjacent to Independent Way, the newly installed public street between Washington Street and Tolman Creek Road. Both buildings would be part of the IPCO Development Corporation service building complex, and would share driveway accesses, parking areas and landscaped areas. The first building is proposed to be 10,919 square feet and would be constructed adjacent to Independent Way. The second proposed building would be 17,859 square feet and would be near the south property line. The application includes a request for an Exception to the Site Development and Design Standards (AMC 18.4.2.040.B.3.a) which call for a ten-foot wide landscape buffer between the building and the street.

COMPREHENSIVE PLAN DESIGNATION: Employment; **ZONING:** E-1; **ASSESSOR'S MAP #:** 391E14BA; **TAX LOT:** 600

ELECTRONIC ASHLAND PLANNING COMMISSION MEETING: *Tuesday February 9, 2021 at 7PM*



Notice is hereby given that the Ashland Planning Commission will hold an electronic public hearing on the above described planning action on the meeting date and time shown above. You can watch the meeting on local channel 9, on Charter Communications channels 180 & 181, or you can stream the meeting via the internet by going to rvtv.sou.edu and selecting 'RVTV Prime.'

The ordinance criteria applicable to this planning action are attached to this notice. Oregon law states that failure to raise an objection concerning this application, or failure to provide sufficient specificity to afford the decision makers an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

Because of the COVID-19 pandemic, application materials are provided online and written comments will be accepted by email. Alternative arrangements for reviewing the application or submitting comments can be made by contacting (541) 488-5305 or planning@ashland.or.us.

A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report will be available on-line at www.ashland.or.us/PCpackets seven days prior to the hearing. Copies of application materials will be provided at reasonable cost, if requested. Under extenuating circumstances, application materials may be requested to be reviewed in-person at the Ashland Community Development & Engineering Services Building, 51 Winburn Way, via a pre-arranged appointment by calling (541) 488-5305 or emailing planning@ashland.or.us.

Anyone wishing to submit comments can do so by sending an e-mail to PC-public-testimony@ashland.or.us with the subject line "**February 9, 2021 PC Hearing Testimony**" by 10:00 a.m. on **Monday, February 8, 2021**. If the applicant wishes to provide a rebuttal to the testimony, they can submit the rebuttal via e-mail to PC-public-testimony@ashland.or.us with the subject line "**February 9, 2021 PH Hearing Testimony**" by 10:00 a.m. on **Tuesday, February 9, 2021**. Written testimony received by these deadlines will be available for Planning Commissioners to review before the hearing and will be included in the meeting minutes.

Oral testimony will be taken during the electronic public hearing. If you wish to provide oral testimony during the electronic meeting, send an email to PC-public-testimony@ashland.or.us by 10:00 a.m. on **Tuesday February 9, 2021**. In order to provide testimony at the public hearing, please provide the following information: 1) make the subject line of the email "**February 9 Speaker Request**", 2) include your name, 3) the agenda item on which you wish to speak on, 4) specify if you will be participating by computer or telephone, and 5) the name you will use if participating by computer or the telephone number you will use if participating by telephone.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact staff project planner Derek Severson at 541-535-5305 or derek.severson@ashland.or.us.

SITE DESIGN AND USE STANDARDS

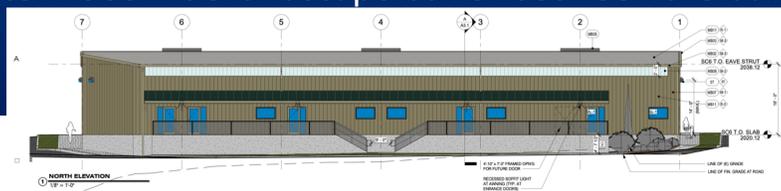
18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards:** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
 - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

Independent Way, Tax Lot #600

PA-T2-2020-00025 – A request for Site Review approval to build two new commercial/industrial buildings on the vacant property adjacent to Independent Way, the newly installed public street between Tolman Creek Road and Washington Street. The two buildings would be part of the IPCO Development Corporation service building complex, and would share driveway accesses, parking areas and landscaped areas. The applicant's 'Building 6' is proposed to be 10,919 square feet and would be constructed adjacent to Independent Way; the applicant's 'Building 5' would be 17,859 square feet and placed behind Building 6, near the south property line. The application also requests an Exception to the requirement to provide a ten-foot wide landscape buffer between the building and the street.



Proposal

The Planning Commission considered the construction of Independent Way, which included the placement of driveways, general location of building pads, grading for site circulation and extension of utilities to enable the further development of the subject property back in 2015. The current request proposes the first new building (the 10,919 square foot Building 6 here) fronting on Independent Way as well as an additional building (the 17,859 square foot Building 5) immediately behind Building 6 as part of the broader IPCO Development Corporation service building complex.

Site Description

The subject property is approximately 2.07 acres, and is located on the south side of the newly constructed Independent Way, a neighborhood commercial collector street between Tolman Creek Road and Washington Street. Hamilton Creek, an intermittent and ephemeral stream, is located along the east boundary of the subject property. Beyond the creek corridor, the subject property's natural features are limited to parking lot trees planted wither previous development of the IPCO service building complex.

Landscaping & Trees

The application includes a tree-inventory and protection plan to address the existing parking lot trees. Staff have also recommended that tree protection be provided for the newly planted street trees in the planting strip along Independent Way. The application materials include a landscape plan detailing the proposed landscaping with a mix of trees and shrubs distributed around the proposed buildings and parking areas.

Independent Way, Tax Lot #600

PA-T2-2020-00025 – A request for Site Review approval to build two new commercial/industrial buildings on the vacant property adjacent to Independent Way, the newly installed public street between Tolman Creek Road and Washington Street. The two buildings would be part of the IPCO Development Corporation service building complex, and would share driveway accesses, parking areas and landscaped areas. The applicant's 'Building 6' is proposed to be 10,919 square feet and would be constructed adjacent to Independent Way; the applicant's 'Building 5' would be 17,859 square feet and placed behind Building 6, near the south property line. The application also requests an Exception to the requirement to provide a ten-foot wide landscape buffer between the building and the street.

Parking

The total proposed square footage of industrial building area is 28,788 square feet, which requires 29 off-street parking spaces. The applicant proposes to provide 32 off-street spaces for the proposed building, which could accommodate future changes in use, as well as an additional eight off-street spaces for the IPCO building to the west. A total of 40 off-street spaces are proposed, including 29 standard spaces, 8 compact spaces, and three ADA-accessible spaces. The application also notes that areas along the driveway curb along Hamilton Creek reserved for truck staging could transition to parking in the future if additional parking we needed to accommodate an intensification of use of the site.

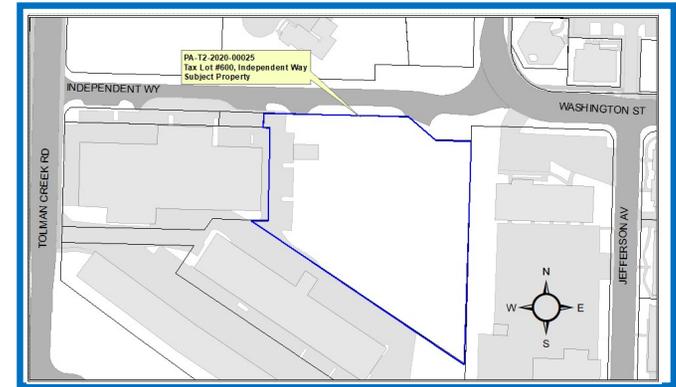
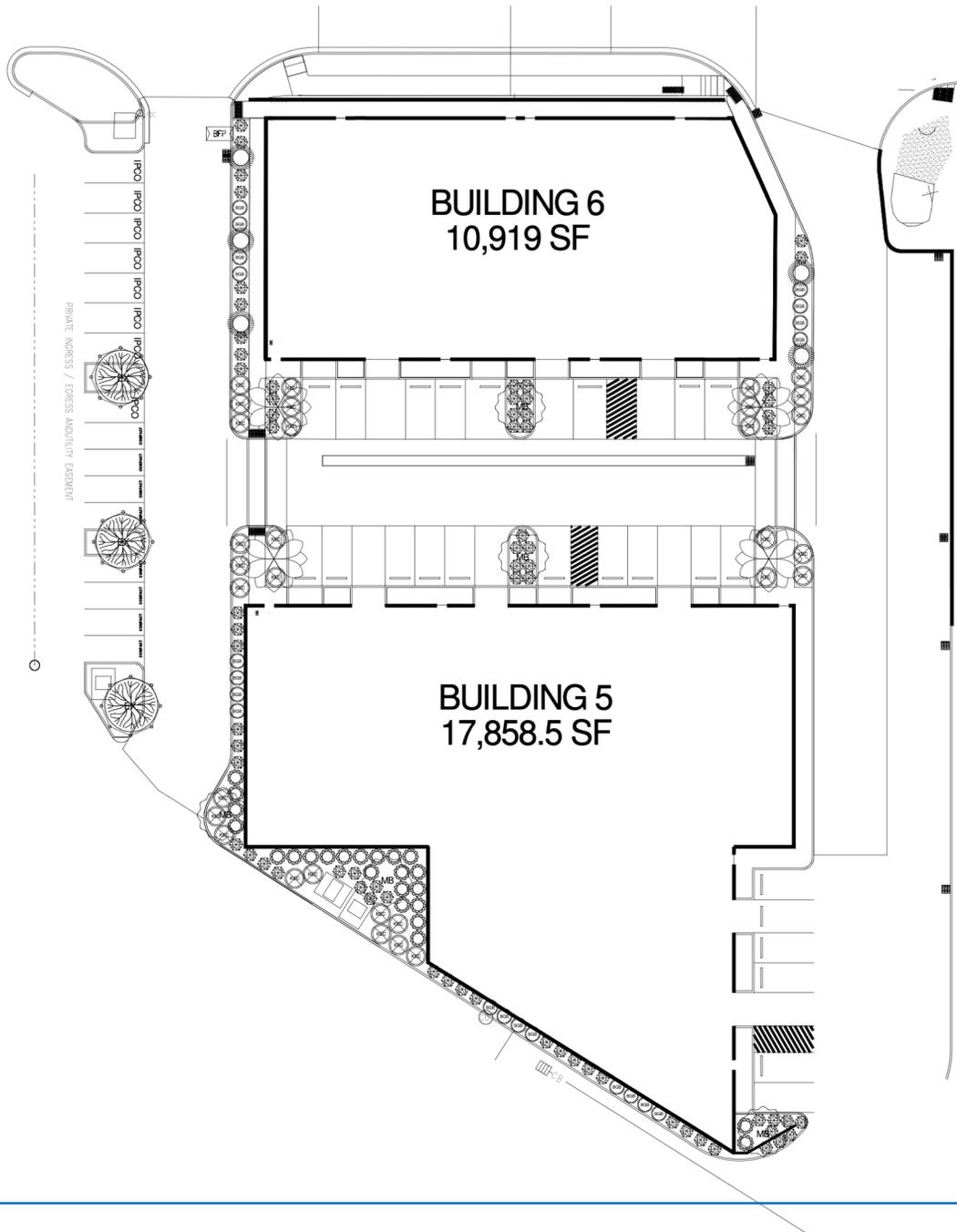
Exception Request

City site design standards call for a ten-foot wide landscaping buffer to be provided between the building and street corridor. The application requests an Exception from this standard, explaining that it will allow the entrance to be placed nearer to the sidewalk and accommodate improved circulation on site for large vehicles and consistent grading across the site. The standard seeks high-quality development that makes a positive contribution to the streetscape to encourage walking, bicycling and transit use while maintaining a sense of place that is uniquely Ashland. For staff, the request does not meet the standards for an Exception, and a condition is recommended to require that the full ten-foot width landscape buffer be provided along the streetscape.

Staff Recommendation

Staff recommends that the application be approved with the conditions detailed in the draft findings included in the Planning Commission's February 2021 meeting packet.

Independent Way, Tax Lot #600



Vicinity Map



Building 6 from Independent Way



Driveway & Parking between Buildings 5 & 6

BEFORE THE PLANNING COMMISSION
March 9, 2021

IN THE MATTER OF PLANNING ACTION #PA-T2-2020-00025, A REQUEST FOR)
SITE DESIGN REVIEW APPROVAL TO CONSTRUCT TWO NEW COMMERCIAL/)
INDUSTRIAL BUILDINGS ON THE VACANT TAX LOT #600 ON INDEPENDENT)
WAY, THE NEWLY CONSTRUCTED PUBLIC STREET BETWEEN WASHINGTON)
STREET AND TOLMAN CREEK ROAD. BOTH BUILDINGS WOULD BE PART OF)
THE IPCO DEVELOPMENT CORPORATION SERVICE BUILDING COMPLEX AND)
WOULD SHARE DRIVEWAY ACCESSES, PARKING AREAS AND LANDSCAPING.)
THE FIRST BUILDING IS PROPOSED TO BE 10,919 SQUARE FEET AND WOULD) **FINDINGS,**
BE CONSTRUCTED ADJACENT TO INDEPENDENT WAY. THE SECOND WOULD) **CONCLUSIONS,**
BE 17,859 SQUARE FEET AND WOULD BE NEAR THE SOUTH PROPERTY LINE.) **& ORDERS**
THE APPLICATION INCLUDES A REQUEST FOR AN EXCEPTION TO THE SITE)
DEVELOPMENT AND DESIGN STANDARDS (AMC 18.4.2.040.B.3.A) WHICH CALL)
FOR A TEN-FOOT WIDE LANDSCAPE BUFFER BETWEEN THE BUILDING AND)
THE STREET.)

APPLICANT/OWNERS: Rogue Planning & Development Services, LLC/)
IPCO Development Corporation)
)

RECITALS:

- 1) Tax lot #600 of Map 39 1E 14BA is a vacant 2.07 acre parcel and is zoned Employment (E-1).
- 2) The applicant is requesting Site Design Review approval for the construction of two new commercial/industrial buildings on Tax Lot #600 adjacent to Independent Way, the newly installed public street between Washington Street and Tolman Creek Road. Both buildings would be part of the IPCO Development Corporation service building complex, and would share driveway accesses, parking areas and landscaped areas. The applicant’s ‘*Building 6*’ is proposed to be 10,919 square feet and would be constructed adjacent to Independent Way. The applicant’s proposed ‘*Building 5*’ would be 17,859 square feet and would be placed behind ‘*Building 6*’, near the south property line. The application includes a request for an Exception to the Site Development and Design Standards (AMC 18.4.2.040.B.3.a) which call for a ten-foot wide landscape buffer between the building and the street. The proposal is outlined in plans on file at the Department of Community Development.
- 3) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:
 - A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
 - B. **Overlay Zones:** *The proposal complies with applicable overlay zone requirements (part 18.3).*

- C. **Site Development and Design Standards:** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
- D. **City Facilities:** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*
- E. **Exception to the Site Development and Design Standards.** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.*
 - 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*
 - 2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*

4) On April 15, 2020 Governor Kate Brown issued Executive Order #20-16 “Keep Government Working: Ordering Necessary Measures to Ensure Safe Public Meetings and Continued Operations by Local Government During Coronavirus (COVID-19) Outbreak.” The Governor’s Order required that public bodies hold public meetings by telephone, video, or through some other electronic or virtual means, whenever possible; that the public body make available a method by which the public can listen to or virtually attend the public meeting or hearing at the time it occurs; that the public body does not have to provide a physical space for the public to attend the meeting or hearing; that requirements that oral public testimony be taken during hearings be suspended, and that public bodies instead provide a means for submitting written testimony by e-mail or other electronic methods that the public body can consider in a timely manner. The subsequently adopted House Bill #4212 further authorized governing bodies in Oregon to conduct all public meetings using telephone or video conferencing technology or through other electronic or virtual means.

8) The Planning Commission, following proper public notice, held an electronic public hearing on February 9, 2020. In keeping with Executive Order #20-16 and subsequent House Bill #4212, this meeting was broadcast live on local television channel 9 and on Charter Communications channels 180 & 181, and was live-streamed over the internet on RVTV Prime at <http://www.rvtv.sou.edu>. A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report were made available on-line seven days prior to the hearing. Those wishing to provide written testimony were able to submit it via e-mail in advance of the hearing, as detailed in the mailed and posted notices, and all written testimony received by the deadlines was made available for Commissioners to review before the hearing and was included in the meeting minutes. In addition, those wishing to participate during the hearing could arrange to provide oral testimony by making arrangements to do so in advance of the meeting. Following the closing of the public hearing and the

record, the Planning Commission considered the materials received and testimony presented and approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the application materials, staff report, public testimony and exhibits received.

2.2 The Planning Commission finds that the proposal for Site Design Review approval meets the applicable criteria for Site Design Review described in AMC 18.5.2.050.

2.3 The Planning Commission concludes that the proposal satisfies all applicable criteria for Site Design Review approval.

The first approval criterion for Site Design Review approval addresses the requirements of the underlying zone, requiring that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”*

The application materials explain that the subject property and all adjacent properties are zoned E-1 (Employment). There are no minimum setbacks within the E-1 zone, and the application explains that the proposed setbacks are the minimum necessary. Along the newly constructed street Independent Way, the applicant’s Building 6 is proposed to have a five-foot setback from the public street, and has been designed to comply with Solar Access Standard B, which allows the structure to cast the same shadow that would be cast by a 16-foot tall fence constructed on the north property line. Within the E-1 zoning district, 40 feet is the maximum building height; here, Building 6 is proposed at just over 20 feet in height, while Building 5 is proposed to be 22 feet tall.

The application materials further explain that the proposed lot coverage is less than the allowed 85 percent in the zone, as the 2.07-acre parcel is proposed to have total lot coverage of approximately 69,495 square feet, or 77.08 percent. 28,775 square feet of this coverage is building footprints, while approximately 40,718 square feet is paved. There will be approximately 4,952 square feet of new landscaped areas within the parking areas proposed.

The property is not located within a Residential-overlay, and as such no residences are proposed and residential density is not considered. Similarly, Floor Area Ratios (FAR) are not considered outside of the Detail Site Review zone. Building 6 occupies the majority of the property frontage, placing the wider side of the building to the street, and is accessed directly from the sidewalk via a centralized stairway that extends from the sidewalk to the raised walkway along the structure's facade. The building has architectural details common to metal buildings in the Employment zoning district. The application materials further note that to allow for potential intensification of uses, at the front of the building an entry/exit door is framed but not installed to preserve the future possibility of creating an additional tenant space, and on the south side of Building 6, area for future windows has been accommodated in the design.

Building 5 is substantially more than 20-feet from the public street, and as such is not required to be oriented to Independent Way. Building 5 incorporates additional areas for openings for roll-up doors and pedestrian entrances on both its front façade (north side) and east side.

The Planning Commission finds that the building and yard setbacks and other applicable standards have been evaluated to ensure consistency with the applicable provisions of part 18.2, and all regulations of the underlying E-1 zoning will be satisfied.

The second approval criterion deals with overlay zones, and requires that, *"The proposal complies with applicable overlay zone requirements (part 18.3)."* The Planning Commission finds that the property contains a reach of Hamilton Creek near the east property line, and as such is subject to both the Physical & Environmental Constraints Overlay (AMC 18.3.10.080) for flood plain corridor lands and to the Water Resources Protection Zones Overlay (AMC 18.3.11) for Hamilton Creek as an "Intermittent and Ephemeral Stream." The Planning Commission further finds that the property is contains wildfire lands and as such is subject to the standards in AMC 18.3.10.100.

The application materials explain that the property contains a Water Resource Protection Zone (WRPZ) and a FEMA Special Flood Hazard Area floodplain for Hamilton Creek, and further notes that Hamilton Creek exits from a 60-inch culvert along the east property line. The proposed development, excepting very small areas of the driving and parking area, is setback more than 30-feet from the mapped centerline of Hamilton Creek. The application further notes that the 2015 Site Review application (PA #2015-00422) which approved the installation of Independent Way included a Limited Activities and Uses permit for the bridge crossing and also permitted a small area of encroachment into the WRPZ to accommodate an area of the drive aisle curbing and a bio-swale/detention area. The application materials assert that the proposal complies with the 2015 approval, and that there are no additional impacts to the WRPZ or floodplain with the development proposed.

The Planning Commission finds that the subject property is also subject to the Physical & Environmental Constraints Overlay standards for wildfire lands, and as such a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 will need to be provided for the review of the Fire Marshal prior to bringing combustible materials onto the property, and any new landscaping proposed will need to comply with these standards and shall not include plants listed on the 'Prohibited Flammable Plant List' per Resolution #2018-028. A condition has been included below to require a final Fire Prevention and Control Plan and plant list be provided for the review and approval of the Fire Marshal prior to the issuance of a building permit or to bringing any combustibles onto the site.

Based on the foregoing, the Planning Commission finds that this criterion dealing with overlay zone requirements is satisfied with the proposal.

The third criterion addresses the Site Development and Design Standards, requiring that “The *proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*” In this instance, the subject property is outside of the Detail Site Review and Historic District overlay zones, and as such the applicable standards are the ‘*Building Placement, Orientation and Design Standards*’ from Chapter 18.4.2, and specifically the standards for Non-Residential Development in AMC 18.4.2.040; the Parking, Access & Circulation standards in Chapter 18.4.3; the Landscaping, Lighting and Screening standards in Chapter 18.4.4; and the Tree Preservation and Protection standards in Chapter 18.4.5.

Basic Site Review Standards for Non-Residential Development (AMC 18.4.2.040)

In addressing these standards, the application materials note that proposed Building 6 is oriented towards Independent Way which is newly constructed with sidewalks and parkrow planting strips with street trees in place. The proposed design provides for direct pedestrian access from the public street to the entrances of the building. The pedestrian entrances are accessed via a raised walkway served by a wide, central stair that leads directly from the public sidewalk to the walkway and entrances. Each pedestrian entrance is clearly visible from the street with commercial entry doors, sidelight windows, awnings to provide pedestrian coverage from sun and rain, and lighting that all enhance the sense of entry to the tenant spaces. There is no automobile circulation or off-street parking between the building and the street. Parking areas are proposed to be placed to the side and rear of the street-fronting building.

The application continues, noting that the majority of the property’s street frontage is occupied by the proposed Building 6, and that the driveways are to the sides of the building allowing positioning of the wider side of the building to the street with no gaps in the frontage. Driveway aprons, vehicle aisles and parking areas are to the sides and rear of the building.

The application materials explain that the proposed site plan does not include the required ten-foot landscape buffer adjacent to the street. The application materials suggest that the purchase agreement when the city acquired right-of-way to construct the new street, shared understandings were detailed with the purchase including that with planning approval, the required landscape buffer would instead be the landscape park row and that since the park row was installed at seven feet, three feet of landscape

buffer remain. The application includes a request for an Exception to eliminate the required landscape buffer entirely which is addressed further below. A size-, species- and planting-specific landscape and irrigation plan will be provided for the review and approval of the Staff Advisor with the building permit submittal.

Refuse and recycle containers are to be located within the buildings and placed outside for pick-up on garbage day each week. The application explains that this is how the majority of the tenants of the property operate, that the arrangement is formalized in the lease agreements, and has worked well for the property owner and for Recology.

The application materials point out that proposed exterior lights are to be “dusk to dawn” LED lights recessed under the awnings and downward directed to avoid directly illuminating adjacent properties. Noises generated by the site are anticipated to be consistent with what can be expected in an Employment zone where uses may include production, manufacturing, and repair.

Parking, Access & Circulation (AMC 18.4.3)

The parking ratio industrial, manufacturing, production, warehousing, and freight uses is the lesser of one parking space per 1,000 square feet of gross floor area or one space for every two employees, plus one space for a company vehicle. Based on the 28,778 square feet of new building area proposed, a total of 29 parking spaces are required ($28,778/1,000 = 28.778$). The application materials note that 32 off-street parking spaces are proposed to address the parking demand here, along with an additional eight spaces proposed to serve the applicant’s Building 1 on the adjacent property. In total, there are 40 off-street parking spaces provided including 29 standard spaces, eight compact spaces and three ADA spaces. The application materials note that the minimum required back-up area of 22-feet is available for each parking space, and that the parking area will be developed to address requirements for landscaping, shade trees, micro-climatic impacts and storm water quality management further explaining that the parking lot has been designed to minimize adverse environmental impacts through the use of a bio-swale filtration as provided in the Rogue Valley Stormwater Design Manual. While the application materials indicate that the design minimizes the micro-climatic and environmental impacts of the parking area, the Commission finds that it is unclear which of the strategies in AMC 18.4.3.080.B.5.a is proposed, and a condition has accordingly been added to require that the building permit submittal clearly address which of these standards (i.e. light-colored or porous paving, additional shade through structures or extra trees) is to be relied upon in the final design.

All of the IPCO Development Corporation Service Building sites are interconnected and accommodate semi-truck and other vehicular traffic through the properties. The new parking area is proposed to be accessed from the driveways that extend from Independent Way. With the new development, pedestrian access is extended from Independent Way along both sides of the proposed Service Building #6 and extends to the entrances of Service Building #5. The sidewalks are raised when crossing drive aisles as required by code. The parking areas provide for adequate back-up and turn around area is provided for on the site plans.

The application materials further note that the area along the curb adjacent to the Hamilton Creek corridor are intended to provide for semi-truck staging on-site, but have the potential to be restriped for parking if the use of the property were to intensify. Bicycle parking is proposed to be located within the buildings.

Tree Preservation & Protection (AMC 18.4.5)

The application explains that in addition to the newly-planted street trees, there are parking lot shade trees along the west property line that will be protected from construction impacts. There are also existing trees in the riparian drainage area on the east side of the driveway and parking area. A tree protection fencing plan has been provided to address the three trees in the parking area. The application details the placement of requisite tree protection fencing in the form of six-foot tall, chain link fencing at the driplines of the trees identified on the provided tree inventory, and further notes that silt fencing will be provided to prevent erosion into the Water Resource Protection Zone before site disturbance. The application recognizes that fencing will need to be installed flush with grade and inspected by the Staff Advisor prior to any site work, and further indicates that no construction activity or excavation will occur within the identified tree protection zones and that no building supplies, soil, equipment, vehicle parking or waste, including chemically injurious materials or liquids, construction debris, run-off, or excess concrete excess, will be allowed in the tree protection zones.

The Commission finds that the newly-planted street trees in the park row planting strip on Independent Way will also need protection during site development, and a condition has been required to require a revised tree protection plan which also addresses the street trees shall be provided for the review and approval of the Staff Advisor with the building permit submittals.

Based on the foregoing, the Planning Commission concludes that other than the single Exception requested, which is discussed further below, the proposal complies with the applicable Site Development and Design Standards of part 18.4.

The fourth approval criterion addresses city facilities, specifically requiring that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.”*

The application indicates that public infrastructure was extended with the construction of Independent Way to serve the subject property, noting that required improvements for a neighborhood commercial collector street including pavement, curb, gutter, a six-foot sidewalk, a seven-foot-wide landscape park row with street trees, and streetlights were installed and utilities were extended. The application materials further detail that utilities in place include an eight-inch water main, an eight-inch sanitary sewer main and a 12-inch storm sewer main within the Independent Way right-of-way. In addition, the application indicates that large electric transformers were installed with previous site work as well as the street installation, providing sufficient electric service capacity to support underground electric service to serve development of the property, and that associated private utility easements were extended through the property with these installations. The application concludes by noting that the installation

of adequate utilities to serve the property was contemplated with the development of Independent Way, and the civil engineer who designed the street extension is also the engineer of record for the current application. Public Works and Engineering staff have confirmed that adequate capacity of utilities to enable the envisioned development of the site was planned and installed with the Independent Way project.

The Planning Commission finds that the site's utilities will be extended to and through the subject property from public utility easements and street right-of-way adjacent to the site, and that based on the findings and conceptual plans provided, adequate key city facilities are available within the adjacent rights-of-way and will be extended by the applicant to serve the proposed development. Conditions have been included below to require that final electric service, utility and civil plans be provided for the review and approval of the Staff Advisor and city departments in conjunction with the building permit submittals, and that civil infrastructure be installed by the applicants, inspected and approved prior to final project approval.

The Planning Commission finds that adequate capacity of city facilities, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property. The Commission concludes that this criterion has been satisfied.

The final criterion for Site Design Review approval addresses "Exception to the Site Development and Design Standards." The application includes a request for a single Exception to the Site Development and Design Standard in AMC 18.4.2.040.B.3.a which requires that, "*Landscape areas at least ten feet in width shall buffer buildings adjacent to streets, except the buffer is not required in the Detail Site Review, Historic District, and Pedestrian Place overlays.*" The subject property is not within a Detail Site Review, Historic District or Pedestrian Place overlay, but an Exception is requested to not provide the required buffer.

The applicable criterion provides that:

The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist:

- 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site, and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty; or*
- 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards;*

Exception Discussion

Request

The application materials assert that when the City of Ashland purchased the property from the current property owners in 2012 to construct the new street, the purchase agreement stipulated that the required landscape buffer would be accommodated within the park row. The application includes a copy of the “Letter of Intent for Sale of Real Property” dated December 13, 2012. Item #3a on page 1 of 4 in the letter notes in part that, “... *Buyer agrees, subject to Planning approval, to allow seller to credit park row landscape for sellers required landscape associated to any future development on said property.*” With the installation of Independent Way, the park row planting strip along the subject property’s frontage was installed at a width of seven feet, which the application suggests leaves an additional three feet of buffer width required. The applicant requests exception from the standard entirely, proposing not to install any portion of the required buffer along the frontage of the property, and explains that the proposed building has been designed with a direct pedestrian connection to the public sidewalk and that there are numerous issues with transients camping and loitering on the property and increased landscape areas encourage loitering.

The application materials provided respond to subsection 2 of the criterion above, explaining that there is a landscape buffer at the west side of the structure and between the western driveway access but not along the building frontage. Additionally, the application suggests that due to the riparian buffer zone, there is an abundant landscaped area provided on the property, substantially more than a typical Employment zoned property. The application materials further assert that not providing a landscape buffer in the zone where no setbacks are required, does not reduce public health or safety and suggests that the wall adjacent to the sidewalk will not harm the livability of the adjacent employment zoned lands as there is no residential zoning. The application materials indicate that the proposed building will make a positive contribution to the streetscape and that the area is not a highly-traveled arterial street but is rather a manufacturing and industrial area where the application materials suggest standards can be flexed to achieve the purpose of the zone which is described as functional, employment zoned land that has access and parking area for large vehicles and business traffic.

The application materials go on to discuss that when the street connection application was made, minor concessions associated with the street design were made to provide a major, City of Ashland TSP driven connectivity project. The application suggests that the reduced buffer is minor when considering the substantial compliance with other standards of the Site Design and Use Standards, the Water Resource and Riparian Protection Zone buffer, grading of the existing site determining the finished grade of proposed site development and the large investment into flexible space for large or small warehouse, storage, distribution, and production facilities. The parking lot layout and the number of spaces provided on-site allow for intensification of the site uses in close proximity to the Ashland Street commercial corridor and Exit 14.

The application materials explain that the entrances for Building 6 are oriented to the public right-of-way and are within 20 feet of the sidewalk. The proposal seeks an Exception to eliminate the

required landscape buffer along the frontage of the property adjacent to the public street to locate the entry closer than 20-feet of the right-of-way, and suggests that the reduced landscape buffer allows for improved large vehicle access to the loading/unloading areas, parking areas and vehicle maneuvering areas. The proposed reduced buffer allows for a wider than standard 22-foot back-up area to allow better turning movements for the large vehicles that presently utilize the IPCO site and will be utilizing the proposed service buildings.

The application asserts that in the Employment Zone, it is critical for the site grades to allow easy access for delivery vehicles, forklifts, pallet movers, etc., cross accessing the internal spaces and the IPCO properties' retention of slight cross-grade, a single finished floor level within the structures, and the existing grades of the developed portions of the site dictated the grade of the building site area. The application materials point out that based on the civil drawings from the 2015 Site Review there was 6½- to 8½-feet of grade change from the back of the sidewalk to the building pad. This grade change at the back of the sidewalk and the need for the site to have a relatively consistent grade from this tax lot to the other tax lots in the IPCO Service Center complex are the reasons this building is raised above the sidewalk with a retaining wall along the frontage.

The application materials emphasize that there is no on-street parking on this side of the street, that the property is Employment-zoned, and that the businesses that presently occupy the majority of the IPCO Service Center are not pedestrian-oriented but instead rely heavily upon delivery traffic. The application concludes that even so, the proposal provides a clear, distinct pedestrian entrance via stairs directly from the sidewalk to the Building #6 entry.

Staff Response

For staff, it is first important to note that a sales agreement could not legally grant a discretionary land use approval outside of the required land use hearing process, and in this case, contrary to the assertion in the application materials, the agreement did not stipulate that the parkrow landscaping would be credited to offset the buffer requirement but rather provided that the buyer (the City) was agreeable with crediting parkrow landscaping toward the overall site landscaping requirements provided that the seller (the current applicant) could obtain the requisite land use approvals. In staff's reading of the Letter of Intent, it appears that the intent was to allow parkrow landscaping to be credited to the 15 percent site landscaping requirements for E-1 zoned property rather than to allow park row landscaping, which is part of the required street corridor improvements, to be substituted for a design standard-required buffer between the street corridor and the building. This would be generally consistent with AMC 18.4.6.050.C. "*Non-conformities Created by Street Dedication*" which provides that, "*When the lot area or setbacks of a lot that conforms to the requirements of the applicable zoning district are reduced ...as a result of dedication of right-of-way for improvement of a street, the remaining lot is deemed in compliance with the...lot coverage... requirements of the zone.*" In staff's reading of the code, the lot coverage requirements in AMC Table 18.2.6.030 are not the same as the design standards-required buffer in AMC 18.4.2.040.B.3.a.

The application materials also speak to minor concessions having been made in the original street design as it was a city project. Independent Way is considered a Commercial Neighborhood Collector, and while Exceptions to the Street Design Standards were initially requested in PA-2015-00422 to eliminate park rows on the north side of the street, staff noted at that time that *“Given the street’s anticipated role as a neighborhood commercial collector likely to serve the Washington Street and Croman employment areas, its anticipated level of vehicle trips with build-out in the area, and the truck circulation associated with the anticipated development of the applicants’ properties, staff are unable to support any Exception to the sidewalk to be constructed on the north side. Staff believes it would be difficult to find that a substandard corridor is ‘equal or superior’ or that the width reduction is necessary to alleviate any difficulty when the street is new and the right-of-way proposed is of a width intended to accommodate standard improvements. Staff has accordingly recommended a condition to require that the full 13-foot width - not counting curb - pedestrian corridor be provided on the north side.”* The decision ultimately required full city-standard improvements on the north side, with the exception of not requiring a parkrow with street trees on the bridge crossing itself to limit impacts in the water resource protection zone and floodplain. While south side sidewalks were not considered in that application, conditions nonetheless included that any additional southside improvements meet city standards and that future development applications consider the remaining sidewalk sections on the south side.

In staff’s assessment, the current request must be considered in light of current standards for an Exception to the Site Development and Design Standards independent of the previous application or the Letter of Intent associated with the city’s purchase of the right-of-way. For staff, while the application makes clear why the applicant wishes to forgo the requisite landscape buffer, it does not clearly establish that either the site’s grades or the intended use’s need to accommodate large truck circulation prevents providing the full ten-foot wide landscaping buffer on a more than two acre site, or that the exception would be consistent with the stated purpose of the chapter which includes to *“Enhance the environment to encourage alternative modes of transportation, such as walking, bicycling, and transit”* and to *“Require high quality development that makes a positive contribution to the streetscape and maintains a sense of place that is distinctly Ashland.”* Absent a stronger demonstration that the site cannot accommodate the full-width buffer, staff cannot support the Exception request.

The Planning Commission finds that the Exception request fails to meet the approval criteria, and the Exception is denied. A condition has been included below to require that a full ten-foot width landscape buffer be provided between the building and the sidewalk in the building permit submittals.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review approval to construct two new industrial buildings along the newly-constructed Independent Way is supported by evidence contained in the whole record. For the Commission, the primary issue with the request is the proposed Exception to the Site Development and Design Standards seeking not to provide the required ten-foot wide landscape buffer between the building and the street called for in AMC 18.4.2.040.B.3.a. For the Commission, the standards broadly and this standard specifically seek to “*Enhance the environment to encourage alternative modes of transportation, such as walking, bicycling, and transit*” and to “*Require high quality development that makes a positive contribution to the streetscape and maintains a sense of place that is distinctly Ashland.*” The required buffer would positively contribute to the pedestrian streetscape, buffering the industrial building adjacent to the sidewalk which is particularly important given that the building will sit above the sidewalk with a blank retaining wall topped by railing. Ultimately the Commission cannot find that either the site’s grade or the intended truck-focused site circulation prevents providing a buffer, and as such the Commission denies the Exception request and has included a condition that the full buffer be provided in the building permit submittals.

With that, the Commission concludes that the development merits approval with the conditions detailed below. Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2021-00025. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2021-00025 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this Site Design Review approval shall be submitted and approved prior to issuance of a building permit.
3. That any new addresses shall be assigned by City of Ashland Engineering Department.
4. That permits shall be obtained from the Ashland Public Works Department prior to any work in the public right of way, including but not limited to permits for new driveway approaches or any necessary encroachments.
5. That the windows on the ground floor shall not be tinted so as to prevent views from into the interior of the building.
6. That the front entrances adjacent to Independent Way shall remain functional and open to the public during all business hours.
7. That a sign permit shall be obtained prior to installation of any new signage. Signage shall meet the requirements of Chapter 18.4.7.
8. That the building permit submittal shall include:
 - a. Identification of all easements, including public and private utility easements, public pedestrian access easements, and fire apparatus access easements.

- b. Solar setback calculations demonstrating that all new construction complies with Solar Setback Standard B in the formula $[(\text{Height} - 16) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$ and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and the height(s) from natural grade.
- c. Lot coverage calculations including all building footprints; driveways, parking, and circulation areas; and any other areas other than natural landscaping. Lot coverage shall be limited to no more than 85 percent as required in AMC 18.2.6.030.
- d. Final electric service, utility and civil engineering plans including grading, erosion control and drainage. All civil infrastructure shall be installed by the applicants, inspected and approved prior to final inspection/occupancy approval.
- e. The final utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer mains and services, manholes and clean-outs, and storm drainage pipes and catch basins, along with any backflow prevention measures required by the Water Department. Any required private or public utility easements shall be delineated on the civil plans.
- f. The final electric design and distribution plan shall include load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment with the Final Plan application. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department.
- g. That storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals. The storm drainage plan shall detail the location and final engineering for all storm drainage improvements associated with the project, and shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions. The storm drainage plan shall demonstrate that post-development peak flows are less than or equal to the pre-development peak flow for the site as a whole, and that storm water quality mitigation has been addressed through the final design.
- h. Final site lighting details.
- i. A final size- and species-specific landscaping plan including irrigation details satisfying the Water Conserving Landscaping Guideline in AMC 18.4.4.030.I. New landscaping shall comply with the General Fuel Modification Area requirements and shall not include plants listed on the Prohibited Flammable Plant List adopted by Resolution #2018-028. All landscaping shall be installed according to the approved plan, and tied into the existing irrigation system, inspected and approved prior to the issuance of a certificate of occupancy.
- j. That a revised Tree Protection Plan consistent with the standards described in 18.4.5 be submitted for review and approval of the Staff Advisor prior to the issuance of a building permit. The plan shall identify the location and placement of fencing around the drip lines of trees identified for preservation and shall include the newly planted street trees in the

parkrow planting strip in front of the proposed Building 6 along Independent Way. The amount of fill and grading within the drip line shall be minimized. Cuts within the drip line shall be noted on the tree protection plan, and shall be executed by handsaw and kept to a minimum. No fill shall be placed around the trunk/crown root.

- k. That the requirements of the Ashland Fire Department relating to approved addressing; fire apparatus access and turn-around; a firefighter access pathway; fire flow; hydrant installation, spacing and clearance; work area; applicable fire sprinkler requirements; fire department connection; key box; extinguishers; limitations on obstructions to fire access; and wildfire hazard area and vegetation requirements shall be satisfactorily addressed in the permit submittals.
 - l. A Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 shall be provided prior to bringing combustible materials onto the property, and any new landscaping proposed shall comply with these standards and shall not include plants listed on the 'Prohibited Flammable Plant List' adopted with Resolution #2018-028.
 - m. The building permit submittals shall verify that the bicycle parking, spacing and coverage requirements are met in accordance with 18.4.3.070.I. Inverted U-racks shall be used for the bicycle parking, and all bicycle parking shall be installed in accordance with design and rack standards in 18.4.3.070.I and J, inspected and approved by the Staff Advisor prior to the issuance of the certificate of occupancy. If bicycle parking is to be provided within the proposed buildings, final interior dimensions of the dedicated bicycle parking areas shall be detailed on the building permit plans to insure adequate space has been provided. A bicycle parking space located inside of a building shall be a minimum of six feet long by three feet wide by four feet high, shall be accessible without moving another bicycle, and shall be clearly marked as reserved for bicycle parking only.
 - n. The building permit submittals shall be modified to reflect the full ten-foot width landscape buffer between the building and the sidewalk required in the Site Development and Design Standards.
 - o. The building permit submittals shall clearly identify which of the strategies in AMC 18.4.3.080.B.5.a is being utilized to minimize the environmental and micro-climatic impacts of the paved areas (i.e. light colored paving, porous paving, or additional shade through added tree canopy or structures).
9. That prior to the issuance of a building permit:
- a. That a Tree Verification Permit shall be applied for and approved by the Staff Advisor prior to permit issuance, tree removal, or any site work including demolition, staging, storage of materials, or excavation. The Tree Verification Permit is to inspect the identification of the tree to be removed and the installation of tree protection fencing for the trees to be protected on and adjacent to the site. The tree protection shall be chain link fencing six feet tall and installed in accordance with AMC 18.4.5.030.
 - b. Silt fencing or other approved means of stream corridor protection and erosion control shall be installed on-site, inspected in conjunction with the Tree Verification Permit, and approved by the Staff Advisor prior to permit issuance or site work.

10. That prior to the final inspection approval or issuance of a certificate of occupancy:
 - a. That all landscaping in the new landscaped areas, and the irrigation system, shall be installed according to the approved plan, inspected and approved by the Staff Advisor.
 - b. Any modifications to the driveway curb-cuts/approaches shall be installed under permit from the Public Works Department and in accordance with the approved plan, inspected and approved prior to the submittal of the final survey plat for signature. The driveway curb cut, apron and entry area shall be sized to standard turn-around dimensions as illustrated in AMC 18.4.6.040.G.5.
 - c. Civil improvements including but not limited to utility installations shall be completed according to approved plans, inspected and approved
 - d. That all exterior lighting shall be selected, placed and down-directed/shrouded so as not directly illuminate adjacent properties. Compliance shall be site-verified by the Staff Advisory prior to final inspection approval or issuance of a certificate of occupancy.
 - e. That the bicycle parking facilities shall be installed according to the approved plan, inspected and approved by the Staff Advisor.

Haywood Norton, *Chair*
Planning Commission Approval

March 9, 2021
Date

Applicant's Statement of Completeness

(To be completed by the Applicant and returned to the City of Ashland)

Re: **PA-T2-2020-00025, Independent Way**
Date Application Expires: **May 17, 2021**

Date Received 12.28.2020 (to be completed by staff)

Pursuant to an Incompleteness Determination, I, the undersigned applicant or agent for the applicant, elects one of the three options below by initialing:

(ADG) **1. Submit All of the Missing Information**
(Initial if elected)

I am submitting all of the information requested in the Incompleteness Determination letter.

Unless checked below, I am requesting that the City of Ashland Planning Division review this additional information within 30 days of submission to determine whether the application is complete. I understand that this 30-day review for completeness period for the new information preserves my opportunity to submit additional materials, should it be determined that the application is still incomplete after the second review. **(Note: The 120-day period for the City of Ashland's final determination of compliance with applicable criteria does not commence until the additional review for completeness period is completed.)**

Check if desired

I waive further review of the information submitted for completeness and direct review of the information submitted for compliance with the Community Development Code criteria, regardless of whether the application is, in fact, later determined by the staff to be incomplete.

I understand that by checking the above statement the application will be evaluated based upon the material submitted and no notice of any missing information will be given. If material information is missing from the application, the application will fail to meet the burden of showing that all criteria are met, and the application will be denied.



()

2. Submit Some of the Requested Information:

(Initial if elected)

Decline to Provide Other Information

I am submitting some of the information requested and declining to submit other information requested in the Incompleteness Determination letter. I understand that by declining to submit all information the City of Ashland believes necessary, the Ashland Planning Division may conclude that the applicable criteria are not met and a Denial will be issued or recommended.

()

3. Decline to Provide any of the Requested Information

(Initial if elected)

I decline to provide any of the information requested. I understand that the Community Development Department may conclude that the applicable criteria are not met and a Denial will be issued or recommended.

Amy Junter

Signed and Acknowledged

(Applicant or Applicant's Agent)

12/24/2020

Date

Return to:

City of Ashland, Planning Division
Attn: Derek Severson, *Senior Planner*
c/o City Hall, 20 East Main Street
Ashland, OR 97520



IPCO Development Corp

Site Review for Commercial Development IPCO Phase 2, Service Buildings 5 and 6

Independent Way



December 28, 2020

**Site Design Review for the construction
of two new commercial/industrial buildings in the Employment Zone**

Subject Property

Property Address: Independent Way (formerly part of 688 Tolman Creek Road)
Map: 39 1E 14BA
Tax lot: 600

Comprehensive Plan Designation: Employment
Zoning: E-1

Overlay Zones: Hamilton Creek FEMA Special Flood Hazard Area
Water Resources Protection Zone
Wildfire Hazards
Severe Constraints Slopes

Owner: IPCO Development Corporation
1425 Greenmeadows Way
Ashland, OR 97520

Design / Build: JB Steel Inc.
PO BOX 4460
Medford, OR 97501

Engineer: Thornton-Daley Engineering
Kirk Daley
PO BOX 476
Jacksonville, OR 97530

Land Use Consultant: Rogue Planning & Development Services
1314-B Center Dr., PMB 457
Medford, OR 97501

Request:

The request is for Site Design Review approval for the construction of two new commercial/industrial buildings as part of the IPCO Development Corporation service building complex. Service Building #5 is a 17,858.5 square foot commercial building. This structure is located near the south property line. Building #6 is proposed as a 10,919 square foot structure. Building #6 is proposed to be adjacent to the newly installed public street, Independent Way. Independent Way connects Washington Street to Tolman Road. Building #5 is proposed to be to the south of Building #6 with shared driveway accesses, parking areas, and landscape areas.

Property Description:

The subject property is a 2.07-acre parcel (Parcel 3) of a four-lot partition plat from 2002.

The trapezoidal-shaped lot has approximately 230 feet of frontage along Independent Way. The lot extends approximately 168 feet to the south on the west side of the lot and 355 feet south along the east side of the property. The rear property line abuts the property at 688 Tolman Creek Road which is a site developed as a warehouse space, parking, and driving surfaces.

Along the east property line is Hamilton Creek, an intermittent/ephemeral stream according to the Water Resource Protection Zone maps. Hamilton Creek is partially culverted along the east property line but daylight into the riparian preservation and creek restoration project area at the location of the new Independent Way bridge crossing of Hamilton Creek.

Hamilton Creek is a FEMA floodplain. A Letter of Map Revision (LOMR) was approved in March 2014 that reduced the regulated floodplain width through the property and modified the Special Flood Hazard Area, greatly reducing the boundaries, depths and area of the FEMA floodplain.

There are no other significant natural features on the site.



The area of development is to the east of the former location of a large-scale, three-shift, employer, Independent Printing Company, founded in 1910. With a reduction in paper needs as we entered a digital era, the printing company closed in 2016. The property owners have since created a multi-structure, physically connected, employment service center. The Service Buildings are part of the IPCO properties that extend along the north and south sides of Independent Way, south to the Railroad Tracks, and west along Hamilton Creek. The service buildings consist of four, existing commercial/employment buildings that house businesses ranging from food processing and packaging, product research and development, sales, service, offices, karate studio, and photography studio. The former IPCO structure (Service Building #1), and the two older tenant structures (Service Buildings #2 and #3) have connected vehicular, including semi-truck, circulation. With the recent development of Service Building #4 in the southeast corner of the IPCO properties, the driveway was extended to Building #4 (PA-T1-2019-00066).

The property proposed for development is served by the public infrastructure has been extended with the construction of Independent Way. Along Independent Way, pavement, curb, gutter, a six-foot sidewalk, a seven-foot-wide landscape park row with street trees, streetlights, and utilities have been installed. Independent Way is open, connecting Tolman Creek Road to Washington Street.

There is an 8-inch water main in Independent Way. An 8-inch sanitary sewer main, and a 12-inch storm sewer main in Independent Way. There is a substantial amount of electric service capacity too and through the property. The service exists underground, large transformers have been installed as part of previous site work and the street installation.

The subject property and the immediately adjacent properties are zoned Employment (E-1). The property to the north is Employment and is occupied by a residential structure and outbuildings. To the northeast, the property is zoned Commercial (C-1). The property to the southeast is zoned Industrial (M-1). The property to the south is Employment and owned in common by the IPCO Development Corporation. Properties to the west are Employment with Residential Overlay and further west, across Tolman Creek Road, properties are zoned Commercial (C-1).



Proposal:

The proposal is for the development of the 2.07 acres Employment zoned lot with two, metal buildings that will provide commercial/industrial tenant shell spaces that are adaptable for business and employment needs including warehouse to office spaces. The structures are designed in a manner that allows for uses similar to the other uses in the E-1 zone on the property and the immediate vicinity.

Service Building #5 is proposed as the south building. The preliminary building design demonstrates general compliance with the building design standards for E-1 zoned lands and the Basic Site Review Standards. This structure is substantially more than 20-feet from the public street. Service Building #5 is proposed as a 17,858.5 square foot, 22-foot tall structure. The main body of the building is proposed as vertical, multi-rib, metal siding. Under the eave line, a clear panel to allow daylight into the tenant space is shown. There are commercial entry doors with sidelights on the north and east façades of the structure. There are four-foot awning overhangs at the entries. The building has structural design and engineering that will allow for openings for additional entry doors and/or additional roll-up doors as the spaces evolve.

Service Building #6 is a metal building that is 10,919 square feet. This single-story structure is proposed to be 18-feet tall. The building is proposed as a vertical ribbed, metal building. There are four, framed openings for commercial type, aluminum and glass, pedestrian entrance/exits to the building. Windows that provide a view into and out of the tenant spaces are proposed on the street fronting facade. Awnings for pedestrian shelter that extend along the entire façade of the structure (as shown), or individual awnings at each entrance will be provided on the final building permit submittal. Recessed soffit lights under the awning are proposed to illuminate the entrance of the individual tenant space.

Service Building #6 extends across the majority of the property's street frontage. There is a significant grade change from Independent Way and the sidewalk up to the grade of the property. The finished floor of the building is higher than the sidewalk grade so pedestrian access to the entrances of the tenant's spaces is from a central stair leading up the raised walkway that extends along the frontage of the building. On the east sides of the building, a ramp is provided to the grade of the driveway and pedestrian walkway leading to the parking area and Service Building #5.

The proposal seeks an exception to the landscape buffering standard along the front of the development between the street and the retaining wall. There is no minimum front, side, or rear yard setback in the Employment Zone but there is a landscape. When the purchase and street improvement agreements were made between the property owners and the City of Ashland, the required ten-foot landscape buffer for developments in the Employment Zone a landscape area of ten feet in width is required to buffer buildings adjacent to streets.

Site Use:

The request is for metal buildings that are capable of many types of commercial tenant space. The proposed uses are designated as light industrial, warehousing, and office facility. Other businesses on Tolman Creek Road include food production, light manufacturing facilities, shopping, auto repair, dining, and warehousing.

The proposal is consistent with the Economic Element of the Comprehensive Plan which states that “the exact mix of retailing, services, wholesaling, and manufacturing is impossible to predict and difficult to control, so forecasting should err on the side of allowing ample, rather than restrictive policies for land availability.” In this case, allowing for the construction of commercial buildings in the zone that have the flexibility to convert to a higher intensity employment use while conforming to the Site Design and parking standards, development in the employment zone should be encouraged to provide a predictable outcome to the approval process when standards are met. (*Comprehensive Plan; Economic Element, Chapter 7.04*)

Both of the proposed buildings are designed in a manner that allows for internal partitions. See attached elevations and floor plans.

Building 6 which is required to occupy the majority of the property frontage and to be parallel to Independent Way requires the building to be at one finish floor to attract the highest and best uses for warehouse or industrial tenants. The existing grade between entrances drop +/-13' between the two drive approaches, so by using methods of balancing the site and maintaining minimal slope across the parking lots for truck deliveries, pedestrian access and handicapped accessibility require the use of a retaining wall system as shown. To attract Class "A" tenants a developer must remove barriers to client indifference. Creating barriers like finish floors at multiple elevations is counterproductive and counterintuitive to the needs of distribution or process manufacturing for the class “A” tenant the property owners are seeking. These types of customers look for and rely on continuous workflow to expedite their processes of work. Associated General Contractors' (AGC) National Chief economist Ken Simonsen states that the #2 growth sector for the construction and development market should be for regional warehouses to supplement the demand for direct shipments of online shopping due to the void of retailers that will survive the Covid-19 pandemic event. The IPCO Development site with interconnected driveways, accesses, and street access, is just the site to meet the needs of the shipping, warehousing, manufacturing markets.

Parking:

The parking demand for industrial, manufacturing, production, warehousing, and freight uses is one parking space per 1,000 square feet gross floor area or one space for every two employees whichever is less, plus one space for a company vehicle. Specific types of these uses are not identified in the land use ordinance. There is a total of 29 required parking spaces based on the warehousing calculation. The proposal provides for a maximum number of parking spaces of 32. There are an additional, eight, formalized parking spaces (labeled IPCO) on the site plan. These spaces are provided for Service Building #1 on the adjacent property. In total, there are 40 parking spaces provided with 29 of those as standard automobile spaces, and three (3) ADA parking spaces, eight (8) are compact. There is a minimum of 22-feet of back up provided for each parking space. The parking areas will be developed as per the requirements for parking lot design for shade trees, microclimatic impacts, and stormwater quality management.

Along the Hamilton Creek Corridor where the curbing is located, these spaces are presently shown as the semi-truck staging area. If the property development intensifies, there would be an opportunity to stripe these spaces to accommodate higher levels of parking demand if necessary.

Bicycle parking is proposed to be located within the building and as demonstrated on the floorplans.

Site Circulation:

All of the IPCO Development Corporation Service Building sites are interconnected and accommodate semi-truck and other vehicular traffic through the properties. The new parking area is proposed to be accessed from the driveways that extend from Independent Way. With the new development, pedestrian access is extended from Independent Way along both sides of the proposed Service Building #6 and extends to the entrances of Service Building #5. The sidewalks are raised when crossing drive aisles as required by code. The parking areas provide for adequate back-up and turn around area is provided for on the site plans.

Tree Protection:

There will be newly planted street trees and there are parking lot shade trees along the west property line that will be protected from construction impacts. There are also trees in the riparian drainage area on the east side of the driveway and semi-parking area. A tree protection fencing plan for the three trees in the parking area, utilizing, six-foot-tall chain link fence is proposed to be installed at the perimeter of the trees as indicated on the sheet L-1 of the plans.

Water Resource Protection Zone and Floodplain:

The 2015 City of Ashland request for site design review and exception to street standards application, included a Limited Use Permit / Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone decision (PA-2015-00422) that allowed for the Independent Way creek crossing and the riparian restoration on the north and south sides of the street has been completed and the street connection between Tolman Creek Road and Washington Street.

At the time of the 2015 decision, it was proposed that the stormwater quality detention area was provided within the Hamilton Creek Water Resource Protection Zone, as shown on the Civil Engineering plans and the Covey/Pardee Impact and Mitigation Area plan (sheet L-3 of 2015-00422) submitted with the proposal delineate the impact and mitigation areas. Additionally, the stormwater would be sent to the Filterra bio-retention systems. Along the east edge of the development, there is a slight encroachment into the water resource protection zone. This disturbed area represents a very minimal area of disturbance in the 40-foot from centerline buffer of a creek that is actually in a culvert and not daylighted in the area where the encroachment occurs.

The proposal is consistent with the Physical Constraints Review for the floodplain crossing that was approved in 2015 for Independent Way. The proposal does not impact the previously approved encroachments into the floodplain.

Findings of fact addressing the criteria from the Ashland Municipal Code are provided on the following pages. For clarity, the criteria are in Times New Roman font and the applicant's responses are in Calibri font.

Criteria from the Ashland Land Use Ordinance

Site Development and Design Review

18.5.2.050 Approval Criteria

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

Finding:

The subject property and the adjacent properties are zoned Employment (E-1). The proposed development will have a positive impact on the newly installed public street through the development of a vehicle-oriented warehouse, industrial development that must provide adequate vehicular access and circulation, functional structures that allow for highest and best uses in the employment zoned area while still providing pedestrian access and structural orientation to the street.

There are no minimum setbacks in the E-1 zone. Setbacks proposed are minimums necessary for the site development. Along Independent Way, Service Building #6 is proposed to have a five-foot setback from the public street. The proposed structure complies with the solar setback standard B. The structure is proposed to be 18-feet from grade with an average height of just over 20-feet in average height.

Building #5 is proposed to be 22-feet tall.

The proposed lot coverage from new development is less than the allowed 85 percent in the zone. The 2.07-acre parcel is proposed to have 28,775 square feet of the building and approximately 40,718 square feet of asphalt. There are 4,951.52 square feet of new landscape areas within the parking areas proposed. The proposed total lot coverage is 69,495 square feet which is 77.08 percent of the 2.07-acre site.

There are no residences proposed and residential density does not apply. The floor area ratios (FAR) are not applicable in the Basic Site Review zone. The buildings are both less than the maximum height of 40-feet allowed in the zone.

Service Building #5 is substantially more than 20-feet from the public street and is not oriented towards the public street.

Service Building #6 occupies the majority of the property frontage. The building is accessed directly from the sidewalk via a centralized stairway that extends from the sidewalk to the raised walkway that extends along the frontage of the structure. The building has architectural details that are commonly found on metal buildings in the Employment zone.

To allow for potential intensification of uses, at the front of the building, an entry/exit door is framed but not installed to create additional tenant space. Additionally, on the south side of Service Building #6, the area for future windows have been accommodated in the design. Service Building #5 incorporates additional areas for openings for roll-up doors and pedestrian entrances on the front façade (north side) and the east side.

The proposed parking area of 40-new parking spaces is to the rear and side of the street fronting structure, Service Building #6. The parking area complies with the parking lot design standards specified in section 18.4.3. Parking lot shade trees will be planted in the landscape buffers and within the parking lot, shade tree planting islands. The parking area is proposed to be surfaced with asphalt, and concrete, or comparable surfaces. The parking areas will be constructed to standards on file in the office of the City Engineer. Provisions have been made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property (see Preliminary Grading Plan).

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

Finding:

The property is within the Water Resource Protection Zone and a FEMA Special Flood Hazard Area floodplain has been identified on the property. Hamilton Creek exits from a 60-inch culvert along the east property line. The proposed development, excepting a very small area of the driving and parking area, is setback more than 30-feet from the mapped centerline of Hamilton Creek. The 2015 Site Review application that allowed for the installation of Independent Way included a Limited Use permit for the Independent Way bridge crossing which was approved. At that time, a small area of encroachment within the Water Resource Protection Zone was proposed. The area of the drive aisle curbing that is within the WRPZ was detailed in the 2015 application site plans as was the bioswale/detention feature.

The proposal can be found to comply with the existing 2015 approval that allowed for Independent Way bridge crossing, stormwater detention, and minor impacts into the Water Resource Protection Zone. New findings addressing the Water Resource Protection Zone Limited Use Activity and Floodplain Development Standards have not been provided as the previous decision assigned conditions of approval for the impacts to the Water Resource Protection Zone. A multi-year management plan for the stormwater facility was discussed in the previous decision. It is anticipated that no additional impacts to the riparian area will be necessary for the proposed site development.

C. Site Development and Design Standards. The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

18.4.2.040.B. Basic Site Review Standards.

1. Orientation and Scale.

a. Buildings shall have their primary orientation toward the street and not a parking area. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings or to one side.

Finding:

Proposed Service Building #6 is oriented towards the public street, Independent Way. The structure has direct access from the public street to the entrances of the building by pedestrians. Entry doors are indicated with the large awning overhangs and the commercial storefront style door with sidelight. There are also windows provided along the frontage. No automobile circulation or off-street parking occurs between the building and the street. The parking areas are to the side and rear of the street fronting building.

b. A building façade or multiple building facades shall occupy a large majority of a project's street frontage as illustrated in Figure 18.4.2.040.B, and avoid site design that incorporates extensive gaps between building frontages created through a combination of driveway aprons, parking areas, or vehicle aisles. This can be addressed by, but not limited to, positioning the wider side of the building rather than the narrow side of the building toward the street. In the case of a corner lot, this standard applies to both street frontages. Spaces between buildings shall consist of landscaping and hard durable surface materials to highlight pedestrian areas.

Finding:

The majority of the property's street frontage is occupied by proposed Service Building #6. The driveways are to the sides of the building allowing positioning of the wider side of the building to the street.

There are no gaps in the frontages. The driveway aprons, vehicle aisles and parking areas are to the sides and to the rear of the building.

c. Building entrances shall be oriented toward the street and shall be accessed from a public sidewalk. The entrance shall be designed to be clearly visible, functional, and shall be open to the public during all business hours.

Finding:

The pedestrian entrances doors of the structure of Service Building #6 are clearly visible in their orientation towards the street. The pedestrian entrances are accessed via a raised walkway, the walkway is served by a wide, central stair that leads directly from the public sidewalk to the sidewalk accessing the entrances. Each pedestrian entrance is clearly visible from the street with

commercial, entry doors, sidelight windows, awnings to provide pedestrian coverage from sun and rain, and lighting that all enhance the sense of entry to the tenant spaces.

d. Building entrances shall be located within 20 feet of the public right of way to which they are required to be oriented. Exceptions may be granted for topographic constraints, lot configuration, designs where a greater setback results in improved access or for sites with multiple buildings, such as shopping centers, where other buildings meet this standard.

Finding:

The building entrances of Service Building #6 are oriented towards the public right-of-way and located within 20-feet of the sidewalk. The proposal seeks an exception to reduce the required landscape buffer along the frontage of the property adjacent to the public street to location the entry closer than 20-feet of the right-of-way. This reduced landscape buffer allows for improved large vehicle access to the loading/unloading areas, parking areas and vehicle maneuvering areas. The proposed reduced buffer allows for a wider than the standard 22-feet of backup area to allow better turning movements for the large vehicles that presently utilize the IPCO site and will be utilizing the proposed service buildings.

In the Employment Zone, it is critical for the site grades to allow easy access for delivery vehicles, forklifts, pallet movers, etc., cross accessing the internal spaces and the IPCO, properties retention of slight cross-grade, a single finished floor area within the structures, the existing grades of the developed portions of the site dictated the grade of the building site area.

Based on civil drawings from the 2015 Site Review there were more than 6 ½ feet to nearly 8 ½ feet of grade change from the back of the sidewalk to the pad area. This grade change at the back of the sidewalk and the need for the site to have a relatively consistent grade from this tax lot to the other tax lots in the IPCO Service Center complex are the reasons this building is raised above the sidewalk with a retaining wall along the frontage. To provide a clear, distinct, pedestrian entrance to the site, the retaining wall has a central stair that provides access to the covered pedestrian entrances. Sidewalk connecting for the public street leads to the Service Building #5 providing pedestrian access. This building is orientated to the internal sidewalk system.

e. Where a building is located on a corner lot, its entrance shall be oriented toward the higher order street or to the lot corner at the intersection of the streets. The building shall be located as close to the intersection corner as practicable.

Finding:

The subject property is not a corner lot.

f. Public sidewalks shall be provided adjacent to a public street along the street frontage.

Finding:

New public sidewalks and street trees were recently installed along the street frontage of Independent Way.

g. The standards in a-d, above, may be waived if the building is not accessed by pedestrians, such as warehouse and industrial buildings without attached offices, and automotive service stations.

Finding:

Not applicable.

2. Streetscape. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

Finding:

No modifications to the newly installed streetscape are proposed.

3. Landscaping.

a. Landscape areas at least ten feet in width shall buffer buildings adjacent to streets, except the buffer is not required in the Detail Site Review, Historic District, and Pedestrian Place overlays.

Finding:

The proposal does not include a ten-foot landscape buffer adjacent to the street. The property is not within the Detail Site Review zone or the Pedestrian Place overlay. The signed purchase agreement when the city of Ashland purchased the property from the IPCO group, included understood conditions of the purchase. One of the terms was that with planning approval, the required landscape buffer would instead be the landscape park row. The park row was installed at seven feet leaving three feet of landscape. The proposal seeks an exception to eliminate three additional feet of landscape buffer.

b. Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

Finding:

A five-foot-wide landscape buffer is proposed with ground covers other shrubberies that is not on the city's prohibited plant list. Parking lot shade trees are proposed. The size, species, and

planting specific landscape and irrigation plan will be provided to the city as required by code and provided with the building permit.

The refuse containers are to be located within the buildings. This is consistent with how the majority of the tenants of the property operate per their lease agreements. On garbage day, the tenant places their refuse container outside of their unit. This is the method that has worked for the property owner and trash collection service provider, Recology.

4. Designated Creek Protection. Where a project is proposed adjacent to a designated creek protection area, the project shall incorporate the creek into the design while maintaining required setbacks and buffering, and complying with water quality protection standards. The developer shall plant native riparian plants in and adjacent to the creek protection zone.

Finding:

See the findings above regarding overlay zones. Silt fencing will be provided to prevent erosion into the Water Resource Protection Zone before site disturbance.

5. Noise and Glare. Artificial lighting shall meet the requirements of section 18.4.4.050. Compliance with AMC 9.08.170.c and AMC 9.08.175 related to noise is required.

Finding:

The exterior lights are proposed LED dusk to dawn lights recessed under the awning and cast light downward. Noises generated by the site will be comparable to noises to be expected in an employment zone that allows for production, manufacturing, and repair.

D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Finding:

Adequate City facilities (utilities) exist to serve the proposed development. New underground infrastructure was extended from Tolman Creek Road to Independent Way. Also, there are private utility easements extended through the property.

The civil engineering firm that proposed and designed Independent Way is the Civil Engineers of record on this project. The installation of adequate facilities was contemplated through the development of Independent Way.

E. Exception to the Site Development and Design Standards. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site, and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty;

Finding:

Not applicable

2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards; or

Finding:

The City of Ashland purchased the property to install the new street from the current property owners. As part of the 2012 purchase agreement, the agreement stipulated that the required landscape buffer would be accommodated within the park row. The park row was installed at seven feet, leaving three feet of landscape buffer width required. The proposal is not providing the landscape buffer along the frontage of the property. The reason for this is that the proposed design has a direct pedestrian connection to the public sidewalk. There are numerous issues with transients camping and loitering on the property and increased landscape areas encourage loitering.

Though not along the entire frontage of the property, there is a landscape buffer at the west side of the structure and between the western driveway access. Additionally, due to the riparian buffer zone, there is an abundant landscaped area provided on the property, substantially more than a typical Employment zoned property.

Not providing a landscape buffer in the zone where no setbacks are required, does not reduce public health or safety. The wall adjacent to the sidewalk will not harm the livability of the adjacent employment zoned lands as there is no residential zoning. The proposed building will make a positive contribution to the streetscape and the area is not a highly traveled arterial street and is a manufacturing and industrial area where the standards can be flexed to achieve the

purpose of the zone which is functional, employment zoned land that has access and parking area for large vehicles and business traffic.

When the street connection application was made, minor concessions associated with the street design were made to provide a major, city of Ashland TSP driven connectivity project. The reduced buffer is minor when considering the substantial compliance with other standards of the Site Design and Use Standards, the Water Resource and Riparian protection zone buffer, grading of existing site determining the finished grade of proposed site development and the large investment into flexible, space for large or small warehouse, storage, distribution, production facilities. The parking lot layout and the number of spaces provided on-site allow for intensification of the site uses in close proximity to the Ashland Street Commercial corridor and the I-5 exit 14.

The building entrances of Service Building #6 are oriented towards the public right-of-way and located within 20-feet of the sidewalk. The proposal seeks an exception to reduce the required landscape buffer along the frontage of the property adjacent to the public street to location the entry closer than 20-feet of the right-of-way. This reduced landscape buffer allows for improved large vehicle access to the loading/unloading areas, parking areas and vehicle maneuvering areas. The proposed reduced buffer allows for a wider than the standard 22-feet of backup area to allow better turning movements for the large vehicles that presently utilize the IPCO site and will be utilizing the proposed service buildings.

In the Employment Zone, it is critical for the site grades to allow easy access for delivery vehicles, forklifts, pallet movers, etc., cross accessing the internal spaces and the IPCO, properties retention of slight cross-grade, a single finished floor area within the structures, the existing grades of the developed portions of the site dictated the grade of the building site area.

Based on civil drawings from the 2015 Site Review there was more than 6 ½ feet to nearly 8 ½ feet of grade change from the back of the sidewalk to the building pad area. This grade change at the back of the sidewalk and the need for the site to have a relatively consistent grade from this tax lot to the other tax lots in the IPCO Service Center complex are the reasons this building is raised above the sidewalk with a retaining wall along the frontage.

There is no on-street parking on this side of the street and the Employment zoned and the business that presently occupy the majority of the IPCO Service Center are not pedestrian oriented businesses but rely heavily upon delivery traffic.

Even so, a clear, distinct, pedestrian entrance via the stairs directly from the sidewalk to the Service Building #6 is provided and clear pedestrian access to the parking area and Service Building #5 are provided with the proposed layout.

3. There is no demonstrable difficulty in meeting the specific requirements for a cottage housing development, but granting the exception will result in a design that equally or better achieves the stated purpose of section 18.2.3.090. (Ord. 3147 § 9, amended, 11/21/2017)

Finding:

Not applicable

18.4.3.080 Vehicle Area Design

B. Parking Area Design. Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in [18.4.3.080.B](#). See also, accessible parking space requirements in section [18.4.3.050](#) and parking lot and screening standards in subsection [18.4.4.030.F](#).

Finding:

The proposed building Service Building #5 is 17,858.5 square feet in area which requires 18 parking spaces. Service Building #6 is 10,919 square feet and requires 11 parking spaces. There are eight spaces for IPCO Service Building #1 on the west property line that are shown on the site plans as part of this development because the driveways are connected. A 40-vehicle parking area is proposed. This accounts for one parking space per 1,000 square feet of floor area and the +one space for a company vehicle. Three ADA accessible spaces with required off-loading zone are also included. The parking lot layout demonstrates that adequate turn around is provided on-site to allow vehicles to exit the parking area in a forward manner. The parking spaces are proposed to be 9 feet by 18 feet. Eight are proposed as compact.

The parking area has potential for expansion through the striping of additional parking spaces if necessary, in the parking area parallel to the creek that is presently shown as large vehicle parking area. In the event a user with higher intensity parking demands requests tenant improvement or business license from the city, these parking spaces can be re-striped as head-in automobile parking and not have a negative impact on the other uses, vehicle access and maneuvering, nor does the change in type of parking space require additional site review as large vehicle parking is not a required standard but is proposed in this case due to the known user types of the IPCO Service Center.

The proposed parking lot is designed to minimize adverse environmental impacts through the use of a bioswale filtration as required by the Rogue Valley Stormwater Design Manual. See the Civil Engineering plan.

The existing parking areas to the west and south are pre-existing, non-conforming, and does not comply with this standard. The proposal does not increase the non-conformity. The proposed building is not accessed by pedestrians as it is an industrial building or warehouse.

18.4.5.030.C. Tree Protection Measures Required.

Finding:

Six-foot tall, chain link fencing is proposed at the driplines of the trees as indicated on the plan. The fencing will be flush with grade. An inspection by City staff will be requested for the installation of the signs. No construction activity will occur within the tree protection zones. No building supplies, soil, waste, equipment, vehicle parking will be allowed in the tree protection zones. No chemically injurious materials or liquids, including construction debris, run-off, or concrete excess waste. There is no excavation proposed within the root zones.

Attachments:

2012 Purchase Agreement

Conceptual Renderings: G2.0

Site Plan: A0.1

Floor Plan (Service Building 5): A1.1

Elevation (Service Building 5): A2.1 and A2.2

Floor Plan (Service Building 6): A1.1

Elevation (Service Building 6): A2.1

Landscape and Tree Protection Plan: L1

Grading, Drainage, Erosion Control Plans

LETTER OF INTENT FOR SALE
OF REAL PROPERTY

December 13, 2012

IPCO Development
640 Tolman Creek Road
Ashland, OR 97520

Re: Purchase of Property for Right of Way between Washington Street & Tolman Creek Road

Dear Messrs:

By this letter, City of Ashland ("Buyer"), presents the manner in which it and IPCO Development ("Seller") agree Buyer may acquire certain real property from Seller as described herein. The parties recognize that the transaction will require further documentation and approvals, including the preparation and approval of a formal agreement setting forth the terms and conditions of the proposed purchase (the "Purchase Agreement"); nevertheless, they execute this letter to evidence their intention to proceed in mutual good faith to complete work required to negotiate terms of a Purchase Agreement that are consistent with this letter.

The proposed terms and conditions include, but are not limited to, the following:

1. **Property.** Buyer will purchase from Seller all interests and rights, owned or used by Seller in connection with an approximate 55 feet wide strip of private property between Washington Street & Tolman Creek Road as further described in Exhibit A, (the "Property"). The Seller acknowledges that the Buyer intends to establish this property as a dedicated right-of-way ("ROW") through the current IPCO Development property. This new ROW would parallel and offset approximately 10 feet to the north of the centerline of the existing Washington Street ROW.
2. **Consideration.** The consideration (the "Purchase Price") will be established pursuant to appraisal by an appraiser selected and paid by the Buyer and subject to reasonable negotiations with Seller. Buyer will not assume any other liabilities or obligations of Seller over other property adjacent to or previously part of parcel or lot through which the Property, as a right-of-way, runs, and Seller will indemnify and hold harmless Buyer against all such other liabilities and obligations.
3. **Purchase Agreement.** The transaction will be subject to the negotiation and execution of a definitive Purchase Agreement with terms satisfactory to Seller and Buyer. The Purchase Agreement will contain representations, warranties and covenants, conditions that are reflected in the IPCO Development conceptual site plan scheme #14 (11-13-13) Exhibit A (attached) and will include without limitation the following:
 - (a) The Buyer proposes, subject to planning approval, to build, own, and maintain this approximately 700 feet long, 28 feet wide paved road with curb, "park row", and 8 feet wide sidewalks on the north side of street. Buyer agrees, subject to Planning approval, to allow seller to credit park row landscape for sellers required landscape associated to any future

MAR 06 2015 Page 1 of 4

City Of Ashland

development on said property. In addition, buyer will install irrigation system and pay water bill in public park row.

- (b) The Buyer will construct and maintain a stream crossing structure over Hamilton Creek, to support the new roadway and sidewalk. The crossing structure will be designed to meet or exceed the most current storm water quality mitigation requirements and standards. Riparian restoration of the creek bed and banks will be included to the maximum extent practicable.
- (c) The Buyer will relocate existing utilities as required, without diminishing utility services quality to the development, including water pressure, electric power, and sewer service line to property line at a location designated by the property owner with proper access to all locations. This shall not be charged back to seller.
- (d) The Buyer will assist the development to adjust the location of the existing conservation easement across private property to the match the new FEMA flood map and Ashland Water Resources Protection Ordinance boundaries. If allowed by FEMA, the Buyer will allow building E as shown with parking at the southeast corner with utility and road access over that area.
- (e) The Buyer, subject to Planning approval, will allow the development to use any land recovered and to use the area over the existing pipe culvert, as deemed appropriate by FEMA, and allow the development to clean out and maintain blackberry and weed infested areas and use these areas for landscape credits.
- (f) Assist the development in preparing a master plan that will include parking and driveways in specified locations to be formally submitted for planning review through the City's planning process. The associated City planning costs will be paid by the Buyer. The Buyer anticipates that the process will include the following steps:
 - a. Submit pre-application for Preliminary Site Layout.
 - b. Draft conservation easement boundary adjustment & delineate "water protection zone."
 - c. Draft right-of-way dedication survey documentation.
 - d. Obtain property owner concurrence RE: ROW dedication & easements.
 - e. Obtain Planning approval for variances.
 - f. Obtain City Council approval RE: ROW dedication & easements.
 - g. Engineering Design & Permitting.
 - h. Planning Approval for Environmental Constraints.
 - i. Site Plan approvals.
- (g) The Buyer agrees to work with the Seller during engineering phase of project to ensure final grades will accommodate ingress and egress on the Seller's driveways in order to minimize impacts of steep grades as much as possible.
- (h) The Seller can continue to drain storm water into Hamilton Creek as long as current state, federal and local storm drain regulations are met.

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MAR 06 2015

City Of Ashland

4. **Access.** To permit Buyer to conduct its due diligence investigation, as long as this letter remains in effect, Seller will permit Buyer and its agents to have reasonable access to the Property for purposes of surveying and planning for the use and design of the Property.
5. **Conditions to Closing.** The closing of the transaction will be subject to certain conditions, including without limitation the following:
 - (a) Funds for the purchase of said land are subject to the buyer successfully securing a grant to purchase the Property.
 - (b) All required approvals, consents, and authorizations of state and federal regulatory authorities shall have been received.
 - (c) All required consents of third parties shall have been received.
 - (d) Buyer shall have completed a due diligence review of the property and its title of Seller satisfactory to Buyer in its sole discretion.

The Buyer, as the City, may be required to make make land use and/or building code decisions affecting development of the subject Right-of-Way and related property according to local and state laws. The Parties therefore acknowledge that the Buyer cannot and does not promise or guarantee any particular planning or building code decision or result as part of or as a condition of achieving the purposes of this letter of intent.

6. **Negotiations with Others.** Until January 1, 2018, the date on which the parties anticipate that a Purchase Agreement will be executed, Seller will not offer its stock or assets to, entertain offers for them from, negotiate for their sale to, or make information about them available (for purposes of sale) to, any third party.
7. **Conduct of Business; Interim Operations.** As long as this letter remains in effect, Seller will use its best efforts to conduct its business in a reasonable and prudent manner in accordance with past practices, to preserve its existing business organizations and relationships with its employees, customers, suppliers, and others with whom it has a business relationship, to preserve and protect its properties, avoid any and all liens, and to conduct its business in compliance with all applicable laws and regulations.
8. **Closing Date.** The closing date under the Purchase Agreement will be the date agreed upon by the parties.
9. **Effect of This Letter.** This letter sets forth the intent of the parties only, is not binding on the parties, and may not be relied on as the basis for a contract by estoppel or be the basis for a claim based on detrimental reliance or any other theory; provided that paragraphs 6 and 7, and this paragraph 9 will be enforceable in accordance with their terms. With the exceptions of paragraphs 4, 6, 7, and this paragraph 9, the parties understand that no party shall be bound until the Purchase Agreement has been negotiated, executed, delivered, and approved by the partners or shareholders of Buyer and Seller, as the case may be.

10. Termination of Negotiations. This letter may be terminated at any time by either party giving written notice to the other. After notice is given, the parties shall be bound only by paragraphs 6, 7, and 9.

If this letter sets forth your intent to proceed in good faith substantially in the manner outlined in this letter, please sign a copy of this letter and return it to Buyer. This letter of intent shall be of no further force and effect if it is not signed by Seller and returned to Buyer by the close of business on 12/13, 2012.

Very truly yours,

By: Dave Kanner

Buyer

Accepted and agreed to:

By: John Bronbacher

Seller

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MAR 06 2015

City Of Ashland



1 **Conceptual View at Building 6 and Independent Way**
12" = 1'-0"



2 **Conceptual View at Building 6 South**
12" = 1'-0"



3 **Conceptual View at Drive Aisle Building 5 & 6**
12" = 1'-0"

Author: 12/21/2020 2:21:49 PM
 W:\Design Projects\600-CURRENT PROJECTS\IPCO\PHASE 2\IPCO PH2_No Dock Scheme 12.21.20.rvt

GARY R. CAPERNA ARCHITECT
 ARCHITECTURE PLANNING
 190 North Ross Lane, Medford Oregon
 P.O. Box 4460, 97501
 Office: 541.773.8325 Fax: 541.773.6523
 Email: garycaperna@charter.net
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 Oregon 5247, Washington 11470
 Member American Institute of Architects

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 THESE DOCUMENTS, THE IDEAS, & DESIGNS INCORPORATED HEREIN, AS INSTRUMENTS OF PROFESSIONAL SERVICE, ARE THE PROPERTY OF GARY CAPERNA, ARCHITECT, AND ARE TO BE USED ONLY AS PROVIDED IN THE CONTRACT BETWEEN HIM AND THE OWNER, AND SHALL NOT BE USED, IN WHOLE OR IN PART, FOR ANY OTHER PROJECT WITHOUT EXPRESS WRITTEN AUTHORIZATION BY GARY CAPERNA, ARCHITECT. COPYRIGHT 2020 BY GARY CAPERNA, ARCHITECT.

J.B. STEEL, INC.
 COMMERCIAL & INDUSTRIAL
 P.O. Box 4460, 190 North Ross Lane
 Medford Oregon 97501
 Office: 541.773.8325 Fax: 541.773.6523
 CCB No. 132902
 Web: WWW.JBSTEELING.COM

PROJECT:
IPCO Development Corp.
CLIENT:
 IPCO Development Corporation
 1425 Green Meadows Way
PROJECT LOCATION:
 688 Tolman Creek Road
 391E14BA-600

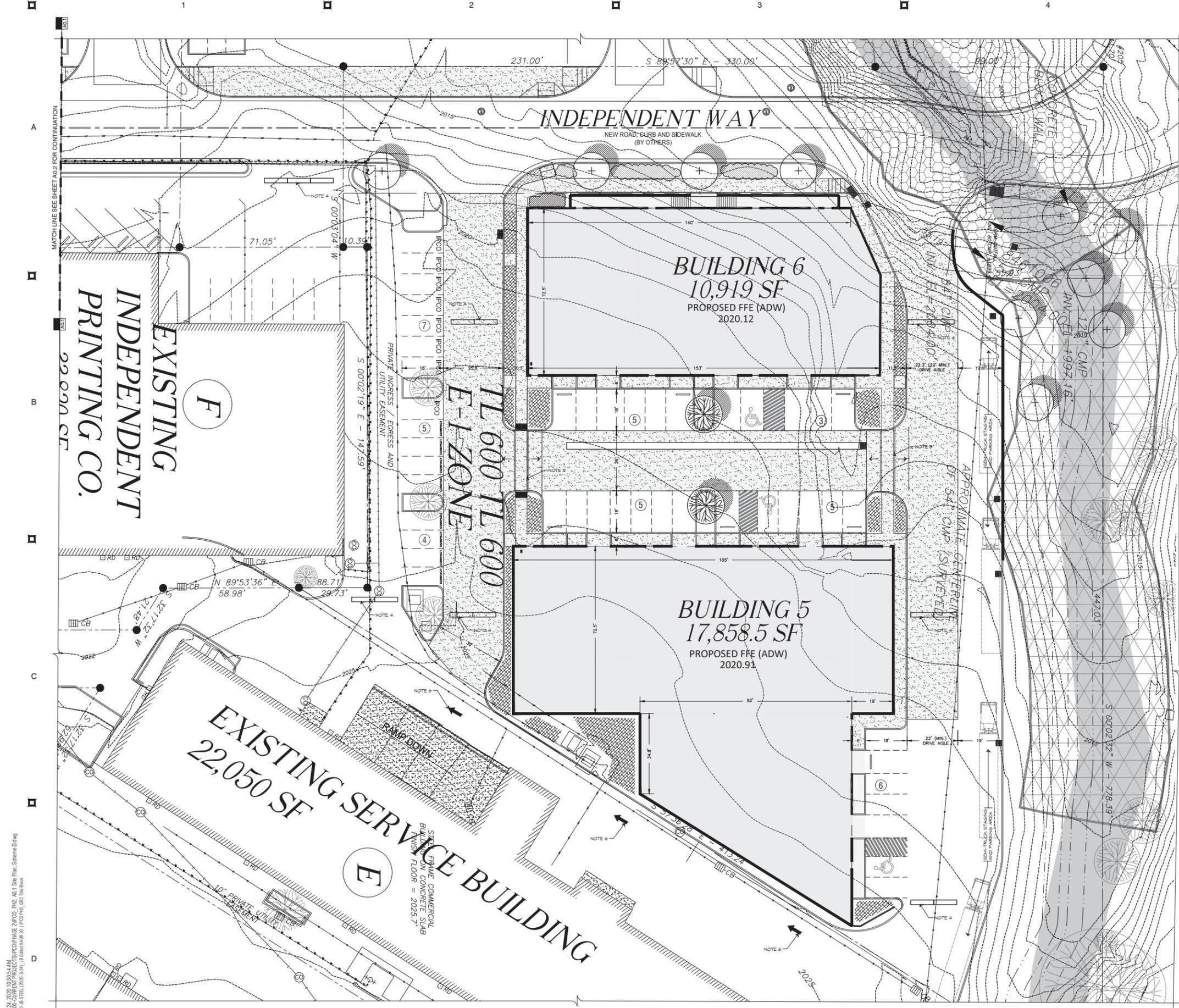
No.	Date	Description

ISSUE: Issue Date
PROJECT NO: 2913-20
DRAWN BY: Author
CHECKED BY: Checker

APPROVED
 SHEET TITLE:
Conceptual Renderings

PLOT DATE:
 12/21/2020 2:21:49 PM

G2.0
 For Application Approval
 Not For Construction



PARKING STANDARDS

PARKING REQUIRED: THESE BUILDINGS ARE INTENDED FOR LIGHT INDUSTRIAL WAREHOUSE USE or OFFICE USE. Parking space count is based on Industrial and Table 18.4.3.040 of the City of Ashland Land Use Ordinance of 1 space per 1000 SF. Up to 50% may be designated as Compact spaces.

BUILDING SC 6:	10,919 / 1000	= 10.9	= 11 spaces
BUILDING SC 5:	17,859 / 1000	= 17.8	= 18 spaces
TOTAL REQUIRED		= 28.7	= 29 spaces min. (32 max.)
TOTAL PROVIDED			= 32 spaces
ADDITIONAL PROVIDED FOR TAX LOT 800			= 8 spaces

THIS PROJECT: 37 standard spaces provided + 3 ADA space = 40 Spaces provided
 COMPACT PARKING: 8 spaces
 ACCESSIBLE PARKING: OSSC, SEC. 1104
 TOTAL REQUIRED ACCESSIBLE PARKING SPACES: 3 (3 Provided)
 Three spaces will be Van accessible
 Noted on Drawing - Yes No
 BICYCLE PARKING - CoFA LUO 18.4.3.070: provide two spaces per primary use or one bicycle parking space for every five automobile parking spaces, whichever is greater, and 50% shall be covered.
 Noted on Drawing - Yes No

- ### GENERAL NOTES
- CONTRACTOR SHALL VERIFY AND CONFIRM EXISTING CONDITIONS SHOWN OR IMPLIED ON DRAWINGS PRIOR TO START OF CONSTRUCTION. NOTIFY A/E OF ANY DISCREPANCIES.
 - EXISTING CONDITIONS BASED ON SURVEY BY POLARIS.
 - SEE CIVIL, ELECTRICAL AND LANDSCAPE DRAWINGS FOR ADDITIONAL INFORMATION.
 - PROVIDE TRUCK-RATED, SURFACE MOUNTED SPEED BUMP AT DRIVES.
 - PROVIDE RAISED WALK AT PATHS CROSSING DRIVE AISLES PER AMC 18.4.3.
 - PROVIDE NEW DIRECTIONAL ARROWS FOR ONE WAY TRAFFIC FLOW.

- ### LEGEND
- (E) EXISTING
 - (N) NEW
 - # PARKING AREA COUNT
 - [Pattern] NEW CONC. PAVING. PROVIDE REINFORCEMENT AT O.H. DOORS. SEE CIVIL DRAWINGS BY OTHERS.
 - [Pattern] NEW A/C PAVING. SEE CIVIL DRAWINGS BY OTHERS
 - [Pattern] PROPOSED STRUCTURE
 - [Pattern] EXISTING STRUCTURE (N.I.C.)
 - [Pattern] NEW PLANTINGS, SEE LANDSCAPE DRAWINGS BY OTHERS
 - [Pattern] EXISTING RIPARIAN MITIGATION BUFFER ZONE
 - [Pattern] FEMA FLOOD PLAIN ZONE A
 - [Symbol] EXISTING TREES TO REMAIN
 - [Symbol] NEW TREES, SEE LANDSCAPE DRAWINGS BY OTHERS
 - [Symbol] NEW TREES AT INDEPENDENT WAY (BY OTHERS) F.I.O., N.I.C.
 - [Symbol] NEW PLANTINGS AT INDEPENDENT WAY (BY OTHERS) F.I.O., N.I.C.

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 OR CCB # 22599 :: CA 298104 A&B :: ID RCE-42730 :: NM 391392
 Web: WWW.JBSTEELINC.COM

PROJECT: A NEW PRE-ENGINEERED METAL BUILDING FOR:
IPCO DEVELOPMENT
 CLIENT:
 IPCO DEVELOPMENT CORPORATION
 1425 Green Meadow Way, Ashland, OR 97520
 Project Location:
 688 Tolman Creek Rd., Ashland, OR 97520
 Map & Tax Lot #: 39 1E 14 BA, 600

BUILDING DEPARTMENT APPROVAL STAMP:

MARK	DATE	DESCRIPTION

ISSUE DATE: 12/24/2020 10:30 AM
 PROJECT NO: 2913-20
 DRAWN BY: JMK
 CHECKED BY: GRC

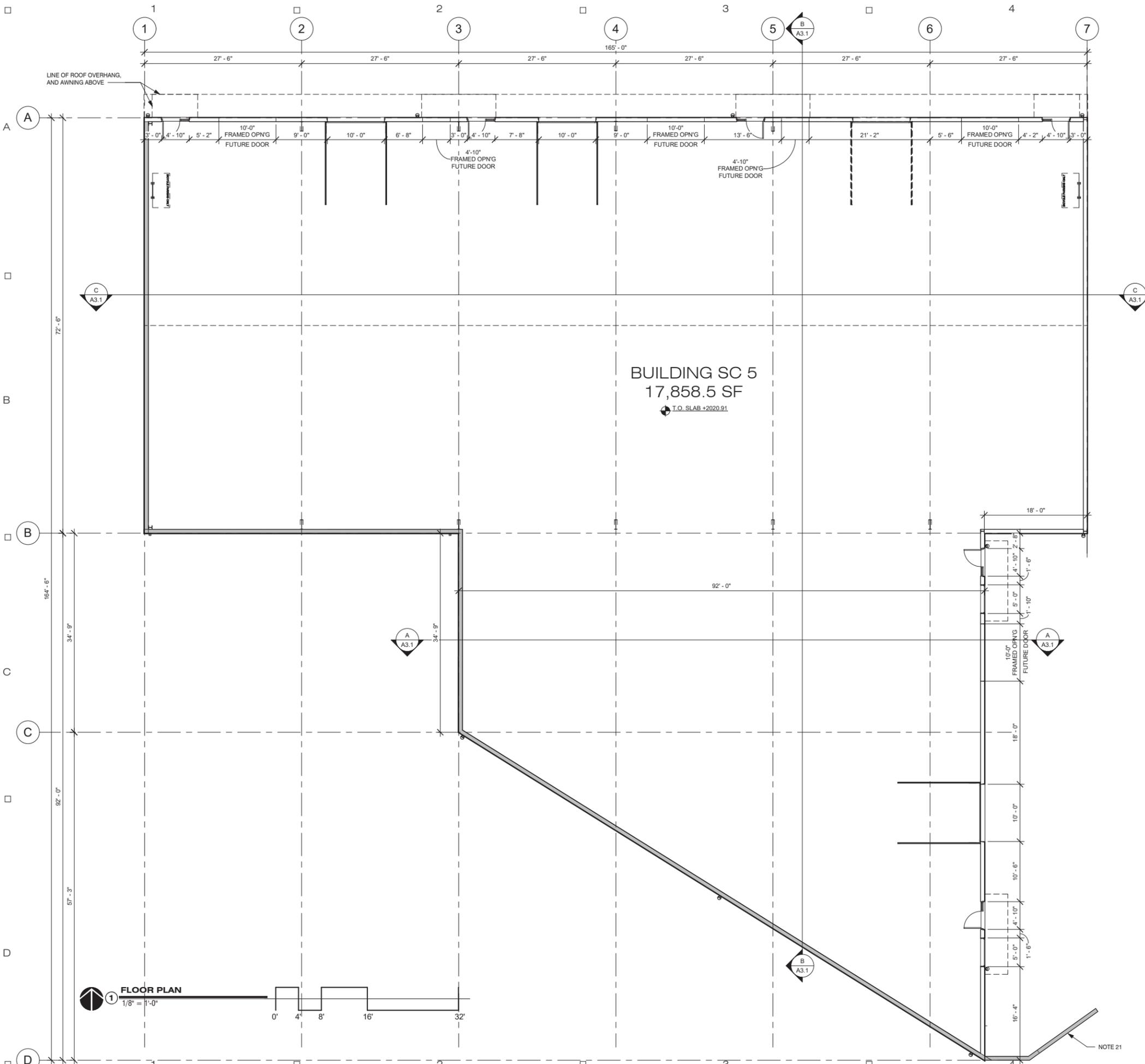
SHEET TITLE:
SITE PLAN

A0.1
 FOR APPLICATION
 NOT FOR CONSTRUCTION

Ldtd: Name: jmk
 Pld Date: Thursday, December 24, 2020 10:30:54 AM
 File Name: W:\Design Projects\IPCO\PHASE 2\IPCO_P2_R12_A0.1_Site Plan_Schedule D.rvt
 XREFS: IPCO, GRC, THE BOOK, IPCO - JB STEEL, 2020-3-24, JB Steel 04.08.20 | IPCO PH2, GRC THE BOOK

PARTIAL SITE DEVELOPMENT PLAN
 SCALE: 1" = 20'-0"
 0 10 20 40

JMK
10/29/2020 2:53:03 PM
W:\Design Projects\100-CURRENT PROJECTS\IPCO\PHASE 2\IPCO PH2_No Dock Scheme 10 14 20.re



GENERAL NOTES

- BEFORE BEGINNING WORK AT THE SITE, WHERE POSSIBLE, & THROUGHOUT THE COURSE OF THE WORK, INSPECT & VERIFY THE LOCATION & CONDITION OF EVERY ITEM AFFECTED BY THE WORK UNDER THIS CONTRACT & REPORT DISCREPANCIES TO ARCHITECT BEFORE DOING WORK RELATED TO THAT BEING INSPECTED.
- APPLICABLE CODES: ALL WORK SHALL BE IN CONFORMANCE WITH ALL FEDERAL, STATE, AND LOCAL CODES. SPECIFICATIONS AND STANDARDS SHALL MEAN, AND ARE INTENDED TO BE THE LATEST EDITION, AMENDMENT OR REVISION OF SUCH REFERENCE STANDARD IN EFFECT AS OF THE DATE OF THE CONTRACT DOCUMENTS.
- ALL STUD WALL FRAMING SHALL BE 2x DOUGLAS FIR (D.F.) WOOD STUDS OR BETTER. 2x4 STUDS AT 16" O.C. FOR INTERIOR WALLS AND 2x6 STUDS @ 24" O.C. FOR PLUMBING WALLS, U.N.O.
- DIMENSIONS ARE TO EXTERIOR STEEL AT EXTERIOR WALLS AND FACE OF STUD AT INTERIOR WALLS, U.N.O.
- DETERMINE LOCATION OF PARTITIONS NOT DIMENSIONED BY THEIR RELATION TO COLUMN FACE OR CENTER, WINDOW JAMB OR MULLION, OR OTHER SIMILAR FIXED ITEM.
- DO NOT DRILL OR CUT JOISTS, BEAMS, COLUMNS OR OTHER STRUCTURAL ELEMENTS UNLESS SPECIFICALLY INDICATED.
- LIGHT AND VENTILATION NOT INDICATED ON THESE PLANS SHALL BE PROVIDED AS PER SECTION 1203 & 1205 OF THE OSSC.
- PROVIDE EXIT ILLUMINATION PER SECTION 1006 & SIGNAGE PER SECTION 1011 OF THE OSSC.
- ALL DOORS AND WINDOWS SHALL COMPLY WITH APPLICABLE ENERGY CODES.
- THRESHOLDS AT DOORWAYS SHALL NOT EXCEED 1/2-INCH. RAISED THRESHOLDS AND FLOOR LEVEL CHANGES GREATER THAN 1/4-INCH AT DOORWAYS SHALL BE BEVELED WITH A SLOPE NOT GREATER THAN ONE UNIT VERTICAL IN TWO UNITS HORIZONTAL (50-PERCENT), PER OSSC 1008.1.7).
- INSTALL A VAPOR BARRIER OF ONE PERM OR LESS AT THE WARM SIDE (IN WINTER) OF ALL EXTERIOR WALLS, ROOF AND CEILING.
- ALL INSULATION INDICATED ON PLANS SHALL COMPLY WITH OR EXCEED THE REQUIREMENTS IN SECTION 720, OSSC FOR SMOKE DENSITY AND FLAME SPREAD.
- R-VALUES INDICATED ON PLANS REPRESENT MINIMUM PERFORMANCE REQUIREMENTS. BATT INSULATION INSTALLED IN METAL OR WOOD FRAMED WALLS SHALL ALSO BE INSULATED TO THE FULL DEPTH OF THE CAVITY, UP TO 6-INCHES IN DEPTH PER TABLE 502.1.1 OF THE OSSC.
- INSTALL ANY REQUIRED ACOUSTICAL CAULKING AT ALL WALL PENETRATIONS (ELECTRICAL JUNCTION BOXES, ETC.), AND AS INDICATED.
- DO NOT MOUNT ELECTRICAL/ COMMUNICATION JUNCTION BOXES BACK TO BACK AND KEEP SEPARATED.
- FINISH MATERIALS USED ON INTERIOR WALLS AND CEILINGS SHALL NOT EXCEED FIRE AND SMOKE RATINGS PER TALBE 803.9 OF THE OSSC.
- VERIFY ALL FINISHES, COLORS, TEXTURES AND STYLES WITH ARCHITECT PRIOR TO PURCHASE OR INSTALLATION.
- OWNER TO RETAIN AND CONTRACTOR SHALL COORDINATE ALL NECESSARY TESTING AND ALL REQUIRED INSPECTIONS.
- FIRE EXTINGUISHERS SHALL BE LOCATED AS REQUIRED BY SECTION 906, OFC. COORDINATE FINAL LOCATION WITH FIRE MARSHALL.
- CIVIL ENGINEERING (BY OTHERS) SHALL COMPLY WITH ANSI A117.1.2009 FOR ADA ACCESSIBILITY TO AND FROM PUBLIC WAY, PARKING AND BUILDING ACCESS.
- PROVIDE ANTISKATE CURB CLIPS AT CONC. RETAINING WALLS EXPOSED AT GRADE.

KEYNOTES

LEGEND

- P.E.M.B. WALL FRAMING AND SHEATHING, OVER CONC. RETAINING WALL, SEE STRUCTURAL AND CIVIL DRAWINGS
- P.E.M.B. WALL FRAMING AND SHEATHING, SEE P.E.M.B. DRAWINGS

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Oregon 5247, Washington 11470
Member American Institute of Architects

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REGISTERED ARCHITECT
GARY R. CAPERNA
STATE OF OREGON
5247

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CCB No. 132902
Web: WWW.JBSTEELING.COM

PROJECT:
IPCO Development Corp.
Service Center 5

CLIENT:
IPCO Development Corporation
1425 Green Meadows Way

PROJECT LOCATION:
688 Tolman Creek Road
391E148A-600

No.	Date	Description

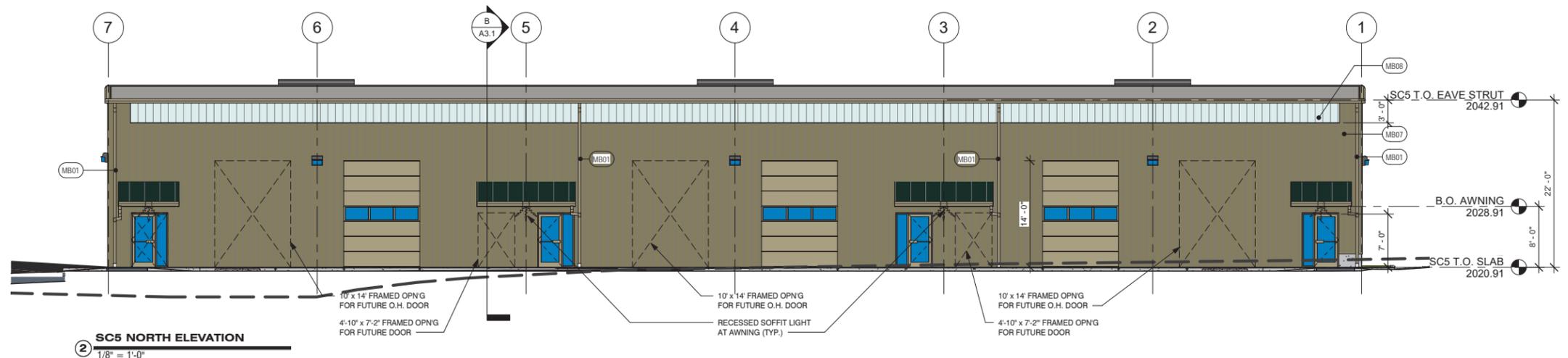
ISSUE: Issue Date
PROJECT NO: 2913-20
DRAWN BY: JMK
CHECKED BY: GRC

APPROVED
SHEET TITLE:
SC5 FLOOR PLAN

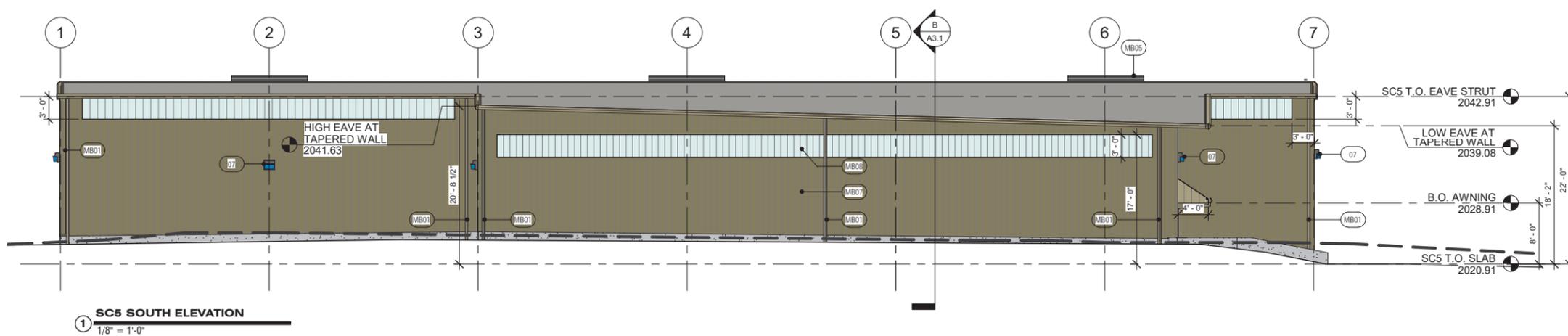
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SC5 NORTH ELEVATION
 1/8" = 1'-0"



SC5 SOUTH ELEVATION
 1/8" = 1'-0"

KEYNOTES

- 07 WALL PACK LIGHT
- MB01 P.E.M.B. DOWN SPOUT (D.S.)
- MB05 P.E.M.B. RIDGE VENT
- MB07 P.E.M.B. PBR PANEL - COLOR BY OWNER
- MB08 P.E.M.B. PBR TRANSLUCENT PANEL

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ISSUE: Issue Date
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SC5 EXTERIOR ELEVATIONS

PLOT DATE:
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KEYNOTES

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- 08 D.S. CONNECTOR
- MB01 P.E.M.B. DOWN SPOUT (D.S.)
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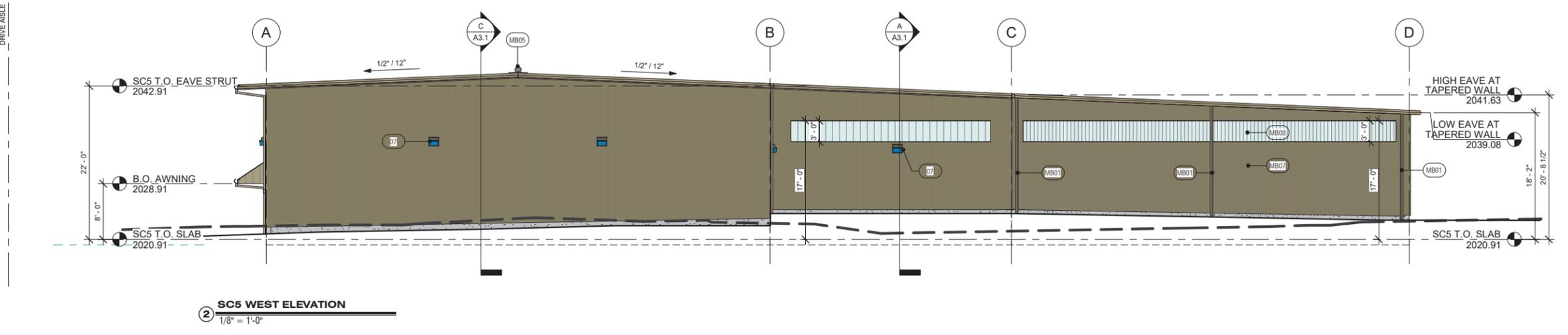
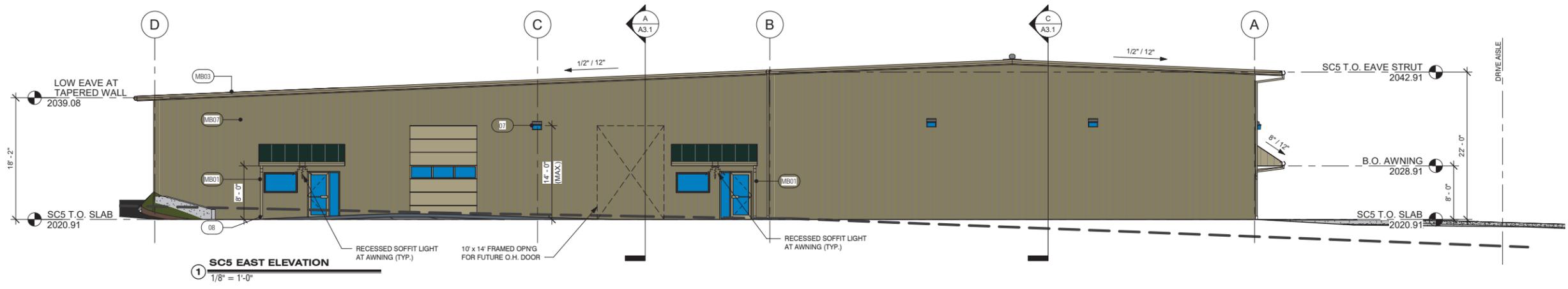
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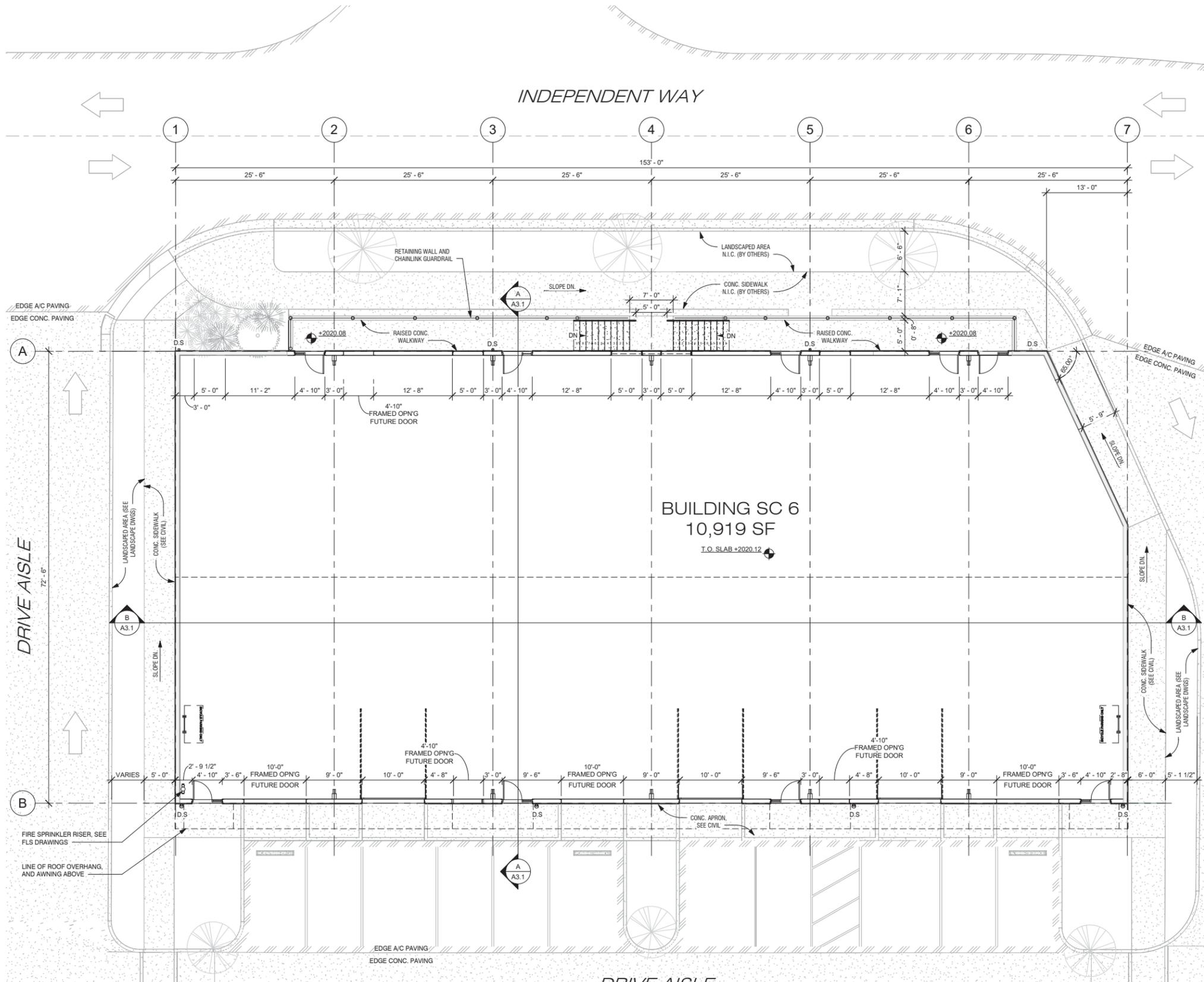
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GENERAL NOTES

- BEFORE BEGINNING WORK AT THE SITE, WHERE POSSIBLE, & THROUGHOUT THE COURSE OF THE WORK, INSPECT & VERIFY THE LOCATION & CONDITION OF EVERY ITEM AFFECTED BY THE WORK UNDER THIS CONTRACT & REPORT DISCREPANCIES TO ARCHITECT BEFORE DOING WORK RELATED TO THAT BEING INSPECTED.
- APPLICABLE CODES: ALL WORK SHALL BE IN CONFORMANCE WITH ALL FEDERAL, STATE, AND LOCAL CODES. SPECIFICATIONS AND STANDARDS SHALL MEAN, AND ARE INTENDED TO BE, THE LATEST EDITION, AMENDMENT OR REVISION OF SUCH REFERENCE STANDARD IN EFFECT AS OF THE DATE OF THE CONTRACT DOCUMENTS.
- ALL STUD WALL FRAMING SHALL BE 2x DOUGLAS FIR (D.F.) WOOD STUDS OR BETTER, 2x4 STUDS AT 16" O.C. FOR INTERIOR WALLS AND 2x6 STUDS @ 24" O.C. FOR PLUMBING WALLS, U.N.O.
- DIMENSIONS ARE TO EXTERIOR STEEL AT EXTERIOR WALLS AND FACE OF STUD AT INTERIOR WALLS, U.N.O.
- DETERMINE LOCATION OF PARTITIONS NOT DIMENSIONED BY THEIR RELATION TO COLUMN FACE OR CENTER, WINDOW JAMB OR MULLION, OR OTHER SIMILAR FIXED ITEM.
- DO NOT DRILL OR CUT JOISTS, BEAMS, COLUMNS OR OTHER STRUCTURAL ELEMENTS UNLESS SPECIFICALLY INDICATED.
- LIGHT AND VENTILATION NOT INDICATED ON THESE PLANS SHALL BE PROVIDED AS PER SECTION 1203 & 1205 OF THE OSSC.
- PROVIDE EXIT ILLUMINATION PER SECTION 1006 & SIGNAGE PER SECTION 1011 OF THE OSSC.
- ALL DOORS AND WINDOWS SHALL COMPLY WITH APPLICABLE ENERGY CODES.
- THRESHOLDS AT DOORWAYS SHALL NOT EXCEED 1/2-INCH. RAISED THRESHOLDS AND FLOOR LEVEL CHANGES GREATER THAN 1/4-INCH AT DOORWAYS SHALL BE BEVELED WITH A SLOPE NOT GREATER THAN ONE UNIT VERTICAL IN TWO UNITS HORIZONTAL (50-PERCENT), PER OSSC 1008.1.7).
- INSTALL A VAPOR BARRIER OF ONE PERM OR LESS AT THE WARM SIDE (IN WINTER) OF ALL EXTERIOR WALLS, ROOF AND CEILING.
- ALL INSULATION INDICATED ON PLANS SHALL COMPLY WITH OR EXCEED THE REQUIREMENTS IN SECTION 720, OSSC FOR SMOKE DENSITY AND FLAME SPREAD.
- R-VALUES INDICATED ON PLANS REPRESENT MINIMUM PERFORMANCE REQUIREMENTS. BATT INSULATION INSTALLED IN METAL OR WOOD FRAMED WALLS SHALL ALSO BE INSULATED TO THE FULL DEPTH OF THE CAVITY, UP TO 6-INCHES IN DEPTH PER TABLE 902.1.1 OF THE OSSC.
- INSTALL ANY REQUIRED ACOUSTICAL CAULKING AT ALL WALL PENETRATIONS (ELECTRICAL JUNCTION BOXES, ETC.), AND AS INDICATED.
- DO NOT MOUNT ELECTRICAL/ COMMUNICATION JUNCTION BOXES BACK TO BACK AND KEEP SEPARATED.
- FINISH MATERIALS USED ON INTERIOR WALLS AND CEILINGS SHALL NOT EXCEED FIRE AND SMOKE RATINGS PER TABLE 803.9 OF THE OSSC.
- VERIFY ALL FINISHES, COLORS, TEXTURES AND STYLES WITH ARCHITECT PRIOR TO PURCHASE OR INSTALLATION.
- OWNER TO RETAIN AND CONTRACTOR SHALL COORDINATE ALL NECESSARY TESTING AND ALL REQUIRED INSPECTIONS.
- FIRE EXTINGUISHERS SHALL BE LOCATED AS REQUIRED BY SECTION 906, OFC. COORDINATE FINAL LOCATION WITH FIRE MARSHALL.
- CIVIL ENGINEERING (BY OTHERS) SHALL COMPLY WITH ANSI A117.1.2009 FOR ADA ACCESSIBILITY TO AND FROM PUBLIC WAY, PARKING AND BUILDING ACCESS.
- PROVIDE ANTISKATE CURB CLIPS AT CONC. RETAINING WALLS EXPOSED AT GRADE.

LEGEND

- P.E.M.B. WALL FRAMING AND SHEATHING, OVER CONC. RETAINING WALL, SEE STRUCTURAL AND CIVIL DRAWINGS
- P.E.M.B. WALL FRAMING AND SHEATHING (TYP.) SEE P.E.M.B. DRAWINGS
- CONC. RETAINING WALL W/ CHAINLINK FENCE, SEE ELEVATIONS FOR ADDITIONAL INFO.
- D.S. P.E.M.B. DOWNSPOUT, SEE P.E.M.B. FOR SIZE, COORDINATE W/ CIVIL & ARCH FOR FINAL LOCATION
- DOOR TAG, SEE SCHEDULE
- WINDOW TAG, SEE SCHEDULE
- FINISH KEY, SEE SCHEDULE
- WALL TYPE TAG (SEE WALL TYPE DETAILS)
- WALL HEIGHT (REFERENCE BUILDING SECTIONS AND ELEVATIONS FOR CONDITIONS NOT SPECIFIED)

KEYNOTES

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PROJECT:
IPCO Development Corp.
Service Center 6
CLIENT:
IPCO Development Corporation
1425 Green Meadows Way

PROJECT LOCATION:
688 Tolman Creek Road
391E14BA-600

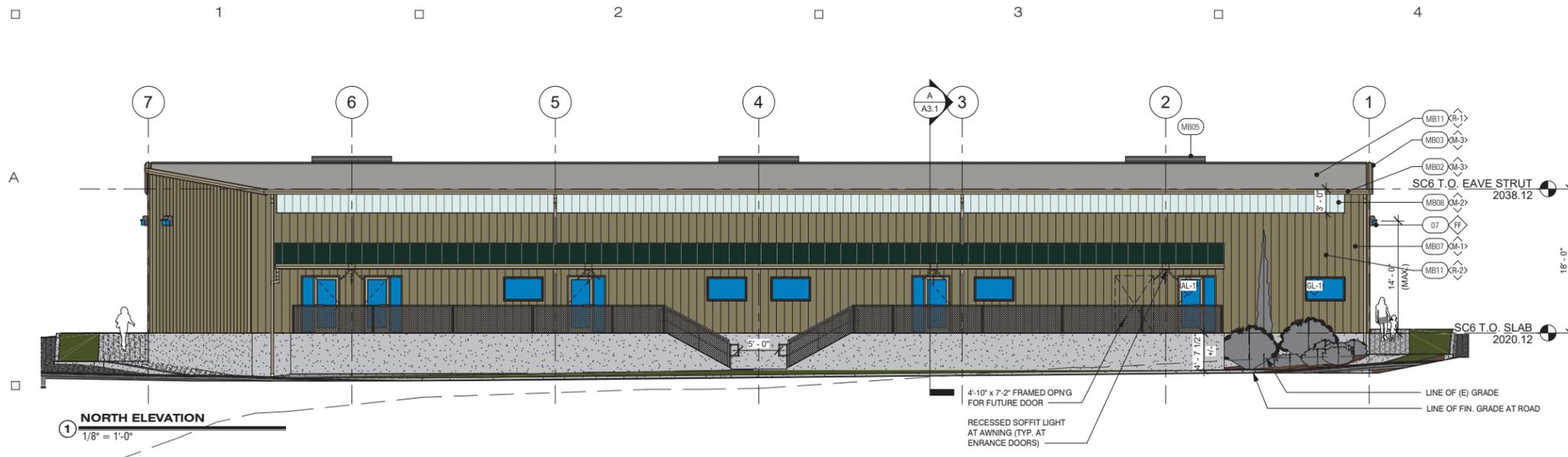
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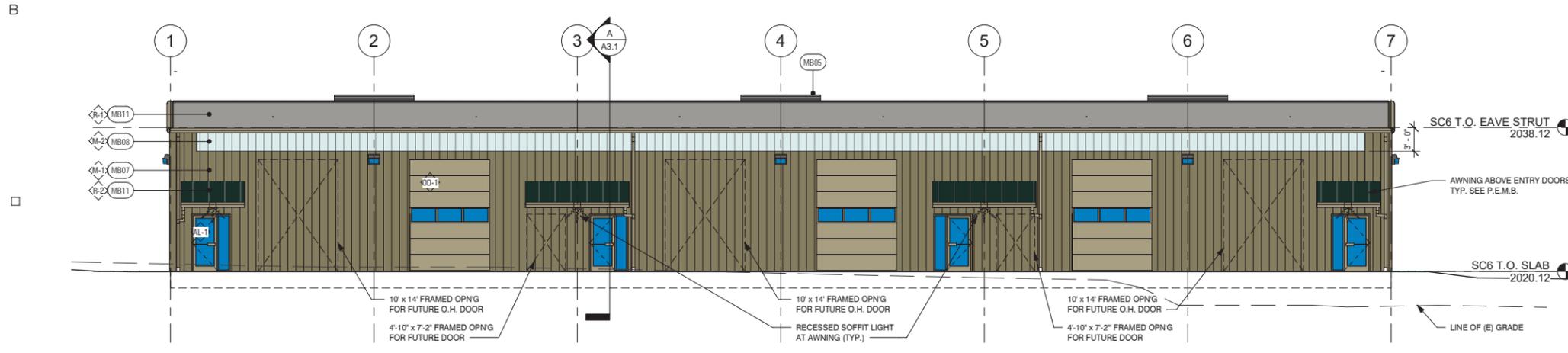
APPROVED
SHEET TITLE:
SC6 FLOOR PLAN

PLOT DATE:
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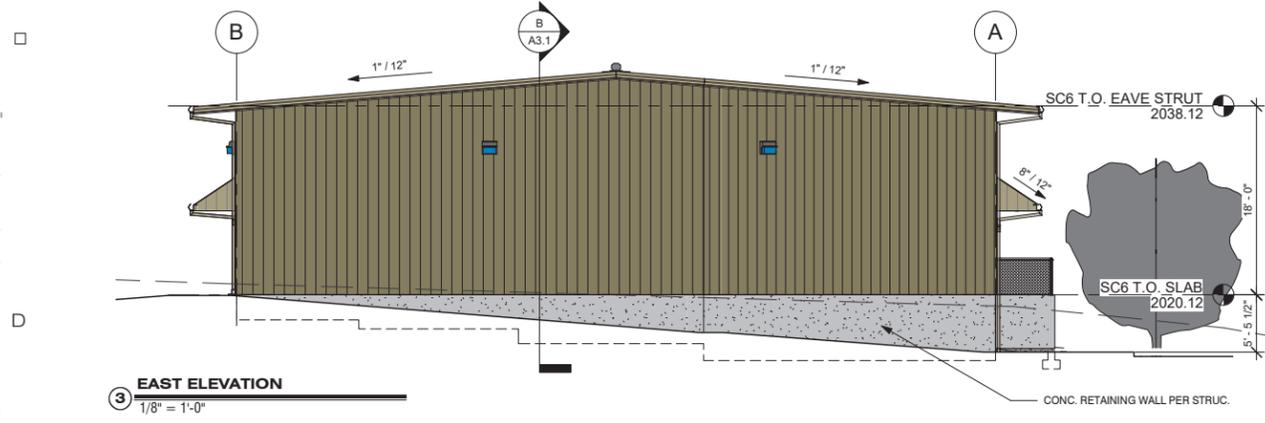
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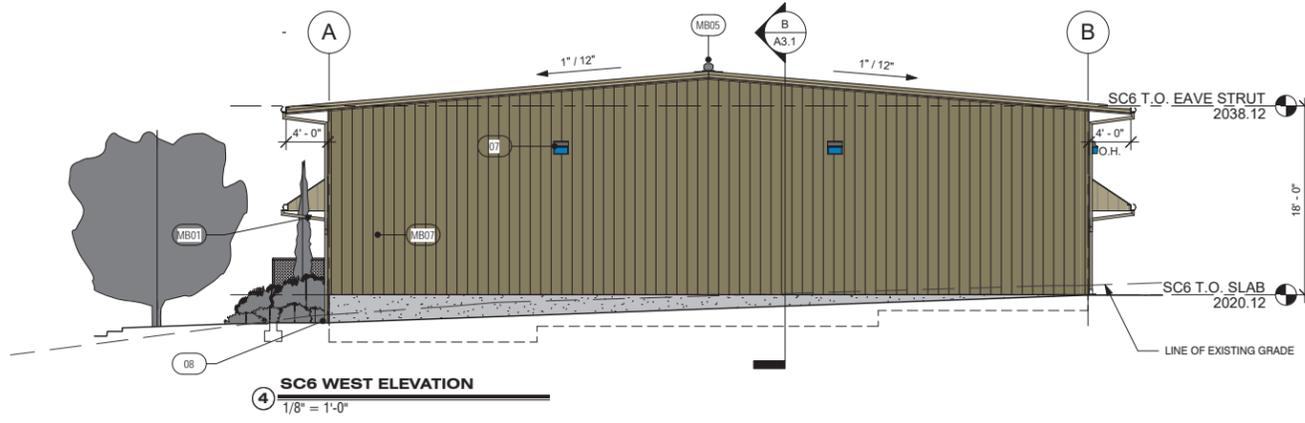
1 NORTH ELEVATION
1/8" = 1'-0"



2 SC6 SOUTH ELEVATION
1/8" = 1'-0"



3 EAST ELEVATION
1/8" = 1'-0"



4 SC6 WEST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE

SYMBOL	MATERIAL	COLOR	MFR./ PRODUCT
FF	FACTORY FINISH	VARIES	VARIES
FP-1	FIELD PAINTED METAL	N/A	BID SOURCE
M-1	METAL SIDING & TRIM	DESERT SAND	P.E.M.B. FACTORY FINISH
M-2	TRANSLUCENT SIDING	STANDARD	P.E.M.B. FACTORY FINISH
M-3	GUTTERS & DOWNSPOUTS	MATCH M-1	P.E.M.B. FACTORY FINISH
R-1	METAL PANELS	GALVALUME	P.E.M.B. FACTORY FINISH
R-2	METAL PANELS	HUNTER GREEN	P.E.M.B. FACTORY FINISH
OD-1	PRE-FINISHED METAL	DESERT TAN	BID SOURCE
AL-1	ALUM. STOREFRONT	ANTIQUE BRONZE	KAWNEER, OR EQ.
GL-1	ALUM. FRAMED WINDOWS	TAN	WINTECH, OR EQ.

ABBREVIATIONS:
P.E.M.B. PRE-ENGINEERED METAL BUILDING
MFR. MANUFACTURER
ALUM. ALUMINUM

NOTES:
1. FINAL FINISH SELECTION TO BE VERIFIED BY OWNER. PROVIDE MFR. & COLOR SPECIFIED OR EQUAL.
2. GLAZING AND STOREFRONT SYSTEM TO MEET ENERGY CODE REQUIREMENTS. SEE CODE COMPLIANCE FORMS FOR REQUIRED VALUES.

- KEYNOTES**
- 07 WALL PACK LIGHT
 - 08 D.S. CONNECTOR
 - MB01 P.E.M.B. DOWN SPOUT (D.S.)
 - MB02 P.E.M.B. PRE-FINISHED GUTTER
 - MB03 P.E.M.B. PRE-FINISHED RAKE TRIM
 - MB05 P.E.M.B. RIDGE VENT
 - MB07 P.E.M.B. PBR PANEL
 - MB08 P.E.M.B. PBR TRANSLUCENT PANEL
 - MB11 P.E.M.B. 26-GAUGE (U.N.O.) DOUBLE-LOK PRE-FINISHED METAL ROOFING PANELS

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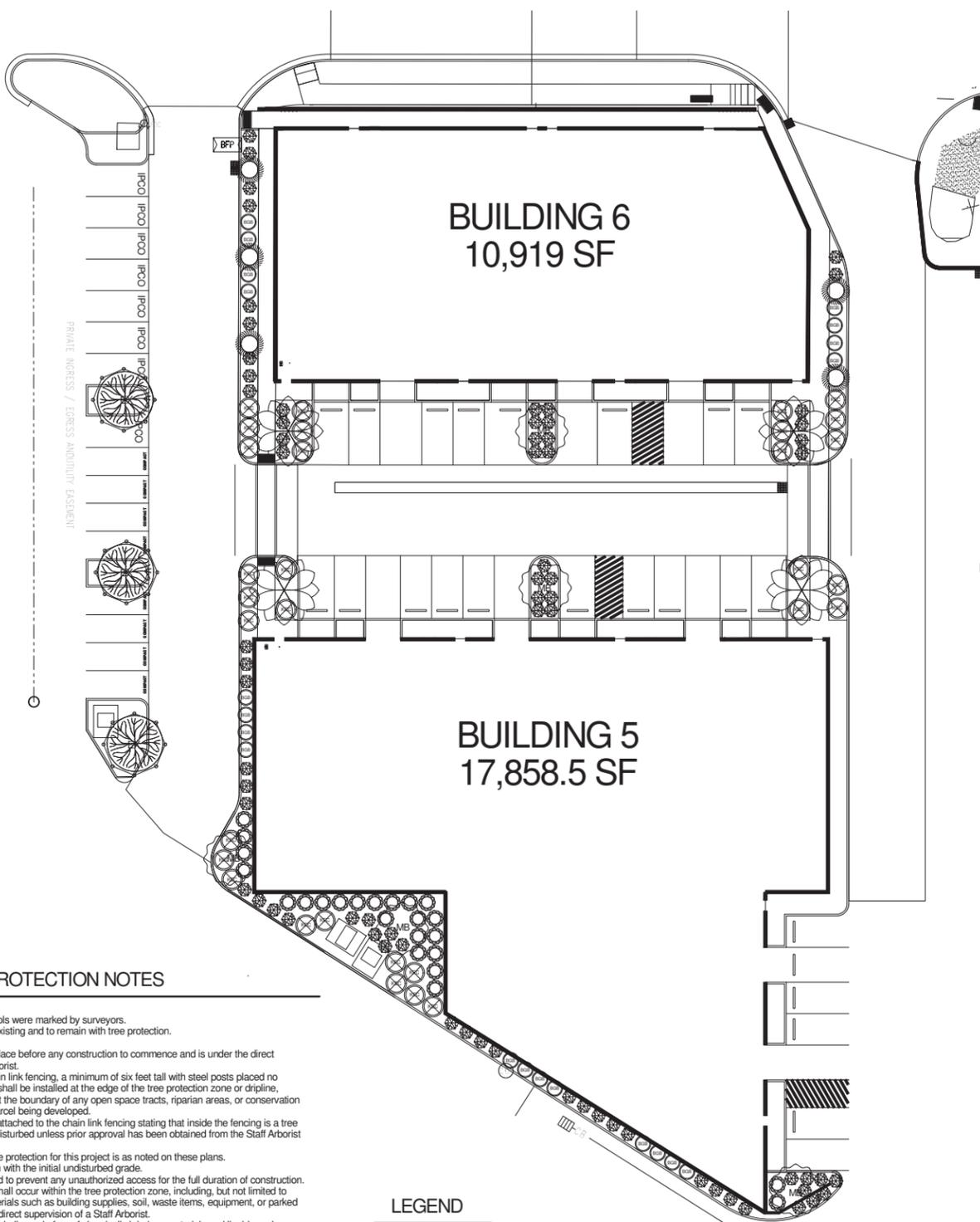
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SC6 EXTERIOR ELEVATIONS

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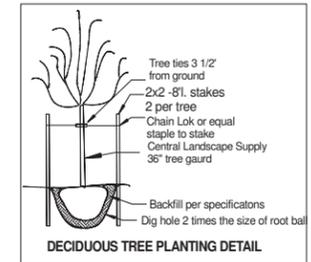
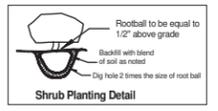
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PLANTING

Quantity	Common Name	Botanical Name	Size
Trees			
5	Hornbeam, Frans Fontaine	Carpinus betulus Frans Fontaine	1 1/2"
5	Maple, Bowhall	Acer rubrum 'Bowhall'	1 1/2"
4	Pear Chanticleer	Pyrus calleryana 'Chanticleer'	1 1/2"
Shrubs			
20	Boxwood, Green Beauty	Buxus microphylla japonica 'Green Beauty'	5g
71	Euonymus, Emerald Gaiety	Euonymus fortunei 'Emerald Gaiety'	2g
24	Laurel, Otto Luyken	Prunus laurocerasus 'Otto Luyken'	3g
Ground Cover / Grasses			
31	Kinnikinnick, Emerald Carpet	Arctostaphylos 'Emerald Carpet'	1g



GENERAL CONSTRUCTION NOTES

- SITE OBSERVATION VISITS**
 - The Landscape Architect shall be notified by the Landscape Contractor 48 hours in advance of all site observation visits required by this document or requested by the Landscape Contractor.
 - The Landscape Contractor shall be present at each site observation visit.
 - All work that is to be viewed by the Landscape Architect shall be ready and in place. The Landscape Architect has the right to have changes made to any or all of the work.
 - Site observation visits by the Landscape Architect are required for:
 - Pre-construction site meeting
 - Sub grading
 - Preliminary irrigation layout, trench locations, P.O.C and vault sizes.
 - Finish grading and soil preparation
 - Placement of plant materials prior to planting
 - Final installation checklist
 - Periodic review of completed job during maintenance period.
 - Final checklist.
 - Landscape Architect may comment and report on any other work being performed as part of any visit.
 - Additional site observation visits may be required by the Landscape Architect at any time. If more than one site observation visit is required for a particular portion of work because of excessive deficiencies (as determined by Landscape Architect), the Landscape Contractor shall be charged for additional observations including during the maintenance period.
 - General preparation of site to include:**
 - Eradication of weeds through the certified application of herbicides, allowing adequate time for effect.
 - Removal, from site, of all existing surface rock and/or debris in planting beds.
 - All shrub beds to be finish raked to a smooth condition prior to mulching.
 - Grave 3/4" - 1 1/2" deep to be placed in all shrub beds to a depth of 3"
 - INCLUDE 365 DAYS OF MAINTENANCE** from the day of acceptance. Including but not limited to:
 - Maintain planting areas in a healthy, weed free condition through a minimum of bi-weekly visits.
 - Replace any material showing signs of stress.
 - Monitor irrigation for correct timing.
 - Provide owner with complete list of instructions for continued care.
- SEE CIVIL ENGINEERS DRAWINGS FOR GRADES

GRADING

- All planting beds to be excavated to a minimum depth of 18" or deeper as determined by surrounding concrete and hardscapes.
- Fill planters with approved top soil or equal as determined by certified soil testing. Fill with sufficient excess to allow for 25% compaction to achieve final grade. IE-24" of loose fill required for 18" depth after settling.
- Placement of any soil to be done in coordination with suitable weather condition so as to prevent damage to soil structure.
- Sub-grading and final grade to consist of to a smooth even grade, no undulation greater than plus or minus 1" within any 10 lineal feet of distance.
- All sub-grades to be adequately firm without being overly compacted.
- Once subgrade is established the Landscape Contractor is to add mature compost at a rate of 3 cu yds per 1,000 sq ft. and then rip to blend with top soil to a depth of 8" As an alternative, pre blended soil matching the specification may be used in place of blending on site.
- Finish grade, after settling, in shrub areas to be a smooth even grade mounded 3" high in the middle of beds and ending 3" below surrounding areas. All finishing grading to promote positive drainage away from structures and to be done in such a way as to eliminate puddling or collection of water.
- Landscape contractor responsible for addressing any drainage problems encountered during the course of construction, with Landscape Architect.
- SEE CIVIL ENGINEERS DRAWINGS FOR ADDITIONAL GRADING INFORMATION

FIRE PREVENTION AND CONTROL PLAN NOTES

- The Whole Parcel noted on this sheet is considered covered as General Fuel Modification Area and is intended to follow Ashland LDC Subsection 18.3.10.100.B
- All standing dead and dying vegetation shall be removed from the property and disposed of appropriately at the time of initial site development. Once done there will be no native existing materials remaining.
- All new plantings, including trees, shrubs and ground cover throughout the site are devoid of any plant materials listed on the Prohibited Flammable Plant List.
- There are no planned structures, including fencing, that are considered to be of flammable material within 5' of any planned building. Any site fencing will be of vinyl construction.

PLANTING

- Plant material to be provided in accordance with species, sizes and quantities indicated below. Substitutions to be made with the approval of landscape architect.
- No planting to proceed until irrigation system is fully functioning in the area to be planted.
- All plant holes to be dug 2 times the volume of their root ball size. Backfill shall consist of 1/3 organic mulch, 2/3 top soil, micorhizae supplement and 16-16-16 fertilizer as follows.
 - 1gal 1oz
 - 3-5gal 2oz
 - larger 4oz
- Plant upright and face to give best appearance or relationship to plants, structures and predominant viewing angle. Trees are to be planted so as to be straight up and down without the assistance of staking. Staking is solely for support against outside forces.
- Loosen and remove twine binding and burrap from around top of each root ball. Scarify root balls of plants exhibiting a root bound condition, being careful not to damage the root balls integrity. Stake and guy trees immediately after this work.
- Place and compact backfill soil mixture carefully to avoid injury to roots, and fill all voids.
- When hole is 2/3 filled with soil, completely soak and allow water to soak away at least two times or more, as necessary to completely water individual plants.
- Guarantee plant materials and related workmanship of installation, beginning after written acceptance of work, for one year.
 - Replace plant material not surviving or in poor condition during guarantee period.
 - Perform all replacement work in accordance with original specifications at no additional cost to Owner.
 - Damage or loss of plant materials due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility.
- Confirm to Landscape Architect the availability of plant materials 30 Days prior to planting.

IRRIGATION DETAILS

- An automatic irrigation system to be provided for all plant materials areas following the specifications outlined on these plans and in accordance with industry standards. System is intended to perform at 10 gpm and 50 psi. Confirm on-site before proceeding.
- All materials are to be new and in original condition.
- Connect to existing irrigation and confirm correct function of the existing double check valve.
- Place manual drain valves as needed at low points in mainline to insure complete drainage during winter shut down.
- Mainline should be located in area with least conflict with surrounding utilities. Mainline location on plan for ease of interpretation
- Control wires, 14ga minimum, to be located below all piping in any ditch
- All drip zones to use PVC laterals to locate a point of connection in each individual planting bed.
 - All surface drip tubing to be Landscaped Product In-line Drip Tubing, .6GPH on 18" OC to buried a range of 3-5' and held down every 5' with J-stakes. Layout to comprise drip tubing laced beginning 12" in from any perimeter hardscape and in equal parallel lines 16" OC thereafter.
 - All Drip zones to include a 150 mesh filter and 30psi pressure regulator
- Sleeving to be provided under all hardscapes by general contractor for irrigation purposes.
- Contractor responsible for any and all safety, security of materials and damage caused by the contractor to existing facilities during the course of installation.
- Irrigation system to be guaranteed against defective material or workmanship for one year from the date of final acceptance. Damage or loss due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility after completion and acceptance of installation.
- Provide owner with an accurate as-built locating all valves, wire splices, main line and any sleeving.
- Use provided irrigation schedule as a base-line and adjust only as needed for varying weather conditions staying within the general scheme provided. See schedule provided elsewhere.
- Provide owner with complete set of written instructions for operation of sprinkler system including spring start up, clock operation, and winterization.
- Walk owner through the entire system describing the operating instructions.



TREE PROTECTION NOTES

- Trees Highlighted as symbols were marked by surveyors.
Trees noted with 'EX' are existing and to remain with tree protection.
- Tree protection to be in place before any construction to commence and is under the direct supervision of the Staff Arborist.
 - Tree protection to be chain link fencing, a minimum of six feet tall with steel posts placed no farther than ten feet apart, shall be installed at the edge of the tree protection zone or dripline, whichever is greater, and at the boundary of any open space tracts, riparian areas, or conservation easements that abut the parcel being developed.
 - Approved signs shall be attached to the chain link fencing stating that inside the fencing is a tree protection zone, not to be disturbed unless prior approval has been obtained from the Staff Arborist for the project.
 - The actual location or tree protection for this project is as noted on these plans.
 - The fencing shall be flush with the initial undisturbed grade.
 - Fencing shall be enclosed to prevent any unauthorized access for the full duration of construction.
 - No construction activity shall occur within the tree protection zone, including, but not limited to dumping or storage of materials such as building supplies, soil, waste items, equipment, or parked vehicles, except under the direct supervision of a Staff Arborist.
 - The tree protection zone shall remain free of chemically injurious materials and liquids such as paints, thinners, cleaning solutions, petroleum products, concrete or dry wall excess, and construction debris or run-off.
 - No excavation, trenching, grading, root pruning, or other activity shall occur within the tree protection zone unless approved by the Staff Arborist.
 - Any work necessary within the dripline is subject to prior approval and direction of the Staff Arborist.
 - Trees being protected will be watered regularly throughout the growing season.
 - Any damage to protected trees shall be reported to the Staff Arborist within 24 hours of observation.
 - Except as otherwise determined by the Staff Arborist, all required tree protection measures set forth in this section shall be instituted prior to any development activities, including, but not limited to clearing, grading, excavation, or demolition work, and shall be removed only after completion of all construction activity, including landscaping and irrigation installation.

LEGEND



NOTE: IF THIS SHEET IS LESS THAN 24" x 36" IT HAS BEEN REDUCED AND IS NOT TO SCALE.



Madara Design Inc
Landscape Design & Consultation
2994 Wells Fargo Rd
Central Point, Or 97502
541-664-7055



J.B. STEEL, INC.
COMMERCIAL & INDUSTRIAL

P.O. Box 4460 - 190 North Ross Lane
Medford, Oregon 97501
Office: 541.773.8325 - Fax: 541.773.6523
OR CCB# - 22599
Web: www.jbsteelinc.com

PROJECT: A NEW PRE-ENGINEERED METAL BUILDING FOR:
IPCO DEVELOPMENT

CUELIENT:
IPCO DEVELOPMENT CORPORATION
1425 Green Meadow Way, Ashland, OR 97520
Project Lt Location:
688 Tolman Creek Rd, Ashland, OR 97520
Map & Tax Lot#: 39 E1, 14 BA, 600

BUILDING DEPARTMENT APPROVAL STAMP:

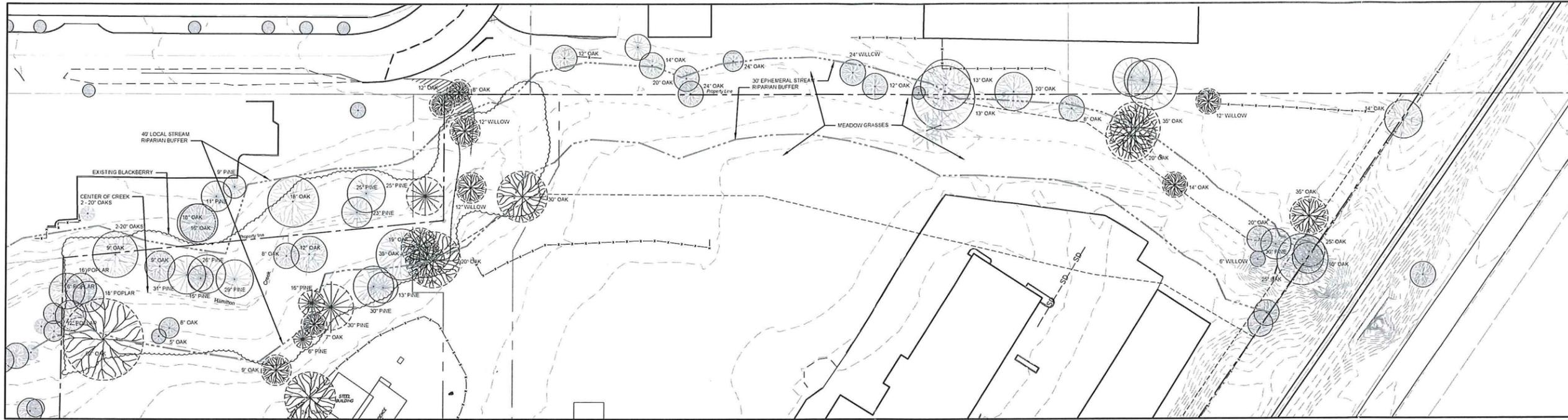
MARK	DATE	DESCRIPTION

Issue date: 11/05/2020
PROJECT NO: 2913-20
DRAWN BY:
CHECKED BY:

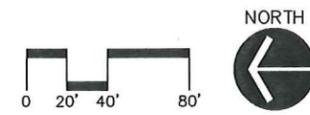
SHEET TITLE:

L1
FOR APPLICATION
NOT FOR CONSTRUCTION

PRELIMINARY - NOT FOR CONSTRUCTION - FOR REVIEW and DISCUSSION PURPOSES ONLY



SITE PLAN - EXISTING CONDITIONS



LOCATION MAP not to scale

REGISTERED
435
Alan D. Pardee
OREGON
10/31/97
LANDSCAPE ARCHITECT

CoveyPardee
LANDSCAPE ARCHITECTS
125 S.W. MAIN ST.
ASHLAND, OR 97103
503.222.0335
info@coveypardee.com

IPCO DEVELOPMENT
WATER RESOURCE PROTECTION PLAN
Tolman Creek Road to Washington Street
Ashland, Oregon

RECEIVED

JUN 01 2015

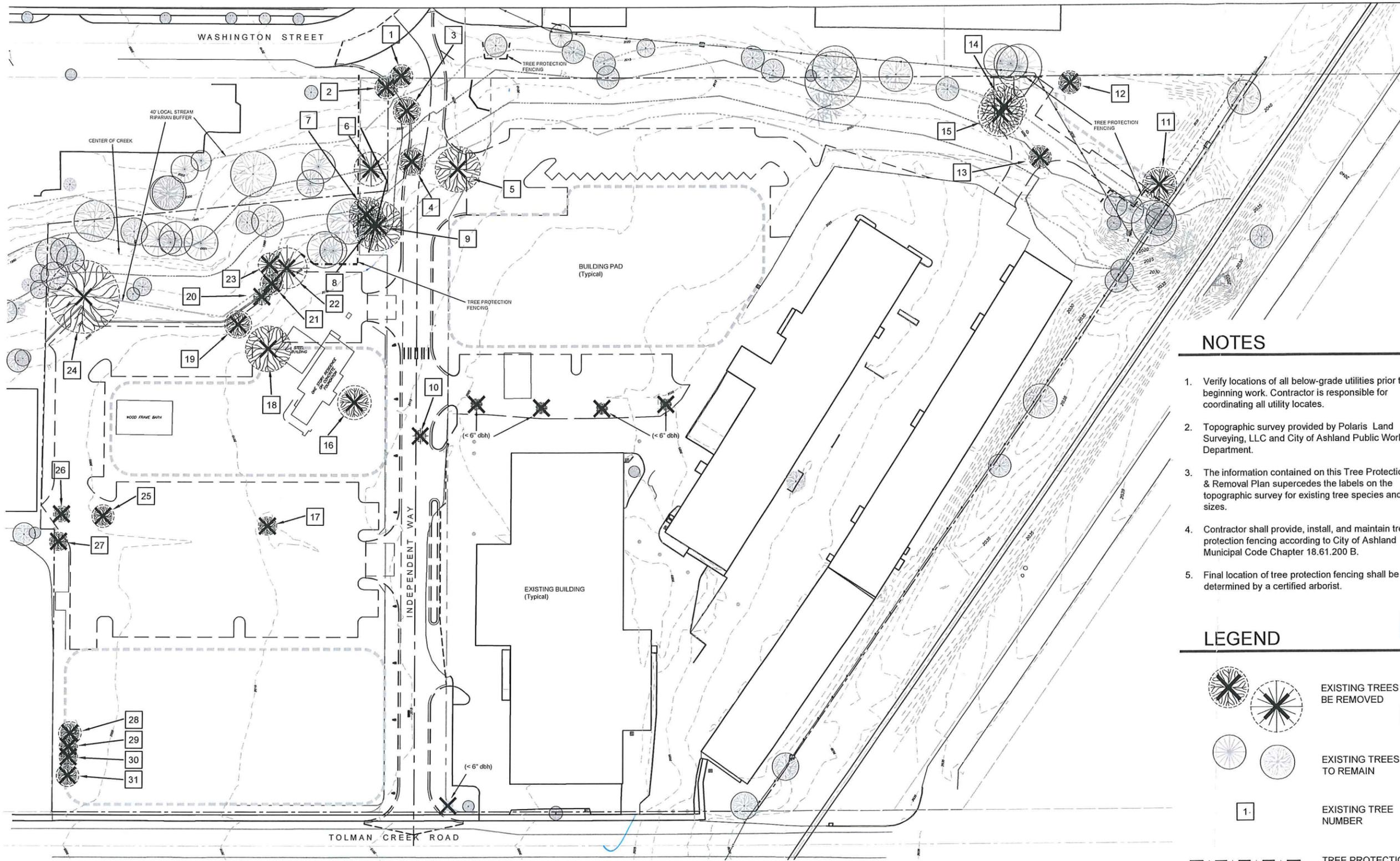
City Of Ashland

NO.	ISSUE/REV.	DATE

SHEET TITLE
EXISTING SITE

DATE	10 MAY 2015
SCALE	1"=40'
DRAWN	ADP
JOB	IPCO

SHEET
L-1
FILE 14163



NOTES

1. Verify locations of all below-grade utilities prior to beginning work. Contractor is responsible for coordinating all utility locates.
2. Topographic survey provided by Polaris Land Surveying, LLC and City of Ashland Public Works Department.
3. The information contained on this Tree Protection & Removal Plan supercedes the labels on the topographic survey for existing tree species and sizes.
4. Contractor shall provide, install, and maintain tree protection fencing according to City of Ashland Municipal Code Chapter 18.61.200 B.
5. Final location of tree protection fencing shall be determined by a certified arborist.

LEGEND

-   EXISTING TREES TO BE REMOVED
-   EXISTING TREES TO REMAIN
-  EXISTING TREE NUMBER
-  TREE PROTECTION FENCING (See Note 4.)

TREE REMOVAL LIST

NOTE: TREES 1 - 10 ARE WITHIN THE INDEPENDENT WAY R.O.W.
TREES 11 - 31 ARE WITHIN THE PRIVATE DEVELOPMENT AREA

1 8" OAK	5 30" OAK	9 20" OAK	13 14" OAK	17 12" MAPLE	21 7" OAK	25 20" POPLAR	29 8" OAK
2 12" OAK	6 25" PONDEROSA PINE	10 6" PONDEROSA PINE	14 25" OAK	18 24" OAK	22 30" PONDEROSA PINE	26 18" POPLAR	30 8" OAK
3 12" WILLOW	7 19" OAK	11 35" OAK	15 20" OAK	19 9" OAK	23 16" PONDEROSA PINE	27 14" POPLAR	31 8" OAK
4 12" WILLOW	8 10" OAK	12 12" WILLOW	16 18" MAPLE	20 6" PINE	24 60" OAK (DYING)	28 9" OAK	

REGISTERED
435
Alan D. Pardee
OREGON
10/31/97
LANDSCAPE ARCHITECT

CoveyPardee
LANDSCAPE ARCHITECTS
205 EAST MAIN, SUITE 8
ASHLAND, OREGON 97102
541.333.1333
info@coveypardee.com

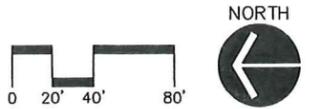
IPCO DEVELOPMENT
Tolman Creek Road to Washington Street
Ashland, Oregon

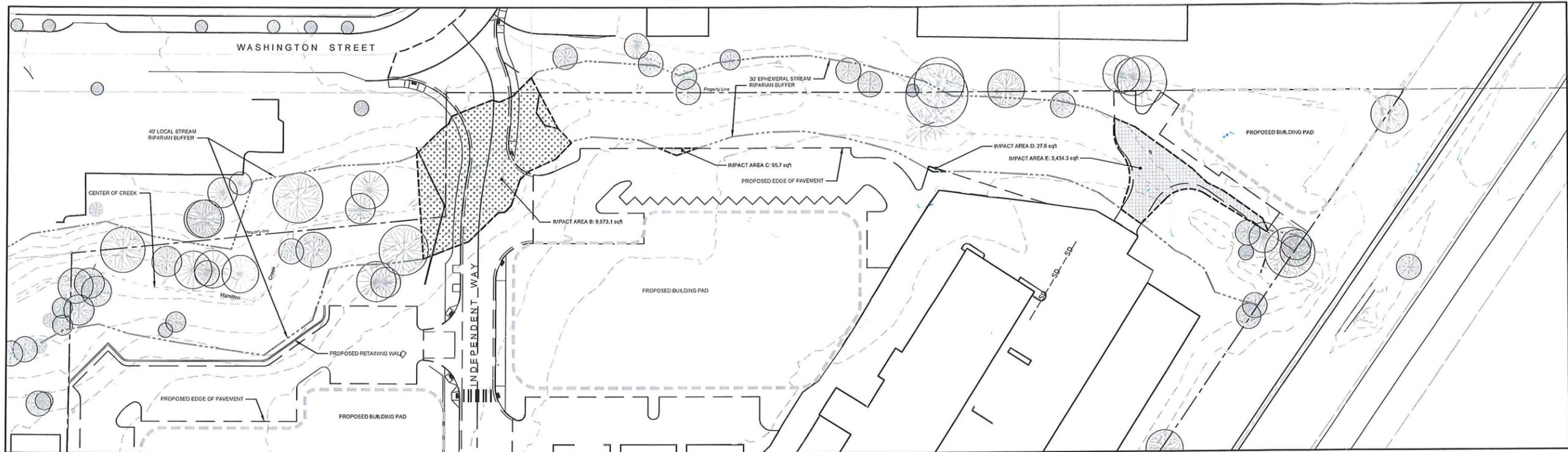
RECEIVED
JUN 01 2015
City of Ashland
NO. ISSUE/REV. DATE

SHEET TITLE
TREE PROTECTION & REMOVAL PLAN

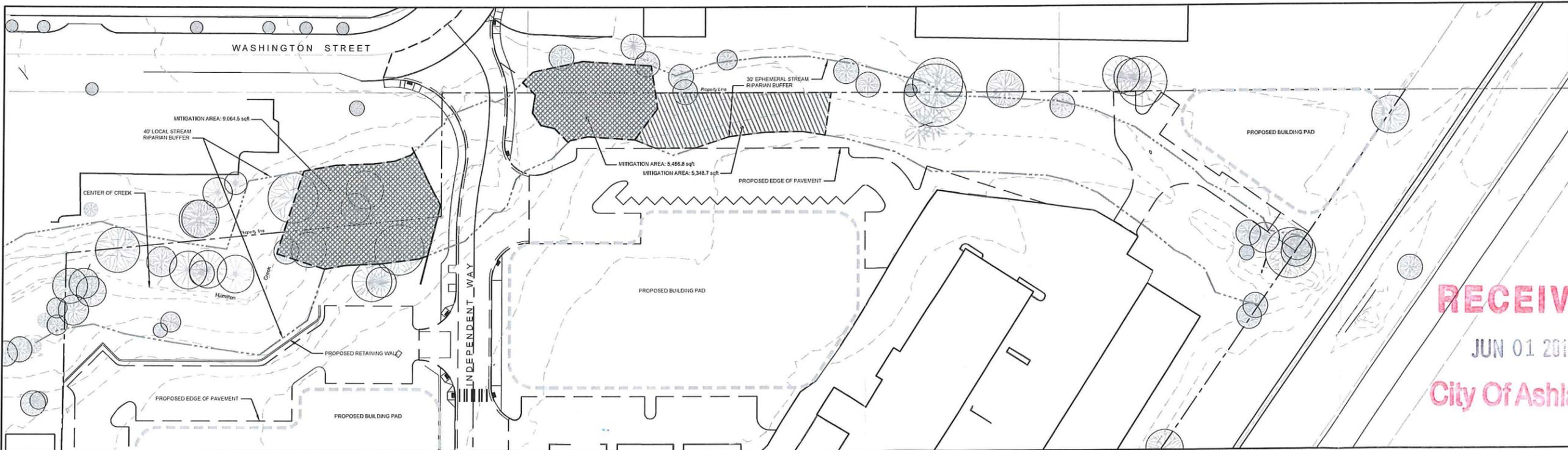
DATE 10 MAY 2015
SCALE 1"=40'
DRAWN ADP
JOB IPCO

SHEET
L-2
FILE 1416-TR





SITE PLAN - IMPACTED AREAS



SITE PLAN - MITIGATION AREAS

KEY

IMPACT AREAS -

- PUBLIC: 9,573.1 sq feet x 1.5 = 14,359.7 sq feet mitigation required
- PRIVATE: 3,557.6 sq feet x 1.5 = 5,336.4 sq feet mitigation required

MITIGATION AREAS -

- PUBLIC: 14,551.3 sq feet mitigation area provided
- PRIVATE: 5,348.7 sq feet mitigation area provided



REGISTERED
435
Alan D. Pardee
OREGON
10/31/97
LANDSCAPE ARCHITECT

CoveyPardee
LANDSCAPE ARCHITECTS
255 EAST MAIN, SUITE 8
ASHLAND, OR 97102
503.261.1333
info@coveypardee.com

IPCO DEVELOPMENT
WATER RESOURCE PROTECTION PLAN
Tolman Creek Road
Ashland, Oregon

NO. ISSUE/REV. DATE

RECEIVED

JUN 01 2015

City Of Ashland

SHEET TITLE
IMPACT &
MITIGATION
AREAS

DATE 10 MAY 2015

SCALE 1"=40'

DRAWN ADP

JOB IPCO

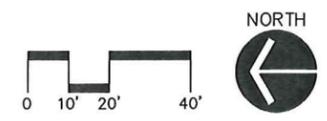
SHEET

L-3

FILE 1416-PR



SCHEMATIC MITIGATION PLAN



KEY

MITIGATION PLANTING	
Riparian Wetland Plants:	Rush, Sedge, Bulrush, Spike-rush, Wapato
Understory (low) Shrubs & Grasses:	Red Osier Dogwood, Currant, Oregon Grape, Orchard Grass, Fescue, Barley
Understory (med) Trees & Shrubs:	Western Dogwood, Vine Maple, Alder, Willow, Ninebark,
Trees:	Big Leaf Maple, Oak, Poplar, Ponderosa Pine

PLANTING NOTE: Mitigation plants will be selected from the species noted above and the City of Ashland's Rogue Basin Native Plants lists. Trees, shrubs, grasses, and riparian wetland plants will be planted from a variety of container sizes as well as by canes and rooted saplings.

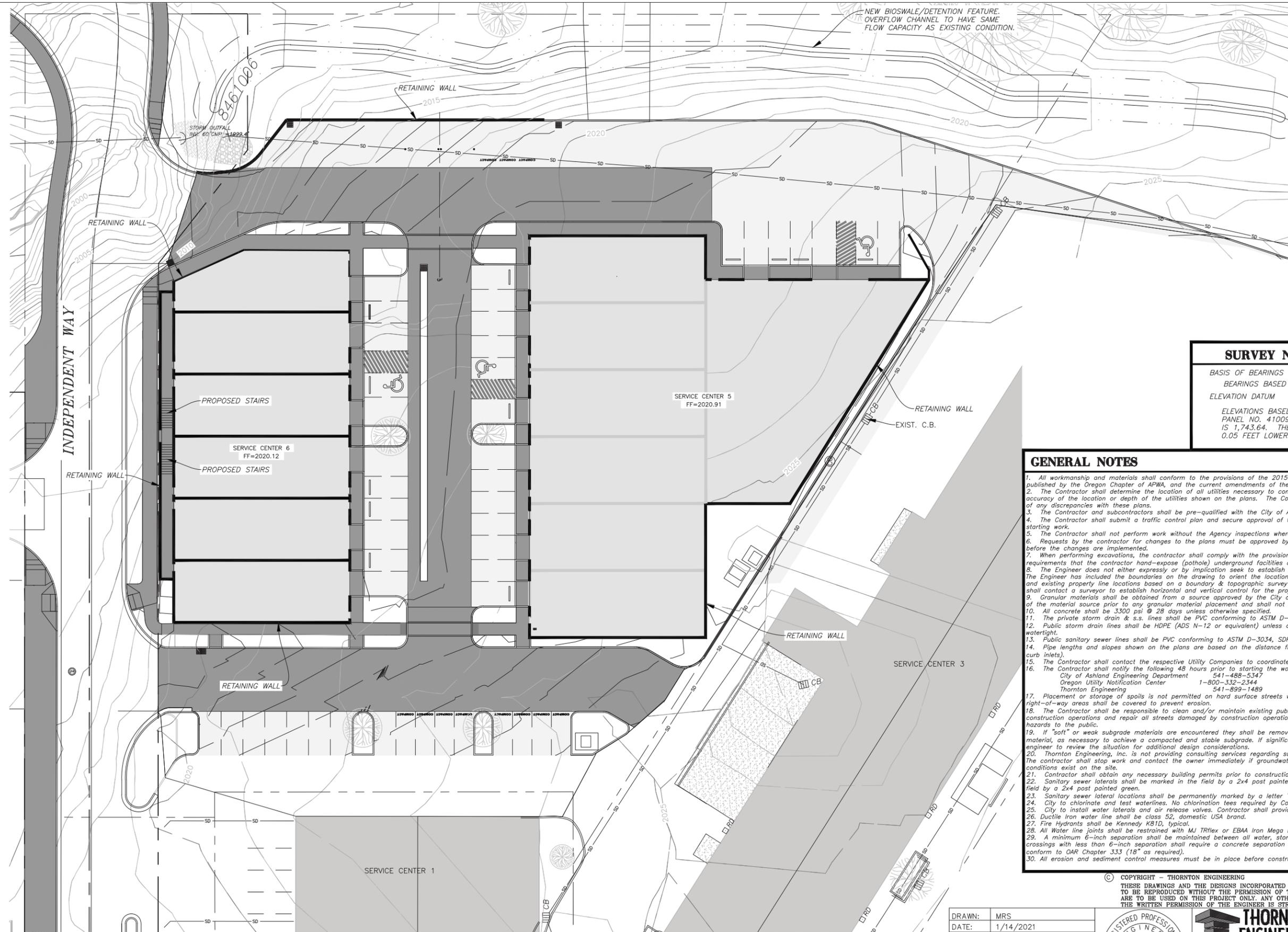
**IPCO DEVELOPMENT
WATER RESOURCE PROTECTION PLAN**
Tolman Creek Road
Ashland, Oregon

NO.	ISSUE/REV.	DATE

SHEET TITLE	
SCHEMATIC MITIGATION PLAN	
DATE	18 MAY 2015
SCALE	1"=20'
DRAWN	ADP
JOB	IPCO

RECEIVED
JUN 01 2015
City Of Ashland

SHEET
L-4
FILE 4414-TR
96



SURVEY NOTES

BASIS OF BEARINGS
 BEARINGS BASED ON FILED SURVEY NO. 16744

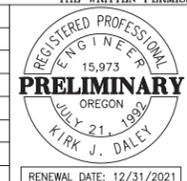
ELEVATION DATUM
 ELEVATIONS BASED ON FEMA BENCHMARK RM1, COMMUNITY PANEL NO. 410090 0001 B. PUBLISHED ELEVATION FOR RM1 IS 1,743.64. THE CITY OF ASHLAND DATUM IN THIS AREA IS 0.05 FEET LOWER THAN THE FEMA DATUM.

- GENERAL NOTES**
- All workmanship and materials shall conform to the provisions of the 2015 Standard Specifications for Public Works Construction published by the Oregon Chapter of APWA, and the current amendments of the City of Ashland.
 - The Contractor shall determine the location of all utilities necessary to complete the work. The Engineer does not guarantee the accuracy of the location or depth of the utilities shown on the plans. The Contractor shall pothole existing utilities and notify Engineer of any discrepancies with these plans.
 - The Contractor and subcontractors shall be pre-qualified with the City of Ashland prior to any construction within City right-of-way.
 - The Contractor shall submit a traffic control plan and secure approval of the plan from the City at least (5) working days prior to starting work.
 - The Contractor shall not perform work without the Agency inspections where inspections are required by the specifications.
 - Requests by the contractor for changes to the plans must be approved by the consulting engineer and the Agency's engineer before the changes are implemented.
 - When performing excavations, the contractor shall comply with the provisions of ORS 757.541 to 757.571, which include requirements that the contractor hand-expose (pothole) underground facilities and use reasonable care to avoid damaging them.
 - The Engineer does not either expressly or by implication seek to establish or delineate the property and right-of-way boundaries. The Engineer has included the boundaries on the drawing to orient the location of the work only. The Engineer has drawn centerline and existing property line locations based on a boundary & topographic survey provided by Polaris Land Surveying. The Contractor shall contact a surveyor to establish horizontal and vertical control for the project.
 - Granular materials shall be obtained from a source approved by the City of Ashland. The contractor shall notify the City Engineer of the material source prior to any granular material placement and shall not change material source without approval.
 - All concrete shall be 3300 psi @ 28 days unless otherwise specified.
 - The private storm drain & s.s. lines shall be PVC conforming to ASTM D-3034, SDR 35. All joints shall be watertight.
 - Public storm drain lines shall be HDPE (ADS N-12 or equivalent) unless otherwise noted on the plans. All joints shall be watertight.
 - Public sanitary sewer lines shall be PVC conforming to ASTM D-3034, SDR 35. All joints shall be watertight.
 - Pipe lengths and slopes shown on the plans are based on the distance from center of manhole or structure (or face of curb for curb inlets).
 - The Contractor shall contact the respective Utility Companies to coordinate relocation or reconstruction of any utilities.
 - The Contractor shall notify the following 48 hours prior to starting the work:
 City of Ashland Engineering Department 541-488-5347
 Oregon Utility Notification Center 1-800-332-2344
 Thornton Engineering 541-899-1489
 - Placement or storage of spoils is not permitted on hard surface streets within public right-of-way. Spoils stored in other than right-of-way areas shall be covered to prevent erosion.
 - The Contractor shall be responsible to clean and/or maintain existing public streets of soil or other debris deposited by construction operations and repair all streets damaged by construction operations in a timely manner to avoid inconveniences or hazards to the public.
 - If "soft" or weak subgrade materials are encountered they shall be removed and replaced with compacted aggregate sub-base material, as necessary to achieve a compacted and stable subgrade. If significant subgrade areas are "soft" contact the geotechnical engineer to review the situation for additional design considerations.
 - Thornton Engineering, Inc. is not providing consulting services regarding subsurface soil and groundwater conditions for this project. The contractor shall stop work and contact the owner immediately if groundwater is encountered, or if unusually soft or unstable soil conditions exist on the site.
 - Contractor shall obtain any necessary building permits prior to construction or verify that the owner has done so.
 - Sanitary sewer laterals shall be marked in the field by a 2x4 post painted white. Storm drain laterals shall be marked in the field by a 2x4 post painted green.
 - Sanitary sewer lateral locations shall be permanently marked by a letter 'S' stamped in the top of curb.
 - City to chlorinate and test waterlines. No chlorination fees required by Contractor. Contractor is to provide pig.
 - City to install water laterals and air release valves. Contractor shall provide excavation, backfill and surfacing.
 - Ductile iron water line shall be class 52, domestic USA brand.
 - Fire Hydrants shall be Kennedy K81D, typical.
 - All Water line joints shall be restrained with MJ TRifix or EBAA Iron Mega Lug.
 - A minimum 6-inch separation shall be maintained between all water, storm drain and sanitary sewer main crossings. Any crossings with less than 6-inch separation shall require a concrete separation as approved by PWD. All water and SS crossings shall conform to OAR Chapter 333 (18" as required).
 - All erosion and sediment control measures must be in place before construction begins.

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JOB NO. 20-047
 FILE: BASEMAP.DWG
 (SEE 14-043)

DRAWN:	MRS
DATE:	1/14/2021
	REVISIONS

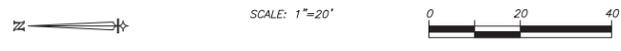


THORNTON ENGINEERING INC. p.o. box 476 • 260 north 3rd street
 jacksonville, oregon 97530
 (541) 899-1489 (541) 899-3419 fax

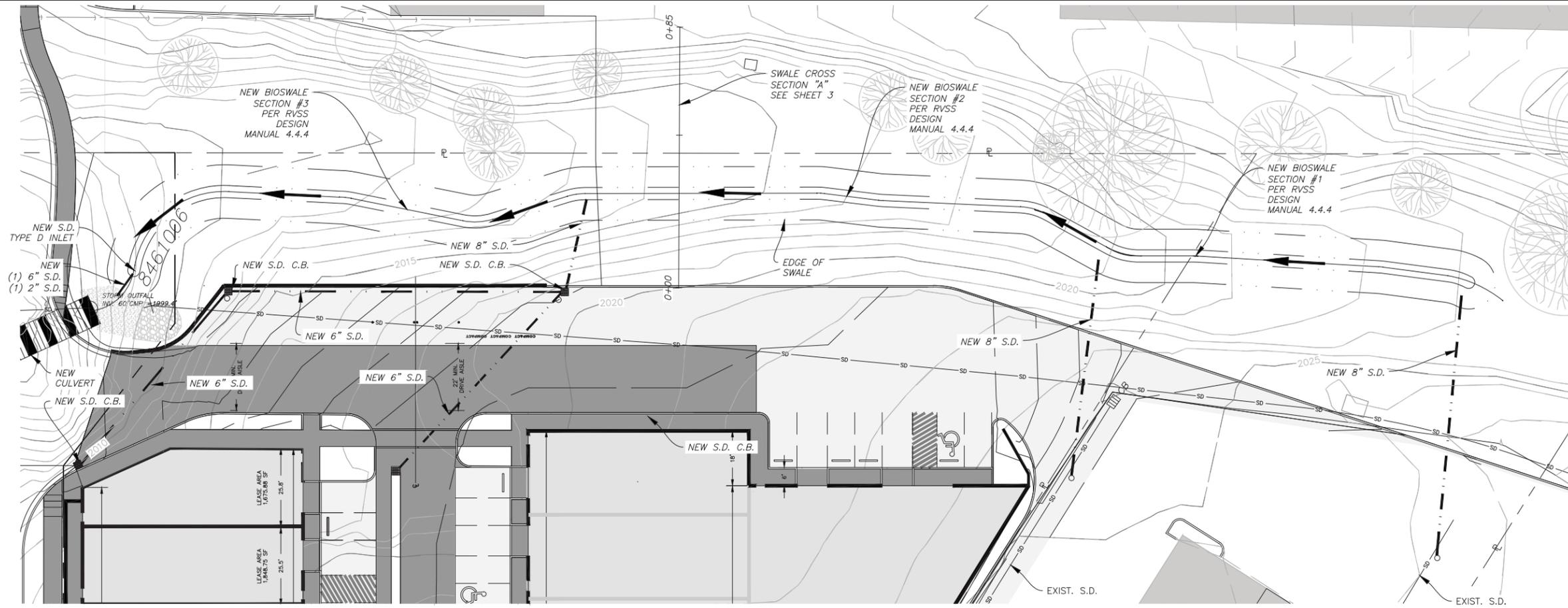
GRADING PLAN
 SERVICE CENTERS 5 & 6
 IPCO DEVELOPMENT CORPORATION
 640 TOLMAN CREEK ROAD
 ASHLAND, OREGON

SHEET
 C3

GRADING PLAN - IPCO DEVELOPMENT INC. (PHASE 2)



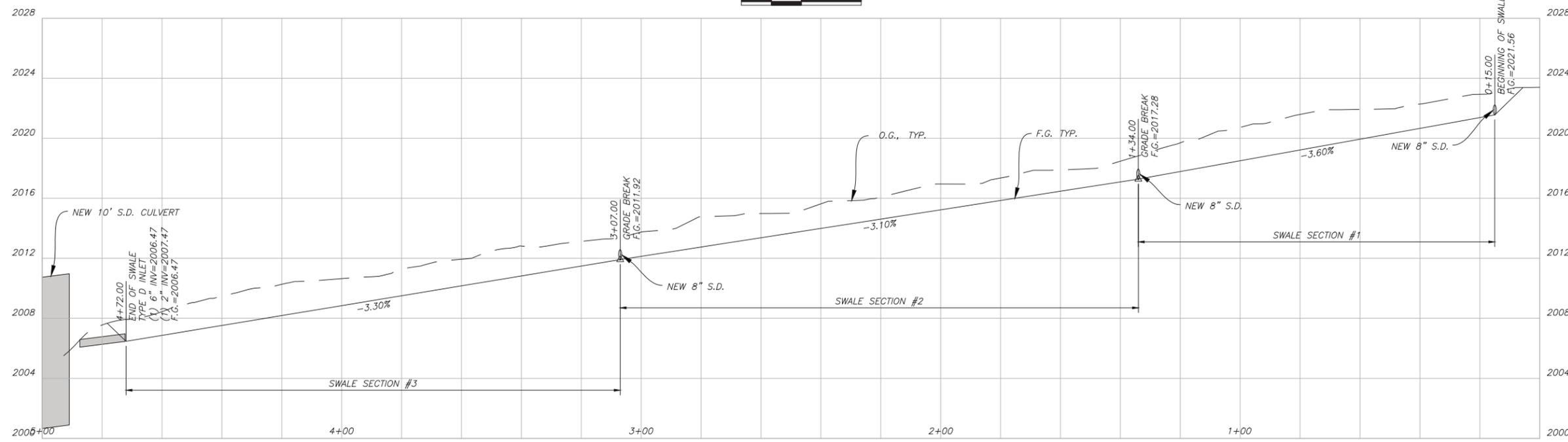
CALL FOR UTILITY LOCATE 48 HOURS
 PRIOR TO EXCAVATION - 1-800-332-2344



SITE PLAN -IPCO DEVELOPMENT

SCALE: 1"=20'
0 20 40

HYDROLOGIC NOTES	
SWALE SECTION #1:	
WATER QUALITY EVENT (1-YEAR STORM)	-Flow into Swale=0.052 cfs
	-Depth: -0+15 to 1+34=0.07'
	-Treatment Residence Time: -0+15 to 1+34=11.66 minutes
CONVEYANCE EVENT (10-YEAR STORM)	-Flow into Swale=0.800 cfs
	-Depth: -0+15 to 1+34=0.30'
	-Total Swale Depth=2.25'
SWALE SECTION #2:	
WATER QUALITY EVENT (1-YEAR STORM)	-Flow into Swale=0.201 cfs
	-Depth: -1+34 to 3+07=0.15'
	-Treatment Residence Time: -1+34 to 3+07=10.68 minutes
CONVEYANCE EVENT (10-YEAR STORM)	-Flow into Swale=1.99 cfs
	-Depth: -1+34 to 3+07=0.50'
	-Total Swale Depth=2.25'
SWALE SECTION #3:	
WATER QUALITY EVENT (1-YEAR STORM)	-Flow into Swale=0.251 cfs
	-Depth: -3+07 to End=0.17'
	-Treatment Residence Time: -3+07 to End=9.48 minutes
CONVEYANCE EVENT (10-YEAR STORM)	-Flow into Swale=2.824 cfs
	-Depth: -3+07 to End=0.58'
	-Total Swale Depth=2.25'

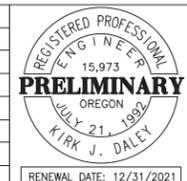


SWALE PROFILE -IPCO DEVELOPMENT

SCALE: 1"=20'
0 20 40

SCALE: 1"=4" VERTICAL
0 2 4

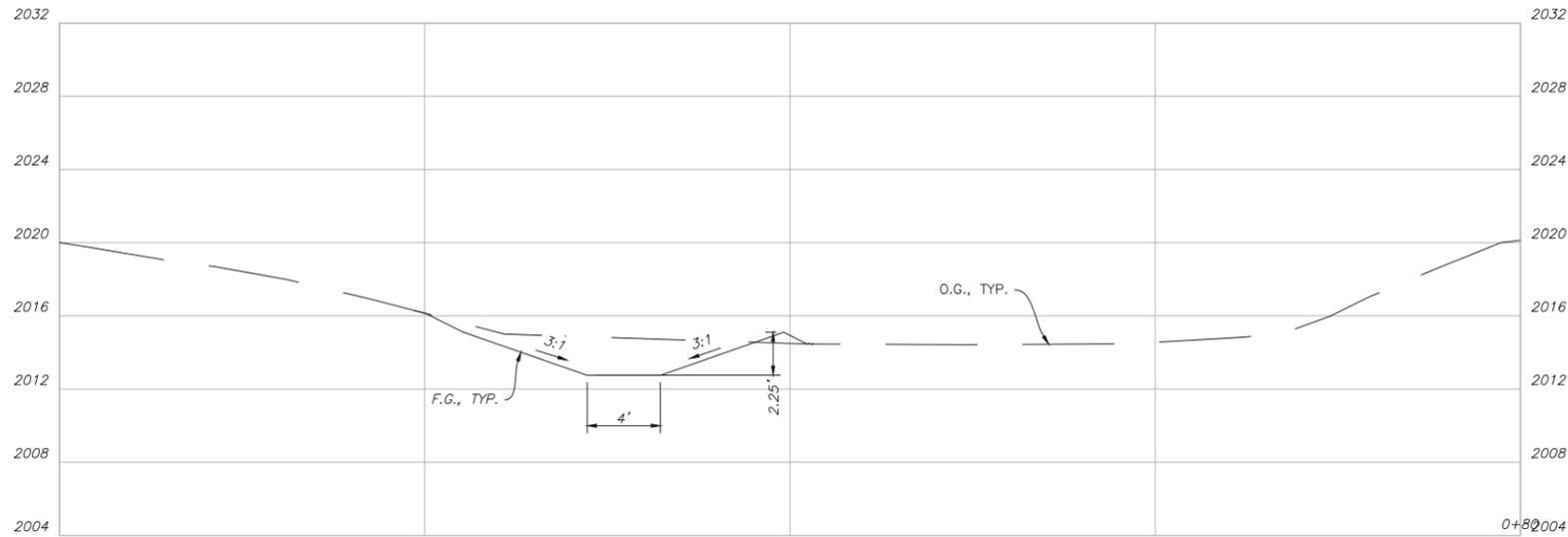
DRAWN:	MRS
DATE:	1/14/2021
REVISIONS:	



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jacksonville, oregon 97530
(541) 899-1489 (541) 899-3419 fax

BIOSWALE PLAN AND PROFILE
SERVICE CENTERS 5 & 6
IPCO DEVELOPMENT CORPORATION
640 TOLMAN CREEK ROAD
ASHLAND, OREGON

SHEET C5

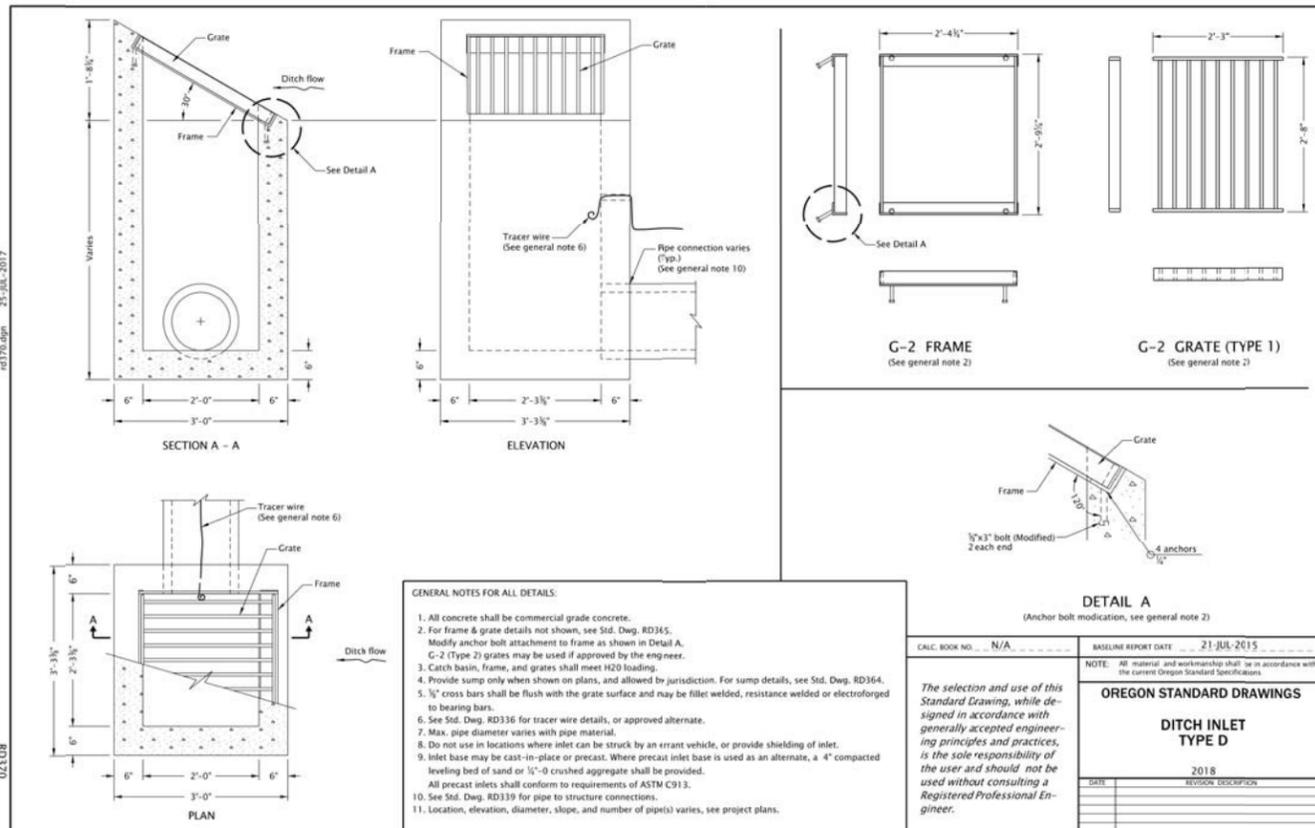


SWALE CROSS SECTION "A"

Scale: 1"=5'



SCALE: 1"=5' VERTICAL



- GENERAL NOTES FOR ALL DETAILS:**
- All concrete shall be commercial grade concrete.
 - For frame & grate details not shown, see Std. Dwg. RD315. Modify anchor bolt attachment to frame as shown in Detail A.
 - G-2 (Type 2) grates may be used if approved by the engineer.
 - Catch basin, frame, and grates shall meet H20 loading.
 - Provide sump only when shown on plans, and allowed by jurisdiction. For sump details, see Std. Dwg. RD364.
 - 1/2" cross bars shall be flush with the grate surface and may be fillet welded, resistance welded or electroforged to bearing bars.
 - See Std. Dwg. RD336 for tracer wire details, or approved alternate.
 - Max. pipe diameter varies with pipe material.
 - Do not use in locations where inlet can be struck by an errant vehicle, or provide shielding of inlet.
 - Inlet base may be cast-in-place or precast. Where precast inlet base is used as an alternate, a 4" compacted leveling bed of sand or 1/2"-0" crushed aggregate shall be provided.
 - All precast inlets shall conform to requirements of ASTM C913.
 - See Std. Dwg. RD339 for pipe to structure connections.
 - Location, elevation, diameter, slope, and number of pipe(s) varies, see project plans.

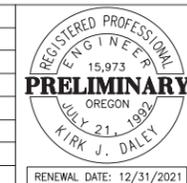
CALL BOOK NO. N/A	BASELINE REPORT DATE 21 JUL 2015
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.	
OREGON STANDARD DRAWINGS	
DITCH INLET TYPE D	
DATE 2018	REVISION DESCRIPTION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

Effective Date: December 1, 2019 – May 31, 2020

RD370

DRAWN:	MRS
DATE:	1/14/2021



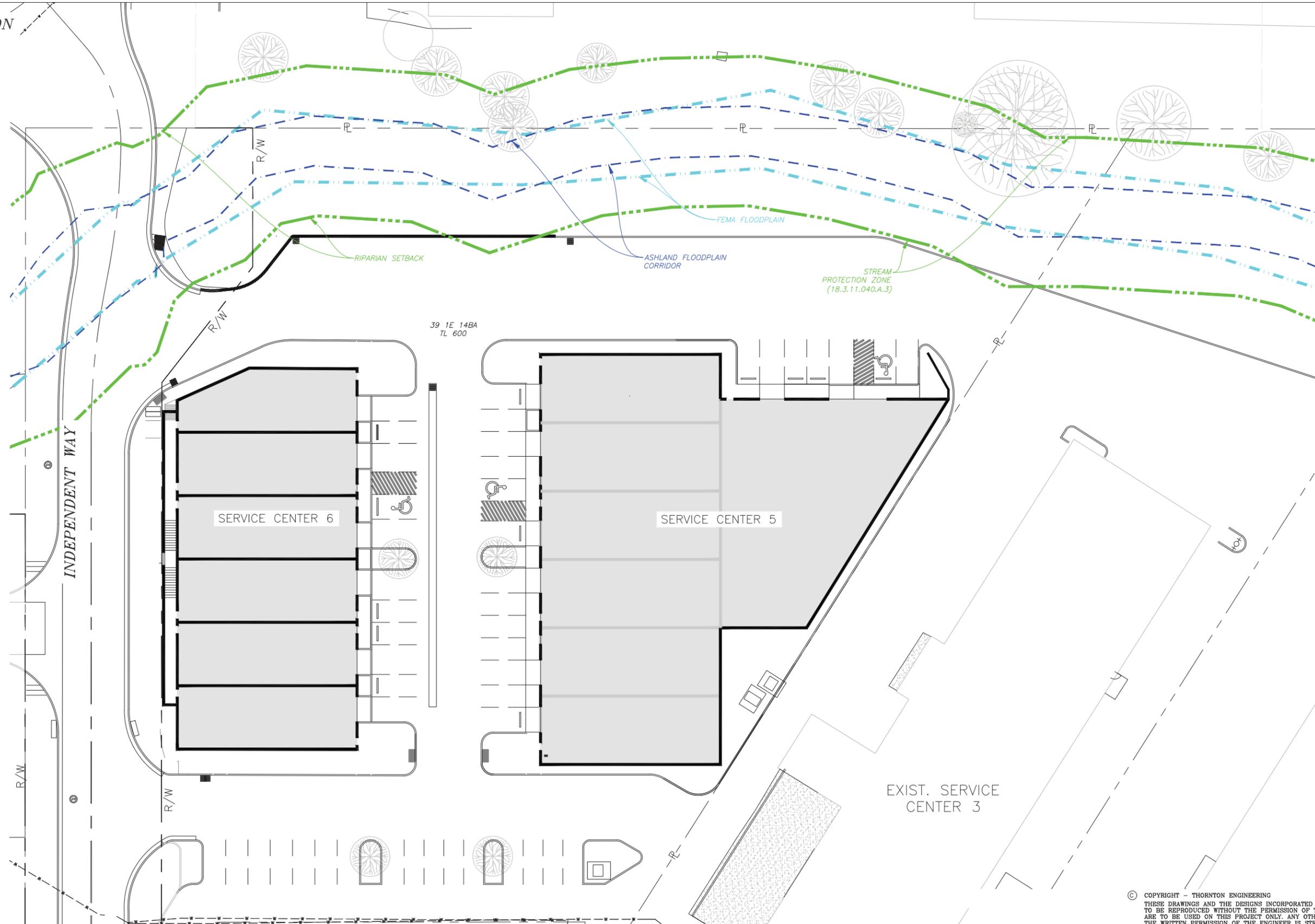
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jacksonville, oregon 97530
(541) 899-1489 (541) 899-3419 fax

DETAIL SHEET
SERVICE CENTERS 5 & 6
IPCO DEVELOPMENT CORPORATION
640 TOLMAN CREEK ROAD
ASHLAND, OREGON

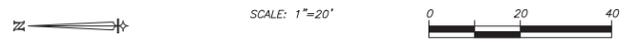
SHEET C6

JOB NO. 20-047
FILE: BASEMAP.DWG

WASHINGTON STREET



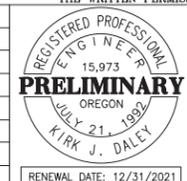
WRPZ AND FLOODPLAIN MAP - IPCO DEVELOPMENT INC. SERVICE CENTERS 5 & 6



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JOB NO. 20-047
 FILE: BASEMAP.DWG
 (SEE 14-043)

DRAWN:	MRS
DATE:	1/14/2021
REVISIONS:	



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 jacksonville, oregon 97530
 (541) 899-1489 (541) 899-3419 fax

WRPZ AND FLOODPLAIN MAP
SERVICE CENTERS 5 & 6
 IPCO DEVELOPEMENT CORPORATION
 640 TOLMAN CREEK ROAD
 ASHLAND, OREGON

SHEET MP1

EROSION & SEDIMENT CONTROL PLANS

IPCO DEVELOPMENT-SERVICE CENTER 5 & 6

NARRATIVE DESCRIPTIONS	
PROPERTY LOCATION/DESCRIPTION	Independent Way--Between Washington Street and Tolman Creek Road Adjacent to 39-1E-148A, tax lots 100, 500, 600 and 900 Ashland, Jackson County, Oregon
LATITUDE/LONGITUDE	42.1835N, 122.6701W
EXISTING SITE CONDITIONS	Undeveloped
DEVELOPED CONDITIONS	City Street
DISTURBED AREA	1.2 Acres
SITE SOIL CLASSIFICATION	23A (22%)-- CAMAS--NEWBERG--EVANS 0-3 PERCENT SLOPES 100A (78%)-- KUBLI LOAM 3 PERCENT SLOPES
ON-SITE SOILS HAVE A SLIGHT EROSION POTENTIAL. EXCESS SOILS GENERATED FROM EXCAVATION, GRADING AND TRENCHING ACTIVITIES FOR EACH PHASE MAY BE TEMPORARILY STORED ON THE SITE WITHIN UNDEVELOPED AREAS. EXCESS SOILS AT THE TIME OF THE FINAL PHASE OR AT ANY TIME PRIOR MAY BE REMOVED AND PLACED OFFSITE.	
RECEIVING WATER BODY	Hamilton Creek

SITE INSPECTION INFORMATION	
PERMITEE'S SITE INSPECTOR:	KEVIN GOLDEN
COMPANY/AGENCY:	CITY OF ASHLAND
PHONE:	541-552-2413
E-MAIL:	Kevin.golden@ashland.or.us
DESCRIPTION OF EXPERIENCE:	CESCL and ESCI certified since 2016. Main inspector on over 50 large scale developments.

INSPECTION SCHEDULE	
SITE CONDITION	MINIMUM FREQUENCY
ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOW MELT, IS OCCURRING. AT LEAST ONCE EVERY 14 CALENDAR DAYS, REGARDLESS OF WHETHER STORMWATER RUNOFF IS OCCURRING.
PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE BEFORE LEAVING THE SITE.
INACTIVE PERIODS GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS	ONCE EVERY MONTH.
PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.
PERIODS DURING WHICH DISCHARGE IS UNLIKELY DUE TO FROZEN CONDITIONS.	MONTHLY. RESUME MONITORING IMMEDIATELY UPON MELT, OR WHEN WEATHER CONDITIONS MAKE DISCHARGES LIKELY.

HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE EC INSPECTOR. ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200-C PERMIT REQUIREMENTS.

INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.

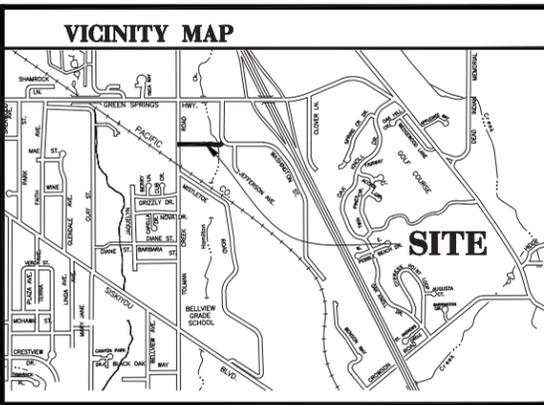
CHANGES TO THE APPROVED ESC PLAN MUST BE SUBMITTED TO RVS IN THE FORM OF AN ACTION PLAN.

RATIONALE STATEMENT

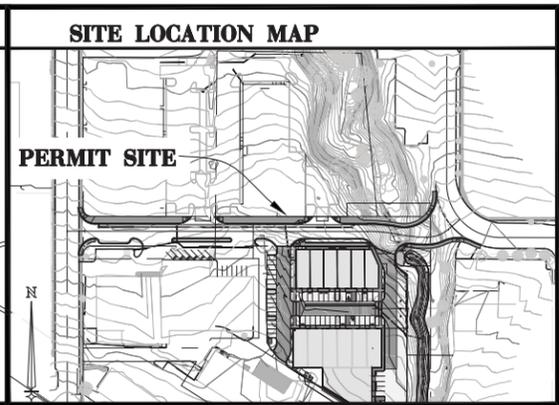
A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S GUIDANCE MANUAL HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMP'S WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOIL CONDITIONS TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS, AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESC PLAN, AN ACTION PLAN WILL BE SUBMITTED.

GENERAL NOTES	
1. Hold a pre-construction meeting of project construction personnel that includes the inspector to discuss erosion and sediment control measures and construction limits. (Schedule A.8.c.i.(3))	22. Implement the following BMPs when applicable: written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures, spill kits in all vehicles, regular maintenance schedule for vehicles and machinery, material delivery and storage controls, training and signage, and covered storage areas for waste and supplies. (Schedule A 7.e.iii.)
2. All inspections must be made in accordance with DEQ 1200-C permit requirements (Schedule A.12.b and Schedule B.1).	23. Use water, soil-binding agent or other dust control technique as needed to avoid wind-blown soil. (Schedule A 7.a.iv)
3. Inspection logs must be kept in accordance with DEQ's 1200-C permit requirements (Schedule B.1.e and B.2).	24. The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient releases to surface waters. Exercise caution when using time-release fertilizers within any waterway riparian zone. (Schedule A.9.b.iii)
4. Retain a copy of the ESCP and all revisions on site and make it available on request to DEQ, Agent, or the local municipality. During inactive periods of greater than seven (7) consecutive calendar days, the above records must be retained by the permit registrant, but do not need to be at the construction site (Schedule B.2.c.)	25. If an active treatment system (for example, electro-coagulation, flocculation, filtration, etc.) for sediment or other pollutant removal is employed, submit an operation and maintenance plan (including system schematic, location of system, location of inlet, location of discharge, discharge dispersion device design, and a sampling plan and frequency) before operating the treatment system. Obtain plan approval before operating the treatment system. Operate and maintain the treatment system according to manufacturer's specifications. (Schedule A.9.d)
5. All permit registrants must implement the ESCP. Failure to implement any of the control measures or practices described in the ESCP is a violation of the permit. (Schedule A 8.a)	26. Temporarily stabilize soils at the end of the shift before holidays and weekends, if needed. The registrant is responsible for ensuring that soils are stable during rain events at all times of the year. (Schedule A 7.b)
6. The ESCP must be accurate and reflect site conditions. (Schedule A.12.c.i)	27. As needed based on weather conditions, at the end of each workday soil stockpiles must be stabilized or covered, or the BMP's must be implemented to prevent discharges to surface waters or conveyance systems leading to surface waters. (Schedule A.7.e.ii (2))
7. Submission of all ESCP revisions is not required. Submittal of the ESCP revisions is only under specific conditions. Submit all necessary revision to DEQ or Agent within 10 days. (Schedule A.12.c.iv and v)	28. Construction activities must avoid or minimize excavation and bare ground activities during wet weather. (Schedule A.7.a.i)
8. Phase clearing and grading to the maximum extent practical to prevent exposed inactive areas from becoming a source of erosion. (Schedule A 7.a.iii)	29. Sediment fence: remove trapped sediment before it reaches one third of the above ground fence height and before fence removal. (Schedule A.9.c.i)
9. Identify, mark, and protect (by construction fencing or other means) critical riparian areas and vegetation including important trees and associated rooting zones, and vegetation areas to be preserved. Identify vegetative buffer zones between the site and sensitive areas (e.g., wetlands), and other areas to be preserved, especially in perimeter areas. (Schedule A.8.c.i.(1) & (2))	30. Other sediment barriers (such as biobags): remove sediment before it reaches two inches depth above ground height, and before BMP removal. (Schedule A.9.c.i)
10. Preserve existing vegetation when practical and re-vegetate open areas. Re-vegetate open areas when practicable before and after grading or construction. Identify the type of vegetative seed mix used. (Schedule A.7.a.v)	31. Catch basins: clean before retention capacity has been reduced by fifty percent. Sediment basins and sediment traps: remove trapped sediments before design capacity has been reduced by fifty percent and at completion of project. (Schedule A.9.c.iii & iv)
11. Maintain and delineate any existing natural buffer within the 50-feet of water of the state (Schedule A.7.b.i and (2)(a)(b)).	32. Within 24 hours, significant sediment that has left the construction site, must be remediated. Investigate the cause of the sediment release and implement steps to prevent a recurrence of the discharge within the same 24 hours. Any in-stream clean up of sediment shall be performed according to the Oregon Division of State Lands required timeframe. (Schedule A.9.b.i)
12. Install perimeter sediment control, including storm drain inlet protection as well as all sediment basins, traps, and barriers prior to land disturbance (Schedule A.8.c.i (5))	33. The intentional washing of sediment into storm sewers or drainage ways must not occur. Vacuuming or dry sweeping and material pickup must be used to cleanup released sediments. (Schedule A.9.b.ii)
13. Control both peak flow rates and total stormwater volume, to minimize erosion at outlets and downstream channels and streambanks. (Schedule A.7.c)	34. The entire site must be temporarily stabilized using vegetation or a heavy mulch layer, temporary seeding, or other method should all construction activities cease for 30 days or more. (Schedule A.7.f.i)
14. Control sediment as needed along the site perimeter and at all operational internal storm drain inlets at all times during construction, both internally and at the site boundary. (Schedule A.7.d.i)	35. Provide temporary stabilization for that portion of the site where construction activities cease for 14 days or more with a covering of blown straw and a tackifier, loose straw, or an adequate covering of compost mulch until work resumes on that portion of the site. (Schedule A.7.f.ii)
15. Establish concrete truck and other concrete equipment washout areas before beginning concrete work. (Schedule A.8.c.i.(6))	36. Do not remove temporary sediment control practices until permanent vegetation or other cover of exposed areas is established. Once construction is complete and the site is stabilized, all temporary erosion controls and retained soils must be removed and disposed of properly, unless doing so conflicts with local requirements. (Schedule A.8.c.iii(1) and D.3.c.ii and iii)
16. Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses. Temporary or permanent stabilization measures are not required for areas that are intended to be left unvegetated, such as dirt access roads or utility pole pads. (Schedule A.8.c.ii.(3))	
17. Establish material and waste storage areas, and other non-stormwater controls. (Schedule A.8.c.i.(7))	
18. Prevent tracking of sediment onto public or private roads using BMPs such as: construction entrance, graveled (or paved) exits and parking areas, gravel all unpaved roads located onsite, or use an exit tire wash. These BMPs must be in place prior to land-disturbing activities. (Schedule A 7.d.ii and A.8.c.i.(4))	
19. When trucking saturated soils from the site, either use water-tight trucks or drain loads on site. (Schedule A.7.d.ii.(5))	
20. Control prohibited discharges from leaving the construction site, i.e., concrete wash-out, wastewater from cleanout of stucco, paint and curing compounds (Schedule A.6)	
21. Use BMPs to prevent or minimize stormwater exposure to pollutants from spills; vehicle and equipment fueling, maintenance, and storage; other cleaning and maintenance activities; and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, fertilizer, pesticides and herbicides, paints, solvents, curing compounds and adhesives from construction operations. (Schedule A.7.e.i.(2))	

INSPECTION FREQUENCY CHART	
SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOWMELT, IS OCCURRING. AT LEAST ONCE EVERY TWO WEEKS, REGARDLESS OF WHETHER OR NOT RUNOFF IS OCCURRING.
2. PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
3. INACTIVE PERIODS GREATER THAN FOURTEEN (14) CALENDAR DAYS.	ONCE EVERY MONTH.
4. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER.	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.
5. PERIODS DURING WHICH DISCHARGE IS UNLIKELY DUE TO FROZEN CONDITIONS.	MONTHLY. RESUME MONITORING IMMEDIATELY UPON MELT, OR WHEN WEATHER CONDITIONS MAKE DISCHARGES LIKELY.



CONTACTS	
OWNER	ENGINEER
IPCO DEVELOPMENT CORP. 1425 GREEN MEADOWS WAY ASHLAND, OREGON 97520 541-621-4151 (Trina Sanford) 541-944-1946 (Zach Brombacher)	THORNTON ENGINEERING, INC. 260 N. 3RD STREET JACKSONVILLE, OR 97530 (541)-899-1489



SHEET INDEX	
EC1	COVER SHEET - EROSION AND SEDIMENT CONTROL PLAN
EC2	SITE PLAN - EROSION & SEDIMENT CONTROL PLAN
EC3	DETAILS - EROSION & SEDIMENT CONTROL PLAN

BMP MATRIX FOR CONSTRUCTION PHASES

REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'S.

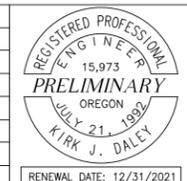
BMP's	YEAR:	2021												2022											
		MONTH #	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11	12				
BIOSWALES																									
CHECK DAMS																									
COMPOST BERM																									
COMPOST BLANKETS																									
CONCRETE TRUCK WASHOUT																									
CONSTRUCTION ENTRANCE																									
DEWATERING(TREATMENT LOCATION, SCHEMATIC, & SAMPLING PLAN REQUIRED)																									
DRAINAGE SWALES (ROADSIDE DITCHES)																									
EARTH DIKES (STABILIZED)																									
ENERGY DISSIPATORS																									
EROSION CONTROL BLANKETS & MATS(TYPE)																									
HYDROSEEDING																									
INLET PROTECTION																									
MULCHES (STRAW OR CELLULOSE WOOD)																									
MYCORRHIZAL BIOFERTILIZERS																									
NATURAL BUFFER ZONE																									
ORANGE FENCING/PROTECTING SENSITIVE/PRESERVED AREAS																									
OUTLET PROTECTION																									
PERMANENT SEEDING AND PLANTING																									
PIPE SLOPE DRAINS																									
PLASTIC SHEETING																									
PRESERVE EXISTING VEGETATION																									
SEDIMENT FENCING																									
SEDIMENT BARRIER																									
SEDIMENT TRAP																									
SODDING																									
SOIL TACKIFIERS																									
STORM DRAIN INLET PROTECTION																									
STRAW WATTLES(OR OTHER MATERIALS)																									
TEMPORARY DIVERSION DIKES																									
TEMPORARY OR PERMANENT SEDIMENTATION BASINS																									
TEMPORARY SEEDING AND PLANTING																									
TREATMENT SYSTEM(OR PLAN REQUIRED)																									
UNPAVED ROADS GRAVELED OR OTHER BMP ON ROAD																									
VEGETATIVE BUFFER STRIPS																									

JOB NO. 20-047
FILE: BASEMAP.DWG

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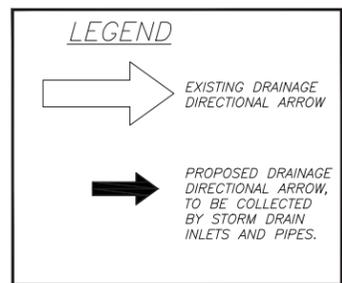
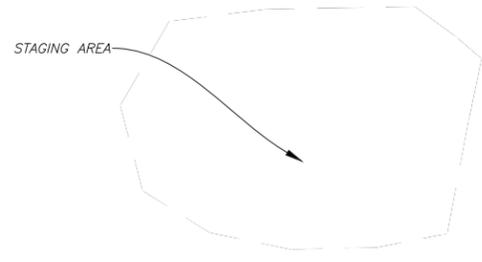


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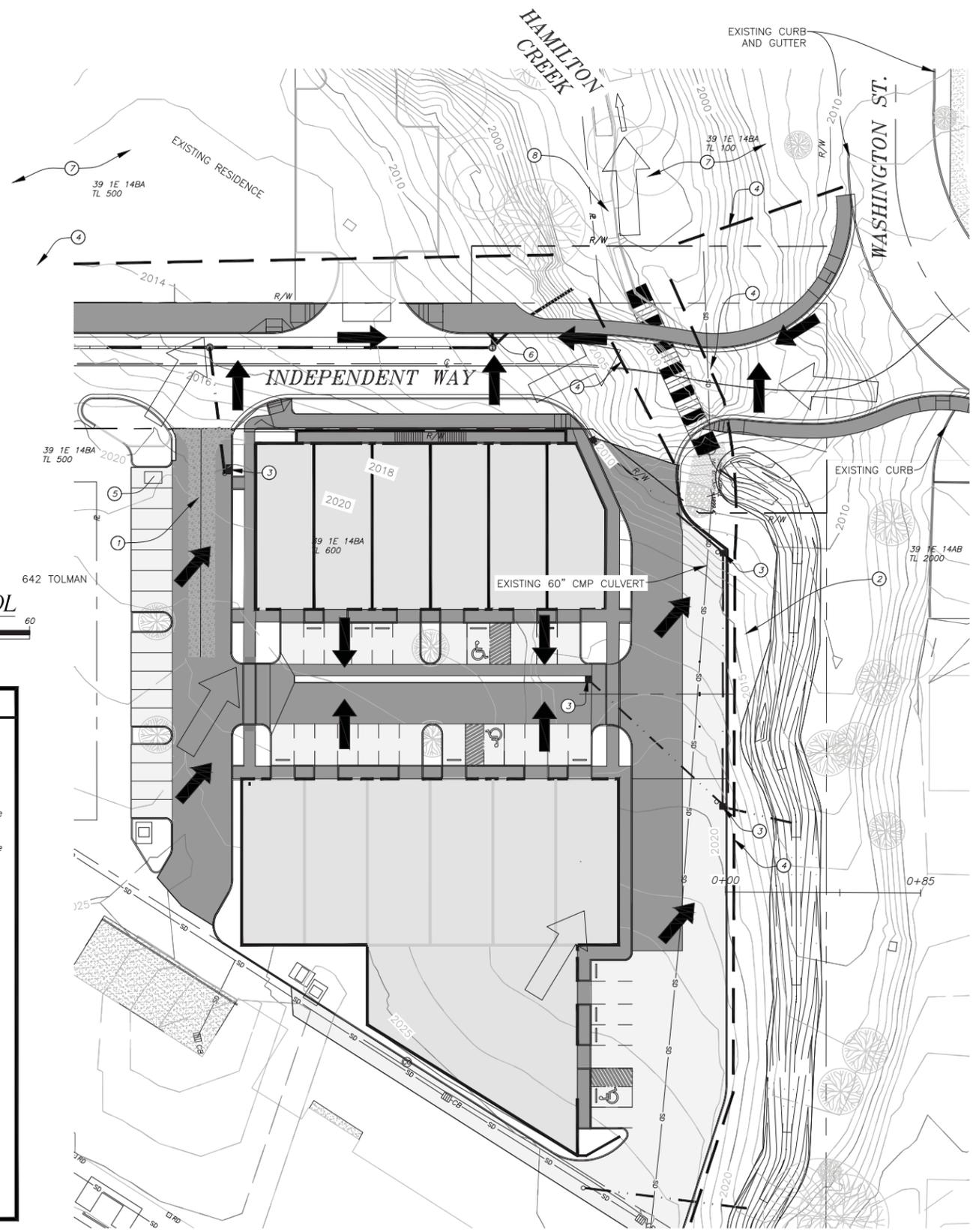
EROSION AND SEDIMENT CONTROL PLAN

COVER SHEET
IPCO DEVELOPEMENT CORPORATION
640 TOLMAN CREEK ROAD
ASHLAND, OREGON

SHEET
EC1



PLAN-EROSION AND SEDIMENT CONTROL
SCALE: 1"=30'



CONSTRUCTION NOTES	
①	Rock entrance for 100' min. To prevent tracking of soil onto roadway, 8" min. thickness of shale or 4"-1" on filter fabric. Line and grade shall conform to future driveway entrance, see RD1000
②	Install temporary orange construction fence to prevent disturbance of stream, riparian and other sensitive areas
③	Future storm drain inlets shall be protected by biofilter bags or equivalent, see RD1015
④	Place 12" high mulch berm Fiber rolls or "wattles" may also be used in lieu of mulch berm
⑤	Provide concrete washout area, see RD1070
⑥	Existing storm drain inlets shall be protected by biofilter bags or equivalent, see RD1015
⑦	Avoid disturbance to existing vegetation to the greatest extent practical until the time of landscaping installation & final stabilization.
⑧	Place biofilter bag check dam, type 3 as needed to control turbidity in creek, see RD1005.

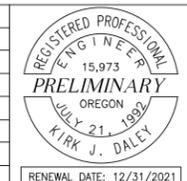
GENERAL NOTES	
1. Seed used for temporary or permanent erosion control seeding may be any seed mix that is generally used by local landscape professionals or may consist of the following: a. Dwarf grass mix (min. 100 lb./ac.) 1. Dwarf perennial ryegrass (80% by weight) 2. Creeping red fescue (20% by weight) b. Standard height grass mix (min. 100lb./ac.) 1. Annual ryegrass (40% by weight) 2. Turf-type fescue (60% by weight) Permanent seeding that is part of site landscaping shall conform to the specifications of the project landscape architect.	7. Areas subject to wind erosion shall use appropriate dust control measures including the application of a fine spray of water, plastic sheeting, straw mulching, or other approved measures.
2. Slope to receive temporary or permanent seeding shall have the surface roughened by means of track-walking or the use of other approved implements. Surface roughening improves seed bedding and reduces run-off velocity.	8. Construction entrances shall be installed at the beginning of construction and maintained for the duration of the project. Additional measures including, but not limited to, tire washes, street sweeping, and vacuuming may be required to insure that all paved areas are kept clean for the duration of the project.
3. Long term slope stabilization measures shall include the establishment of permanent vegetative cover via seeding with approved mix and application rate.	9. Active inlets to storm water systems shall be protected through the use of approved inlet protection measures. All inlet protection measures are to be regularly inspected and maintained as needed.
4. Temporary slope stabilization measures shall include: covering exposed soil with plastic sheeting, erosion matting, straw ground cover, wood chips, mulch, or other approved measures. Sheetting or matting shall be weighted or staked down to prevent movement due to wind or water. Straw or other materials (as applicable) applied to slopes greater than 3:1 shall be track-walked or held in place by a tackifier.	10. Saturated materials that are hauled off-site must be transported in water-tight trucks to eliminate spillage of sediment and sediment-laden water.
5. Stockpiled soil or strippings shall be placed in a stable location and configuration. During "wet weather" periods, stockpiles shall be covered with plastic sheeting or straw mulch. Sediment fence is required around the perimeter of the stockpile unless the stockpile is located in a flat (< 2% slope) or depressed area with about 20 feet of grassy or vegetated buffer zone on all sides.	11. An area shall be provided for the washing out of concrete trucks in a location that does not provide run-off that can enter the storm water system. If the concrete wash-out area can not be constructed greater than 50' from any discharge point, secondary measures such as berms or temporary settling pits may be required. The wash-out shall be located within six feet of truck access and be cleaned when it reaches 50% of the capacity.
6. Exposed cut or fill areas shall be stabilized through the use of temporary seeding and mulching, erosion control blankets or mats, mid-slope sediment fences or wattles, or other appropriate measures. Slopes exceeding 25% may require additional erosion control measures.	12. Sweepings from exposed aggregate concrete shall not be transferred to the storm water system. Sweepings shall be picked up and disposed in the trash.
	13. Avoid paving in wet weather when paving chemicals can run-off into the storm water system.
	14. Use bmps such as check-dams, berms, and inlet protection to prevent run-off from reaching discharge points.
	15. Cover catch basins, manholes, and other discharge points when applying seal coat, tack coat, etc. To prevent introducing these materials to the storm water system.

NOTE:
SEE BMP MATRIX ON SHEET EC1 FOR TIMING OF EROSION CONTROL MEASURES & ADDITIONAL EROSION CONTROL CONSIDERATIONS

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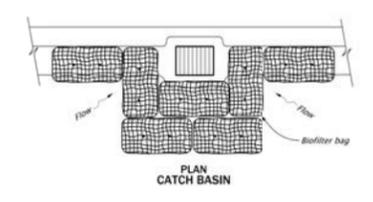
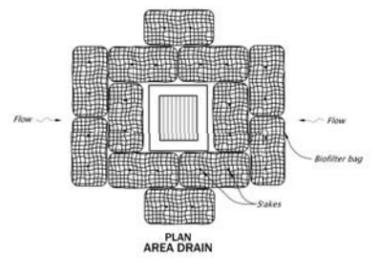
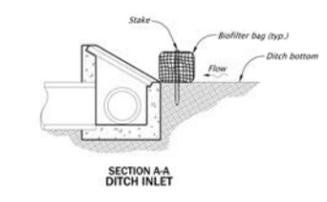
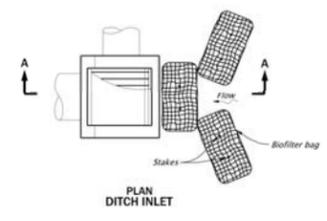
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EROSION AND SEDIMENT CONTROL PLAN
SITE PLAN
IPCO DEVELOPEMENT CORPORATION
640 TOLMAN CREEK ROAD
ASHLAND, OREGON

JOB NO. 20-047
FILE: BASEMAP.DWG

RD1015.dgn 07-10-2020

RD1015



BIOFILTER BAGS - TYPE 4
NOT TO SCALE

NOTES:
1. Stake biofilter bags with 2"x2"x36" wood stakes, and use a minimum 2 stakes per bag. Drive stakes a minimum of 6" into the ground and flush with the top of the bags.
2. Omit stakes when bags are placed on pavement surface.
3. Overlap all bag joints 6".

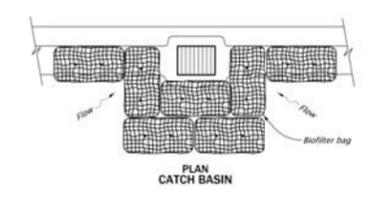
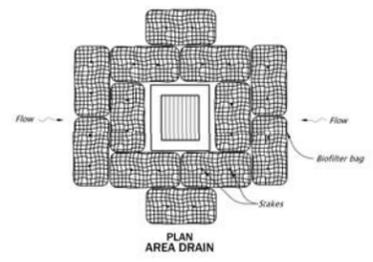
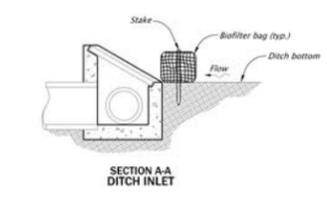
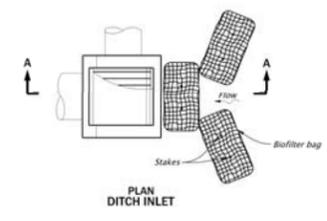
4. Biofilter bags used on active roadways are easily displaced and made ineffective if struck by vehicles. If struck by a cyclist, falls with injury could result. On active roadways alternative inlet protection should be considered.

CALC. BOOK NO.	6402, 6406, 6407	SID DATE	July, 2020
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.			
OREGON STANDARD DRAWINGS			
INLET PROTECTION TYPE 4			
DATE	2021	REVISION	DESCRIPTION

Effective Date: December 1, 2020 - May 31, 2021 RD1015

RD1015.dgn 07-10-2020

RD1015



BIOFILTER BAGS - TYPE 4
NOT TO SCALE

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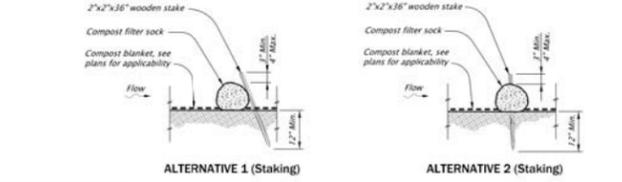
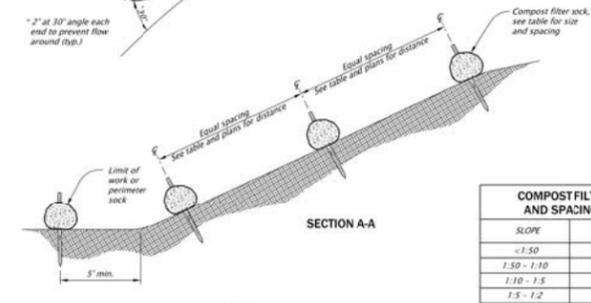
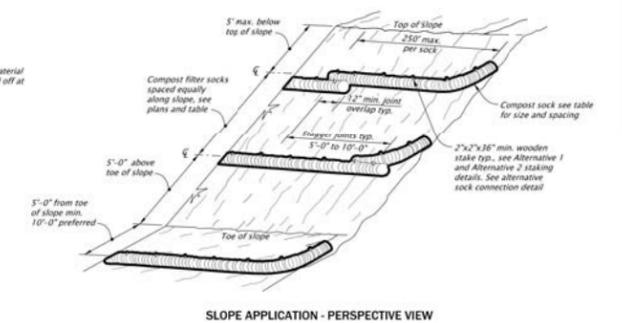
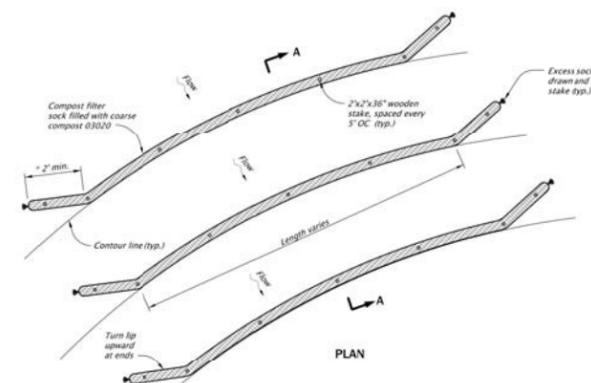
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CALC. BOOK NO.	6402, 6406, 6407	SID DATE	July, 2020
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.			
OREGON STANDARD DRAWINGS			
INLET PROTECTION TYPE 4			
DATE	2021	REVISION	DESCRIPTION

Effective Date: December 1, 2020 - May 31, 2021 RD1015

RD1032.dgn 07-10-2020

RD1032



SLOPE	SPACING (ft)	DIAMETER (in)
< 1:50	250	8
1:50 - 1:10	125	12
1:10 - 1:5	100	12
1:5 - 1:2	50	18
> 1:2	25	18

NOTE: Fully biodegradable compost sock mesh is recommended for permanent installations. Where compost socks must be moved or removed, synthetic sock mesh should be used.

COMPOST FILTER SOCK
NOT TO SCALE

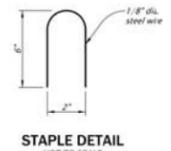
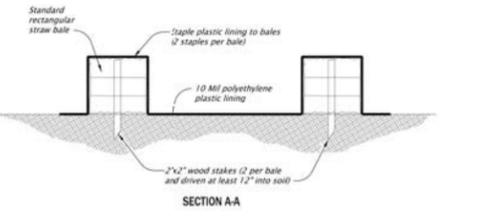
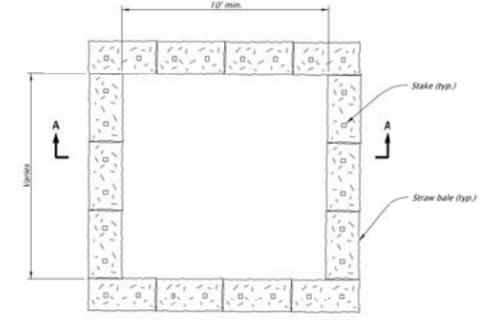
CALC. BOOK NO.	6403, 6404, 6405	SID DATE	July, 2020
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.			
OREGON STANDARD DRAWINGS			
SEDIMENT BARRIER TYPE 8			
DATE	2021	REVISION	DESCRIPTION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

Effective Date: December 1, 2020 - May 31, 2021 RD1032

RD1070.dgn 07-10-2020

RD1070



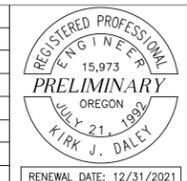
CALC. BOOK NO.	6403, 6404, 6405	SID DATE	July, 2020
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.			
OREGON STANDARD DRAWINGS			
CONCRETE TRUCK WASH OUT			
DATE	2021	REVISION	DESCRIPTION

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EROSION AND SEDIMENT CONTROL PLAN
DETAILS
IPCO DEVELOPEMENT CORPORATION
640 TOLMAN CREEK ROAD
ASHLAND, OREGON

CALL FOR UTILITY LOCATE 48 HOURS
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