

**ASHLAND PLANNING COMMISSION
REGULAR MEETING
July 14, 2020
AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center Council Chambers, 1175 E. Main Street
- II. **ANNOUNCEMENTS**
- III. **CONSENT AGENDA**
A. **Approval of Minutes**
1. June 9, 2020 Regular Meeting
2. June 23, 2020 Special Meeting
- IV. **PUBLIC FORUM**
- V. **UNFINISHED BUSINESS**
A. Approval of Findings for PA-T2-2020-00017, 210 Alicia Street.
B. Approval of Findings for PA-T2-2020-00019, Vacant Tax Lots #10104 & #10105 on First Street (North of Lithia Way, Across from the Post Office).
- VI. **TYPE II PUBLIC HEARINGS**
A. **PLANNING ACTION: #PA-T2-2020-00020**
SUBJECT PROPERTY: 705 Helman Street (Helman Elementary School)
OWNER/APPLICANT: HMK Company/Ashland School District #5
DESCRIPTION: A request for Site Design Review and Conditional Use Permit approvals to allow the construction of a new 23,755 square foot, single-story school building for the Helman Elementary School property at 705 Helman Street. The requested Conditional Use Permit is to allow the expansion of an existing non-conforming development pattern where both the existing and proposed new parking and circulation are located between the buildings and the street, and for the on-site relocation of previously approved signage. The proposal includes the demolition of two existing classroom buildings (the A & B quad's) and requests a Tree Removal Permit to remove 12 significant trees. COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-5; MAP: 39 1E 04 BD; TAX LOTS: 600, 2700, 2800 & 2900.
- VII. **ADJOURNMENT**

**CITY OF
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

**CITY OF
ASHLAND**
ASHLAND PLANNING COMMISSION
REGULAR MEETING
MINUTES - *Draft*
June 9, 2020

I. CALL TO ORDER:

Chair Haywood Norton called the meeting to order at 7:00 p.m.

Commissioners Present:

Michael Dawkins
Alan Harper
Kerry KenCairn
Haywood Norton
Roger Pearce
Lynn Thompson

Staff Present:

Bill Molnar, Community Development Director
Maria Harris, Planning Manager
Brandon Goldman, Senior Planner
Dana Smith, Executive Assistant

Absent Members:

Council Liaison:

Stef Seffinger, absent

II. ANNOUNCEMENTS

Community Development Director Bill Molnar announced the City Council passed the second reading of the ordinance amending plaza requirements in the downtown during their meeting June 2, 2020. The City Council would hear first reading on the open space amendments at their meeting June 16, 2020. Mr. Molnar was looking into the feasibility of allowing live public testimony during the electronic meetings. They were hoping to start that with the meetings in July.

III. PUBLIC FORUM - None

IV. CONSENT AGENDA

A. Approval of Minutes

1. May 12, 2020 Regular Meeting
2. May 26, 2020 Special Meeting

Commissioner Pearce/Thompson m/s approved the minutes of May 12, 2020. Voice Vote: all AYES. Motion passed.

Commissioner Pearce/Dawkins m/s approved the minutes of May 26, 2020. Voice Vote: all AYES. Motion passed.

V. PUBLIC FORUM - None

VI. TYPE II PUBLIC HEARINGS CONT'D

A. PLANNING ACTION: #PA-T2-2020-00017

SUBJECT PROPERTY: 210 Alicia Street

OWNER/APPLICANT: David Scott Construction, LLC

DESCRIPTION: A request for Outline Plan subdivision and Site Design Review approvals for a 12-unit, 13-lot Cottage Housing Development for the property located at 210 Alicia Street. The application also requests a Tree Removal Permit to remove two trees including one 36-inch diameter multi-trunked Willow tree proposed to be removed as a hazard, and a 20-inch Plum tree proposed to be removed to accommodate driveway installation. COMPREHENSIVE PLAN DESIGNATION: Single-Family Residential; ZONING: R-1-5; ASSESSOR'S MAP: 39 1E 04DB; TAX LOT #: 1700. (Please note: The record and public hearing is closed

on this matter. The Planning Commission's consideration of this item will be limited to their deliberation and decision. No further submittals (evidence or argument) will be accepted into the record.)

Chair Norton read the rules of the public hearing during an electronic meeting. The record and public hearing were closed for 210 Alicia. The Commission was limited to deliberation and decision only.

Ex Parte

The Commission had no ex parte contact on the matter since the meeting May 12, 2020.

Chair Norton explained a power outage at the RVTV building in Medford interrupted the meeting on May 12, 2020. The record and hearing were closed during that meeting prior to the outage. The Chair continued the closed public hearing to the meeting May 26, 2020. At that meeting, there was some confusion when the item was re-noticed that prompted the Commission to continue it to this meeting. The public hearing and record were still closed, and no new evidence could be submitted. During the period the power was out, the Commission had discussed sewer capacity, emergency egress and garbage pick-up.

Staff Report

Mr. Molnar provided a presentation (see attached):

- Proposal
- Outline Plan – Parking
- Vicinity Map
- Survey
- Outline Plan & Site Design Map
- Planning Commission Deliberations – May 12
 - Planning Action Record – Closed on May 12 – Three items discussed during deliberation after losing the RVTV feed:
 1. Sewer Capacity
 2. Fire Department Access – Ralph Sartain, Fire Marshall
 3. Garbage pick-up. There was nothing in the record from Ashland Recology on how they would accommodate garbage pick-up.

Mr. Molnar addressed the emails and comments received after the record closed at the meeting May 12, 2020. There had been some discussion on submitting the comments if they pertained to procedural issues. After reviewing them it became clear that aside from concerns regarding RVTV, they reiterated issues already in the record. Staff determined not to receive them into the closed record.

Deliberations & Decision

Commissioner Pearce noted the public comments concerns regarding parking overflow. It was not one of the land use criteria. The proposal was for a cottage housing development, and it met the parking requirements. For concerns on blocking solar access, it was addressed in the application and met the solar ordinance. Comment concerning the potential increase in traffic and crime was not land use criteria either.

Commissioner Pearce/Harper m/s to approve PA-T2-2020-00017 with the conditions in the staff report.

DISCUSSION: Commissioner KenCairn stated it met the criteria. Commissioner Pearce wanted to ensure the Findings included the application met the block length connectivity under **18.2.3.090.C.3** and waived the requirement to dedicate additional public right of way. The other item for the Findings was the minimum driveway separation of 24-feet required in **18.4.3.080.C.3**. The current driveway was an existing condition and did not need the 24-feet. It was a non-conforming lot. **Roll Call Vote: Commissioner KenCairn, Harper, Thompson, Pearce, Norton, and Dawkins. The motion passed unanimously.**

VII. TYPE II PUBLIC HEARINGS

A. PLANNING ACTION: #PA-T2-2020-00019

SUBJECT PROPERTY: Vacant Tax Lots #10104 & #10105 on First Street (North of Lithia Way, Across from the Post Office)

OWNER/APPLICANT: Randy Jones for First Place Partners, LLC

DESCRIPTION: A request to consolidate two vacant lots and construct a new 10,547 square foot, three-story mixed use building as the third and final phase of the First Place subdivision. The building's ground floor will be primarily commercial space, while the second floor will include three one-bedroom residential units and the third floor will have one two-bedroom residential unit. The proposal includes modifications to the common area landscaping and parking configuration to provide more efficient access from the accessible parking to the entrances, and two requests for Exceptions to the Site Development & Design Standards' "Downtown Design Standards" to allow for a staggered street setback and to allow vertical windows installed together in groups of three that are more horizontal than vertical. COMPREHENSIVE PLAN

DESIGNATION: Commercial; ZONING: C-1; ASSESSOR'S MAP: 39 1E 09BA; TAX LOTS: #10104 & #10105

Ex Parte Contact

The Commission declared no ex parte contact but drove by site frequently.

Staff Report

Senior Planner Brandon Goldman provided a presentation (see attached)

- Plaza North – Notice Map
- Plaza North site
- Plaza North – Proposal
- Renderings from 2014-15 on top and current design on the bottom
- First Place Subdivision Original Site Plan
- 2014-15 Place Central/East/North Approved Plan
- 2020 Plaza North Proposed Site Plan
- Plaza North – 1st Floor
- Plaza North – 2nd Floor
- Plaza North – 3rd Floor
- Plaza North – East Elevation (from First Street/USPS)
- Plaza North – Southeast View
- Plaza North - North Elevation (toward open space)
- Plaza North - Northwest View
- Plaza North - West Elevation (rear, from parking lot)
- Plaza North - South Elevation (from driveway)
- Plaza North – Southwest View
- Plaza North - Setback Exception
- AMC 18.4.2.060.C.1 & C.6 Recommended Treatments
- Plaza North – Window Exception
- Similar Window Exception Granted 2015 & 2019 for Plaza East
- AMC 18.4.2.060.C.1 & C.6 Recommended Treatments
- Wall Section & Materials
- First Place Subdivision Parking Allocations
- Plaza North: Landscape Planting & Tree Protection Plans

Questions of Staff

Commissioner Thompson asked if the parking adequacy would be elevated if the commercial space use was more intense than office or retail. Mr. Goldman explained the Building Division would review any change in occupancy. The Planning Division would review the site to ensure there was adequate parking and it was zoned appropriately.

Commissioner Thompson questioned the two on-street parking spaces. The project was within 200 feet of a C-1-D zone, so the on-street parking credits were not automatically counted. It was not a discretionary decision for the Commission. It was noted the Findings referred to 82 spaces instead of 84 spaces. Commissioner Thompson wanted the Findings to be clear that the Commission was not allocating the on-street parking spaces and only approving 82 spaces.

Commissioner Harper asked if there were any conditions in the old subdivision approval that would impact the current proposal. Mr. Goldman had gone through the previous approvals and for the most part, everything was completed. There was a note regarding the amount of parking and how it correlated to the number of bike spaces required. The Findings for this proposal remedied bicycle parking. It would also replace vegetation that had not survived.

Commissioner Harper wanted to know if there were any standards or conditions that needed approval for the lot consolidation request. Mr. Goldman responded there was not. This was a modification to the previous approval. Lots 3 and 4 were consolidated in 2019. This request would consolidate lots 4 and 5. The consolidation did not trigger any setback requirements.

Mr. Goldman confirmed two balconies did extend into the common area where the parking was in the rear. Commissioner Harper was concerned the balconies extended onto another property and didn't how the Commission could approve that.

At approximately 8:08 p.m. the Commission was notified that the RVTV livestream of the meeting was interrupted.

Chair Norton continued both public hearings to June 23, 2020, 7:00 p.m. Staff would re-notice both public hearings.

Applicant's Presentation (See attached)

Written Testimony - None

Rebuttal by Applicant - None

Deliberations & Decision

VIII. TYPE III PUBLIC HEARINGS CONT'D

A. PLANNING ACTION: #PA-T3-2019-00001

SUBJECT PROPERTY: 1511 Hwy 99 N

**OWNER/AGENTS/APPLICANT: Linda Zare/Casita Developments, LLC & Kendrick Enterprise, LLC/
Rogue Planning & Development Services, LLC**

DESCRIPTION: A request for Annexation of a 16.87-acre parcel and Zone Change from County RR-5 Rural Residential) to City R-2 (Low Density, Multi-Family Residential) for the properties located at 1511 Highway 99 North. The annexation is to include adjacent railroad property and state highway right-of-way. The application includes conceptual details for the future phased development of 196 apartments (1- and 2-Bedrooms, ranging from 480-701 square feet) in 14 two-story buildings; Outline Plan subdivision and Site Design Review development approvals are not requested here, and would be applied for subsequent to annexation. The application also requests an Exception to Street Standards to deviate from city standard parkrow and sidewalk improvements to respond to constraints of right-of-way width and existing encroachments. COMPREHENSIVE PLAN DESIGNATION: Multi-Family Residential; ZONING: Existing – County RR-5, Proposed – City R-2; ASSESSOR'S MAP: 38 1E 32; TAX LOT#'s: 1700 & 1702.

Item continued to June 23, 2020 at 7:00 p.m.

IX. ADJOURNMENT

Meeting adjourned 8:18 p.m.

*Submitted by,
Dana Smith, Executive Assistant*

**CITY OF
ASHLAND**
ASHLAND PLANNING COMMISSION
SPECIAL MEETING
MINUTES - *Draft*
June 23, 2020

I. CALL TO ORDER:

Chair Haywood Norton called the meeting to order at 7:06 p.m.

Commissioners Present:

Michael Dawkins
Alan Harper
Haywood Norton
Roger Pearce
Lynn Thompson

Staff Present:

Bill Molnar, Community Development Director
Brandon Goldman, Senior Planner
Derek Severson, Senior Planner
Dana Smith, Executive Assistant

Absent Members:

Kerry KenCairn

Council Liaison:

Stef Seffinger, absent

II. ANNOUNCEMENTS

Community Development Director Bill Molnar announced that Interim Public Works Director Scott Fleury would form a committee to evaluate updates to the water and storm drain system charges. Commissioner Pearce volunteered to serve on the committee. The first reading of the Open Space ordinance amendments was postponed due to concerns expressed by KDA Homes. Staff would review the concerns and bring the item back to the Commission at the Study Session in July.

III. PUBLIC FORUM - None

IV. TYPE II PUBLIC HEARINGS CONT'D

A. PLANNING ACTION: #PA-T2-2020-00019

SUBJECT PROPERTY: Vacant Tax Lots #10104 & #10105 on First Street (North of Lithia Way, Across from the Post Office)

OWNER/APPLICANT: Randy Jones for First Place Partners, LLC

DESCRIPTION: A request to consolidate two vacant lots and construct a new 10,547 square foot, three-story mixed use building as the third and final phase of the First Place subdivision. The building's ground floor will be primarily commercial space, while the second floor will include three one-bedroom residential units and the third floor will have one two-bedroom residential unit. The proposal includes modifications to the common area landscaping and parking configuration to provide more efficient access from the accessible parking to the entrances, and two requests for Exceptions to the Site Development & Design Standards' "Downtown Design Standards" to allow for a staggered street setback and to allow vertical windows installed together in groups of three that are more horizontal than vertical. **COMPREHENSIVE PLAN**

DESIGNATION: Commercial; **ZONING:** C-1; **ASSESSOR'S MAP:** 39 1E 09BA; **TAX LOTS:** #10104 & #10105

Chair Norton read the rules of the electronic public hearing. This item was on the agenda for the meeting on June 9, 2020. An issue occurred with RVTV and the livestream was interrupted. The public hearing was continued to this meeting.

Ex Parte Contact

Commissioners Dawkins, Harper, and Pearce had no ex parte on the matter but drove past the site frequently. Commissioner Thompson declared no ex parte but had accessed the north parking lot to observe traffic during peak times.

Chair Norton had no ex parte and no site visit since June 9, 2020.

Questions of Staff

Senior Planner Brandon Goldman gave a report on the applicant's response to questions asked at the meeting on June 9, 2020. Jerome White, the architect submitted additional testimony that addressed concerns the Commission raised regarding a deck encroaching into a common area and the request for two on-street parking spaces within 200-feet of a C-I-D zone.

The applicant consulted their attorney regarding the deck. The Covenants, Conditions and Restrictions could create an allowance for the easements to run with the land. It would give the applicant the ability to build an encroachment into the common area and maintain it over time.

The two on-street parking spaces were on First Street and outside the 200-foot buffer. The applicants were requesting two on-street parking spaces for potential change of use in the future. The parking spaces were included in the original subdivision from 2012. If this application was approved, there would be no further development on the site.

Applicant's Submittal (See attached)

Written Testimony - None

Rebuttal by Applicant - None

Deliberations & Decision

Commissioner Thompson/Pearce m/s to approve PA-T2-2020-00019, with the exception of the two on-street parking places. DISCUSSION: Commissioner Thompson noted aspects she liked about the project. The code allowed on-street parking credits to reduce the number of required off-street parking. In this case the applicant did not need an on-street parking credit because they had enough parking off street. Commissioner Pearce agreed with Commissioner Thompson adding it did not prohibit them from requesting then if the use changed. He thought the findings should link the exceptions to the purpose of the site design review in **18.5.2.010. Roll Call Vote: Commissioner Harper, Pearce, Dawkins, Thompson and Norton, YES. Motion passed.**

V. TYPE III PUBLIC HEARINGS CONT'D

A. PLANNING ACTION: #PA-T3-2019-00001

SUBJECT PROPERTY: 1511 Hwy 99 N

**OWNER/AGENTS/APPLICANT: Linda Zare/Casita Developments, LLC & Kendrick Enterprise, LLC/
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DESCRIPTION: A request for Annexation of a 16.87-acre parcel and Zone Change from County RR-5 Rural Residential) to City R-2 (Low Density, Multi-Family Residential) for the properties located at 1511 Highway 99 North. The annexation is to include adjacent railroad property and state highway right-of-way. The application includes conceptual details for the future phased development of 196 apartments (1- and 2-Bedrooms, ranging from 480-701 square feet) in 14 two-story buildings; Outline Plan subdivision and Site Design Review development approvals are not requested here, and would be applied for subsequent to annexation. The application also requests an Exception to Street Standards to deviate from city standard parkrow and sidewalk improvements to respond to constraints of right-of-way width and existing encroachments. COMPREHENSIVE PLAN DESIGNATION: Multi-Family Residential; ZONING: Existing – County RR-5, Proposed – City R-2; ASSESSOR'S MAP: 38 1E 32; TAX LOT#'s: 1700 & 1702.

Staff Report / Questions of Staff

Senior Planner Derek Severson provided a presentation (see attached):

- Grand Terrace Annexation
- Contiguity & the Railroad Property
- Grand Terrace Annexation – AMC 18.5.8.060 Boundaries
- Oregon Revised Statutes (ORS) 222.170 “Triple Majority”
- Oregon Revised Statutes (ORS) 222.170 2017 ORS 222.170
- Oregon Revised Statutes (ORS) 222.170; Subject Properties; ODOT Right of Way

The Commission could find the proposal to annex the applicant’s properties, the adjacent ODOT right of way and the railroad to achieve contiguity and satisfy the **ORS 222.170 Triple Majority** option. It would not need consent from the Railroad.

Commissioner Thompson explained this was the first she was hearing of this option. It would be difficult to reach a determination. Commissioner Dawkins agreed and wanted to know how ODOT and the Railroad would determine a valuation of their properties. Commissioner Pearce explained there was no assessed value on either property. It was the assessed value of the private properties. It was half the owners who owned more than half the land in the annexation area that was worth more than half the assessed value. He clarified it would be contiguous by including the additional property in the annexation.

Commissioner Thompson confirmed the Triple Majority option would annex the Railroad without their consent. She thought the code section was about avoiding an election to annex property without people’s consent. Chair Pearce clarified it was one of three statutes that talked about annexation by consent before a public hearing. An order or election was not required if there was a Triple Majority.

Mr. Molnar further clarified this was an update on key issues and the contiguity was evolving. ORS 222.170 was preliminarily discussed but needed more information. A draft report would come before the Commission for consideration prior to it going to the City Council.

Commission Harper asked if the owners of the properties circled by the annexation wanted to be included. Staff explained the neighbors were noticed and some had sent written comments. Staff would reach out and let them know the proposed boundaries. Key considerations were services they currently had and what the obligation would be if they came into the city.

Staff clarified the ODOT right of way would remain a state highway that was in city limits until it met areas where the city had jurisdictional limits. It would be subject to ODOT standards. The City would have no control over signals or crosswalks.

- Transportation and Circulation – Access
- Frontage Improvements (North)
- Frontage Improvements (South)
- Transit Improvements

Commissioner Thompson asked about a two-way bike lane to avoid left turns across traffic. Staff noted the Transportation Commission had raised the same question. Interim Public Works Director Scott Fleury thought they could have a two-way bike lane within the existing right of way pending ODOT approval.

- Affordability Calculations & Constrained Lands

The applicant proposed to reduce the affordability requirement by removing the unbuildable areas and wetland buffers and base their density calculations on 13.7.5 acres of the site. It would result in 7-10 fewer affordable units. In the Code, the minimum residential density for annexation called for 90% of the base density of the zone to be provided with annexation unless reductions in the total number of units were necessary to that minimum density to accommodate significant natural features, topography, access limitations or physical constraints. The section requiring affordable housing stated a percentage of the base density did not provide for any reduction to accommodate significant natural

features or physical constraints. The applicant was contending the number of required affordable units should be reduced based on the total land area after the unbuildable lands were removed from the calculation. Presently, the code did not provide for any such reduction. Staff was currently proposing code revisions that would remove unbuildable lands from consideration in the calculations. The Commission could choose to add in their recommendation to the City Council the applicant made a reasonable argument in support of this approach; that staff had drafted code language reflecting the change and the legislative nature of an annexation would allow the City Council to take that approach.

The Commission did not support revising the ordinance for this specific project. They were open to potential changes in the future. The Commission needed to approve the application based on the current code. The applicant could ask the City Council for a legislative fix during that public hearing.

- Next Steps
 - Commissioners to weigh-in on issues identified and indicate any additional information they believe is needed from the applicant or staff to arrive at a formal recommendation to Council.
 - Motion to continue the action to a date certain – July 14, 2020 or later.
 - Based on discussion tonight, staff will prepare a formal recommendation with draft findings for consideration and adoption at the continued meeting.

The Commission wanted more detailed analysis on contiguity in the recommendation to the City Council. The Commission also agreed the number of affordable housing units did not meet the requirement.

Transportation concerns included traffic volume and speed, entering and exiting the site and bicycle safety. Both left turns seemed problematic. Some were not persuaded the north entrance should be limited to emergency traffic only. Other comments supported a two-way bike lane but thought it should be separated. The Commission could recommend the City talk to ODOT about reducing the speed.

If the annexation was approved, the number of units could be increased or reduced. It would depend on the conditions the City Council put on the annexation. The City Council could adopt the annexation by ordinance and conduct a public hearing and vote on whether to annex or not. The application would be conditioned upon first going through the site design review process before approving the annexation.

Commissioner Pearce commented the issues the applicant and property owners were having regarding the easement were outside of the Commission's purview. He also wanted to know if the applicant had abandoned their request for street design standards. The code applied to new developments and not annexations. He suggested including that it was irrelevant in the recommendation to the City Council. Mr. Molnar clarified the exception would be determined during site review. Annexation criteria were very specific to pedestrian improvements along the frontage. It was something the applicant would have to do. Commissioner Pearce added they did not meet the city's standards because it was in ODOT's right of way.

The Commission continued the meeting to July 28, 2020 at 7:00 p.m. The item would not be re-noticed. Anyone who had submitted comments would be notified.

Commissioner Dawkins/Pearce m/s to continue the public hearing to July 28, 2020 at 7:00 p.m. Voice Vote: All AYES. Motion passed unanimously.

VI. ADJOURNMENT

Meeting adjourned 8:43 p.m.

*Submitted by,
Dana Smith, Executive Assistant*

Meeting Minutes Attachments

Meeting Presentations, exhibits and documents submitted during the meetings can be accessed online on the Agendas and Minutes page.

Hard copies are available upon request.

FINDINGS

PA-T2-2020-00017
210 Alicia Street

BEFORE THE PLANNING COMMISSION
July 14, 2020

IN THE MATTER OF PLANNING ACTION #PA-T2-2020-00017, A REQUEST FOR)
OUTLINE PLAN SUBDIVISION AND SITE DESIGN REVIEW APPROVALS FOR A)
12-UNIT/13-LOT COTTAGE HOUSING DEVELOPMENT LOCATED AT 210 ALICIA)
AVENUE. THE APPLICANTION ALSO REQUESTS A TREE REMOVAL PERMIT)
TO REMOVE TWO TREES INCLUDING ONE 36-INCH DIAMETER MULTI-)
TRUNKED WILLOW TREE PROPOSED TO BE REMOVED AS A HAZARD, AND A) FINDINGS,
20-INCH PLUM TREE PROPOSED TO BE REMOVED TO ACCOMMODATE DRIVE-) CONCLUSIONS &
WAY INSTALLATION.) ORDERS

OWNER/APPLICANT: DAVID SCOTT CONSTRUCTION, LLC/)
ROGUE PLANNING)
& DEVELOPMENT SERVICES, LLC)
)

RECITALS:

- 1) Tax lot #1700 of Map 39 1E 04DB is located at 210 Alicia Avenue and is zoned Single Family Residential (R-1-5).
- 2) The applicant is requesting Outline Plan subdivision and Site Design Review approvals for a 12-unit, 13-lot Cottage Housing Development for the property located at 210 Alicia Street. The application also requests a Tree Removal Permit to remove two trees including one 36-inch diameter multi-trunked Willow tree proposed to be removed as a hazard, and a 20-inch Plum tree proposed to be removed to accommodate driveway installation. The proposal is outlined in plans on file at the Department of Community Development.
- 3) The criteria for Outline Plan approval are described in **AMC 18.3.9.040.A.3** as follows:
 - a. *The development meets all applicable ordinance requirements of the City.*
 - b. *Adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection, and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.*
 - c. *The existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.*
 - d. *The development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.*
 - e. *There are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.*
 - f. *The proposed density meets the base and bonus density standards established under this chapter.*

- g. *The development complies with the Street Standards.*
- 4) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:
- A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
 - B. **Overlay Zones:** *The proposal complies with applicable overlay zone requirements (part 18.3).*
 - C. **Site Development and Design Standards:** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
 - D. **City Facilities:** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*
 - E. **Exception to the Site Development and Design Standards.** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.*
 - 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*
 - 2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*
- 5) The development standards for Cottage Housing Development are detailed in **AMC 18.2.3.090** as follows:
- C. **Development Standards.** *Cottage housing developments shall meet all of the following requirements.*
 - 1. **Cottage Housing Density.** *The permitted number of units and minimum lot areas shall be as follows:*

| Table 18.2.3.090.C.1 Cottage Housing Development Density | | | | | |
|---|---|---|---|---|---------------------------------------|
| Zones | Maximum Cottage Density | Minimum number of cottages per cottage housing development | Maximum number of cottages per cottage housing development | Minimum lot size (accommodates minimum number of cottages) | Maximum Floor Area Ratio (FAR) |
| R-1-5, NN-1-5 NM-R-1-5 | 1 cottage dwelling unit per 2,500 square feet of lot area | 3 | 12 | 7,500 sq.ft. | 0.35 |
| R-1-7.5 NM-R-1-7.5 | 1 cottage dwelling unit per 3,750 square feet of lot area | 3 | 12 | 11,250 sq.ft. | 0.35 |

2. Building and Site Design.

- a. **Maximum Floor Area Ratio:** The combined gross floor area of all cottages and garages shall not exceed a 0.35 floor area ratio (FAR). Structures such as parking carports, green houses, and common accessory structures are exempt from the maximum floor area calculation.
- b. **Maximum Floor Area.** The maximum gross habitable floor area for 75 percent or more of the cottages, within developments of four units or greater, shall be 800 square feet or less per unit. At least two of the cottages within three unit cottage housing developments shall have a gross habitable floor area of 800 square feet or less. The gross habitable floor area for any individual cottage unit shall not exceed 1000 square feet.
- c. **Height.** Building height of all structures shall not exceed 18 feet. The ridge of a pitched roof may extend up to 25 feet above grade.
- d. **Lot Coverage.** Lot coverage shall meet the requirements of the underlying zone outlined in Table 18.2.5.030.A.
- e. **Building Separation.** A cottage development may include two-unit attached, as well as detached, cottages. With the exception of attached units, a minimum separation of six feet measured from the nearest point of the exterior walls is required between cottage housing units. Accessory buildings (e.g., carport, garage, shed, multipurpose room) shall comply with building code requirements for separation from non-residential structures.

f. **Fences.** Notwithstanding the provisions of section 18.4.4.060, fence height is limited to four feet on interior areas adjacent to open space except as allowed for deer fencing in subsection 18.4.4.060.B.6. Fences in the front and side yards abutting a public street, and on the perimeter of the development shall meet the fence standards of section 18.4.4.060.

3. **Access, Circulation, and Off-Street Parking Requirements. Notwithstanding the provisions of chapter 18.3.9 Performance Standards Option and 18.4 Site Development and Site Design Standards, cottage housing developments are subject to the following requirements:**

a. **Public Street Dedications.** Except for those street connections identified on the Street Dedication Map, the Commission may reduce or waive the requirement to dedicate and construct a public street as required in 18.4.6.040 upon finding that the cottage housing development meets connectivity and block length standards by providing public access for pedestrians and bicyclists with an alley, shared street, or multi-use path connecting the public street to adjoining properties.

b. **Driveways and parking areas.** Driveway and parking areas shall meet the vehicle area design standards of section 18.4.3.

i. Parking shall meet the minimum parking ratios per 18.4.3.040.

ii. Parking shall be consolidated to minimize the number of parking areas, and shall be located on the cottage housing development property.

iii. Off-street parking can be located within an accessory structure such as a multi-auto carport or garage, but such multi-auto structures shall not be attached to individual cottages. Single-car garages and carports may be attached to individual cottages. Uncovered parking is also permitted provided that off street parking is screened in accordance with the applicable landscape and screening standards of chapter 18.4.4.

4. **Open Space. Open space shall meet all of the following standards.**

a. A minimum of 20 percent of the total lot area is required as open space.

b. Open space(s) shall have no dimension that is less than 20 feet unless otherwise granted an exception by the hearing authority. Connections between separated open spaces, not meeting this dimensional requirement, shall not contribute toward meeting the minimum open space area.

c. Shall consist of a central space, or series of interconnected spaces.

d. Physically constrained areas such as wetlands or steep slopes cannot be counted towards the open space requirement.

- e. *At least 50 percent of the cottage units shall abut an open space.*
- f. *The open space shall be distinguished from the private outdoor areas with a walkway, fencing, landscaping, berm, or similar method to provide a visual boundary around the perimeter of the common area.*



- g. *Parking areas and driveways do not qualify as open space.*

Figure 18.2.3.090 Cottage Housing Conceptual Site Plans

- 5. **Private Outdoor Area.** *Each residential unit in a cottage housing development shall have a private outdoor area. Private outdoor areas shall be separate from the open space to create a sense of separate ownership.*
 - a. *Each cottage unit shall be provided with a minimum of 200 square feet of usable private outdoor area. Private outdoor areas may include gardening areas, patios, or porches.*
 - b. *No dimension of the private outdoor area shall be less than 8 feet.*
- 6. **Common Buildings, Existing Nonconforming Structures and Accessory Residential Units.**
 - a. **Common Buildings.** *Up to 25 percent of the required common open space, but no greater than 1,500 square feet, may be utilized as a community building for the sole use of the cottage housing residents. Common buildings shall not be attached to cottages.*
 - b. **Carports and garage structures.** *Consolidated carports or garage structures, provided per 18.2.3.090.C.3.b, are not subject to the area limitations for common buildings.*
 - c. **Nonconforming Dwelling Units.** *An existing single-family residential structure*

built prior to the effective date of this ordinance (date), which may be nonconforming with respect to the standards of this chapter, shall be permitted to remain. Existing nonconforming dwelling units shall be included in the maximum permitted cottage density. 1,000 square feet of the habitable floor area of such nonconforming dwellings shall be included in the maximum floor area permitted per 18.2.3.090C.2.a. Existing garages, other existing non-habitable floor area, and the nonconforming dwelling's habitable floor area in excess of 1,000 square feet shall not be included in the maximum floor area ratio.

- d. **Accessory Residential Units.** New accessory residential units (ARUs) are not permitted in cottage housing developments, except that an existing ARU that is accessory to an existing nonconforming single-family structure may be counted as a cottage unit if the property is developed subject to the provisions of this chapter.

7. Storm Water and Low-Impact Development.

- a. Developments shall include open space and landscaped features as a component of the project's storm water low impact development techniques including natural filtration and on-site infiltration of storm water.
- b. Low impact development techniques for storm water management shall be used wherever possible. Such techniques may include the use of porous solid surfaces in parking areas and walkways, directing roof drains and parking lot runoff to landscape beds, green or living roofs, and rain barrels.
- c. Cottages shall be located to maximize the infiltration of storm water run-off. In this zone, cottages shall be grouped and parking areas shall be located to preserve as much contiguous, permanently undeveloped open space and native vegetation as reasonably possible when considering all standards in this chapter.

8. Restrictions.

- a. The size of a cottage dwelling may not be increased beyond the maximum floor area in subsection 18.2.3.090.C.2.a. A deed restriction shall be placed on the property notifying future property owners of the size restriction.

6) The criteria for a Tree Removal Permit are described in **AMC 18.5.7.040.B** as follows:

- 1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable

danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.

- b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

2. Tree That is Not a Hazard. *A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*

- a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.*
- b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*
- c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
- d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
- e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

7) On April 15, 2020 Governor Kate Brown issued Executive Order #20-16 “*Keep Government Working: Ordering Necessary Measures to Ensure Safe Public Meetings and Continued Operations by Local Government During Coronavirus (COVID-19) Outbreak.*” The Governor’s Order required that public bodies hold public meetings by telephone, video, or through some other electronic or virtual means, whenever possible; that the public body make available a method by which the public can listen to or virtually attend the public meeting or hearing at the time it occurs; that the public body does not have to provide a physical space for the public to attend the meeting or hearing; that requirements that oral public testimony be taken during hearings be suspended, and that public bodies instead provide a means for submitting written testimony by e-mail or other electronic methods that the public body can consider in a timely manner

8) The Planning Commission, following proper public notice, held an electronic public hearing on May 12, 2020. In keeping with Executive Order #20-16, this meeting was broadcast live on local television channel 9 and on Charter Communications channels 180 & 181, and was live-streamed over the internet on RVTV Prime at <http://www.rvtv.sou.edu>. A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report were made available on-line at <http://www.ashland.or.us/Page.asp?NavID=17902> seven days prior to the hearing. The applicant was required to submit any presentation materials for consideration at the hearing by 3:30 p.m. on Friday, May 8th, and these materials were made available on-line and e-mailed to Commissioners. Those wishing to provide testimony were invited to submit written comments via e-mail to PC-public-testimony@ashland.or.us with the subject line "May 12 PC Hearing Testimony" by 3:30 p.m. on Monday, May 11, 2020, and these comments were made available on-line and e-mailed to Commissioners. The applicant was invited to provide written rebuttal to these public comments by 3:30 p.m. on Tuesday, May 12th and these arguments were posted on-line and e-mailed to Commissioners in advance of the electronic public hearing. All written testimony received by the deadlines was made available for Commissioners to review before the hearing and has been included in the meeting minutes. As provided in the Governor's Executive Order #20-16, no oral public testimony was taken during the hearing.

During the initial hearing on May 12, 2020 there were technical difficulties with broadcasting the meeting due to a power outage, with the broadcast failing after the hearing and record were closed but before Planning Commission deliberations had concluded. As such, the meeting was continued for the Planning Commission deliberations and decision until 7:00 p.m. on Tuesday, June 9, 2020 at which time the meeting was reconvened electronically and the Planning Commission, after consideration of the materials received, approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the staff report, written public testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Outline Plan approval, Site Design Review approval, Cottage Housing, and Tree Removal Permit meets all applicable criteria for Outline Plan approval described in AMC 18.3.9.040.A.3; for Site Design Review described in AMC 18.5.2.050; for a Cottage Housing Development described in AMC 18.2.3.090; and for a Tree Removal Permit as described in AMC 18.5.7.040.B.

2.3 The Planning Commission concludes that the proposal satisfies all applicable criteria for Outline Plan approval.

The first approval criterion for Outline Plan approval is that, *“The development meets all applicable ordinance requirements of the City.”* The Commission finds that the proposal meets all applicable ordinance requirements, is requesting no Variances or Exceptions, and that this criterion has been satisfied.

The second approval criterion for Outline Plan approval is that, *“Adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection, and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.”*

The Planning Commission notes that the application materials assert that adequate key City facilities can be provided to serve the development, and that based on consultations with representatives of the various City departments (i.e. water, sewer, streets and electric) the proposed small cottage housing units will not cause a City facility to operate beyond capacity.

Water, Sewer, Electricity and Urban Storm Drainage

Water

The application explains, and Public Works has confirmed, that there is a six-inch water main in Sylvia Street, a four-inch water main in Alicia Street, and a fire hydrant is in place directly across from the driveway on Alicia Street. The applicant further asserts that there is adequate water pressure available to provide water service to the proposed new units.

Sanitary Sewer

The application explains, and Public Works has confirmed, that there is a six-inch sanitary sewer line within the right-of-way for Alicia Street and Sylvia Street. The applicant further notes that in discussions with the sanitary sewer department, there are no reported capacity issues in the vicinity. The application concludes that the 12 proposed small, water-efficient units should not cause the system to operate beyond its current capacity. Public Works staff have indicated they do not believe that this development will be putting enough new flow into the system to

negatively impact downstream capacity, that lines are very flat in this neighborhood, and they see no issues for sanitary sewer capacity, noting that the development drains into a sewer trunk line east of Sylvia Street, and on into the Oak Street line north of Nevada Street where there are no known capacity issues.

Electricity

The application indicates that electrical infrastructure is available in the immediate vicinity, and that the applicant has worked with the electrical department to design the provided electrical service plan, and is seeking to address desired solar panel installation and net-metering while addressing the neighborhood's existing electrical infrastructure.

Urban Storm Drainage

The application notes, and Public Works has confirmed, that there is a ten-inch storm sewer line within the Sylvia Street right-of-way. The applicant explains that the project is required to employ low impact development standards under the City's Cottage Housing Development Standards as well as under the Rogue Valley Sewer Services (RVSS) Standards for Storm Water Management, and the low impact development measures proposed including the use of pervious walkways and rain-barrel catchment of roof drainage to reduce the amount of storm water generated. The applicant proposes to capture, detain, treat and regenerate all storm drainage on the property through the use of a StormTech system which will detain and treat storm water before releasing it into the existing irrigation ditch adjacent to the parking lot. The applicant asserts that this should result in no added impacts to the Sylvia Street storm drain facilities.

Police & Fire Protection

An existing fire hydrant is in place directly across Alicia Avenue from the driveway entrance. As is typical, the Fire Marshal will review the final civil drawings and building permit submittals for compliance with fire codes relative to water supply and fire apparatus access, and conditions have been included below to require that the applicants address the requirements of the Fire Department including but not limited to approved addressing, fire apparatus access, fire hydrant distance and fire flow, as part of the Final Plan application submittal. With regard to the current Outline Plan, the Fire Marshal has indicated that he sees no issues which would preclude this development from occurring as they would be required to provide adequate access as well as hydrant placement and potentially sprinkling units that cannot meet fire requirements. The Fire Marshal concludes that in the event of an evacuation order from this area, there are two current means to access Oak Street and at some point, with infill likely, additional access to Carol and/or Clinton could develop.

Adequate Transportation

Alicia Avenue is a residential neighborhood street, as are nearby Sylvia Street, Oak Lawn Avenue, and Sleepy Hollow Drive which form the street system for the neighborhood off of Oak Street here. The Alicia Avenue right-of-way is 47 feet in width, and is paved to a width of approximately 20 feet. There are no sidewalks, curbs or gutters in place on either side of the

street, and right-of-way beyond the pavement is largely surfaced in gravel and accommodates pedestrian circulation and intermittent on-street parking.

The Planning Commission finds that the driveway leading to the site's proposed parking area is proposed to be 20-feet in width with a five-foot-wide sidewalk along its east side connecting from Alicia Avenue to the internal pedestrian circulation connecting to each unit and continuing through to the proposed open space. The scale of the proposed development does not trigger a Traffic Impact Analysis or other transportation assessment. Planning staff have noted that in recently considering a similarly sized cottage housing development at 476 North Laurel Street recently, a 12-unit cottage housing development was found to generate approximately 88 average daily trips (ADT) with eight p.m. peak hour trips and six a.m. peak hour trips while the trigger point for a Traffic Impact Analysis is 50 peak hour trips. Engineering staff have indicated that while no trip counts are available for Alicia Avenue, they would estimate that the existing daily trips on the street at around 100 ADT. The Commission finds that a residential neighborhood street is assumed to be able to accommodate up to 1,500 ADT, and as such the street has adequate transportation capacity to serve the 12 proposed small homes.

The application includes preliminary Grading, Utility and Erosion Control Plans prepared by Registered Professional Engineer Scott D. Pingle of KAS & Associates, Inc. which identify existing facilities available in the adjacent rights-of-way along with proposed connections; meter and transformer placement; and storm water control, detention and treatment systems. The Planning Commission finds that the site's utilities will be extended to and through the subject property from public utility easements and street right-of-way adjacent to the site, and that based on the conceptual plans and details from the various service providers, adequate key city facilities are available within the adjacent rights-of-way and will be extended by the applicant to serve the proposed development. Conditions have been included below to require that final electric service, utility and civil plans be provided for the review and approval of the Staff Advisor and city departments in conjunction with the Final Plan submittal, and that civil infrastructure be installed by the applicants, inspected and approved prior to the signature of the final survey plat.

The third criterion for approval of an Outline Plan is that, "*The existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.*"

Trees

The Commission notes that six trees are identified on the subject property including: a 20-inch Plum which is proposed to be removed due to its location relative to necessary driveway improvements; a 14-inch unspecified deciduous tree; a 10-inch Walnut; an eight-inch Willow; a ten-inch Willow; and a 36-inch multi-trunked Willow which is proposed to be removed as a hazard tree. There is also a 14-inch Pine on the adjacent property near the west property line.

The Commission finds that the application includes an assessment of the trees by Christopher John, a certified arborist with Canopy, LLC. John notes that the large Willow has three trunks

(21½-inches, 24-inches and 30-inches) in close proximity to one another, and that all three exhibit evidence of previous large limb failure, extensive rot and fungal growth, and multiple structural defects including cracks, and overall poor health. The arborist's assessment is that this Willow is not suitable for an urban setting and would pose a hazard for people on the property, all the more so with further development, and he accordingly recommends that this Willow be removed and replaced with a more suitable tree.

With regard to the 20-inch Plum located near the northern entrance to the property, the arborist explains that the tree is quite large for its species, and as the species is prone to do it has been losing limbs. He goes on to emphasize that Plums require maintenance that this tree has not received and as a result it has a poor form and limb failure. He concludes that this combined with the location relative to the driveway and parking lead him to recommend removal and replacement of the tree.

For the remaining trees to be preserved and protected, the arborist recommends installation of tree protection fencing as required in the City's Tree Preservation & Protection Ordinance (AMC 18.4.5), avoiding compaction within the tree protection zones, root protection during work within tree protection zones, and periodic watering during the months of June through September.

Potential Wetland

The Commission notes that a possible wetland which is not noted on the city's Local Wetlands Inventory has been identified on the subject property, and the applicant suggests it was formed because the "Million Ditch" irrigation canal passes through the property and continues on to the property immediately to the east. The potential wetland area has been preliminarily assessed by a wetland biologist with Northwest Biological Consultants who has provided a letter indicating the wetland is a small area affected by irrigation water overflow from an open ditch and disconnected pipe which has since been repaired, and notes that the presence of upland soils and weak indicators of soils, plants, and hydrology suggest the presence of a small, marginal wetland. The letter goes on to note that with the pipe repaired and the artificial water source eliminated, new data will be collected this spring to determine whether removal of the artificial water source has eliminated the source of artificial hydrology for the potential wetland. The wetland biologist indicates that they believe this will be the case and that as such, the area will be determined not to be a jurisdictional wetland upon review by the Oregon Department of State Lands (DSL). The wetland biologist concludes that pending that review by DSL, no ground disturbing activities are to take place within the wetland area or its buffer.

The Planning Commission finds that if the possible wetland is found to be jurisdictional by DSL, an area extending 20 foot beyond its upland edge would be required to be protected within a Water Resource Protection Zone (WRPZ) as provided in AMC 18.3.11. While there is not a clear identification in the materials of the potential wetland's full extent with its protection zone, there is an area identified for protection with silt fencing within the proposed open space on the applicant's Erosion Control Plan (Sheet B2) which correlates to cattails and reeds growing on-site.

The Planning Commission finds that the applicant has identified and assessed the site's natural features including a possible wetland and six trees and proposed to preserve and protect all but two of the trees. One, a large Willow, has been found by a certified arborist to pose a hazard due to overall poor health including large limb failures, extensive rot, fungal growth and multiple structural defects. The other, a large Plum, has not been properly cared for resulting in poor form, and limb failure and it is proposed for removal as well. The remaining trees are to be preserved and protected. Similarly, the applicant has enlisted the services of a wetland biologist to assess a possible wetland on the property – the site's irrigation system has been repaired, further assessment and formal delineation of the wetland is underway, the site plan incorporates an open space configuration to preserve and protect the possible wetland and buffer, and no further ground disturbance is to occur pending DSL concurrence with the applicant's delineation. If determined to be jurisdictional through a formal delineation, the full extent of the wetland and associated buffer zone will need to be clearly detailed in the Final Plan materials and protected from disturbance within the development's open space. Conditions have been included below to require that for the wetland, a delineation be prepared, submitted for review and concurrence obtained from DSL, and the results incorporated into the Final Plan drawings including protection of the WRPZ. For the trees, conditions have been recommended to include tree protection fencing installation and verification before site work, and incorporation of the arborist's recommendations into a revised Tree Protection Plan for inclusion with the Final Plan submittal.

The fourth criterion for approval of an Outline Plan is that, *“The development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.”* The Planning Commission finds that the development will not prevent adjacent land from being developed with the uses envisioned by the Comprehensive Plan. Adjacent properties to the north, south and west are largely developed with single family residences as envisioned in the Comprehensive Plan. The property immediately to the east is separated from the subject property by slopes ranging from 15 percent to more than 35 percent, and as such any future development would need to take access from Clinton Street.

The fifth approval criterion is that, *“There are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.”* The Planning Commission finds that at the time the Final Plan application is submitted, “Covenants, Conditions and Restrictions (CC&R's)” documents will be drafted by the project's land use attorney and surveyor for review and approval by the City. These documents will address the project's common area in order provide adequate assurances relative to open space maintenance. The Commission further finds that if the project is to be completed in phases, the open space shall be completed no later than the sixth unit's occupancy, and all private landscaping improvements are to be completed prior to each unit's individual occupancy. Conditions requiring draft CC&R's and phasing details be included with the final plan submittal have been included below. Based on the foregoing, the Commission concludes that the proposal complies with the fifth approval criterion.

The sixth criterion is that, *“The proposed density meets the base and bonus density standards established under this chapter.”* AMC Table 18.2.3.090.C.1 Cottage Housing Development Density addresses the permissible number of cottages for a cottage housing development in the R-1-5 zoning district providing

that one cottage per 2,500 square feet of lot area is allowed, with a maximum number of 12 cottages. The Planning Commission finds that the 54,722 square foot property here will accommodate 12 cottages ($54,722/2,500 = 21.89$) and 12 are proposed which complies with the allowed Cottage Housing Development Density. Based on the foregoing, the Commission concludes that the proposal complies with the sixth approval criterion.

The final Outline Plan approval criterion is that, “*The development complies with the Street Standards.*” The subject property fronts on Alicia Avenue for a width of approximately 35 feet at the intersection with Sylvia Street. Alicia Avenue is a residential neighborhood street, as are nearby Sylvia Street, Oak Lawn Avenue, and Sleepy Hollow Drive which form the neighborhood’s street system off of Oak Street here. The Alicia Avenue right-of-way is 47 feet in width, and is paved to a width of approximately 20 feet. There are no sidewalks, curbs or gutters in place on either side of the street, and right-of-way beyond the pavement is largely surfaced in gravel and used both for pedestrian travel and scattered on-street parking.

For residential neighborhood streets, City street standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb and seven-foot parking bays on each side, with an 11- to 14-foot queuing travel lane. The city standard cross-section includes a 25- to 28-foot curb-to-curb paved width in a 50- to 55-foot right-of-way.

The Planning Commission notes that the existing street frontage is only 34-feet 4-inches in width, and the proposed driveway is to take up 30-feet of that width. The Commission finds that with the limited frontage taken up virtually in its entirety with required driveway improvements, there is no additional width for sidewalk installation. As such, a condition has been included below to instead require that the applicant instead sign-in favor of a Local Improvement District (LID) for the future improvement of Alicia Avenue, and of Oak Lawn Avenue which provides a connection out to Oak Street and its sidewalk system.

The Cottage Housing Development Standards (AMC 18.2.3.090.C.3.a) generally provide that except for street connections identified on the Transportation System Plan’s Street Dedication Map (Figure 10-1), the Planning Commission may reduce or waive requirement to dedicate and construct a public street according to the Street Design Standards in AMC 18.4.6.040 upon a finding that the Cottage Housing Development meets connectivity and block length standards by providing public access for pedestrians and bicyclists with an alley, shared street, or multi-use path connecting the public street to adjoining properties. The Commission finds that the existing street system within the immediate neighborhood meets the block length standards – existing block lengths are 165-175 feet where the block length standards call for a maximum length of 300-400 feet – and while not fully improved to City street design standards, functions comparably to a shared street and provides adequate connectivity through the neighborhood and out to Oak Street. City park land is located along the Bear Creek corridor to the northeast, and a path from the subject property’s driveway through the site, across a neighboring private property, to the park property would be approximately 450 feet and traverse severely constrained slopes. The park property is less than 500 feet from the driveway entrance traveling due north on Sylvia Street, and as such the Commission finds that additional right-of-way or easement dedication is not merited.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Outline Plan subdivision approval under the Performance Standards Options chapter.

2.4 The Planning Commission concludes that the proposal satisfies all applicable criteria for Site Design Review approval.

The first approval criterion addresses the requirements of the underlying zone, requiring that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”* The Planning Commission finds that the building and yard setbacks and other applicable standards have been evaluated to ensure consistency with the applicable provisions of part 18.2, and all regulations of the underlying R-1-5 zoning will be satisfied.

The second approval criterion deals with overlay zones, and requires that, *“The proposal complies with applicable overlay zone requirements (part 18.3).”* The Planning Commission finds that the property is within the Performance Standards Option (PSO) overlay zone, which requires that all developments other than partitions or individual dwelling units be processed under Chapter 18.3.9., and that the proposal involves a 12-unit cottage housing development and 13-lot subdivision for which the applicant has requested Outline Plan approval under the PSO-Overlay chapter 18.3.9.

The Planning Commission further finds that the subject property is located within the Wildfire Lands Overlay, and as such a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 will need to be provided for the review of the Fire Marshal prior to bringing combustible materials onto the property. New landscaping proposed will need to comply with these standards and shall not include plants listed on the Prohibited Flammable Plant List per Resolution 2018-028. Conditions to this effect have been included below.

The Commission finds that while no wetlands are identified on the subject property in the Local Wetlands Inventory, the applicant has identified a possible wetland on the property, a wetland biologist has been retained to assess the possible wetland, the possible wetland and a 20-foot buffer has been incorporated into the proposed open space, the applicant has proposed to limit ground disturbance until a delineation has been reviewed by the Oregon Department of State Lands and the applicant has planned all site improvements outside of the possible wetland and its buffer zone. The applicant further proposes to include a conservation area with restrictions stipulates that the uses and activities within the Water Resource Protection Zone shall be consistent with the provisions of AMC 18.3.11.

Based on the foregoing, the Planning Commission finds that this criterion is satisfied.

The third criterion addresses the Site Development and Design Standards, requiring that *“The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.”* The Planning Commission finds that the proposal complies with the applicable Site Development and Design and that the various plans have been prepared based on these standards

and the recently adopted Cottage Housing ordinance. With regard to the parking requirements in AMC 18.4.3, cottage housing units less than 800 square feet require one off-street parking space be provided per unit, while units greater than 800 square feet and less than 1,000 square feet require 1½ spaces. Cottage Housing Developments are exempted from the requirement to provide on-street parking. Here, nine of the 12 units are 800 square feet while three are 999 square feet, and a total of 14 spaces are required $[(9 \times 1) + (3 \times 1.5) = 13.5]$. The Commission finds that 14 off-street parking spaces are proposed to fully satisfy the requirements for the 12 units proposed units here. Carports are considered by code to be garages, and separate bicycle parking facilities are not required where a garage is available. The Planning Commission finds that all required off-street parking has been provided on site, that on-street and bicycle parking are not required, and concludes that the third criterion has been satisfied.

The fourth approval criterion addresses city facilities, specifically requiring that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.”* The Planning Commission finds that adequate capacity of city facilities, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property, and that these items are addressed in detail in the Outline Plan discussion in section 2.3 above. The Commission concludes that this criterion has been satisfied.

The final criterion for Site Design Review approval addresses “Exception to the Site Development and Design Standards.” The Planning Commission finds that the applicant has not requested any Exceptions to the Site Development and Design Standards, and as such this criterion does not apply.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

2.5 The Planning Commission finds that the proposal satisfies all applicable standards specific to Cottage Housing Development.

The Planning Commission finds the proposal complies with the allowed development density, floor area ratio, height and lot coverage standards, with 12 cottages proposed for a 54,722 square foot parcel and a combined floor area ratio of 0.18. 75 percent of the proposed cottages are 800 square feet in gross habitable floor area, all of the cottages are proposed with roof peaks less than 25 feet from grade, exhibits have been provided to demonstrate that cottages within the development will not cast a shadow upon the roof of another cottage, and cottages along the north property line are noted as being designed to comply with Solar Setback Standard A. Lot coverage is proposed at 42 percent and is within the allowed standards for the R-1-5 zoning district.

The Commission further finds that building separations are equal or greater than the six-foot minimum for cottages. With the exception of the attached units all cottages are separated by at least nine feet.

The Commission finds that any fencing will comply with the limitations of the fence code and will not exceed four feet on interior areas adjacent to open space except as allowed for deer fencing, and a

condition has been included below to requires that the CC&R's detail the fencing limitations.

The Planning Commission finds that the existing driveways separation between the subject property and 732 Sylvia Street to the north is non-conforming. There are curbs on Alicia or Sylvia, but the two driveways are immediately adjacent to one another and there is no additional frontage to create separation. The Commission finds that the existing non-conformity will not be made more non-conforming with the proposed development here.

The Commission finds that the driveway and parking area proposed meet the vehicle area design standards in AMC 18.4.3. All spaces are to be standard nine-foot by 18-foot spaces with a 24-foot back-up area provided. The Commission further finds that the proposal meets the off-street parking requirements of AMC 18.4.3.040, providing 14 spaces for the 12 cottages in a single, consolidated parking area. Parking is to be provided in carports on each side of the drive aisle, and the carports will include solar panels as part of the applicant's effort to achieve zero net energy for the project. The driveway is proposed to be improved to 20-feet in width which complies with the minimum driveway width for access to a parking area for 14-parking spaces. The driveway has been designed to accommodate a fire truck turn-around, and will be designated as such on-site, as required by the Ashland Fire Department.

The property has frontage along Alicia Avenue. There are no street connections identified in the vicinity on the City's Street Dedication Map, and adjacent development, natural features and topography pose difficulties for future street extension. The applicant has proposed a walkway along the east side of the driveway extending from the street into and around the development for the use of residents and guests of the proposed cottage housing development.

The Planning Commission finds that 14,701 square feet, or 26.86 percent of the site, is proposed in open space, where a minimum of 20 percent is required. The proposed common area open spaces consist of turf areas with pathways for easy access to and from the covered parking spaces to the cottage units. To the east of the parking area, a large landscape common area with decomposed granite paths at the edges of the estimated wetland buffer zone is proposed. This common area is connected to the formal walkways and a 20-foot turf strip to a community garden area that is along the west property line. A total area of 14,701 square feet of the lot area is dedicated to open spaces, which include 12,028 square feet of open space plus the possible wetland and its buffer which total 2,673 square feet in area.

The Commission finds that the open spaces have no dimensions of less than 20 feet, and are connected with five-foot walkways. The open space is generally centrally located, and all units abut common open spaces that are 20-feet in width or greater. The cottages are arranged around the edges of the property with the primary common open space generally centered on the site. Eight of the units abut this open space. Additionally, along the north and south sides of the parking area, a 20-foot landscaped area with common walkway is proposed, and Lot #6 abuts the community garden open space. Of the 12 units proposed, seven directly abut the larger open spaces and all abut an at least 20-foot wide common open space.

The Commission further finds that the common open space is separated from the private outdoor areas. Private outdoor areas are proposed adjacent to the units with pathways that connect the entrances of the units to and through the open space with a walkway leading to the parking areas and public street beyond. Each cottage unit has a private, useable outdoor area of at least 200 square feet which includes garden areas, and porches or patios. These private areas do not have any dimensions less than eight feet. Low fencing and landscaping will provide visual boundaries around the perimeter of the common areas and between the private yards.

The Commission finds that the development proposes on-site infiltration through an on-site catchment system that is a part of the project's low impact development storm water measures detailed in the civil and landscape drawings. Landscape garden bed filtration systems, permeable walkways and rain barrels are to be provided to allow for natural filtration and on-site filtration, and site drainage has been engineered to be filtered per regional (RVSS) standards with a StormTech system which, based on percolation rate of the soils and the storm water event studies, will retain and regenerate all storm water on-site.

Based on the foregoing, The Planning Commission concludes that, as detailed above and with the conditions discussed, the proposal is consistent with the Cottage Housing Development Standards.

2.6 The Planning Commission notes that there are six trees on the subject property including: a 20-inch Plum which is proposed to be removed due to its location relative to necessary driveway improvements; a 14-inch unidentified deciduous tree; a 10-inch Walnut; an eight-inch Willow; a ten-inch Willow; and a 36-inch multi-trunked Willow which is proposed to be removed as a hazard tree. The application also notes that there is a 14-inch Pine on the adjacent property near the west property line.

The Commission also notes that the application includes an assessment of the trees by Christopher John, a certified arborist with Canopy, LLC. The project arborist notes that the large Willow has three trunks (21½-inches, 24-inches and 30-inches) in close proximity to one another, and that all three exhibit evidence of previous large limb failure, extensive rot and fungal growth, and multiple structural defects including cracks, and overall poor health. His assessment is that this Willow is not suitable for an urban setting and poses a hazard for people on the property, all the more so with further development of the site. He recommends that this Willow be removed as a hazard and replaced with a suitable tree. This tree is located within the buffer zone of the possible wetland identified, however hazard tree removal is exempt from regulation under the Water Resources Protection Zones (WRPZ) Ordinance in AMC 18.3.11.

The Commission further notes that with regard to the 20-inch Plum located near the northern entrance to the property, the arborist indicates that the tree is quite large for its species, and as the species is prone to do it has been losing limbs. He goes on to emphasize that Plums require maintenance that this tree has not received and as a result it has a poor form and has experienced limb failure. He concludes that this combined with the location relative to the driveway and parking lead him to recommend removal and replacement of the tree. The applicant notes that removal of this tree allows the site to develop in a manner consistent with applicable Site Design standards, and that the removal will not have any impact

on erosion, soil stability, flow of surface waters, protection of adjacent trees, windbreaks, or tree densities. The Commission finds that 23 new trees are identified in the Landscape Plan provided (Sheet L-101), which more than satisfies the one-for-one mitigation requirement.

The Commission further notes that the Ashland Tree Commission was unable to convene its regular monthly meeting for May of 2020 due to the City Administrator's state of emergency declaration for the Coronavirus (COVID-19) outbreak, which suspended advisory commission meetings. As such there is no Tree Commission recommendation. As provided in AMC 2.25.040, the failure of the Tree Commission to make a recommendation on any individual planning action shall not invalidate that action.

The Commission finds that the remaining trees which are to be preserved are proposed to be protected with six-foot tall chain link fencing as recommended by the arborist and required in the City's Tree Preservation & Protection Ordinance (AMC 18.4.5). In addition, the arborist has recommended that the applicant avoid soil compaction within the tree protection zones, provide for root protection during any work within tree protection zones, and periodically water preserved trees from during the warmer months (June through September). Conditions have been included to require tree protection fencing installation and verification before site work, and to incorporate the arborist's recommendations into a revised Tree Protection Plan for inclusion with the Final Plan submittal.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Tree Protection and for Tree Removal Permits to remove two trees.

SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Outline Plan subdivision and Site Design Review approvals for a 12-unit/13-lot Cottage Housing development, and Tree Removal Permit to remove two trees is supported by evidence contained within the whole record.

The project is intended to as a zero net energy development with solar panels installed on the two carport buildings, and will include 12 modestly sized units developed around a generous central open space which includes an area reserved to protect a possible wetland, and in the Commission's assessment is the type of development envisioned with the adoption of the Cottage Housing ordinance. The Commission believes that the development merits approval with the conditions detailed below.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2020-00017. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2020-00017 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein, including that no ground-disturbing activities are to take place within the potential wetland area or its associated buffer until it has been determined whether the potential wetland is jurisdictional and concurrence has been obtained from the Oregon Department of State Lands (DSL).
2. That any new addresses shall be assigned by City of Ashland Engineering Department. Street and subdivision names shall be subject to City of Ashland Engineering Department review for compliance with applicable naming policies.
3. That permits shall be obtained from the Ashland Public Works Department prior to any work in the public right of way, including but not limited to permits for new driveway approaches or any necessary encroachments.
4. That a Tree Verification Permit shall be applied for and approved by the Ashland Planning Division prior to any site work including excavation, staging or storage of materials, or excavation permit issuance. The Tree Verification Permit is to inspect the identification of the two trees to be removed and the installation of tree protection fencing for trees to be protected on adjacent properties. Standard tree protection consists of chain link fencing six feet tall and installed in accordance with the requirements of AMC 18.4.5.030.B. No construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
5. That the applicant shall obtain approval of Demolition/Relocation Review Permits through the City of Ashland Building Division prior to demolition of existing structures on the subject property if found to be necessary by the Building Official.
6. That a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 of the Ashland Land Use Ordinance shall be provided prior to bringing combustible materials onto the property, and any new landscaping proposed shall comply with these standards and shall not include plants listed on the Prohibited Flammable Plant List per Resolution 2018-028.
7. That the Final Plan submittal shall include:
 - a. That draft CC&Rs for the Homeowner's Association shall be provided for review and approval of the Staff Advisor with the Final Plan application. The CC&R's shall describe responsibility for the maintenance of all common use-improvements including driveways, parking areas, carports, landscaping/open space, and storm water facilities. The cottage housing fencing limitations, floor area limitations and the prohibition on ARU's shall be clearly addressed in the CC&R's.
 - b. The approved Tree Protection Plan and accompanying standards for compliance shall be noted in the CC&Rs. The CC&Rs must state that deviations from the plan shall be considered a violation of the Planning Application approval and therefore subject to penalties described in the Ashland Municipal Code.
 - c. A wetland delineation with concurrence from the Oregon Department of State Lands (DSL). If the delineation identifies a jurisdictional wetland, the wetland and its protection zone shall be clearly identified in the Final Plan drawings.
 - d. A phasing plan for the completion of the development.
 - e. Final site lighting details.
 - f. Final lot coverage calculations demonstrating how lot coverage is to comply with the applicable coverage allowances of the zoning district. Lot coverage includes all building

footprints, driveways, parking areas and other circulation areas, and any other areas other than natural landscaping.

- g. All easements including but not limited to public and private utilities, irrigation, mutual access and circulation, and fire apparatus access shall be indicated on the Final Plan submittal for review by the Planning, Engineering, Building and Fire Departments.
- h. Final electric service, utility and civil engineering plans including. All civil infrastructure shall be installed by the applicants, inspected and approved prior to the submittal of the final survey plat for review and signature.
 - i. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer mains and services, manholes and clean-outs, and storm drainage pipes and catch basins, along with any backflow prevention measures required by the Water Department because there is a non-potable water source (irrigation) on the property. Any required private or public utility easements shall be delineated on the civil plans.
 - ii. The final electric design and distribution plan shall include load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment with the Final Plan application. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department. Electric services shall be installed underground to serve all lots within the applicable phase prior to submittal of the final survey plat for review and signature. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.
 - iii. The storm drainage plan shall detail the location and final engineering for all storm drainage improvements associated with the project, and shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions. The storm drainage plan shall demonstrate that post-development peak flows are less than or equal to the pre-development peak flow for the site as a whole, and that storm water quality mitigation has been addressed through the final design.
- i. A final grading and erosion control plan.
- j. A final Tree Protection Plan addressing the trees on the property to be preserved and trees on adjacent properties within 15 feet of the property line. The plan shall identify the location and placement of fencing around the drip lines of trees identified for preservation as required in AMC 18.4.5.030.B.1. The amount of fill and grading within the drip line shall be minimized. Cuts within the drip line shall be noted on the tree protection plan,

and shall be executed by handsaw and kept to a minimum. No fill shall be placed around the trunk/crown root. The recommendations of the project arborist in terms of soil compaction, root protection and periodic water shall be incorporated into this plan.

- k. A final size- and species-specific landscaping plan including irrigation details satisfying the Water Conserving Landscaping Guideline in AMC 18.4.4.030.I. New landscaping shall comply with the General Fuel Modification Area requirements and shall not include plants listed on the Prohibited Flammable Plant List adopted by Resolution #2018-028. All landscaping shall be installed according to the approved plan, and tied into the existing irrigation system, inspected and approved prior to the issuance of a certificate of occupancy.
 - l. That the requirements of the Ashland Fire Department relating to fire hydrant distance, spacing and clearance; fire flow; fire apparatus access, approach, turn-around, and firefighter access pathway; approved addressing; fire sprinkler and extinguishers as applicable; limits on fencing and gates which would impair access; and wildfire hazard area requirements shall be satisfactorily addressed in the Final Plan submittals. Fire Department requirements shall be included in the civil drawings, and a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements of AMC 18.3.10.100.A.2. shall be included with the Final Plan submittal.
8. A final survey plat shall be submitted for the review and approval of the Staff Advisor within 12 months and approved by the City of Ashland within 18 months of this approval. Prior to submittal of the final subdivision survey plat for review and signature:
- a. The final survey plat shall include a deed restriction notifying future property owners that the size of a cottage dwelling may not be increased beyond the maximum floor area in subsection 18.2.3.090.C.2.a. This size limitation shall also be addressed in the development CC&R's.
 - b. All easements including but not limited to public and private utilities, mutual access, and fire apparatus access shall be indicated on the final survey plat as required by the Ashland Engineering Division.
 - c. The driveway approach shall be installed under permit from the Public Works Department and in accordance with the approved plan, inspected and approved prior to the submittal of the final survey plat for signature.
 - d. Subdivision infrastructure improvements including but not limited to utility installations shall be completed according to approved plans prior to submittal of the final survey plat for review and signature.
 - e. The driveway shall be paved to 20-foot width, a vertical clearance of 13-feet, 6-inches and be able to withstand 44,000 lbs. The flag drive shall be constructed so as to prevent surface drainage from flowing over the private property lines and/or the public way.
 - f. Electric services shall be installed underground to serve all lots, inspected and approved. The electric service plan shall be reviewed and approved by the Ashland Electric, Building, Planning and Engineering Divisions prior to installation.
 - g. Sanitary sewer laterals and water services including connection with meters at the street shall be installed to serve all lots, inspected and approved.

- h. The property owner shall sign in favor of Local Improvement Districts (LIDs) for the future street improvements, including but not limited to paving, sidewalks, parkrow with irrigated street trees, curb, gutter, storm drainage and undergrounding of utilities, for Alicia and Oak Lawn Avenues. This LID agreement shall be signed and recorded concurrently with the final survey plat. Nothing in this condition is intended to prohibit an owner/developer, their successors or assigns from exercising their rights to freedom of speech and expression by orally objecting or participating in the LID hearing or to take advantage of any protection afforded any party by City ordinances and resolutions.
9. That the building permit submittals shall include:
- a. Final permit drawings addressing all requirements of the Building Division, including but not limited to that the attached units or any units where exterior walls are less than three feet from a property line shall address fire separation requirements, and fire-rated assemblies for attached units shall include a sound transmission class rating of 45 for airborne sound.
 - b. Identification of all easements, including public and private utility easements, mutual access easements and fire apparatus access easements.
 - c. Solar setback calculations demonstrating that the northern units comply with Solar Setback Standard A in the formula $[(\text{Height} - 6) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$ and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and the height(s) from natural grade. Other cottage housing units shall provide demonstration of compliance with the Cottage Housing Development Standards requiring that the not cast a shadow on the roof area of another cottage.
 - d. That storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system through the curb or gutter at a public street, a public storm pipe, an approved public drainage way, or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.

Planning Commission Approval

July 14, 2020
Date

FINDINGS

PA-T2-2020-00019

**Vacant Tax Lots #10104 & #10105 on First Street
(North of Lithia Way, Across from the Post Office)**

BEFORE THE PLANNING COMMISSION
July 14, 2020

IN THE MATTER OF PA-T2-2020-00019, A REQUEST FOR SITE DESIGN REVIEW) APPROVAL TO CONSTRUCT THE THIRD AND FINAL PHASE OF THE FIRST) PLACE SUBDIVISION FOR THE PROPERTY LOCATED AT THE CORNER OF) LITHIA WAY AND FIRST STREET. THE PROPOSED PHASE THREE) REQUESTS SITE DESIGN REVIEW APPROVAL TO CONSTRUCT A NEW THREE-) STORY, MIXED-USE BUILDING - THE APPLICANT’S “PLAZA NORTH” - ON) LOTS #4 AND #5, TWO VACANT LOTS AT THE NORTHEAST CORNER OF) THE SITE ALONG FIRST STREET, ACROSS FROM THE U.S. POST OFFICE.) THE PROPOSAL INCLUDES CONSOLIDATION OF THE TWO LOTS AND) CONSTRUCTION OF A 10,547 SQUARE FOOT, THREE-STORY MIXED USE) BUILDING INCLUDING GROUND FLOOR COMMERCIAL SPACE AND FOUR) RESIDENTIAL UNITS ON THE UPPER FLOORS. THE APPLICATION) REQUESTS TO MODIFY THE COMMON AREA LANDSCAPING AND PARK-) ING CONFIGURATION TO PROVIDE MORE EFFICIENT ACCESS FROM THE) ACCESSIBLE PARKING TO THE ENTRANCES, AND EXCEPTIONS TO THE) SITE DEVELOPMENT AND DESIGN STANDARDS’ “DOWNTOWN DESIGN) STANDARDS” TO ALLOW A STAGGERED STREET SETBACK AND THE) UPPERFLOOR WINDOWS TO BE INSTALLED IN GROUPS OF THREE WHICH) WHEN VIEWED TOGETHER ARE MORE HORIZONTAL THAN VERTICAL.)

**FINDINGS,
CONCLUSIONS
& ORDERS**

APPLICANT/OWNER: Randy Jones for First Place Partners, LLC)

RECITALS:

- 1) Tax lots 10104 and 10105 of Map 39 1E 09 BA are located on First Street across from the U.S. Post Office, and are Lots #4 and #5 within the First Place Subdivision. Both lots are zoned Commercial (C-1).
- 2) The applicant is requesting Site Design Review approval to construct the third phase of the First Place Subdivision, which is located at the corner of Lithia Way and First Street.

Phase One included the construction of a three-story 18,577 square foot mixed-use building (designated as “Plaza West” by the applicants) with basement parking, commercial space on the first floor, and ten residential units split between the ground, second and third floors. This building was approved for the subdivision’s Lot #1 by the Planning Commission in 2012, is now addressed as 175 Lithia Way, and is occupied by Pony Espresso Coffeehouse Café and Washington Federal Bank.

Phase Two is now under construction and consists of a three-story mixed-use building, the applicant’s “Plaza East”, on Lots #2 and #3 of the subdivision, at the corner of Lithia Way and First Street. The

building is approximately 32,191 square feet, with basement parking, ground floor commercial, and 34 residential units providing artist housing for the Oregon Shakespeare Festival distributed between the ground, second and third floors.

Phase Three proposed here involves a request to consolidate the subdivision's two remaining vacant lots (Lots #4 and #5) to construct a new 10,547 square foot, three-story mixed use building as the third and final phase of the First Place subdivision. The building's ground floor will be primarily commercial space, while the second floor will include three one-bedroom residential units and the third floor will have one two-bedroom residential unit. The proposal includes modifications to the common area landscaping and parking configuration to provide more efficient access from the accessible parking to the entrances, and two requests for Exceptions to the Site Design and Use Standards' Downtown Design Standards to allow for a staggered street setback and to allow vertical windows to be grouped in threes on the upper floor so that they appear to be more horizontal than vertical.

Proposed site improvements are outlined on the plans on file at the Department of Community Development.

- 3) The criteria for Site Design Review approval are described in AMC Chapter 18.5.2.050 as follows:
 - A. ***Underlying Zone.*** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
 - B. ***Overlay Zones.*** *The proposal complies with applicable overlay zone requirements (part 18.3).*
 - C. ***Site Development and Design Standards.*** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
 - D. ***City Facilities.*** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.*
 - E. ***Exception to the Site Development and Design Standards.*** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1, 2, or 3, below, are found to exist.*
 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty;*

2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards; or*
3. *There is no demonstrable difficulty in meeting the specific requirements for a cottage housing development, but granting the exception will result in a design that equally or better achieves the stated purpose of section 18.2.3090 (Ord. 3147 § 9, amended, 11/21/2017).*

4) On April 15, 2020 Oregon Governor Kate Brown issued Executive Order #20-16 “*Keep Government Working: Ordering Necessary Measures to Ensure Safe Public Meetings and Continued Operations by Local Government During Coronavirus (COVID-19) Outbreak.*” The Governor’s Order required that public bodies hold public meetings by telephone, video, or through some other electronic or virtual means, whenever possible; that the public body make available a method by which the public can listen to or virtually attend the public meeting or hearing at the time it occurs; that the public body does not have to provide a physical space for the public to attend the meeting or hearing; that requirements that oral public testimony be taken during hearings be suspended, and that public bodies instead provide a means for submitting written testimony by e-mail or other electronic methods that the public body can consider in a timely manner.

5) The Planning Commission, following proper public notice, held an electronic public hearing on June 9, 2020 which was abbreviated due to technical difficulties with Rogue Valley Television’s live broadcast. The hearing was therefor continued to June 23, 2020 and re-noticed accordingly. In keeping with Executive Order #20-16, this meeting was broadcast live on local television channel 9 and on Charter Communications channels 180 & 181, and was live-streamed over the internet on RVTV Prime at <https://rvtv.sou.edu>. A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report were made available on-line at <http://www.ashland.or.us/Agendas.asp?SectionID=0&CCBID=198> on June 2nd, and again on June 16th, seven days prior to the hearing dates. The applicant was required to submit any presentation materials for consideration at the June 23rd hearing continuation by 3:30 p.m. on June 22nd, and these materials were made available on-line and e-mailed to Commissioners. Those wishing to provide testimony were invited to submit written comments via e-mail to PC-public-testimony@ashland.or.us with the subject line “June 23 PC Hearing Testimony” by 10:00 a.m. on Monday, June 22, 2020, for these comments to be made available on-line and e-mailed to Commissioners. The applicant was invited to provide written rebuttal to these public comments by 12:00 noon on Tuesday, June 23th and as no public comments were received other than from the applicant, no rebuttal was provided. All written testimony received by the deadlines was made available for Commissioners to review before the hearing and has been included in the meeting record. As provided in the Governor’s Executive Order #20-16, no oral public testimony was taken during the hearing. Following the close of the hearing, the Planning Commission approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the following index of exhibits, data and testimony is used:

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, and Miscellaneous Exhibits lettered with an "M"

SECTION 2. CONCLUSORY FINDINGS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the Staff Report, and the public testimony and exhibits received electronically.

2.2 The Planning Commission finds that the proposal to construct the third and final phase of the First Place Subdivision for the property located at the corner of Lithia Way and First Street meets all applicable criteria for Site Design Review as described in AMC 18.5.2.050, and that the proposals for Exceptions to the Site Development and Design Standards' "Downtown Design Standards" meet all applicable criteria for an Exception to the Site Development and Design Standards described in AMC 18.5.2050.E.

2.3 The Planning Commission notes that the current application involves the development of the two remaining vacant lots created with the six-lot "First Place" subdivision of the parent parcels approved by the Planning Commission in October of 2012 as PA #2012-01122. In conjunction with that approval, a Site Review permit to construct a three-story 18,577 square foot mixed-use building with a basement consisting of basement parking, commercial and residential space on the first floor and residential space on the second and third floors was approved as Phase I. This building, now called "Plaza West" by the applicants, is complete and occupied with a bank and coffee shop on the ground floor, and a total of ten residential units. The Commission further finds that site infrastructure including most utilities, paving of the driveway and parking areas, sidewalks, street trees, and streetlights were installed along both frontages with completion of the subdivision improvements and the subsequent development of Lot #1 as Plaza West.

The Commission further notes that the proposed Phase Two of the First Place subdivision is now under construction and consists of a three-story mixed-use building, the applicants' "Plaza East", on Lots #2 and #3 of the subdivision, at the corner of Lithia Way and First Street. The building is approximately 32,191 square feet, with basement parking, ground floor commercial, and 34 units of artist housing for the Oregon Shakespeare Festival distributed between the ground, second and third floors.

The Commission finds that the proposed Phase Three of the First Place subdivision development requires Site Design Review approval to construct a new mixed-use building (the applicants’ “Plaza North”) on Lots #4 and #5 at the northeast corner of the site, with both lots fronting on First Street across from the U.S. Post Office parking area. The proposal includes consolidation of the two lots to construct a 10,547 square foot, three-story mixed-use building including ground floor commercial space and four residential units above. The application also includes requests to modify the common area landscaping and parking configuration to provide for more efficient circulation between the accessible parking and the building entrances by moving an accessible parking space nearer to the building and adding a walkway from the space to the First Street entrance, and two requests for Exceptions to the Site Design and Use Standards’ Downtown Design Standards to allow for a staggered street setback and to allow vertical windows to be installed in groups of three on the upper floors in a manner that appears more horizontal than vertical.

2.4 The Planning Commission finds that the proposal involves a mix of permitted commercial uses (retail and office) and residential units. The commercial uses are outright permitted in the Retail Commercial (C-1) district, and residential units are a special permitted use in the district. C-1 zoning regulations require a minimum of 65 percent of the gross floor area of the ground floor of the building be used for permitted or special permitted uses. Here, the proposal designates 81.4 percent of the ground floor area for Plaza North as commercial space, with the remaining 18.6 percent of the ground floor and the full second and third floors dedicated to accommodating the four residential units.

With the current application and the first two phases (Plaza West and Plaza East), 41 units of the parent parcel’s 43-unit residential density will be built on site. The applicants propose to allocate parking between the proposed buildings, and have provided calculations demonstrating how the available 55 spaces of surface parking and 27 garage spaces are to be allocated between the buildings, as illustrated in the table below, along with calculations demonstrating that the 82 parking spaces as allocated will accommodate the proposed commercial and residential uses proposed. e commercial uses in addition to the proposed residential units. The proposed allocations are summarized in the table below.

| FIRST PLACE SUBDIVISION ALLOCATIONS | | | |
|--|--|---|--|
| BUILDING | SURFACE PARKING (55 Spaces Available) | GARAGE PARKING (27 Spaces Available) | RESIDENTIAL UNITS (43-Unit Available Density) |
| Plaza West (18,577 s.f.) | 15 | 12 | 10 |
| Plaza East (32,191 s.f.) | 26 | 14 | 27* |
| Plaza North (10,547 s.f.) | 14 | 1 | 4 |
| TOTAL | 55 | 27 | 41 |

The Commission finds that a request for allocation of two on-street parking credits as part of this application is not presently necessary to reduce the required off-street parking spaces as the proposal accommodates needed parking on-site without the additional parking credits. In the event a change of use is proposed, which increases the number of parking spaces required, the allocation of available on street parking credits may be considered at that time. A condition has been recommended below requiring that revised parking allocation information be provided with each building permit as the commercial tenant spaces develop or tenant occupancies change to verify that the parking allocated will accommodate the parking required for all existing and proposed uses.

The C-1 zoning district does not require standard setbacks from property lines unless a parcel abuts a residential zoning district, in which case a ten-foot per story rear yard setback and a ten-foot side yard setback are required. The Commission finds that as proposed, the building lots here do not directly abut the R-2 residential zoning district to the north, however the common area parcel north of Plaza North abuts the R-2 zoning district and there is at least a 38-foot separation between the residential property and the proposed three-story Plaza North building. Section drawings have been included on the applicant's Sheet A.1 to demonstrate compliance with Solar Access Ordinance "Standard B" for the proposed Plaza North building.

The proposed building height for the Plaza North building is 40 feet, which has been calculated based on an average of the finished grade on all four elevations. 40 feet is the maximum height permitted in the C-1 zone.

The landscaping plan provided identifies 16.4 percent of the site as included in landscaped areas, which satisfies both the 15 percent requirement for the C-1 district and the required seven percent landscaping requirement for the parking area. In keeping with the requirements of the Site Review Chapter, the application materials provided identify that both the required 15 percent landscaping and seven percent parking lot landscaping are to be provided with completion of the project. The landscaping plans provided include full irrigation plans. A new trash enclosure is illustrated in the parking lot behind Plaza North, and the application further notes that all lights are to be selected and placed to avoid direct illumination of adjacent residential properties. Conditions to ensure that these items are installed and maintained according to standards have been included below.

The bicycle parking requirements in AMC 18.4.3.070 call for at least one bicycle parking space to be provided for every five automobile parking spaces, with fifty percent of these spaces to be covered, and that additional covered bicycle parking spaces be provided for each residential unit. For the 55 surface automobile parking spaces proposed, at least 11 bicycle parking spaces are required to be provided on site and half of these must be covered. Additionally, four covered spaces are required for the three residential units in Plaza North which do not have individual garages. As originally approved, the subdivision proposal identified five "inverted U" racks for ten bicycle parking spaces in the plaza space to the west of Plaza West and three "inverted U" racks for six bicycle parking spaces adjacent to the walkway north of what is to be Plaza North. These eight racks would provide 16 bicycle parking spaces, exceeding the amount required. As currently installed, only three racks for six spaces are in place adjacent to Plaza West and two

racks for four spaces are in place near the proposed Plaza North. A total of at least 15 spaces are required, and a total of ten of these must be covered. A condition has been added below to require that the additional bicycle parking and coverage in keeping with the requirements of AMC 18.4.3.070.I & .J be identified in the building permits and installed prior to occupancy of the building proposed here.

The First Place subdivision lies within the Detail Site Review Zone, the Downtown Design Standards Zone, and the Downtown and Railroad historic districts. As a result, the application is subject to the Basic Site Review Standards for Commercial Development, Detail Site Review Standards, Additional Standards for Large Scale Projects, the Downtown Design Standards and Historic District Design Standards. The Planning Commission finds that because site layout, parking, pedestrian and vehicular access and circulation, and landscaping were largely addressed through the 2012 Subdivision and Site Review applications, the current review is focused largely on the design of the proposed new Plaza North building relative to the applicable design standards.

The Commission finds that the proposed building designs meet the Basic Site Review Standards. Plaza North relates well to the First Street frontage. Streetscape and landscape amenities are being provided in conformance with standards. Parking and circulation are placed behind the buildings, and requisite parking lot landscaping and screening are detailed in the plans provided in a manner consistent with the original approvals and minor modifications here. The application recognizes the requirements to address noise and glare, and notes that noise will be within limits typical of the permitted use and will not exceed standards, and that lighting will be appropriately placed and directed to avoid directly illuminating adjacent properties.

The Commission also finds that the Detail Site Review Standards are fully addressed with the proposed Plaza North building. The project is subject to meeting at least a 0.50 Floor Area Ratio (FAR), and the application includes calculations demonstrating that with the full build-out proposed, the subdivision FAR will be at approximately 1.26, while the Plaza North property is at an approximately 2.0 FAR. The building's mass is divided into vertical bays, and it has strong entries from the sidewalk emphasized through design elements. Awnings are not proposed along the First Street façade due to the stepped setbacks and use of landscaped areas, but instead relies on two recessed entries to provide protection from the elements.

The Plaza North building is subject to Large Scale Design Standards, as its floor area is greater than 10,000 square feet. The application explains that the building complies with the 45,000 square foot floor area limitation as applied within the Downtown Design Standards Zone, noting that the proposed building consists of 10,547 square feet of gross floor area.

The proposed building's design incorporates elements to the façade that are articulated to relate to the human scale, with the building's length along First Street divided into six vertical bays in keeping with the historic downtown pattern, with two recessed entrances to provide pedestrians with protection from the elements and relate the building to a more human scale. The Large Scale requirements call for one square foot of plaza or public space to be provided for every 10 square feet

of gross floor area. The application notes that the development as proposed, including the floor area of all buildings, will have a combined floor area of 61,551 square feet which would require 6,155 square feet of plaza or public space and that the project includes 6,211 square feet of public plaza space which is in excess of that required. The application also notes that the plaza space provided incorporates a mix of at least four of the six elements for plazas and public spaces as called for in the standards, with sitting areas, areas that provide sunlight and shade, protection from wind, trees, and potential outdoor eating areas. Trash and recycling facilities are to be provided in an enclosure within the common parking lot behind the building

The Commission further finds that other than the Exceptions requested, which are discussed more fully below, the building's design complies with the Downtown Design Standards. The applicant has proposed multi-story, downtown-style buildings which extend from side lot line to side lot line placed generally at the back of the sidewalk, and which incorporate large street-level windows and transparent doors. The building incorporates horizontal and vertical rhythms through divisions on the façade as required by the standards, and provides for some variation in parapet height to provide the traditional streetscape appearance sought by the standards. The building incorporates varied architectural and material treatments to provide a base for a sense of strength, flat roofs with parapets, and creates a varied streetscape with distinct character which is in keeping with the standards seeking to maintain the traditional rhythms of the historic downtown.

The Planning Commission finds that the proposed building designs are generally in keeping with the height, scale, massing, setbacks, roof forms, and rhythm of openings typical of the surrounding area and sought in the Historic District and Downtown Design Standards.

The Planning Commission finds that public facilities and utilities were installed with the subdivision's infrastructure following the Planning Commission's approval of the project in 2012. These included:

- Some electric infrastructure was extended in association with the 2007 Subdivision approval, and the 2012 subdivision infrastructure work and subsequent development of Plaza West completed the installation of transformers necessary to serve the site and first building. Three-phase electrical service is available to the site, and the Electric Department has previously indicated that there is adequate power available to serve the full development of the property with the extension of the necessary individual services for each of the proposed buildings.
- Existing four-inch water mains are available in both Lithia Way and First Streets, and a new eight-inch water line was extended to provide a connection to B Street as part of the subdivision improvements in 2007. Four-inch laterals were also extended to each of the individual lots with the 2007 subdivision work.
- A six-inch sewer line in First Street was upgraded to eight-inches to serve the project as part of the 2007 subdivision improvements.
- A private 12-inch storm drain line was installed on site, and a new 12-inch public storm drain line was installed in First Street to convey stormwater run-off from the site to the existing storm drain line at B and First Streets as part of subdivision improvements in 2007. With completion of the current request, 16.4 percent of the site is proposed to be landscaped, reducing run-off

from the site, which was until the 2007 subdivision improvements entirely covered with pavement and buildings, and a bio-swale is to be installed in the northeastern portion of the common area lot to allow for on-site detention and filtration of stormwater before it enters the city storm sewer system.

- Paved access is provided directly from First Street and to Pioneer Street via an easement through the existing City of Ashland public parking lot. With the subdivision's initial site work in 2007, two curb cuts were removed from Lithia Way and one from First Street in order to comply with city and state requirements for controlled access.
- As part of the 2007 subdivision improvements, the existing public sidewalks along the project perimeter on both Lithia Way and First Street were widened. The installation of street trees, tree grates, irrigation and streetlights complying with downtown streetlight spacing requirements were completed in conjunction with Phase One.
- There is an existing transit stop located along Lithia Way nearby, between First and Second Streets, a short walk from the subject property.

The Planning Commission finds that water, sewer, paved access to and through the development site, electricity, urban storm drainage and adequate transportation to and through the subject property can and will be provided, with site utilities completed with the subdivision infrastructure and individual services to the proposed buildings to be completed under the current request; vehicular access provided from existing fully-improved streets; sidewalks which are to be widened to meet current street standards along Lithia Way; and easements which were provided with the subdivision to increase vehicular and pedestrian connectivity to and through the site. Conditions have been added below to require final electrical service and utility plans for the proposed building for the review and approval of the Planning, Building, Public Works and Electric Departments in conjunction with building permit review.

2.5 The proposed Plaza North building, as the applicants Phase Three, includes two requests for Exceptions to the Site Development and Design Standards' Downtown Design Standards. One of these would allow for a staggered street setback of the building relative to the First Street sidewalk, and the other would allow upper floor windows that are more horizontal than vertical. AMC 18.4.2.060.C.2.a states that, "*Except for arcades, alcoves and other recessed features, buildings shall maintain a zero setback from the sidewalk or property line... Areas having public utility easements or similar restricting conditions shall be exempt from this standard.*" The proposed Plaza North building steps back from the sidewalk's edge, and the application explains that this is due to the property, which is otherwise rectangular, having an acute angle with First Street. The applicants go on to explain that this provided the opportunity to create a stepped façade that works better with the lot shape and which creates a more interesting streetscape. The design places the corners of each step at or near the First Street sidewalk, steps back a prescribed distance, and then steps again at the point the front wall intersects with First Street sidewalk. The applicant asserts that this stepping of the façade eases the transition from the commercial zone to the residential zone, and further explains that the alternative, a flat façade angled with the street, would be contrary to the character of the downtown. The applicant emphasizes that the design here is based on a traditional building setting in the downtown rather than the shape of the lot. Entrances are recessed in keeping with the standards (AMC 18.4.2.060.C.2.b) to emphasize their respective

locations, and landscaping will be provided in the stepped back areas. The applicants argue that the standard, and the majority of buildings in the downtown, are based on a 90 degree frontage and that the proposed design is an attempt to create a more traditional frontage treatment in response to what is roughly a 120 degree angled frontage, and which they believe is consistent with the intent of the standard in seeking to create an engaging pedestrian streetscape. The Planning Commission finds that the building design creating the stepping back of the façade as proposed addresses the unusual triangular configuration of the site in a manner that will not negatively impact adjacent properties, and results in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

The application also requests Exception from AMC 18.4.2.060.C.4.c, which states that “*Upper floor window orientation shall primarily be vertical (height greater than width).*” The applicant asserts that placing two or three vertical windows grouped in a single bay compliments the building’s architecture, which is divided into vertical bays that step down as the building gets closer to the residential zone to the north, and helps to maintain the rhythm of openings sought in the standards. The applicant further suggests that the intent of the standard was to ensure that individual windows were more vertical. The application includes photos to illustrate similar groupings of vertical windows on historic buildings within the downtown.

The Commission concurs, and finds that these windows do not overwhelm the character of the façade. AMC Figure 18.4.2.060.C.1 and C.6, referenced as recommended treatments in the standard, both illustrate similar groupings of vertical windows. The Commission finds that the horizontal groupings of vertical windows as proposed results in a design that equally or better achieves the stated purpose of the Historic District Design Standards. The Commission further finds that the efforts to step the building’s façade back with the angled frontage while stepping the building’s vertical mass down as it gets nearer to the adjacent residential neighborhood provide an appropriate and effective transition that will not negatively impact adjacent properties, and results in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review approval to construct the third and final phase of the First Place Subdivision and for Exceptions to the Site Design and Use Standards’ Downtown Design Standards are supported by evidence contained within the whole record.

The project poses a number of challenges in that it involves developing an entire block of the downtown under the same set of standards while maintaining contextual compatibility with a downtown that has developed and evolved organically over more than 150 years, and doing so while managing the transition between the intensity of the downtown core and an historic residential neighborhood literally just over the fence.

During the 2012 review of the original Subdivision application and request for Site Review approval for Plaza West, it was noted that the project could result in a large, prominent downtown site which had stalled with the economy and languished for years developing to a degree beyond that required by city standards while providing increased vehicular and pedestrian connectivity, aesthetic improvements, and a significant reduction in stormwater run-off. It was further suggested that the first proposed building, with ten residential units including one affordable unit, could inject a new vitality into the Lithia Way corridor while at the same time the subdivision could provide for a smooth transition between the intense commercial uses of the downtown and the less intense, residential character of the adjacent neighborhood. With the Plaza West now occupied, Plaza East under construction with workforce housing for Oregon Shakespeare Festival's artists, and the proposed completion here of Plaza North as the final phase to complete development of the site, the Commission finds that the applicants have effectively met the challenges posed in designing buildings that, while compatible with one another, their surroundings and various design standards, still manage to maintain strong individual characters that contribute positively to the streetscape and the downtown. Plaza North manages to balance this design compatibility with the buildings along Lithia Way while stepping back with the angle of First Street and down with its slope to blend the entire project smoothly into the Railroad District.

The site layout, parking, pedestrian and vehicular access and circulation, and landscaping were largely completed with the 2012 Subdivision and Site Review approvals, and the current review focuses on the design of Plaza North relative to the applicable design standards. For the Commission, the proposed Plaza North building can be found to satisfy the relevant approval criteria for Site Review and Exception to the Downtown Design Standards and merits approval.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve PA-T2-2020-00019. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then PA-T2-2020-00019 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant are conditions of approval unless otherwise modified herein.
2. The windows on the ground floor of the proposed building shall not be tinted so as to prevent views from outside of the building into the interior of the building, and the commercial entrances adjacent to First Street shall remain functional and open to the public during all business hours.
3. That prior to site work, storage of materials or building permit issuance, tree protection measures shall be installed, inspected and approved on site by the Staff Advisor through a Tree Verification Permit.
4. That any necessary construction closure or detouring of the sidewalks shall be approved by the Ashland Engineering and Planning Departments prior to issuance of permits or work in the right-of-way.
5. That a Fire Prevention and Control Plan addressing the General Fuel Modification Area

requirements in AMC 18.3.10.100.A.2 of the Ashland Land Use Ordinance shall be provided prior to bringing combustible materials onto the property, and any new landscaping proposed shall comply with these standards and shall not include plants listed on the Prohibited Flammable Plant List per Resolution 2018-028.

6. To obtain priority building permit plan check processing as provided in AMC 15.04.092.2, the applicant shall provide the following documentation with the building permit submittals demonstrating the steps being taken in working towards LEED certification: a) hiring and retaining a LEED Accredited Professional (AP) as part of the project team throughout design and construction of the project; and b) providing the LEED checklist indicating the credits that will be pursued. Building permit submittals must clearly specify the materials, systems and strategies to be used in achieving the credits. A final report shall be prepared by the LEED AP and presented to the City upon completion of the project verifying that the project has met the LEED standard.
7. Sign permits shall be obtained prior to installation of any new signage. All signage shall be consistent with the requirements of AMC 18.4.7.
8. That prior to the issuance of a building permit:
 - a) The plans submitted for the building permit shall be in conformance with those approved here. If the plans submitted for the building permit are not in conformance with those approved as part of this application, an application to modify this Site Review approval shall be submitted and approved prior to issuance of a building permit.
 - b) All easements shall be shown on the building permit submittals.
 - c) That a final drainage plan shall be submitted at the time of a building permit for review and approval by the Engineering, Building, and Planning Divisions. Storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.
 - d) A final utility plan for the project shall be reviewed and approved by the Planning, Engineering, and Building Divisions prior to issuance of a building permit. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins.
 - e) The applicant shall submit a final electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment for each building. This plan must be reviewed and approved by the Electric Department prior to building permit submittals. Transformers, cabinets and vaults shall be located in areas least

- visible from the street, while considering the access needs of the Electric Department.
- f) That mechanical equipment shall be screened from view from First Street. The location and screening of mechanical equipment shall be detailed on the building permit submittals.
 - g) Exterior lighting shall be shown on the building permit submittals and appropriately shrouded so there is no direct illumination of surrounding properties.
 - h) That the building materials and the exterior colors shall be identified in the building permit submittals. The information shall be consistent with the colors, texture, dimensions and shape of materials and building details proposed and approved as part of this land use application. Exterior building colors shall be muted colors, as described in the application. Bright or neon paint colors shall not be used in accordance with AMC 18.4.2.040.C.4.b in the Detail Site Review Standards.
 - i) Building permit submittals shall identify all required bicycle parking installations. Inverted u-racks shall be used for the bicycle parking, and the building permit submittals shall verify that the bicycle parking spacing and coverage requirements are met in accordance with AMC 18.4.3.070.I & .J. A total of at least 11 bicycle parking spaces shall be provided on the common area of the project, and at least six of these must be covered. An additional four spaces of covered bicycle parking shall be identified and provided to serve the three residential units in Plaza North that do not have individual garages.
 - j) Final solar setback calculations demonstrating that all new construction complies with Solar Setback Standard B in the formula $[(\text{Height} - 16) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$ and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and the height(s) from natural grade shall be included in building permit submittals.
 - k) Prior to any work within the public rights-of-way, all necessary permits must be obtained from the Public Works/Engineering Department. Prior to the issuance of permits or commencement of any site work in the Oregon Department of Transportation's (ODOT) right-of-way for Lithia Way, the applicant shall provide proof of also having obtained required approvals and permits from ODOT. The applicants shall maintain a vision clearance triangle that complies with ODOT and City of Ashland standards.
 - l) Revised parking allocation information shall be provided with each building permit as the commercial tenant spaces develop and are occupied to verify that the parking allocated is sufficient for the uses proposed.
- 7) That prior to the issuance of a certificate of occupancy:
- a) All landscaping and irrigation shall be installed according to the approved plans, inspected and approved by the Staff Advisor. Landscaping and hardscaping in the

common area north of Plaza North which has not been maintained or has been damaged shall be replaced according to the approved plans, and the northernmost street tree on First Street shall be replaced prior to issuance of the certificate of occupancy.

- b) All bicycle parking shall be installed in accordance with design, placement, coverage and rack standards in AMC 18.4.3.070.I & .J prior to the issuance of the certificate of occupancy. A total of at least 11 bicycle parking spaces are to be provided on the common area of the project, and at least six of these must be covered. An additional four covered bicycle parking spaces shall be provided for the three units in Plaza North which do not have individual garages.
- c) An opportunity to recycle site of equal or greater size than the solid waste receptacle for each building shall be included in the trash enclosure in accordance with the Recycling Requirements of AMC 18.4.4.040.
- d) That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
- e) The requirements of the Building Department shall be satisfactorily addressed, including but not limited to that the mixed-use occupancy is required to be fire sprinkled, that construction may not cross property lines, and that the site and structures are required to meet all accessibility requirements.
- f) The requirements of the Fire Department shall be satisfactorily addressed, including approved addressing; fire apparatus access including angle of approach, necessary easements, and review of any obstructions such as fences or gates; fire flow; fire hydrant distance, spacing, flow and clearance; fire department connection; and a “Knox Box” key box. Fire Department requirements shall be included on the construction documents, and if a fire protection vault is required, the vault shall not be located in the sidewalk corridor.

Planning Commission Approval

July 14, 2020

Date

**TYPE II
PUBLIC HEARING**

**PA-T2-2020-00020
705 Helman Street**



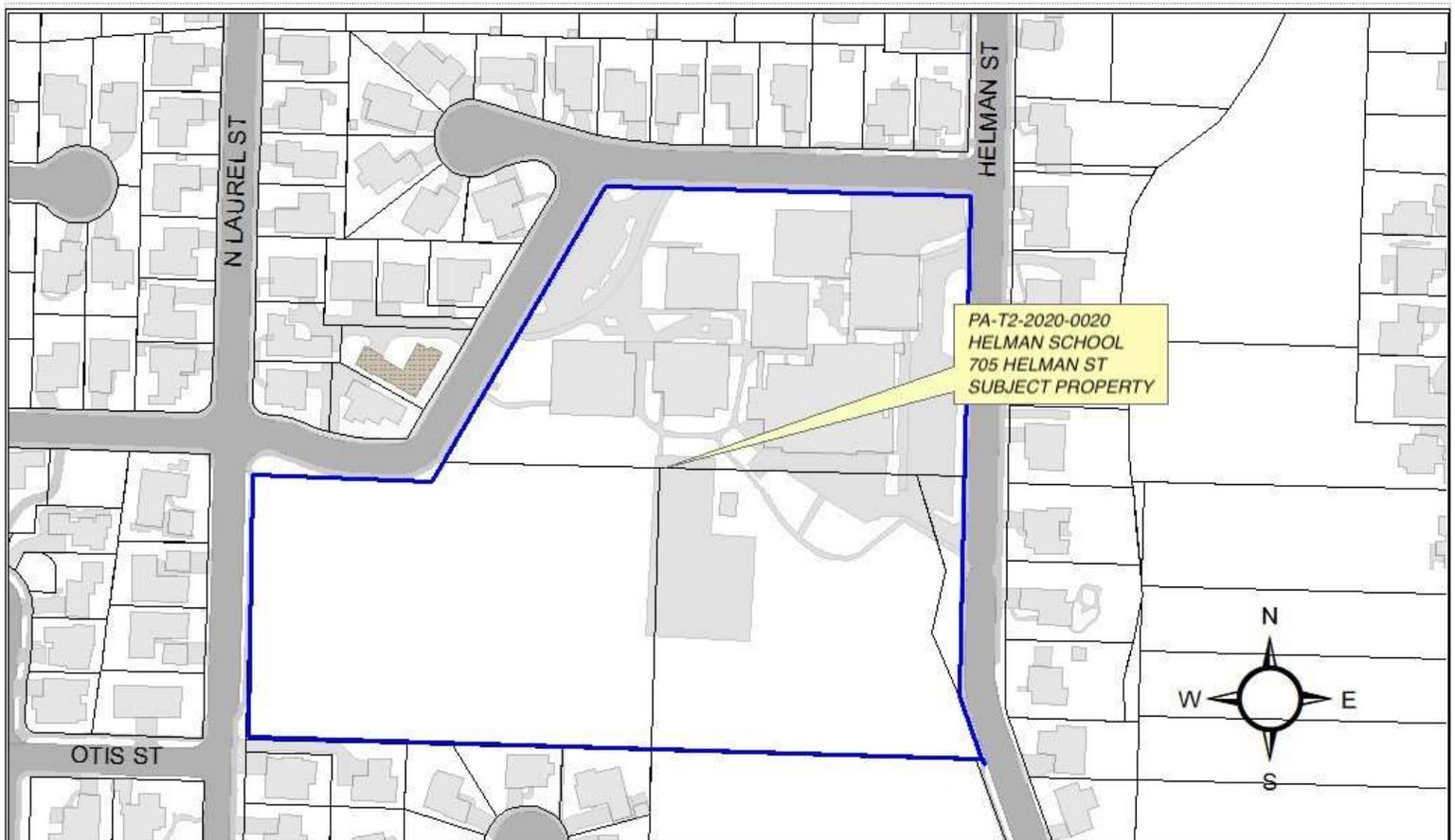
PLANNING ACTION: PA-T2-2020-00020

SUBJECT PROPERTY: 705 Helman Street (Helman Elementary School)

APPLICANT/OWNER: HMK Company/Ashland School District #5

DESCRIPTION: A request for Site Design Review and Conditional Use Permit approvals to allow the construction of a new 23,755 square foot, single-story school building for the Helman Elementary School property at 705 Helman Street. The requested Conditional Use Permit is to allow the expansion of an existing non-conforming development pattern where both the existing and proposed new parking and circulation are located between the buildings and the street, and for the on-site relocation of previously approved signage. The proposal includes the demolition of two existing classroom buildings (the A & B quad's) and requests a Tree Removal Permit to remove 12 significant trees. **COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-5; MAP: 39 1E 04 BD; TAX LOTS: 600, 2700, 2800 & 2900.**

ELECTRONIC ASHLAND PLANNING COMMISSION MEETING: *Tuesday, July 14, 2020 at 7:00 PM*



Notice is hereby given that the Ashland Planning Commission will hold an electronic public hearing on the above described planning action on the meeting date and time shown above. You can watch the meeting on local channel 9, on Charter Communications channels 180 & 181, or you can stream the meeting via the internet by going to rvtv.sou.edu and selecting 'RVTV Prime.'

The ordinance criteria applicable to this planning action are attached to this notice. Oregon law states that failure to raise an objection concerning this application, or failure to provide sufficient specificity to afford the decision makers an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

Because of the COVID-19 pandemic, application materials are provided online and written comments will be accepted by email. Alternative arrangements for reviewing the application or submitting comments can be made by contacting (541) 488-5305 or planning@ashland.or.us.

A copy of the application, including all documents, evidence and applicable criteria relied upon by the applicant, and a copy of the staff report will be available on-line at www.ashland.or.us/PCpackets seven days prior to the hearing. Copies of application materials will be provided at reasonable cost, if requested. Under extenuating circumstances, application materials may be requested to be reviewed in-person at the Ashland Community Development & Engineering Services Building, 51 Winburn Way, via a pre-arranged appointment by calling (541) 488-5305 or emailing planning@ashland.or.us.

Anyone wishing to submit comments can do so by sending an e-mail to PC-public-testimony@ashland.or.us with the subject line "**July 14 PC Hearing Testimony**" by 10:00 a.m. on Monday, July 13, 2020. If the applicant wishes to provide a rebuttal to the testimony, they can submit the rebuttal via e-mail to PC-public-testimony@ashland.or.us with the subject line "**July 14 PC Hearing Testimony**" by 10:00 a.m. on Tuesday, **July 14, 2020**. Written testimony received by these deadlines will be available for Planning Commissioners to review before the hearing and will be included in the meeting minutes.

Oral testimony will be taken during the electronic public hearing. If you wish to provide oral testimony during the electronic meeting, send an email to PC-public-testimony@ashland.or.us by 10:00 a.m. on Tuesday, July 14, 2020. In order to provide testimony at the public hearing, please provide the following information: 1) make the subject line of the email "**July 14 Speaker Request**", 2) include your name, 3) the agenda item on which you wish to speak on, 4) specify if you will be participating by computer or telephone, and 5) the name you will use if participating by computer or the telephone number you will use if participating by telephone.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact Derek Severson at #541-552-2040 or Derek.Severson@ashland.or.us.

SITE DESIGN AND USE STANDARDS

18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. Exception to the Site Development and Design Standards: The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum

which would alleviate the difficulty.; or

2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

CONDITIONAL USE PERMITS

18.5.4.050.A

A Conditional Use Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.
3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.
 - a. Similarity in scale, bulk, and coverage.
 - b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
 - c. Architectural compatibility with the impact area.
 - d. Air quality, including the generation of dust, odors, or other environmental pollutants.
 - e. Generation of noise, light, and glare.
 - f. The development of adjacent properties as envisioned in the Comprehensive Plan.
 - g. Other factors found to be relevant by the approval authority for review of the proposed use.
4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.
5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.
 - a. WR and RR. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - b. R-1. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - d. C-1. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
 - e. C-1-D. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 1.00 gross floor to area ratio, complying with all ordinance requirements.
 - f. E-1. The general office uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
 - g. M-1. The general light industrial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, complying with all ordinance requirements.
 - h. CM-C1. The general light industrial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
 - i. CM-OE and CM-MU. The general office uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area, complying with all ordinance requirements.
 - k. CM-NC. The retail commercial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area ratio, complying with all ordinance requirements.
 - l. HC, NM, and SOU. The permitted uses listed in chapters 18.3.3 Health Care Services, 18.3.5 North Mountain Neighborhood, and 18.3.6 Southern Oregon University District, respectively, complying with all ordinance requirements.

TREE REMOVAL PERMIT

18.5.7.040.B

1. Hazard Tree. A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
 - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. Tree That is Not a Hazard. A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
 - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or

existing windbreaks.

- c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
- d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
- e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

APPROVAL CRITERIA FOR GOVERNMENTAL SIGNS

18.4.7.120

Governmental agencies may apply for a Conditional Use Permit to place a sign that does not conform to this chapter when it is determined that, in addition to meeting the criteria for a conditional use, the sign is necessary to further that agency's public purpose.

DEMOLITION AND RELOCATION STANDARDS

15.04.216

- A. For demolition or relocation of structures erected more than 45 years prior to the date of the application:
 1. The applicant must demonstrate that either subparagraphs a or b apply:
 - a. The structure cannot be rehabilitated or reused on site as part of any economically beneficial use of the property. In determining whether an economically beneficial use can be made of the property, the Demolition Review committee may require the applicant to:
 - (i) Furnish an economic feasibility report prepared by an architect, developer, or appraiser, or other person who is experienced in rehabilitation of buildings that addresses the estimated market value of the property on which the building lies, both before and after demolition or removal, or
 - (ii) Market the property utilizing a marketing plan approved by the Demolition Review Committee or by advertising the property in the Ashland Daily Tidings and Medford Mail Tribune at least eight times and at regular intervals for at least 90 days and by posting a for sale sign on the property, four to six square feet in size and clearly visible from the street, for the same 90 day period.
 - b. The structure proposed for demolition is structurally unsound despite efforts by the owner to properly maintain the structure.
 2. In addition to subparagraphs a or b above, the applicant must also:
 - a. Submit a redevelopment plan for the site that provides for replacement or rebuilt structure for the structure being demolished or relocated. The replacement or rebuilt structure must be a minimum of 1,000 square feet, unless the structure being demolished or relocated is less than 1,000 square feet. If the structure is less than 1,000 square feet, the replacement structure must be a minimum of 500 square feet. The redevelopment plan must indicate in sufficient detail the nature, appearance and location of all replacement or rebuilt structures. No replacement structure is required, however, if:
 - (i) the applicant agrees to restrict the property to open space uses and a finding is made that such restriction constitutes a greater benefit to the neighborhood than redevelopment would, or
 - (ii) the structure being demolished or relocated is a nonhabitable accessory structure.
 - b. Demonstrate, if the application is for a demolition, the structure cannot be practicably relocated to another site.
 3. If a permit is issued and the redevelopment plan:
 - a. Requires a site review permit, no demolition or relocation may occur until the site review permit has been issued, unless the site is restricted to open space uses as provided in section 15.04.216.A.2.
 - b. Does not require a site review permit, no demolition or relocation may occur until the building permit has been issued for the replacement or rebuilt structure, unless the site is restricted to open spaces uses as provided in section 15.04.216.A.2.
 4. The Demolition Review Committee may require the applicant to post with the City a bond, or other suitable collateral as determined by the City administrator, ensuring the safe demolition of the structure and the completed performance of the redevelopment plan.
- B. For demolition or relocation of structures erected less than 45 years from the date of the application:
 1. The applicant:
 - a. Has the burden of proving the structure was erected less than 45 years from the date of the application. Any structure erected less than 45 years from the date of the application, which replaced a structure demolished or relocated under section 15.04.216, shall be considered a structure subject to the standards in subsections 15.04.216.
 - b. Must submit a redevelopment plan for the site that provides for a replacement or rebuilt structure being demolished or relocated. The replacement or rebuilt structure must be a minimum of 1,000 square feet, unless the structure being demolished or relocated is less than 1,000 square feet. If the structure is less than 1,000 square feet, the replacement structure must be a minimum of 500 square feet. The redevelopment plan must indicate in sufficient detail the nature, appearance and location of all replacement or rebuilt structures. No replacement structure is required, however, if:
 - (i) the applicant agrees to restrict the property to open space uses and a finding is made that such restriction constitutes a greater benefit to the neighborhood than redevelopment would, or
 - (ii) the structure being demolished or relocated is a nonhabitably accessory structure.
 2. If a permit is issued and the redevelopment plan:
 - a. Requires a site review permit, no demolition or relocation may occur until the site review permit has been issued, unless the site is restricted to open space uses as provided in section 15.04.216.B.
 - b. Does not require a site review permit, no demolition or relocation may occur until a building permit has been issued for the structure or structures to be replaced or rebuilt, unless the site is restricted to open space uses as provided in section 15.04.216.B.

- C. For any demolition approved under this section, the applicant is required to salvage or recycle construction and demolition debris, in accordance with a demolition debris diversion plan that complies with the requirements adopted the Demolition Review Committee. The applicant shall submit such a plan with the application for demolition.

For any relocation approved under this section, the applicant must also comply with the provisions of Chapter 15.08. (Ord. 2925, amended, 04/18/2006; Ord. 2891, amended, 11/19/2002; Ord. 2858, amended, 06/20/2000; Ord. 2852, added, 01/21/2000)

Helman Elementary School

PA-T2-2020-00020 – A request for Site Review & Conditional Use permits for construction of a new 23,755 square foot, single-story classroom building for Helman Elementary. The Conditional Use Permit is to allow expansion of an existing non-conforming development pattern where the existing and proposed parking and circulation are located between buildings and the street, and for the relocation of the dragon tile mural. The proposal includes demolition of two existing classroom buildings (“A Quad” and “B Quad”) and requests Tree Removal Permits to remove 12 significant trees.



Proposal Details

Proposal

The proposal would demolish the A & B quad buildings near the corner of Helman and Randy and remove a total of 12 significant trees to construct a new single-story classroom building more central to the property. A new main entry plaza would be created near the corner, a total of 17 parking spaces added, and circulation configured to better accommodate pick-up and drop-off traffic on site. The previously approved dragon tile mural would be located to a new wall facing Helman Street.

Site Description

The subject property includes four lots totaling approximately 9½-acres, and is zoned Single Family Residential (R-1-5). Campus buildings including 16 classrooms in four quad buildings; a central building housing administrative offices, cafeteria, multi-purpose room and a music room; and parking are clustered on the northern portion of the site, while a playground, tennis courts, a basketball court and large field are to the south. The school opened in 1966 with additions in the 1970's and in 2008, and currently has an enrollment capacity of 350 students.

Landscaping & Trees

The application identifies 106 trees on campus. With the redevelopment of the property proposed, 12 significant trees are to be removed and more than 50 new trees are to be planted.

Helman Elementary School

PA-T2-2020-00020 – A request for Site Review & Conditional Use permits for construction of a new 23,755 square foot, single-story classroom building for Helman Elementary. The Conditional Use Permit is to allow expansion of the existing non-conforming development pattern where the existing and proposed parking and circulation are located between buildings and the street, and for the relocation of the dragon tile mural. The proposal includes demolition of two existing classroom buildings (“A Quad” and “B Quad”) and requests Tree Removal Permits to remove 12 significant trees.

Key Issues

Parking & Circulation

17 new parking spaces are proposed to be constructed in a new lot on the southeastern corner of the site along Helman Street. The applicant proposes to accommodate all 63 required parking spaces on site, and to reconfigure site circulation to better accommodate pick-up and drop-off queuing on the site to limit impacts to the surrounding streets. A driveway at the corner of Helman and Randy is being closed, and new landscape bio-swales installed to address stormwater detention and water quality requirements. New covered bicycle parking is also proposed to be added.

The existing parking between buildings and the street is not consistent with Site Design standards, and expanding this non-conforming parking placement is subject to Conditional Use Permit approval. Staff believe that the existing site lay-out and school use pose difficulties in complying with the standard, and with the new entry plaza and removal of a driveway near the corner, the campus orientation and pedestrian safety are improved while also benefitting campus security by providing controlled access into the campus.

Demolition

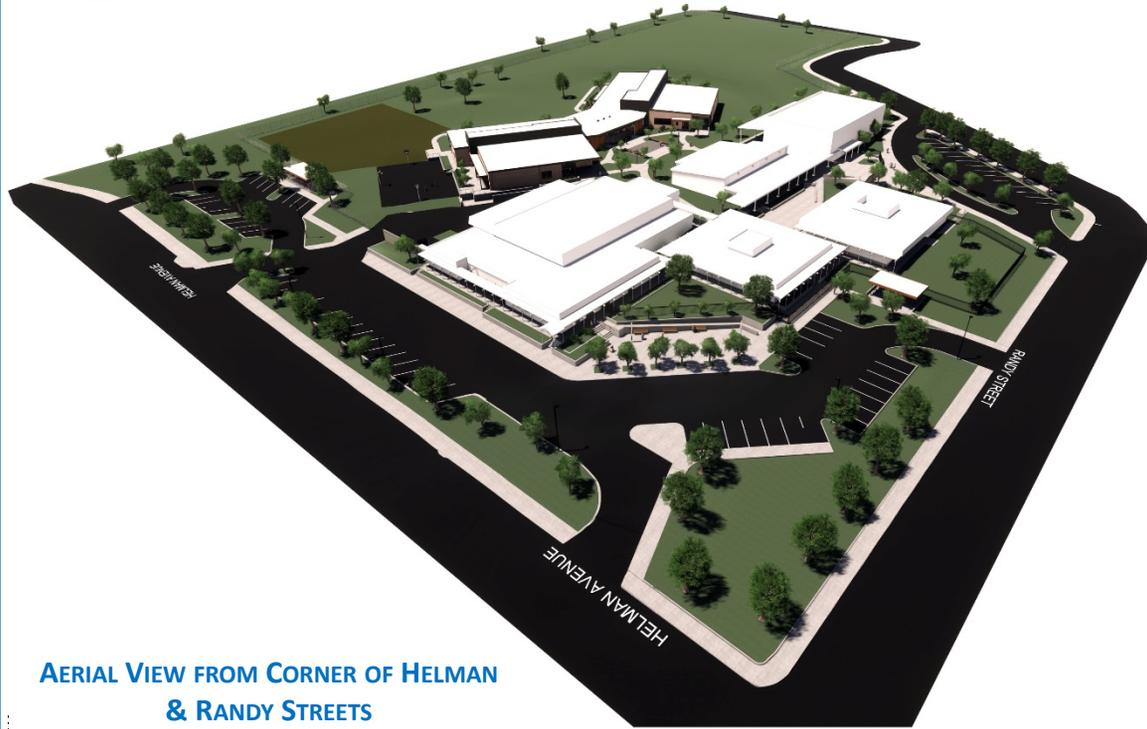
The A and B quad buildings near the corner of Helman and Randy Streets are to be demolished, and a new main entry plaza constructed.

Staff Recommendation

Staff recommends that the application be approved with the conditions detailed in the draft findings included in the Planning Commission’s July 2020 meeting packet.



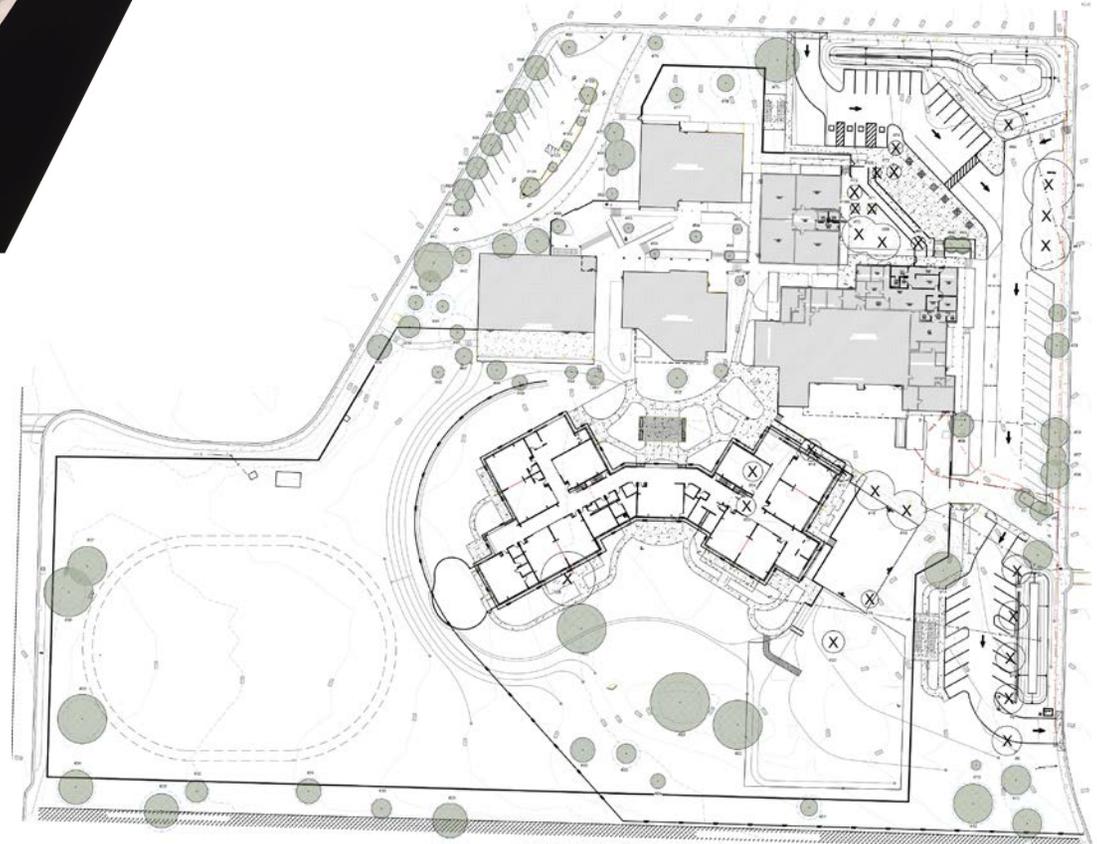
Helman Elementary School



AERIAL VIEW FROM CORNER OF HELMAN
& RANDY STREETS



Proposed Site Plan



NORTH ELEVATION FROM NORTH COURTYARD

DRAFT - BEFORE THE PLANNING COMMISSION - DRAFT

August 11, 2020

IN THE MATTER OF PLANNING ACTION #PA-T2-2020-00020, A REQUEST FOR)
SITE DESIGN REVIEW AND CONDITIONAL USE PERMIT APPROVALS TO)
ALLOW THE CONSTRUCTION OF A NEW 23,755 SQUARE FOOT, SINGLE-STORY)
CLASSROOM BUILDING FOR THE HELMAN ELEMENTARY SCHOOL PROPERTY))
AT 705 HELMAN STREET. THE REQUESTED CONDITIONAL USE PERMIT IS TO)
ALLOW EXPANSION OF AN EXISTING NON-CONFORMING DEVELOPMENT)
PATTERN WHERE BOTH EXISTING AND PROPOSED PARKING AND CIRCUL-)
ATION ARE LOCATED BETWEEN BUILDINGS AND THE STREET, AND FOR)
THE RELOCATION OF PREVIOUSLY APPROVED SIGNS. THE PROPOSAL IN-)
CLUDES THE DEMOLITION OF TWO EXISTING CLASSROOM BUILDINGS – THE)
A AND B QUADS - AND REQUESTS A TREE REMOVAL PERMIT TO REMOVE 12)
SIGNIFICANT TREES.)

**FINDINGS,
CONCLUSIONS &
ORDERS**

OWNER/APPLICANT: HMK COMPANY/ASHLAND SCHOOLD DIST. #5)
)
)

RECITALS:

- 1) Tax lots 600, 2700, 2800 & 2900 of Map 39 1E 04BD comprise the Helman Elementary School campus located at 705 Helman Street and are zoned Single Family Residential (R-1-5).
- 2) The applicant is requesting Site Design Review and Conditional Use Permit approvals to allow the construction of a new 23,755 square foot, single-story school building for the Helman Elementary School property at 705 Helman Street. The requested Conditional Use Permit is to allow the expansion of an existing non-conforming development pattern where both the existing and proposed new parking and circulation are located between the buildings and the street, and for the on-site relocation of a previously approved signage. The proposal includes the demolition of two existing classroom buildings (“A Quad” and “B Quad”) and requests a Tree Removal Permit to remove 12 significant trees. The proposal is outlined in plans on file at the Department of Community Development.
- 3) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:
 - A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
 - B. **Overlay Zones:** *The proposal complies with applicable overlay zone requirements (part 18.3).*
 - C. **Site Development and Design Standards:** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
 - D. **City Facilities:** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*

E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.;* or
2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*

4) The approval criteria for a Conditional Use Permit are detailed in **AMC 18.5.4.050.A** as follows:

1. *That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.*
2. *That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.*
3. *That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.*
 - a. *Similarity in scale, bulk, and coverage.*
 - b. *Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.*
 - c. *Architectural compatibility with the impact area.*
 - d. *Air quality, including the generation of dust, odors, or other environmental pollutants.*
 - e. *Generation of noise, light, and glare.*
 - f. *The development of adjacent properties as envisioned in the Comprehensive Plan.*
 - g. *Other factors found to be relevant by the approval authority for review of the proposed use.*
4. *A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.*
5. *For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.*
 - b. **R-1.** *Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.*

5) The approval criteria for a Tree Removal Permit are described in **AMC 18.5.7.040.B** as follows:

1. **Hazard Tree.** *A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
 - a. *The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.*
 - b. *The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*
2. **Tree That is Not a Hazard.** *A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
 - a. *The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.*
 - b. *Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*
 - c. *Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
 - d. *Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
 - e. *The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

6) The Demolition and Relocation Standards are described in detail in **AMC 15.04.216** as follows:

- A. *For demolition or relocation of structures erected more than 45 years prior to the date of the application:*
 1. *The applicant must demonstrate that either subparagraphs a or b apply:*
 - a. *The structure cannot be rehabilitated or reused on site as part of any economically beneficial use of the property. In determining whether an economically beneficial use can be made of the property, the Demolition Review committee may require the applicant to:*
 - (i) *Furnish an economic feasibility report prepared by an architect, developer, or appraiser, or other person who is experienced in*

- rehabilitation of buildings that addresses the estimated market value of the property on which the building lies, both before and after demolition or removal, or*
- (ii) Market the property utilizing a marketing plan approved by the Demolition Review Committee or by advertising the property in the Ashland Daily Tidings and Medford Mail Tribune at least eight times and at regular intervals for at least 90 days and by posting a for sale sign on the property, four to six square feet in size and clearly visible from the street, for the same 90 day period.*
- b. The structure proposed for demolition is structurally unsound despite efforts by the owner to properly maintain the structure.*
- 2. In addition to subparagraphs a or b above, the applicant must also:*
- a. Submit a redevelopment plan for the site that provides for replacement or rebuilt structure for the structure being demolished or relocated. The replacement or rebuilt structure must be a minimum of 1,000 square feet, unless the structure being demolished or relocated is less than 1,000 square feet. If the structure is less than 1,000 square feet, the replacement structure must be a minimum of 500 square feet. The redevelopment plan must indicate in sufficient detail the nature, appearance and location of all replacement or rebuilt structures. No replacement structure is required, however, if:
 - (i) the applicant agrees to restrict the property to open space uses and a finding is made that such restriction constitutes a greater benefit to the neighborhood than redevelopment would, or*
 - (ii) the structure being demolished or relocated is a nonhabitable accessory structure.**
 - b. Demonstrate, if the application is for a demolition, the structure cannot be practicably relocated to another site.*
- 3. If a permit is issued and the redevelopment plan:*
- a. Requires a site review permit, no demolition or relocation may occur until the site review permit has been issued, unless the site is restricted to open space uses as provided in section 15.04.216.A.2.*
 - b. Does not require a site review permit, no demolition or relocation may occur until the building permit has been issued for the replacement or rebuilt structure, unless the site is restricted to open spaces uses as provided in section 15.04.216.A.2.*
- 4. The Demolition Review Committee may require the applicant to post with the City a bond, or other suitable collateral as determined by the City administrator, ensuring the safe demolition of the structure and the completed performance of the redevelopment plan.*

B. *For demolition or relocation of structures erected less than 45 years from the date of the application:*

1. *The applicant:*

a. *Has the burden of proving the structure was erected less than 45 years from the date of the application. Any structure erected less than 45 years from the date of the application, which replaced a structure demolished or relocated under section 15.04.216, shall be considered a structure subject to the standards in subsections 15.04.216.*

b. *Must submit a redevelopment plan for the site that provides for a replacement or rebuilt structure being demolished or relocated. The replacement or rebuilt structure must be a minimum of 1,000 square feet, unless the structure being demolished or relocated is less than 1,000 square feet. If the structure is less than 1,000 square feet, the replacement structure must be a minimum of 500 square feet. The redevelopment plan must indicate in sufficient detail the nature, appearance and location of all replacement or rebuilt structures. No replacement structure is required, however, if:*

(i) *the applicant agrees to restrict the property to open space uses and a finding is made that such restriction constitutes a greater benefit to the neighborhood than redevelopment would, or*

(ii) *the structure being demolished or relocated is a nonhabitable accessory structure.*

2. *If a permit is issued and the redevelopment plan:*

a. *Requires a site review permit, no demolition or relocation may occur until the site review permit has been issued, unless the site is restricted to open space uses as provided in section 15.04.216.B.*

b. *Does not require a site review permit, no demolition or relocation may occur until a building permit has been issued for the structure or structures to be replaced or rebuilt, unless the site is restricted to open space uses as provided in section 15.04.216.B.*

C. *For any demolition approved under this section, the applicant is required to salvage or recycle construction and demolition debris, in accordance with a demolition debris diversion plan that complies with the requirements adopted the Demolition Review Committee. The applicant shall submit such a plan with the application for demolition.*

For any relocation approved under this section, the applicant must also comply with the provisions of Chapter 15.08. (Ord. 2925, amended, 04/18/2006; Ord. 2891, amended, 11/19/2002; Ord. 2858, amended, 06/20/2000; Ord. 2852, added, 01/21/2000)

7) On April 15, 2020 Governor Kate Brown issued Executive Order #20-16 “*Keep Government Working: Ordering Necessary Measures to Ensure Safe Public Meetings and Continued Operations by Local Government During Coronavirus (COVID-19) Outbreak.*” The Governor’s Order required that public bodies hold public meetings by telephone, video, or through some other electronic or virtual means, whenever possible; that the public body make available a method by which the public can listen to or virtually attend the public meeting or hearing at the time it occurs; that the public body does not have to provide a physical space for the public to attend the meeting or hearing; that requirements that oral public testimony be taken during hearings be suspended, and that public bodies instead provide a means for submitting written testimony by e-mail or other electronic methods that the public body can consider in a timely manner. The Oregon Legislature subsequently passed House Bill #4212 which authorizes local governments to hold all meetings of their governing bodies, including taking public testimony, using telephone or video conferencing technology or through other electronic or virtual means provided that they supply a means by which the public can listen to or observe the meeting. This bill requires that recordings of the meetings be made available to the public if technology allows, and includes provisions similar to the Governor’s order allowing public testimony to be taken in writing via e-mail or other electronic means.

8) The Planning Commission, following proper public notice, held an electronic public hearing on July 14, 2020. In keeping with Executive Order #20-16, this meeting was broadcast live on local television channel 9 and on Charter Communications channels 180 & 181, and was live-streamed over the internet on RVTV Prime at rvtv.sou.edu.

The application, including all documents, evidence and applicable criteria relied upon by the applicant, and the staff report were made available on-line seven days prior to the hearing, with in-person review by appointment, and printed copies available at a reasonable cost. Those wishing to provide testimony were invited to submit written comments via e-mail by 10:00 a.m. on Monday, July 13, 2020, and the applicant was able to provide written rebuttal to this testimony by 10:00 a.m. on Tuesday, July 14, 2020. Comments and rebuttal received were made available on-line and e-mailed to Planning Commissioners before the hearing and included in the meeting minutes. As provided in the Governor’s Executive Order #20-16, testimony was also taken electronically during the tele-conferenced meeting from those members of the public who had pre-arranged to provide oral testimony by 10:00 a.m. on Tuesday, July 14, 2020.

After the closing of the hearing and the record, the Planning Commission deliberated and approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the index of exhibits, data, and testimony below will be used:

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the staff report, written public testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Site Design Review approval, Conditional Use Permit, and Tree Removal Permit meets all applicable criteria for Site Design Review described in AMC 18.5.2.050; for Conditional Use Permit described in AMC 18.5.4.050; and for a Tree Removal Permit described in AMC 18.5.7.040.B.

2.3 The Planning Commission concludes that the proposal satisfies all applicable criteria for Site Design Review approval.

The first approval criterion addresses the requirements of the underlying zone, requiring that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”* The Planning Commission finds that the building and yard setbacks and other applicable standards have been evaluated to ensure consistency with the applicable provisions of part 18.2, and all regulations of the underlying R-1-5 zoning will be satisfied.

The second approval criterion deals with overlay zones, and requires that, *“The proposal complies with applicable overlay zone requirements (part 18.3).”* The Planning Commission finds that the property is within the Performance Standards Option (PSO) overlay zone, which requires that all developments other than partitions or individual buildings be processed under Chapter 18.3.9., however the proposal here is limited to the development of school buildings on existing lots and does not require subdivision of the property.

The Planning Commission further finds that the subject property is located within the Wildfire Lands Overlay, and as such a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 will need to be provided for the review and approval of the Fire Marshal prior to bringing combustible materials onto the property. New landscaping proposed will need to comply with these standards and shall not include plants listed on the Prohibited Flammable Plant List per Resolution 2018-028. Conditions to this effect have been included below.

Based on the foregoing, the Planning Commission finds that this criterion is satisfied.

The third criterion addresses the Site Development and Design Standards, requiring that “The *proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*”

The Planning Commission finds that as proposed, the new classroom building being considered is being placed more than 100 feet from the sidewalk, and existing parking and circulation between the campus buildings and the street is being expanded through requests for Exceptions to the Site Development and Design Standards and a Conditional Use Permit discussed later in this section and in section 2.4. Parking areas are being shifted away from the street, on-site stormwater detention and new landscaping are being added, and controlled access standards better addressed with the removal of a driveway which currently exits into the crosswalk at the corner of Helman and Randy Streets.

Staff Discussion Items

For staff, the following items in part 18.4 merit Planning Commission consideration.

Parking & Circulation (AMC 18.4.3)

See the Staff Discussion of Parking in Section 2.4 below.

Bicycle Parking (AMC 18.4.3.070)

The application materials indicate that 70 covered bicycle parking spaces are required, based on the applicable ratio of one covered space for every five students and an enrollment capacity of 350 students. The application goes on to note that only 12 covered bicycle parking spaces are in place, and indicates that the applicant here would add a 20 stall bicycle parking structure on the north side of campus accessible from Randy Street and an additional 29 space structure west of the new parking lot along Helman. This would yield 61 covered bicycle spaces, or 87 percent of the 70 spaces required.

With the approval of the gym and library additions in Planning Action #2007-01756, it was noted that 66 bicycle parking spaces were required for the 330 student enrollment. At the time, there were 68 spaces already in place on campus in non-standard racks (*i.e. uncovered, wheel-bender racks*) and 12 new covered parking spaces were added adjacent to the new gym for a total of 80 bicycle parking spaces available on campus. In staff’s view if some or all of the previously approved bicycle parking has been removed in the interim, with the proposal here the applicant should provide the full 70 covered spaces required.

Pedestrian Access & Circulation (18.4.3.090)

The proposal includes the construction of a new security fence around the perimeter of the campus to limit access. Presently, there is a paved pedestrian access easement from the cul-de-sac on Parkside Drive, near 535 Parkside, to the south of campus which was required of the subdivision to enable students from the subdivision to the south to safely and efficiently access campus. Given that the Pedestrian Access and Circulation Standards in AMC 18.4.3.090.B.3.b call for providing pedestrian connections to off-site adjacent uses to the site to the extent practicable and there is already an improved easement in place to provide such a connection, staff believe that eliminating this access is a concern and that, at a minimum, there should be a gated neighborhood access point that can be unlocked and monitored during pick-up and drop-off periods to enable a safe and direct route to school. A condition

to this effect is included below (#7k).

The fourth approval criterion addresses city facilities, specifically requiring that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.”* The Planning Commission finds that adequate capacity of city facilities, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

The Commission notes that existing services are in place and currently serve the campus and its buildings. The applicant asserts that adequate city facilities exist to service the proposed new classroom building, and further indicates that the proposal substantially upgrades the storm drainage facilities, which are currently inadequate. The applicant emphasizes that the civil engineering plans (Sheets C2.1 Erosion Control Plan, C3.0 Overall Civil Site Plan, and C.4 Overall Grading and Drainage Plan) provide necessary details to demonstrate proposed site development and construction can comply with city standards. The applicant further details:

- **Water:** There is an existing six-inch water main in Helman Street, and a six-inch main in Randy Street. There are fire hydrants on Randy Street including a hydrant and fire sprinkler vault west of the gymnasium building. There are hydrants on Helman Street. A fire connection vault is proposed to be located adjacent to Helman Street. The water line sizes are substantial and water pressure is 90 p.s.i. at the Helman Street hydrant, which is adequate to address the water needs for the new structure.
- **Sewer:** There is an eight-inch sanitary sewer line in Randy Street, and there are 18-inch and 12-inch sanitary sewer lines in Helman Street. The applicant notes that in discussion with the Wastewater Department Supervisor, no capacity issues with the public sanitary sewer lines have been identified.
- **Electrical:** There are major overhead electrical facilities along Helman Street, and private facilities including junction boxes and vaults are in place. The application explains that the new structure has been designed and engineered to be solar-ready, and areas for future solar panel installation have been reserved in the roof plan. The applicant indicates that they are unaware of any electrical capacity issues.
- **Urban Storm Drainage:** There is an 18-inch storm sewer main in Helman Street. The development proposal includes substantial storm water quality improvements including the creation of two large landscaped bio-swales. The final Civil engineering will be designed to the standards of the DEQ MS4 General Permit, Phase 2, and the storm water system also be designed to comply with all of Ashland’s specific storm water quality design standards.
- **Transportation:** The applicant notes that there are existing curbside sidewalks in place along all frontages, and indicates that no changes to the existing curbside sidewalk configuration are proposed.

According to the Transportation System Plan, Laurel Street is classified as a Residential Neighborhood Collector. Laurel was recently subject to a Local Improvement District to install sidewalks in the Helman School neighborhood, and no changes to the Laurel Street frontage are proposed.

Randy Street is classified as a Neighborhood Residential Street, and currently has paving, curbs, gutters and curbside sidewalks in place along the property frontage, but no parkrows. The proposal would remove 3 of the five existing driveway curb cuts on Randy, including one that is immediately adjacent to the intersection and crosswalk, and reinstall a new driveway cut in a location which complies with controlled access standards and serves a new one-way circulation. The applicant emphasizes that these proposed changes to the driveways improve pedestrian safety by increasing driveway spacing away from the most heavily used intersection, while the proposed changes to the parking areas increase the length of the driveway and vehicular maneuvering area on site in order to better accommodate parent drop-off and pick-up on site, without pushing traffic onto the adjacent public streets, and the new one-way vehicular traffic circulation is to increase student and pedestrian safety.

Helman Street is considered an Avenue. Helman Street along the frontage of the school is not improved to current avenue standards – there is paving, curb, gutter and curbside sidewalks in place, but no parkrows. The application proposes to plant street trees behind the sidewalk and retain two existing driveway curb cuts and add one additional new driveway cut which complies with controlled access standards. No other changes to the Helman Street frontage are proposed by the applicant.

The Planning Commission notes that the application materials assert that facilities are in place to serve the existing campus buildings, and adequate key City facilities can be provided to serve the new classroom building, and that based on consultations with representatives of the various City departments (i.e. water, sewer, streets and electric) the proposed addition will not cause a City facility to operate beyond capacity. The Commission further finds that the project is intended to improve accessibility, safety, security and site circulation, but with the demolitions and addition proposed, neither the student enrollment or staffing are to be increased. The application includes civil drawings to address the changes in site grading, drainage, utilities and access associated with the proposal, and conditions have been included below to require that final civil drawings detailing the final utility and infrastructure improvements be provided for review and approval prior of the Building, Planning, Fire, Public Works and Electric Departments prior to building permit issuance.

The Commission concludes that this criterion has been satisfied.

The final criterion for Site Design Review approval addresses “*Exception to the Site Development and Design Standards.*”

The application materials recognize that the existing and proposed site development pattern including the placement of parking and vehicular access between the buildings and the street, placement of the new building roughly 180 feet from the property line and not oriented to the corner of Helman and Randy

Street, and the lack of pedestrian entrances open to the general public from the sidewalk necessitate exception to the design standards.

The applicant suggests that the use of the site as an elementary school can be found to be a unique which poses a demonstrable difficulty in meeting these standards in that schools in 2020 cannot be open to the general public like the typical commercial building considered in the standards. For student and staff safety and security, access to the campus must be restricted, and the funding source for the current project is through a local bond measure which sought to improve accessibility, structural safety, energy efficiency and campus security for an elementary school original built in 1960's. And the existing site layout establishes building and parking placement which pose challenges to increasing compliance with the applicable standards without full redevelopment of the campus. The applicant concludes that the exceptions requested are the minimum necessary to accommodate the re-development of the parking area and allow for the construction of a new classroom building.

The Planning Commission finds that the proposal involves the demolition of the two existing quad buildings nearest the corner of Helman and Randy Street, and the placement of a proposed new classroom addition more central to the campus rather than removing parking to put them nearer the corner.

The Planning Commission concurs with the applicant that the unique nature of the elementary school use poses challenges in meeting standards seeking a streetscape orientation without parking between buildings and the street and placement of buildings close to the sidewalk in that while a school is a public building subject to the Basic Site Review Standards for Non-Residential Development, it is at the same time a use which requires campus access controls to insure the safety and security of students and staff, and which seeks to avoid bringing cars into the mix of uses interior to the campus.

The Planning Commission notes that while the new classroom building is being placed in a location more central to the campus, rather than orienting to the corner as the standards would seek, the applicant is creating a new main entry plaza which orients the campus better to the corner and the neighborhood and places campus administrative functions in a location where they can oversee a single, controlled campus access point. The new classroom building responds to the campus character and broader neighborhood context through a scale and placement which also attempts to preserve views of Mt. Ashland and Grizzly Peak for the campus and its neighbors. The Commission finds that the proposed site plan creates a more cohesive campus with a strong central interior courtyard space centered on the library, provides a layout where access can be better controlled to maintain campus security, improves the campus orientation to the corner, improves pedestrian safety by addressing existing non-conforming driveway locations near the Helman and Randy intersection, and provides for new on-site detention of storm water in proximity to the parking as called for in current standards. The Commission further finds that the proposed improvements are in keeping with the general intent of the standards.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

2.4 The Planning Commission finds that the proposal satisfies the applicable standards for Conditional Use Permit approval with regard to the expansion of a non-conforming development.

The first criterion for Conditional Use Permit approval is, “*That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.*” The Planning Commission finds that the use of the property as a public school is an allowed use in the zone and the setbacks, lot coverage, building height, and parking conform to the R-1-5 zoning district standards, and further finds here that the request here is limited to considering the expansion of the existing non-conforming development pattern which places parking and circulation between buildings and the street.

The second criterion for a Conditional Use Permit is, “*That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.*” As noted in Section 2.3 above, the application includes civil drawings detailing site grading, drainage, utilities and access associated with the proposal, and conditions have been included to require that final engineered civil drawings detailing the utility and infrastructure improvements be provided for review prior to building permit issuance, and the Planning Commission finds that adequate capacity of City facilities can and will be provided.

Staff Discussion: Increase in Parking & Circulation Between the Building & the Street

The third Conditional Use Permit criterion is, “*That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone: a) Similarity in scale, bulk, and coverage; b) Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities; c) Architectural compatibility with the impact area; d) Air quality, including the generation of dust, odors, or other environmental pollutants; e) Generation of noise, light, and glare; f) The development of adjacent properties as envisioned in the Comprehensive Plan; and g) Other factors found to be relevant by the approval authority for review of the proposed use.*” In weighing these impacts, the criteria here explain that the target use in the R-1 zones is residential use developed to the densities detailed in AMC 18.2.5, which for the R-1-5 zoning here is 4.5 dwelling units per acre. The roughly 9.5 acres campus could accommodate roughly 42.75 dwelling units.

In considering the proposed expansion of parking and circulation between the buildings and the street, staff would first note that during the Site Design Review for the gym and library additions constructed in 2008-2009 (PA-2007-01756) it was noted that 60 automobile parking spaces were required to serve the 240 seat capacity of the gym at the then-applicable parking ratio of one space per four seats. The parking in place was found to satisfy the parking requirements with 53 parking spaces to be provided off-street and the use of seven on-street parking credits. The school property has a total of approximately 1,998 lineal feet of frontage on the three adjacent streets.

The current application looks at the current parking ratio of one parking space per 75 square feet of public assembly area and the 4,725 square feet of assembly space to arrive at a 63 space parking

requirement. The applicant notes that only 49 spaces are in place on site, and propose to add a new 17 space parking lot between the building and the street to fully accommodate the parking required on-site, with no reliance on on-street parking credits.

The increase in parking between the building and the street is based on bringing the site into compliance with a new parking ratio and an effort to reconfigure circulation and parking in such a way that parent pick-up and drop-off circulation impacts can be better absorbed on the campus itself and in so doing reduce or eliminate the impacts of on-street parking and pick-up and drop-off traffic to the neighborhood streetscape.

In staff's view, the added parking merits careful consideration first in that the existing parking was previously found to be adequate and there does not appear to have been an underlying parking problem since. There are no changes to student enrollment or staffing associated with the current proposal. For staff, while there are certainly benefits to better accommodating added vehicular queuing on site rather than on surrounding streets, there needs to be some balancing of the impacts to the streetscape of placing more parking between the buildings and the street (pedestrian friendliness, aesthetics) and of increased impervious surface (increased run-off, the heat island effect) and the benefits of making more efficient use of existing, available on-street parking. The Commission may wish to consider whether the additional parking is necessary given the amount of available on-street parking, and in seeking to balance these factors, staff would recommend that the Commission consider requiring that the main entry plaza treatment (light colored, permeable pavers) be extended to include the driveway and seven spaces between the new plaza and the corner to provide an extension of the plaza space, strengthening the plaza while retaining the potential to accommodate overflow parking as needed.

In looking at the proposed site circulation, staff believes that the middle driveway on Helman Street could be eliminated, with one-way traffic exiting from the new driveway roughly 130 feet to the south.

With the modified parking treatment near the plaza and elimination of the middle Helman Street driveway curb cut, staff believes that the changes to parking and circulation including improved driveway spacing near the Helman can be found to be beneficial to pedestrian safety while lessening impacts to the streetscape from pick-up and drop-off traffic and strengthening the campus's presence in the neighborhood streetscape with the new main entry plaza at the corner. Should the Commission concur, a condition (#7k) has been added to reflect the staff recommendation.

The fourth criterion is that, "*A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.*" Here, the Planning Commission finds that as detailed in AMC Table 18.2.2.030.D, public schools are a permitted use in all R-1 zones.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Conditional Use Permit approval.

2.5 The Planning Commission finds that the proposal satisfies the applicable standards for Conditional Use Permit approval with regard to modification to the School District's approved sign permit program

under AMC 18.4.7.120 which provides that, “*Governmental agencies may apply for a Conditional Use Permit to place a sign that does not conform to this chapter when it is determined that, in addition to meeting the criteria for a conditional use, the sign is necessary to further that agency's public purpose.*” Helman School’s murals were originally approved in Planning Action 2009-00322, and were subsequently incorporated into the district’s master sign permit program under Planning Action PA-2012-00899 which allowed a dragon wall graphic on the then-new gym and two existing student-designed/student-installed tile murals in addition to wall, ground and directional signage. A number of other murals and a tile-mosaic bench are also in place on campus, but are exempt from permitting because they are not visible from the adjacent public rights-of-way. As proposed, the dragon tile mural on the north side of the administration building, facing Randy Street, will be moved with demolition and replaced on a wall to be installed to screen mechanical equipment. With the move, the mural will be visible from Helman Street.

In originally administratively approving the murals in 2009, staff found that the student-designed/student-installed murals directly served the school’s public purpose not only in providing a direct and creative participatory educational experience but also in fostering a sense of connectedness between the students, the built environment of the school and their larger community. With the demolition of the two quad buildings, the applicant has proposed to relocate the dragon tile mural, and the Commission finds that this relocation remains in keeping with the original sign permit approval.

2.6 The Planning Commission finds that the proposal satisfies the applicable standards for a Tree Removal Permit for a tree that is not a hazard.

The first approval criterion for a Tree Removal Permit is that, “*The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.*” The Commission notes that 12 significant trees are proposed for removal, and that the applicant explains that the removals are to permit the proposal to be consistent with applicable ordinance requirements and standards, including applicable Site Development and Design Standards.

The second approval criterion is that, “*Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*” The applicant indicates that the requested tree removals will not have significant negative impacts on erosion, soil stability, the flow of surface waters, protection of adjacent trees, or existing windbreaks, and further explains that the areas where trees are to be removed will be redeveloped with structures, hardscaping, or will re-landscaped.

The third criterion is that, “*Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*” The applicant indicates that there are several trees within 200-feet of the subject property, and further suggests that the relative proximity to the heavily vegetated Ashland Creek corridor across Helman Street provides substantial species diversity, canopy coverage, and tree densities in the vicinity. The applicant concludes

that the proposed development will ultimately replace the canopy, tree densities, sizes, and species diversity associated with the requested removals.

The fourth criterion for Tree Removal Permit approval notes that, *“Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.”* The Commission finds that there is no residential component associated with the current application.

The final Tree Removal criterion is that, *“The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.”* The Commission finds that mitigation trees sufficient to meet this requirement are proposed throughout the property. 12 significant trees proposed for removal and the Landscape Plants plans (Sheets L3.00-L3.01) call for over 50 replacement trees including Kentucky Coffee trees, Zelkovas, flowering Cherries, Maple, Birch, and Lindens and include planting of new required street trees and 26 proposed shade trees for the parking areas to reduce the microclimatic impacts of the pavement.

The Commission further notes that the Ashland Tree Commission was unable to convene its regular monthly meeting for July of 2020 due to the City Administrator’s state of emergency declaration for the Coronavirus (COVID-19) outbreak, which suspended advisory commission meetings. As such there is no Tree Commission recommendation. As provided in AMC 2.25.040, the failure of the Tree Commission to make a recommendation on any individual planning action shall not invalidate that action.

The Commission finds that the remaining trees which are to be preserved are proposed to be protected with six-foot tall chain link fencing as recommended by the arborist and required in the City’s Tree Preservation & Protection Ordinance (AMC 18.4.5). Conditions have been included to require tree protection fencing installation and verification before site work.

The Planning Commission concludes that as detailed above and with the conditions discussed, the proposal complies with the requirements for Tree Protection and for Tree Removal Permits to remove a total of 12 significant trees.

2.5 With regard to the proposed demolition of the “A” and “B” quad buildings, the Planning Commission notes that the demolition and relocation of existing buildings is regulated through AMC Chapter 15 *“Buildings and Construction”* with approval of permits by the Building Official and the potential for appeal to the Demolition Review Committee.

The Commission finds that the applicant has indicated that the two quad buildings are to be demolished following completion of the new classroom building, and a condition has been included below to make clear that the applicant will need to obtain requisite permits for demolition through the Building Official prior to commencement of demolition work.

SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review, Conditional Use and Tree Removal permit approvals to construct a new 23,755 square-foot, single-story classroom building and associated changes to the campus site planning, relocate approved signage and remove 12 significant trees is supported by evidence contained within the whole record.

The school property is an existing non-conforming development in that the existing placement of parking between the buildings and the streets is contrary to the city's Basic Site Review standards which seek to place parking behind buildings or to one side and have the building placed at and oriented to the streetscape. As proposed here, this non-conformity would be retained and expanded through a Conditional Use Permit. The Commission finds that both the existing building lay-out on site and the school use pose difficulties in complying with the standards and as proposed the applicant is creating a new entry plaza near the corner of Helman and Randy Streets which creates an overall campus orientation to the corner and the neighborhood and places the school's administrative functions at a single, controllable access point for the sake of campus safety and security. The proposed new building's placement and scale are in direct response to a community public process by the School District which ultimately identified the need for a single-story structure placed more interior to the campus to preserve views of Mt. Ashland and Grizzly Peak for the campus and for the neighborhood, and in so doing a more cohesive campus with a central interior courtyard will be created and the library will become a clear center for the campus. In addition, with the changes proposed the controlled access issues with the northern parking lot's driveway exiting into the crosswalk are to be remedied, new on-site storm water detention facilities installed to better respond to standards, and site circulation issues addressed to handle a greater proportion of the daily pick-up and drop-off traffic and parking on-site rather than in the surround neighborhood streetscape.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2020-00020. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2020-00020 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant are conditions of approval unless otherwise modified herein.
2. That the plans submitted for the building permit shall be in conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this approval shall be submitted and approved prior to the issuance of a building permit.
3. That a sign permit shall be obtained prior to the installation of signage. Signage shall be consistent with that described herein and shall be placed in a manner consistent with the vision clearance standards of AMC 18.2.4.040.
4. That all requirements of the Fire Department shall be satisfactorily addressed, including approved addressing; fire apparatus access including aerial ladder access, turn-around, firefighter access

- pathways and work area; fire hydrant spacing, distance and clearance; fire flow; fire sprinkler system if applicable; fire extinguishers; limitations on gates or fences; providing required fuel breaks; and meeting the general fuel modification area standards.
5. That mechanical equipment shall be screened from view from the surrounding streets, and the location and screening of all mechanical equipment shall be detailed on the building permit submittals.
 6. That the applicant shall obtain applicable demolition permits through the Building Division if deemed necessary by the Building Official prior to the commencement of any building demolition on site.
 7. That building permit submittals shall include:
 - a. The identification of all easements, including but not limited to public or private utility, irrigation and drainage easements, fire apparatus access easements, and public pedestrian access easements.
 - b. The identification of exterior building materials and paint colors for the review and approval of the Staff Advisor. Colors and materials shall be consistent with those described in the application and very bright or neon paint colors shall not be used.
 - c. Specifications for all exterior lighting fixtures. Exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
 - d. Revised landscape and irrigation plans shall be provided for the review and approval of the Staff Advisor with the building permit submittals. These revised plans shall address: 1) required size and species-specific planting details and associated irrigation plan modifications, including the requirements for programmable automatic timer controllers and a maintenance watering schedule with seasonal modifications; 2) final lot coverage and required landscaped area calculations, including all building footprints, driveways, parking, and circulation areas, and landscaped areas. Lot coverage shall be limited to no more than 50 percent, and the calculations shall demonstrate that the requisite 50 percent landscaping and seven percent parking lot landscaping are provided; 3) the mitigation requirements of AMC 18.5.7 by detailing the mitigation for the 12 significant trees to be removed on a one-for-one basis through replanting planting on-site, replanting off-site, or payment to the city's Tree Fund in lieu of replanting; and 4) sight-obscuring screening of the parking lot with a landscape buffer in keeping with the requirements of AMC 18.4.3.080.E.6.a.iv and 18.4.4.030.F.2.
 - e. A Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements in AMC 18.3.10.100.A.2 of the Ashland Land Use Ordinance shall be provided prior to bringing combustible materials onto the property, and any new landscaping proposed shall comply with these standards and shall not include plants listed on the Prohibited Flammable Plant List per Resolution 2018-028.

- f. Final storm water drainage, grading and erosion control plans for the review and approval of the Engineering, Building and Planning Departments. The storm water plan shall address Public Works/Engineering standards requiring that post-development peak flows not exceed pre-development levels. Any necessary drainage improvements to address the site's storm water shall be provided at the applicants' expense. Storm water from all new impervious surfaces and run-off associated with peak rainfall events must be collected on site and channeled to the city storm water collection system (*i.e., curb gutter at public street, public storm pipe or public drainage way*) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.
- g. A final utility plan for the project for the review and approval of the Engineering, Planning and Building Divisions. The utility plan shall include the location of any necessary connections to public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins. The utility plan shall also address Water Department requirements relative to cross connections and premises isolation. Meters, cabinets, vaults and Fire Department Connections shall be located outside of pedestrian corridors and in areas least visible from streets, sidewalks and pedestrian areas, while considering access needs. Any necessary service extensions or upgrades shall be completed by the applicant at applicant's expense.
- h. A final electric design and distribution plan including load calculations and locations of all primary and secondary services including any transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric, Engineering, Building and Planning Departments prior to the issuance of excavation or building permits. Transformers, cabinets and vaults shall be located outside the pedestrian corridor in areas least visible from streets, sidewalks and pedestrian areas, while considering the access needs of the Electric Department. Any necessary service extensions or upgrades shall be completed at the applicant's expense.
- i. That the applicants shall provide final engineered plans for any work in the street rights-of-way including any changes to sidewalks, driveway aprons or pedestrian crossings for the review of the Planning and Public Works/Engineering Departments.
- j. Identification or required bicycle parking, which includes 70 covered bicycle parking spaces. Inverted u-racks shall be used for the outdoor bicycle parking, and all bicycle parking shall be installed in accordance with the standards in 18.4.3.070.I, inspected and approved prior to the issuance of the certificate of occupancy. The building permit submittals shall verify that the bicycle parking spacing and coverage requirements are met.
- k. A revised site plan that extends the new entry plaza treatment (*i.e. light colored/permeable pavers*) to include the driveway and seven parking spaces between the new plaza and the

corner to provide an extension of the plaza space, strengthen the plaza while retaining the potential to accommodate overflow parking as needed; which removes the middle driveway curb cut on Helman Street; and which provides a monitored/gated access point from the Parkside Drive pedestrian easement.

8. That prior to any site work including staging, storage of materials, demolition or tree removal, the applicant shall mark the 12 trees to be removed and install protection fencing for the trees to be retained, and obtain a Tree Verification Inspection so that the Staff Advisor can verify that the trees identified on site for removal are consistent with the approved plan, and that those trees to be protected have tree protection fencing in place in a manner consistent with the approved plans.
9. That prior to the issuance of a building permit all necessary building permits fees and associated charges, including permits and connections fees for any new utilities, and system development charges for water, sewer, storm water, parks, and transportation (*less any credits for existing structures*) shall be paid.
10. That prior to the issuance of a certificate of occupancy or final project approval:
 - a. That the required automobile and bicycle parking shall be installed according to the approved plan, inspected and approved by the Staff Advisor.
 - b. All hardscaping including the sidewalk corridor, on site circulations routes, parking lots and driveways; landscaping; and the irrigation system shall be installed according to the approved plan, inspected, and approved by the Staff Advisor.
 - c. That the screening for the trash and recycling containers shall be installed in accordance with the Site Design and Development Standards prior to the issuance of a certificate of occupancy. An opportunity to recycle site of equal or greater size than the solid waste receptacle shall be included in the trash enclosure in accordance with 18.4.4.040.
 - d. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
 - e. All required utility service and equipment installations and street frontage improvements, shall be installed under permit from the Public Works Department and in accordance with the approved plans, inspected and approved by the Staff Advisor.
 - f. Replacement trees to mitigate the trees removed shall be planted and irrigated according to the approved plan, or alternative mitigation demonstrated.

Planning Commission Approval

August 11, 2020
Date



City of Ashland
Community Development Department
51 Winburn Way
Ashland, OR 97520
Telephone: 541-488-5305
Inspection Line: 541-552-2080

| |
|----------------------|
| PERMIT NUMBER |
| PA-T2-2020-00020 |
| Apply Date: 6/8/2020 |

Plan Type: Type II Planning Action

Work Class: Type II Planning Action

| Map & Tax Lot | Property Address |
|---------------|------------------|
| 391E04BD2900 | 705 Helman St |

| Owner Information | Applicant Information |
|---|--|
| Owner: Ashland School District 5 Owner 201 S Mountian Ave Address: Ashland, OR 97535 Phone: (541) 482-2811 | Applicant: Rogue Planning and Development Applicant 33 N Central Ave 213 Address: Medford, OR 97501 Phone: (541) 951-4020 |

| Project Description |
|--|
| Type 2 commercial site review with type 1 CUP. |

| Fees | |
|----------------------------------|-------------|
| Fee Description: | Amount: |
| Commercial Site Review (Type II) | \$52,190.75 |
| Conditional Use Permit (Type I) | \$1,092.00 |

Applicant: _____

Date: _____

| | |
|--------------------|--------------------|
| Total Fees: | \$53,282.75 |
|--------------------|--------------------|

10,000,000
Valuation



Planning Division
51 Winburn Way, Ashland OR 97520
541-488-5305 Fax 541-488-6006

ZONING PERMIT APPLICATION

FILE # PA-T2-2020-00020

DESCRIPTION OF PROJECT Site Review approval and Conditional Use Permit for new elementary school building

DESCRIPTION OF PROPERTY

Pursuing LEED® Certification? YES NO

Street Address 705 Helman Street

Assessor's Map No. 39 1E 04BD

Tax Lot(s) 2900, 2800, 2700 & 600

Zoning R-1-5

Comp Plan Designation Single-Family Residential

APPLICANT

Name HMK Company Phone 541.499.7996 E-Mail mike.freeman@hmkco.org

Address Unit 46 N. Front Street, City Medford, ZIP 97501

PROPERTY OWNER

Name Ashland School District #5 Phone 541.482.2811 E-Mail steve.mitzel@ashland.k12.or.us

Address 885 Siskiyou Blvd. City Ashland Zip 97520

SURVEYOR, ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OTHER

Title Planning Consultant Name Rogue Planning & Development Services, LLC Phone 541.951.4020 E-Mail amygunter.planning@gmail

Address 33 N Central Avenue, Suite 213 City Medford Zip 97501

Title Architects Name BBT Architects Phone 541.382.5535 E-Mail ralexander@bbtarchitects.com

Address 1140 SW Simpson Avenue, Suite 200 City Bend Zip 97702

See attached findings for complete list of project team

I hereby certify that the statements and information contained in this application, including the enclosed drawings and the required findings of fact, are in all respects, true and correct. I understand that all property pins must be shown on the drawings and visible upon the site inspection. In the event the pins are not shown or their location found to be incorrect, the owner assumes full responsibility. I further understand that if this request is subsequently contested, the burden will be on me to establish:

- 1) that I produced sufficient factual evidence at the hearing to support this request;
- 2) that the findings of fact furnished justifies the granting of the request;
- 3) that the findings of fact furnished by me are adequate, and further
- 4) that all structures or improvements are properly located on the ground.

Failure in this regard will result most likely in not only the request being set aside, but also possibly in my structures being built in reliance thereon being required to be removed at my expense. If I have any doubts, I am advised to seek competent professional advice and assistance.

[Signature]
Applicant's Signature

6/5/2020
Date

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As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner.

[Signature]
Property Owner's Signature (required)

6/5/2020
Date

JUN 05 2020
City Of Ashland

(To be completed by City Staff)

Date Received 6/5/20

Zoning Permit Type Type 2

Filing Fee \$ 53,282.75

OVER ►►

Site Design Review and Conditional Use Permit



Helman Elementary School



Ashland School District

inspiring learning for life...

BBT ARCHITECTS

HMK
COMPANY

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ROGUE PLANNING & DEVELOPMENT SERVICES, LLC

June 5, 2020

**Site Design Review for Addition of more than 10,000 SF
to non-residential structure and
Conditional Use Permit to expand the non-conforming elementary school site.**

SUBJECT PROPERTY:

ADDRESS:

705 HELMAN STREET

MAP & TAX LOTS:

39 1E 04BD; 2900, 2800, 2700 and 600

PROPERTY OWNER:

ASHLAND SCHOOL DISTRICT 005

885 Siskiyou Boulevard

Ashland, Oregon 97520

Phone: 541.482.2811

Contact: Steve Mitzel

Email: steve.mitzel@ashland.k12.or.us

OWNER REPRESENTATIVE:

HMK COMPANY

60 2nd Street, Unit 602

Central Point, Oregon 97502

Phone: 503.453.2836

Contact: Mike Freeman

Email: mike@hmkco.org

ARCHITECT:

BBT ARCHITECTS

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Bend, Oregon 97702

Phone: 541.382.5535

Contact: Matthew Guthrie

Email: mguthrie@bbtarchitects.com

STRUCTURAL ENGINEER:

WALKER STRUCTURAL ENGINEERING LLC

2863 NW Crossing Dr., Suite 201

Bend, OR 97701

Phone: 541.330.6869

Contact: Jonny Walker

Email: jwalker@walkerse.com

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Email: kerry@kencairnlandscape.com

**CONSTRUCTION MANAGER
/ GENERAL CONTRACTOR:**

ADROIT CONSTRUCTION COMPANY
185 Mistletoe Road
Ashland, OR 97520
Phone: 541.482.4098
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PLANNING CONSULTANT:

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Request:

The request is for Site Design Review and Conditional Use Permit to expand and enlarge a non-conforming development to allow for the construction of a 23,775 square foot, one-story classroom building at the Helman Elementary School campus.

To improve accessibility, safety, security, and site circulation, an enhanced pedestrian entry plaza area is proposed. The parent pick-up/drop off area and existing non-conforming parking area is proposed to be reconfigured. The proposal includes the expansion of non-conforming site development with parking area and vehicular access between the structures and the street.

The request includes a tree removal permit to remove 12 significant trees. New landscape areas including mitigation trees, street trees, and improved water-efficient irrigation are proposed. The proposal includes a new stormwater management system including above-ground detention swales.

New entry doors on the north side of the office building, and interior renovations to the existing administration offices and interior renovation and mechanical equipment upgrades to classroom building "C Quad" are proposed.

The mechanical equipment service yard is proposed to be screened and buffered from the residential neighborhood.

The basketball court and playground area will be relocated and new security fencing is proposed.

To facilitate site modifications, the proposal necessitates the demolition of two (2) existing classroom buildings.

Property Description:

The subject property consists of four tax lots and is occupied by Helman Elementary School. The property is bound by Helman Street to the east, Randy Street to the north, and Laurel Street to the west. In total, the Helman Elementary School campus consists of roughly 9.5 acres.

The Helman Elementary School campus buildings and parking lots are clustered on the northern portion of the site. The playground area is to the south of the campus buildings. There is a playground structure, and equipment area, two tennis courts, and a basketball court. There is also a large, approximately 3.5-acre grass field area.

Helman Elementary School was opened in 1966 with additions in the 1970s and 2008. The school consists of 16 classrooms in four, four-room quad buildings, a central building nearest Helman Street houses the administrative and staff offices, library, kitchen-cafeteria/multi-purpose room, and a music room. Helman Elementary is in an open campus format with unsecured, outdoor walkways connecting the

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campus quads and the school buildings. The most recent addition was the approximately 6,400 square foot gymnasium and 5,010 square foot library on the northwest portion of the Helman Elementary School campus, near Randy Street, which was granted by the Planning Commission in December of 2007. The improvements made at that time generally comply with city standards and excepting fencing, no modifications to the parking areas associated are proposed.

Helman Elementary School has an enrollment capacity of 350 students, averaging 300 students per year. The class sizes range from 20 – 30 students. There are 12 classroom teachers and a robust Title IX program. There are typically 20 – 30 staff in all the various admin, classified, and teaching staff positions. No changes to student or staffing populations are proposed.

The existing campus layout including the orientation to street, location of the parking between building and the street, lack of landscape buffering of the existing parking areas, driveway curb cut spacing, and street improvements including the curbside sidewalk and lack of street trees are non-conforming development situations. When Helman Elementary School was constructed, there was not a site review process or standards..

In 2009 a Conditional Use Permit was granted for the installation of a dragon mural on the gymnasium wall and to allow for the retention of two existing student-designed and installed tile murals. The school's changeable copy, reader board sign, and wall signs have approval through the Ashland School District Master Sign Permit approval from 2012.

Helman Elementary School and all properties in the vicinity to the north, south, east, and west are zoned Single Family Residential (R-1-5).

The Helman Elementary School property is bound on three sides by public rights-of-way. The three city streets, Helman, Laurel, and Randy Streets are improved with curb, gutter, pavement, and curbside sidewalks. The existing street improvements are existing non-conforming due to the curbside sidewalks and lack of street trees. There are three driveway curb cuts on Randy Street. The one at the intersection is non-conforming. Two driveways are accessing the site from Helman Street, the one nearest the intersection has non-conforming spacing from the intersection.



Proposal:

In 2018, the voters approved a School Bond measure. The primary goals of the bond are to improve the health and safety of all students in the district, but individually noted was Helman Elementary School and its needs for redesigned entry, redeveloped more efficient and spatially adequate instruction area and secure student circulation.

Helman Elementary School as an open campus and does not have a clearly visible, secure entry that limits points of access onto the school campus. Nor are there any restrictions for visitor access, and anyone can enter the campus and bypass checking into the office area.

As existing, Helman Elementary School layout and orientation are towards the parking area, the location of the administrative offices cannot change but the proposed entry plaza area, enhanced office entry doors, the improved parking areas, and the perimeter security fencing and gates upgrades will increase the safety of the teachers, students, classified employees, volunteers, and community members that utilize the elementary school campus.

Additionally, seismic upgrades, HVAC, Mechanical, Plumbing, and Electrical upgrades with a focus on green technologies and renewable energy-focused design and construction are proposed.

Detailed Proposal:

Helman Elementary School lacks the spatial capacity for site-based special education, and students receiving those services had to be bussed or taken to another school. These neighborhood children were not able to learn at their neighborhood school with their siblings and neighborhood friends. It was noted that the 1960s and 1970s era school construction did not accommodate flexible, functional instructional spaces and no gathering areas. Helman Elementary School's layout also lacked eyes on the street functions in the front office. In these times, security and eyes on-campus access is critical to student and faculty wellbeing and safety. The proposed modifications to the Helman Elementary School layout and new classroom building construction address these very important issues.

The project team, BBT Architect, and HMK Management worked with the Helman staff, the school districts Core Team, the School User Groups, and the Site Core teams to refine the layout and design. The proposal is the result of many months of community collaboration. The proposed design, layout, and construction are consistent with the policies of the Climate Energy Action Plan and review of the proposal from Brightworks Sustainability are provided in the application materials.

The proposal includes the demolition of two (2) existing classroom buildings, A Quad is a 4,680 square foot structure, B Quad is a 4,680 square foot structure. These two structures are nearest to the intersection of Helman Street and Randy Street. The area of these structures will be redeveloped the enhanced campus entry area and the redesigned parking lot.

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The entry plaza area orients the campus to the intersection of Helman Street and Randy Street. This area is also the student drop off and pick-up for parents/guardians. This plaza area is accessed from the redesigned non-conforming parking lot at the corner of Helman Street and Randy Street. The improved entry plaza provides enhanced building orientation towards the location of the entrance into the school campus and the administrative offices to the street intersection. The entry plaza area has been designed in a manner to provide clear, visible, and functional entry to the campus with direct access to the public sidewalk while providing increase security through restricted campus access points.

There are 2,260 square feet of interior renovations to the existing administration office area portion of the cafeteria and administrative building. These interior renovations provide a more functional administrative services entrance, leading to a reception area. A new entry door into the office is proposed on the north side of the administrative offices. This is where all visitors and volunteers sign in. The main campus entry for students is through gated access to the north of the offices.

There are 380 square feet of interior renovations to the C Quad building. This is to improve the restrooms, plumbing, electrical, and mechanical provided within the Quad building.

A 23,755 square foot, single-story classroom building is the largest improvement proposed for Helman Elementary School. The new classroom building is proposed at the south end of the Helman Elementary School campus area. This building is proposed to have a central entry hall with a large, multi-purpose flex room. The entry is flanked by building wings that are oriented to the northwest and northeast. The multipurpose room will provide for much-needed all-purpose space for staff meeting area, community meeting area, and student gathering area.

The building wings are proposed to be developed with wide, accessible hallways, office space, single occupancy (adult/staff) restrooms, office space, custodial and mechanical areas as well. Both wings will have dedicated recycling areas.

The northwest wing is proposed to have three general classrooms, a sensory classroom, and SPED dedicated classroom area. There are two new kindergarten classrooms with kinder-sized restrooms, general activity spaces for small group breakout areas.

The northeast wing includes five general classrooms, two breakout activity spaces, and small group space. The student restroom is located in this wing.

The central campus area connecting new construction to the existing building areas is proposed to be redeveloped with the creations of a central courtyard area and redevelopment of the walkway system to provide pedestrian and ADA accessibility to the entire campus from the entry plaza to the relocated basketball court and relocated playground area.

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The proposal includes the reconfiguration of an existing parking area and the expansion of a non-conforming parking lot.

The mechanical equipment service yard is proposed to be screened and buffered from the residential neighborhood across the street.

The basketball court is proposed to be relocated, the playground area relocated and renovated, the installation of a walking track on the west side of the property, and stormwater management structures are proposed to be installed.

The proposal includes a new perimeter security fencing. The proposed fencing will allow for the students on campus to be contained during the school hours and will have only one unlocked entrance during the school hours to direct all campus visitors to the main entry area and administrative services offices. Other gates are proposed around the perimeter, these will be able to be unlocked by those granted permission by the Ashland School District and/or Helman Elementary School as the property is listed (but not acquired) on the Parks Open Space Plan.

Access and Site Circulation:

There are six, driveway approaches to the Helman Elementary School site. There is a one-way vehicular loop from Randy Street, looping back north to Randy Street. This is the west parking lot and the “bus loop” where bussed students enter the campus through secure, gated access on the north side of the library. This gate will be open during bus drop off and pick up, remaining closed throughout the school day. There are no modifications proposed to this location.

There is another vehicular accessed parking area at the intersection of Randy and Helman Street. The north lot is accessed from Randy Street and has non-conforming driveway access at the southwest corner of the intersection that exists right at the Randy Street and Helman Street stop sign and a heavily utilized crosswalk. This area is proposed to become a formal student drop-off area which Helman Elementary School previously lacked. The parking area is proposed to be modified by shifting the parking area away from the street, closure of the non-conforming curb cut, and development of a stormwater bioswale/ponding area. This parking area is proposed to be developed as a one-way from Randy Street, and connecting to the existing east parking lot on Helman Street.

The east parking lot is accessed from Helman Street, this curb cut is non-conforming due to Helman Street classification as an Avenue in the Transportation System Plan (TSP). No changes are proposed to this curb cut and no increases in the non-conformity are proposed. The east parking area is one-way to the south, with diagonal, head-in parking, and a one-way exit onto Helman Street. Two lanes are provided that allow for northbound and southbound exiting. There is a landscape buffer along the parking area and the street. No changes are proposed to this parking area.

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Parking:

There are 49, onsite vehicle parking spaces. Based on the 4,725 square foot area of the assembly spaces, there should be 63, on-site parking spaces. The proposal includes an increase in the number of parking spaces provided on-site through the creation of a new, 17 space parking area (south lot). This lot is proposed to be extended as one way from the existing east parking lot. The parking spaces are proposed to be diagonal spaces in three banks of four and one bank of five. The parking lot is proposed to be buffered from Helman Street with a stormwater bioswale/pond area. This parking area is proposed to have parking lot shade tree islands and accessible walkways.

Helman Elementary School requires 70 bicycle parking spaces, all covered. Though several Helman Elementary School students ride their bicycles to school, never have anywhere near 70 bicycles been present at campus. There are presently 12 covered bicycle parking spaces on-site, this is a pre-existing, non-conforming situation. The proposal adds bicycle parking on the north side of the campus accessible from the Randy Street entry to campus, a 20 stall, covered secure bicycle parking structure is proposed. Along the west side of the new south parking lot, a covered, secure bicycle parking structure for 29 bicycles is proposed. There is presently 17 percent of the required bicycle parking spaces provided, the proposal provides for 94 percent of the bicycle parking spaces. This is a substantial increase and will provide more than enough parking spaces for the students that ride bicycles to school presently and an increase in riders.

Trees and Landscaping:

A detailed Landscape and Tree Protection and Removal Plan have been provided. There are 106 trees on-site, of those, 89 are six inches in diameter at breast height (DBH) or greater. There are 12 significant trees proposed for removal to facilitate site construction and development. A significant tree is a conifer tree having a trunk 18 caliper inches or larger in diameter at breast height (DBH), or a deciduous tree having a trunk 12 caliper inches in DBH.

The trees proposed for removal include five (5), 12-inch DBH pear trees (*Pyrus*); a 14-inch DBH Oak tree (*Quercus*); two (2) maple trees, one 14-inch DBH and one 17-inch DBH; a 16-inch European beech (*Fagus*); and two Norway spruce (*Picea abies*) one 16-inch DBH, and one 20-inch DBH. The largest trees proposed for removal are three (3) black locusts (*Robinia pseudoacacia*) in a landscape planter along Helman Street. Two of the black locust trees are 19-inches DBH and one is a 24-inch DBH. Three trees are greater than six inches in diameter at breast height that are proposed for relocation.

The proposed tree protection plan retains a substantial of the trees on-site, and the landscape plan uses a variety of deciduous shade trees, shrubs, and ground covers. Implementing water-conserving landscape and irrigation design, the proposed landscape plan and the future irrigation plan can demonstrate compliance with the standards and is appropriate in a school grounds setting.

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Findings of Fact:

The following information addressing the findings of fact for the applicable criteria from the Ashland Municipal Code is provided on the following pages. For clarity, the criteria are in Arial font and the applicant's responses are in Times New Roman font.

Additionally, a phasing line is shown on the civil site plans. This demarcation is intended to assist school staff in maintaining school operations with minimal disruptions during construction. The new classroom addition is scheduled to be completed first, followed by building demolition, building alterations, and remaining site work. The contractor and project funds have been secured for the entire scope of work.

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Criteria from the Ashland Land Use Ordinance

Site Development Design Standards Approval Criteria:

18.5.2.050 Approval Criteria

An application for Site Design Review shall be approved if the proposal meets the criteria in subsections A, B, C, and D below.

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

Finding:

The subject property is zoned single-family residential (R-1-5). Public schools are a permitted use in the R-1-5 zone.

The proposed new classroom building exceeds all the setbacks in the zone and the structure is more than 15-feet from Helman, the front yard, more than 10-feet from Laurel Street, the rear, and more than six-feet from the south property line. The proposed building is 24-feet at the highest point, the top of the mechanical room. This is less than the maximum building height in the R-1-5 zone of 30-feet average height. The proposed height is similar to homes in the vicinity that are one- and two-story structures.

The maximum allowed lot coverage in the zone is 50 percent. The proposed development and existing surfaces cover 38 percent of the campus which is less than the maximum coverage in the zone.

Overall Site: 432,115 SF

Asphalt (including some synthetic material at playground): 60,668 SF

Concrete Flatwork: 45,414 SF

Building: 58,216 SF

Impervious: 164,298 SF

Pervious: 270,171 SF

Impervious Percentage of Site: 38%

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The property is exempt from density and floor area ratio standards.

The proposed architecture is consistent with elementary school design. Though not residential, the proposed new structure has pitched roofs and eaves which are common design elements found in residential construction. The site development standards place substantial emphasis on the pedestrian accessibility to the commercial business and the layout of sites requiring the parking be to the rear or side of the structure. Due to the nature of elementary school campus safety and security, the structure has substantial setbacks from the street, and the entrances are not accessible from the sidewalks. The parking and vehicular circulation occur between the structure and the street as well.

The proposed new classroom building is oriented towards Helman Street with large windows, doors that access the classroom areas, the new building is not accessible to the public from the sidewalk due to safety considerations.

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

Finding:

The southern portion of tax lots 600 and 2700 are within the Performance Standards Overlay. Performance Standards Overlay applies to subdivisions and is not applicable.

C. Site Development and Design Standards. The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

18.4.2.040 Non-Residential Development

Finding:

The property is developed with a public elementary school that was first constructed in the late 1960s. The Helman Elementary School campus and the development layout is non-residential but serves the surrounding residential neighborhood. The development of the majority of the campus including the locations of the parking areas, orientation to the streets, setbacks, site coverage, are non-conforming concerning the present site development standards.

Additionally, other standards that typically apply to commercial development or typical residential development when developed to the standards for placement, orientation, and design of building from the Site Development Design Standard, conflict with the student and staff campus safety goals.

The proposed modifications to the site layout and access bring the property closer to compliance with the standards by shifting the parking spaces and student drop off area away from the street and providing a landscape bioswale. The south parking lot is proposed to be installed as a one-way (north to south) extension of the existing parking area. The parking areas located between the new classroom building and the street is proposed to be buffered from Helman Street with a large bioswale.

The new classroom structure is designed in a manner that addresses the purpose and intent of the Site Development and Design Standards Administration for Review. The proposal provides for the protection of the health, safety, and welfare of the students through the design that supports resource conservation and renewable energy sources and high-efficiency construction, HVAC, mechanical and plumbing efficiencies, and electric upgrades. The building is proposed to have a wide facade along Helman Street with windows and doors. The structure is set back from the street +/- 100-feet. This is substantially further than a typical commercial or residential development but not atypical of a large acre, elementary school campus.

The proposed redesigned entry plaza area and administrative offices provide for a safe and comfortable entry providing improved surveillance of the public spaces and funnels all campus visitors to the secure entry to the Helman Elementary School Campus.

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The existing structures and the new structure are proposed a substantially further distance from the public street than a commercial or employment development due to the nature of the use as a public, elementary school. The proposed design though does provide a positive impact on the streetscape with residential forms instead of an unadorned concrete or brick structure of rectangular, forms found in a commercial district.

Landscaping is proposed to enhance the site and provide screening of the parking lot and trees to provide cooling of the surface parking areas.

The proposed onsite vehicle, pedestrian, and bicycle transportation improvements are proposed which will enhance the pedestrian environment and will improve bicycle transit by providing an abundance of bicycle parking facilities.

B. Basic Site Review Standards.

1. Orientation and Scale.

a. Buildings shall have their primary orientation toward the street and not a parking area. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or to one side. See Figure [18.4.2.040.B.1.](#)

Finding:

See finding g.

b. A building façade or multiple building facades shall occupy a large majority of a project's street frontage as illustrated in Figure [18.4.2.040.B](#), and avoid site design that incorporates extensive gaps between building frontages created through a combination of driveway aprons, parking areas, or vehicle aisles. This can be addressed by, but not limited to, positioning the wider side of the building rather than the narrow side of the building toward the street. In the case of a corner lot, this standard applies to both street frontages. Spaces between buildings shall consist of landscaping and hard durable surface materials to highlight pedestrian areas.

Finding:

See finding g. The campus is made up of individual structures connected via paved walkways and landscaped plaza area.

c. Building entrances shall be oriented toward the street and shall be accessed from a public sidewalk. The entrance shall be designed to be clearly visible, functional, and shall be open to the public during all business hours.

Finding:

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See finding g.

d. Building entrances shall be located within 20 feet of the public right of way to which they are required to be oriented. Exceptions may be granted for topographic constraints, lot configuration, designs where a greater setback results in an improved access or for sites with multiple buildings, such as shopping centers, where other buildings meet this standard.

Finding:

See finding g.

e. Where a building is located on a corner lot, its entrance shall be oriented toward the higher order street or to the lot corner at the intersection of the streets. The building shall be located as close to the intersection corner as practicable.

Finding:

The new construction is to the rear of the existing campus buildings and not located at the corner. There is an entrance to the new structure proposed facing Helman Street, it is not accessible from the sidewalk due to the use of the property as an elementary school.

The proposed removal of Quad A opens the area of the administrative offices and the entrance gate to be visible from the public street and oriented towards the intersection.

f. Public sidewalks shall be provided adjacent to a public street along the street frontage.

Finding:

There are public sidewalks adjacent to all of the public street frontages. The property frontage is bound by curbside sidewalks. The curbside sidewalks are pre-existing, non-conforming. Excepting where the curb cut on Randy Street at the intersection is proposed to be closed, no changes to the existing curbside sidewalks are proposed.

g. The standards in a-d, above, may be waived if the building is not accessed by pedestrians, such as warehouses and industrial buildings without attached offices and automotive service stations.

Finding:

The proposal seeks to waive the standards of a. – d. above because though the new classroom building is accessed by students/parents/guardians as pedestrians from the neighborhood, the building is not a business that is accessible to the general public and the structure is not “open to the public during business hours”. The proposed security fencing and redesigned entry plaza area are intended to direct all pedestrian traffic to the

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school towards the administrative services offices where the only access to the secure campus is located.

The existing buildings are oriented toward the parking area which is a pre-existing, non-conforming development pattern, and the parking area is between the buildings and the street. The proposed entry plaza area will provide a better orientation to the main campus entry at the northeast corner of the administrative services/cafeteria building. The removal of Quad A opens the view from the street to the entry stairs and office building and the main entrance to the Helman Elementary School campus.

Having the parking and the student drop off area between the building and the street is provided as a safety measure. The students that are dropped off by parents/guardians allowing for a short travel distance from the guardian to the secure entry to the campus.

The expanded parking area is between the new structure and the street. This is to increase the amount of parking to be closer to the demand reducing the use of on-street parking by staff and visitors to the campus. The enhanced student drop-off will improve the vehicle stacking that occurs on to the streets during the morning and afternoon drop off and pick up.

The student access to the basketball court and the relocated playground area is around the east side of the new classroom structure.

2. Streetscape. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

Finding:

There are new street trees proposed to replace trees removed due to their species, Black Locust, and their incompatibility with construction. There are new street trees proposed to be installed every 30-feet, behind the curbside sidewalk, along with the areas of the property where there is proposed development. There are eight trees proposed along the bioswale pond area at the intersection of Randy and Helman Street. There four street trees proposed in the area where the black locust trees are removed from and two new street trees to fill in the gaps along the Helman Street frontage where street trees are not present. New street trees are proposed behind the sidewalk, abutting the bioswale/pond area between the new classroom building and the street.

The proposed street trees include large stature trees such as Bonfire Sugar Maples, Zelkovas, Kentucky Coffee trees. There are River Birches proposed for the bioswale areas. The trees will be planted according to the standards found in AMC 18.4.4.030.E.

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3. Landscaping.

- a. Landscape areas at least ten feet in width shall buffer buildings adjacent to streets, except the buffer is not required in the Detail Site Review, Historic District, and Pedestrian Place overlays.

Finding:

The existing and proposed structures are buffered by at least ten feet from the street. Due to the nature of the use of the property as a public elementary school, lawn area, and hardscape are the primary landscape materials. The entry areas and the areas immediately surrounding the new classroom building include hardscape and landscape planter areas that include trees, shrubbery, and ground covers. The proposed landscape area at the new entry plaza area enhances the site orientation towards the intersection of Helman Street and Randy.

- b. Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

18.4.4.030 Landscaping and Screening

B. Minimum Landscape Area and Coverage. All lots shall conform to the minimum landscape area standards of the applicable zoning district (see Table 18.2.5.030.A - C for residential zones and Table 18.2.6.030 for non-residential zones). Except as otherwise provided by this chapter, areas proposed to be covered with plant materials shall have plant coverage of not less than 50 percent coverage within one year and 90 percent coverage within five years of planting.

Finding:

The areas of disturbance from the construction are proposed to be landscaped with a formal landscape plan. A large area of the property is lawn area and will remain as such. All areas of proposed landscaping provide for plant materials that grow to 90 percent coverage within five years of planting.

C. Landscape Design and Plant Selection. The landscape design and selection of plants shall be based on all of the following standards:

- 1. Tree and Shrub Retention. Existing healthy trees and shrubs shall be retained, pursuant to chapter 18.4.5. Consistent with chapter 18.4.5 Tree Preservation and Protection, credit may be granted toward the landscape area requirements where a project proposal includes preserving healthy vegetation that contribute(s) to the landscape design.

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Finding:

The tree protection and preservation plan and the tree removal plan call for the removal of the trees that are within the areas of construction and that would not survive the impacts from construction.

Of the more than 100 trees on the site, there are only 12 significant trees proposed for removal.

2. Plant Selection.

- a. Use a variety of deciduous and evergreen trees, shrubs, and ground covers.
- b. Use plants that are appropriate to the local climate, exposure, and water availability. The presence of utilities and drainage conditions shall also be considered.
- c. Storm Water Facilities. Use water-tolerant species where storm water retention/detention or water quality treatment facilities are proposed.
- d. Crime Prevention and Defensible Space. Landscape plans shall provide for crime prevention and defensible space, for example, by using low hedges and similar plants allowing natural surveillance of public and semi-public areas, and by using impenetrable hedges in areas where physical access is discouraged.
- e. Street Trees. Street trees shall conform to the street tree list approved by the Ashland Tree Commission. See the Ashland Recommended Street Tree Guide.

Finding:

The proposed landscape plan has been created by a local landscape architect. The proposed plan uses a variety of deciduous trees, shrubs and ground covers. Due to the wildfire hazards overlay, no evergreen trees are proposed. The plants selected are appropriate for the local climate and exposure. Water tolerant species such as River birch and a wetland prairie seed mix are proposed within the stormwater detention facilities.

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The planting plan for the entry plaza area allows for natural surveillance of the public space. Fencing to direct all visitors to the common entrance at the Administrative Office / Cafeteria building has been proposed.

Street trees are proposed to be planted along the Randy Street frontage where the parking area is proposed for relocation and the construction of a stormwater detention facility is proposed. New street trees are proposed along Helman Street behind the sidewalk. The street trees on Helman Street are chosen for their compatibility with overhead power lines.

3. Water Conserving Landscaping. Commercial, industrial, non-residential, and mixed-use developments that are subject to chapter 18.5.2 Site Design Review, shall use plants that are low water use and meet the requirements of 18.4.4.030.I Water Conserving Landscaping.

Finding:

The proposed landscaping plant materials is low water use and meet the requirements of 18.4.4.030.I.

4. Hillside Lands and Water Resources. Landscape plans for land located in the Hillside Lands overlay must also conform to section 18.3.10.090 Development Standards for Hillside Lands, and in the Water Resources overlay must also conform to section 18.3.11.110 Mitigation Requirements for Water Resource Protection Zones.

Finding:

Not applicable.

5. Screening.

a. Evergreen shrubs shall be used where a sight-obscuring landscape screen is required.

b. Where a hedge is used as a screen, fire-resistant and drought-tolerant evergreen shrubs shall be planted so that not less than 50 percent of the desired screening is achieved within two years and 100 percent is achieved within four years. Living groundcover in the screen strip shall be planted such that 100 percent coverage is achieved within two years.

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Finding:

There are no areas where evergreen screening shrubs are required.

6. Plant Sizes.

a. Trees shall be not less than two-inch caliper for street trees, and 1.5-inch caliper for other trees at the time of planting.

b. Shrubs shall be planted from not less than one gallon containers, and where required for screening shall meet the requirements of 18.4.4.030.C.5 Screening.

Finding:

All plant and tree species will be planted in accordance with the specifications.

D. Tree Preservation, Protection, and Removal. See chapter 18.4.5 for Tree Protection and Preservation and chapter 18.5.7 for Tree Removal Permit requirements.

Finding:

Tree removal findings are provided on Page 38 of these findings.

E. Street Trees. The purpose of street trees is to form a deciduous canopy over the street. The same effect is also desired in parking lots and internal circulation streets; rows of street trees should be included in these areas where feasible.

All development fronting on public or private streets shall be required to plant street trees in accordance with the following standards and chosen from the recommended list of street trees.

1. Location of Street Trees. Street trees shall be located in the designated planting strip or street tree wells between the curb and sidewalk, or behind the sidewalk in cases where a planting strip or tree wells are or will not be in place. Street trees shall include irrigation, root barriers, and generally conform to the standards established by the Community Development Department.

Finding:

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The street trees are proposed to be located behind the sidewalk and on the private property Due to lack of right of way behind the sidewalk. All street trees will have irrigation and will conform to the standards of the Community Development Department.

2. Spacing and Placement of Street Trees. All street tree spacing may be made subject to special site conditions that may, for reasons such as safety, affect the decision. Any such proposed special condition shall be subject to the Staff Advisor's review and approval. The placement, spacing, and pruning of street trees shall meet all of the following requirements.

a. Street trees shall be placed at the rate of one tree for every 30 feet of street frontage. Trees shall be evenly spaced, with variations to the spacing permitted for specific site limitations, such as driveway approaches.

Finding:

The proposed street trees are placed at 30-foot intervals and are evenly spaced along the Randy Street and Helman Street frontages where shown on the landscape plans.

b. Street trees shall not be planted closer than 25 feet from the curb line of intersections of streets or alleys, and not closer than ten feet from private driveways (measured at the back edge of the sidewalk), fire hydrants, or utility poles.

Finding:

No street trees are proposed within 25-feet of the intersection of Randy Street and Helman Street. The proposed street trees are more than ten feet from the driveway. All fire hydrants are also not within ten feet of street trees.

c. Street trees shall not be planted closer than 20 feet to light standards. Except for public safety, no new light standard location shall be positioned closer than ten feet to any existing street tree, and preferably such locations will be at least 20 feet distant.

Finding:

No street trees are proposed within 20-feet of streetlights.

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d. Street trees shall not be planted closer than 2.5 feet from the face of the curb. Street trees shall not be planted within two feet of any permanent hard surface paving or walkway. Sidewalk cuts in concrete for trees, or tree wells, shall be at least 25 square feet; however, larger cuts are encouraged because they allow additional air and water into the root system and add to the health of the tree. Tree wells shall be covered by tree grates in accordance with City specifications.

Finding:

The street trees are proposed behind the existing curbside sidewalk. No tree wells are proposed.

e. Street trees planted under or near power lines shall be selected so as to not conflict with power lines at maturity.

Finding:

The street trees proposed on Helman Street are proposed to be compatible with overhead power lines and are setback so as to provide adequate clearance for the Zelkovas.

f. Existing trees may be used as street trees if there will be no damage from the development which will kill or weaken the tree. Sidewalks of variable width and elevation, where approved pursuant to section 18.4.6.040 Street Design Standards, may be utilized to save existing street trees, subject to approval by the Staff Advisor.

Finding:

The existing, healthy street trees that will not be damaged during construction are proposed to be preserved and counted towards the total number of street trees along the Helman Street and Randy Street frontages where development is proposed.

3. Pruning. Street trees, as they grow, shall be pruned to provide at least eight feet of clearance above sidewalks and 12 feet above street roadway surfaces.

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Finding:

The existing street trees will be pruned to provide adequate clearance above the street.

4. Replacement of Street Trees. Existing street trees removed by development projects shall be replaced by the developer with those from the street tree list approved by the Ashland Tree Commission. The replacement trees shall be of size and species similar to the trees that are approved by the Staff Advisor. See the Ashland Recommended Street Tree Guide.

Finding:

There are three large stature black locust trees on the Helman Street side that are on the private property, not within the public right of way but function like street trees. These trees will be replanted with size and species-appropriate street trees.

F. Parking Lot Landscaping and Screening. Parking lot landscaping, including areas of vehicle maneuvering, parking, and loading, shall meet the following requirements. Single-family dwellings and accessory residential units are exempt from the requirements of subsection 18.4.4.030.F.2, below.

1. Landscaping.

a. Parking lot landscaping shall consist of a minimum of seven percent of the total parking area plus a ratio of one tree for each seven parking spaces to create a canopy effect.

Finding:

There are two parking areas proposed or modified as part of this request. The first is the modified north parking lot, this lot has 18 parking spaces. There are six trees provided that will create a canopy effect. There is more than seven percent of the area for the 18 parking spaces devoted to the landscape area. These areas include three landscape islands, the large bioswale/pond area, and the landscape area to the west of the driveway into the property from Randy Street.

The new parking lot area to the south of the existing Helman Street parking lot consists of three areas with four parking spaces and one area with five spaces. There are two parking lot landscape

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peninsulas and a large bioswale area between the parking spaces in the street which provide more than seven percent of the parking area in landscaping. There are eight trees provided around the parking area.

Parking lot shade trees are proposed to be added to the existing Helman Street parking area in the landscape buffer adjacent Helman Street.

b. The tree species shall be an appropriate large canopied shade tree and shall be selected from the street tree list approved by the Ashland Tree Commission to avoid root damage to pavement and utilities, and damage from droppings to parked cars and pedestrians. See the Ashland Recommended Street Tree Guide.

Finding:

The parking lot shade trees are a mixture of Zelkovas, Maple trees, and Kentucky Coffeetree. These species have large canopies and are not known to cause root damage or droppings onto vehicles or pedestrians.

c. The tree shall be planted in a landscaped area such that the tree bole is at least two feet from any curb or paved area.

Finding:

The trees are at least two feet from any curb or paved areas.

d. The landscaped area shall be distributed throughout the parking area and parking perimeter at the required ratio.

Finding:

The landscape areas are distributed in the parking area and at the perimeter.

e. That portion of a required landscaped yard, buffer strip, or screening strip abutting parking stalls may be counted toward required parking lot landscaping but only for those stalls abutting landscaping as long as the tree species, living plant material coverage, and placement distribution criteria are also met. Front or exterior yard landscaping may not be substituted for the interior landscaping required for interior parking stalls.

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Finding:

There are substantial buffers around the parking area and within the parking lots landscaped with tree species and living plant material distributed to meet the placement standards. The area of landscaping that screens and buffers the parking areas exceed the minimum areas required.

2. Screening.

a. Screening Abutting Property Lines. A five-foot landscaped strip shall screen parking abutting a property line. Where a buffer between zones is required, the screening shall be incorporated into the required buffer strip, and will not be an additional requirement.

Finding:

The parking areas where abutting a property line are proposed to be buffered from the sidewalk by five feet or more.

b. Screening Adjacent to Residential Building. Where a parking area is adjacent to a residential building it shall be set back at least eight feet from the building, and shall provide a continuous hedge screen.

Finding:

There is no parking adjacent to a residential building.

c. Screening at Required Yards.

i. Parking abutting a required landscaped front yard or exterior yard shall incorporate a sight obstructing hedge screen into the required landscaped yard.

Finding:

Where the new parking spaces are proposed along Helman Street, the parking spaces are more than 20-feet from the property line. The parking spaces are 30-feet, 2-inches from the property line. This exceeds the minimum front yard setback. The parking spaces are on the west side of a landscaped bioswale. No hedges are proposed due to the substantial setback and bioswale planting area.

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The redeveloped parking area accessed from Randy Street is setback 24-feet, 4-inches, and substantially exceeds the required 10-foot side yard setback.

ii. The screen shall grow to be at least 36 inches higher than the finished grade of the parking area, except within vision clearance areas, section 18.2.4.040.

Finding:

Not applicable, there are no hedges proposed due to the substantial setback from the parking area to the property lines and the landscape bioswale/pond area between the parking area and the street.

iii. The screen height may be achieved by a combination of earth mounding and plant materials.

Finding:

Not applicable.

iv. Elevated parking lots shall screen both the parking and the retaining walls.

Finding:

The parking area is not proposed to be elevated.

G. Other Screening Requirements. Screening is required for refuse and recycle containers, outdoor storage areas, loading and service corridors, mechanical equipment, and the City may require screening other situations, pursuant with the requirements of this ordinance.

1. Recycle and Refuse Container Screen. Recycle and refuse containers or disposal areas shall be screened from view by the placement of a solid wood fence or masonry wall five to eight feet in height to limit the view from adjacent properties or public rights-of-way. All recycle and refuse materials shall be contained within the screened area.

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Finding:

An eight-foot-tall masonry wall is proposed to limit the view from the public right of way of the refuse and recycle area in the service yard.

2. Outdoor Storage. Outdoor storage areas shall be screened from view, except such screening is not required in the M-1 zone.

Finding:

Not applicable

3. Loading Facilities and Service Corridors. Commercial and industrial loading facilities and service corridors shall be screened when adjacent to residential zones. Siting and design of such service areas shall reduce the adverse effects of noise, odor, and visual clutter upon adjacent residential uses.

Finding:

A new service corridor area is proposed. The corridor area is proposed to be screened using an eight-foot-tall masonry wall straight and secure, chain link fencing.

4. Mechanical Equipment. Mechanical equipment shall be screened by placement of features at least equal in height to the equipment to limit view from public rights-of-way, except alleys, and adjacent residentially zoned property. Mechanical equipment meeting the requirements of this section satisfy the screening requirements in 18.5.2.020.C.4.

Finding:

The mechanical equipment will be screened by the placement of an eight-foot-tall masonry wall limiting view from the public right of way and the adjacent residential zoned properties. The mural on the side of Quad B that is being removed, it is proposed to be relocated to this screening wall.

a. Roof-mounted Equipment. Screening for roof-mounted equipment shall be constructed of materials used in the building's exterior construction and include features such as a parapet, wall, or other sight-blocking features. Roof-mounted solar collection devices are exempt from this requirement pursuant to subsection 18.5.2.020.C.4.

Finding:

Not applicable

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b. Other Mechanical Equipment. Screening for other mechanical equipment (e.g., installed at ground level) include features such as a solid wood fence, masonry wall, or hedge screen.

Finding:

The ground level mechanical equipment within the new service yard will be screened with an eight-foot-tall masonry wall.

H. Irrigation. Irrigation systems shall be installed to ensure landscape success. If a landscape area is proposed without irrigation, a landscape professional shall certify the area can be maintained and survive without artificial irrigation. Irrigation plans are reviewed through a Ministerial process at the time of building permit submittals.

Finding:

There is an irrigation system is proposed. The irrigation system will comply with the water-conserving landscape standards of the city of Ashland.

I. Water Conserving Landscaping. Water has always been a scarce, valuable resource in the Western United States. In the Rogue Valley, winter rains give way to a dry season spanning five to seven months. Lack of water during the dry summer season was a major problem facing early settlers. Their creative solutions greatly altered the development of this region. Talent Irrigation District's and other district's reservoirs and many miles of reticulating canals are an engineering marvel.

Finding:

Water-conserving landscape design has been proposed within the non-turf areas. The plants proposed around the landscape areas excepting the bio Swale are drought tolerant and are suited for the Rogue Valley climate that way.

J. Maintenance. All landscaping shall be maintained in good condition, or otherwise replaced by the property owner; dead plants must be replaced within 180 days of discovery. Replacement planting consistent with an approved plan does not require separate City approval. (Ord. 3158 § 6, amended, 09/18/2018; Ord. 3155 §§ 12, 13, amended, 07/17/2018)

Finding:

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All landscaped areas will be maintained in good condition or will otherwise be replaced.

18.4.4.040 Recycling and Refuse Disposal Areas

A. Recycling. All residential, commercial, and manufacturing developments that are subject to chapter 18.5.2 Site Design Review shall provide an opportunity-to-recycle site for use of the project occupants.

1. Residential. All newly constructed residential units, either as part of an existing development or as a new development, shall provide an opportunity-to-recycle site in accord with the following standards.

Finding:

Not applicable

2. Commercial. Commercial developments having a refuse receptacle shall provide a site of equal or greater size adjacent to or with access comparable to the refuse receptacle to accommodate materials collected by the local sanitary service franchisee under its on-route collection program for purposes of recycling.

Finding:

The Helman Elementary School students and staff recycle as much paper, plastics, food waste, etc., as possible to reduce the flow of materials into the landfill. A refuse receptacle that provides adequate collection area of materials produced at the school including recycling has been provided within the service yard area.

B. Service Areas. Recycling and refuse disposal areas shall be located to provide truck access and shall not be placed within any required front yard or required landscape area.

Finding:

The recycling and refuse disposal area are provided within the new screened service yard that is accessed from the parking lot adjacent to Helman Street. The recycling and refuse disposal areas are not within the front yard or a required landscape area.

C. Screening. Recycle and refuse disposal area screening shall be provided pursuant to section 18.4.4.030.G.1.

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Finding:

An eight-foot-tall masonry wall is proposed to limit the view from the public right of way of the refuse and recycle area in the service yard.

18.4.4.050 - Outdoor Lighting

Finding:

All exterior lighting is attached to the buildings and will be directed on to the subject property. No artificial lighting will be directed to illuminate adjacent residential properties. New light standards within the parking area will be pedestrian-scale and will not illuminate adjacent residential properties.

18.4.4.060 - Fences and Walls

B. Design Standards. Fences, walls, hedges, and screen planting shall meet the following standards, where height is measured pursuant to subsection 18.4.4.060.B.2, below. See Figure 18.4.4.060.B.1 for illustration of maximum fence heights.

Finding:

The majority of the perimeter fencing exists. The fencing at the perimeter of the school area is a six-foot, chain link fence. The existing chain-link fencing is set back more than 20 feet from the front property along Helman Street, more than 10 feet from the Randy Street side of the property, and 10 feet from the Laurel Street side of the property. Since the fence is outside of all of the setback areas it is allowed to exceed the fence height standards from 18.4.4.060.B.2. Metal gates and decorative fencing is proposed to be added. All proposed fencing is outside of setback areas and not subject to the height standards.

18.4.3 Parking Access and Circulation:

Finding:

The elementary school requires 66 vehicle parking spaces.

Per Table 18.4.3.040:

1.5 Spaces per classroom

$17 * 1.5 = 25.5$ (17 Classrooms)

1 space per 75 square feet of public assembly area, whichever is greater

Public Assembly Area:

Gymnasium = $4,880 / 75 = 65.0667$

Cafeteria = $4882 / 75 = 65.0933$

Library = $3720 / 75 = 49.6$

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Required Parking based on the largest assembly space filled to capacity = 66 spaces.

There are 52 parking spaces available on the site. This is non-conforming as the assembly area parking calculations are not increasing due to the new construction but are required for the cafeteria area which is part of the original construction of the cafeteria.

17 spaces in west lot accessed from Randy Street (bus loop)
18 spaces in the north lot at Helman Street and Randy Street
17 spaces in the east lot on Helman Street
Total
52

The proposal increases the onsite parking by 17 spaces through the development of the new south parking area. This increases the allocated 69 total spaces and brings the property into conformance with the parking standards.

The proposed parking area relocation adds Accessible Parking spaces and expands the onsite parking to accommodate the parking demands of the elementary school. The 69 spaces is not a more than ten percent increase and are permissible under AMC 18.4.3.030.B.

Accessible parking spaces as required by Oregon building code and federal regulations will be provided.

The proposal requires one bicycle parking space for every five students. This requires 70, covered bicycle parking spaces. The student capacity has not changed and is not increasing. 12 spaces are existing on-site. The proposal increases the provided parking by 49 spaces which substantially decreases the non-conformity.

18.4.3.080 Vehicle Area Design

A. Parking Location

1. Except for single and two-family dwellings, required automobile parking facilities may be located on another parcel of land, provided said parcel is within 200 feet of the use it is intended to serve.

Finding:

The parking is on parcels owned by Ashland School District

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2. Except as allowed in the subsection below, automobile parking shall not be located in a required front and side yard setback area abutting a public street, except alleys.

Finding:

The parking areas exist along the frontage of Helman Street, between the building and the street. The redeveloped parking area and the new parking area are further away from the street than minimum setback distances.

B. Parking Area Design.

Finding:

The reconstructed and new parking areas are proposed to be designed in accordance with the standards. The proposed parking spaces are 9' X 18' with up to 50 percent of the provided parking spaces as compact.

The parking spaces have the required back up, necessary for the types of spaces, head-in, and angled.

The parking area has been designed to minimize adverse environmental impacts. One shade tree is required for every seven spaces. Fifty percent more shade created by shade trees is proposed to address the microclimatic effects of the parking area. The proposal includes the required shade trees in the reconstructed parking area which has 17 spaces and requires three trees, there are six proposed immediately adjacent to the parking area. There are 17 spaces in the new parking area and eight shade trees are proposed. The existing parking area on Helman does not have shade trees as it is preexisting, non-conforming. There are two new street trees which will also provide shade for the parking area.

The parking lot is designed to capture and treat surface run-off through large, landscape swales.

C. Vehicular Access and Circulation.

Finding:

The proposed access modifications remove extra curb cuts and non-conforming driveway aprons that lack separation from the Randy Street and Helman Street intersection. The proposed layout improves on-site circulation and maintains and improves transportation system safety and operations.

The proposed circulation system accommodates expected traffic on the site and improves, ideally eliminates the problems caused at morning drop off and

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afternoon pick up. The on-site circulation system incorporates street-like features such as sidewalks, plaza areas for gathering, and shade trees. Pedestrian connections on the site and adjacent sidewalks are proposed.

The distance from a street intersection to the driveway on Randy Street is being increased to provide more than the required minimum 35-feet through the removal of a 25-foot wide apron that is at the intersection. A new, 20-foot wide apron 210-feet from the intersection is proposed to access the redeveloped parking area from Randy Street.

The existing driveway apron on Helman Street, nearest the intersection, does not comply with standards but is not proposed to be altered. The other two driveway aprons meet spacing standards. A new driveway apron proposed to exit the new, south parking lot. This driveway apron is more than 100-feet from the nearest apron.

No obstructions will be placed in the vision clearance areas of the driveways.

D. Driveways and Turn-Around Design.

Finding:

All proposed driveways will be 20-feet wide. There are pedestrian sidewalks provided adjacent to the driveway. Adequate aisles are provided so that all vehicles enter the street in a forward manner.

No obstructions may be placed in the vision clearance areas.

E. Parking and Access Construction.

Finding:

All required parking areas, aisles, turn-arounds, and driveways will be paved with an asphalt surface.

The reconstructed and new parking areas, aisles, and turn arounds will have an onsite collection, treatment, and detention of drainage waters.

All parking spaces will be clearly and permanently marked.

The reconstructed and new parking areas are not adjacent to the street and are more than 20-feet from the street, a site obscuring hedge or other site obscuring barrier is not proposed.

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There is more than seven percent landscaping in and immediately adjacent to the parking areas. The landscaping is uniformly distributed throughout the parking area and provided with irrigation facilities and protective curbs.

18.4.3.090 Pedestrian Access and Circulation

Finding:

The proposal is intended to provide the students, staff, parents, a safe, reasonably direct, and convenient walkway connections between primary building entrances and all adjacent streets.

18.4.5.030 Tree Protection.

Finding:

The trees proposed for protection and removal were evaluated by a local landscape architect with extensive arborist knowledge. All trees on the tree protection plan will have a six-foot chain link fence installed at the dripline of the trees (or as depicted on the plan) to protect them from the impacts of construction. (See Sheet L.100)

18.4.7 Signs.

Finding:

There is an existing sign program for Helman Elementary School and the Ashland School District (2009-0322) The existing “dragon tile” mural that is on the north side of the administration building is proposed to be relocated to the new screening wall for the service yard. This mural is presently visible from the right of way. See attached graphic of the dragon mural and sketch of location on the screening wall.

4. Designated Creek Protection. Where a project is proposed adjacent to a designated creek protection area, the project shall incorporate the creek into the design while maintaining required setbacks and buffering, and complying water quality protection standards. The developer shall plant native riparian plants in and adjacent to the creek protection zone.

Finding:

Not applicable

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5. Noise and Glare. Artificial lighting shall meet the requirements of section 18.4.4.050. Compliance with AMC 9.08.170.c and AMC 9.08.175 related to noise is required.

Finding:

All artificial lighting will meet the lighting standards.

6. Expansion of Existing Sites and Buildings. For sites that do not conform to the standards of section 18.4.2.040 (i.e., nonconforming developments), an equal percentage of the site must be made to comply with the standards of this section as the percentage of building expansion. For example, if a building area is expanded by 25 percent, then 25 percent of the site must be brought up to the standards required by this document.

Finding:

It can be found that the site development is not required to comply with the standards of section 18.4.2.040.B.1. a. – d. because of 18.4.2.040.B.1.g., that allows for regulations to be waived if the building is not accessed by pedestrians, the code lists specific commercial businesses. The elementary school, though accessed by pedestrians, the pedestrians are students and their guardians, not the general public. All campus visitors, including students, are directed to the front entrance where a security gate for student morning access is proposed and parents and visitors will be allowed in only through the secure office space. The proposed entry plaza area enhances the orientation to the street.

Substantial elements of the site are being brought into conformance with the site development standards. The installation of stormwater detention bioswales, planting of street trees, increasing driveway separation from the intersection, and a 50 percent increase in covered bicycle parking spaces all increase site conformity. The proposed site improvements reduce adverse effects on surrounding property owners and the general public through increased safety. The site modifications, the new classroom structure, and the modified administration building further energy conservation efforts within the City, to enhance the environment for students walking and cycling to campus.

D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Finding:

Adequate city facilities exist to service the proposed new classroom building. The proposal substantially upgrades the storm drainage facilities, where inadequate facilities exist. The Civil engineering plans provide necessary details to demonstrate proposed site development and construction can comply with city standards. See sheets C2.1 Erosion Control Plan, C3.0 Overall Civil Site Plan, and C.4 Overall Grading and Drainage Plan.

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Water: There is an existing six-inch water main in Helman Street. There is also a six-inch main in Randy Street. There are fire hydrants on Randy Street including a hydrant and fire sprinkler vault west of the gymnasium building. There are hydrants on Helman Street. A fire connection vault is proposed to be located adjacent to Helman Street. The water line sizes are substantial and there is 90 PSI at the Helman Street hydrant, which is adequate water needs for the new structure.

Sanitary Sewer: There is an eight-inch sanitary sewer line in Randy Street. There is an 18-inch and 12-inch sanitary sewer line in Helman Street. In discussion with the Wastewater Department Supervisor, there are no capacity issues with the public sanitary sewer lines.

Electrical: There are major overhead electrical facilities along Helman Street. There are private facilities including junction boxes and vaults. To the project team's knowledge, there are no capacity issues. The new structure is designed and engineered to be solar-ready. Area for future solar panel installation locations is shown on the roof plan.

Storm Sewer: There is an 18-inch Storm sewer main in Helman Street. The development proposal includes substantial stormwater quality improvements. There are two large, landscaped bioswales proposed. The final Civil engineering will be designed to the standards of the DEQ MS4 General Permit phase 2. The system will be designed to comply with all of Ashland's specific stormwater quality design standards.

Transportation: There are curbside sidewalks on all frontages. No changes to the non-conforming, curbside sidewalks are proposed.

According to the Transportation System Plan, Laurel Street is classified as a Neighborhood Collector. No changes to the Laurel Street are proposed.

Randy Street is a Residential Street. The proposal removes non-conforming and extra driveways and proposes driveways that comply with controlled access standards. The proposed changes improve pedestrian safety by increasing driveway spacing away from the most heavily used intersection. The proposed changes to the parking area and increasing the length of the driveway and vehicular maneuvering area onsite to facilitate parent drop off and pick up without pushing traffic onto the public streets. The one-way vehicular traffic circulation is proposed which increases student and pedestrian safety.

Helman Street is considered an Avenue. Helman Street along the frontage of the school is not designed to avenue standards. Excepting the proposed driveway curb cut that complies with controlled access standards and the installation of street trees, no changes to the Helman Street frontage are proposed.

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E. Exception to the Site Development and Design Standards.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or

Finding:

The funding for the proposed site modifications and new construction is from a local bond measure with emphasis on accessibility, structural safety, energy efficiency, and campus security of the 1960s elementary school campus. The existing and proposed site development such as parking and vehicular access between the building and the street, large setbacks from the property lines and lack of pedestrian entrances open to the public from the sidewalk, not orienting the new construction to the intersection and not continuing a require an exception to the design standards.

The use of the site as an elementary school could be seen as a unique use of a site as there are only three elementary schools in Ashland. Schools in the current times are not “open” for general business. For student and staff safety and security, access to the campus is highly restricted. The site design standards purpose to create a business environment that is safe and comfortable but does not translate to school development on an existing campus. The existing site layout including locations of existing parking areas and building locations, playground, and basketball courts prevent alterations to the site layout to increase orientation to the street.

The exceptions requested are the minimum necessary to accommodate the redevelopment of the parking area and allow for the construction of a new classroom building.

2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards; or

Finding:

Not applicable, see finding above.

3. There is no demonstrable difficulty in meeting the specific requirements for a cottage housing development, but granting the exception will result in a design that equally or better achieves the stated purpose of section 18.2.3.090.

Finding:

Not applicapble.

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18.5.4.050 Conditional Use Permit

A. Approval Criteria.

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.

The proposal seeks to expand and enlarge a non-conforming development pattern of the parking and vehicular maneuvering area to accommodate a public school in the single-family residential zone. The use of the property as a public school is an allowed use in the zone and the setbacks, lot coverage, building height, and parking conform to the R-1-5 zoning district standards.

Public Schools are addressed in Chapter 9 of the Comprehensive Plan. The proposal adds much-needed gathering and assembly type spaces to accommodate areas for group learning, staff, and community meeting area. This achieves the goals of the Comprehensive Plan to make a maximum effort toward the utilization of present and future educational and recreational facilities and resources through public (bond measure), private (PTO, Ashland Booster Clubs, and community support) and city cooperation. The Comprehensive Plan encourages cooperation between the City and School District when new school facilities are considered or when City action affects the School District, this provides the city discretion to offer leniency instead of strict adherence to the site development standards that apply to non-residential development.

Though the campus will be completely secured with fencing, options to retain community access outside of school hours are being discussed. The primary issue with allowing access outside of school hours is that too many community members allow dogs to run free and dog debris and school settings are highly incompatible. There is a place holder area for a track shown on site plans. This is not part of the project scope.

2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.

Finding:

See the findings above.

3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.

Finding:

The target use in the zone is residential development with the potential for around 50 residential parcels. The proposed expand and enlarge the parking and vehicular areas that are between the existing

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structures and the street, and between the proposed classroom building and the street will not have any greater adverse material effects on the livability of the impact area than a 50+ residential subdivision.

a. Similarity in scale, bulk, and coverage.

Finding:

This is a challenging criterion to address since the conditional use permit is to expand and enlarge the parking and vehicular areas that are between the existing structures and the street, and between the proposed classroom building and the street. Due to the nature of the use, the campus school buildings are not similar in scale and bulk. The site does have less coverage than allowed in the residential zone.

b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.

Finding:

The proposed generation of traffic and the effects on the surrounding streets from the elementary school will not be negatively impacted. It can be found the proposed modified parking lot at the intersection of Randy Street and Helman Street and increased intersection spacing for the access driveway will improve vehicular circulation from the streets through the property. The proposed layout facilitates one-way vehicular traffic and provides additional on-site, parent pick up and drop off areas. Additional covered bicycle parking is proposed to provide secure parking for riders.

c. Architectural compatibility with the impact area.

Finding:

The proposed structure is architecturally compatible with the other Helman Elementary School structures. The impact area is underdeveloped or residential which does not provide any basis for comparison of architectural compatibility.

d. Air quality, including the generation of dust, odors, or other environmental pollutants.

Finding:

The proposed expansion of the non-conforming parking areas and circulation between the building and the street will not have greater adverse negative impacts on air quality, including the generation of dust, odors, or other environmental pollutants when compared to residential uses. The proposed stormwater quality treatment structures will substantially reduce environmental pollutants. The new classroom building is designed to be solar-ready, has dedicated recycling areas.

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e. Generation of noise, light, and glare.

Finding:

The proposed parking lot expansion and new school building will not have a greater generation of noise, light, or glare than the existing campus buildings mechanical equipment and will not create more light or glare than what a large residential would generate. The proposed service yard and mechanical equipment will have visual and noise barriers.

f. The development of adjacent properties as envisioned in the Comprehensive Plan.

Finding:

The elementary school will have no discernable impact on the development of adjacent properties as envisioned in the comprehensive plan.

4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.

Finding:

Public schools are a permitted use in the residential zone.

18.5.7.040 Tree Removal Permit.

Tree That is Not a Hazard.

a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.

Finding:

The 12 trees are proposed for removal to permit the applicant to be consistent with other applicable ordinance requirements and standards applicable to the Site Design Standards.

b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.

Finding:

The tree removals will not have significant negative impacts on erosion, soil stability, the flow of surface waters, protection of adjacent trees, or existing windbreaks. Areas from where trees are removed will be redeveloped with structures, hardscaping, and re-landscaped.

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c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.

Finding:

There are several trees within 200-feet of the subject property. The proximity to the heavily vegetated Ashland Creek area provides substantial species diversity, canopy coverage, and tree densities. The proposed development replaces canopy, tree densities, sizes, and species diversity.

d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.

Finding:

No residential component is part of the application.

e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

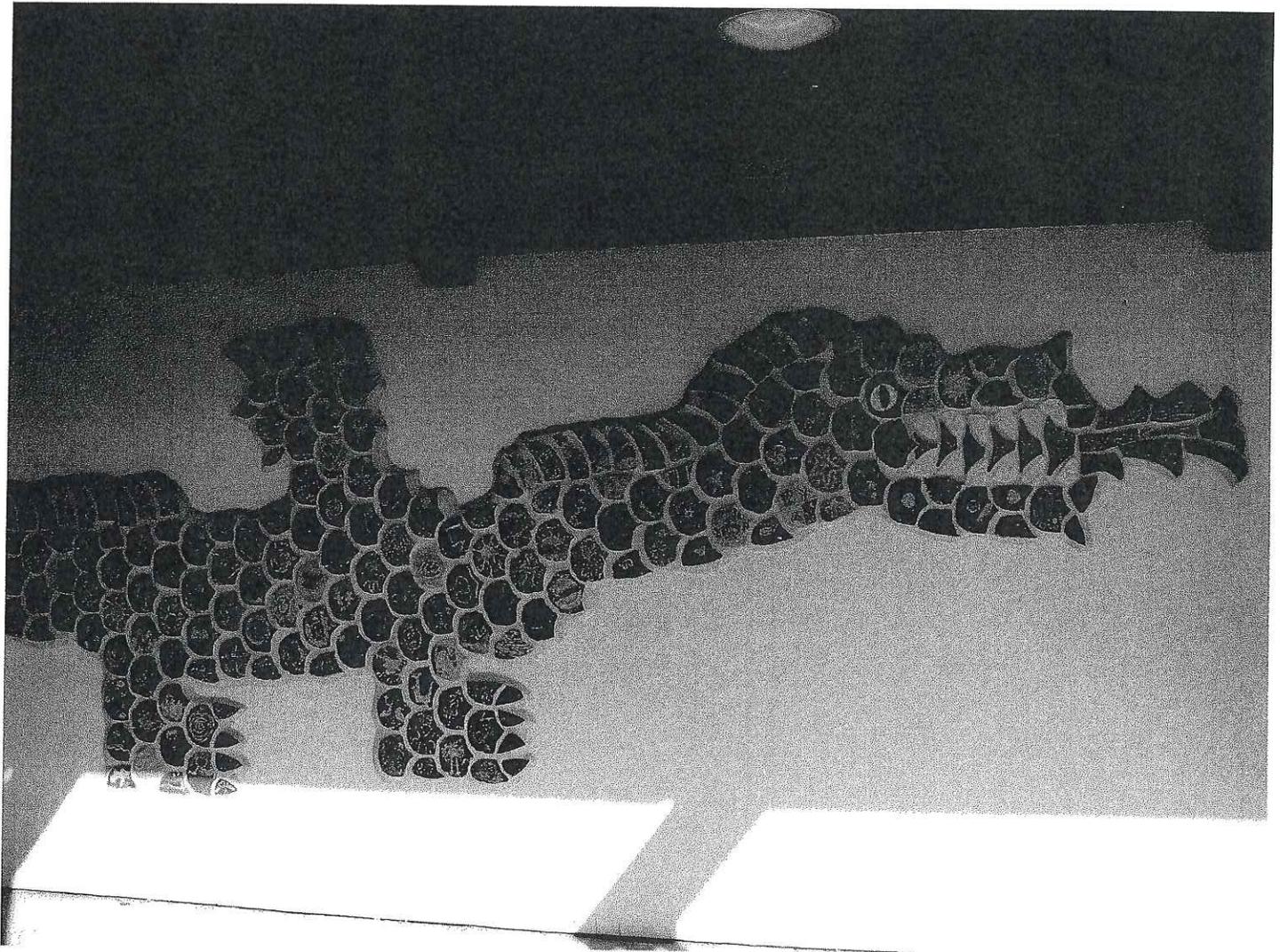
Finding:

Mitigation trees are proposed throughout the property. There are 12 significant trees proposed for removal. The landscape plan calls for over 50 replacement trees. These include Kentucky Coffeetrees, Zelkova, flowering cherries, maple, birch, and Lindens. Required mitigation of 12 removed trees, is achieved through the installation of the required street trees and the proposed 26 shade trees for the parking areas to reduce the microclimatic impacts of the pavement.

Attachments:

- Dragon Tile Mural
- Sketch of mechanical service yard screen
- Cover Sheet (G0.00)
- Standards Sheet (G0.01)
- Civil Engineering Sheets (C1.2 – C6.1)
- Landscape Plan Sheets (L1.00 – L3.01)
- Architectural Renovation Sheets (AR2.01 and AR3.01)
- Architectural Plan Sheets (A1.01 – A2.03; A3.01; A3.02; A4.01; A4.02 and A9.01)

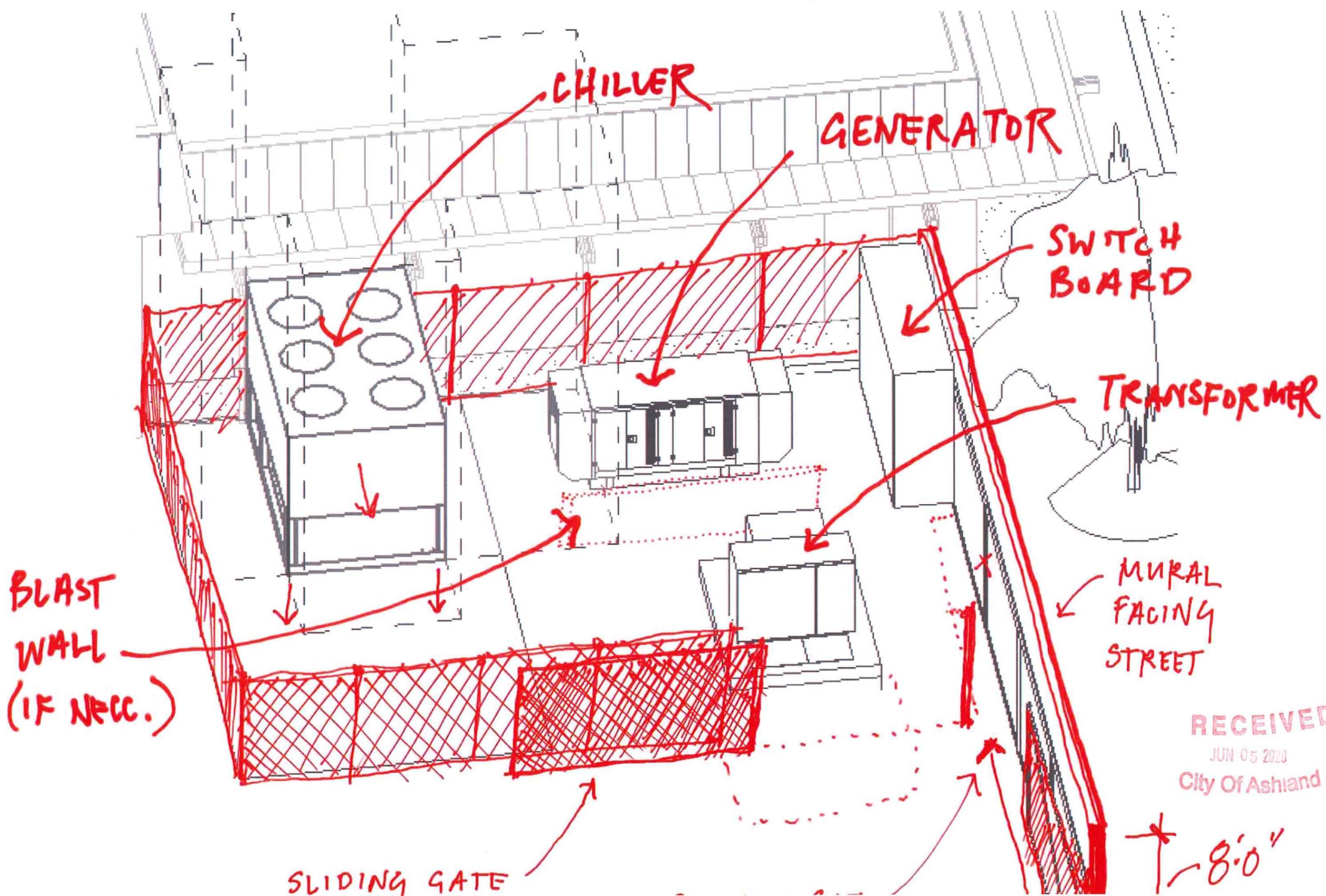
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① "DRAGON TILE" MURAL RECEIVER

JUN 05 2020

City Of Ashland



CHILVER

GENERATOR

SWITCH BOARD

TRANSFORMER

BLAST WALL (IF NECC.)

MURAL FACING STREET

SLIDING GATE

RECEIVED
JUN 05 2020
City Of Ashland

8'0"

ABBREVIATIONS

| | | | |
|-----------|--|--------|--------------------------------------|
| ACT | ACOUSTICAL CEILING TILE | MECH | MECHANICAL |
| ADA | AMERICANS WITH DISABILITIES ACT | MFG | MANUFACTURING |
| AFF | ABOVE FINISH FLOOR | MFR | MANUFACTURER |
| AFP | ACOUSTICAL FIBERBOARD PANEL | MIN | MINIMUM |
| AL | ALUMINUM | MTL | METAL |
| ANSI | AMERICAN NATIONAL STANDARDS INSTITUTE | MP | METAL PANEL |
| APPROX | APPROXIMATELY | NA | NOT APPLICABLE |
| ASTM | AMERICAN SOCIETY FOR TESTING MATERIALS | NIC | NOT IN CONTRACT |
| ARCH | ARCHITECTURAL | NO | NUMBER |
| @ | AT | NTS | NOT TO SCALE |
| AWC | ACOUSTIC WALL COVERING | OC | ON CENTER |
| BO | BOARD | OFCI | OWNER FURNISHED CONTRACTOR INSTALLED |
| BLDG | BUILDING | OFOI | OWNER FURNISHED OWNER INSTALLED |
| CB | CATCH BASIN | OH | OVERHEAD |
| CJ | CONTROL JOINT | OP | OPPOSITE |
| CMU | CONCRETE MASONRY UNIT | OSSC | OREGON STRUCTURAL SPECIALTY CODE |
| COL | COLUMN | P | PAINT |
| CONC | CONCRETE | PB | PUSH BUTTON |
| CONT | CONTINUOUS | PL | PLASTIC LAMINATE |
| COORD | COORDINATE | PLAM | PLASTIC LAMINATE |
| CPT | CHAIR TILE | PLYWD | PLYWOOD |
| CT | CERAMIC TILE | PS | PROJECTION SCREEN |
| CL | CENTER LINE | PR | PAIR |
| DEG | DEGREE | PT | PRESSURE TREATED |
| DIA OR Ø | DIAMETER | PVC | POLYVINYL CHLORIDE |
| DIM | DIMENSION | RAF | RUBBER ATHLETIC FLOORING |
| DN | DOWN | RB | RUBBER BASE |
| DR | DOOR | RD | ROOF DRAIN |
| DS | DOWNSPOUT | REF | REFERENCE |
| DTL | DETAIL | REQD | REQUIRED |
| (E) | EXISTING | RF | RUBBER FLOORING |
| EL / ELEV | ELEVATION | RM | ROOM |
| EMT | ELECTRICAL METAL TUBING | RO | ROUGH OPENING |
| EPF | EPOXY FLOOR PAINT | RR | RESTROOM |
| EQ | EQUAL | SAMF | SELF-ADHERING MEMBRANE FLASHING |
| EXP JT | EXPANSION JOINT | SC | SEALED CONCRETE |
| EXT | EXTERIOR | SDI | STATIC DISSIPATIVE TILE |
| FC | FIBER CEMENT | SECT | SECTION |
| FD | FLOOR DRAIN | SF | SQUARE FEET |
| FE | FIRE EXTINGUISHER | SHT | SHEET |
| FEC | FIRE EXTINGUISHER IN CABINET | SIM | SIMILAR |
| FF | FINISH FLOOR | SM | SHEET METAL |
| FFHB | FROST FREE HOSE BIB | SS | STAINLESS STEEL |
| FOM | FACE OF MASONRY | STD | STANDARD |
| FOS | FACE OF STUD | SIF | SEAMLESS TROWELED FLOORING |
| FRP | FIBERGLASS-REINFORCED PLASTIC | STL | STEEL |
| FT | FOOT, FEET | STRUCT | STRUCTURAL |
| GA | GAUGE | STV | SMART TV |
| GALV | GALVANIZED | TB | TACK BOARD |
| GYP | GYPSUM | TBD | TO BE DETERMINED |
| GWB | GYPSUM WALL BOARD | TEMP | TEMPERED |
| GWS | GLASS WRITING SURFACE | TO | TOP OF |
| HB | HOSE BIBB | TP | TOILET PARTITION |
| HDWR | HARDWARE | TS | TUBE STEEL |
| HM | HOLLOW METAL | TWS | VINYL-WRAPPED TACKABLE WALL SURFACE |
| HS | HOLLOW STEEL | TYP | TYPICAL |
| HORIZ | HORIZONTAL | UL | UNDERWRITERS LABORATORY |
| HT | HEIGHT | UNO | UNLESS NOTED OTHERWISE |
| IN | INCH, INCHES | VCT | VINYL COMPOSITION TILE |
| INSUL | INSULATION | VERT | VERTICAL |
| INT | INTERIOR | VIF | VERIFY IN FIELD |
| JT | JOINT | W/ | WITH |
| LAM | LAMINATED | WD | WOOD |
| LVT | LUXURY VINYL TILE | WCM | WALK OFF MAT |
| MAX | MAXIMUM | WRB | WEATHER RESISTIVE BARRIER |

SYMBOLS

| | |
|--|------------------------------------|
| | GRID BUBBLE |
| | ROOM TAG |
| | OCCUPANCY TAG |
| | WALL ASSEMBLY TAG |
| | WALL ASSEMBLY TYPE STUD WIDTH |
| | EXTERIOR/INTERIOR ELEVATION TAG |
| | SECTION TAG |
| | KEYNOTE TAG |
| | WINDOW TAG |
| | DOOR TAG |
| | CASEWORK TAG |
| | TOILET ACCESSORY TAG |
| | ELEVATION HEIGHT TAG |
| | DETAIL TAG |
| | EXIT SIGN TAG |
| | FLOOR MATERIAL TAG |
| | PATTERN DIRECTION TAG |
| | BID ALTERNATE TAG |
| | REVISION TAG |
| | CEILING HEIGHT TAG |

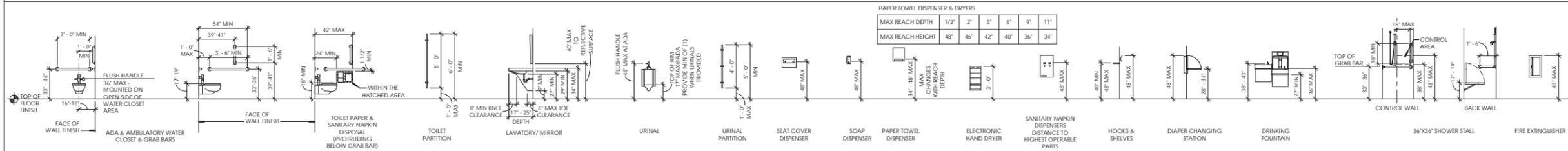
BBT ARCHITECTS
 1140 SW Simpson Ave. - Suite 200
 Portland, OR 97205
 T 503.582.5535 | F 503.589.8033

NOT FOR CONSTRUCTION

**ASHLAND SCHOOL DISTRICT
 HELMAN ELEMENTARY SCHOOL ADDITION & RENOVATION**

705 HELMAN ST.
 ASHLAND, OR 97520

ADA STANDARDS: GENERAL REQUIREMENTS FOR RESTROOM FIXTURES AND ACCESSORIES



1 ADA STANDARDS
 G0.01 SCALE: 1/4" = 1'-0"

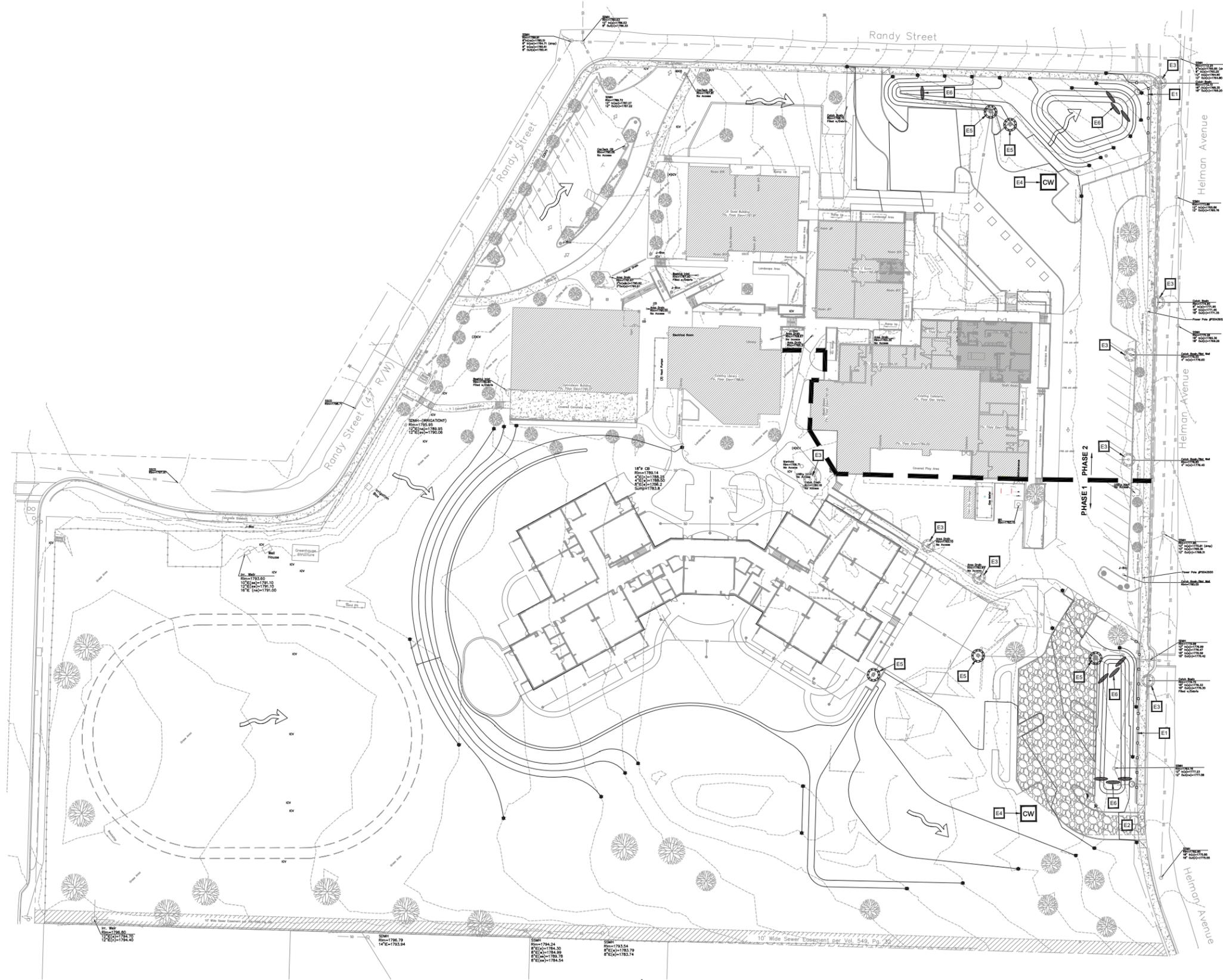
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Project Number 1907
 Date 06.05.2020

LAND USE

STANDARDS SHEET

G0.01



PLAN VIEW - EROSION CONTROL PLAN
SCALE: 1" = 30' - 0"

ESCP LEGEND

- E1** INSTALL SEDIMENT FENCE PER ODOT DETAIL RD1040 ON SHEET C2.2
 - E2** INSTALL TEMPORARY CONSTRUCTION ENTRANCE (TCE) PER PER ODOT DETAIL RD1000 ON SHEET C2.2
 - E3** INSTALL INLET PROTECTION ON EXISTING CATCH BASINS PER ODOT DETAIL RD1010 ON SHEET C2.2
 - E4** INSTALL CONCRETE WASH
 - E5** INSTALL INLET PROTECTION ON NEW CATCH BASINS PER ODOT DETAIL RD1010 ON SHEET C2.2
 - E6** INSTALL ROCK CHECK DAM PER ODOT DETAIL RD1005 ON SHEET C2.2
 - CW** EX. SURFACE FLOW DIRECTION
- APPROXIMATE AREAS OF CUT
(NOTE: CUT/FILL AREAS REPRESENT EXISTING GROUND ELEVATION VERSUS FINISHED GRADE)
- APPROXIMATE AREAS OF FILL

WET WEATHER CONSTRUCTION

THE SITE SOILS ARE CONSIDERED VERY MOISTURE SENSITIVE AND, AS SUCH, ARE SUSCEPTIBLE TO DISTURBANCE BY CONSTRUCTION EQUIPMENT, PARTICULARLY DURING PERIODS OF WET WEATHER. DURING WET WEATHER, THE CONTRACTOR SHALL MINIMIZE TRAFFIC ON PREPARED SOIL SUBGRADE AREAS. IF THE SITE SOILS ARE EXPOSED DURING WET WEATHER, THE USE OF CRUSHED ROCK PLACED AS ENGINEERED FILL IN THE BOTTOM OF THE EXCAVATIONS MAY BE NECESSARY TO PROTECT THE SUBGRADE. THE GRADING CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO LIMIT SURFACE DISTURBANCE AND PROTECT THE SITE GRADING AREA FROM EXCESSIVE RUNOFF EROSION.

ESCP RESPONSIBILITY

IT IS THE INTENT OF THIS TEMPORARY EROSION AND SEDIMENT CONTROL PLAN THAT STORM WATER RUNOFF BE CONTROLLED AT ALL TIMES TO PREVENT SOIL EROSION AND TO MAINTAIN WATER QUALITY. ANY AND ALL MEASURES NECESSARY TO DO SO SHALL BE EMPLOYED BY THE CONTRACTOR.

1. REGARDLESS OF SITE, WEATHER, SOIL OR OTHER CONDITIONS, THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ENSURING THAT EROSION DOES NOT OCCUR ON THE SITE AND THAT POLLUTED OR SILT-LADEN RUNOFF DOES NOT LEAVE THE SITE OR ENTER INTO ANY CREEK, STREAM, WETLAND OR WATER BODY ON THE SITE.
2. BEYOND THE MINIMUM REQUIREMENTS SHOWN ON THIS PLAN, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SELECTING AND IMPLEMENTING APPROPRIATE METHODS, "BEST MANAGEMENT PRACTICES" (BMPs), FOR STORM WATER TREATMENT AND CONTROL THAT MEET THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTION.
3. THE CONTRACTOR SHALL REPORT ALL WATER QUALITY CONCERNS AND ACTIVITIES TO THE PROJECT ENGINEER. IN THE EVENT THAT THE INSTALLED WATER QUALITY CONTROL MEASURES ARE INEFFECTIVE AT CONTROLLING EROSION AND SEDIMENT, THE CONTRACTOR SHALL IMMEDIATELY REPORT TO AND CONSULT WITH THE PROJECT ENGINEER TO FIND AN APPROPRIATE REMEDY. ALL CONSTRUCTION ACTIVITIES, WITH THE EXCEPTION OF EROSION AND SEDIMENT CONTROL MEASURES, SHALL CEASE UNTIL SUCH TIME AS THE WATER QUALITY IS BROUGHT UNDER CONTROL.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MONITORING WEATHER FORECASTS AND ANTICIPATING STORM ACTIVITY AND SHALL SCHEDULE ALL PROJECT ACTIVITIES IN ANTICIPATION OF THE WEATHER.
5. ALL SUPPLIES AND MATERIALS NECESSARY FOR IMPLEMENTING BMPs SHALL BE STORED ON SITE AND SHALL BE IMMEDIATELY AVAILABLE FOR USE. SUCH SUPPLIES AND MATERIALS SHALL INCLUDE, BUT NOT BE LIMITED TO, STRAW BALES OR OTHER MULCHING MATERIAL, SILT FENCING AND STAKES, FILTER FABRIC, ETC.
6. DURING AND AFTER RUNOFF PRODUCING STORM EVENTS, CONTRACTOR SHALL MONITOR ALL EROSION CONTROL MEASURES AND SHALL PRIORITIZE IMPLEMENTATION AND MAINTENANCE OF EROSION AND SEDIMENT CONTROL MEASURES ABOVE ALL OTHERS.

SEQUENCING

1. ROUGH GRADE POND AND SWALE AREAS TO 12" BELOW FINISHED GRADE.
2. INSTALL STORM CONVEYANCE PIPES AS SHOWN ON C4.8.
3. INSTALL TEMPORARY INLET PROTECTION AND ROCK CHECK DAMS AS SHOWN ON THIS SHEET.
4. MONITOR AND REPLACE CHECK DAMS AND INLET PROTECTION AS NEEDED DURING CONSTRUCTION. REMOVE EXCESS SILT AND DEBRIS AS REQUIRED BY EROSION CONTROL INSPECTOR.
5. IMPORT AND PLACE PLANTING SOIL PER DETAIL AND SPECS ON C6.1.
6. PROVIDE TEMPORARY MULCH OR MATTING PER C6.1 IF REQUIRED BY EROSION CONTROL INSPECTOR.
7. INSTALL PLANTS AND VEGETATION PER LANDSCAPE PLAN AND SPECS ON C6.1.
8. UPON APPROVAL BY EROSION CONTROL INSPECTOR AND CITY OF ASHLAND, REMOVE TEMPORARY INLET PROTECTION AND CHECK DAMS.

NOTE: IMPLEMENT EROSION CONTROL MEASURES PER EROSION AND SEDIMENT CONTROL PLANS PRIOR TO VEGETATION BEING DISTURBED. CONTACT CITY OF ASHLAND ENGINEERS AT FOR EROSION CONTROL INSPECTION PRIOR TO THE START OF GRADING OR EXCAVATION.

BBT ARCHITECTS
1160 NW Simpson Ave., Suite 100
Bend, Oregon 97702
1.541.382.5535 | 1.541.389.8033



Consultant
POWELL engineering + consulting
1160 NW Simpson Ave., Suite 100
Bend, Oregon 97702
541.613.0723 phone
www.powellengineeringconsulting.com

VERIFY SCALES
Vertical Scale: 1" = 10'-0"
Horizontal Scale: 1" = 30'-0"

ASHLAND SCHOOL DISTRICT
Helman Elementary School Addition & Renovations

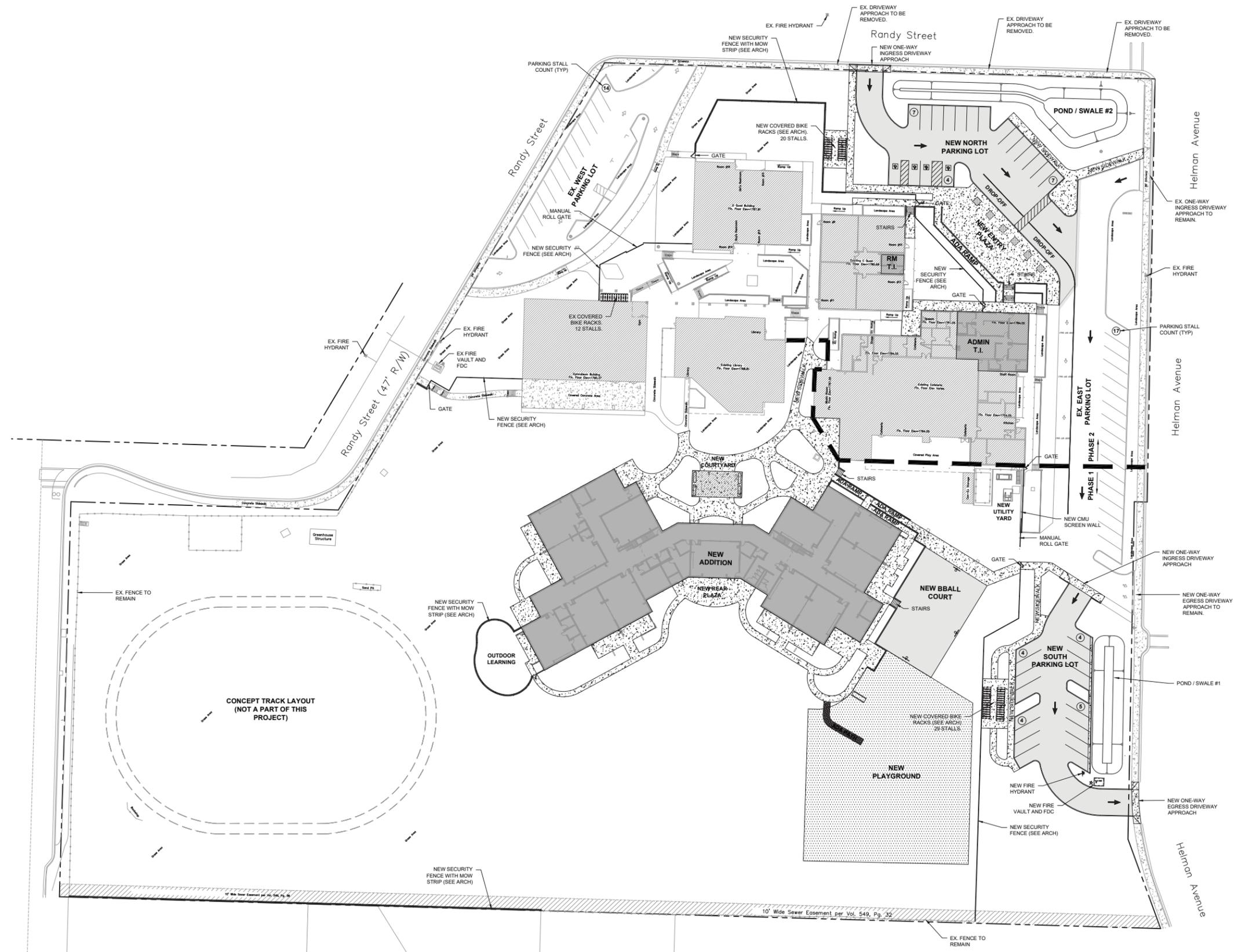
705 HELMAN ST.
ASHLAND, OR 97520

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Project Number 1907
Date 06.05.2020

LAND USE
EROSION CONTROL PLAN

C2.1



PLAN VIEW - OVERALL CIVIL SITE PLAN
SCALE: 1" = 30' - 0"



PARKING CALCULATIONS

EXISTING PARKING STALLS PRIOR TO REDEVELOPMENT:
TOTAL STALLS = 49 STALLS

PARKING STALLS FOLLOWING REDEVELOPMENT:

| | |
|-----------------------|------------------|
| W. PARKING LOT = | 14 STALLS |
| N. PARKING LOT = | 18 STALLS |
| E. PARKING LOT = | 17 STALLS |
| S. PARKING LOT = | 17 STALLS |
| TOTAL STALLS = | 66 STALLS |

TOTAL NUMBER OF ADA STALLS = 4 STALLS

GENERAL NOTES

- SEE LANDSCAPE FOR TREE REMOVAL PLAN.
- SEE LANDSCAPE FOR PLANTING AND IRRIGATION PLAN.

BBT ARCHITECTS
REGISTERED PROFESSIONAL ENGINEER
11640 SW Simpson Ave., Suite 100
Bend, Oregon 97702
1.541.382.5535 | 1.541.389.8033



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www.powellengineeringconsulting.com



ASHLAND SCHOOL DISTRICT
Helman Elementary
School Addition & Renovations

705 HELMAN ST.
ASHLAND, OR 97520

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Date 06.05.2020

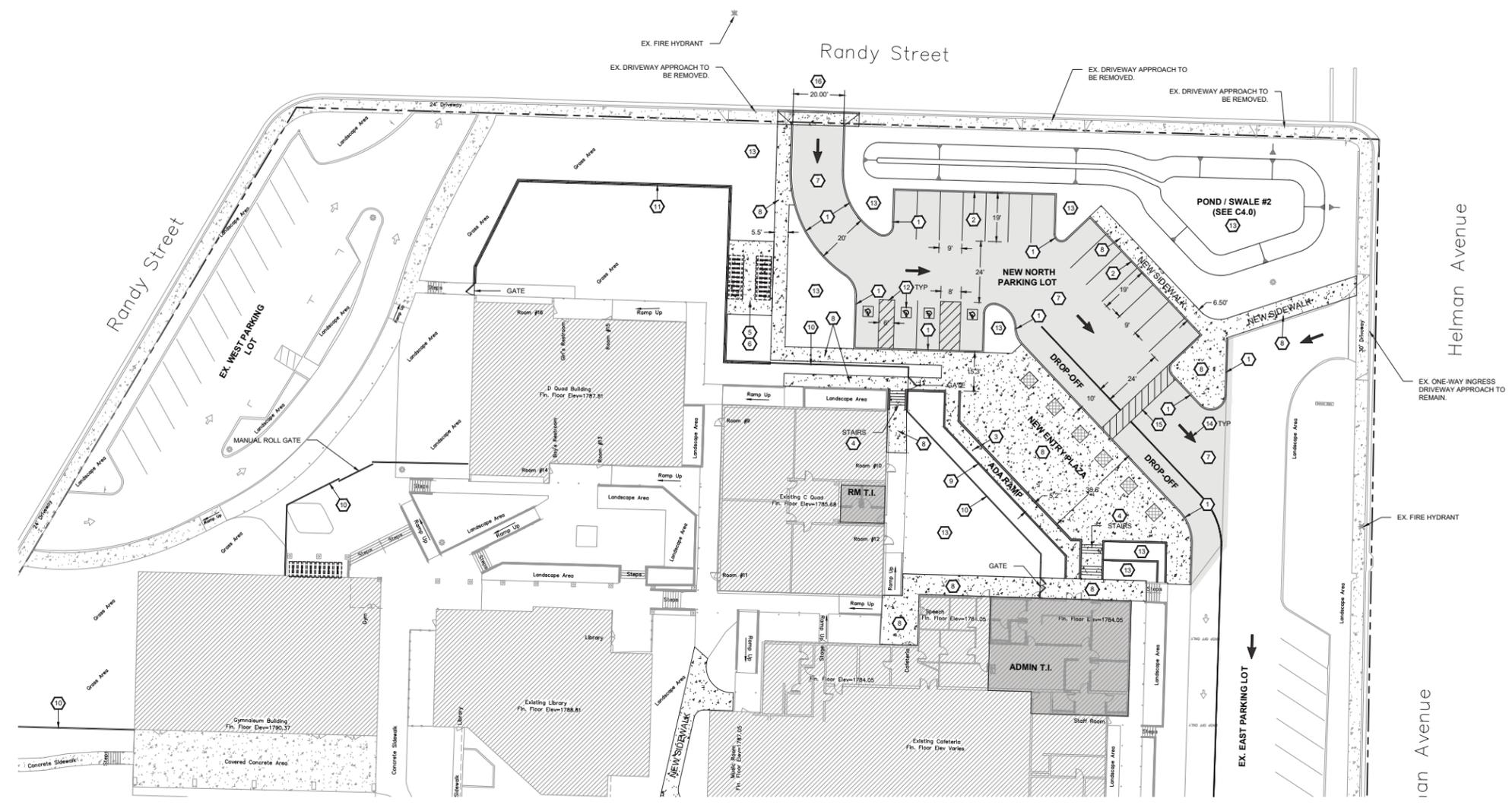
LAND USE
OVERALL CIVIL SITE PLAN

C3.0

Drawing Name: C:\Users\powell\OneDrive - Powell Engineering and Consulting, LLC\Documents\CAD\Drawings\CAD\Drawings\C3.0\Overall Civil Site Plan.dwg | Last Save: June 1, 2020 10:49 AM | Plot Date: June 1, 2020 10:51 AM | By: Todd Powell

Received 6.25.2020

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KEYNOTES

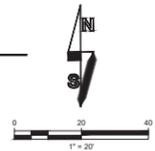
- 1 CONSTRUCT STANDARD VERTICAL CURB.
- 2 CONSTRUCT CURB AND GUTTER.
- 3 CONSTRUCT ADA RAMP WITH HANDRAILS.
- 4 CONSTRUCT CONCRETE STAIRS.
- 5 INSTALL BIKE RACK (SEE L ARCH).
- 6 CONSTRUCT BIKE RACK SHELTER (SEE ARCH).
- 7 CONSTRUCT ASPHALT PAVING SECTION. (SEE GEOTECH REPORT)
- 8 CONSTRUCT CONCRETE SIDEWALK.
- 9 CONSTRUCT RETAINING WALL
- 10 INSTALL 6' TALL SECURITY FENCE GATES AS SHOWN. (SEE L ARCH)
- 11 INSTALL 6' TALL SECURITY FENCE CENTERED ON CONCRETE MOW STRIP. (SEE L ARCH)
- 12 INSTALL ADA SYMBOL PARKING SYMBOL, ACCESS AISLE STRIPING AND SIGNS.
- 13 INSTALL LANDSCAPING AND IRRIGATION (SEE L ARCH)
- 14 INSTALL PAINTED TRAFFIC FLOW ARROW PER MUTCD AND CITY STANDARDS.
- 15 INSTALL PAINTED CROSSWALK PER MUTCD AND CITY STANDARDS.
- 16 CONSTRUCT NEW COMMERCIAL DRIVEWAY APPROACH. WIDTH AS SPECIFIED.

GENERAL NOTES

1. SEE LANDSCAPE FOR TREE REMOVAL PLAN.
2. SEE LANDSCAPE FOR PLANTING AND IRRIGATION PLAN.
3. SEE LANDSCAPE PLANS FOR PATHWAY AND COURTYARD IMPROVEMENTS NOT SHOWN ON THE CIVIL DRAWINGS.

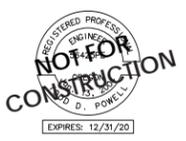
PLAN VIEW - CIVIL SITE PLAN (NORTH)

SCALE: 1" = 20' - 0"



KEYNOTES

BBT ARCHITECTS
 1160 W. SIMPSON AVE. - SUITE 100
 BEND, OREGON 97702
 541.382.5535 | 541.389.8033



Consultant
POWELL
 engineering + consulting
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 Medford, OR 97501
 541.613.0723 phone
 www.powellengineeringconsulting.com

VERIFY SCALES
 ARCHITECTURE: 1/8" = 1'-0"
 CIVIL: 1" = 20'-0"
 MECHANICAL/ELECTRICAL: 1/4" = 1'-0"

ASHLAND SCHOOL DISTRICT
Helman Elementary School Addition & Renovations

705 HELMAN ST.
 ASHLAND, OR 97520

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Project Number 1907
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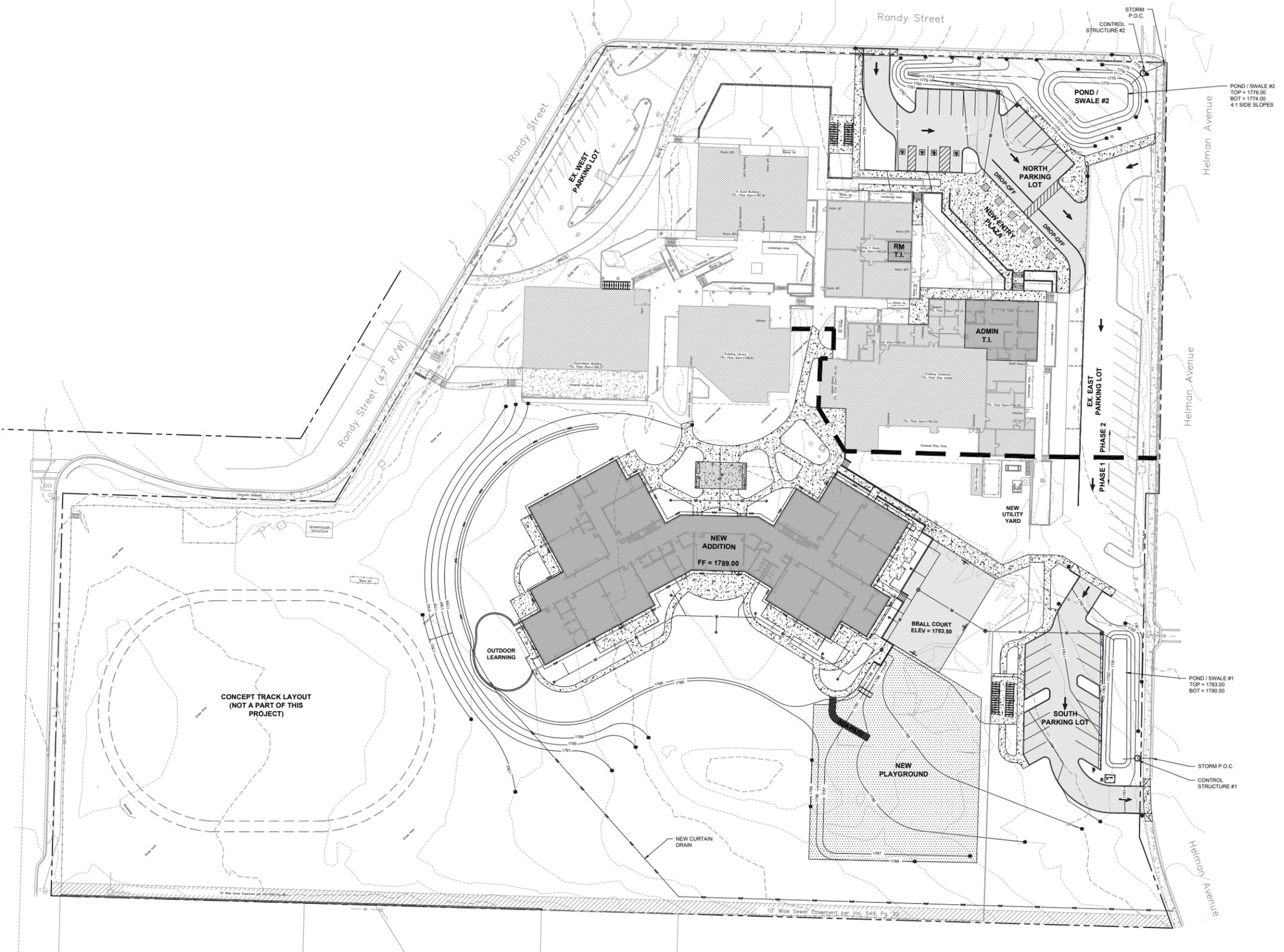
LAND USE
CIVIL SITE PLAN (NORTH)

C3.2

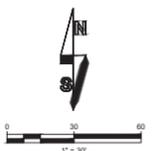
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 Plot Date: June 1, 2020 10:48 AM By: Todd Powell
 Plot Date: April 30, 2020 4:15 PM
 Last Save: April 30, 2020 4:15 PM
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PLAN VIEW - OVERALL GRADING & DRAINAGE PLAN
SCALE: 1" = 30' - 0"



GENERAL NOTES
1. SEE LANDSCAPE FOR TREE REMOVAL PLAN.
2. SEE LANDSCAPE FOR PLANTING AND IRRIGATION PLAN.

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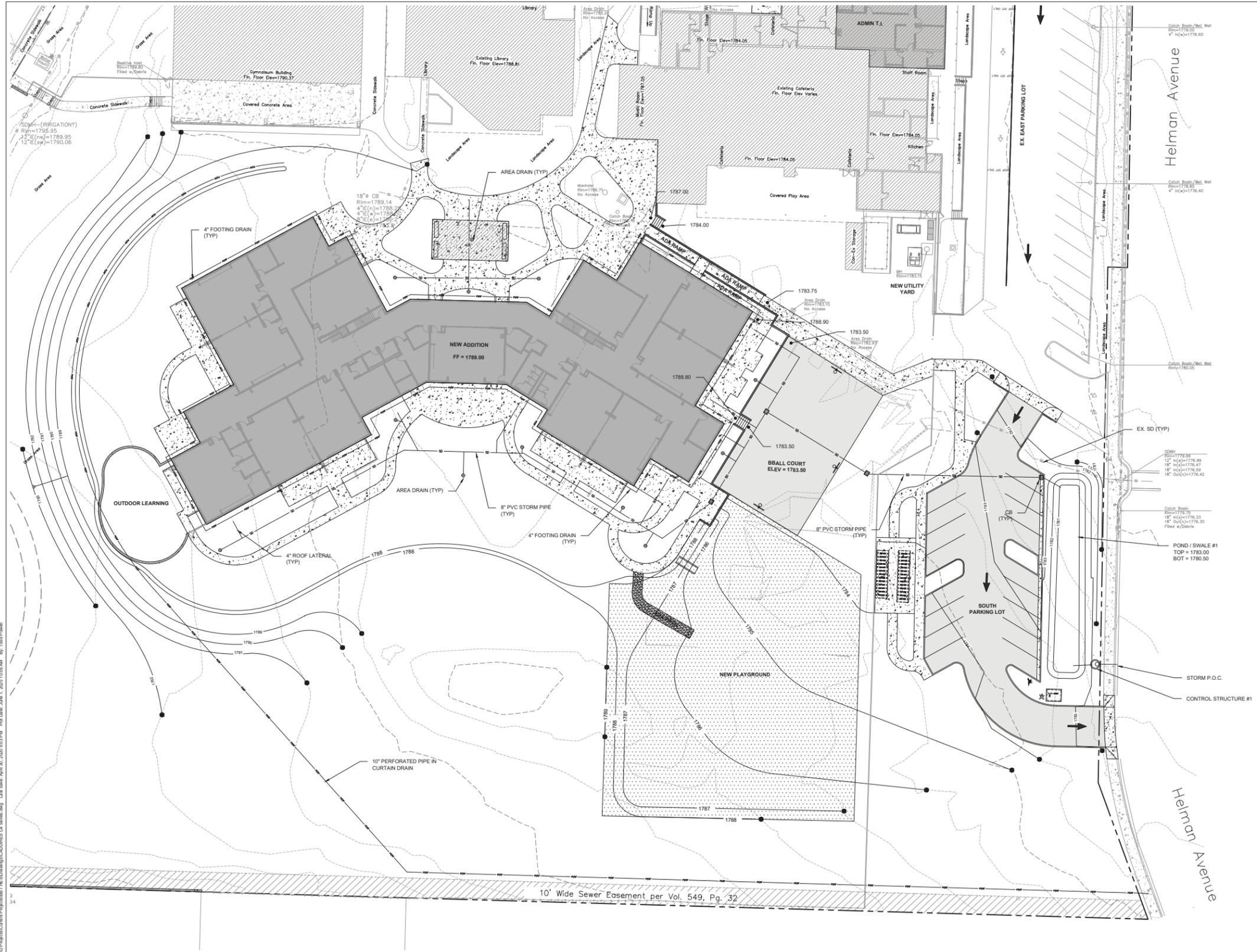
Project Number 1907
Date 06.05.2020

LAND USE
OVERALL GRADING & DRAINAGE PLAN

C4.0

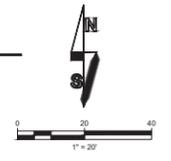
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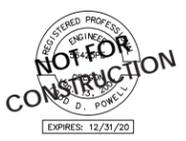


PLAN VIEW - GRADING & DRAINAGE PLAN (SOUTH)

SCALE: 1" = 20' - 0"



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VERIFY SCALES
 ARCHITECTURAL: 1/8" = 1'-0"
 CIVIL: 1" = 40'-0"
 MECHANICAL/ELECTRICAL: 1/8" = 1'-0"
 PLUMBING: 1/4" = 1'-0"
 STRUCTURAL: 1/4" = 1'-0"

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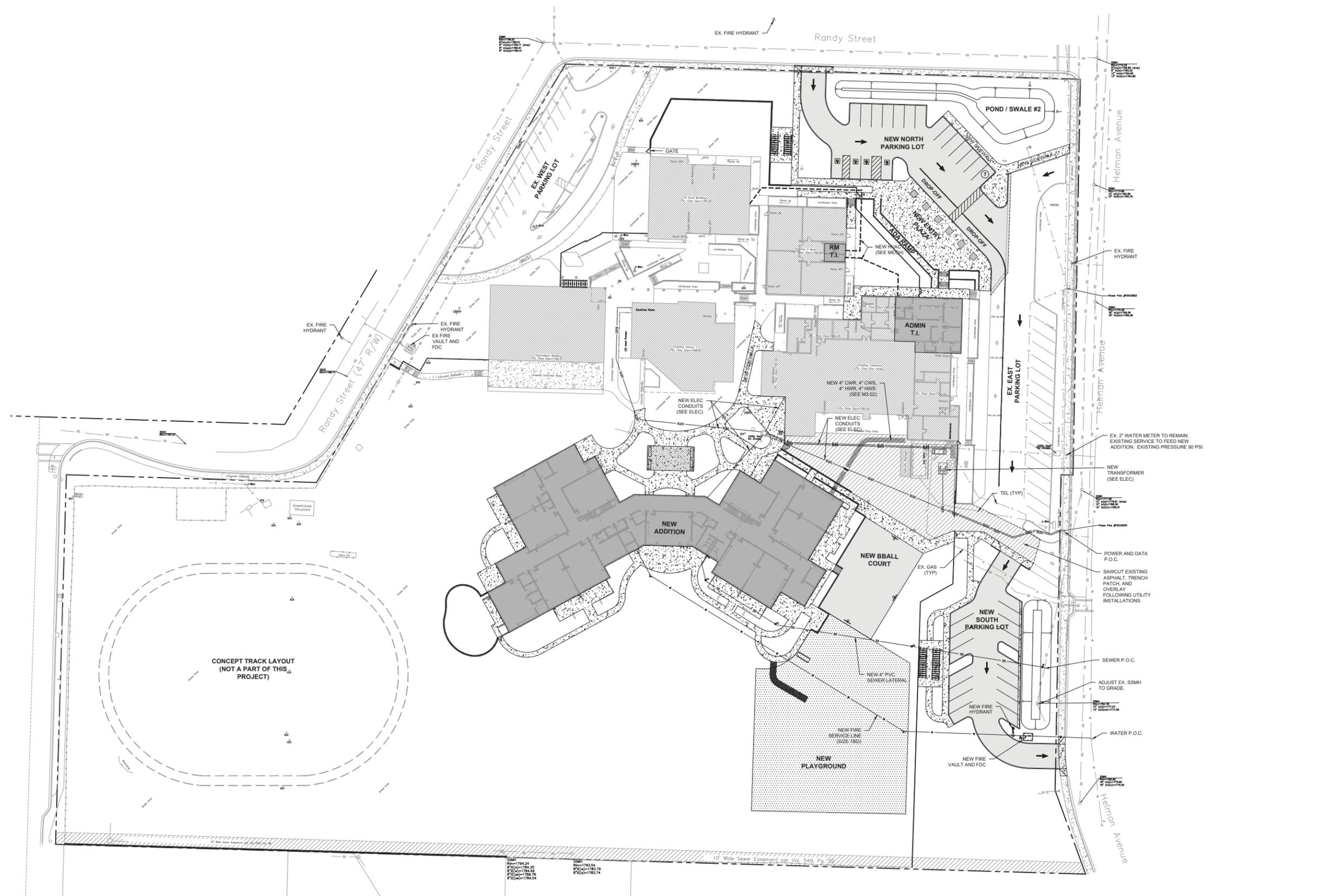
LAND USE
GRADING & DRAINAGE PLAN (SOUTH)

C4.1

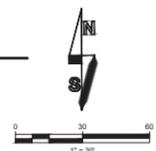
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 Plot Date: June 1, 2020 10:58 AM
 By: Todd Powell

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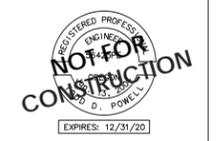
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PLAN VIEW - OVERALL SITE UTILITY PLAN
SCALE: 1" = 30' - 0"



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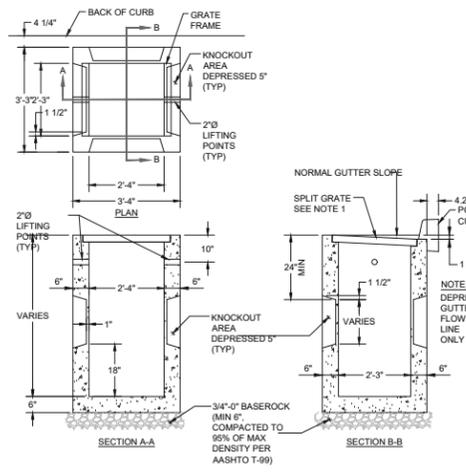
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Date 06.05.2020

LAND USE
OVERALL SITE UTILITY PLAN

C5.0

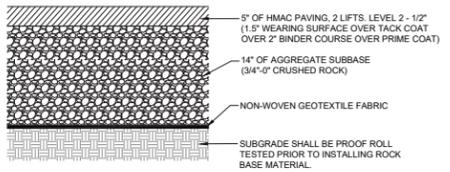
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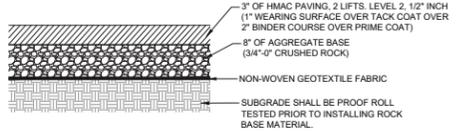
- NOTES**
1. GRATE AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION. GRATE AND FRAME TO BE ODOT G-2 TYPE 2 (BICYCLE SAFE).
 2. FOR PRECAST BOX, CURB MUST BE HAND FORMED 1/2\"/>

1 CATCH BASIN
SCALE: NTS



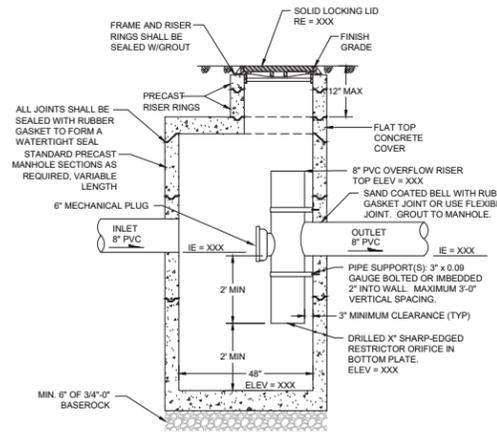
- NOTES**
1. AGGREGATE BASE AND SUBBASE SHALL BE INSTALLED IN MAXIMUM 6\"/>

6 ASPHALT SECTION - DRIVE AISLES
SCALE: NTS



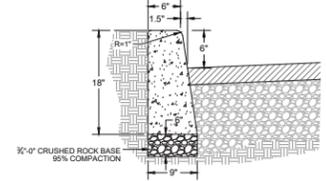
- NOTES**
1. AGGREGATE BASE AND SUBBASE SHALL BE INSTALLED IN MAXIMUM 6\"/>

9 ASPHALT SECTION - PARKING STALLS
SCALE: NTS

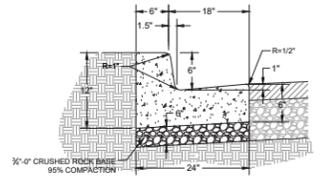


- NOTE**
- ALL PRECAST SECTIONS SHALL CONFORM TO REQUIREMENTS OF ASTM C 478.

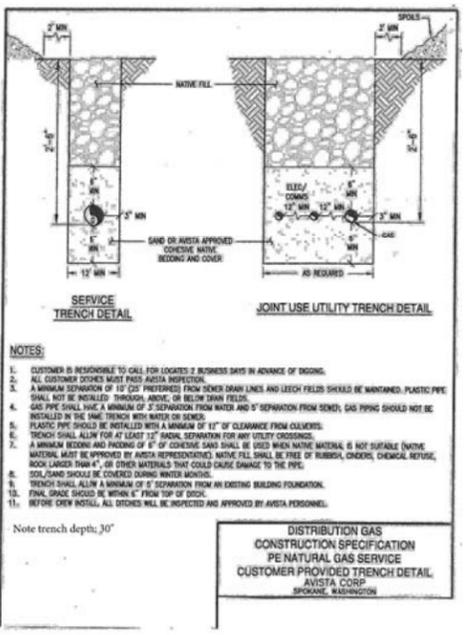
2 CONTROL STRUCTURE MANHOLE
SCALE: NTS



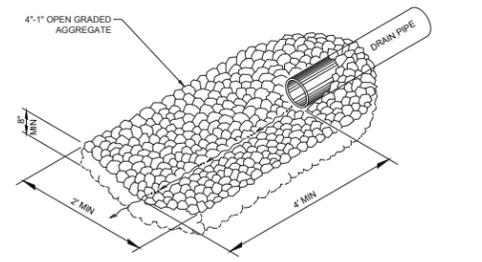
7 VERTICAL CURB
SCALE: NTS



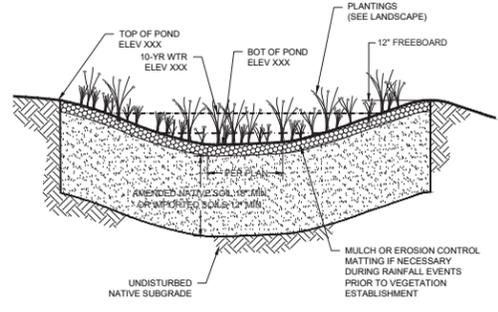
8 CURB & GUTTER
SCALE: NTS



3 AVISTA GAS TRENCH
SCALE: NTS

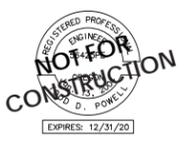


4 RIPRAP DISSIPATION PAD
SCALE: NTS



5 SECTION - POND / BIOSWALE
SCALE: NTS
MODIFIED RVSS BMP 8.01

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LAND USE
PROJECT DETAILS

C6.0

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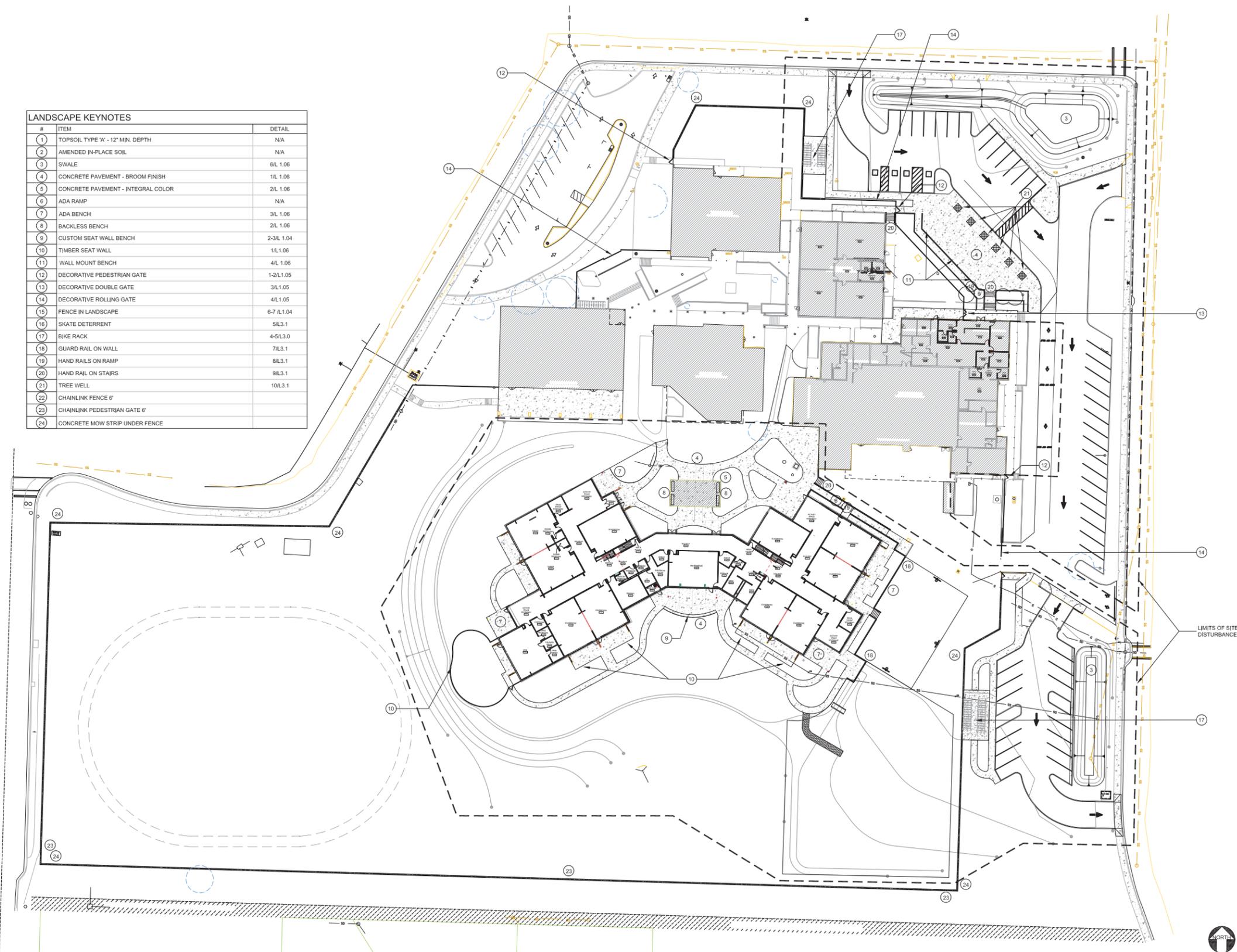
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| # | ITEM | DETAIL |
|----|------------------------------------|-------------|
| 1 | TOPSOIL TYPE 'A' - 12" MIN. DEPTH | N/A |
| 2 | AMENDED IN-PLACE SOIL | N/A |
| 3 | SWALE | 6/L 1.06 |
| 4 | CONCRETE PAVEMENT - BROOM FINISH | 1/L 1.06 |
| 5 | CONCRETE PAVEMENT - INTEGRAL COLOR | 2/L 1.06 |
| 6 | ADA RAMP | N/A |
| 7 | ADA BENCH | 3/L 1.06 |
| 8 | BACKLESS BENCH | 2/L 1.06 |
| 9 | CUSTOM SEAT WALL BENCH | 2-3/L 1.04 |
| 10 | TIMBER SEAT WALL | 1/L 1.06 |
| 11 | WALL MOUNT BENCH | 4/L 1.06 |
| 12 | DECORATIVE PEDESTRIAN GATE | 1-2/L 1.05 |
| 13 | DECORATIVE DOUBLE GATE | 3/L 1.05 |
| 14 | DECORATIVE ROLLING GATE | 4/L 1.05 |
| 15 | FENCE IN LANDSCAPE | 6-7 /L 1.04 |
| 16 | SKATE DETERRENT | 5/L 3.1 |
| 17 | BIKE RACK | 4-5/L 3.0 |
| 18 | GUARD RAIL ON WALL | 7/L 3.1 |
| 19 | HAND RAILS ON RAMP | 8/L 3.1 |
| 20 | HAND RAIL ON STAIRS | 9/L 3.1 |
| 21 | TREE WELL | 10/L 3.1 |
| 22 | CHAINLINK FENCE 6' | |
| 23 | CHAINLINK PEDESTRIAN GATE 6' | |
| 24 | CONCRETE MOW STRIP UNDER FENCE | |



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LAND USE

LANDSCAPE SITE PLAN

L 1.01

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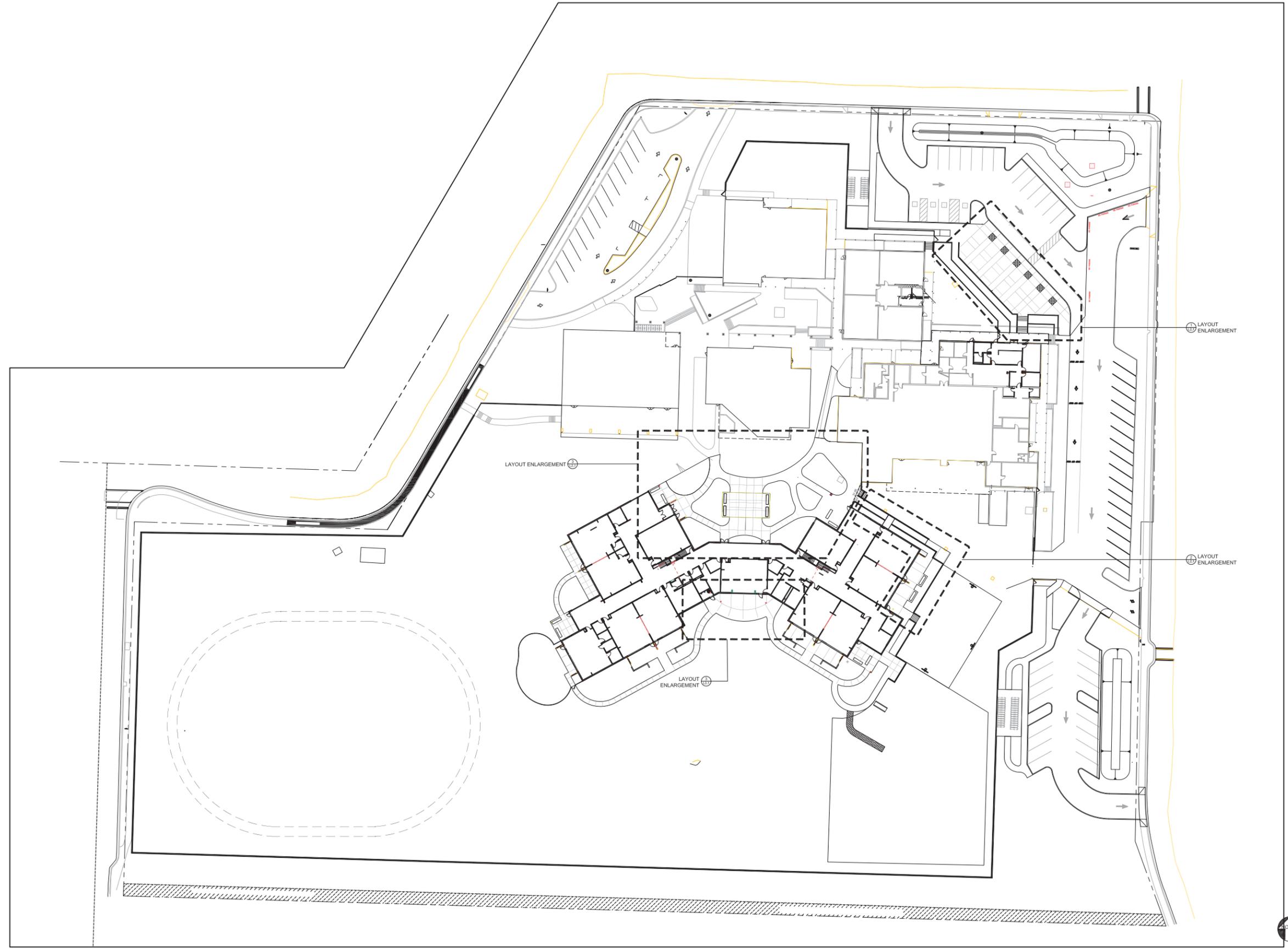
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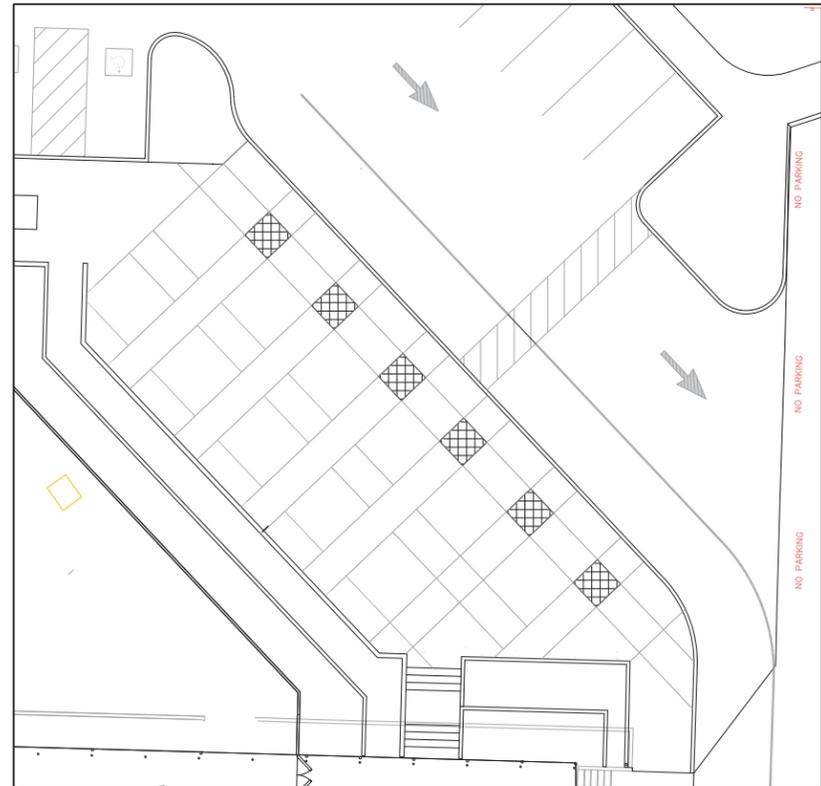
LAND USE

**LANDSCAPE
LAYOUT PLAN**

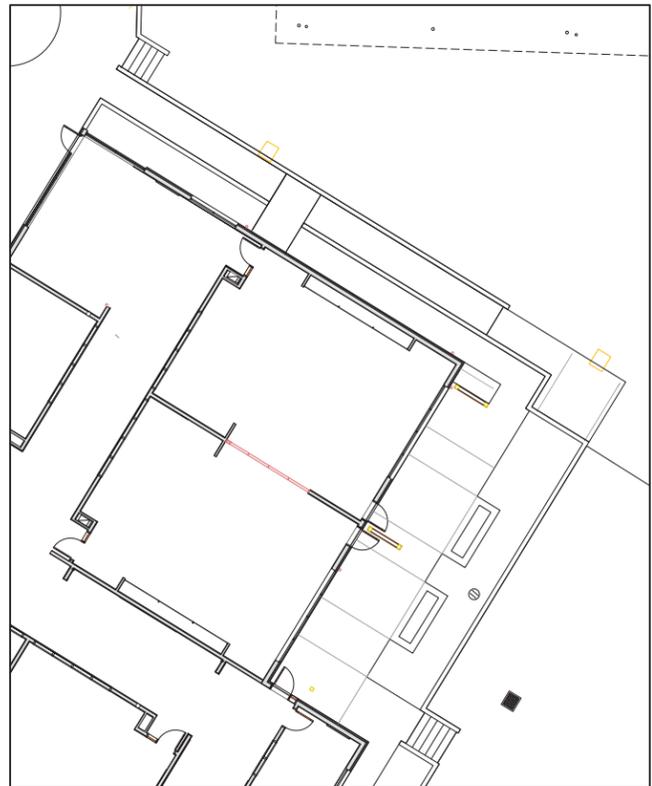
L1.02



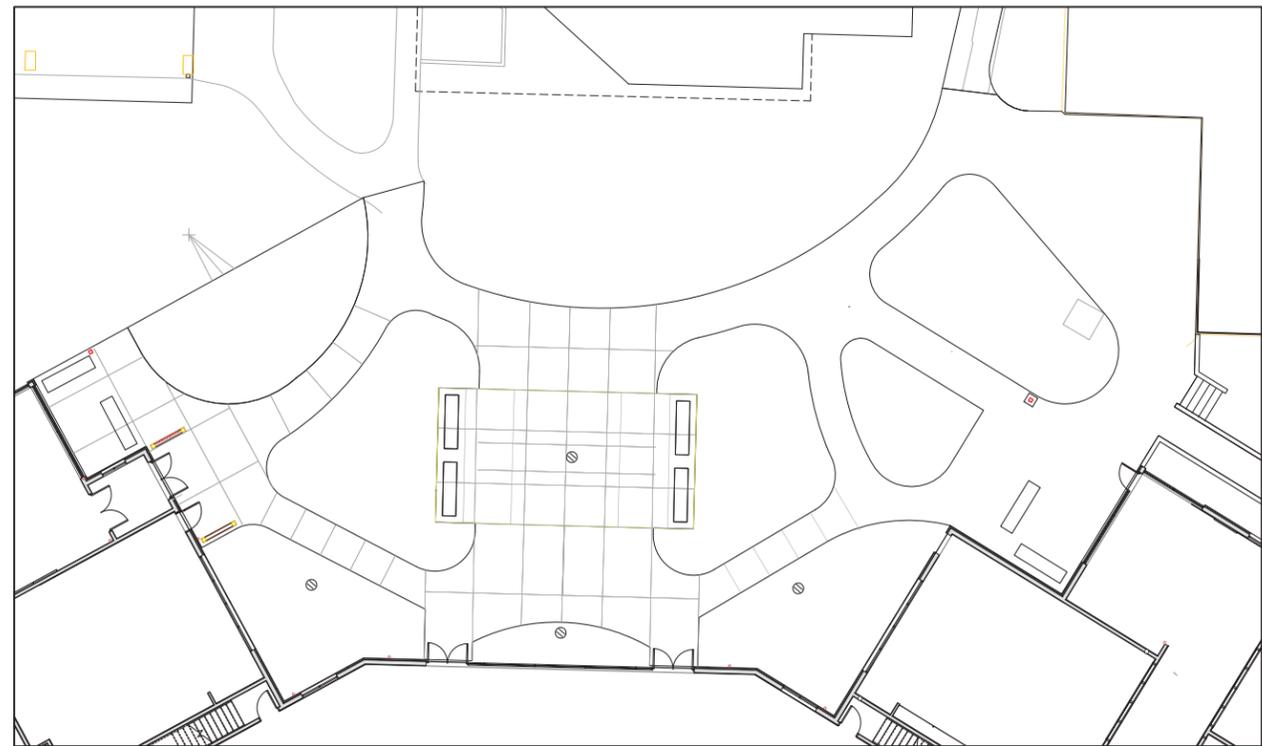
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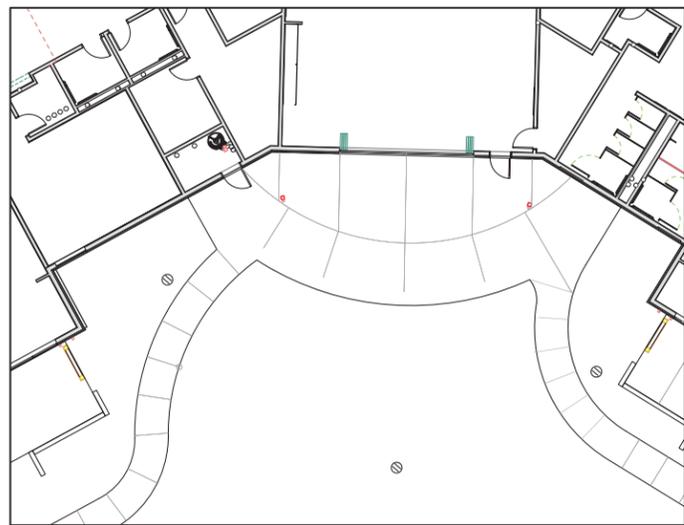
1 LAYOUT
Scale: 1"= 10'-0"
0' 5' 10' 20' 30'



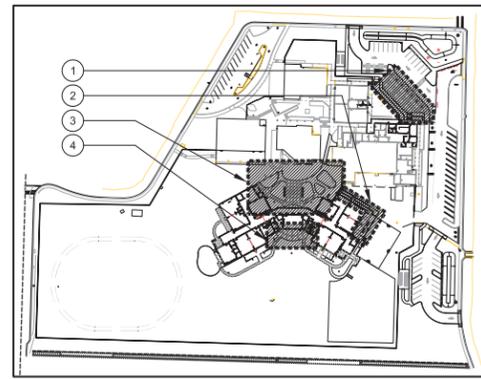
2 LAYOUT
Scale: 1"= 10'-0"
0' 5' 10' 20' 30'



3 LAYOUT
Scale: 1"= 10'-0"
0' 5' 10' 20' 30'



4 LAYOUT
Scale: 1"= 10'-0"
0' 5' 10' 20' 30'



SITE PLAN KEY
NTS

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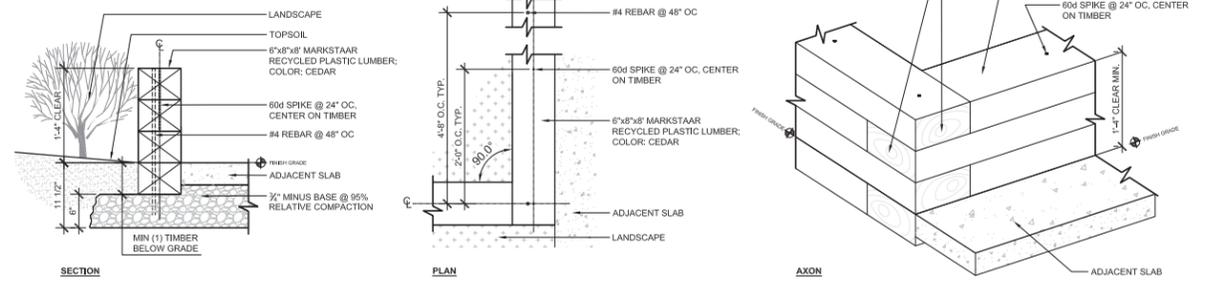
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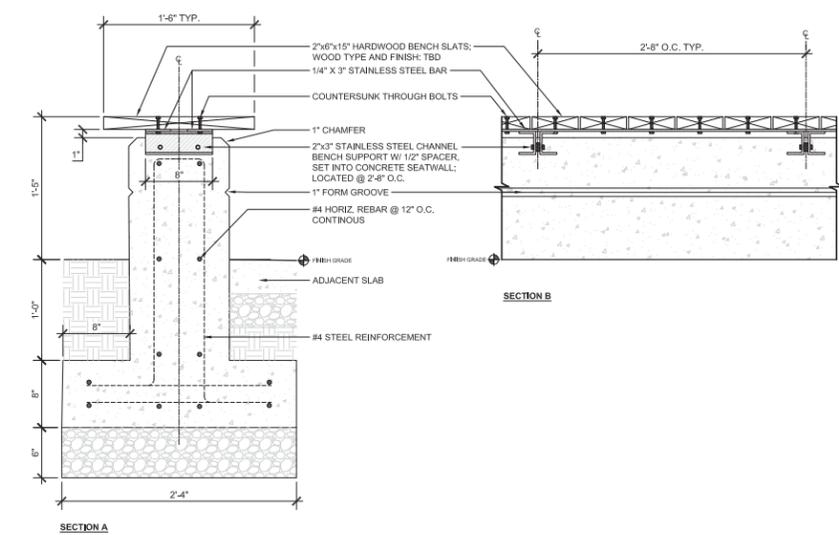
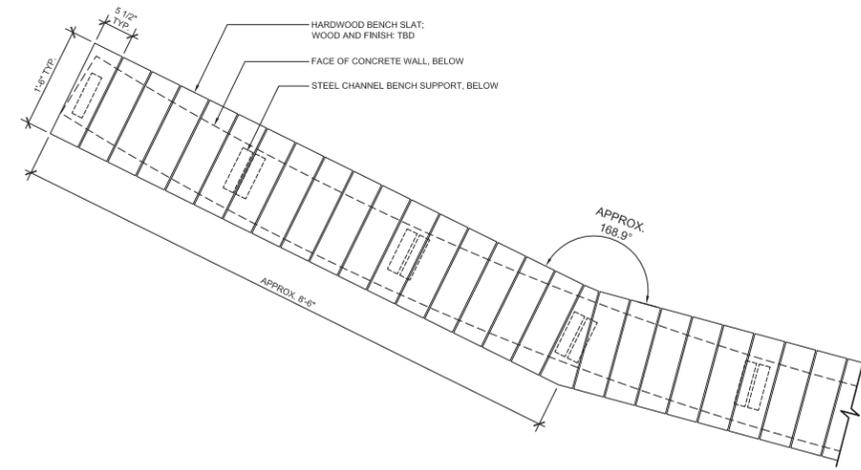
LAYOUT ENLARGEMENTS

L1.03

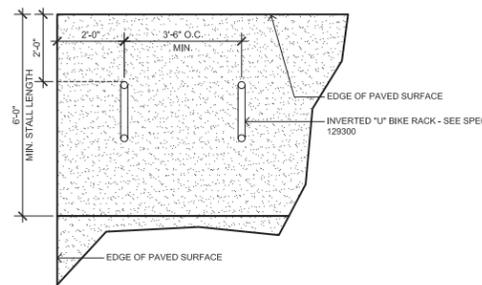


1 **TIMBER WALL**
L3.0 Scale: 1"= 1'-0"

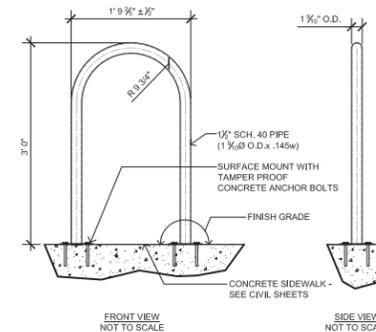
2 **CUSTOM SEAT WALL BENCH**
L3.0 Scale: 1"= 1'-0"



3 **SECTION: CUSTOM SEAT WALL BENCH**
L3.0 Scale: 1 1/2"= 1'-0"

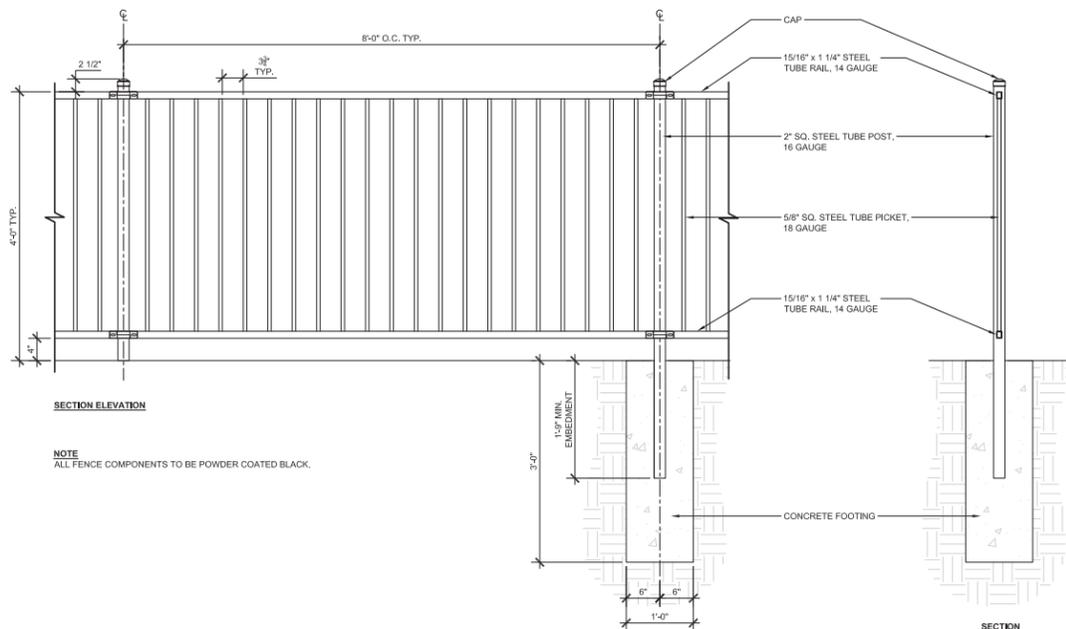


4 **PLAN: BIKE RACK LAYOUT**
Scale: 1/2"= 1'-0"

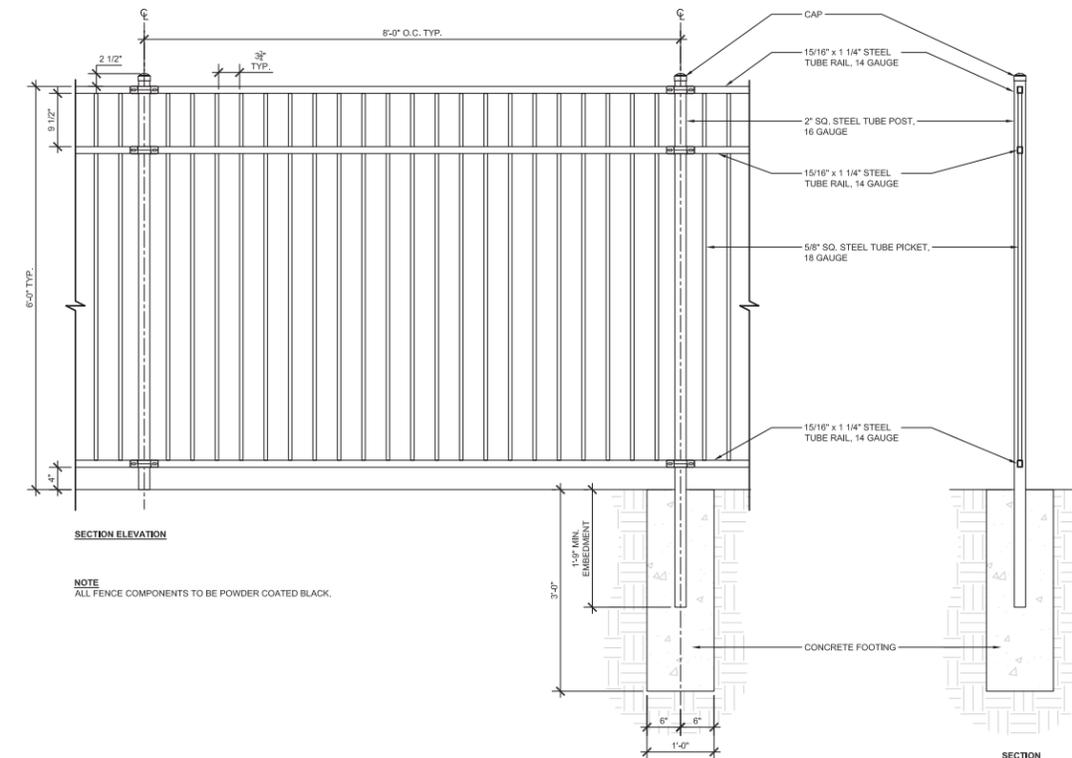


NOTE:
BIKE RACK TO BE POWDER COATED BLACK PER SPECIFICATION 129300.
INSTALLATION TO BE IN COMPLETE ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.

5 **SECTION: INVERTED "U" BIKE RACK**
Scale: 1"= 1'-0"



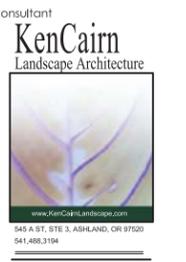
6 **4 FT. STEEL FENCE**
Scale: 1"= 1'-0"



7 **6 FT. STEEL FENCE**
Scale: 1"= 1'-0"

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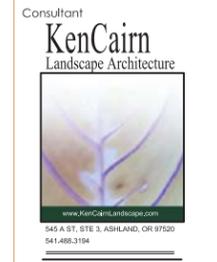
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LAND USE

DETAIL SHEETS
1 OF 3

L 1.04

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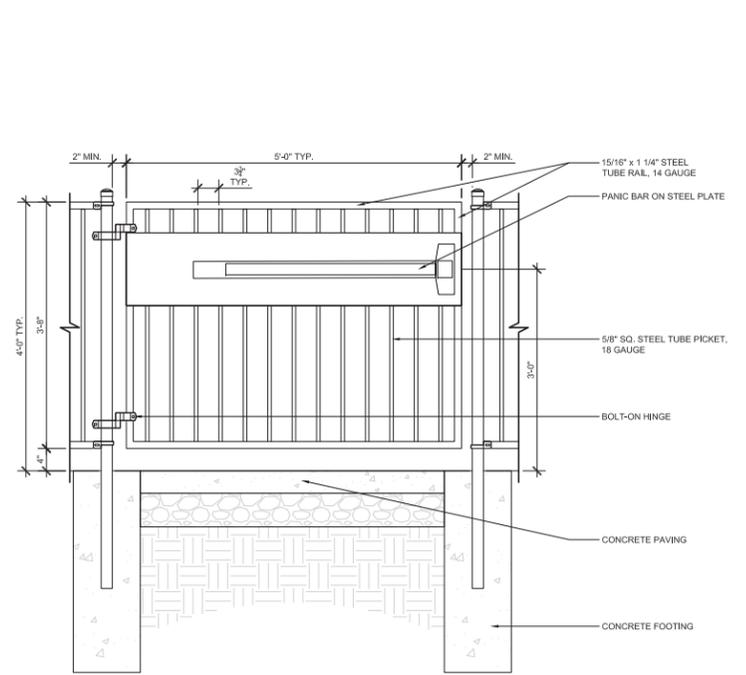
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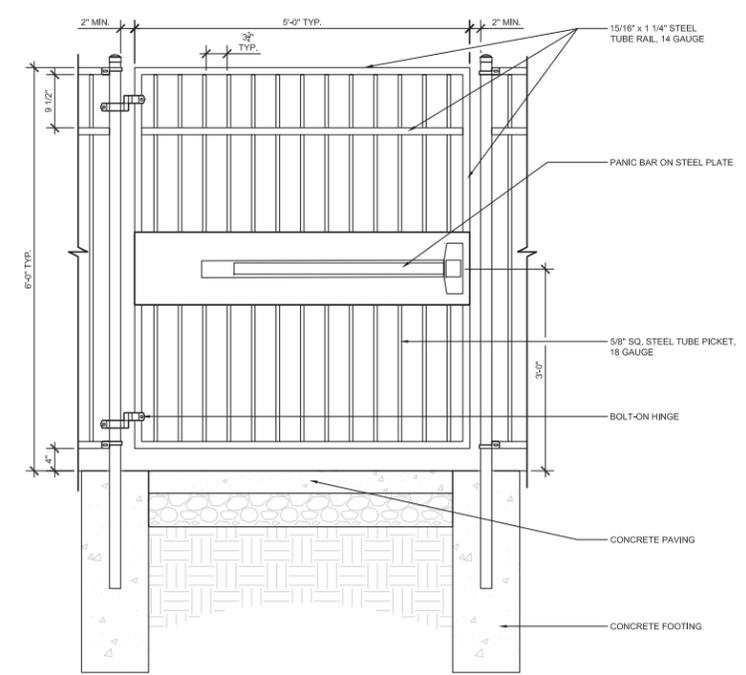
DETAIL SHEET 2 OF 3

L 1.05



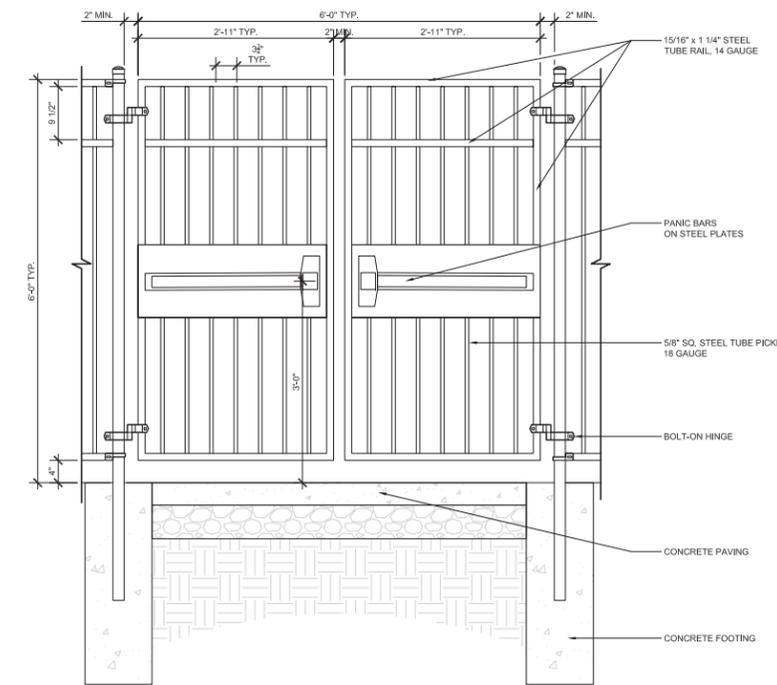
NOTE
 ALL FENCE COMPONENTS TO BE POWDER COATED BLACK.

1 4 FT. STEEL PEDESTRIAN GATE
 Scale: 1"= 1'-0"
 1/4 FOOT FENCING



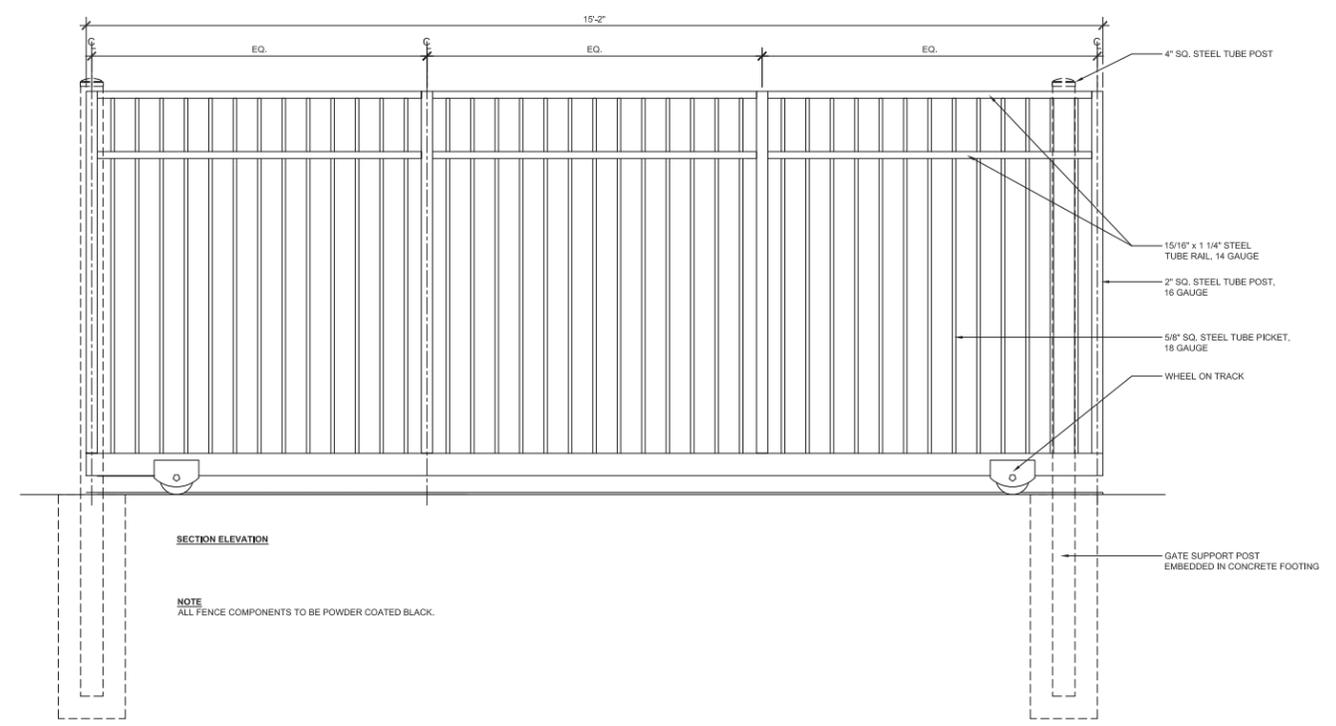
NOTE
 ALL FENCE COMPONENTS TO BE POWDER COATED BLACK.

2 6 FT. STEEL PEDESTRIAN GATE
 Scale: 1"= 1'-0"
 1/4 FOOT FENCING



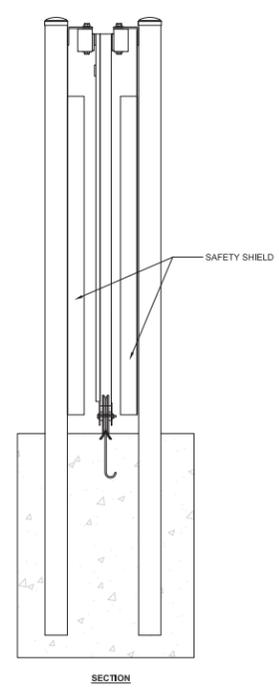
NOTE
 ALL FENCE COMPONENTS TO BE POWDER COATED BLACK.

3 STEEL PEDESTRIAN DOUBLE GATE
 Scale: 1"= 1'-0"
 1/4 FOOT FENCING



NOTE
 ALL FENCE COMPONENTS TO BE POWDER COATED BLACK.

4 STEEL ROLLING GATE
 Scale: 1"= 1'-0"
 1/4 FOOT FENCING



SECTION

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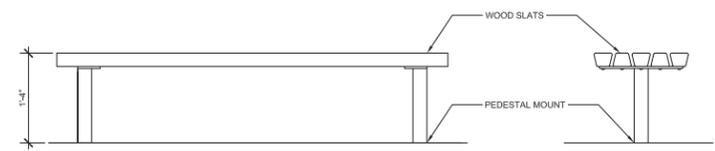
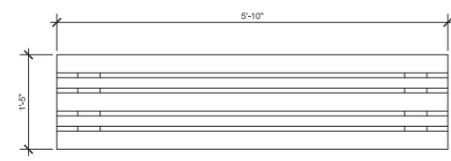
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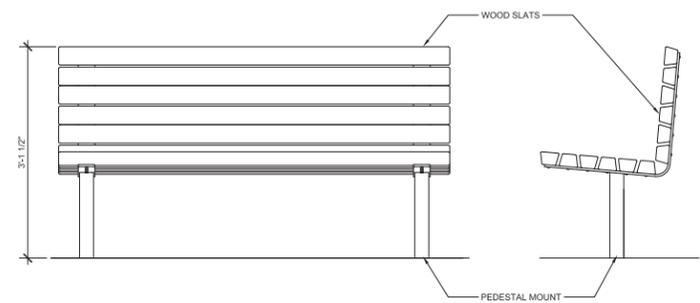
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DETAIL SHEETS
3 OF 3

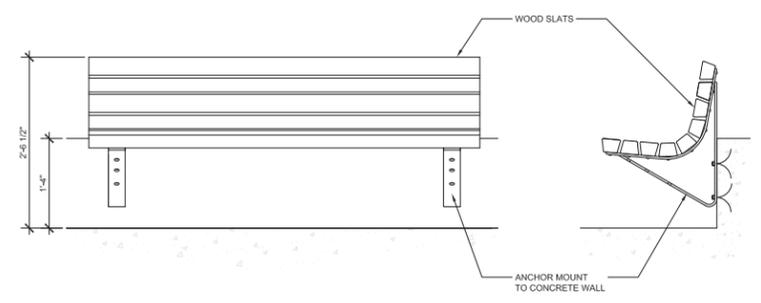
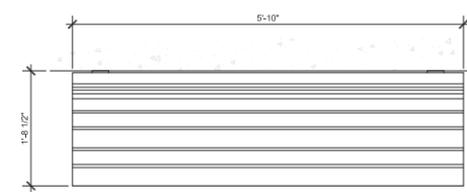
L 1.06



1 BACKLESS BENCH
Scale: 1"= 1'-0"



2 ADA BENCH
Scale: 1"= 1'-0"



3 WALL MOUNT BENCH
Scale: 1"= 1'-0"

4 CONCRETE PAVING - BROOM FINISH
Scale: 1"= 1'-0"

5 SKATE DETERRENT
Scale: 1"= 1'-0"

6 SWALE
Scale: 1"= 1'-0"

7 GUARD RAIL ON WALL
Scale: 1"= 1'-0"

8 HAND RAILS ON RAMP
Scale: 1"= 1'-0"

9 HANDRAIL ON STAIRS
Scale: 1"= 1'-0"

10 TREE WELL
Scale: 1"= 1'-0"

Drawing Name: Z:\PROJECTS\2019\1942 RES\DWG\Sheet_L1.06 Detail.dwg Last Saved: April 30, 2020 3:21 PM Plot Date: June 1, 2020 3:42 PM By: Kerry

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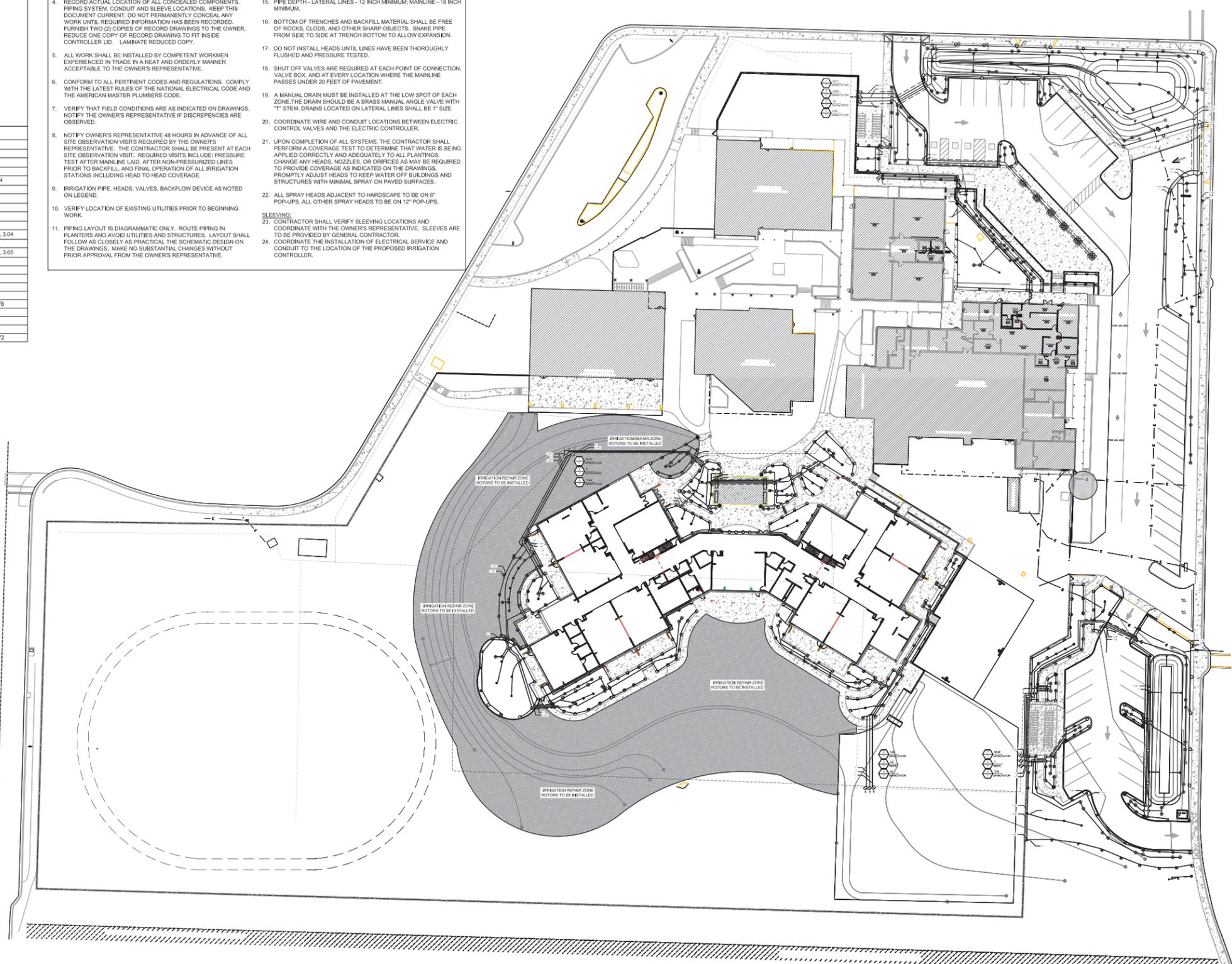
IRRIGATION LEGEND

| SYM. | ITEM |
|------|--|
| | HUNTER ICV-101G (SPRAY) |
| | MAIN LINE: SCH. 40 PVC (3") |
| | MAIN LINE: SCH. 40 PVC (3") |
| | LATERAL LINES SHALL BE SCHEDULE 40 PVC. (1") |
| | LATERAL LINES SHALL BE SCHEDULE 40 PVC. (1.14") |
| | LATERAL LINES SHALL BE SCHEDULE 40 PVC. (1.12") |
| | SLEEVES - SCH. 40, MIN. SIZE SHALL BE 2x DIA. OF PASSING PIPE. |
| | CONTROLLER: HUNTER IC-M SERIES WITH SOLAR-SYNC |
| | ZONE I.D. |
| | GPM APPLICATION |
| | VALVE SIZE |
| | POINT OF CONNECTION - SEE DETAIL XLX |

| | | | | | |
|--|----------------|-------------------------|-------------------|-------|------------------------|
| | 1/4, 1/2, FULL | HUNTER PRS30 | MP-Rotator 800 SR | 8" | 0.07, 0.14, 0.30 |
| | 1/4, 1/2, FULL | HUNTER PRS30 | MP-ROTATOR 1000 | 8" | 0.11, 0.21, 0.44 |
| | 1/4, 1/2, FULL | HUNTER PRS30 | MP-ROTATOR 1000 | 10" | 0.135, 0.27, 0.54 |
| | 1/4, 1/2, FULL | HUNTER PRS40 | MP-ROTATOR 1000 | 14" | 0.19, 0.38, 0.75 |
| | 1/4, 1/2, FULL | HUNTER PRS40 | MP-ROTATOR 2000 | 18" | 0.40, 0.74, 1.47 |
| | 1/4, 1/2, FULL | HUNTER PRS30 | MP-ROTATOR 3000 | 28" | 0.71, 1.51, 2.30, 3.04 |
| | 1/4, 1/2, FULL | HUNTER PRS40 | MP-ROTATOR 3000 | 30" | 0.85, 1.82, 2.75, 3.65 |
| | END, CENTER | HUNTER PRS40 | MP-ROTATOR STRIP | STRIP | 0.19, 0.38 |
| | | HUNTER RZWS-16-25-CV | | | 0.25 |
| | 1/4, 1/2, FULL | HUNTER PROS-06-PRS30-CV | PRO ADJUSTABLE | 8" | 0.44, 0.88, 1.76 |
| | 1/4, 1/2, FULL | HUNTER PROS-06-PRS30-CV | PRO ADJUSTABLE | 10" | 0.50, 1.0, 2.0 |
| | 1/4, 1/2, FULL | HUNTER PROS-06-PRS30-CV | PRO ADJUSTABLE | 15" | 0.93, 1.86, 3.72 |

IRRIGATION NOTES

- THE CITY OF ASHLAND TO REVIEW AND APPROVE ALL LANDSCAPE RELATED ITEMS PRIOR TO BEGINNING LANDSCAPE INSTALLATION.
- REVIEW SPECIFICATION SECTION 328400 FOR ALL ASPECTS OF THE IRRIGATION SYSTEM INSTALLATION.
- MAINTAIN AT JOB SITE ONE (1) COPY OF DRAWINGS, SPECIFICATIONS, ADDENDA, AND APPROVED SHOP DRAWINGS, CHANGE ORDERS, AND OTHER PROJECT DOCUMENTS.
- RECORD ACTUAL LOCATION OF ALL CONCEALED COMPONENTS, PIPING SYSTEM, CONDUIT AND SLEEVE LOCATIONS. KEEP THIS DOCUMENT CURRENT. DO NOT PERMANENTLY CONCEAL ANY WORK UNTIL REQUIRED INFORMATION HAS BEEN RECORDED. FURNISH TWO (2) COPIES OF RECORD DRAWINGS TO THE OWNER. REDUCE ONE COPY OF RECORD DRAWING TO FIT INSIDE CONTROLLER LID. LAMINATE REDUCED COPY.
- ALL WORK SHALL BE INSTALLED BY COMPETENT WORKMEN EXPERIENCED IN TRADE IN A NEAT AND ORDERLY MANNER ACCEPTABLE TO THE OWNER'S REPRESENTATIVE.
- CONFORM TO ALL PERTINENT CODES AND REGULATIONS. COMPLY WITH THE LATEST RULES OF THE NATIONAL ELECTRICAL CODE AND THE AMERICAN MASTER PLUMBERS CODE.
- VERIFY THAT FIELD CONDITIONS ARE AS INDICATED ON DRAWINGS. NOTIFY THE OWNER'S REPRESENTATIVE IF DISCREPANCIES ARE OBSERVED.
- NOTIFY OWNER'S REPRESENTATIVE 48 HOURS IN ADVANCE OF ALL SITE OBSERVATION VISITS REQUIRED BY THE OWNER'S REPRESENTATIVE. THE CONTRACTOR SHALL BE PRESENT AT EACH SITE OBSERVATION VISIT. REQUIRED VISITS INCLUDE: PRESSURE TEST AFTER MAINLINE Laid, AFTER NON-PRESSURIZED LINES PRIOR TO BACKFILL, AND FINAL OPERATION OF ALL IRRIGATION STATIONS INCLUDING HEAD TO HEAD COVERAGE.
- IRRIGATION PIPE, HEADS, VALVES, BACKFLOW DEVICE AS NOTED ON LEGEND.
- VERIFY LOCATION OF EXISTING UTILITIES PRIOR TO BEGINNING WORK.
- PIPING LAYOUT IS DIAGNOSTIC ONLY. ROUTE PIPING IN PLANTERS AND AVOID UTILITIES AND STRUCTURES. LAYOUT SHALL FOLLOW AS CLOSELY AS PRACTICAL THE SCHEMATIC DESIGN ON THE DRAWINGS. MAKE NO SUBSTANTIAL CHANGES WITHOUT PRIOR APPROVAL FROM THE OWNER'S REPRESENTATIVE.
- ALL LATERAL PIPE SHALL BE 1" IN SIZE UNLESS OTHERWISE INDICATED ON THE PLAN.
- COORDINATE ALL IRRIGATION EQUIPMENT LOCATIONS WITH OTHER CONTRACTORS. NOTIFY THE OWNER'S REPRESENTATIVE IF CONFLICTS ARE ENCOUNTERED.
- ALL SPRINKLER HEADS ALONG SIDEWALKS SHALL BE TWO INCHES FROM SIDEWALKS.
- PIPE DEPTH - LATERAL LINES - 12 INCH MINIMUM; MAINLINE - 18 INCH MINIMUM.
- BOTTOM OF TRENCHES AND BACKFILL MATERIAL SHALL BE FREE OF ROCKS, CLODS, AND OTHER SHARP OBJECTS. SHAKE PIPE FROM SIDE TO SIDE AT TRENCH BOTTOM TO ALLOW EXPANSION.
- DO NOT INSTALL HEADS UNTIL LINES HAVE BEEN THOROUGHLY FLUSHED AND PRESSURE TESTED.
- SHUT OFF VALVES ARE REQUIRED AT EACH POINT OF CONNECTION, VALVE BOX, AND AT EVERY LOCATION WHERE THE MAINLINE PASSES UNDER 20 FEET OF PAVEMENT.
- A MANUAL DRAIN MUST BE INSTALLED AT THE LOW SPOT OF EACH ZONE. THE DRAIN SHOULD BE A BRASS MANUAL ANGLE VALVE WITH "T" STEM. DRAINS LOCATED ON LATERAL LINES SHALL BE 1" SIZE.
- COORDINATE WIRE AND CONDUIT LOCATIONS BETWEEN ELECTRIC CONTROL VALVES AND THE ELECTRIC CONTROLLER.
- UPON COMPLETION OF ALL SYSTEMS, THE CONTRACTOR SHALL PERFORM A COVERAGE TEST TO DETERMINE THAT WATER IS BEING APPLIED CORRECTLY AND ADEQUATELY TO ALL PLANTINGS. CHANGE ANY HEADS, NOZZLES, OR ORIFICES AS MAY BE REQUIRED TO PROVIDE COVERAGE AS INDICATED ON THE DRAWINGS. PROMPTLY ADJUST HEADS TO KEEP WATER OFF BUILDINGS AND STRUCTURES WITH MINIMAL SPRAY ON PAVED SURFACES.
- ALL SPRAY HEADS ADJACENT TO HARDSCAPE TO BE ON 8" POP-UPS. ALL OTHER SPRAY HEADS TO BE ON 12" POP-UPS.
- SLEEVING: CONTRACTOR SHALL VERIFY SLEEVING LOCATIONS AND COORDINATE WITH THE OWNER'S REPRESENTATIVE. SLEEVES ARE TO BE PROVIDED BY GENERAL CONTRACTOR.
- COORDINATE THE INSTALLATION OF ELECTRICAL SERVICE AND CONDUIT TO THE LOCATION OF THE PROPOSED IRRIGATION CONTROLLER.



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Consultant
KenCairn
 Landscape Architecture

 545 A ST, STE 3, ASHLAND, OR 97520
 541.486.3194

ASHLAND SCHOOL DISTRICT
Helman Elementary
School Addition & Renovations

705 HELMAN ST.
 ASHLAND, OR 97520

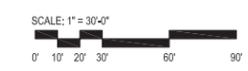
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LANDSCAPE IRRIGATION

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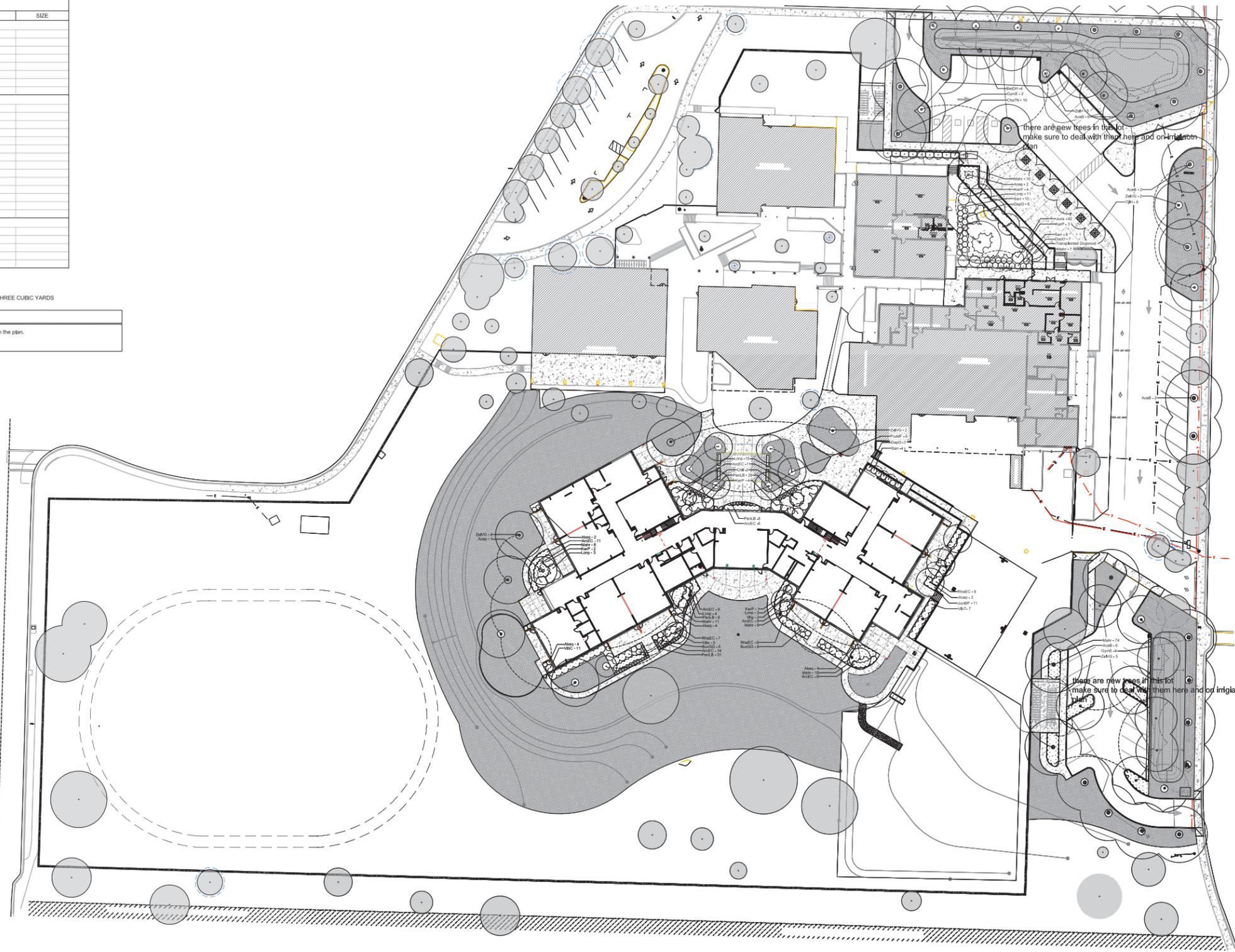
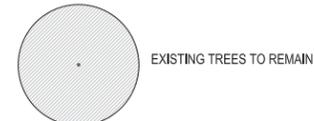


| SYMBOL | BOTANICAL NAME | COMMON NAME | SIZE |
|---------------------|---|---|------|
| TREES | | | |
| AcCB | Acer saccharum 'Bonfire' | Bonfire Sugar Maple | |
| Acsp | Acer palmatum spp. | Green Japanese Maple | |
| BeDH | Betula nigra 'Dura Heat' | Dura Heat River Birch | |
| GynE | Gymnocladia dioica 'Espresso' | Espresso Kentucky Coffeetree | |
| PruMF | Prunus serrulata 'M. Fuji' | Mt. Fuji Japanese Flowering Cherry | |
| TrH | Tilia cordata 'Halka' | Summer Sprite Linden | |
| Vit | Vitex agnus | Chaste Tree | |
| ZalVG | Zelkova serrata 'Village Green' | Village Green Japanese Zelkova | |
| SHRUBS | | | |
| ArcaEM | Arcostaphylos 'Emerald Carpet' | Emerald Carpet Manzanita | |
| Akeq | Akebia quinata | Chocolate Vine | |
| BuxGG | Buxus sempervirens 'Green Gem' | Green Gem Boxwood | |
| ChaTN | Chaenomeles speciosa 'Toyo-Nishiki' | Toyo-Nishiki Contorted Flowering Quince | |
| ChL | Chilopsis linearis | Desert Willow | |
| DapG | Daphne odora 'Aureo Marginala' | Variiegated Winter Daphne | |
| JunBP | Juniperus conferta 'Blue Pacific' | Blue Pacific Shore Juniper | |
| KerP | Kerria japonica 'Pleniflora' | Japanese Kerria | |
| LorP | Lonicera pileolata | Privet Honeysuckle | |
| Mah | Mahonia repens | Creeping Oregon Grape | |
| PenLB | Pennisetum alopecuroides 'Little Bunny' | Little Bunny Dwarf Fountain Grass | |
| RhaEC | Rhamnus californica 'Eve Case' | Eve Case Coffeeberry | |
| Sar | Sarcococca ruscifolia | Fragrant Sweetbox | |
| VibC | Viburnum tinus 'Compactum' | Compact Laurustinus Viburnum | |
| GROUND COVER | | | |
| | SunMark Diamond Green | | |
| | SunMark Wetland Prairie Mix | | |

NOTE: MATURE COMPOST SHALL BE ADDED TO THE TOPSOIL OF LANDSCAPING AREAS AT A RATE OF THREE CUBIC YARDS OF COMPOST PER 1,000 SQUARE FEET OF LANDSCAPING AREA TO BE PLANTED.

PRELIMINARY LANDSCAPE NOTES

1. All landscaping planting areas shall receive clean, sandy loam topsoil to a minimum depth of 12" or as noted on the plan.
2. All planting areas shall receive 3" of uninfested bark mulch.
3. The proposed irrigation system will consist of low volume distribution.



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Helman Elementary School Addition & Renovations

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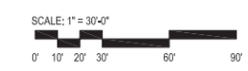
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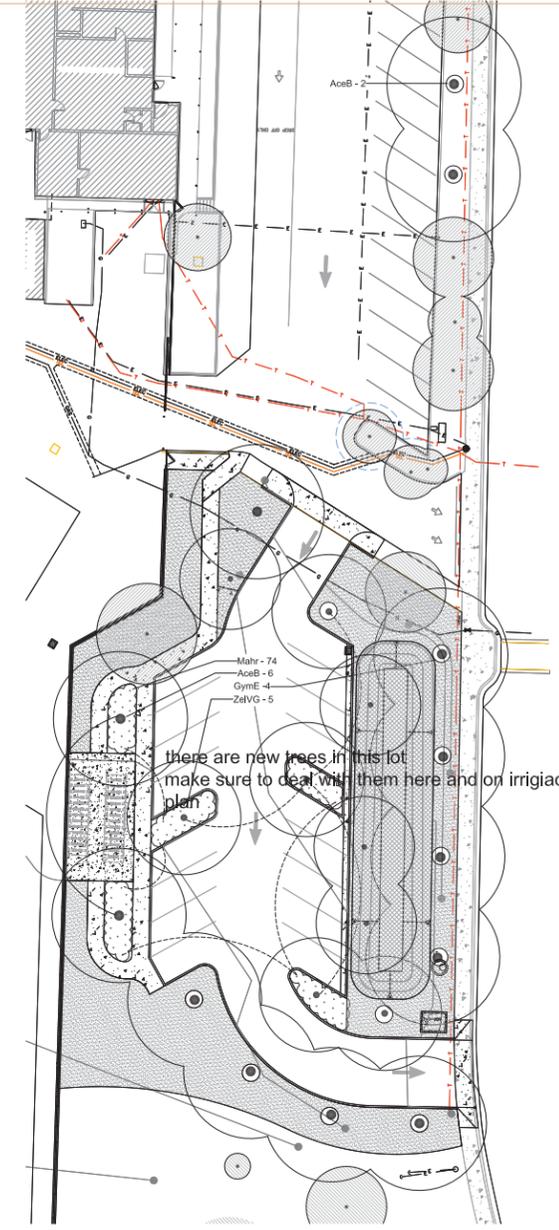
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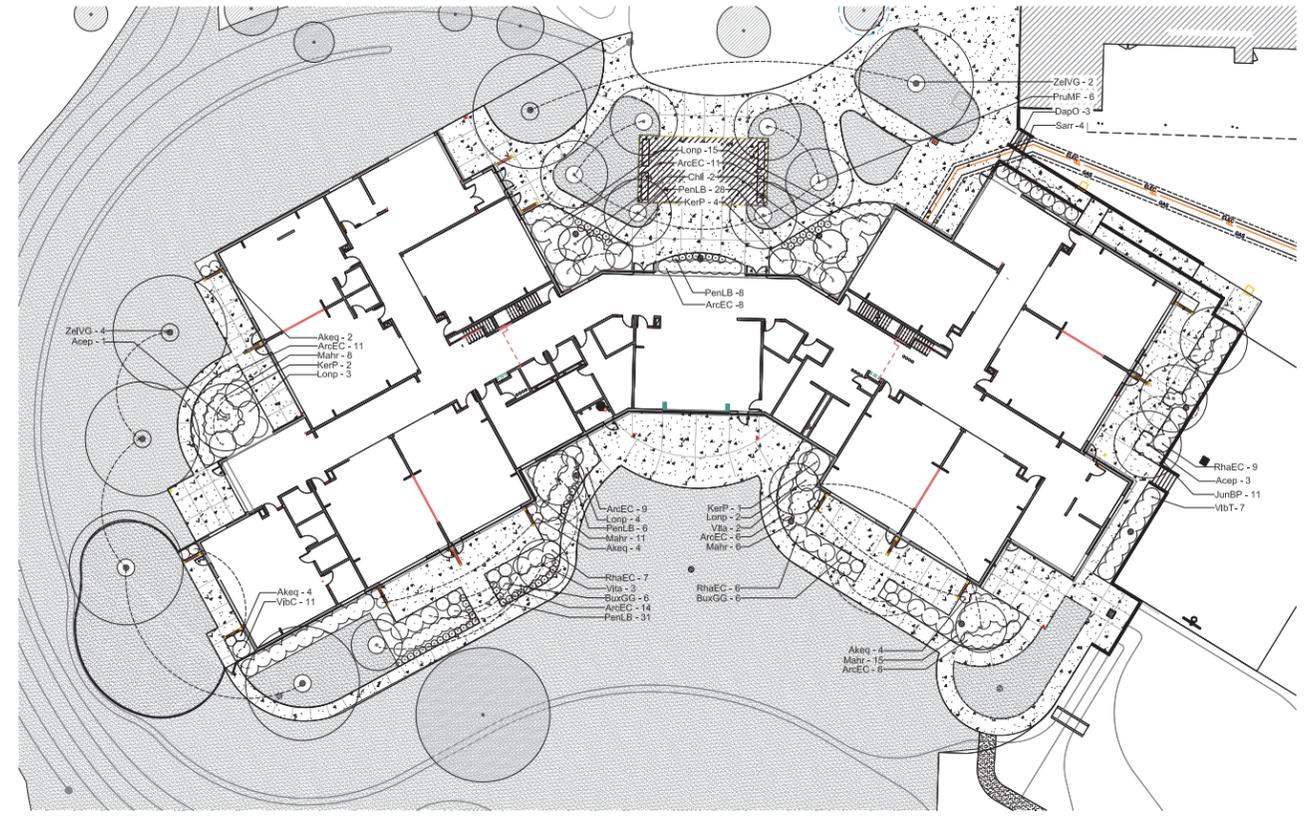
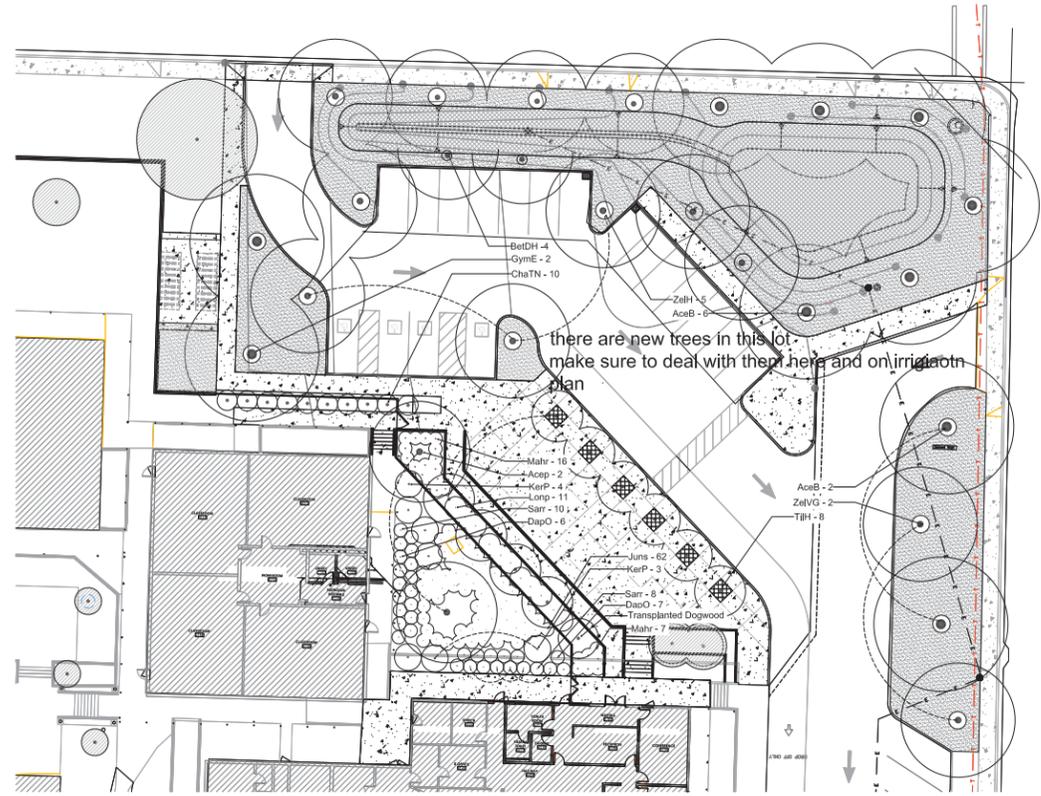
LANDSCAPE PLANTS

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| SYMBOL | BOTANICAL NAME | COMMON NAME | SIZE |
|---------------------|---|---|------|
| TREES | | | |
| AceB | Acer saccharum 'Bonfire' | Bonfire Sugar Maple | |
| Acep | Acer palmatum spp. | Green Japanese Maple | |
| BetDH | Betula nigra 'Dura Heart' | Dura Heart River Birch | |
| GymE | Gymnocladus dioica 'Espresso' | Espresso Kentucky Coffeetree | |
| PruMF | Prunus serrulata 'Mt. Fuji' | Mt. Fuji Japanese Flowering Cherry | |
| TiH | Tilia cordata 'Halke' | Summer Sprite Linden | |
| Via | Vitex agnus | Chaste Tree | |
| ZelVG | Zelkova serata 'Village Green' | Village Green Japanese Zelkova | |
| SHRUBS | | | |
| ArcEM | Arctostaphylos 'Emerald Carpet' | Emerald Carpet Manzanita | |
| Akeq | Akebia quinata | Chooslate Vine | |
| BuxGG | Buxus sempervirens 'Green Gem' | Green Gem Boxwood | |
| ChaTN | Chaenomeles speciosa 'Toyo-Nishiki' | Toyo-Nishiki Contorted Flowering Quince | |
| Chil | Chilopsis linearis | Desert Willow | |
| DapO | Daphne odora 'Aureo Marginata' | Variegated Winter Daphne | |
| JunBP | Juniperus conferta 'Blue Pacific' | Blue Pacific Shore Juniper | |
| KerP | Kerria japonica 'Pleniflora' | Japanese Kerria | |
| Lonp | Lonicera pileata | Pivvel Honeysuckle | |
| Mahr | Mahonia repens | Creeeping Oregon Grape | |
| PenLB | Pennisetum alopecuroides 'Little Bunny' | Little Bunny Dwarf Fountain Grass | |
| RhaEC | Rhamnus californica 'Eve Case' | Eve Case Coffeeberry | |
| Sarr | Sarcococca ruscifolia | Fragrant Sweetbox | |
| VibC | Viburnum tinus 'Compactum' | Compact Laurustinus Viburnum | |
| GROUND COVER | | | |
| [Symbol] | SunMark Diamond Green | | |
| [Symbol] | SunMark Wetland Prairie Mix | | |



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 Helman Elementary
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LANDSCAPE PLANTS

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GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE SHEET G0.05 FOR WALL TYPES
- C. SEE SHEET AR3.01 FOR DOOR SCHEDULE
- D. PLAN DIMENSIONS ARE MEASURED FROM XXX.
- E. ALL EXTERIOR WALLS ARE TYPE xx U.N.O.
- F. ALL INTERIOR WALLS ARE TYPE W6 U.N.O.

RENOVATION PLAN KEYNOTES

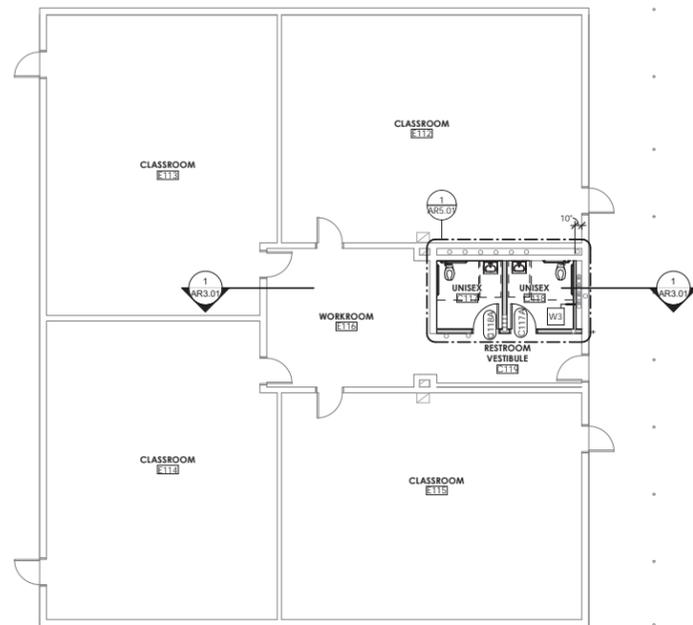
- 1. BUILT-IN CASEWORK TO REMAIN
- 2. SHELVING TO REMAIN
- 3. EXISTING CORNER, OFOI
- 4. INFILL INTERIOR WALL TO ALIGN WITH EXISTING

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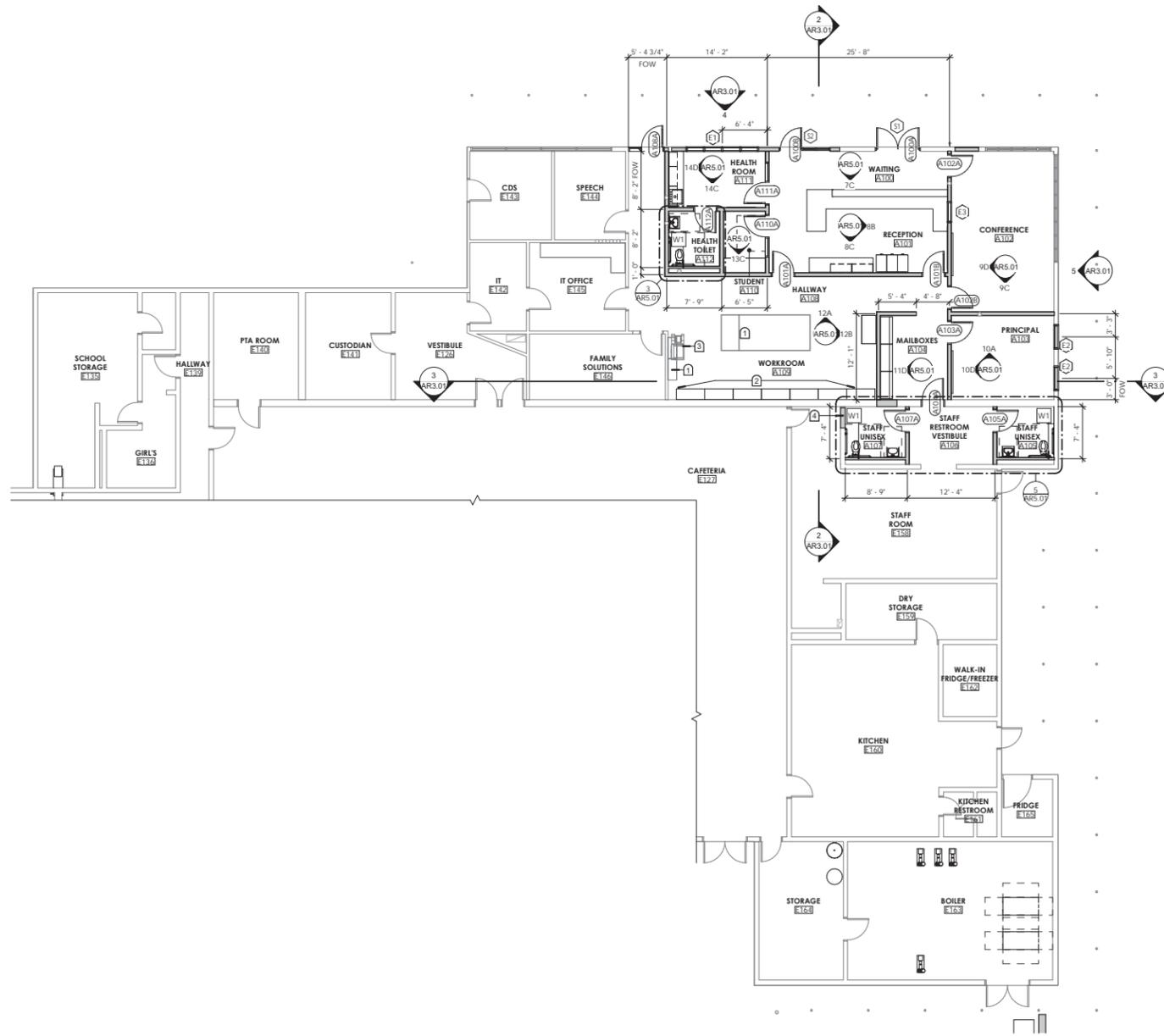
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 HELMAN ELEMENTARY SCHOOL ADDITION & RENOVATION**

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4 FLOOR PLAN - 'C' QUAD
 SCALE: 1/8" = 1'-0"



2 FLOOR PLAN - ADMINISTRATION
 SCALE: 1/8" = 1'-0"



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LAND USE
 RENOVATION PLANS

AR2.01

GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE SHEET G0.05 FOR WALL TYPES
- C. SEE SHEET ARB.01 FOR DOOR SCHEDULE
- D. PLAN DIMENSIONS ARE MEASURED FROM XXX.
- E. ALL EXTERIOR WALLS ARE TYPE xx U.N.O.
- F. ALL INTERIOR WALLS ARE TYPE W6 U.N.O.

RENOVATION EXTERIOR KEYNOTES

- 1. INFILL SIDING TO MATCH EXISTING
- 2. MATCH EXISTING FINISHES

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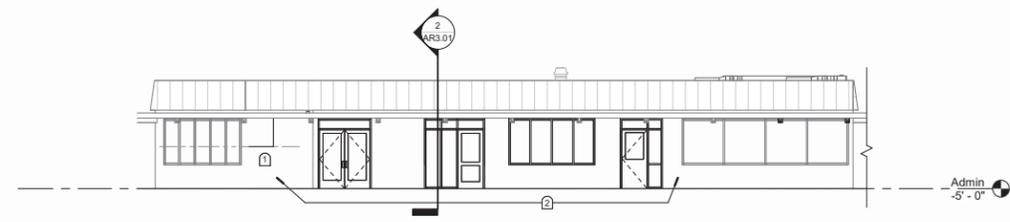
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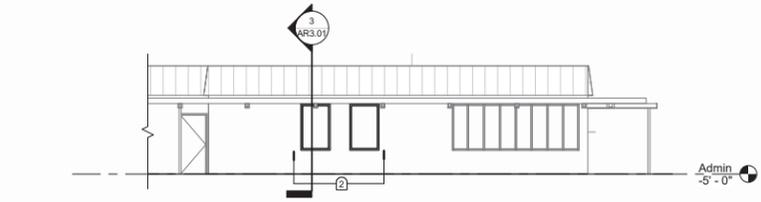
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RENOVATION
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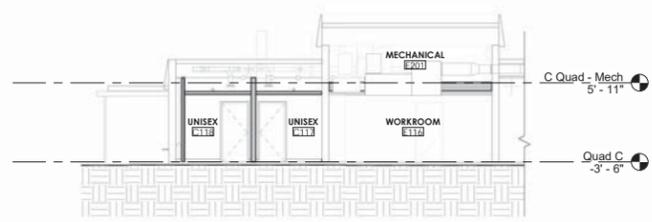
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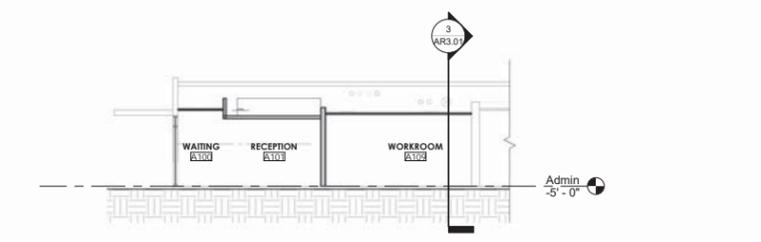
4 NORTH ELEVATION - ADMINISTRATION
 SCALE: 1/8" = 1'-0"



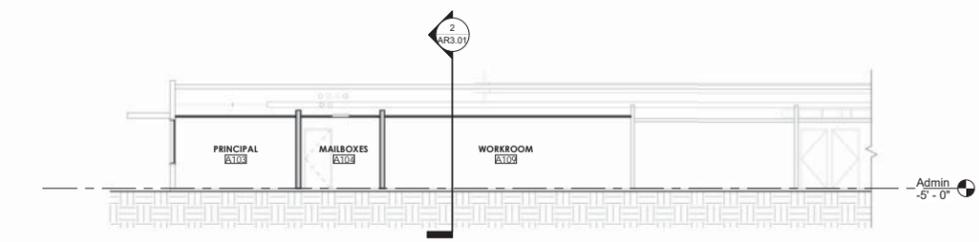
5 EAST ELEVATION - ADMINISTRATION
 SCALE: 1/8" = 1'-0"



1 BUILDING SECTION - 'C' QUAD
 SCALE: 1/8" = 1'-0"

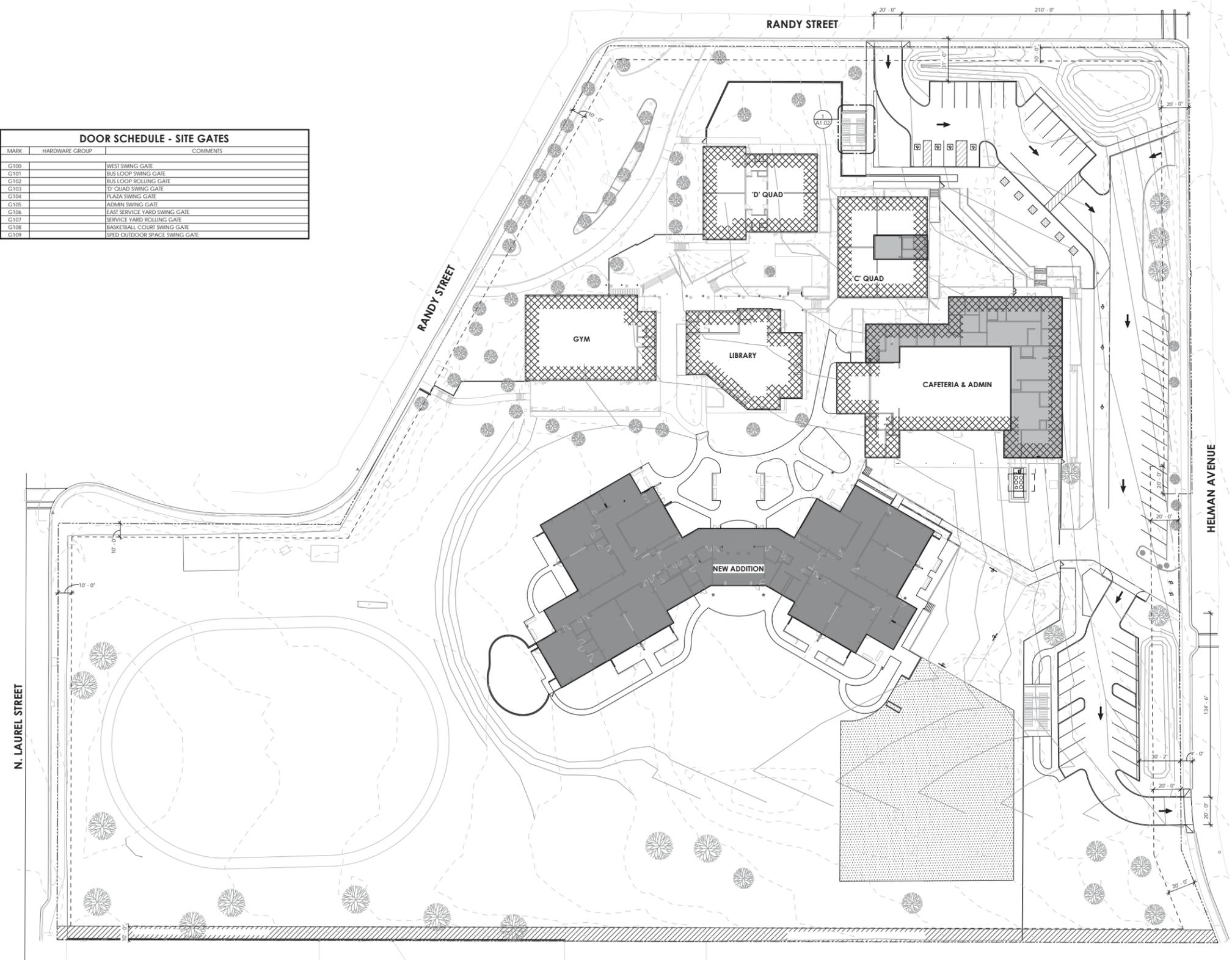


2 BUILDING SECTION - ADMINISTRATION
 SCALE: 1/8" = 1'-0"



3 BUILDING SECTION - ADMINISTRATION
 SCALE: 1/8" = 1'-0"

| DOOR SCHEDULE - SITE GATES | | |
|----------------------------|----------------|--------------------------------|
| MARK | HARDWARE GROUP | COMMENTS |
| G100 | | WEST SWING GATE |
| G101 | | BUS LOOP SWING GATE |
| G102 | | BUS LOOP ROLLING GATE |
| G103 | | D' QUAD SWING GATE |
| G104 | | PLAZA SWING GATE |
| G105 | | ADMIN SWING GATE |
| G106 | | EAST SERVICE YARD SWING GATE |
| G107 | | SERVICE YARD ROLLING GATE |
| G108 | | BASKETBALL COURT SWING GATE |
| G109 | | SPEED OUTDOOR SPACE SWING GATE |



- SITE LEGEND**
- NEW BUILDING ADDITION
 - ALTERATIONS WITHIN EXISTING BUILDING
 - EXISTING BUILDINGS
 - PROPERTY LINE
 - SET BACK
- SITE PLAN KEYNOTES**

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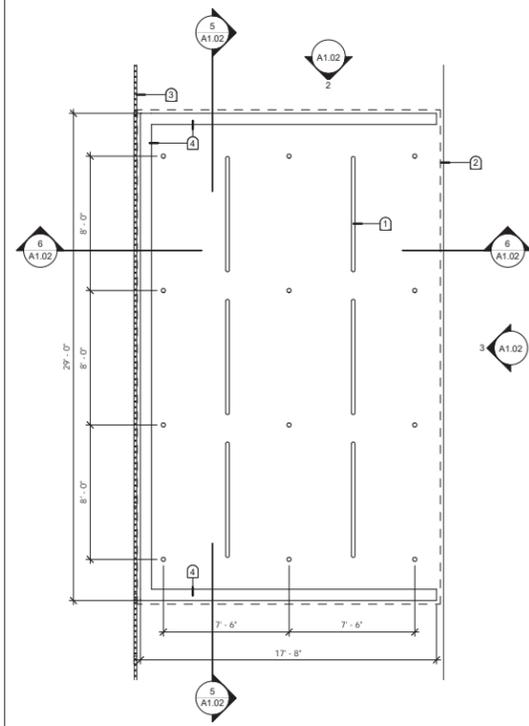
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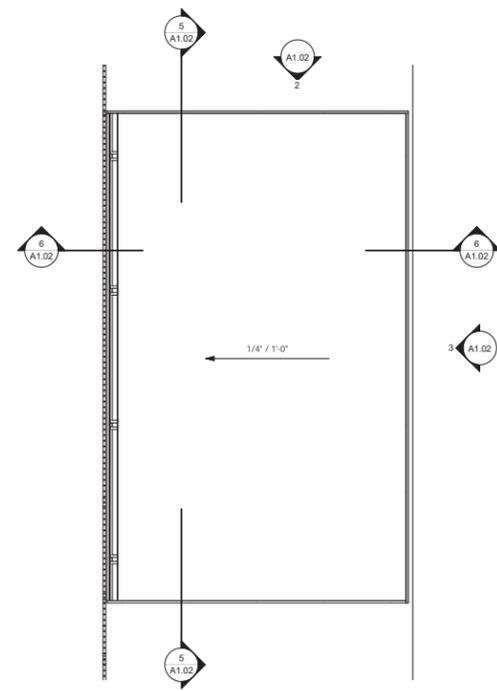
SITE PLAN

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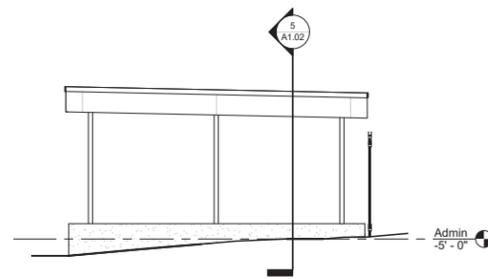




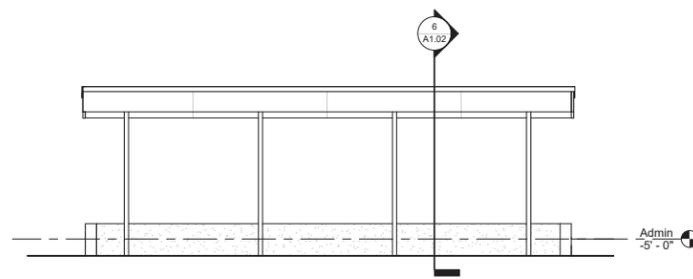
1 ENLARGED PLAN - BIKE CANOPY
SCALE: 1/4" = 1'-0"



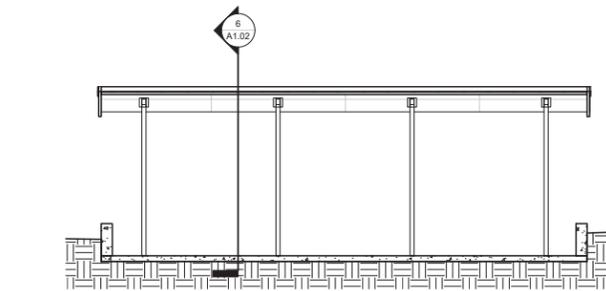
4 ENLARGED ROOF PLAN - BIKE CANOPY
SCALE: 1/4" = 1'-0"



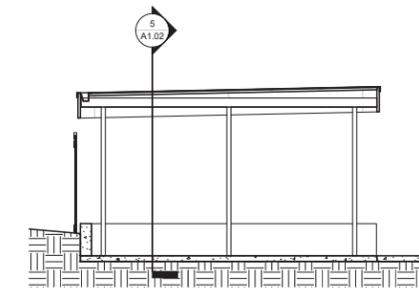
2 BIKE CANOPY - NORTH ELEVATION
SCALE: 1/4" = 1'-0"



3 BIKE CANOPY - EAST ELEVATION
SCALE: 1/4" = 1'-0"



5 BIKE CANOPY - NORTH-SOUTH SECTION
SCALE: 1/4" = 1'-0"



6 BIKE CANOPY - WEST-EAST SECTION
SCALE: 1/4" = 1'-0"

SITE ENLARGED PLAN KEYNOTES

- 1 BIKE RACK, TYP
- 2 ROOF OVERHANG, SHOWN DASHED
- 3 FENCING
- 4 8" CONCRETE WALL
- 5 EXISTING MURAL, RELOCATED

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ENLARGED SITE - BIKE
CANOPY

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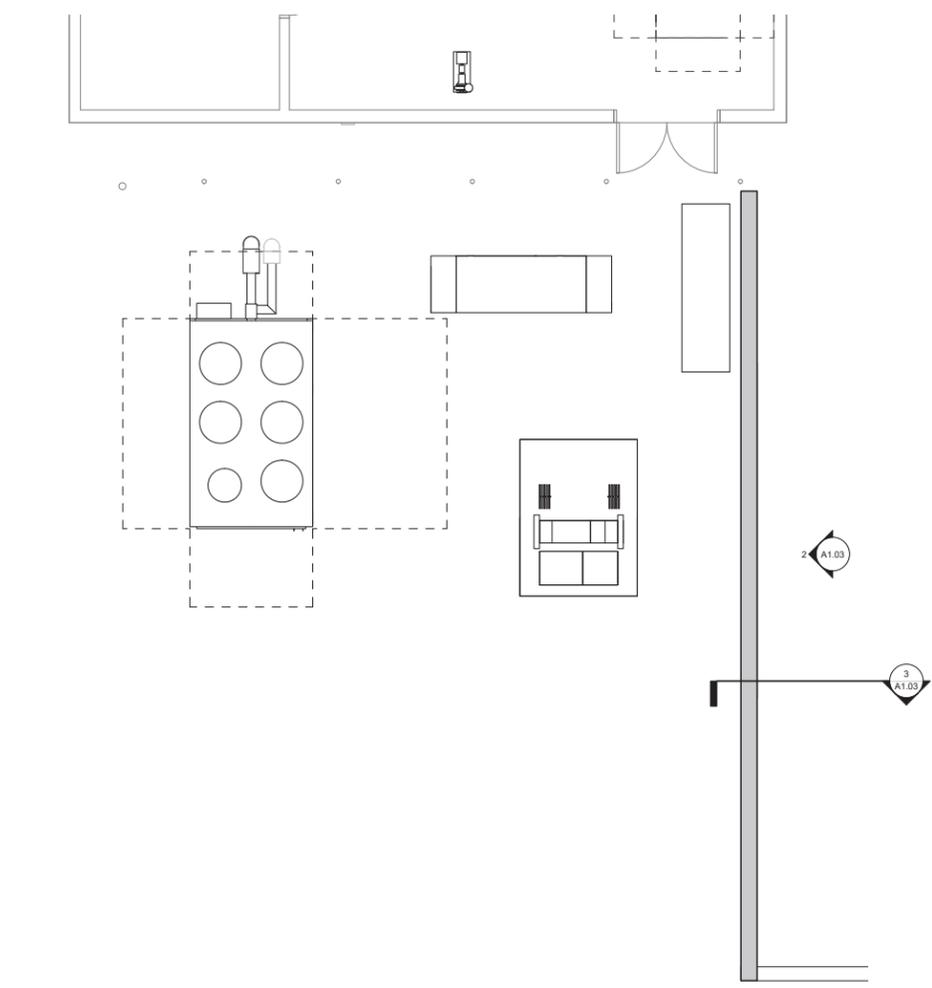
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 SCHOOL ADDITION &
 RENOVATION**

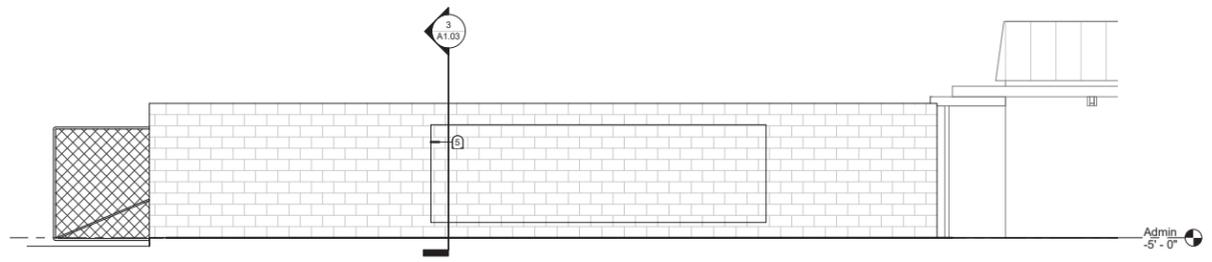
705 HELMAN ST.
 ASHLAND, OR 97520

SITE ENLARGED PLAN KEYNOTES

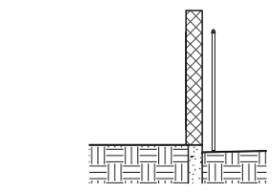
- 1 BIKE RACK, TYP
- 2 ROOF OVERHANG, SHOWN DASHED
- 3 FENCING
- 4 8" CONCRETE WALL
- 5 EXISTING MURAL, RELOCATED



1 SITE - SERVICE YARD
 SCALE: 1/4" = 1'-0"



2 SITE - SERVICE YARD CMU WALL
 SCALE: 1/4" = 1'-0"



3 SITE - SERVICE CMU WALL
 SCALE: 1/4" = 1'-0"

| No. | Description | Date |
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Project Number 1907
 Date 06.05.2020

LAND USE

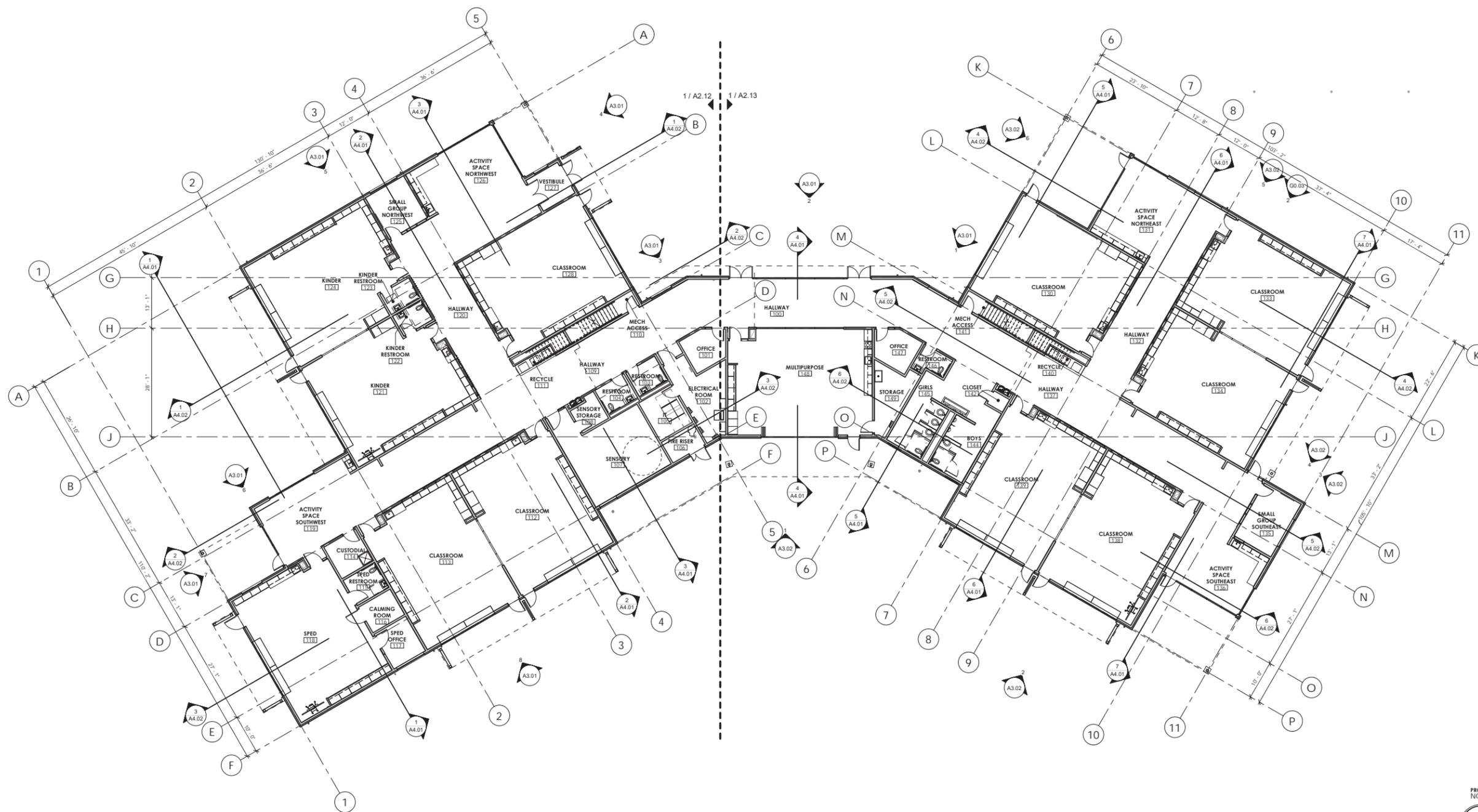
ENLARGED SITE -
 SERVICE YARD

A1.03

**NOT FOR
 CONSTRUCTION**

**ASHLAND SCHOOL DISTRICT
 HELMAN ELEMENTARY
 SCHOOL ADDITION &
 RENOVATION**

705 HELMAN ST.
 ASHLAND, OR 97520



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Project Number 1907
 Date 06.05.2020

LAND USE

OVERALL PLAN - LEVEL
 1

A2.01



GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE SHEET G0.05 FOR WALL TYPES
- C. PLAN DIMENSIONS ARE MEASURED FROM THE OUTSIDE FACE OF STUDS / FACE OF CONCRETE ON EXTERIOR WALLS TO THE CENTER LINES OF INTERIOR WALLS AND OPENINGS U.N.O.
- D. GRID LINES ALIGN WITH FACE OF STUD U.N.O.
- E. MECHANICAL ROOM FLOORING TO BE CON-2, UNO
- F. MECHANICAL ROOMS ARE OPEN TO STRUCTURE, UNO

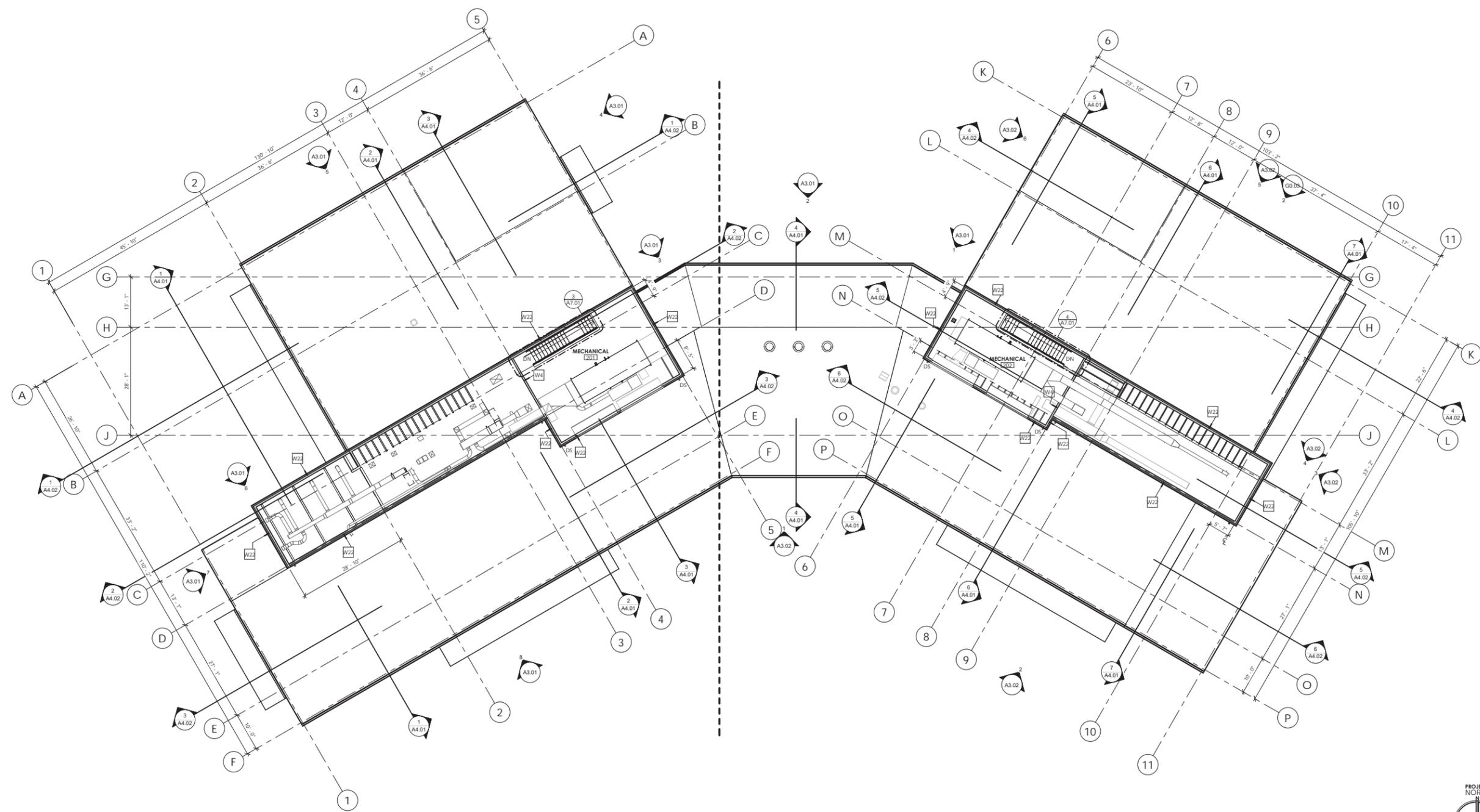
BBT ARCHITECTS

1140 SW Simpson Ave., Suite 200
Bend, Oregon 97702
1.541.382.5535 | 1.541.389.8033

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**ASHLAND SCHOOL DISTRICT
HELMAN ELEMENTARY
SCHOOL ADDITION &
RENOVATION**

705 HELMAN ST.
ASHLAND, OR 97520



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Project Number 1907
Date 06.05.2020

LAND USE

OVERALL PLAN -
MECH LEVEL

A2.02



1 OVERALL MECHANICAL PLAN
SCALE: 3/32" = 1'-0"

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ROOF KEYNOTES

- 1 TUBULAR SKYLIGHTS
- 2 PHOTOVOLTAIC (PV) ARRAY - FINAL SIZE AND LOCATION TO BE DETERMINED

GENERAL NOTES

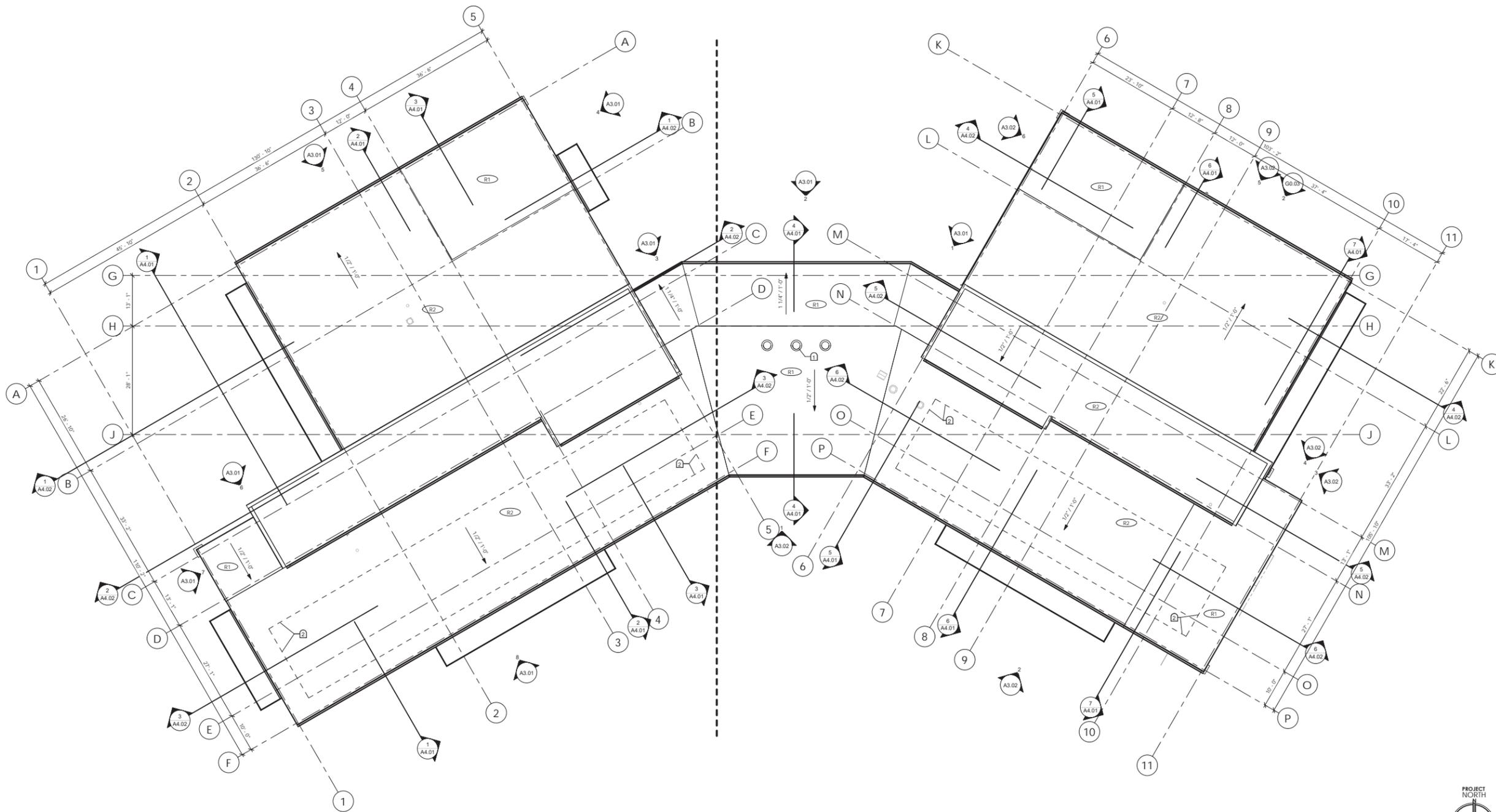
- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE SHEET G0.05 FOR ASSEMBLY TYPES
- C. PLAN DIMENSIONS ARE MEASURED FROM THE OUTSIDE FACE OF STUDS / FACE OF CONCRETE ON EXTERIOR WALLS TO THE CENTER LINES OF INTERIOR WALLS AND OPENINGS U.N.O.
- D. GRID LINES ALIGN WITH FACE OF STUD U.N.O.

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**ASHLAND SCHOOL DISTRICT
 HELMAN ELEMENTARY SCHOOL ADDITION & RENOVATION**

705 HELMAN ST.
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 Date 06.05.2020

LAND USE

ROOF PLAN



A2.03

1 OVERALL ROOF PLAN
 SCALE: 3/32" = 1'-0"

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GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE SHEET G0.05 FOR WALL TYPES
- C. SEE SHEET A8.01 FOR DOOR SCHEDULE
- D. PLAN DIMENSIONS ARE MEASURED FROM THE OUTSIDE FACE OF STUDS / FACE OF CONCRETE ON EXTERIOR WALLS TO THE CENTER LINES OF INTERIOR WALLS AND OPENINGS U.N.O.
- E. GRID LINES ALIGN WITH FACE OF STUD U.N.O.

EXTERIOR MATERIALS LEGEND

-  FIBER CEMENT
-  HORIZONTAL METAL PANEL

EXTERIOR KEYNOTES

- 1 LIGHT TUBE

NOT FOR CONSTRUCTION

**ASHLAND SCHOOL DISTRICT
 HELMAN ELEMENTARY
 SCHOOL ADDITION &
 RENOVATION**

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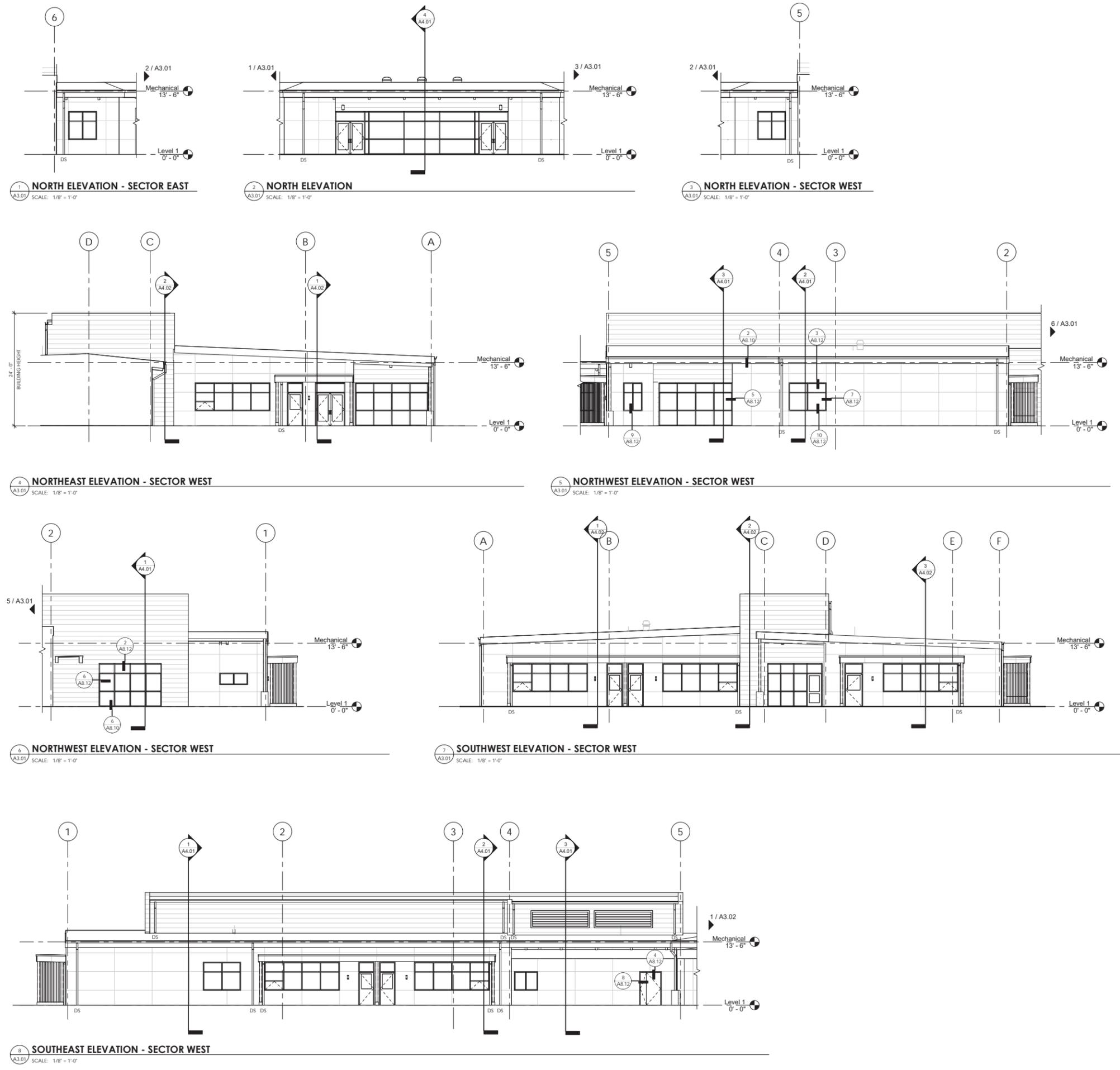
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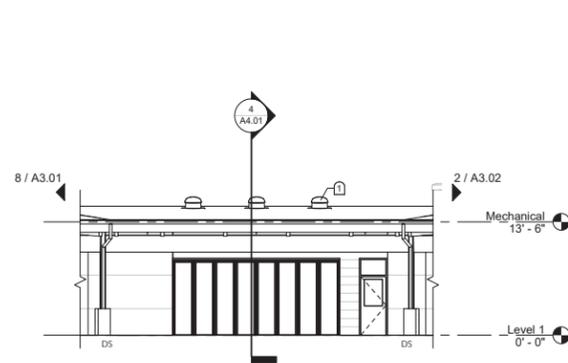
Project Number 1907
 Date 06.05.2020

LAND USE

ELEVATIONS

A3.01

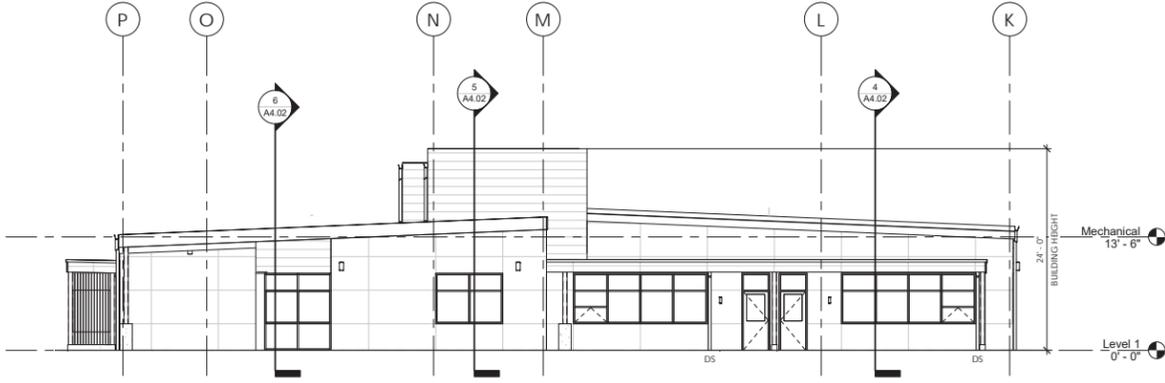




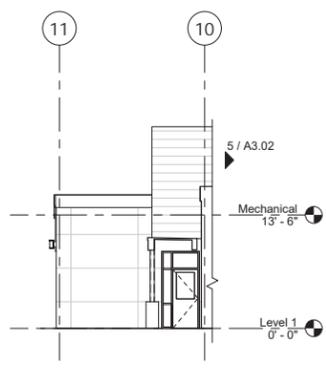
1 SOUTH ELEVATION
A3.02 SCALE: 1/8" = 1'-0"



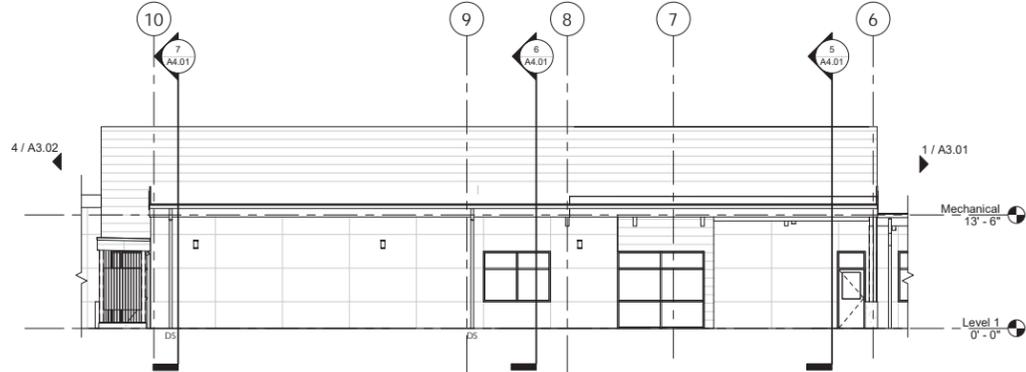
2 SOUTHWEST ELEVATION - SECTOR EAST
A3.02 SCALE: 1/8" = 1'-0"



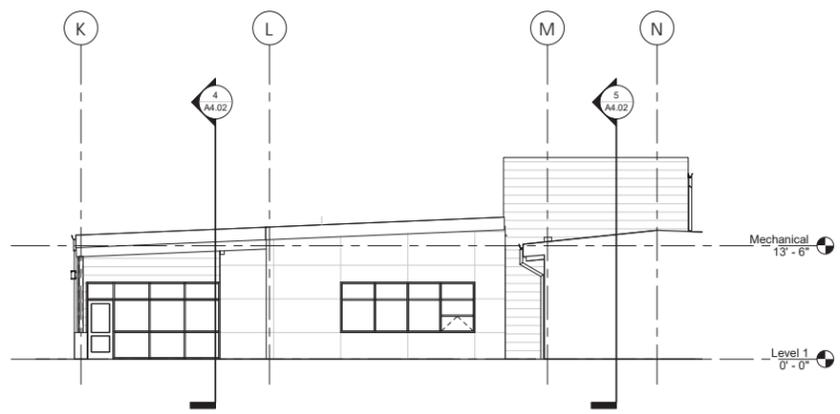
3 SOUTHEAST ELEVATION - SECTOR EAST
A3.02 SCALE: 1/8" = 1'-0"



4 NORTHEAST ELEVATION - SECTOR EAST
A3.02 SCALE: 1/8" = 1'-0"



5 NORTHEAST ELEVATION - SECTOR EAST
A3.02 SCALE: 1/8" = 1'-0"

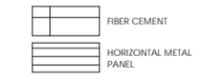


6 NORTHWEST ELEVATION - SECTOR EAST
A3.02 SCALE: 1/8" = 1'-0"

GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS.
- B. SEE SHEET G0.05 FOR WALL TYPES.
- C. SEE SHEET A8.01 FOR DOOR SCHEDULE.
- D. PLAN DIMENSIONS ARE MEASURED FROM THE OUTSIDE FACE OF STUDS / FACE OF CONCRETE ON EXTERIOR WALLS TO THE CENTER LINES OF INTERIOR WALLS AND OPENINGS U.N.O. GRID LINES ALIGN WITH FACE OF STUD U.N.O.
- E. GRID LINES ALIGN WITH FACE OF STUD U.N.O.

EXTERIOR MATERIALS LEGEND



EXTERIOR KEYNOTES

- 1 LIGHT TUBE

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**ASHLAND SCHOOL DISTRICT
HELMAN ELEMENTARY
SCHOOL ADDITION &
RENOVATION**

705 HELMAN ST.
ASHLAND, OR 97520

| No. | Description | Date |
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Project Number 1907
Date 06.05.2020

LAND USE

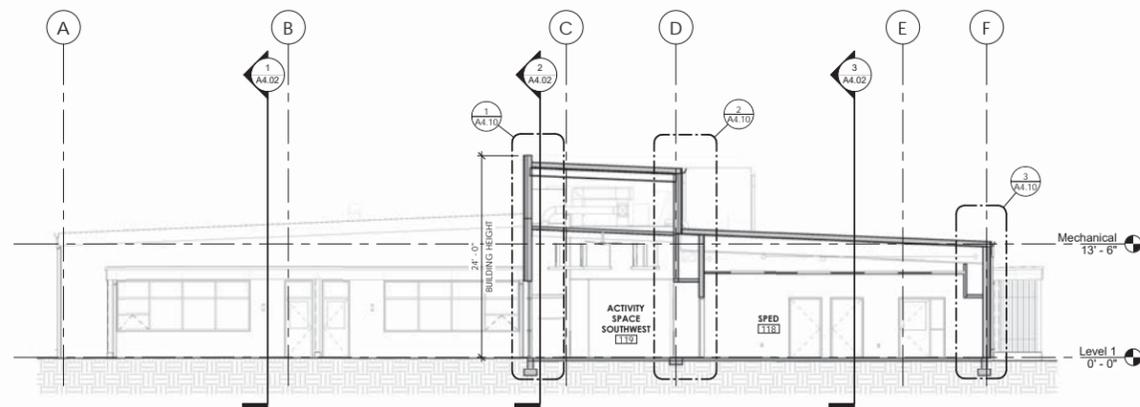
ELEVATIONS

A3.02

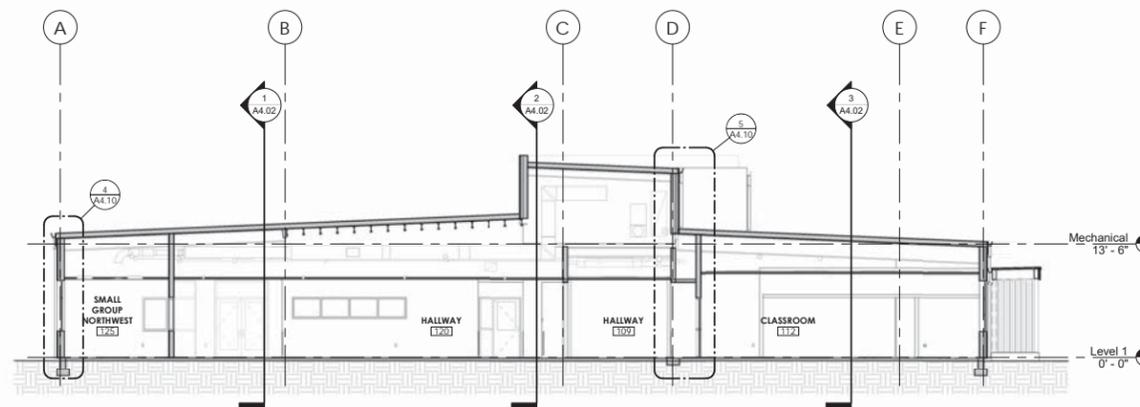
**NOT FOR
CONSTRUCTION**

**ASHLAND SCHOOL
DISTRICT
HELMAN ELEMENTARY
SCHOOL ADDITION &
RENOVATION**

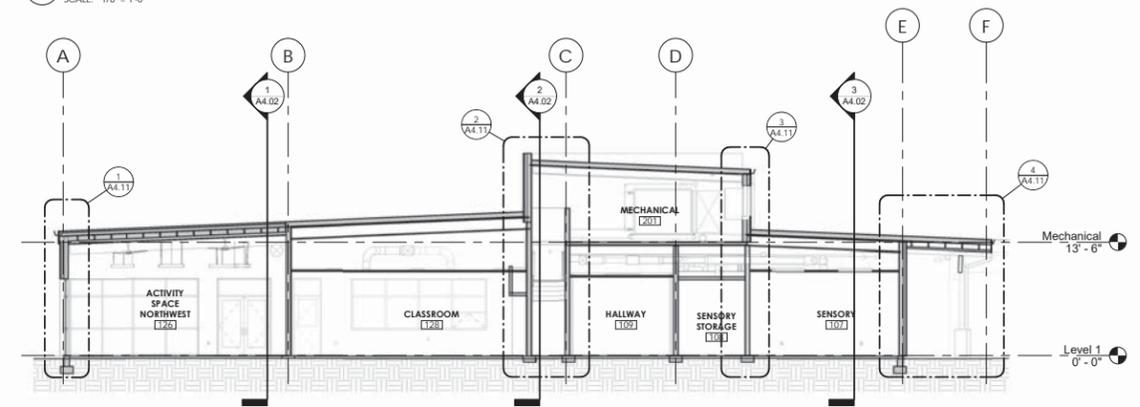
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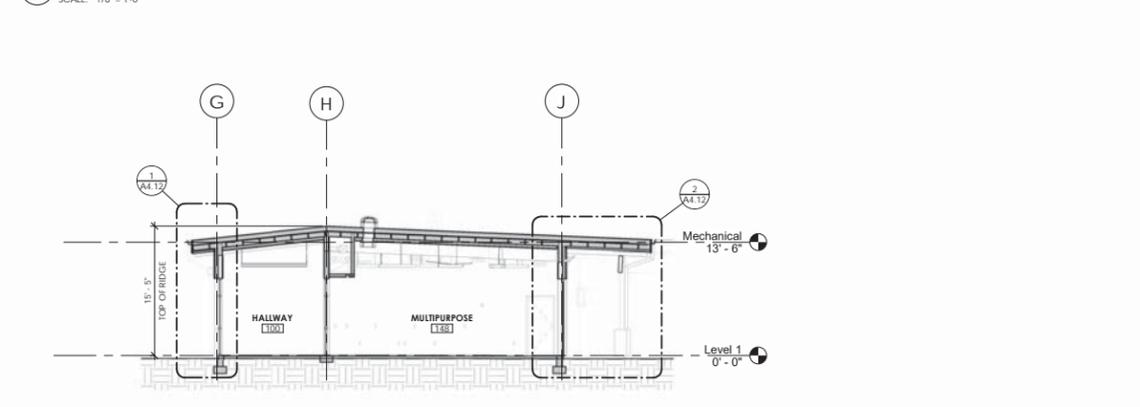
1 BUILDING SECTION
SCALE: 1/8" = 1'-0"



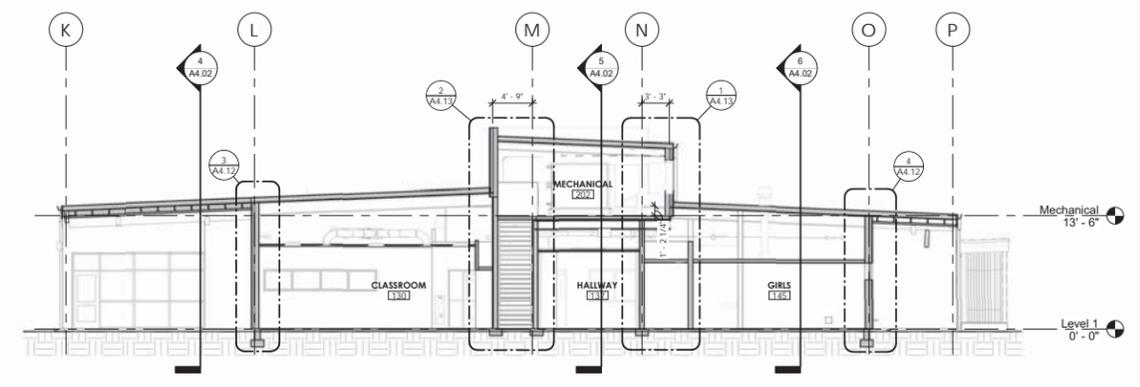
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SCALE: 1/8" = 1'-0"



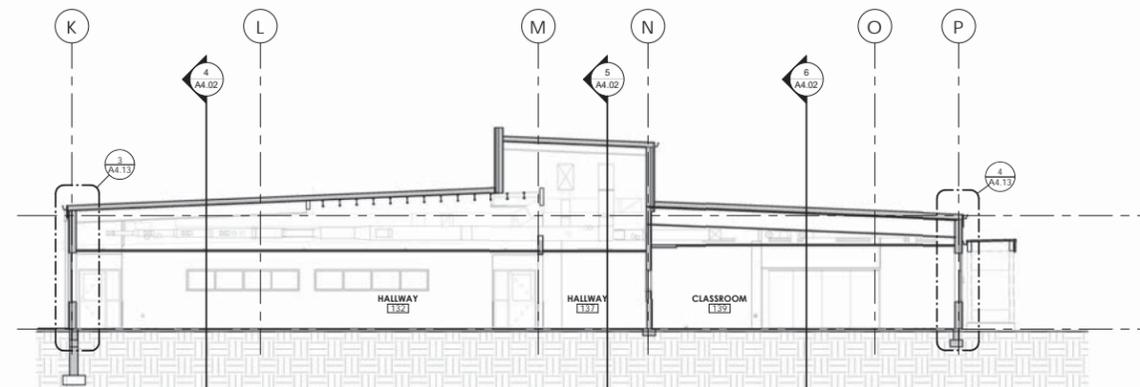
3 BUILDING SECTION
SCALE: 1/8" = 1'-0"



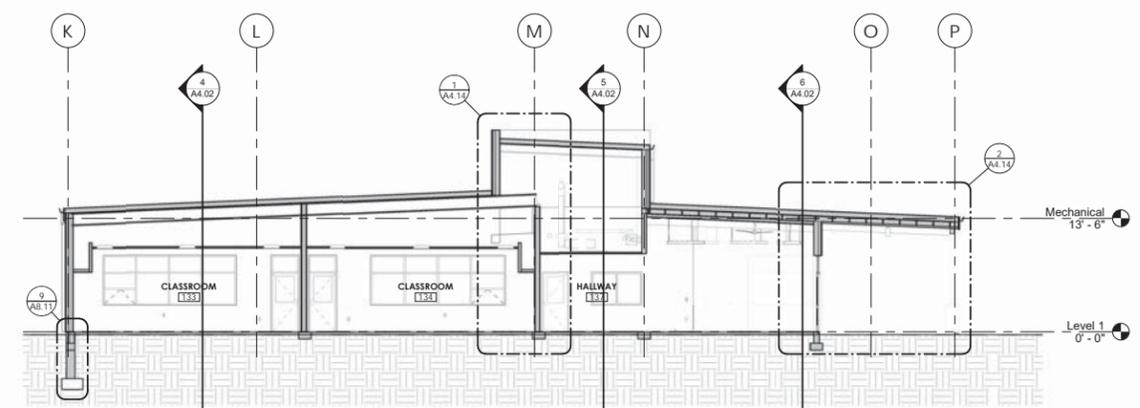
4 BUILDING SECTION
SCALE: 1/8" = 1'-0"



5 BUILDING SECTION
SCALE: 1/8" = 1'-0"



6 BUILDING SECTION
SCALE: 1/8" = 1'-0"



7 BUILDING SECTION
SCALE: 1/8" = 1'-0"

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Date 06.05.2020

LAND USE

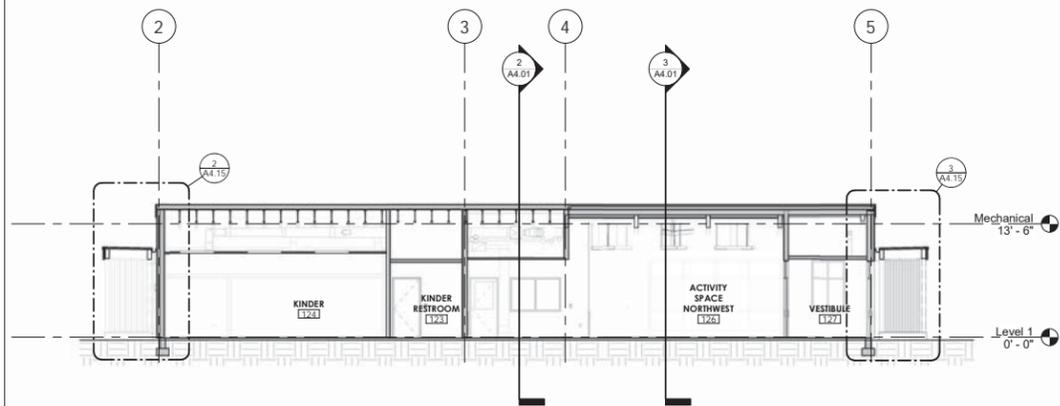
BUILDING SECTIONS

A4.01

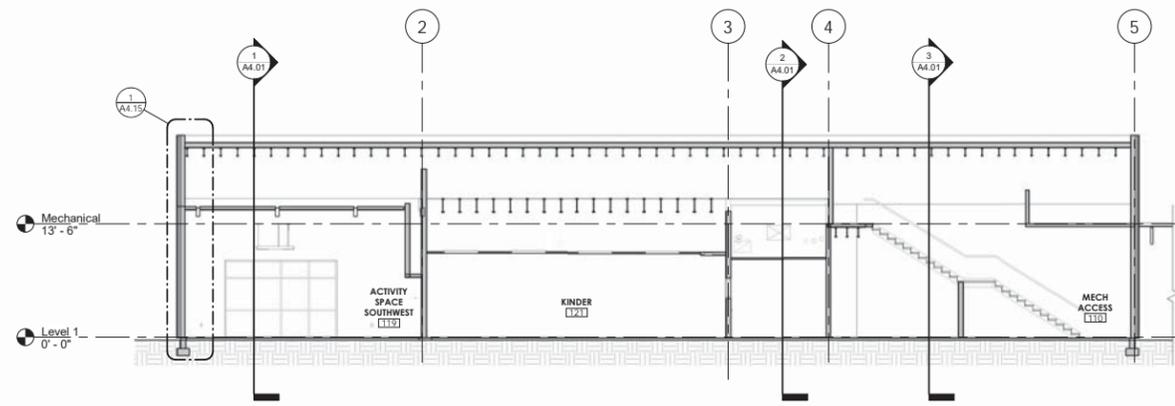
NOT FOR CONSTRUCTION

**ASHLAND SCHOOL DISTRICT
HELMAN ELEMENTARY SCHOOL ADDITION & RENOVATION**

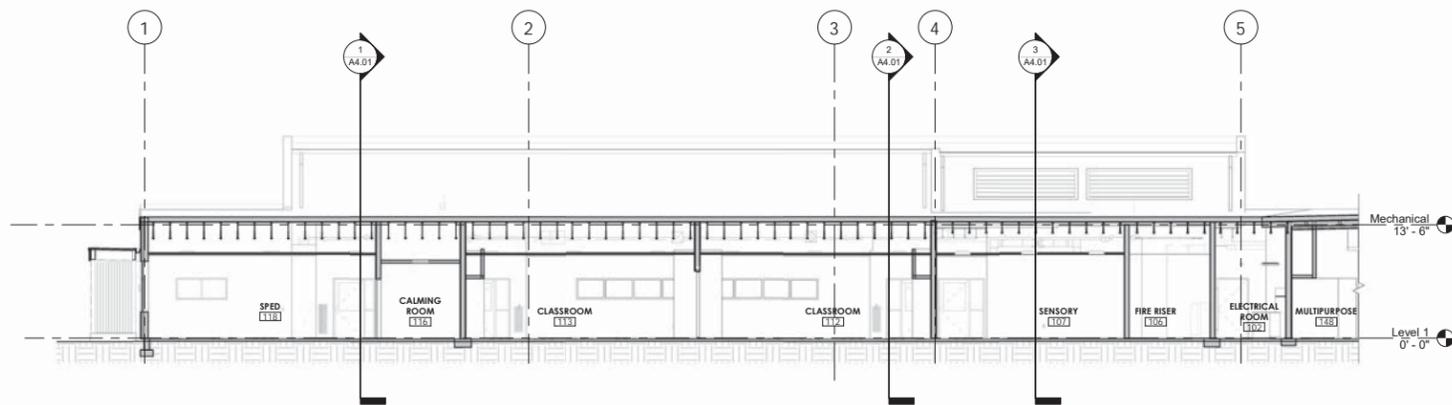
705 HELMAN ST.
ASHLAND, OR 97520



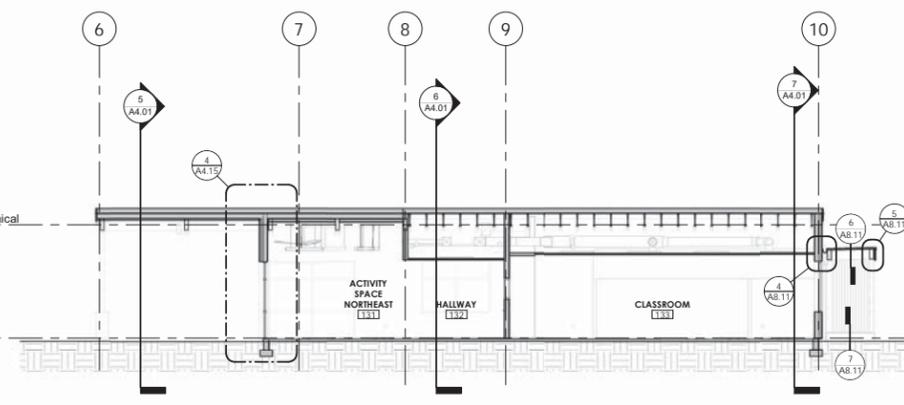
1 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"



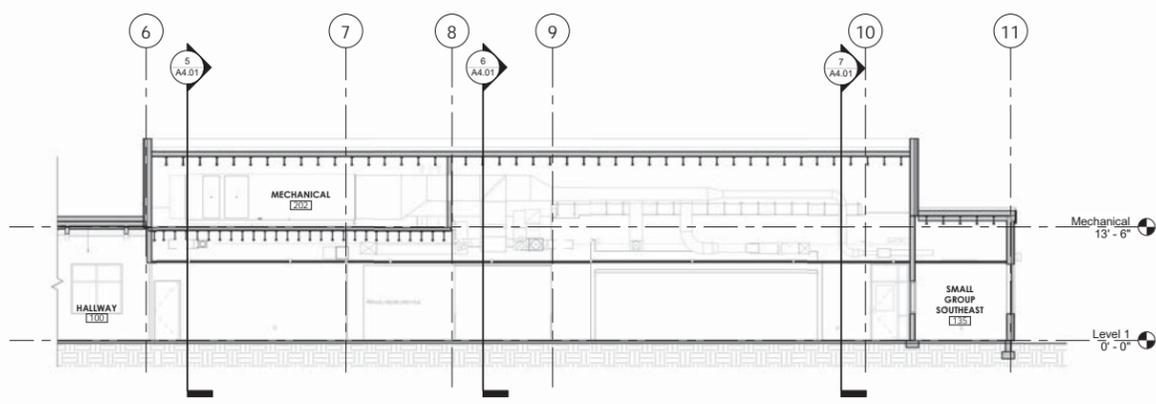
2 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"



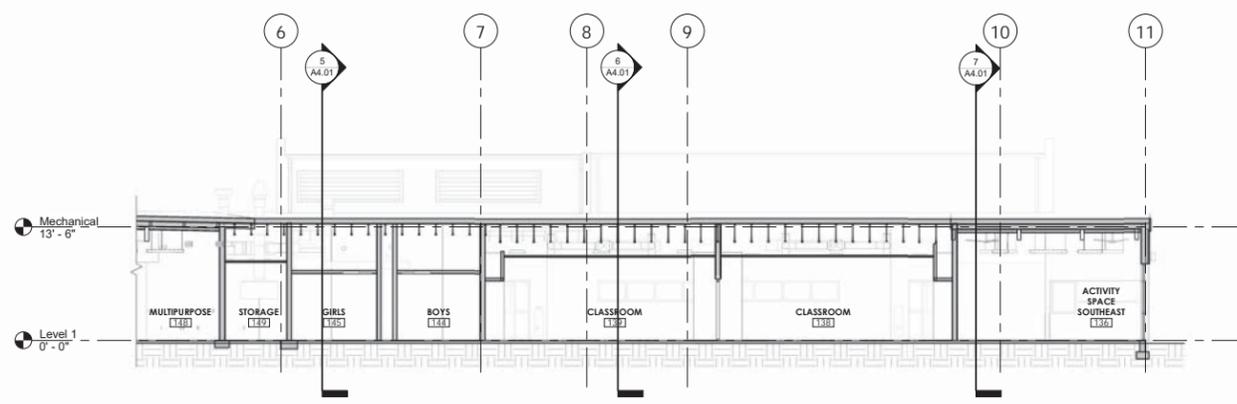
3 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"



4 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"



5 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"



6 BUILDING SECTION
A4.02 SCALE: 1/8" = 1'-0"

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Project Number 1907
Date 06.05.2020

LAND USE

BUILDING SECTIONS

A4.02

GENERAL NOTES

- A. SEE SHEET G0.01 FOR ARCHITECTURAL ABBREVIATIONS AND SYMBOLS
- B. SEE G0.05 FOR ASSEMBLY TYPES
- C. SEE A6.10 & A6.11 FOR CEILING TYPES
- D. SEE A9.10 & A9.11 FOR FLOORING TYPES
- E. ALL ROOMS TO HAVE RUBBER BASE, RB-1, UNO
- F. ALL GWB SOFFITS TO BE PAINTED P-1, UNO

| ROOM FINISH SCHEDULE - ADDITION | | | | | | | |
|---------------------------------|--------------------------|----------------|-----------------|-----------------|------------|-----------|--------------------------|
| NUMBER | NAME | CEILING FINISH | WALL FINISH | | | | COMMENTS |
| | | | NORTH (A) | EAST (B) | SOUTH (C) | WEST (D) | |
| 100 | HALLWAY | | P-1, WP-1 | P-1 | P-1, WP-1 | P-1 | |
| 101 | OFFICE | | P-3 | P-1 | P-1 | P-1 | |
| 102 | ELECTRICAL ROOM | | P-1 | P-1 | P-1 | P-1 | |
| 103 | RESTROOM | P-1 | WP-5 | WP-5 | CT-1 | WP-5 | |
| 104 | RESTROOM | P-1 | WP-5 | WP-5 | CT-1 | WP-5 | |
| 105 | IT | | P-1 | P-1 | P-1 | P-1 | |
| 106 | FIRE RISER | | P-3 | P-1 | P-1 | P-1 | |
| 107 | SENSORY | | P-1 | P-1 | P-1 | P-1 | |
| 108 | SENSORY STORAGE | P-1 | P-1 | P-1 | P-1 | P-1 | |
| 109 | HALLWAY | | P-1, WP-1, WP-2 | P-1 | P-1, WP-1 | P-1 | |
| 110 | MECH ACCESS | | P-1 | P-1 | P-1 | P-1 | |
| 111 | RECYCLE | P-1 | P-1 | P-1 | P-1 | P-1 | |
| 112 | CLASSROOM | | P-1 | P-1 | P-2 | P-1, P-3 | |
| 113 | CLASSROOM | | P-3 | P-1, P-6 | P-1 | P-1 | |
| 114 | CUSTODIAL | P-1 | P-1 | P-1, FRP-1 | P-1, FRP-1 | P-1 | |
| 115 | SPEED RESTROOM | P-1 | WP-4 | CT-1 | WP-4 | WP-4 | |
| 116 | CALMING ROOM | P-1 | P-1 | P-1 | P-1 | P-1 | |
| 117 | SPEED OFFICE | | P-1 | P-1 | P-1 | P-1 | |
| 118 | SPEED | | P-3 | P-1 | P-1 | P-1 | |
| 119 | ACTIVITY SPACE SOUTHWEST | P-2 | P-1 | P-1, P-2, WP-2 | P-1, WP-1 | P-1 | GWB SOFFIT TO BE PAINTED |
| 120 | HALLWAY | | P-4, WP-2 | P-1, WP-1 | P-1 | P-1, WP-1 | |
| 121 | KINDER | | P-1, P-6 | P-1 | P-1 | P-1 | |
| 122 | KINDER RESTROOM | P-1 | CT-1 | CT-1 | WP-5 | WP-5 | |
| 123 | KINDER RESTROOM | P-1 | CT-1 | CT-1 | WP-5 | WP-5 | |
| 124 | KINDER | | P-1 | P-1 | P-1, P-5 | P-1 | |
| 125 | SMALL GROUP NORTHWEST | | P-1 | P-1 | P-1 | P-1 | |
| 126 | ACTIVITY SPACE NORTHWEST | P-4 | P-1 | P-1, WP-1 | P-1, WP-1 | P-1, P-4 | GWB SOFFIT TO BE PAINTED |
| 127 | VESTIBULE | P-1 | P-3 | P-1 | P-1 | P-1 | |
| 128 | CLASSROOM | | P-1, P-2 | P-1 | P-1 | P-1 | |
| 130 | CLASSROOM | | P-1 | P-1, P-2 | P-1 | P-1 | |
| 131 | ACTIVITY SPACE NORTHEAST | P-3 | P-1 | P-1, WP-1 | P-1 | P-1, P-3 | GWB SOFFIT TO BE PAINTED |
| 132 | HALLWAY | | P-1, WP-1 | P-1, WP-1 | P-1 | P-1 | |
| 133 | CLASSROOM | | P-1 | P-1 | P-1 | P-1, P-6 | |
| 134 | CLASSROOM | | P-1 | P-1, P-5 | P-1 | P-1 | |
| 135 | SMALL GROUP SOUTHEAST | | P-1 | P-1 | P-1 | P-1 | |
| 136 | ACTIVITY SPACE SOUTHEAST | P-3 | P-1, WP-1 | P-1, P-3 | P-1 | P-1 | GWB SOFFIT TO BE PAINTED |
| 137 | HALLWAY | | P-3 | P-1, WP-1, WP-2 | P-3, WP-2 | P-1, WP-1 | |
| 138 | CLASSROOM | | P-1, P-4 | P-1 | P-1 | P-1 | |
| 139 | CLASSROOM | | P-1 | P-1 | P-1, P-6 | P-1 | |
| 140 | RECYCLE | P-1 | P-1 | P-1 | P-1 | P-1 | |
| 141 | MECH ACCESS | | P-1 | P-1 | P-1 | P-1 | |
| 142 | CLOSET | P-1 | P-3 | P-1 | P-1 | P-1 | |
| 143 | ENTRY | P-1 | WP-1 | P-1 | WP-1 | CT-1 | |
| 144 | BOYS | P-1 | CT-1 | CT-1, WP-3 | WP-3 | WP-3 | |
| 145 | GIRLS | P-1 | WP-6 | CT-1, WP-6 | CT-1 | WP-6 | |
| 146 | RESTROOM | P-1 | WP-4 | WP-4 | WP-4 | CT-1 | |
| 147 | OFFICE | | P-1 | P-1 | P-1 | P-1 | |
| 148 | MULTIPURPOSE | | P-1 | P-1 | P-1 | P-1 | |
| 149 | STORAGE | | P-1 | P-1 | P-1 | P-1 | |
| 201 | MECHANICAL | | P-1 | P-1 | P-1 | P-1 | |
| 202 | MECHANICAL | | P-1 | P-1 | P-1 | P-1 | |

| INTERIOR FINISH LEGEND | | | | | | |
|------------------------|---------------------------------------|--------------|--------------------------|-------------------------|---|----------|
| SPEC SECTION | MATERIAL | ABBREVIATION | MANUFACTURER | MODEL NAME / # | FINISH/COLOR | COMMENTS |
| 03 3511 | CONCRETE FLOOR FINISHES | | | | | |
| | POLISHED CONCRETE FLOOR | CON-1 | | | | |
| | SEALED CONCRETE | CON-2 | | | | |
| 06 2000 | FINISH CARPENTRY | | | | | |
| | MEDIUM DENSITY FIBERBOARD | WP-1 | | | CLEAR FINISH | WANSKOT |
| | MEDIUM DENSITY FIBERBOARD | WP-2 | | | PAINTED | |
| | WOOD PLANK CEILINGS | WD-1 | | | CLEAR FINISH | |
| | WOOD HOOKS | WD-2 | | | MATCH DOORS | |
| 06 4100 | ARCHITECTURAL WOOD CASEWORK | | | | | |
| | PLASTIC LAMINATE | PL-1 | FORMICA | | COUNTER TOPS: 8830 ELEMENTAL CONCRETE | |
| | | PL-2 | FORMICA | | CASEWORK: 765-58 NATURAL MAPLE | |
| 06 8316 | FIBERGLASS REINFORCED PANELING | | | | | |
| | WALL PROTECTION | FRP-1 | MARLITE | | WHITE S100G | |
| 08 1414 | FLUSH WOOD DOORS | | | | | |
| | WOOD DOOR | DR-1 | VT INDUSTRIES | | WHITE MAPLE VENEER, CLEAR FINISH | |
| 09 3000 | TILING | | | | | |
| | CERAMIC TILE | CT-1 | DALTILE | LINEAR 6" X 24" | SEMGLOSS, COLOR: ARCTIC WHITE 0190 | |
| 09 5100 | ACOUSTICAL CEILINGS | | | | | |
| | ACOUSTICAL CEILING TILE | ACT-1 | ARMSTRONG | | CORTEGA SECOND LOOK II | |
| | | ACT-2 | ARMSTRONG | | CERAMAGUARD FINE FISSURED 608 | |
| 09 6500 | RESILIENT FLOORING | | | | | |
| | RESILIENT TILES | MRT-1 | FORBO | MARMOLEUM MCT | MCT-629 EIGER | |
| | | MRT-2 | FORBO | MARMOLEUM MCT | MCT-3888 STONE | |
| | RUBBER BASE | RB-1 | TARKETT | | BURNT UMBER 4" | |
| 09 6723 | RESINOUS FLOORING | | | | | |
| | EPOXY FLOORING | EP-1 | DURAFLEX | POLYCRETE MD | | |
| 09 6813 | TILE CARPETING | | | | | |
| | CARPET TILE | CPT-1 | TARKETT | MODULAR 24" X 24" | PATTERN: FORMATION 11251, COLOR: PALE UMBER 58202 | |
| | | CPT-2 | TARKETT | MODULAR 24" X 24" | PATTERN: FORMATION 11251, COLOR: CAERULEUS 58210 | |
| | | CPT-3 | TARKETT | MODULAR 24" X 24" | PATTERN: FORMATION 11251, COLOR: VERTE 58211 | |
| | WALK OFF MAT | WOM-1 | SHAW | STEPPIN' OUT, 24" X 24" | PATTERN: WELCOME II 51031, COLOR: STERLING 31557 | |
| 09 8414 | ACOUSTIC FABRIC WALL SYSTEMS | | | | | |
| | ACOUSTICAL WALL PANELS | AWP-1 | | | GULFORD OF MAINE: FR701 2100, COLOR: CEMENT MIX 750 | |
| | | AWP-2 | | | GULFORD OF MAINE: FR701 2100, COLOR: FIR 743 | |
| | | AWP-3 | | | GULFORD OF MAINE: FR701 2100, COLOR: COBALT 420 | |
| 09 9000 | PAINTING | | | | | |
| | PAINT | P-1 | MILLER | | FIELD | |
| | | P-2 | MILLER | | GRAY | |
| | | P-3 | MILLER | | ACCENT 1 - BLUE | |
| | | P-4 | MILLER | | ACCENT 2 - GREEN | |
| | | P-5 | MILLER | | ACCENT 3 - DARK GREEN | |
| | | P-6 | MILLER | | ACCENT 4 - YELLOW | |
| 10 1101 | VISUAL DISPLAY BOARDS | | | | | |
| | MARKER BOARD | MB | CLARIDGE | | | |
| | TACKABLE WALL SURFACE | TWS-1 | FORBO | | HELM 8321-50 | |
| | | TWS-2 | -- | | TEACHING WALL | |
| | GLASS WRITING SURFACE | GWS | CLARUS | | | |
| 10 2113.13 | METAL TOILET COMPARTMENTS | | | | | |
| | TOILET PARTITIONS | TP | HADRAN | | | |
| 10 2400 | WALL COVERINGS | | | | | |
| | WALL PROTECTION | WP-3 | CONSTRUCTION SPECIALTIES | ACROVYN | BLUESTONE | |
| | | WP-4 | CONSTRUCTION SPECIALTIES | ACROVYN | ASPEN | |
| | | WP-5 | CONSTRUCTION SPECIALTIES | ACROVYN | SERENE GREEN | |
| | | WP-6 | CONSTRUCTION SPECIALTIES | ACROVYN | BUTTERCREAM | |
| 10 2401 | WALL AND CORNER GUARDS | | | | | |
| | CORNER GUARDS | CG-1 | | | 1" STAINLESS STEEL | |
| 12 2400 | WINDOW SHADES | | | | | |
| | ROLLER SHADES | RS-1 | MECHO | ECOVEIL FABRIC | 1563 GREY | |

| EXTERIOR FINISH LEGEND | | | | | | |
|------------------------|--|--------------|-----------------------|----------------|-----------------------|---------------|
| SPEC SECTION | MATERIAL | ABBREVIATION | MANUFACTURER | MODEL NAME / # | FINISH/COLOR | COMMENTS |
| 04 2000 | UNIT MASONRY | | | | | |
| | CONCRETE MASONRY UNIT | CMU-1 | | | | SERVICE WALL |
| 05 1200 | STRUCTURAL STEEL FRAMING | | | | | |
| | STRUCTURAL STEEL EXPOSED | -- | | | PAINTED - DARK BRONZE | CANOPES |
| 05 5000 | METAL FABRICATIONS | | | | | |
| | DECORATIVE STEEL PLATES | -- | | | PAINTED - DARK BRONZE | CANOPES |
| 05 5213 | PIPE AND TUBE RAILINGS | | | | | |
| | RAILINGS | -- | | | PAINTED - DARK BRONZE | ALL RAILINGS |
| 06 1800 | GLUED-LAMINATED CONSTRUCTION | | | | | |
| | GLU-LAM BEAMS | -- | | | | |
| 06 2000 | FINISH CARPENTRY | | | | | |
| | WOOD SOFFITS | WD-3 | | | | |
| 07 4213 | METAL WALL AND SOFFIT PANELS | | | | | |
| | METAL WALL PANELS | MP-1 | TAYLOR METALS | BOX-RIBBED | DARK BRONZE | |
| 07 4646 | FIBER CEMENT SIDING | | | | | |
| | FIBER CEMENT PANELS | FC-1 | AMERICAN FIBER CEMENT | CEMBRIT PATINA | P545 (911 SAND) | |
| | | FC-2 | AMERICAN FIBER CEMENT | CEMBRIT PATINA | P313 (915 TURFA) | |
| 07 4200 | SHEET METAL FLASHING & TRIM | | | | | |
| | SHEET METAL | -- | | | PAINTED - DARK BRONZE | MAIN BLDG |
| | | -- | | | PAINTED - COPPER | CANOPES |
| 08 1113 | HOLLOW METAL DOORS AND FRAMES | | | | | |
| | HOLLOW METAL DOOR | -- | | | PAINTED - COPPER | TO MATCH FC-1 |
| | | -- | | | PAINTED - BEGE | TO MATCHFC-2 |
| | | -- | | | PAINTED - DARK BRONZE | TYPICAL |
| | HOLLOW METAL FRAME | -- | | | PAINTED - COPPER | TO MATCH FC-1 |
| | | -- | | | PAINTED - BEGE | TO MATCHFC-2 |
| | | -- | | | PAINTED - DARK BRONZE | TYPICAL |

BBT ARCHITECTS
 1140 SW Simpson Ave., Suite 200
 Bend, Oregon 97702
 T 541.382.5535 | F 541.389.8033

NOT FOR CONSTRUCTION

**ASHLAND SCHOOL DISTRICT
 HELMAN ELEMENTARY SCHOOL ADDITION & RENOVATION**

705 HELMAN ST.
 ASHLAND, OR 97520

| No. | Description | Date |
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Project Number 1907
 Date 06.05.2020

LAND USE

FINISH SCHEDULES

A9.01



NORTH ELEVATION FROM NORTH COURTYARD



NORTH COURTYARD LOOKING SOUTHEAST



SOUTHWEST ELEVATION LOOKING EAST



SOUTHWEST CORNER LOOKING EAST



AMPHITHEATER



SOUTH COURTYARD LOOKING WEST



EAST ELEVATION



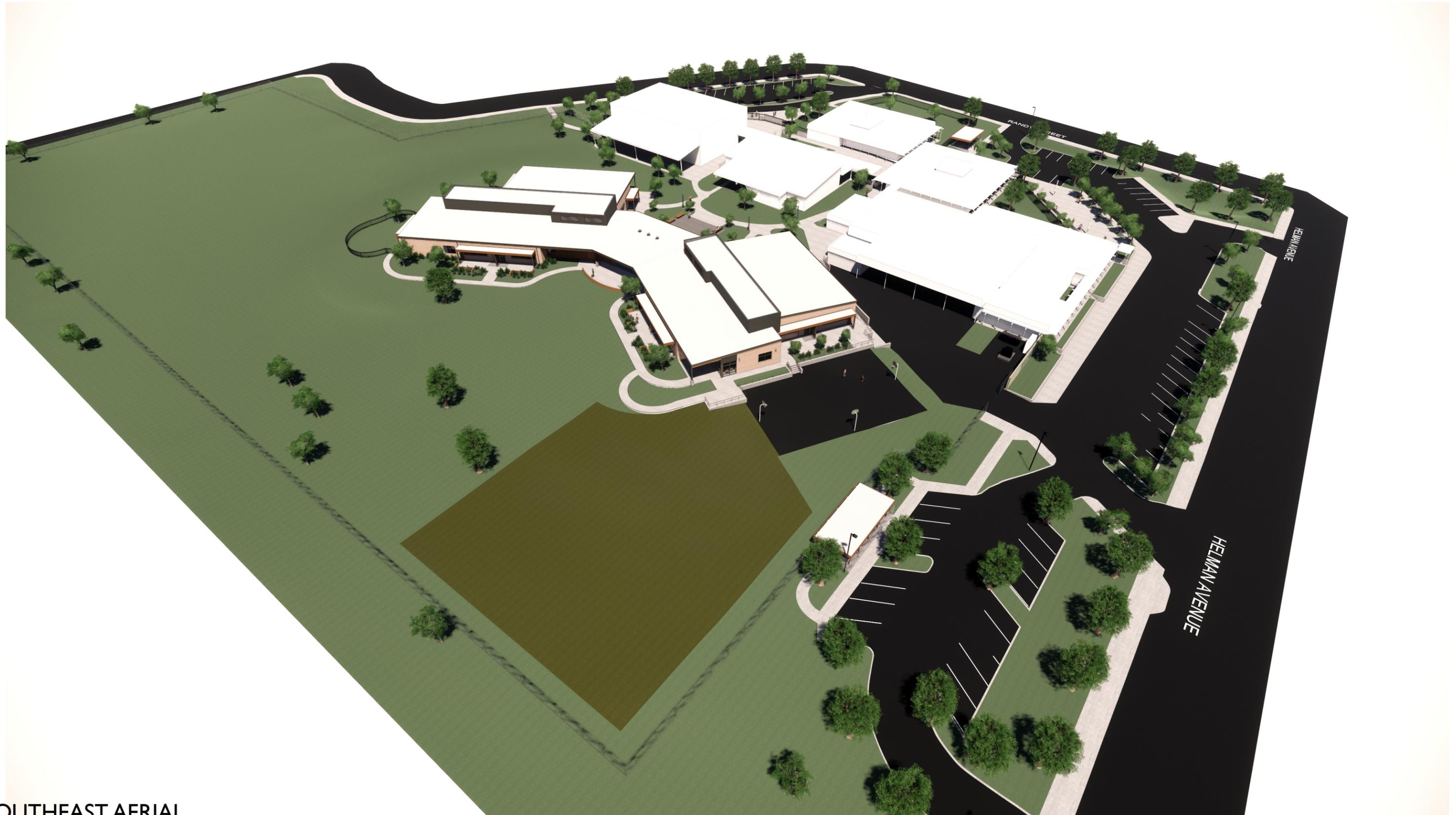
OUTDOOR CLASSROOM SPACE

OUTDOOR CLASSROOM ELEVATION



BASKETBALL COURTS

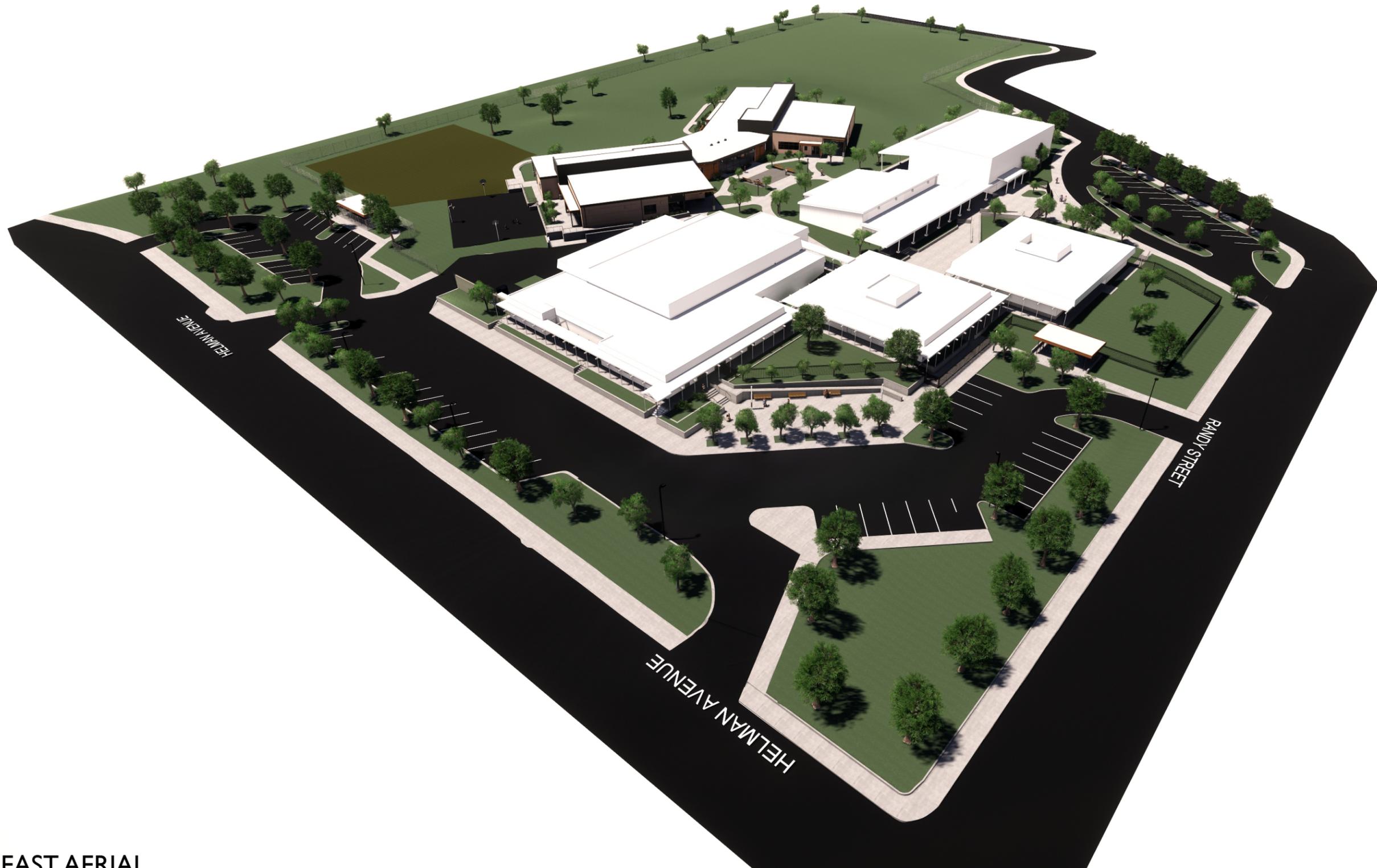
SOUTHEAST CORNER LOOKING NORTH



SOUTHEAST AERIAL

LAND USE RENDERS

Received 7.1.2020



NORTHEAST AERIAL

LAND USE RENDERS

Received 7.1.2020