

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
September 12, 2017  
AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center Council Chambers, 1175 E. Main Street
- II. **ANNOUNCEMENTS**
- III. **AD-HOC COMMITTEE UPDATES**
- IV. **CONSENT AGENDA**  
A. **Approval of Minutes**  
1. August 8, 2017 Study Session.  
2. August 22, 2017 Regular Meeting.
- V. **PUBLIC FORUM**
- VI. **UNFINISHED BUSINESS**  
A. **Adoption of Findings for PA-2017-01199, 707 Helman Street.**
- VII. **TYPE II PUBLIC HEARINGS**  
A. **PLANNING ACTION: PA-2017-00406**  
**SUBJECT PROPERTY: 2300 Siskiyou Boulevard**  
**OWNER/APPLICANT: Jake Hayes & Angie Renick-Hayes**  
**DESCRIPTION: A request for Outline Plan, Final Plan and Site Design Review approval for a seven-lot/six-unit subdivision as Phase III of the West Bellview Subdivision under the Performance Standards Options Chapter (AMC 18.3.9) for the property located at 2300 Siskiyou Boulevard. The application includes requests for: the modification of the West Bellview Subdivision (PA #96-131) to allow additional units, an Exception to the Site Development and Design Standards to allow the placement of two parking spaces between the buildings and the street, and a request for a Tree Removal Permit to remove four trees six-inches in diameter at breast height (d.b.h.) or greater.**  
**COMPREHENSIVE PLAN DESIGNATION: Low-Density Multi-Family Residential; ZONING: R-2; ASSESSOR'S MAP: 39 1E 14CA; TAX LOT #: 7800**
- B. **PLANNING ACTION: PA-2017-01507**  
**SUBJECT PROPERTY: 330 Maple Street**  
**OWNER/APPLICANT: Maple LLC/Rettinger & Associates, Inc.**  
**DESCRIPTION: A request for Site Design Review approval to construct a new 29,400 square foot mixed-use building consisting of basement parking, medical suites on the first floor and two**

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

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**residential units on the second floor for the property located at 330 Maple Street. The application includes: a request for Exception to the Site Development and Design Standards to allow automobile circulation between the building and the street to allow a patient drop-off area from Maple Street; a request for an Exception to Street Standards to allow a smaller than typically-required separation between driveways; and a Tree Removal Permit to remove two trees six-inches in diameter at breast height (d.b.h.) or greater. COMPREHENSIVE PLAN DESIGNATION: Health Care; ZONING: HC; ASSESSOR'S MAP: 39 1E 05DB; TAX LOT #: 2000**

**VIII. ADJOURNMENT**

**CITY OF  
ASHLAND**



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**CITY OF  
ASHLAND**  
**ASHLAND PLANNING COMMISSION**  
**MINUTES**  
**August 8, 2017**

**CALL TO ORDER**

Chair Roger Pearce called the meeting to order at 7:01 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Troy Brown, Jr.  
Michael Dawkins  
Debbie Miller  
Melanie Mindlin  
Haywood Norton  
Roger Pearce  
Lynn Thompson

**Staff Present:**

Bill Molnar, Community Development Director  
Derek Severson, Senior Planner  
Dana Smith, Executive Assistant

**Absent Members:**

None

**Council Liaison:**

Dennis Slattery, absent

**ANNOUNCEMENTS**

Community Development Director Bill Molnar announced staff would attend a study session with the Council in September regarding the cottage housing ordinance. A commissioner would attend the meeting as well.

Chair Pearce announced the Land Use Board of Appeals (LUBA) issued the decision in the McMonagle vs. Ashland appeal. LUBA overturned and remanded the City's decision. Mr. Molnar would forward copies to the Commission.

**AD-HOC COMMITTEE UPDATES**

Commissioner Mindlin noted the Wildfire Mitigation Commission would address the wildfire mitigation ordinance soon.

**CONSENT AGENDA**

**A. Approval of Minutes**

1. July 11, 2017 Regular Meeting.
2. July 25, 2017 Study Session.

Commissioners Dawkins/Norton m/s to approve the July 11, 2017 meeting minutes. **Voice Vote: ALL AYES.** Motion passed. Commissioners Brown and Thompson abstained.

Commissioners Mindlin/Thompson m/s to approve the July 25, 2017 Study Session minutes. **Voice Vote: ALL AYES.** Motion passed.

**PUBLIC FORUM** - None

**UNFINISHED BUSINESS**

- A. Adoption of Findings for PA-2017-01059, 1068 East Main Street.**

Senior Planner Derek Severson explained staff formulated the Findings for open space on page 7 and 8 so the condition was a response to the plans and not a statement of precedent. The Commission wanted to look at it further in a study session.

At Chair Pearce's suggestion, the Commission agreed to the following changes to the Findings:

- Page 4, change **Section 2. Conclusory Findings** to **Section 2. Findings and Conclusions**.
- Page 8, change the first sentence in the last paragraph from **"The Planning Commission finds that the use of the term "open space" appears to be used inconsistently throughout the Land Use Ordinance,"** to **"The Planning Commission finds that the term "open space" appears to be used in different contexts in different sections of the Land Use Ordinance."**

**Commissioners Thompson/Dawkins m/s to approve the Findings for PA-2017-01059, 1068 East Main Street.**

**DISCUSSION:** The Commission declared no ex parte regarding the matter. **Voice Vote: all AYES. Motion passed.**

## **TYPE II PUBLIC HEARINGS**

### **A. PLANNING ACTION: PA-2017-01199**

**SUBJECT PROPERTY: 707 Helman Street**

**OWNER/APPLICANT: PDK Properties**

**DESCRIPTION: A request for preliminary subdivision plat approval to create an eight-lot subdivision for the property located at 707 Helman Street. The application also includes a request for an Exception to Street Standards to install curbside sidewalks along the full frontage of the property where city Street Standards would typically require that a park row planting strip with street trees be installed between the curb and sidewalk. The application also includes a Tree Removal Permit to remove one significant tree (#33) an 18-inch diameter Ponderosa Pine. COMPREHENSIVE PLAN DESIGNATION: Single-Family Residential; ZONING: R-1-5; ASSESSOR'S MAP: 39 1E 04BC; TAX LOT #: 100.**

Commissioner Pearce read aloud the public hearing procedures for land use hearings.

### **Ex Parte Contact**

Commissioners Dawkins, Thompson, Pearce, and Norton declared site visits. Commissioners Brown and Mindlin drove past the site. Commissioner Miller attended the site visit and drove past the site on several occasions. No ex parte contact was reported.

### **Staff Report**

Senior Planner Derek Severson explained this was a Preliminary Subdivision Pat Approval that would create an eight lot subdivision. The application was being processed under the subdivision chapter instead of the performance standards options. It looked at whether the proposal met the dimensional requirements of the zoning and access in utilities, not the base densities and open space requirements. There was also an exception to the street standards to install five-foot curbside sidewalks along the frontage of the property, instead of having a seven foot parkrow between the curb and sidewalk. A tree removal permit was issued to remove a Ponderosa Pine tree.

The subject property was at the corner of Randy Street and Laurel Street despite the Helman address. It was 1.14 acres or approximately 49,901 square feet with a 1,971 square foot house slated for demolition. The current driveway circulated through the property. The applicants wanted to remove 41 trees on the property. In residential zones, only significant trees required a tree removal permit.

The applicant requested a sidewalk exception due to the slope at the edge of Laurel Street that wrapped around Randy Street. A park row would push sidewalk improvements 12 feet back from the curb into the hillside. Having a five-foot sidewalk would lessen the amount of retaining wall needed for installation.

The Building Department approved the house demolition pending Commission approval of the redevelopment plan. The application included a drainage and draining plan with storm drainage detention on site. The utility plan was included. The applicants added solar envelopes illustrating potential eave heights and the setback needed to comply with Solar Standard A in the solar ordinance.

The Tree Commission supported the tree removal request. They wanted a tree protection plan to ensure the trees on neighboring properties were protected particularly during the bioswale installation. The Tree Commission also wanted street trees planted behind the sidewalk if there was not a park row. Another request would maintain the trees in perpetuity in lieu of street trees in a park row. The Commission hoped landscaping for the eight lots would mitigate the removal of the 41 trees.

Staff supported the request. The project would develop eight new earth advantage platinum homes, sidewalks, and limited driveways to two. The project benefited the neighborhood and met the approval criteria with the conditions recommended in the report.

### **Questions of Staff**

Staff was asked if widening the sidewalk at the corner across from the school to create a refuge area was considered. Mr. Severson Derek responded they had not discussed widening that area.

Mr. Severson explained a curb cut with a truncated dome was the thermal plastic piece installed at the curb cut of a crosswalk for ADA access and blind people. This would go in at the southwest corner where the sidewalk crossed to the school.

### **Applicant's Presentation**

**Amy Gunter/Kyle Taylor**/Ms. Gunter explained the property was at the northeast corner of Randy Street and Laurel Street. The 707 Helman Street address was remnant from a larger track when the property was Cherry Knoll Dairy. The knoll consisted of sandstone and granite soils, with rock not far below. A survey slide of the property showed the trees, the existing structure, powerlines and existing driveway. The plan addressed storm drainage down to the square footage of each structure. One change removed the tapered flare at the Randy Street side of the driveway. They did solar envelopes for each lot due to the topography of the site.

Certain lots would have onsite detention for storm water that would drain to Laurel Street and others would drain to Randy Street. The Tree Commission expressed concern for a small deciduous tree located on a neighboring property where lots 3 and 4 came together. An existing six-foot-tall fence protected the tree and eliminated the need for additional protective fencing. Engineering for the bioswale was not developed to the point of knowing if it would require excavation or retaining walls and fill.

Street trees were planted to shade the street. Due to the property's topography, street trees would be planted several feet above the actual street. A landscape architect will work with the property owner on each building permit to address the street tree planting standard.

They would install a 14-foot standard residential street light at the intersection of Laurel Street and Randy Street. Some of the hill would be scraped off to address the grades.

Ms. Gunter spoke against widening the sidewalk at Randy Street to create a refuge for children to cross to the school. A mid-block crossing was dangerous and a crosswalk at that location would not be approved. Widening the sidewalk at Randy Street would require public pedestrian easements or right of way dedication as well as additional retaining walls.

The front yard setback for lot 6 was on Laurel Street. It was 15 feet with a side yard setback of 10 feet. Lot 7 had 15

feet due to the bioswale and potentially two front yards. The official setback was 10-foot.

Sidewalks would be installed prior to the survey plat. Typically, street trees went in the park row and were planted during the street improvements. In this circumstance, the street trees would be in the front yard area. Due to construction disturbing the trees, the applicants were requesting to have trees planted and irrigated on each lot prior to the issuance of the certificate of occupancy for each unit. That allowed the owner to pick their own trees and possibly plant more.

Commissioner Dawkins wanted to add a requirement that street trees were planted sooner, were consistent, and closer to the sidewalk. The applicant could use small caliber trees.

Community Development Director Bill Molnar explained generally sidewalks with a park row were often in the right of way and trees were planted at the same time and water metered. These street trees would be on the private property of each home. Irrigation was an issue. The Commission could require the applicant's to initially install a common meter to irrigate the trees and have it removed once the homes were built. Another issue was grading the sites for each home. He was not sure they could identify space for a street tree or know how grading would affect the tree.

Mr. Taylor addressed the continuous bioswale and explained there will be joint access and private easements granted for the facility. Ms. Gunter added they will have mutual access easements and maintenance agreements. Owners will purchase an agreement requiring them to keep the street trees alive, and maintain the bioswales and driveways.

#### **Public Testimony**

**William Zentner II/134 Cypress Circle/**He did not have a problem with the development. He wanted to know if there was a grading setback from the property line or if the developers could create up to the property line. Would blasting be involved to remove the rock? He opposed the taller street light. These were proposed single story homes but lot 2 and 3 had 7-foot grade drops between the driveway and the back of the house. He wanted to verify the sidewalk wrapped around the entire property. If the existing fence was replaced, would it be a privacy fence or an open one? What types of street trees would be planted and was there a specification? He wanted to know about fence protection while the common swale was under construction. Also, how close would the grading come to the property line.

#### **Applicant's Rebuttal**

Ms. Gunter explained blasting was not allowed in the city. She was not aware of a grading setback requirement. There will be daylight basements on the backside of both homes on lots 2 and 3. By code they were single story structures and the basement would adhere to basement code. The subdivision would use 6.5-foot privacy fencing with possible deer wire. Street tree placement could dictate grading and possibly require small tree protection fences. The timing of planting the trees could also inhibit construction and storing materials on the property. On Laurel Street, the trees would be near the houses on top of the hill.

Commissioner Dawkins commented on street tree placement. If there was not a park row, the tree had to be close enough to the sidewalk to shade the street. Putting the trees up on the slope made them ineffective for street shading. Trees could grow on steep slopes.

Commissioner Mindlin wanted to know the standard on how far back from the sidewalk a tree could be planted to be considered a street tree. The Findings could reference the requirement or the Commission could set one for the project. Mr. Severson thought the slope was rock and it might be difficult to plant unless they used tree wells and had the tree either behind the sidewalk or on top of the retaining wall.

Mr. Molnar addressed the sidewalk refuge and explained the standard for a sidewalk on a residential street was 5-6 feet and the planting strip was 7-8 feet. The Planning Commission had the authority to choose within that range. The standard also allowed for seating in park rows in residential areas. He recommended involving the Public Works

Department for having a crossing from the potential refuge. Mr. Severson added the Public Works Department was hesitant to encourage mid-block crossings but might support one at the intersection of Laurel and Randy. Commissioner Norton did not think the Public Works Department would support a crossing at the location of the refuge if there was on street parking. It was mid-block and on a curve. He would have issues with a crossing there as well.

### **Deliberations & Decision**

Commissioner Brown spoke to **IV. Conclusions and Recommendations 6. k.** regarding street trees. He understood the issues and agreed with Commissioner Dawkins that a tree that far away may not serve the purpose. A temporary watering system could resolve some of the problems. He wanted the landscape architect to review planting street trees on the slope. Commissioner Mindlin wanted to set a standard for tree placement. Mr. Molnar explained the code required one tree for every 30 feet of street frontage. Trees behind the sidewalk could not be closer than 2 feet to the actual sidewalk. Commissioner Miller did not want the street trees to interfere with the construction. Chair Pearce agreed with Commissioner Brown and thought street trees should be planted prior to final approval. Commissioner Dawkins suggested requiring the street trees to be 3-5 feet from the sidewalk. He did not think they would interfere with the construction. Commissioner Brown added the canopy at 15 years must shade the sidewalk.

**Commissioners Dawkins/Brown m/s to approve PA-2017-01199 with the additional language to 6.k. that the trees will be no more than 5 feet from the sidewalk and must be trees that will shade the sidewalk.**

**DISCUSSION:** Commissioner Dawkins liked that the project would build energy efficient units. Commissioner Brown, initially wanted 6-foot sidewalks, but all the other sidewalks were 5 feet and there was no reason to make it wider. He also liked the project.

Commissioner Thompson read #2. In the Tree Commission Findings from August 3, 2017 and thought it should be omitted. She also questioned whether the last sentence in #2 **“These trees shall be maintained in perpetuity in lieu of street trees within the park row planting strip,”** was necessary since it was covered in **IV. Conclusions and Recommendations 6. k.** Commissioner Brown thought it was necessary and that it should go into the overall documentation. Commissioner Thompson suggested adding it to **6.k.** Commissioner Mindlin thought they should adopt the Tree Commission Findings with the Planning Commission modification. Commissioner Thompson suggested in addition to omitting #2 in the Tree Commission Findings, they should add the last sentence to **IV. Conclusions and Recommendations 6. j.** as part of the CC&Rs maintenance of street trees.

Commissioner Dawkins agreed to amend the main motion and delete the first sentence of Section 2 in the Tree Commission recommendation of August 3, 2017 and add the second sentence to **IV. Conclusions and Recommendations 6. j.**

Commissioner Mindlin addressed the last sentence in #2 of the Tree Commission Findings and suggested changing **“These trees shall be maintained in perpetuity in lieu of street trees within the park row planting strip,”** to **“These trees shall be maintained in perpetuity as street trees.”** The Commission agreed to add it to section **6. j.** under **IV. Conclusions and Recommendations.** Commissioner Dawkins accepted the amendment as part of the of main motion. **Roll Call Vote on amended motion: Commissioners Norton, Miller, Dawkins, Brown, Pearce, Thompson, and Mindlin, YES. Motion passed.**

### **ADJOURNMENT**

Meeting adjourned at 8:28 p.m.

*Submitted by,  
Dana Smith, Executive Assistant*

**CITY OF  
ASHLAND**  
**ASHLAND PLANNING COMMISSION**  
**MINUTES**  
**August 22, 2017**

**CALL TO ORDER**

Chair Roger Pearce called the meeting to order at 7:02 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Troy Brown, Jr.  
Michael Dawkins  
Debbie Miller  
Melanie Mindlin  
Roger Pearce  
Lynn Thompson

**Staff Present:**

Bill Molnar, Community Development Director  
Maria Harris, Planning Manager  
Dana Smith, Executive Assistant

**Absent Members:**

Haywood Norton

**Council Liaison:**

Dennis Slattery, absent

**ANNOUNCEMENTS**

Community Development Director Bill Molnar announced a study session with the City Council September 18, 2017, to discuss cottage housing code changes. There is a tentative date in October for a study session with Fregonese & Associates to discuss draft suggestions for the Transit Triangle. The Planning Commission would present the annual commission update during the December 5, 2017, City Council meeting.

Commissioner Dawkins noted Councilor Morris' concerns that Planning Action 2017-01059 for 1068 East Main Street was promoted as workforce housing in the newspaper. The Planning Commission had no control of how an applicant presented a project in the newspaper. However, Commissioner Dawkins thought the Commission might consider defining workforce housing and discuss parameters in a public forum.

Councilor Morris had also been concerned how the applicant could do ownership housing at the location. Mr. Molnar explained multifamily zones R-2 and R-3 have always allowed apartments and ownership. Staff looked through the past twenty years of data on R-2 and R-3. The district was originally created as part of the comprehensive plan with the idea of having a certain percentage of apartments built. The market changed so much since the 1990s that it is more profitable and efficient to do ownership housing. The Croman project had a standard in the multifamily area that limited how much ownership housing could occur and required a certain percentage of rental. The Planning Commission could consider that as a policy if they were interested. Requiring a percentage of rental was currently not in the Ashland Municipal Code.

**PUBLIC FORUM** None

**UNFINISHED BUSINESS**

**A. Accessory residential units within existing homes**

Planning Manager Maria Harris explained adding flexibility for Accessory Residential Units (ARUs) within existing homes came from the Planning Commission's retreat. She provided an example of a three story house with a daylight basement converted into an ARU. Each floor was approximately 800 square feet. In this case, the square footage was over 500 feet and the owner used on street credits to satisfy parking requirements.

Another example was a two story house on a hill. The main house was 2000 square feet with 1400 square feet in the basement. The applicants put the ARU in the top part of the house at 768 square feet. Size limitations and configuration prevented having an ARU on the bottom floor. The owner also used on street credits for two parking spaces. Questions for the Commission to consider were allowing more flexibility in size, possible parking requirements, and making a recommendation to the Council regarding System Development Charges. Other questions were impacts to the neighborhood and allowing two small ARU's instead of one.

ARUs had a kitchen, a separate electric meter and were subject to multifamily residential design. If it was within an existing house and there were no exterior changes it did not really apply in terms of building design. Site planning was parking, having an open space area for the second unit, and landscaping standards.

Some City Council members were interested in creating incentives for homeowners wanting to have an ARU within an existing home. One suggestion was waiving SDCs since it would not have the same impact as building a separate ARU on a lot.

Commissioner Thompson supported making it more flexible but was concerned about changing parking requirements. On street credits would allow the Community Development Department and the Planning Commission to evaluate the impact. Chair Pearce thought parking and the size limit of half the square footage of the primary residence were the most critical restrictions the Commission needed to review. Commissioner Mindlin added SDCs were another barrier.

Chair Pearce recently attended the Real Estate Land Use Mid-Year Conference that included a discussion on Senate Bill (SB) 1051. One statute that applied to all housing stated housing could only be permitted if there were clear and objective standards, not discretionary standards. He thought the City's code was clear and objective. There were two tracks, clear and objective or site review.

SB 1051 would require cities the size of Ashland to allow areas zoned for detached single family dwellings to have the ability to develop at least one accessory dwelling unit for each detached single family dwelling. This would be subject to reasonable local regulations relating to siting and design. It defined an accessory dwelling unit as an interior, attached or detached residential structure used in connection with or as an accessory to a single family dwelling.

Commissioner Miller was in favor of increasing the square footage of an ARU to 70%-75%. It would provide a reasonable living space and storage. Commissioner Mindlin supported loosening the regulations on accessory residential units within existing homes. Commissioner Brown agreed and thought parking could present an issue. He did not think a set number was needed. Parking and the number of bedrooms would constrain the number of units in an existing home. Commissioner Dawkins added having different addresses would also regulate the number of units.

Staff would bring back options to make units within an existing home more flexible and possibly change the name of accessory residential units.

## **B. Continuation of open space discussion**

Community Development Director Bill Molnar explained an issue with the definition of open space was that it referred to land that was in joint ownership. There were inconsistencies within the Ashland Municipal Code that needed to be fixed at some point. Another issue was the 8% requirement for a recreation area when it was referred to as an open space. For the past fifteen years, designers of private developments thought the 8% could either be commonly owned areas, privately and commonly owned, or privately owned. City Council needed to interpret the code. The discussion tonight sought direction from the Commission.

A project had to have a certain percent of the lot landscaped and 8% of the lot area needed to be recreation space. Commissioner Dawkins wanted a better definition of recreation space. Historically, recreation space would have meant lawns and that presently went against water wise landscaping.

Mr. Molnar explained 20 years ago during a site design standards review, the City created the standard requiring a percentage of area in a commercial development for public space. While the standard for residential required 35% landscaping, it could all be ornamental. It raised the question if the City should require a minimum amount that was surfaced usable. Recreation was not the best term either. The intent was having a certain percentage that was usable. That was why patios and porches were added. It leveled out a small area with possible semi private landscaping and discouraged lawns.

The Commission discussed recreational space. Comments equated it to a potential gathering space or community gardens. Other comments questioned the need to have a gathering space. One suggestion would let the developer decide as long as there was 8% open space. The developer could also decide whether the recreational space was privately or commonly owned. Another comment supported having some lawn area for common space. Mr. Molnar thought commonly owned made sense when there were significant natural features like a creek corridor or wetland. Those areas were maintained and protected better over time and recommended retaining that language. He clarified if the 8% had to be in a common area it could not be utilized for private patios. It would reduce the number of patios and porches proposed in order to meet the 8% resulting in more stoops.

Commissioner Mindlin suggested not including door access, walkways or door swings and making the space private but more usable. Mr. Molnar thought the Commission could require a base on minimal use of space using a dimension of not less than 6 feet. Commissioner Mindlin added they needed to define the exclusions for paths to the doors with a minimum area to one side or the other. Usable was defined as a minimum of 6x8 feet. Chair Pearce thought if the Commission wanted to preserve common open space they could address the design issue by giving people a credit of 50% or 60% for private open spaces.

Commissioner Mindlin noted a possible equality of access issue in multiple story complexes. If all common space was private, the upstairs residents might not have any. Mr. Molnar responded if it was private there could be a distribution standard where each unit had to have some. It could be a porch or a deck.

Commissioner Brown cautioned aggregating responsibilities to developers. They should be allowed to go as far as reasonable in order to provide a livable environment. Requirements needed to be defined clearly so the City could say no if necessary.

Planning Commission majority wanted the code clarified on the 8% requirement for open space. They wanted to continue applying it as it had been with options on common, private, or partially common and private space. Porches and decks included in the 8% could not have obstructions. Walkways should not go towards the 8% open space. The Commission wanted open space, common open space, recreational open space, and private open space defined and in the Definitions section of the code. Permitted use as OS open space should be classified as parks because it was public open space.

## **ADJOURNMENT**

Meeting adjourned at 8:22 p.m.

*Submitted by,  
Dana Smith, Executive Assistant*

**BEFORE THE PLANNING COMMISSION**  
**September 12, 2017**

IN THE MATTER OF PLANNING ACTION #2017-01199, A REQUEST FOR )  
PRELIMINARY SUBDIVISION PLAT APPROVAL TO CREATE AN EIGHT-LOT )  
SUBDIVISION FOR THE PROPERTY LOCATED AT 707 HELMAN STREET. THE )  
APPLICATION ALSO INCLUDES REQUESTS FOR: AN EXCEPTION TO STREET )  
STANDARDS TO INSTALL CURBSIDE SIDEWALKS ALONG THE FULL FRONT- )  
AGE OF THE PROPERTY WHERE CITY STREET STANDARDS WOULD TYPI- ) **FINDINGS,**  
CALLY REQUIRE THAT A PARK ROW PLANTING STRIP WITH STREET TREES ) **CONCLUSIONS,**  
BE INSTALLED BETWEEN THE CURB AND SIDEWALK; AND A TREE RE- ) **& ORDERS**  
MOVAL PERMIT TO REMOVE ONE SIGNIFICANT TREE (#33), AN 18-INCH )  
DIAMETER PONDEROSA PINE. )  
)  
)  
)

**OWNER/APPLICANT:** PDK Properties LLC )  
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**RECITALS:**

- 1) Tax lots #100 of Map 39 1E 04BC is located at 707 Helman Street and is zoned R-1-5, Single Family Residential.
  
- 2) The applicants are preliminary subdivision plat approval to create an eight-lot subdivision for the property located at 707 Helman Street. The application also includes requests for: An Exception to Street Standards to install curbside sidewalks along the full frontage of the property where city Street Standards would typically require that a park row planting strip with street trees be installed between the curb and sidewalk; and a Tree Removal Permit to remove one significant tree (#33), an 18-inch diameter Ponderosa Pine. The proposal is outlined in plans on file at the Department of Community Development.
  
- 3) The criteria for Preliminary Plat approval for a new subdivision are described in AMC 18.5.3.070 as follows:
  - A. *Approval Criteria. The approval authority, pursuant to subsection 18.5.3.030.A, may approve, approve with conditions or deny a preliminary subdivision plat on findings of compliance with all of the following approval criteria.*
    1. *The subdivision plan conforms to applicable City-adopted neighborhood or district plans, if any, and any previous land use approvals for the subject area.*
    2. *Proposed lots conform to the requirements of the underlying zone, per part 18.2, any applicable overlay zone requirements, per part 18.3, and any applicable development standards, per part 18.4 (e.g., parking and access, tree preservation, solar access and orientation).*

3. *Access to individual lots necessary to serve the development shall conform to the standards contained in section 18.4.3.080 Vehicle Area Design.*
4. *The proposed streets, utilities, and surface water drainage facilities conform to the standards in chapter 18.4.6, and allow for transitions to existing and potential future development on adjacent lands. The preliminary plat shall identify all proposed public improvements and dedications.*
5. *All proposed private common areas and improvements, if any, are identified on the preliminary plat and maintenance of such areas (e.g., landscaping, tree preservation, common areas, access, parking, etc.) is ensured through appropriate legal instrument (e.g., Covenants, Conditions and Restrictions (CC&R's)).*
6. *Required State and Federal permits, as applicable, have been obtained or can reasonably be obtained prior to development.*

**B. *Conditions of Approval.*** *The approval authority may attach such conditions as are necessary to carry out provisions of this ordinance, and other applicable ordinances and regulations.*

4) The criteria for an Exception to Street Standards are described in AMC 18.4.6.020.B.1 as follows:

- a. *There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.*
- b. *The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.*
  - i. *For transit facilities and related improvements, access, wait time, and ride experience.*
  - ii. *For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.*
  - iii. *For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.*
- c. *The exception is the minimum necessary to alleviate the difficulty.*
- d. *The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.*

5) The criteria for a Tree Removal Permit are described in AMC 18.5.7.040.B as follows:

1. ***Hazard Tree.*** *A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*

- a. *The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.*
  - b. *The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*
2. **Tree That is Not a Hazard.** *A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
- a. *The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.*
  - b. *Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*
  - c. *Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
  - d. *Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
  - e. *The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

6) The Planning Commission, following proper public notice, held a public hearing on August 8, 2017 at which time testimony was received and exhibits were presented. Subsequent to the closing of the hearing, the Planning Commission approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

## SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

## SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the staff report, public hearing testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Preliminary Plat, Exception to Street Standards and Tree Removal Permit approvals meets all applicable criteria for Preliminary Plat approval for a new Subdivision described in AMC 18.5.3.070; for an Exception to Street Standards described in AMC 18.4.6.020.B.1; and for a Tree Removal Permit as described in AMC 18.5.7.040.B.

2.3 The Planning Commission finds that the existing 1,971 square foot home located on top of the knoll near the center of the property was constructed in or around 1910 for the son of Abel Helman, and later served as the home of Clarence and Sadie Williams, and was once known as the "Cherry Knoll Dairy." The property, while outside the adopted historic district, had historic significance but fell into significant disrepair. The applicant has demonstrated that damage to the structure from prior neglect was significant enough that the cost to remedy it would outweigh any reasonable value, and they further suggest that the placement of the home would significantly impact the potential lot layout if the property were to be subdivided.

With this in mind, the applicants have requested a Demolition Permit, noting that the un-insulated home has a failing sandstone and block foundation that lacks footings; has unsupported porches with framing directly in contact with the soil; the interior has been gutted with all wall surfaces stripped; the exterior has significant amounts of rot, with the rear porch completely rotten and lacking a floor; all mechanical, electrical, and plumbing systems are either not in place or are in need of replacement, including a broken sewer lateral. The Demolition Permit application indicated that the first deferred maintenance notes in the County Assessor's records appeared in 1988 and the condition was allowed to worsen for 30 years. The application included a report from a professional structural engineer who determined that the building was unsafe and advised that any consideration of renovation should give serious consideration to the value of the existing structure.

The requested Demolition Permit was approved by the Building Official on June 14, 2017 subject to approval by the Planning Commission of a redevelopment plan for the property. Notice of the Demolition Permit approval was posted on-site and published in the local newspaper, and no hearing was requested during the appeal period.

2.4 The Planning Commission finds that the proposal satisfies all applicable criteria for Preliminary Plat approval for a new Subdivision.

The first approval criterion for preliminary plat approval is that, *“The subdivision plan conforms to applicable City-adopted neighborhood or district plans, if any, and any previous land use approvals for the subject area.”* The subdivision plan conforms to applicable standards, and that there are no neighborhood plans or previous land use approvals that affect the subject property.

The second approval criterion for preliminary plat approval is that, *“Proposed lots conform to the requirements of the underlying zone, per part 18.2, any applicable overlay zone requirements, per part 18.3, and any applicable development standards, per part 18.4 (e.g., parking and access, tree preservation, solar access and orientation).”* The application materials assert that the proposed subdivision conforms to the requirements of the underlying R-1-5 zone, and that there are no overlay zone requirements which apply to the property. The application further indicates that the proposed subdivision complies with the standards in part 18.4, including that each lot will have two off-street parking spaces, the two proposed flag lots will have a third required visitor’s space, and each residence is to have a garage which will accommodate bicycle parking as well.

The solar access requirements of AMC 18.4.8 require that all land divisions creating new lots either demonstrate that all lots are designed to permit the location of a 21-foot high structure with a solar setback which does not exceed 50 percent of the lot's north-south lot dimension, or identify a solar envelope to define the height requirements that will protect the applicable solar access standard. The application includes a table delineating the required setbacks for ten-, 12- and 18-foot eaves for each of the proposed lots to define the height requirements that will protect the applicable solar access standard, as required in the ordinance.

The third approval criterion for preliminary plat approval is that, *“Access to individual lots necessary to serve the development shall conform to the standards contained in section 18.4.3.080 Vehicle Area Design.”* The application indicates that access is consistent with AMC 18.4.3.080, explaining that proposed driveways and parking are to meet the standards of the ordinance and that the number of driveways has been reduced to minimize the impacts of multiple driveways on the streetscape including potential conflicts for pedestrians and vehicles.

The fourth approval criterion for preliminary plat approval is that, *“The proposed streets, utilities, and surface water drainage facilities conform to the standards in chapter 18.4.6, and allow for transitions to existing and potential future development on adjacent lands. The preliminary plat shall identify all*

*proposed public improvements and dedications.”* The application explains that the proposed street improvements are limited to five-foot wide curbside sidewalks along the property’s full frontage, and an Exception to Street Standards has been requested to install these sidewalks without the standard park row planting strip configuration. This Exception is addressed later in this document. The application further notes that the utility plan is consistent with standards, and that there is limited future development potential on adjacent lands as the neighborhood is largely developed. Utilities for the projects are discussed in the application as follows:

- **Water:** The application materials provided note that six-inch water services are available in both the Laurel Street and Randy Street rights-of-way. The application proposes to utilize the existing water meter for one lot, and install seven new meters to serve the remaining lots.
- **Sanitary Sewer:** The application materials provided note that six-inch sanitary sewer services are available in both of the adjacent rights-of-way and will be stubbed out to each lot. The application further notes that the existing sewer line serving the house has failed and will be decommissioned in conjunction with the development.
- **Electric Service:** Electric service is to be undergrounded and the city street light on the south side of Randy Street is to be re-serviced from the new underground transformers. Pedestrian scale streetlights are to be installed where required, and the applicants note that they anticipate street light installation to be required at the intersection of Randy and Laurel Streets.
- **Stormwater Drainage:** The existing home is served by an eight-inch storm drain line in the Laurel Street right-of-way. The application notes that dealing with the site’s stormwater was complicated by a general lack of facilities in the immediate area, and that they propose to place individual stormwater catchment and detention systems around the perimeter of the property that will slow and treat run-off before it is directed into the city system.

Conditions have been included below to require that final utility plans be provided for the review and approval of the Planning, Building, Public Works/Engineering, and Electric Departments prior to the signature of the final survey plat, and that any meters, vaults, transformers, etc. be placed outside of the sidewalk pedestrian corridor.

The fifth approval criterion for preliminary plat approval is that, *“All proposed private common areas and improvements, if any, are identified on the preliminary plat and maintenance of such areas (e.g., landscaping, tree preservation, common areas, access, parking, etc.) is ensured through appropriate legal instrument (e.g., Covenants, Conditions and Restrictions (CC&R’s).”* The application explains that the proposed private shared driveways and turn-around areas will have easements for access and maintenance, and CC&R’s or a similar instrument will be created to address shared easement responsibilities. Conditions have been included below to require that easements be identified on the final plat submittal, and that a draft of the CC&R’s or similar instrument be provided for review and approval of the Staff Advisor prior to the signature of the final survey plat.

The sixth and final approval criterion for preliminary plat approval is that, *“Required State and Federal permits, as applicable, have been obtained or can reasonably be obtained prior to development.”* The applicants explain that to their knowledge, there are no applicable state or federal permits necessary for the development of the site.

2.5 The Planning Commission finds that the application satisfies the applicable criteria for an Exception to Street Standards to install five-foot wide curbside sidewalks for the full frontage of the parent parcel, rather than installing the typically required seven-foot parkrow planting strip with irrigated street trees between the curb and sidewalk.

The first criterion for an Exception is that, *“There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.”* The application asserts that there are physical constraints to creating a parkrow planting strip with a sidewalk, explaining that due to the topography of the property, installation of standard street improvements would require substantial amounts of additional excavation and the construction of retaining walls to retain the slope behind the sidewalk.

The second criterion is that, *“The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable: 1) For transit facilities and related improvements, access, wait time, and ride experience; 2) For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic; and 3) For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.”* The application suggests that the curbside sidewalk will provide for an adequate level of comfort for walking along the roadway, noting also that Randy Street in particular has low amounts of vehicle traffic. The applicants emphasize that the curbside sidewalk would connect with an existing curbside pattern found throughout the neighborhood and would complete the link in the “Safe Routes to School” for Helman Elementary School.

The third criterion is that, *“The exception is the minimum necessary to alleviate the difficulty.”* The application indicates the request to match the existing development pattern with a curbside sidewalk rather than creating taller retaining walls to support cuts necessary to accommodate standard park row planting strip installation is the minimum exception necessary to address topographical constraints.

The fourth criterion is that, *“The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.”* The application asserts that the Exception is consistent with the Purpose and Intent of the Street Standards, which allows for variations to fit the site and situation. The applicants further suggest that the placement of street trees behind the sidewalk will maintain the desired low-speed environment where people feel comfortable and the maximum number of people can walk. The applicants also note that a new curb cut with truncated dome to facilitate pedestrian connectivity across both Laurel and Randy Streets will be installed to improve neighborhood connectivity.

In visiting the site, Planning staff concurred with the applicant’s assessment that particularly along Laurel Street significant excavation and retaining would be needed to accommodate sidewalk and parkrow installation. Slopes along Laurel exceed 35 percent for much of the frontage, and there is as much as an

eight-foot elevation gain in the 15 feet immediately behind the curb. While the Randy Street frontage is less steep, it has two areas where the slopes appear to exceed 35 percent as well, and other areas where the slopes are in excess of 25 percent. The Planning Commission finds that given the slopes and the established sidewalk pattern in the neighborhood, curbside sidewalks are an appropriate frontage treatment.

The Planning Commission finds that a standard parkrow configuration accommodating street trees between the curb and sidewalk is intended to establish canopy trees between pedestrians and cars to provide a physical buffer and calm traffic but also to provide canopy coverage to shade the sidewalk, reduce the ‘heat island effect’ and absorb carbon monoxide. While the Commission finds that appropriately selected street trees with irrigation placed behind a curbside sidewalk seems the best option for the long-term establishment of healthy canopy coverage here, Commissioners further find that given site topography across the full extent of the frontage it is important that street trees be selected to provide the best canopy coverage, planted no more than five feet from the back of the sidewalk, and planted with irrigation in place prior to signature of the final survey plat so that the desired canopy can begin to establish itself as soon as possible.

2.6 The Planning Commission finds that all planning actions are required to include a Tree Preservation/Protection Plan in accordance with AMC 18.4.5.030 which addresses all trees greater than six-inches in diameter at breast height (d.b.h.) on the property and on adjacent properties within 15 feet of the property line. With regard to protecting trees on adjacent properties within 15 feet of the property line, the application suggests that trees on the property to the north are protected by a six-foot tall solid wood fence and that homes will be sufficiently setback from the north property line due to solar setbacks that the neighbors’ trees will not be negatively impacted.

The application notes that there are 41 trees on the property, and that all have been evaluated by an ISA-certified arborist. The application indicates that the arborist found that the trees have been neglected for years and that only a few merit considering preservation. The application further indicates that many of the trees are less than 18-inches in diameter at breast height and as such are “below the threshold of required preservation.” The application suggests that the trees that are in the best health are poorly placed when considered in light of the property’s highest and best use as single-family residentially-zoned property as they fall within the buildable areas, storm water detention facilities, driveways and circulation areas, or where retaining walls will be necessary for the new public sidewalk. The application concludes that due to the trees’ conditions and placement relative to the development of the site, all of the site’s trees will be removed and emphasizes that because of the property’s single family zoning and the presence of a single family residence, it is exempt from the tree removal permit requirements.

The Planning Commission finds that while AMC 18.5.7.020.C.2 does provide that, “*Removal of trees in single family residential zones on lots occupied only by a single family detached dwelling and associated accessory structures, except as otherwise regulated by chapters 18.3.10 Physical and Environmental Constraints and 18.3.11 Water Resource Protection Zones*” is exempt from Tree Removal Permit requirements, in this case the dwelling on the property has been approved for demolition, and the current application proceeds as the redevelopment of a vacant residential property. In AMC 18.5.7.030.B.3, “*Removal of significant trees, as defined in part 18.6, on vacant property zoned for residential purposes*

*including but not limited to R-1... zones,”* requires a Tree Removal Permit. The application has provided written findings for a Tree Removal Permit addressing the removal of Tree #33, an 18-inch diameter Ponderosa Pine.

The Tree Removal findings provided indicate that the tree is located in the building envelope for the proposed Lot #8, and its removal will not have a negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees or existing windbreaks, and will not have a significant negative impact of the tree densities, sizes and canopies. The application emphasizes that the tree is a stand-alone tree, not part of a windbreak, and that a structure will be built in this location eliminating potential erosion and soil stability concerns. The application further suggests that alternative lot layouts were considered, but the applicants ultimately determined that it came down to the expense of losing one entire building lot to preserve the tree given that the critical root zone would extend roughly 31 ½ feet from the tree leaving less than a 30-foot by 60-foot area to accommodate a residence, yard area, driveway and parking. The application concludes that the tree should be considered exempt from removal permit requirements, is not preservable due to its placement and proximity to buildable areas, and will be mitigated with a five- to six-foot tall conifer on one of the private lots in addition to the planting of street trees along the property’s perimeter.

In considering the proposal at its regular meeting on August 3, 2017, the Tree Commission recommended that the application be approved subject to the following recommendations:

- 1) That a Tree Protection Plan prepared by a certified arborist be provided prior to signature of the final survey plat to address the protection of trees on adjacent properties during the site excavation associated with the subdivision infrastructure installation including the bio-swales along the north property line. Tree protection measures shall be installed, inspected and approved prior to any site work including demolition, staging or storage of materials on the subject property.
- 2) That street trees shall be installed behind the sidewalk on the individual street-fronting lots prior to the issuance of each certificate of occupancy. These trees shall be maintained in perpetuity in lieu of street trees within the park row planting strip.
- 3) That the Commission hopes that with the ultimate development of the property, all 41 trees proposed to be removed here can ultimately be mitigated through the planting of required street trees, a five- to six-foot conifer to mitigate the Ponderosa Pine’s removal, and the individual landscaping likely to be planted on the eight individual lots over time.

In considering these recommendations, Planning Commissioners find that #1 and #3 should be incorporated to the conditions of approval below. With regard to recommendation #2, Planning Commissioners find that trees need to be planted prior to signature of the final plat rather than delaying their planting until individual certificates of occupancy in order to allow for the establishment of canopy coverage and off-set that lost in not having a full city standard park row planting strip. Planning Commissioners concur that the trees planted on private lots in lieu of street trees need to be maintained as street trees in perpetuity. The Tree Commission requirements as modified have been incorporated into the conditions below.

### SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Preliminary Plat approval, Exception to Street Standards, and a Tree Removal Permit is supported by evidence contained within the whole record.

The proposal will re-develop a long-neglected property in an otherwise already developed neighborhood with eight new Earth Advantage® Platinum homes, and in so doing construct new sidewalks along the property's full frontage while limiting the number of driveways to two to minimize conflict points in the streetscape opposite Helman Elementary School.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #2017-01199. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2017-01199 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That the recommendations of the Ashland Tree Commission from their August 3, 2017 regular meeting that:
  - a. A Tree Protection Plan prepared by a certified arborist be provided prior to signature of the final survey plat to address the protection of trees on adjacent properties during the site excavation associated with the subdivision infrastructure installation including the bio-swales along the north property line. Tree protection measures shall be installed, inspected and approved prior to any site work including demolition, staging or storage of materials on the subject property; and
  - b. That the Commission hopes that with the ultimate development of the property, all 41 trees proposed to be removed here can ultimately be mitigated through the planting of required street trees, a five- to six-foot conifer to mitigate the Ponderosa Pine's removal, and the individual landscaping installed on the eight individual lots;

shall be conditions of approval, where consistent with the applicable standards and with final approval by the Staff Advisor.

3. That one replacement tree (a five- to six-foot conifer) to mitigate the Ponderosa Pine tree removed shall be planted and irrigated according to the approved plan within 12 months of the Ponderosa Pine's removal.
4. That any new addresses shall be assigned by City of Ashland Engineering Department.
5. That permits shall be obtained from the Ashland Public Works Department prior to any work in the public right of way.
6. That prior to the signature of the final survey plat:
  - a. That a final survey plat shall be submitted within 18 months of the final decision date of the preliminary partition plat for review and approval by the City of Ashland.

- b. All easements for public and private utilities, fire apparatus access, and reciprocal utility, maintenance, and access shall be indicated on the final survey plat as required by the Ashland Engineering Division.
- c. That a final utility plan for the parcels shall be submitted for review and approval by the Planning, Engineering, and Building Divisions prior to signature of the final survey plat. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer lines, storm drain lines and electric services.
- d. That the location and final engineering for all storm drainage improvements associated with the project, shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions prior to signature of the final survey plat.
- e. That the applicant shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located outside of the sidewalk corridor, in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department.
- f. That the electric services shall be installed underground to serve all eight parcels prior to signature of the final survey plat. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.
- g. That the sanitary sewer laterals and water services including connection with meters at the street shall be installed at the applicants' expense for all eight parcels prior to the signature of final survey plat.
- h. That the two new driveway approaches shall be permitted through the Engineering Division. The driveway curb cuts shall be installed, the old/unused curb cuts closed, and all work inspected and approved prior to the signature of the final survey plat.
- i. That the flag drive shall be paved to a 22-foot width as proposed, with a vertical clearance of 13 feet, six inches, and be able to withstand 44,000 pounds prior to the signature of the final survey plat. The flag drive shall be constructed so as to prevent surface drainage from flowing over the private property lines, sidewalks and/or public ways in accordance with AMC 18.5.3.060.G.
- j. That CC&Rs or similar instrument shall be provided for review and approval of the Staff Advisor prior to signature of the final survey plat. The CC&R's shall describe responsibility for the shared use and maintenance of driveways and circulation areas, bio-swales and street trees. Street trees shall be installed behind the sidewalk on the individual street-fronting lots and maintained in perpetuity as street trees.
- k. That street trees, one per 30 feet of street frontage planted no more than five feet from the back of the sidewalk and associated retaining walls and selected to shade the sidewalk, shall be installed on the Laurel and Randy Street frontages prior to the signature of the final survey plat. All street trees shall be chosen from the adopted Street Tree List and shall be

planted in accordance with the specifications contained therein. The street trees shall be irrigated.

- l. Engineered construction drawings for the public sidewalks along Laurel and Randy Streets shall be submitted for review and approval of the Ashland Planning and Engineering Divisions prior to work in the street right-of-way and prior to installation of improvements in the pedestrian corridor. All public improvements including but not limited to five-foot curbside sidewalks, street trees, and street lighting shall be installed to City of Ashland standards across the entire frontage of the property under permit from the Public Works Department and in accordance with the approved plan prior to signature of the final survey plat. If necessary for alignment of frontage improvements, the area for city standard street improvements shall be dedicated as public street right-of-way or a public pedestrian access easement provided. The right-of-way dedication shall be submitted for review and approval of the City of Ashland Engineering and Planning Departments.

7. That the building permit submittals shall include:

- a. Identification of all easements, including but not limited to any public or private utility easements.
- b. Solar setback calculations demonstrating that all new construction complies with Solar Access Standard A and the solar envelopes described in the application. Permit submittals shall include solar calculations in the form of  $[(\text{Height} - 6)/(0.445 + \text{Slope}) = \text{Required Solar Setback}]$  and elevation drawings or cross sections clearly identifying the highest shadow producing point(s) and their height(s) from natural grade to support the calculations.
- c. Demonstration that exterior lighting shall be directed onto the property and shall not directly illuminate adjacent properties. Exterior lighting details including fixture specifications, placement details and shrouding details (*if necessary*) shall be provided on building permit submittals.
- d. Lot coverage calculations including all building footprints, driveways, parking, and circulation areas. Lot coverage shall be limited to no more than fifty percent as required in AMC 18.2.5.030.A.
- e. That storm water from all new impervious surfaces and run-off associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.

8. That prior to the issuance of a building permit:

- a. That all necessary building permits fees and charges, including permits for new electric and water services, and system development charges for water, sewer, storm water, parks, and transportation shall be paid prior to the issuance of a building permit.
- b. The applicable requirements of the Ashland Fire Department, including approved addressing, fire apparatus access including easement and turn-around, firefighter access

pathway, fire flow, hydrant distance and clearance, work area, fire season fire prevention requirements, and weed abatement during construction shall be complied with prior to issuance of the building permit or the use of combustible materials. Fire Department requirements shall be included on the engineered construction documents. If a fire protection vault is required, the vault shall not be located in the sidewalk corridor.

9. That prior to the issuance of a certificate of occupancy:
  - a. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.

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Planning Commission Approval

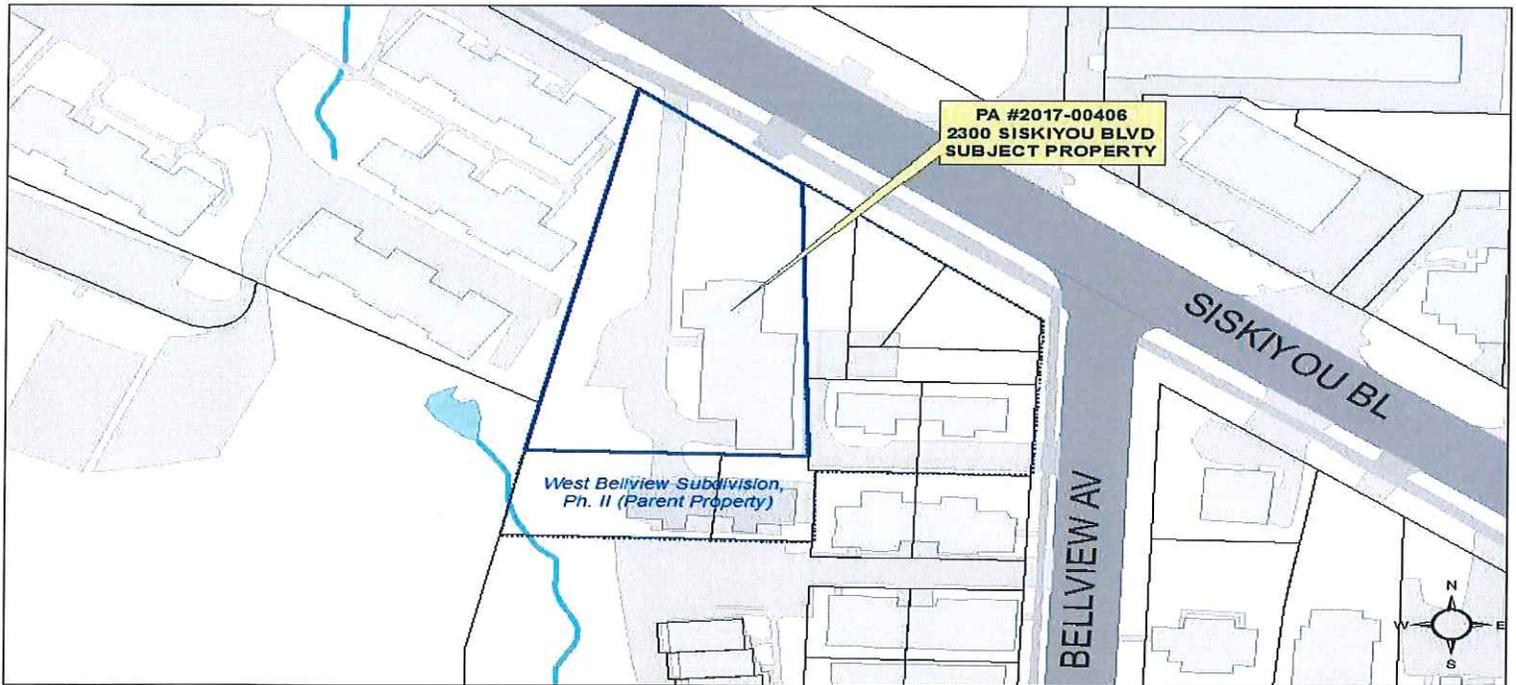
September 12, 2017  
Date



**PLANNING ACTION:** 2017-00406  
**SUBJECT PROPERTY:** 2300 Siskiyou Boulevard  
**OWNER/APPLICANT:** Jake Hayes & Angie Renick-Hayes  
**DESCRIPTION:** A request for Outline Plan, Final Plan and Site Design Review approval for a seven-lot/six-unit subdivision as Phase III of the West Bellview Subdivision under the Performance Standards Options Chapter (AMC 18.3.9) for the property located at 2300 Siskiyou Boulevard. The application includes requests for: the modification of the West Bellview Subdivision (PA #96-131) to allow additional units, an Exception to the Site Development and Design Standards to allow the placement of two parking spaces between the buildings and the street, and a request for a Tree Removal Permit to remove four trees six-inches in diameter at breast height (d.b.h.) or greater. **COMPREHENSIVE PLAN DESIGNATION:** Low-Density Multi-Family Residential; **ZONING:** R-2; **ASSESSOR'S MAP:** 39 1E 14CA; **TAX LOT #:** 7800

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **Thursday, September 7, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING: *Tuesday, September 12, 2017 at 7:00 PM, Ashland Civic Center, 1175 East Main Street***



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102-.35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## OUTLINE PLAN APPROVAL

### 18.3.9.040.A.3

Approval Criteria for Outline Plan. The Planning Commission shall approve the outline plan when it finds all of the following criteria have been met.

- a. The development meets all applicable ordinance requirements of the City.
- b. Adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection, and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.
- c. The existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.
- d. The development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.
- e. There are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.
- f. The proposed density meets the base and bonus density standards established under this chapter.
- g. The development complies with the Street Standards.

## APPROVAL CRITERIA FOR FINAL PLAN

### 18.3.9.040.B.5

Final Plan approval shall be granted upon finding of substantial conformance with the Outline Plan. This substantial conformance provision is intended solely to facilitate the minor modifications from one planning step to another. Substantial conformance shall exist when comparison of the outline plan with the final plan meets all of the following criteria.

- a. The number of dwelling units vary no more than ten percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.
- b. The yard depths and distances between main buildings vary no more than ten percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Ordinance.
- c. The open spaces vary no more than ten percent of that provided on the outline plan.
- d. The building size does not exceed the building size shown on the outline plan by more than ten percent.
- e. The building elevations and exterior materials are in conformance with the purpose and intent of this ordinance and the approved outline plan.
- f. That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.
- g. The development complies with the Street Standards.
- h. Nothing in this section shall limit reduction in the number of dwelling units or increased open space provided that, if this is done for one phase, the number of dwelling units shall not be transferred to another phase, nor the open space reduced below that permitted in the outline plan.

## SITE DESIGN AND USE STANDARDS

### 18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

TREE REMOVAL PERMIT

18.5.7.040.B

1. Hazard Tree. A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. Tree That is Not a Hazard. A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
  - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

**ASHLAND PLANNING DEPARTMENT  
STAFF REPORT**

September 12, 2017

**PLANNING ACTION:** #2017-00406

**OWNER/APPLICANT:** Jake Hayes & Angie Renick-Hayes

**LOCATION:** 2300 Siskiyou Boulevard

**ZONE DESIGNATION:** R-2

**COMP. PLAN DESIGNATION:** (Low Density, Multi-Family Residential)

**ORDINANCE REFERENCES:**

(See also <http://www.codepublishing.com/OR/Ashland/#!/LandUse/index.html#NT> )

- 18.2.4 General Regulations for Base Zones
- 18.2.5 Standards for Residential Zones
- 18.3.9 Performance Standards Options & PSO Overlay
- 18.4.2 Building Placement, Orientation & Design
- 18.4.3 Parking, Access and Circulation
- 18.4.4 Landscaping, Lighting & Screening
- 18.4.5 Tree Preservation and Protection
- 18.4.6 Public Facilities
- 18.4.7 Signs
- 18.4.8 Solar Access
- 18.5 Application Review Procedures and Approval Criteria
- 18.5.2 Site Design Review
- 18.5.3 Land Divisions & Property Line Adjustments
- 18.5.7 Tree Removal
- 18.6.1 Definitions

**APPLICATION DEEMED COMPLETE ON: September 2, 2017**

**REQUEST:** A request for Outline Plan, Final Plan and Site Design Review approval for a seven-lot/six-unit subdivision as Phase III of the West Bellview Subdivision under the Performance Standards Options Chapter (AMC 18.3.9) for the property located at 2300 Siskiyou Boulevard. The application includes requests for: the modification of the West Bellview Subdivision (PA #96-131) to allow additional units, an Exception to the Site Development and Design Standards to allow the placement of two parking spaces between the buildings and the street, and a request for a Tree Removal Permit to remove four trees six-inches in diameter at breast height (d.b.h.) or greater.

**I. Relevant Facts**

## **1) Background - History of Application**

In December 1989, a Minor Land Partition to create two lots was approved (PA #89-221). This approval separated the subject property and the additional parcels in what is now Phase II of the West Bellview Subdivision from its parent property to the south which was developed as the 18-unit West Bellview Subdivision.

In November 1990, the property was re-zoned from High Density Multi-Family Residential (R-3) and Single Family Residential (R-1-7.5) to Low Density Multi-Family Residential (R-2). (PA #90-178/Ordinance #2603). The findings noted that the adoption of site design guidelines, floodplain corridor regulations, and performance standards options had impacted the ability to develop the property to its maximum density, and that the zone change allowed for a more reasonable transition between zones.

In April 1993, an application for Site Review approval for a 20-unit condominium complex was approved (PA #92-132). Building permit plans for this project were submitted and reviewed, but were never issued and the approval was ultimately expired in 1995.

In February 1997, an application for Site Review, Outline and Final Plan approval for a nine-lot multi-family subdivision to consist of one existing home and eight new units was approved (PA #96-131). This approval was granted a one-year extension in April of 1998 (PA #98-079) and the plat for Phase II of the West Bellview Subdivision was recorded creating the subdivision lots and associated easements, although no units were built until after 2011. This approval included Condition #16 which required a deed restriction in favor of the City of Ashland prohibiting the further division of the property. In reviewing the minutes from the 1997 approval, which have been included in the packets, it appears that the Planning Commission was concerned over coordinating transportation issues associated with developing the property, noting that as the properties go into separate ownership the ability to cooperate in terms of shared driveways and easements would be lost. If the current application is approved, the applicants would need to request that the City Council agree to remove the deed restriction in favor of the city.

In April 1998, an application for a Conditional Use Permit and Site Review approval was granted to allow a medical office in the existing residence on the subject property (PA #98-037).

In December 2008, an application for Site Review approval to construct thirteen condominium units was granted approval (PA #2008-00911). This approval was to have replaced the previously approved Performance Standards Options subdivision approved under PA #96-131. The 2008 approval received an 18-month extension in December of 2009 (PA #2009-01467), but ultimately expired without a plat being recorded.

The property sold in 2011, and the new owner began building the units approved under the lot configuration approved and vested with PA #96-131 shortly thereafter.

There are no other planning actions of record for the property.

## **2) Detailed Description of the Site and Proposal**

### ***Site Description***

The subject property consists of a single, roughly trapezoidal parcel with an area of approximately 25,116 square feet, while the broader parent subdivision includes eight additional parcels and a total lot area of approximately 1.16 acres located at the southwest corner of the intersection of Siskiyou Boulevard and Bellview Avenue.

The subject property contains a single-story, approximately 2,974 square foot single family residence near its southeast corner. This home would be demolished to develop the property according to the current proposal.

The subject property is zoned Low Density Multi-Family Residential (R-2). To the east and west, adjacent properties are zoned High Density Multi-Family Residential (R-3). Immediately to the south is the West Bellview Subdivision, an 18-unit multi-family subdivision consisting of five duplexes and eight townhomes zoned Low Density Multi-Family Residential (R-2), and further south properties are zoned Single Family Residential (R-1-7.5). To the north, across Siskiyou Boulevard, properties are zoned High Density Multi-Family Residential (R-3) and Employment (E-1). Immediately to the southwest of the subject property is a 2.41-acre parcel accessed off of upper Clay Street via Chitwood Lane. This parcel is owned by the City of Ashland and intended for future development as a neighborhood park.

The application includes a tree inventory identifying 12 trees of six-inches in diameter at breast height (d.b.h.) or greater on the subject property. Of these, four are proposed to be removed in conjunction with the current proposal, while four additional trees are identified as having been previously removed. It is staff's understanding that three of the four trees identified as having been previously removed were taken out as dead trees to reduce fire hazard in consultation with the Fire Department. The fourth, while in obvious decline, remains in place on the site.

Clay Creek, a Local Stream, is located just to the southwest of the subject property, and its associated water resource protection zone and flood plain traverse the southwesterly corner of the site. Only a small portion of the creek is day-lighted in this vicinity. The creek flows into a detention pond on an adjacent property to the west and is then piped through the Ashlander Apartments property immediately to the west of the subject property.

### ***Current Proposal***

The current application requests Outline Plan, Final Plan and Site Design Review approval for a seven-lot/six-unit subdivision which would be a third phase of the West Bellview Subdivision under the Performance Standards Options Chapter (AMC 18.3.9). The application includes requests for: the modification of the West Bellview Subdivision (PA #96-131) to allow additional units, an Exception to the Site Development and Design Standards to allow the placement of two parking spaces between the buildings and the street, and a request for a Tree Removal Permit to remove four trees six-inches in diameter at breast height (d.b.h.) or greater.

## II. Project Impact

Both Outline Plan approval and the construction of attached housing are subject to Type II procedures and require a decision by the Planning Commission through a public hearing process.

### Outline & Final Plan

The first approval criterion for Outline Plan approval is that, *“The development meets all applicable ordinance requirements of the City.”* The application materials assert that the proposal meets all applicable ordinance requirements and that this criterion has been satisfied.

The second approval criterion for Outline Plan approval is that, *“Adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection, and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.”* The application indicates that adequate key city facilities can and will be provided with the proposal. The application includes a utility plan illustrating services proposed to be provided by the applicants.

- **Water:** The plan provided illustrates the project connecting to the existing water main in Siskiyou Boulevard and providing new meters and services on the frontage to serve the proposed units.
- **Sewer:** The application illustrates a sanitary sewer line connecting to existing facilities across Siskiyou Boulevard and providing connections to the proposed units from within the driveway.
- **Electricity:** The plan provided illustrates extending electrical service from existing facilities on Bellview down Siskiyou Boulevard to a new pull box on the property frontage, then extending service to a new transformer interior to the property and serving the proposed units from that transformer.
- **Urban storm drainage:** The application illustrates a stormwater line connecting to existing facilities in Siskiyou Boulevard and providing a line serving proposed catch basins within the driveway. It is unclear that the storm water requirements for detention and water quality will be addressed, and a condition has been included that these be fully addressed to the satisfaction of the Engineering Division prior to the approval of a building permit.
- **Paved Access & Adequate Transportation:** Siskiyou Boulevard along the property's frontage is a Boulevard or Avenue, and is also a state highway under the jurisdiction of the Oregon Department of Transportation. Siskiyou Boulevard is currently improved with paving including two travel lanes and bike lanes, as well as a parkrow planting strip and a multi-use path which is paved in asphalt. It currently lacks curbs, and there are no street trees within the planting strip. The asphalt path was fully replaced recently by the applicants' team, and the applicants are requesting to defer frontage improvements until they can be planned comprehensively through a local improvement district.

Conditions have been recommended below to require that final electric service, utility and civil plans be provided for the review and approval of the Staff Advisor and city department prior to the signature of the final survey plat, and that the applicants obtain necessary

approvals from the Oregon Department of Transportation, and provide copies of these approvals to the city, prior to any work in the state highway right-of-way.

The third criterion for approval of an Outline Plan is that, *“The existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.”* The application notes that the bank of Clay Creek is the most prominent natural feature of the site, and the applicants have proposed to protect the portion of the floodplain on the property and additional bank area as common open space while expanding and existing easement providing access to the open space. In addition, the applicants note that the trees that are in a condition to be preserved are proposed for preservation and protection as well.

The fourth criterion for approval of an Outline Plan is that, *“The development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.”* The applicants assert that the proposal will not prevent adjacent properties from being developed in a manner consistent with the Comprehensive Plan.

The fifth approval criterion is that, *“There are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.”* The application indicates that there will be a Homeowner’s Association which will hold title to the common driveway and be responsible for its maintenance, and that association rules and guidelines will also determine limitations for fencing, requirements for upkeep of individual units and will hold title including easements to all common elements. A condition of approval has been recommended below to require that a draft copy of the CC&R’s be provided for the review and approval of the Staff Advisor prior to recording of the final survey plat to ensure that there are adequate provisions for the maintenance of open space and common areas, including landscaping and street trees, and appropriate limitations on fencing.

The sixth criterion is that, *“The proposed density meets the base and bonus density standards established under this chapter.”* The subject property is 0.58 acres, and at the base density of 13½ dwelling units per acre has a base density of 7.89 units. Only six units are proposed here.

The final Outline Plan approval criterion is that, *“The development complies with the Street Standards.”* Siskiyou Boulevard along the property’s frontage is a Boulevard or Avenue, and is also a state highway under the jurisdiction of the Oregon Department of Transportation. Siskiyou Boulevard is currently improved with paving including two travel lanes and bike lanes, as well as a parkrow planting strip and a multi-use path which is paved in asphalt. It currently lacks curbs, and there are no street trees within the planting strip. The asphalt path was fully replaced recently by the applicants’ team, and the applicants are requesting to defer frontage improvements until they can be planned comprehensively through a local improvement district. Conditions are recommended below to require that the applicants plant street trees in the park row planting strip and that they sign in favor of a future local improvement district to complete full city standard frontage improvements along the property’s Siskiyou Boulevard frontage.

### **Final Plan**

As noted in AMC 18.3.9.050.B, Final Plan approval is to be granted upon a finding of substantial conformance with the Outline Plan approval. This substantial conformance provision is intended solely to facilitate the potential for minor modifications from one planning step to another as part of what is often a two-step subdivision process. Substantial conformance shall exist when comparison of the Outline Plan with the Final Plan meets the following criteria:

- a. *The number of dwelling units vary no more than ten percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.*
- b. *The yard depths and distances between main buildings vary no more than ten percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Ordinance.*
- c. *The open spaces vary no more than ten percent of that provided on the outline plan.*
- d. *The building size does not exceed the building size shown on the outline plan by more than ten percent.*
- e. *The building elevations and exterior materials are in conformance with the purpose and intent of this ordinance and the approved outline plan.*
- f. *That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.*
- g. *The development complies with the Street Standards.*
- h. *Nothing in this section shall limit reduction in the number of dwelling units or increased open space provided that, if this is done for one phase, the number of dwelling units shall not be transferred to another phase, nor the open space reduced below that permitted in the outline plan.*

In this instance, because Outline Plan and Final Plan are requested concurrently, there is no variation between the Outline Plan and the Final Plan and a simple finding of substantial conformance can be made for all Final Plan criteria.

### **Site Design Review**

The development of attached housing requires Site Design Review approval and is subject to the “Building Placement, Orientation and Design” standards for residential development found in AMC 18.4.2.030.

The first criterion for Site Design Review is that, “*The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*” The application materials provided assert that the proposed development complies with all applicable provisions for the underlying zone detailed in AMC 18.2 including building and yard setbacks, lot area, dimension, density, floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

The second approval criterion is that, *“The proposal complies with applicable overlay zone requirements (part 18.3).”* In this instance, the property is located within the Low-Density, Multi-Family Residential R-2 zoning district but outside of any overlay zones.

The third criterion for the Site Design Review approval is that, *“The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E (which addresses Exceptions) below.”* Lighting is detailed as including front and rear entry lights on each unit as well as three pedestrian scale lights along the driveway. The application indicates these lights are to be down-directed to avoid direct illumination of adjacent properties. The residences fronting on Siskiyou (Units #1 and #6) have been designed to present buildings oriented to the street, with entrances oriented to Siskiyou Boulevard and pedestrian access via the common driveway to the sidewalk.

Building materials and paint colors are to be compatible with the surrounding area, and very bright primary or neon-type paint colors, which attract attention to the building or use, are noted as being unacceptable.

AMC 18.4.3.080.D requires that driveway serving more than seven parking spaces shall be at least 20 feet in width, and the driveway is illustrated as paved to at least a 20-foot width.

Two off-street parking spaces are required for each of the proposed units, and the applicants illustrates that each unit will have one garage space and one surface space available. Private garages also serve to address required bicycle parking.

A condition has been recommended to require that one street tree chosen from the street tree list is to be placed in the parkrow planting strip for each 30 feet of frontage on Siskiyou Boulevard in keeping with section 18.4.4.030.E.

Residential developments that are subject to the provisions of this chapter are also to provide an area equal to at least eight percent of the lot area, or 2,037 square feet in this case, dedicated to open space for recreational use by the tenants of the development. This area may not be covered with shrubs, bark mulch, or other ground covers that do not provide suitable surface for human use. Lawn and decks, patios, and similar areas that provide for recreational use are eligible for consideration as open space. The application indicates that more than eight percent is dedicated to recreational area and open space with approximately 2,143 square feet identified as addressing this standard in the form of common open space including the flood plain corridor boundary and an area above it reserved for recreation space. A condition has been recommended below to require that specific surface treatments to support recreational use of this space consistent with the standards be detailed on a final landscape and irrigation plan.

Landscaping, recycling and refuse disposal areas are required to be provided pursuant to chapter 18.4.4, and the applicants note that individual trash and recycling containers will be provided for each unit and stored in the individual garages.

The fourth approval criterion for Site Design Review addresses city facilities and requires that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and*

*will be provided to the subject property.”* Public facilities requirements are addressed earlier in the Outline Plan section of this report above.

Performance Standards Subdivision are subject to the solar access requirements of AMC 18.4.8 which require that land divisions creating new lots either demonstrate that the lots are designed to permit the location of a 21-foot high structure with a solar setback which does not exceed 50 percent of the lot's north-south lot dimension or to identify a solar envelope to define the height requirements that will protect the applicable solar access standard. In this instance, with units proposes as attached and a large area of street right-of-way to the north, the only area of concern would be internal solar access on Units #3 and #4. The application indicates that demonstration of compliance will be provided, but it has not been included in the application. A condition requiring that it be addressed prior to final plat has been recommended below.

### **Exception to the Site Development and Design Standards**

The application includes a request for an Exception to the Site Development and Design Standards to allow the placement of two surface parking spaces between Unit #6 and Siskiyou Boulevard. There has been difficulty through several iterations of the plan, attempting to accommodate the required driveway width and trees while providing open space which works. In the current configuration, open space has shifted to a common lot adjacent to the flood plain in an attempt to make the best use of this space as an amenity for all residents, rather than trying to address open/recreation space through individual patio spaces. In accommodating this configuration and placing the driveway to preserve trees, the applicants have had to shift two surface parking spaces to between Unit #6 and Siskiyou Boulevard. In staff's assessment this will not negatively impact adjacent properties and is consistent with the standards and is the minimum necessary to alleviate the difficulty. In speaking with neighbors, a number have expressed concern that garage parking is often used as storage pushing parking demand intended for garages to the surrounding streets. It seems that the applicants' efforts to provide a mix of surface and garage spaces helps to address this concern, and in visiting the site staff believes that a combination of low fencing or landscape screening could minimize the impacts of this parking at the street. Conditions have been recommended below to require some screening (either fencing or plantings) between this parking and the street, and that garages be used only for parking rather than as storage.

### **Trees**

All planning actions are required to include a Tree Inventory and Tree Preservation/Protection Plan in accordance with AMC 18.4.5.030. In this instance, the application materials explain that there are 12 trees on the tree survey from January of 2016., and of these three that were listed as dead and one that was listed as failing have been removed as hazards. In addition, Tree #9 was uprooted during a storm in 2016 and has been removed. Trees #5-7 and #10-11 are proposed for removal to accommodate the proposal, while the applicant asserts that with careful pruning they believe that Trees #8 and #12 can be preserved. The applicants are proposing that four feet of the 20-foot driveway be installed in pervious pavement in order to provide water to the root system of Tree #12.

The application suggests that the removal of these trees will not have a negative impact on erosion, soil stability, flow of surface waters, adjacent trees or existing windbreaks, nor will it

impact canopy or species diversity in the vicinity. The applicants note that several diverse specimens were retained during construction of prior phases of the development and that these trees are now thriving. The applicants propose to provide mitigation trees as required.

As this report is being prepared, the application has yet to be reviewed by the Tree Commission. A condition is recommended below to make the recommendations of the Tree Commission from their September 7, 2017 meeting conditions of approval, where consistent with applicable criteria and standards and with final approval by the Staff Advisor.

### **III. Procedural - Required Burden of Proof**

**The criteria for Outline Plan approval from the Performance Standards Options Chapter are detailed in AMC 18.3.9.040.A.3 as follows:**

- a. *The development meets all applicable ordinance requirements of the City.*
- b. *Adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection, and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.*
- c. *The existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.*
- d. *The development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.*
- e. *There are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.*
- f. *The proposed density meets the base and bonus density standards established under this chapter.*
- g. *The development complies with the Street Standards.*

**The criteria for Final Plan approval from the Performance Standards Options Chapter are detailed in AMC 18.3.9.040.B.5 as follows:**

*Final Plan approval shall be granted upon finding of substantial conformance with the Outline Plan. This substantial conformance provision is intended solely to facilitate the minor modifications from one planning step to another. Substantial conformance shall exist when comparison of the outline plan with the final plan meets all of the following criteria.*

- a. *The number of dwelling units vary no more than ten percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.*
- b. *The yard depths and distances between main buildings vary no more than ten percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Ordinance.*
- c. *The open spaces vary no more than ten percent of that provided on the outline plan.*

- d. *The building size does not exceed the building size shown on the outline plan by more than ten percent.*
- e. *The building elevations and exterior materials are in conformance with the purpose and intent of this ordinance and the approved outline plan.*
- f. *That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.*
- g. *The development complies with the Street Standards.*
- h. *Nothing in this section shall limit reduction in the number of dwelling units or increased open space provided that, if this is done for one phase, the number of dwelling units shall not be transferred to another phase, nor the open space reduced below that permitted in the outline plan.*

**The criteria for Site Review approval from the Site Design Review Chapter are detailed in AMC 18.5.2.050 as follows:**

- A. ***Underlying Zone:*** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
- B. ***Overlay Zones:*** *The proposal complies with applicable overlay zone requirements (part 18.3).*
- C. ***Site Development and Design Standards:*** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
- D. ***City Facilities:*** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*
- E. ***Exception to the Site Development and Design Standards.*** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.*
  - 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*
  - 2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*

**IV. Conclusions and Recommendations**

The proposal provides for the redevelopment of an underutilized site more in keeping with the underlying zoning, and in so doing provides common recreation space to take advantage of the site's proximity to Clay Creek as an amenity to all residents of the development, rather than privatizing it into individual backyards.

In staff's view, the application with the attached conditions complies with all applicable City ordinances, and merits approval. Should the Commission concur, staff would recommend that following conditions be attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That the plans submitted for the building permit shall be in conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify the current Site Design Review approval shall be submitted and approved prior to issuance of a building permit.
3. That any new addresses shall be assigned by City of Ashland Engineering Department.
4. That permits shall be obtained from the Oregon Department of Transportation (ODOT) and evidence of approval provided to the Ashland Public Works/Engineering Division prior to any work in the Siskiyou Boulevard state highway right-of-way.
5. That the applicants shall obtain approval of a Demolition Permit through the Building Division prior to the demolition of the existing home if deemed necessary by the Building Official.
6. That all recommendations of the Ashland Tree Commission shall be conditions of approval where consistent with applicable standards and criteria and with final approval the Staff Advisor.
7. That prior to the signature of the final survey plat:
  - a. That a final survey plat shall be submitted within 18 months of the final decision date of the preliminary partition plat for review and approval by the City of Ashland.
  - b. All easements for public and private utilities, fire apparatus access, and reciprocal utility, maintenance, and access shall be indicated on the final survey plat as required by the Ashland Engineering Division.
  - c. That a final utility plan for the parcels shall be submitted for review and approval by the Planning, Engineering, and Building Divisions prior to signature of the final survey plat. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer lines, storm drain lines and electric services. Vaults and meters shall be located outside of the sidewalk corridor, in areas least visible from streets, while considering access needs.
  - d. That the location and final engineering for all storm drainage improvements associated with the project, shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions prior to signature of the final survey plat. Storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building

- permit submittals.
- e. That the applicant shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located outside of the sidewalk corridor, in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department.
  - f. That the electric services shall be installed underground to serve all six proposed units parcels prior to signature of the final survey plat. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.
  - g. That the sanitary sewer laterals and water services including connection with meters at the street shall be installed at the applicants' expense for all six proposed units prior to the signature of final survey plat.
  - h. That the driveway shall be paved to 20-foot width, with a vertical clearance of 13 feet, six inches, and be able to withstand 44,000 pounds prior to the signature of the final survey plat. The driveway shall be constructed so as to prevent surface drainage from flowing over the private property lines, sidewalks and/or public ways in accordance with AMC 18.5.3.060.G.
  - i. That common area and open space improvements (i.e. landscaping and irrigation, etc.) shall be installed or bonded for in accordance with the procedures in the Subdivision chapter prior to signature of Final Survey Plat.
  - j. That CC&Rs for the Homeowner's Association shall be provided for review and approval of the Staff Advisor prior to signature of the final survey plat. The CC&R's shall describe responsibility for the maintenance of all common use-improvements including landscaping, driveways, planting strips and street trees, and include a clear statement that garages are to be used only for parking and not for storage.
  - k. That all fencing shall be consistent with the provisions of the "Fences and Walls" requirements in AMC 18.4.4.060. The draft CC&Rs shall include stipulations on height limitations for front, side and rear yard, and shall note that fences adjacent to common open space areas shall not exceed four feet. The location and height of fencing shall be identified at the time of building permit submittals, and fence permits shall be obtained prior to installation.
  - l. A final landscaping and irrigation plan to include irrigation details satisfying the requirements of the Water Conserving Landscaping Guidelines and Policies found in AMC 18.4.4 which includes: 1) A clear delineation of the proposed treatment of the required recreation/open space; 2) Identification and placement of required street trees and mitigation trees; 3) Identification of the selection and placement of measures to screen the two parking spaces in front of Unit #6 (i.e. sight obscuring landscape screening and/or fencing); 4) Final proposed lot coverage calculations including all building footprints,

driveways, parking, and circulation areas shall be provided for the review and approval of the Staff Advisor. Lot coverage shall be limited to no more than 65 percent as allowed in the R-2 zoning district; 5) Irrigation details satisfying the requirements of the Site Design and Use Standards Water Conserving Landscaping Guidelines and Policies and showing park row improvements

- m. The applicants shall provide solar setback calculations demonstrating that the proposed Lots #3 and #4 comply with the solar access requirements of AMC 18.4.8.040.
  - n. That street trees, one per 30 feet of street frontage, shall be installed on the Siskiyou Boulevard frontage prior to the signature of the final survey plat. All street trees shall be chosen from the adopted Street Tree List and shall be planted in the park row plantings strip in accordance with the specifications contained therein. The street trees shall be irrigated.
  - o. The applicants shall obtain City Council approval to release the current deed restriction on the property which restricts it from further development.
  - p. That the property owner shall sign in favor of a Local Improvement District for the future street improvements, including but not limited to paving, curbs, gutters and storm drainage, park rows with street trees, and sidewalks for Siskiyou Boulevard prior to signature of the final survey plat. The agreement shall be signed and recorded concurrently with the final survey plat.
8. That the building permit submittals shall include:
- a. Identification of all easements, including but not limited to any public or private utility easements or fire apparatus access easements.
  - b. Solar setback calculations in the form  $[(\text{Height}-6)/(0.445 + \text{Slope}) = \text{Required Solar Setback}]$  demonstrating that proposed Lots #3 and #4 comply with the solar access requirements of AMC 18.4.8. Permit submittals shall include elevation drawings or cross sections clearly identifying the highest shadow producing point(s) and their height(s) from natural grade to support the required calculations.
  - c. Demonstration that exterior lighting shall be directed onto the property and shall not directly illuminate adjacent properties. Exterior lighting details including fixture specifications, placement details and shrouding details (*if necessary*) shall be provided on building permit submittals.
  - d. That exterior building materials and paint colors shall be compatible with the surrounding area and consistent with those described in the application materials. Sample exterior building colors shall be provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with the Site Design and Development Standards.
  - e. That storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.
  - f. That the building permit drawings shall including calculations demonstrating

that the proposed buildings comply with the Separation Between Buildings requirements of AMC 18.3.9.070.B.

9. That prior to the issuance of a building permit:
  - a. That the tree protection fencing and other tree preservation measures shall be installed according to the approved plan, inspected and approved by the Staff Advisor prior to any site work including demolition, staging or storage of materials, or the issuance of a building permit. The tree protection shall be chain link fencing six feet tall and installed in accordance with 18.4.5.030.C. and no construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
  - b. That all necessary building permits fees and charges, including permits for new electric and water services, and system development charges for water, sewer, storm water, parks, and transportation shall be paid prior to the issuance of a building permit.
  - c. The requirements of the Ashland Fire Department, including approved addressing, fire apparatus access, and fire sprinklers for Units #3 and #4, shall be complied with prior to issuance of the building permit or the use of combustible materials. Fire Department requirements shall be included on the engineered construction documents. If a fire protection vault is required, the vault shall not be located in the sidewalk corridor.
  
10. That prior to the issuance of a certificate of occupancy:
  - a. That all landscaping and irrigation, including irrigated street trees, shall be installed in accordance with the approved plans, inspected and approved by the Staff Advisor.
  - b. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
MINUTES  
FEBRUARY 11, 1997**

**CALL TO ORDER**

The meeting was called to order at 7:05 p.m. by Chairperson Barbara Jarvis. Other Commissioners present were Finkle, Carr, Howe, Hearn, Armitage and Gardiner. Giordano and Bass were absent. Staff present were McLaughlin, Molnar and Yates.

**PUBLIC FORUM**

No one came forth to speak.

**ELECTION OF OFFICERS**

Armitage nominated Jarvis for Chair, Hearn seconded the nomination, Howe moved to close the nomination. Carr nominated Armitage for Vice Chair, Armitage withdrew his name as he will only be serving another year. Armitage nominated Bass for Vice Chair. Hearn moved to approve the slate as nominated. Finkle seconded the motion and the slate was unanimously approved.

**APPROVAL OF MINUTES AND FINDINGS**

Gardiner moved to approve the Minutes and Findings of the January 28, 1997 Hearings Board, Jarvis seconded the motion and they were approved as amended (add the planning action number and description on the first page).

Carr moved to approve the Minutes and Findings of the January 28, 1997 Regular Meeting, Howe seconded the motion and they were approved. Armitage did not vote as he was not present.

**TYPE II PLANNING ACTIONS**

**PLANNING ACTION 96-131  
REQUEST FOR OUTLINE PLAN AND SITE REVIEW APPROVAL FOR A NINE-LOT SUBDIVISION  
UNDER THE PERFORMANCE STANDARDS OPTION  
2300 SISKIYOU BOULEVARD  
APPLICANT: HARLAN DEGROODT**

**Site Visits and Ex Parte Contacts**

Site visits were made by all but Armitage.

**STAFF REPORT**

Molnar gave a brief history of the site. The hearing was opened in December and continued to this evening. The parcel is at the corner of Bellview and Siskiyou with a large existing house on the property. The application is to divide the site into nine separate tax lots. The house will be retained with the small building being removed and eight new units constructed in four sets of two. There are two designs, one similar to those on Bellview with a garage in between the units and the other design is a townhouse with common wall with the parking in the rear. There is an existing parking area off Siskiyou that will serve the large house. A new driveway will be constructed on Bellview toward Siskiyou to access six units and an existing 20 foot wide driveway that serves the first phase of Bellview Estates will serve two units in this project.

The site is fairly wooded. The project has been reviewed by the Tree Commission. The majority of the large evergreens along Siskiyou will be retained due to the location along the property line. There are Siberian elms on the site and they tend to be problems when anything is developed around them. The applicant will try to incorporate at least one of the two Port Orford cedars (see Condition 12). Staff suggested modifications to the driveway to give the cedar a better chance of survival by curbing more toward the existing residence and doing pavers in that section.

Staff is concerned because many of the private or recreational spaces did not qualify for a yard area. The landscaping plan has been modified to show small lawn areas for each unit with the exception of unit 23 (unit along Bellview). The outdoor space for 23 will be a good sized covered front porch and front yard area. No other common outdoor areas are proposed other than the driveway area. Staff suggested there be some consideration for access back to the creek. The revised plan included in the packet was modified to include a pedestrian easement walkway that will come down behind the private backyards of units 1 and 2.

Howe wondered if there was a pathway along the top of the creek. Molnar said nothing was noted as part of Final Plan but the owner intends to design a natural path along the creek which will allow people to walk along the bank.

The other items mentioned in the initial Staff Report have been addressed by the applicant and meet the criteria. Staff recommended approval of the application with the attached 15 Conditions. Molnar showed a video of the site.

Finkle said that sometimes a condition is included for future traffic lights. McLaughlin said the Commission could include that and require signature in favor of a future light if they found that the traffic will increase enough to require a light at Tolman and Siskiyou.

Finkle commented that in Condition 13, make sure the driveway has pavers.

Jarvis asked what the distance will be between the two curb cuts. Molnar thought it was at the minimum of 50 feet and 100 feet from the intersection. Jarvis wondered if it was good traffic planning to have curb cuts so close with a dense development and more cars coming and going. Also, this is a fairly significant subdivision with no organized open space. It appears the development is hooked to the neighboring development. McLaughlin said they have had several discussions with the applicant less about open space but more about design. The land has been broken up into discreet properties and individual applications filed on those to where it has made it difficult to consolidate access points and creates a situation of choosing between the lesser of two evils: do we put the driveway access onto Siskiyou Boulevard or a driveway 100 feet from Bellview? This parcel is a remnant left over from the first development. As the property goes into separate ownership, the ability to cooperate is lost to shared driveways, easements, etc. The open space was the part not put into the whole package. Staff has done their best to put this together in a package.

Jarvis is concerned that there could be another division of the property. Then there could be 11 units. McLaughlin said if the Commission feels there is a potential for an additional unit, a condition can be attached as if it were a full development plan.

Howe said it is possible that a different buyer could have purchased the proposed parcel where they

could not make use of the drive already set up. There would had to have been an even closer curb cut toward Siskiyou. She is not going to fight the battle that this could be one development. She would like to see a limitation placed so the parcel with the existing house cannot be divided further.

Hearn has similar concerns as Jarvis, however, it is not fair to penalize the applicant just because it is owned by one person when it could be owned by two.

McLaughlin said the original land owner, Mr. Anderson, was adamant about keeping his residential property separate from the portion being developed. In hindsight, what should have been required as part of the partition, was some sort of development plan that would insure an easy development link between the two parcels even if under separate ownership.

Carr thought it looked like it would be a crowded development. She agreed with Jarvis with regard to the open space. She is concerned that three more units could be added at a future date. Carr thought the expense of a traffic light on this development is inordinate.

McLaughlin said the applicant has not proposed 11 lots. The Commission has the option of showing how he will meet the open space requirement or you could limit the development of the three lots.

#### **PUBLIC HEARING**

HARLAN DEGROODT, 706 Oak Knoll, explained he did not have the option to develop the property but after Mr. Anderson died, one of the agreements with Mrs. Anderson is that she wants her house retained as close to its original condition as possible. In the beginning he wanted to make it clear he was not trying to develop the other parcel. He has tried not to disturb the character of the existing house at all. Because there are less than ten units, he has tried to maximize the individual private spaces of the units. He undertook the development in order to integrate this development with what is already there. This is not a second phase of a development. He does not intend to ever come back before this Commission with a development proposal for the property (with the house).

Howe asked if DeGrootd would prefer to have a restriction to never subdivide the property again or redesign as if it were an 11 unit project. DeGrootd said because of his contractual agreement, he would have to go with the deed restriction.

#### Staff Response

Molnar said if there were an 11 lot development, the open space would be 2500 square feet. Included is the viewpoint overlook which is approximately 300 square feet.

#### **COMMISSIONERS DISCUSSION AND MOTION**

McLaughlin suggested wording on Condition 16: That a deed restriction in favor of the City of Ashland be recorded for Lot 27, the existing home, prohibiting further division of the property. (That will not prohibit further planning actions if someone wanted to do, for example, a bed and breakfast.)

The Commissioners did not wish to ask for signature in favor of a traffic light.

Carr moved to approve PA96-131 with the attached Conditions and addition of Condition 16 referencing

a deed restriction to prohibit further development. Add Condition 17 that the new driveway be shifted five feet to the west in order to create additional pervious surface below the Port Orford cedar's dripline. Driveway pavers which allow infiltration of water be installed within the dripline of the tree. Such site improvements to be indicated at the time of signature of survey plat. Armitage seconded the motion and it carried unanimously.

#### OTHER

There will be a joint meeting with the City Council February 25th. Jarvis will be out of town and Howe will be unable to attend. They will be discussing local improvement districts and the draft cellular tower ordinance.

Planning Commissioner's Work Day - Set for May 3rd.

Armitage felt there was a lot of confusion surrounding the definition of building envelopes. Is there a way to give out examples to the public so they can better understand a building envelope versus a building footprint?

Thanks were extended to Finkle for his time and commitment to the Planning Commission.

#### ADJOURNMENT

The meeting was adjourned at 9:00 p.m.



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March 8, 2017  
Revised August 1, 2017

**ZONING PERMIT APPLICATION FOR THE CONSTRUCTION OF SIX UNITS LOCATED AT 2300 SISKIYOU BOULEVARD  
FINDINGS OF FACT**

**NARRATIVE**

The Owner proposes to construct six single story residential units on six pad lots on an existing .58 acre site located at 2300 Siskiyou Boulevard within the City limits of Ashland. The lot is part of an approved, three phase planned unit development known as Siskiyou Bellview. The redevelopment of this existing lot will be the final phase of this development originally approved in 1997. The original approval was for a maximum of 15 units on two over-sized lots located on the southwest corner of Bellview Avenue and Siskiyou Boulevard. Previously approved phases included two lots in the West Bellview Subdivision planned unit development, and six lots on the corner parcel. The owner of 2300 Siskiyou is proposing to create 6 additional units on that site, one less than the maximum density allowed, but still meeting Ashland's overall density goals within the city.

The two original sites contained a number of specimen trees, located primarily around the perimeter of the corner lot. During the construction of new units in Phases 1 and 2 of the project, the safety of specimen trees still surviving (several had died because of old age and lack of care) was addressed and they are looking better because of it. The trees along Siskiyou Blvd in front of number 2300 did not fare as well, however. The current tree survey indicates that most of those surviving trees are in poor shape or dead, and in fact, the city has already removed several. There is only one specimen tree that can still be saved and the proposed site plan accommodates that. In addition, there are two trees at the rear of the property that are in good shape, although not specimen trees that can be accommodated in the landscape design.

In this final phase (Phase 3) of the Siskiyou-Bellview development the current owner proposes to subdivide the remaining original lot into six building lots and one access and open space lot to be owned and managed by an Association of the owners of the 6 newly created lots. The Association will be responsible for driveway maintenance and the general upkeep of the common open space. Individual unit owners will be responsible for their own home exterior maintenance. There exists one pedestrian oriented access easement along the northern edge of the parcel to connect the West Bellview Subdivision to a deeded public space on the opposite bank of Clay Creek. This owner proposes providing an additional easement for the benefit of the newly created lots to the same location paralleling the original easement. At one point a bridge was proposed by the City that would connect this bank with a to-be-created park area on the west side of Clay Creek. To date that has not been developed, but the plan remains.

**PREVIOUS APPROVAL CONDITIONS OF PA #96-131**

The site received final approval on March 11, 1997 for 8 additional units on existing or new lots, with a maximum density of 15 units. That approval was contingent upon meeting 17 conditions, all of which

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were met. One condition requires modification or additional approval, as indicated below, for this proposal to be approved.

*CONDITION 16: "That a deed restriction in favor of the City of Ashland be placed on the lot containing the existing residence prohibiting the further division of the property."*

On July 28, 1998 a document was recorded on the title of the property that states "Agree that Lot 27, in accordance with the conditions for outline and final plan and site review approval in the city of Ashland planning Actions 96-131 and 96-079 is prohibited from being further divided unless approved by the City of Ashland."

From the minutes of the meeting where this action was originally discussed, it appears the developer of the property at the time had a contractual agreement with the original owner of the property in that he agreed to keep the single-family house as a life estate for the original owner's wife, and Condition 16 was a reflection of that developer's informal agreement with the original landowner.

Fast forward 20 years, and the reason for the development limitation is long gone. Today a lone single-family residence in this location is out of character with the surrounding neighborhood and inconsistent with Ashland's stated residential density goals. In brief, I believe the time has come to lift the ban and allow the redevelopment of this lot to be consistent with the neighborhood – and with the Planning Commission's preferences in 1996.

Removing that previous restriction is part of the approval request for this proposed redevelopment.

**OTHER GENERAL DEVELOPMENT ISSUES**

**DENSITY:** The Planning Action of 1996 allowed for the overall density of 15 units for this entire development. Eight additional units were approved under that action, leaving available density of six additional units (plus the existing house. This request is for five additional units (plus one unit replacing the existing single-family residence) for a total density of 14 units on the parcel approved for 15 units. The proposed density is less than the maximum allowed under the 1996 approval.

**NATURAL FEATURES:** The most prominent natural feature is the bank of Clay Creek. The Creek doesn't actually touch this site (it grazes the SW corner of Lot 7801 of Phase II of the Siskiyou Woods PUD), but the Creek flood plain does run along the west boundary of this site. It has been taken in to account in the overall development of this lot.

The original development provided a dedicated reserve area at the NW corner of Lot 7801 and the SW corner of Lot 7800 that was intended to connect at some point in the future with a planned City park to be located on the west side of Clay Creek. In conjunction with that a pedestrian access easement was established to allow residents of Siskiyou-Bellview Phase I access to this area. Under the proposed Phase II plan, an additional pedestrian access easement is proposed to connect the units of Phase II to this platted recreation access area.

**ORIENTATION TO THE STREET:** The two units that front on Siskiyou Boulevard will have front entries that face the street, as well as pedestrian access from the sidewalk.

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**PERIMETER SETBACKS:** Perimeter setbacks meet the requirements of AMC 18.3.9.040.

**OPEN SPACE/RECREATIONAL OPEN SPACE:** We have accounted for open space and recreation space in a number of different ways. In both Phase I and Phase II each unit has its own private open space. In addition, there was always planned to be the open space bisected by the Lot 7800/7801 property line available to residents of both phases of the project. Access to that space is assured by the pedestrian access easement already in place that connect to it.

This final phase is proposing common open space that exceeds the 8% minimum required by the Land Development Code to be located adjacent to the already easement protected open space. In addition, each unit will have its own private outdoor space in the form of a patio or deck.

**WATER RESOURCE PROTECTION:** As mentioned earlier, Clay Creek is a local water resource. While it does not directly connect to this parcel, its potential flood plain does impact the site. That limitation has been taken into account and unit development is outside this sensitive area. The sensitive area is indicated on the proposed site plan.

**FLOOR PLAIN CORRIDOR LANDS:** Again, in this location the City of Ashland Flood Plain map diverges from the FEMA Flood Plain map for this area. The Ashland map is more stringent. We have used the Ashland map to identify the area that is considered flood sensitive. We have attached a copy of that map to this Application for review and reference.

#### **APPLICABLE SECTIONS OF THE LAND DEVELOPMENT CODE TO SUPPORT APPROVAL OF THIS PHASE OF THE DEVELOPMENT**

The following sections of the Ashland Municipal Code are applicable to this Application: AMC 18.3.9.040.A; AMC 18.5.2.050; AMC 18.4.6.020.B.1 and AMC 18.5.7.040.B.2.

**Section 18.1.3 – Legal Lot.** As part of the process for development of this site, six new pad lots will be created, with the remainder lot becoming a parcel controlled by a homeowners' association and owned in common by the 6 pad lot owners. This criterion is met by this project.

**Section 18.2.2.030 – Allowed Uses.** The land is currently zoned R-2, residential medium density. Only single family dwellings are proposed for this development and all will be on their own pad lot. Single family dwellings in either single or paired configurations are allowed in this zone. This criterion is met by this project.

**Section 18.2.3 – Special Uses Standards.** There are no special uses proposed within this development. This criterion is met by this project.

**Section 18.2.4.020 – Mechanical Equipment.** Mechanical equipment for the units will be ground mounted and will be on an interior side of the lot and will be screened from the public right-of-way. This criterion is met by this project.

**Section 18.2.2.030 – Vision Clearance.** No obstructions will be placed within the vision clearance area where the drive meets the public right-of-way. This criterion is met by this project.

**Section 18.2.5.030 – Standards for Residential zones.**  
Front yard setback – 15 feet

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Front porch setback – 8 feet  
Side yard setback – 6 feet  
Rear yard setback – 10 feet, plus 5' for half story.  
Maximum height – 35 feet  
Building Separation for buildings on same site – 1/2 the height of the tallest building.  
Maximum Lot Coverage – 65%  
Minimum Landscaped area – 35%  
Minimum Outdoor Recreation Space – 8% of developed area

Setbacks and areas are graphically shown on the site plan for City review. This criterion is met by this project.

**Section 18.2.5.050 – Affordable Housing.** No development bonuses are requested for affordable units.

**Section 18.2.5.060 – Yard Exceptions.** No yard exceptions are requested for this project.

**Section 18.2.5.080 – Residential Density.** For the R-2 zone density is calculated as 13.5 DU per Acre. For a .58 Acre site the calculated density is 7.83 DUs. This proposal is for 6 units. This criterion is met by this project.

**Section 18.2.5.090 – Standards for New Single Family Dwellings.** Units shall utilize at least two of the following design features to provide visual relief along the front of the residence:

1. Dormers
2. Gables
3. Recessed entries
4. Covered porch entries
5. Cupolas
6. Pillars or posts
7. Bay window (min. 12" projection)
8. Eaves (min. 6" projection)
9. Off-sets in building face or roof (min. 16")

The units employ design features 1, 2, 3, 4, 6, 7, 8 and 9. This criterion is met by this project.

**Section 18.3.1 – Special Overlay Districts.** This site is not located within any special overlay district so this Section does not apply.

**Section 18.3.9 – Performance Standards Option.**

k. A written statement containing an explanation of:

i. *The character of the proposed development and the manner in which it has been designed to take advantage of the Performance Standards concept.* The overall concept of the development is to keep it low-scale, matching the densities and the open spaces of the adjacent development. The new units are primarily one-story to minimize the solar impact and view interruption to adjoining properties. The units will be similar in siding and architectural detailing as the existing units.

ii. *The proposed manner of financing.* Project financing will be private lender.

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iii. *The present ownership of all the land included within the development.* The land is presently owned by this developer.

iv. *The method proposed to maintain common open areas, buildings and private thoroughfares.* The site will be covered by a Homeowners' Association. The Association will hold title to the common driveway and be responsible for its maintenance. Association Rules and Guidelines will also determine limitations for fencing and requirements for the upkeep of individual units.

The Association will also hold title to all common elements (including easements) and be responsible for their maintenance.

v. *The proposed time schedule of the development.* The developer would like to begin construction of the project as soon as permits are issued. In practical terms, that means they are expecting to begin in the Fall of 2017.

vi. *The findings of the applicant showing that the development meets the criteria set forth in this ordinance and the Comprehensive Plan.* See this narrative and findings.

#### **PART 4 – SITE DEVELOPMENT STANDARDS**

##### **Section 18.4.2.030 – Residential Development.**

###### **A. Purpose and Intent.**

###### **1. Crime Prevention and Defensible Space.**

a. *Parking Layout* – Each unit is provided with one garage space. In addition, there are also 6 open parking spaces provided, all located within a few feet of a unit front door and visible from unit windows. In addition, driveways to each unit are deep enough to provide another parking space even though these are not counted towards the overall parking requirement. This criterion is met by this project.

b. *Orientation of Windows* – Windows are located on every building elevation so that vulnerable areas can be easily surveyed by residents. This criterion is met by this project.

c. *Service and Laundry Areas* – Service and laundry areas are located within the units. The community mail box kiosk is located in a visible and defensible spot along the main driveway.

d. *Hardware* – The site is designed with the principals of defensible space, so no additional security hardware is proposed. This criterion is met by this project.

e. *Lighting* – Site security lighting is to be provided in two ways: each front porch and rear entry will have a security light. There will also be three low scale residential street lights proposed for the main drive. These lights will be designed in such a way as to not shine above the horizontal plane to minimize lighting adjacent properties. This criterion is met by this project.

f. *Landscaping* – Landscaping in a for-sale development ultimately will be under the purview of future owners, but the initial landscape plan will not include tall shrubs or other features that will discourage informal surveillance of the site by residents. This criterion is met by this project.

B. *Applicability.* The above Section applies to this project, except as modified by Section 18.5.2.020.

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*C. Building Orientation.*

1. *Building Orientation to Street* – Residential Buildings located within 20 feet of a street shall have the primary entrance facing the street. In this section of the entire Siskiyou Bellview development the building orientation of the two units fronting Siskiyou Boulevard have entrances facing the public street. All other units face the internal drive.

2. *Limitation on Parking Between Primary Entrance and Street* – Automobile circulation or off-street parking is not allowed between the building and the street. Automobile parking and vehicular site circulation do not occur between the buildings and the public right-of-way. This criterion is met by this project.

3. *Build-to Line* – No such limitation exists on this parcel. This Section is not applicable.

*D. Garages.*

1. *Alleys and Shared Driveways* – This site does not abut an alley, but it does have a shared driveway. All the garages face the shared driveway. This criterion is met by this project.

2. *Setback for Garage Opening Facing Street* – Garages are setback farther than 20 feet from the street, so the project complies with this Section of the Code. This criterion is met by this project.

*E. Building Materials.* Buildings are designed to have muted earth tone colors on the exteriors. This criterion is met by this project.

*F. Streetscape.* The site has 102 feet of frontage along Siskiyou Boulevard. Consequently, three street trees from the approved list will be provided along the right-of-way frontage. This criterion is met by this project.

*G. Landscaping and Recycle/Refuse Disposal Areas.* Garbage cans and recycling containers will be stored in unit garages. This criterion is met by this project.

*H. Open Space.* Each unit is to be provided with recreation and open space, and an outdoor patio space has been designated on the site plan. In addition, there is an existing recreation easement and access to the creek that will be maintained. The intention, once upon a time, was for someone to construct a bridge across the creek. That can still be accomplished if abutting property owners agree to do it. This criterion is met by this project.

**Section 18.4.3.040** – *Parking ratios.* Two spaces per unit are required. Two spaces on site are provided for the project. There is no on-street parking at this location, so no credits are requested for on-street parking. This criterion is met by this project.

**Section 18.4.3.050** – *Accessible Parking.* This is a single family development, so this Section does not apply to this project.

**Section 18.4.3.060** – *Parking Management Strategies.* This Section does not apply to this project.

**Section 18.4.3.070** – *Bicycle Parking for Residential Units.* Bicycle parking will be provided in the garage of each unit. This criterion is met by this project.

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**Section 18.4.3.080 – Vehicle Area Design.**

A. *Parking Location.* This Section does not apply to this project.

B. *Parking Area Design.* Spaces for vehicles, including garages shall be a minimum of 9 feet by 18 feet. No compact spaces are proposed. Exterior spaces are 9 x18 feet. Garages are 11 x 20 feet. This criterion is met by this project.

C. *Vehicular Access and Circulation.* This Section does not apply to this project.

D. *Driveways and Turn-Around Design.* Driveways serving seven or less and less than 150 feet in length shall have a driving surface of at least 12 feet. The driveway for this project is proposed to be 15 feet in width and less than 150 feet. This criterion is met by this project.

E. *Parking and Access Construction.* All driveways and parking areas shall be paved, with storm water drainage designed to not produce runoff onto the public right-of-way. Parking areas with more than one space will be striped and will have either curb stops or a curb to prevent vehicles from driving onto landscaped areas. This criterion is met by this project.

**Section 18.4.3.090 – Pedestrian Access and Circulation.** This Section does not apply to this project.

**Section 18.4.4.030 – Landscaping and Screening.** Landscaping plants will be installed to provide 90% coverage within 5 years of the date of installation. Native plants and water conserving plants will be provided. Street trees will have a minimum 2" caliper at the time of installation. Other trees to be provided will have a minimum 1.5" caliper at the time of installation. The site plantings will be on a fully automated irrigation system meeting the requirements of the Land Development Code. See the landscape planting plan for additional details. This criterion is met by this project.

**Section 18.4.4.040 – Recycling and Refuse Disposal Areas.** All units will be issued refuse and recycling containers, which will be stored in individual unit garages. This criterion is met by this project.

**Section 18.4.4.050 – Outdoor Lighting.** Minimal outdoor lighting is proposed for the development. There will be three residential style street lights along the driveway, and exterior lights at the building entrances. No general site lighting is proposed. This criterion is met by this project.

**Section 18.4.4.060 – Fences and Walls.** There are some existing fences and low retaining walls at the site perimeter where this site intersects with adjacent parcels. Those are expected to remain. No interior fences or landscaping walls are currently proposed. This criterion is met by this project.

**Section 18.4.5 – Tree Preservation.** An existing tree inventory/ survey has been completed. The proposed plan has been superimposed over the existing tree inventory and trees that are to remain or be demolished have been identified. For those trees to remain, tree protections are indicated on the plan and protection details have been provided. There are no heritage trees on this site. This criterion is met by this project.

**Section 18.4.6.020 – Exceptions and Variances**

1. *Exceptions to Street Standards.* This section is not applicable as no public streets are proposed. The development will only have a privately owned and maintained driveway.

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**Section 18.4.6.030 – Public Facilities.** The existing sidewalk along Siskiyou is currently an asphalt paved multi-use path. The path is separated from the motoring surface by a roadside storm swale. That path was constructed in the recent past and is heavily used by both pedestrians and bicyclists. Recently the City has changed its standards and wants to alter the public right-of-way improvements to a standard sidewalk, park strip curb, gutter and bike lane. The adjacent parcel (Phase 1 of this project) was allowed to defer these changes until such time as a preponderance of property owners along this stretch of Siskiyou Boulevard, or the City, were in a position to make these improvements over a significant stretch of the right-of-way.

To change a mere 100 feet of frontage will cause chaos and could lead to unsafe conditions, especially during inclement weather. The Developer wishes to defer these changes until such time as more parties sign on to a project and is agreeable to that stipulation.

**Section 18.4.6.040 – Street Design Standards.** No new public streets are proposed as part of this development. This Section does not apply to this project.

**Section 18.4.6.070 – Sanitary Sewer and Water Service Improvements.** This project will be connected to City water and sewer mains that are found in Siskiyou Boulevard. There is adequate capacity in the lines to serve the additional units. This criterion is met by this project.

**Section 18.4.6.080 – Storm Drainage and Surface Water Management Facilities.** The site will have storm water management design that meets current State of Oregon and City of Ashland requirements. This criterion is met by this project.

**Section 18.4.6.090 – Utilities.** Franchise utilities will be provided to each unit in the proposed development. All franchise utilities will be placed under ground. They will include electricity, gas, cable, internet and telephone. This criterion is met by this project.

**Section 18.4.7 – Signs.** This Section is not applicable to this project.

**Section 18.4.8.030 – Solar Setback.** We have provided a solar shading analysis sheet with the application packet. This analysis shows the requirements of this section have been met.

**Section 18.4.9 – Disc Antennas.** This Section is not applicable to this project.

**Section 18.4.10 – Wireless Communication Facilities.** This Section is not applicable to this project.

**Section 18.5.2.020.B – Residential Design Review.** This project is subject to design review. The review shall be a Type I review. 18.5.2.050 Approval Criteria An application for Site Design Review shall be approved if the proposal meets the criteria in subsections A, B, C, and D below. The approval authority may, in approving the application, impose conditions of approval, consistent with the applicable criteria.

*A. Underlying Zone.* The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

*B. Overlay Zones.* The proposal complies with applicable overlay zone requirements (part 18.3).

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*C. Site Development and Design Standards.* The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

*D. City Facilities.* The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

*E. Exception to the Site Development and Design Standards.* The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

- 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*
- 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Land Use Ordinance Development and Design Standards.*

Lot 7800 is a rather large lot with a relatively small frontage along Siskiyou Boulevard. The lot is zoned R-2. We believe strict adherence to the provisions of the subdivision sections of AMC 18.5.3 would lead to an inferior development, while meeting the letter or the ordinance would not meet the overall goals of the City of Ashland for housing. By utilizing performance standards and the Site Plan Review standards we believe this development will better address those goals, promote more responsible development and protect available site open space for all residents within the three phases of the Siskiyou Bellview Planned Unit Development.

**Section 5.7.040 – Tree Removal Permits.**

*2. Tree That is Not a Hazard.* A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

*a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.*

There are twelve trees listed on the tree survey, which was performed in January of 2016. The three listed as dead and the one listed as failing (trees 1-4) have been removed as hazards. In addition, tree #9 was uprooted in a storm during 2016 and has also been removed. Trees 5-7 and 10-11 will need to be removed for the development. We think that with careful pruning, trees 8 and 12 can be retained. The developer is proposing that four feet of the 20-foot driveway be pervious pavement which will increase the ability of groundwater to percolate to the root system of Tree 12.

*b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.* The removal of so indicated trees will not have a negative impact on erosion, soil stability, flow of surface waters, adjacent trees or existing windbreaks.

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*c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered 18.5.7 – Tree Removal Permits City of Ashland 5-60 Land Use Ordinance and no reasonable alternative exists to allow the property to be used as permitted in the zone.*

The removal of these trees will no impact on the tree canopy or species diversity within a 200-foot radius of the property. There are several diverse specimen trees with Phase I of the project that were able to be retained during the construction and are actually thriving in their new environment. For other reasons the specimen trees on the site (primarily the giant sequoias) have already failed pre-development. The remaining trees are not particularly desirable trees. We can, however, work around trees 8 and 12.

*d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*

The site is very tight. The requirement to avoid development within the City designated flood plain does not allow for the alternate placement of units.

*e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

The owner agrees to the replacement of trees in the manner specified by the City.

## PROJECT TEAM

**Project Owner:**  
Raven Woodworks

Ashland, OR 97520

**Architect:** -Oregon Architecture Inc.  
Mark McKechnie, A.I.A.  
mark@oregonarchitecture.biz  
132 West Main Street Suite 101  
Medford, Oregon 97501  
Telephone: 541.772.4372

**Landscape Architect:** -Madara Design Inc.  
Thomas Madara  
tom.madara@madaradesign@yahoo.com  
2994 Wells Fargo Rd  
Central Point, Or 97502  
541-664-7055

# RESIDENTIAL DEVELOPMENT SISKIYOU-BELLVIEW PHASE II

**2300 SISKIYOU BOULEVARD. • ASHLAND, OREGON  
MAP & TAX LOT NUMBER: 39 1E 14CA, TL 7800**

## DRAWING INDEX

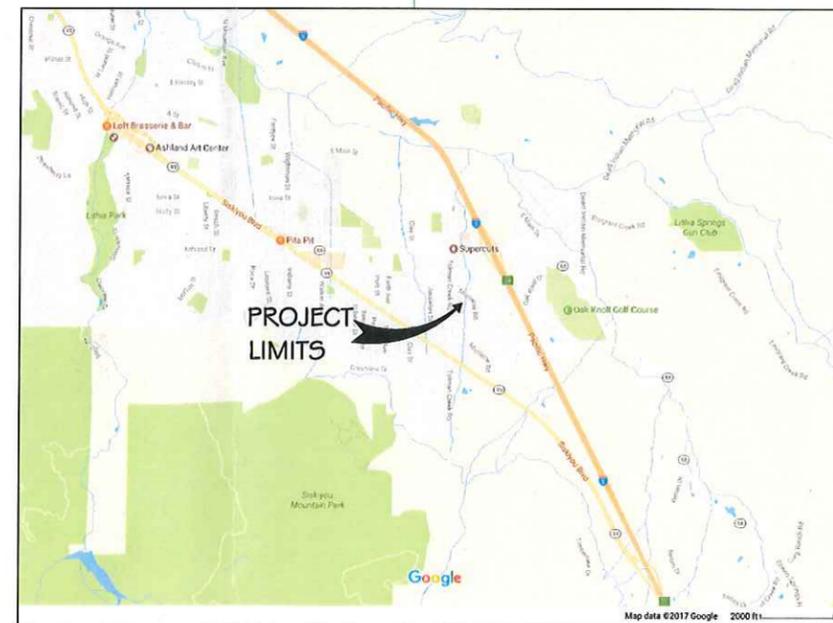
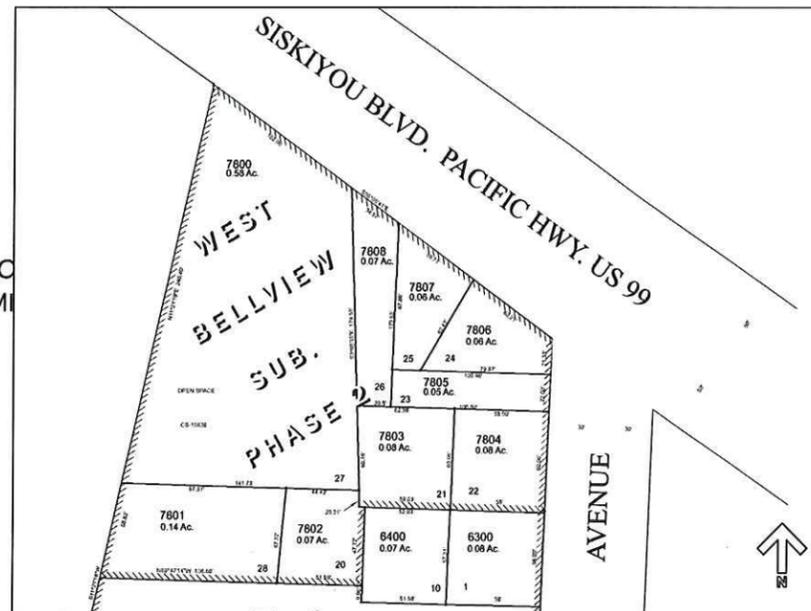
**GENERAL**  
GO.00 COVER SHEET, PROJECT DATA,

**ARCHITECTURAL**  
AO.01 OVERALL DEVELOPMENT PLAN  
AO.02 PHASE II PLAN  
AO.03 SITE GRADING AND DRAINAGE  
AO.04 SITE UTILITY PLAN  
AO.05 TREE PROTECTION PLAN  
AO.06 FLOOD PLAIN MAP

A2.01 UNITS 1 AND 2 ELEVATIONS  
A2.02 UNITS 3 AND 4 ELEVATIONS  
A2.03 UNITS 5 AND 6 ELEVATIONS

LI TREE PROTECTION PLAN

## VICINITY MAP/ PROJECT LOCATION



## BUILDING SUMMARY

**PROJECT SCOPE:**  
6 RESIDENTIAL TOWNHOUSES

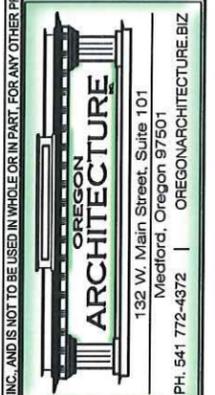
**SITE ADDRESS:** Siskiyou Bellview Phase II  
2300 Siskyou Blvd.  
Ashland Or 97520

**PARCEL NO.:** 39 1E 14CA, TL 7800  
**ZONING DISTRICT:** R-2

**CONSTRUCTION TYPE:** V-B  
**SPRINKLERS:** n/a

**APPLICABLE CODES:**  
CITY OF ASHLAND, OREGON  
2014 Oregon Residential Specialty Code (ORSC)  
2014 Oregon Electrical Specialty Code (OESC)  
2014 Oregon Plumbing Specialty Code (OPSC)  
2014 Oregon Mechanical Specialty Code (OMSC)

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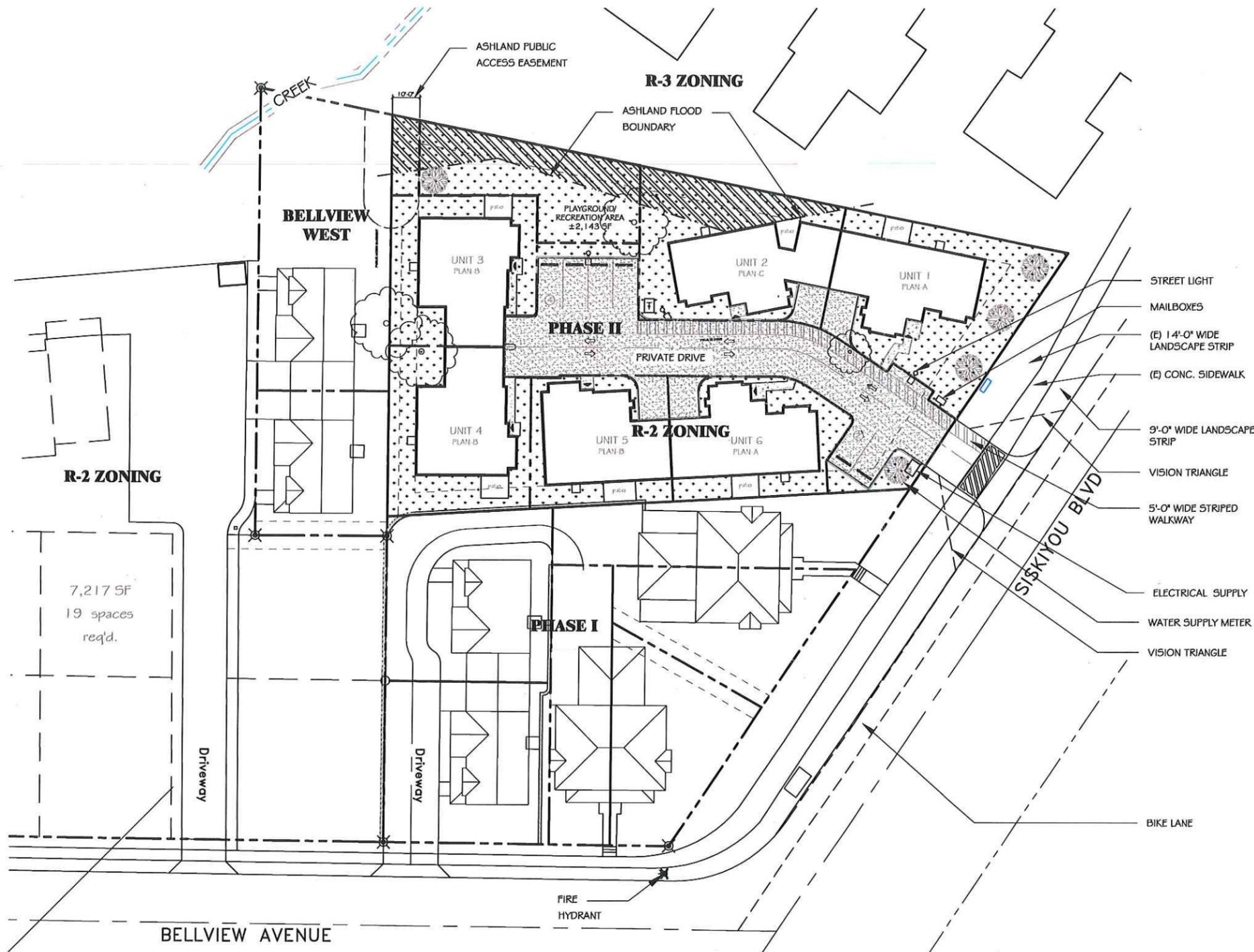
**SISKIYOU BELLVIEW  
PHASE II**  
Project Location:  
Raven Woodworks  
2300 Siskiyou Boulevard  
ASHLAND, OREGON 97520  
MAP: 39 1E 14CA, TAX LOT: 7800

Approval Signature	Date
REVISIONS	BY
DEVELOPMENT APP	CLL
08-03-17	
PLOT DATE:	8/2/17
PHASE:	
DRAWN BY:	P.M.
SHEET:	

**GO.00**  
COVER SHEET

**SITE LEGEND**

-  LANDSCAPE AREA
-  FLOOD PLAIN AREA
-  PROPOSED LIGHT POLE
-  EXISTING FIRE HYDRANT
-  EDGE OF FLOOD PLAIN
-  PROPERTY LINE
-  SETBACK LINES
-  TOPO 10' CONTOUR
-  CENTER LINE OF ROADWAY



**PHASE II DEVELOPMENT SUMMARY**

**PROJECT SCOPE:**  
Six, two story residential townhouses with three bedrooms, three baths and single car garages

**ADDRESS:** 2300 SISKIYOU BOULEVARD  
ASHLAND, OREGON  
39 1E 14CA, TL 7800

**MAP AND TAX LOT:** 39 1E 14CA, TL 7800

**ZONING DISTRICT:** Residential (R-2)

**BUILDING CODES:**  
2014 Oregon Residential Specialty Code (ORSC)  
2014 Oregon Electrical Specialty Code (OESC)  
2014 Oregon Plumbing Specialty Code (OPSC)  
2014 Oregon Mechanical Specialty Code (OMSC)

**SITE AREA:** 25,461 SF (0.58 AC)

**BUILDING SHELL:** 6 -Two Story Townhouses (footprint)

TYPE A AREA 1,420 SF X 2 = 2,840 SF  
TYPE B AREA 1,447 SF X 3 = 4,341 SF  
TYPE C AREA 1,450 SF X 1 = 1,450 SF  
**TOTAL AREA 8,631 SF**

**LOT COVERAGES:**

BUILDINGS 8,631 SF-34%  
IMPERMEABLE AREA 6,282 SF-25%  
LANDSCAPED AREA 10,548 SF-41% (35% min.)  
**25,461 SF-100%**

**RECREATION SPACE** 2,143 SF (8% actual/min.)

**CONSTRUCTION TYPE:** V-B  
**OCCUPANCY GROUP:** R2  
**FUNCTION OF SPACE:** Residential  
**SPRINKLERS:** No

**FRONTAGE LANDSCAPING:**  
1 TREE AT 30 FT O.C. 4 REQUIRED / 4 PROVIDED

**OVERALL DEVELOPMENT PLAN**  
SCALE: 1" = 20'-0"



TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

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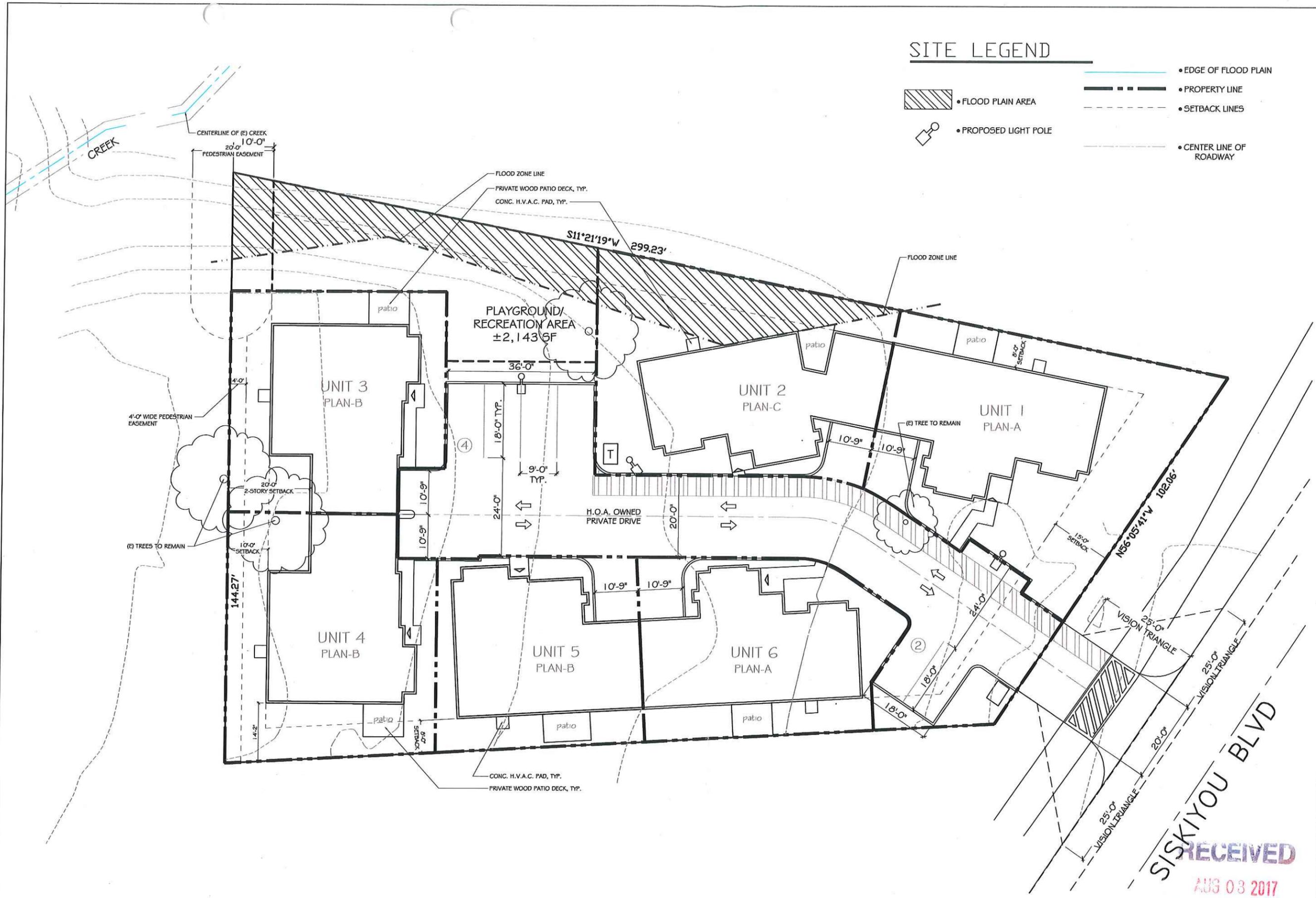
**OREGON ARCHITECTURE**  
132 W. Main Street, Suite 101  
Medford, Oregon 97501  
PH. 541 772-4372 | OREGONARCHITECTURE.BIZ

**SISKIYOU BELLVIEW PHASE II**  
Project Location:  
Raven Woodworks  
2300 Siskiyou Boulevard  
ASHLAND, OREGON 97520  
MAP: 39 1E 14CA TAX LOT: 7800

Approval Signature	Date
REVISIONS	BY
DEVELOPMENT APP 08-03-17	CLL
PLOT DATE:	8/2/17
PHASE:	
DRAWN BY:	P.M.
SHEET:	

**A0.01**  
SITE PLAN

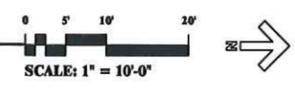
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**SITE LEGEND**

- FLOOD PLAIN AREA
- PROPERTY LINE
- SETBACK LINES
- PROPOSED LIGHT POLE
- EDGE OF FLOOD PLAIN
- CENTER LINE OF ROADWAY

**PHASE II SITE PLAN**



TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

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**SISKIYOU BELLVIEW PHASE II**  
 Project Location:  
 Raven Woodworks  
 2300 Siskiyou Boulevard  
 ASHLAND, OREGON 97520  
 MAP: 39 1E 14CA TAX LOT: 7800

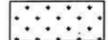
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REVISIONS	BY
DEVELOPMENT APP 08-03-17	CLL
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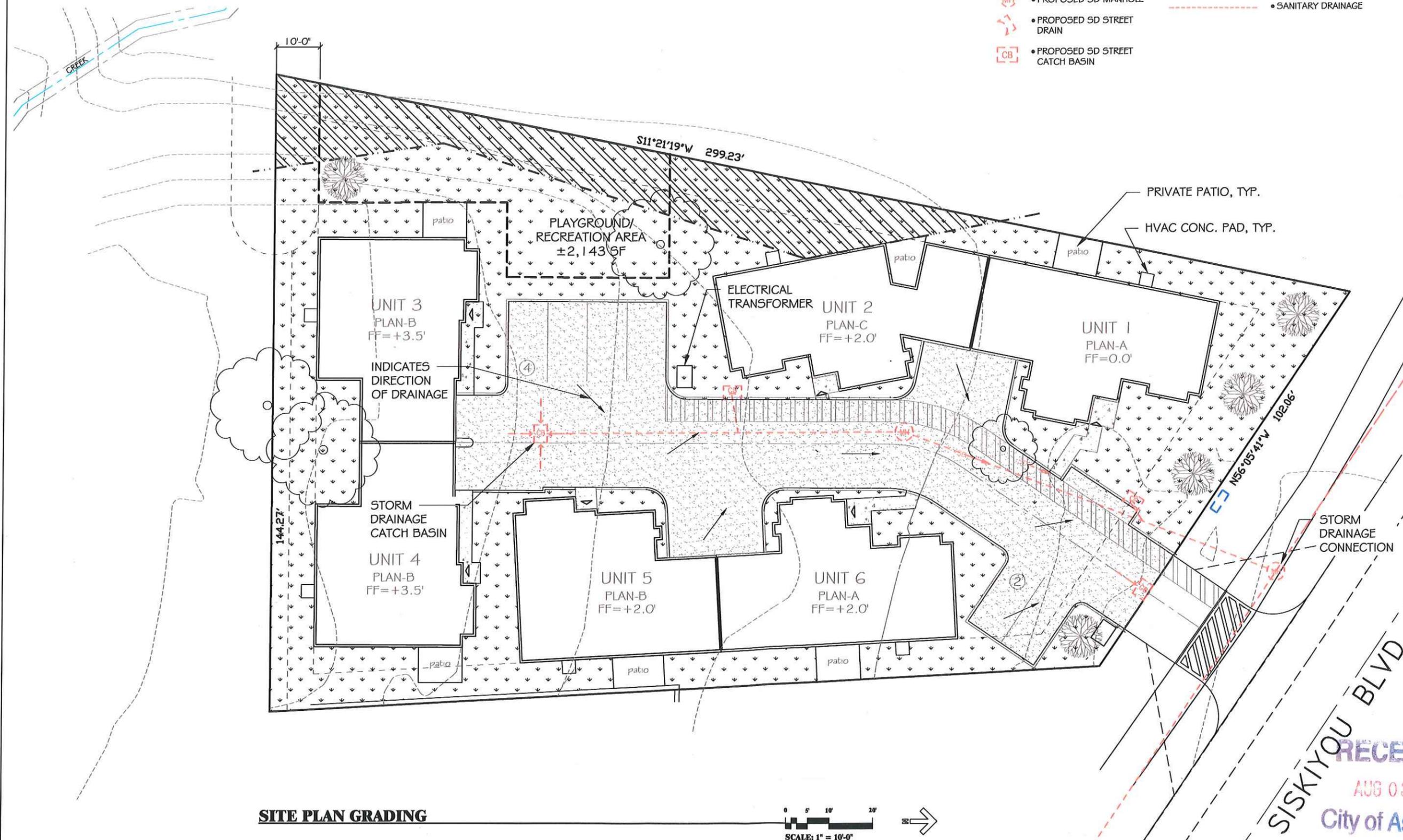
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**A0.02**  
 SITE PLAN

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# SITE LEGEND

-  LANDSCAPE AREA
-  FLOOD PLAIN AREA
-  PROPOSED LIGHT POLE
-  PROPOSED SD MANHOLE
-  PROPOSED SD STREET DRAIN
-  PROPOSED SD STREET CATCH BASIN
-  EDGE OF FLOOD PLAIN
-  PROPERTY LINE
-  SETBACK LINES
-  TOPO 10' CONTOUR
-  CENTER LINE OF ROADWAY
-  SANITARY DRAINAGE



## SITE PLAN GRADING



TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE SIXTEENTH INCH.

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**SISKIYOU BELLVIEW PHASE II**  
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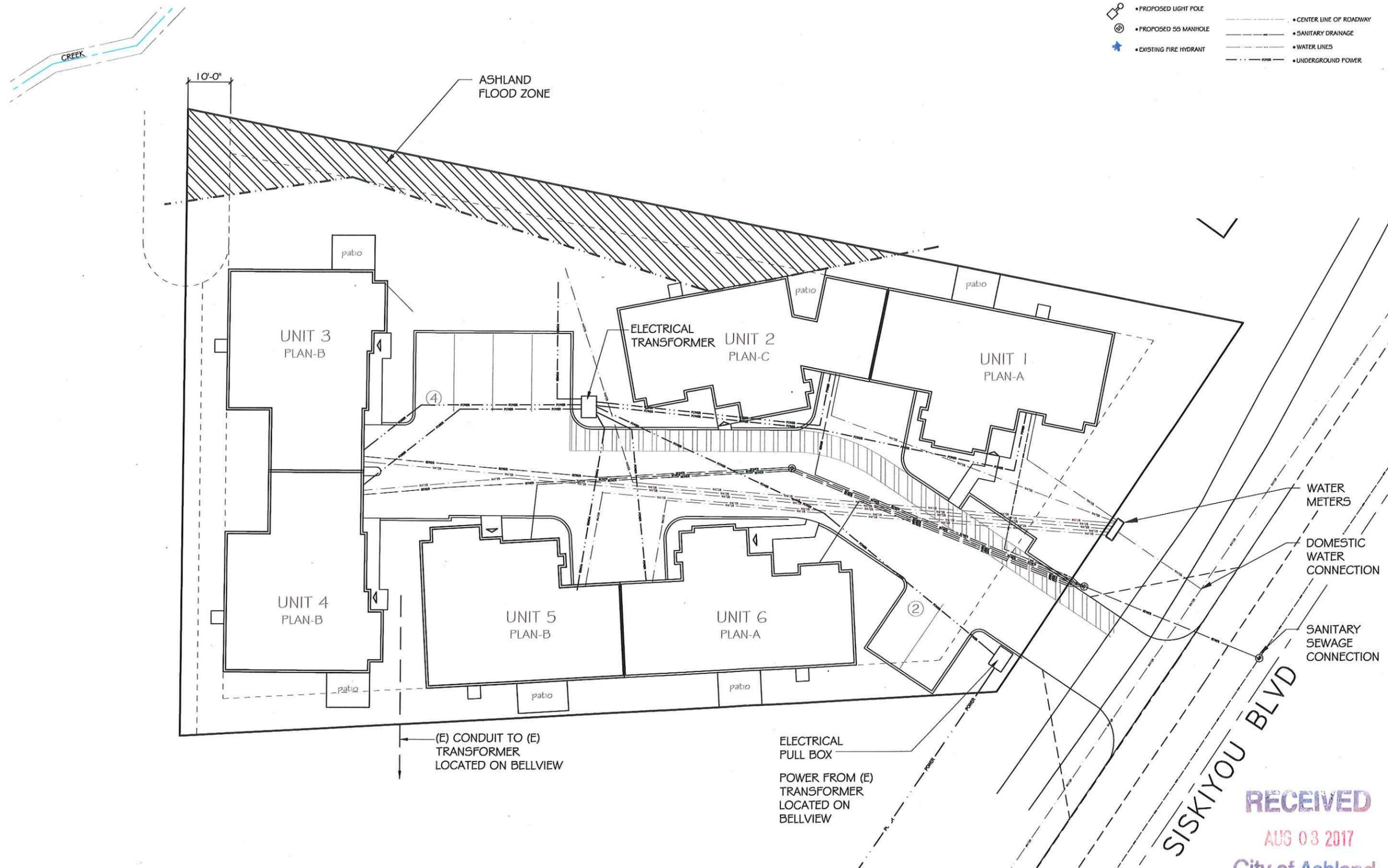
Approval Signature	Date
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**SITE LEGEND**

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-  PROPOSED LIGHT POLE
-  PROPOSED SS MANHOLE
-  EXISTING FIRE HYDRANT
-  EDGE OF FLOOD PLAIN
-  PROPERTY LINE
-  SETBACK LINES
-  CENTER LINE OF ROADWAY
-  SANITARY DRAINAGE
-  WATER LINES
-  UNDERGROUND POWER



**SITE UTILITY PLAN**



WATER METERS  
 DOMESTIC WATER CONNECTION  
 SANITARY SEWAGE CONNECTION  
 SISKIYOU BLVD

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REVISIONS	BY
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Report Prepared by  
 Thomas Madara  
 International Society of Arboriculture  
 Certified Arborist License Number PN-6204-A

**Madara Design Inc**  
 Landscape Architecture, Design & Consultation  
 2994 Wells Fargo Rd  
 Central Point, Or 97502  
 541-664-7055

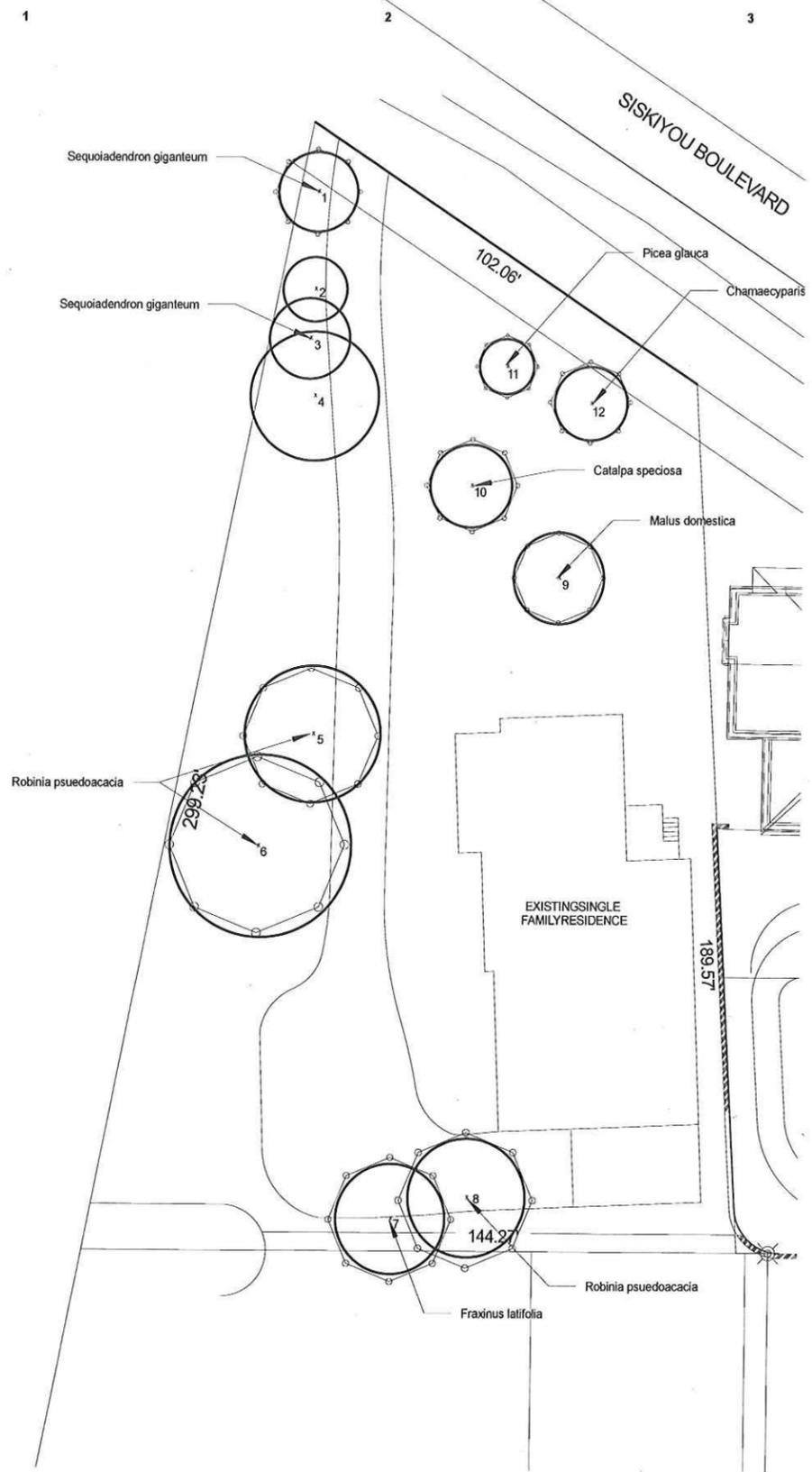
**RAVEN WOODWORKS INC.**  
 2300 SISKIYOU BLVD.  
 Ashland, Oregon

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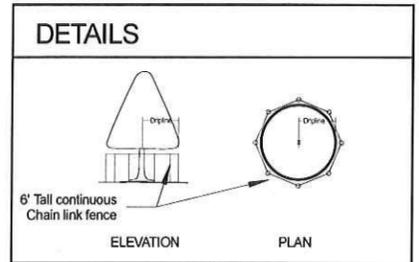
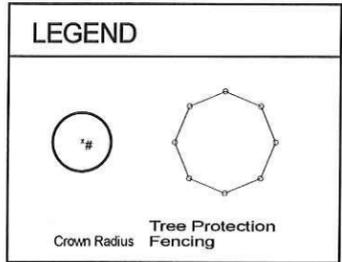
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 CHECKED BY: TM

01.08.16  
 SCALE: 1/16" = 1'

**L1**



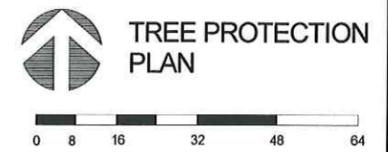
TREE INVENTORY							
Tree Number	Species	DBH	Height	Crown Radius	Tree Protection Zone Radius	Condition	Notes
1	Sequoiadendron giganteum	21	40	9	9	Failing	May need to be removed if health continues to decline
2	Sequoiadendron giganteum	25	40	7	0	Dead	Shall be removed. HAZARD TREE
3	Sequoiadendron giganteum	14	35	6	0	Dead	Shall be removed. HAZARD TREE
4	Sequoiadendron giganteum	36	45	10	0	Dead	Shall be removed. HAZARD TREE
5	Robinia pseudoacacia	21	33	15	15	moderate	
6	Robinia pseudoacacia	10	38	20	20	moderate	
7	Fraxinus latifolia	11	45	11	11	moderate	
8	Robinia pseudoacacia	10	30	8	8	moderate	
9	Malus domestica	7	20	10	10	moderate	
10	Catalpa speciosa	13	30	9	9	moderate	
11	Picea glauca	8	20	6	6	good	
12	Chamaecyparis lawsoniana	10	20	8	8	good	



**TREE PROTECTION NOTES**

1. Tree protection to be in place before any construction to commence and is under the direct supervision of the Staff Arborist.
2. Tree protection to be chain link fencing, a minimum of six feet tall with steel posts placed no farther than ten feet apart, shall be installed at the edge of the tree protection zone or dripline, whichever is greater, and at the boundary of any open space tracts, riparian areas, or conservation easements that abut the parcel being developed.
3. Approved signs shall be attached to the chain link fencing stating that inside the fencing is a tree protection zone, not to be disturbed unless prior approval has been obtained from the Staff Arborist for the project.
4. The actual location or tree protection for this project is as noted on these plans.
5. The fencing shall be flush with the initial undisturbed grade.
6. Fencing shall be enclosed to prevent any unauthorized access for the full duration of construction.
7. No construction activity shall occur within the tree protection zone, including, but not limited to dumping or storage of materials such as building supplies, soil, waste items, equipment, or parked vehicles.
8. The tree protection zone shall remain free of chemically injurious materials and liquids such as paints, thinners, cleaning solutions, petroleum products, concrete or dry wall excess, and construction debris or run-off.
9. No excavation, trenching, grading, root pruning, or other activity shall occur within the tree protection zone unless approved by the Staff Arborist.
10. Any work necessary within the dripline is subject to prior approval and direction of the Staff Arborist.
11. Trees being protected will be watered regularly via a temporary watering system until surrounding landscape and irrigation is complete.
12. Tree(s) to be removed that are within the dripline of any trees to remain shall be removed only by a certified arborist.
13. Any damage to protected trees shall be reported to the Staff Arborist within 24 hours of observation.
14. Except as otherwise determined by the Staff Arborist, all required tree protection measures set forth in this section shall be instituted prior to any development activities, including, but not limited to clearing, grading, excavation, or demolition work, and shall be removed only after completion of all construction activity, including landscaping and irrigation installation.

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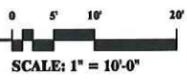
NOTE: IF THIS SHEET IS LESS THAN 24" x 36" IT HAS BEEN REDUCED AND IS NOT TO SCALE.

**SITE LEGEND**

- LANDSCAPE AREA
- FLOOD PLAIN AREA
- PROPOSED LIGHT POLE
- PROPOSED TREE TO BE REMOVED
- PROPOSED NEW TREE
- EXISTING TREE TO REMAIN
- EDGE OF FLOOD PLAIN
- PROPERTY LINE
- SETBACK LINES
- TOPO 1'0" CONTOUR
- CENTER LINE OF ROADWAY
- SANITARY DRAINAGE
- WATER LINES
- NEW UNDERGROUND POWER



**Tree Protection Plan**



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City of Ashland

**OREGON ARCHITECTURE**  
132 W. Main Street, Suite 101  
Medford, Oregon 97501  
PH. 541.772-4372 | OREGONARCHITECTURE.BIZ

**SISKIYOU BELLVIEW  
PHASE II**

Project Location:  
Raven Woodworks  
2300 Siskiyou Boulevard  
ASHLAND, OREGON 97520  
MAP: 39 1E 14CA TAX LOT: 7800

Approval Signature	Date
REVISIONS	BY
PLOT DATE:	8/2/17
PHASE:	
DRAWN BY:	P.M.
SHEET:	

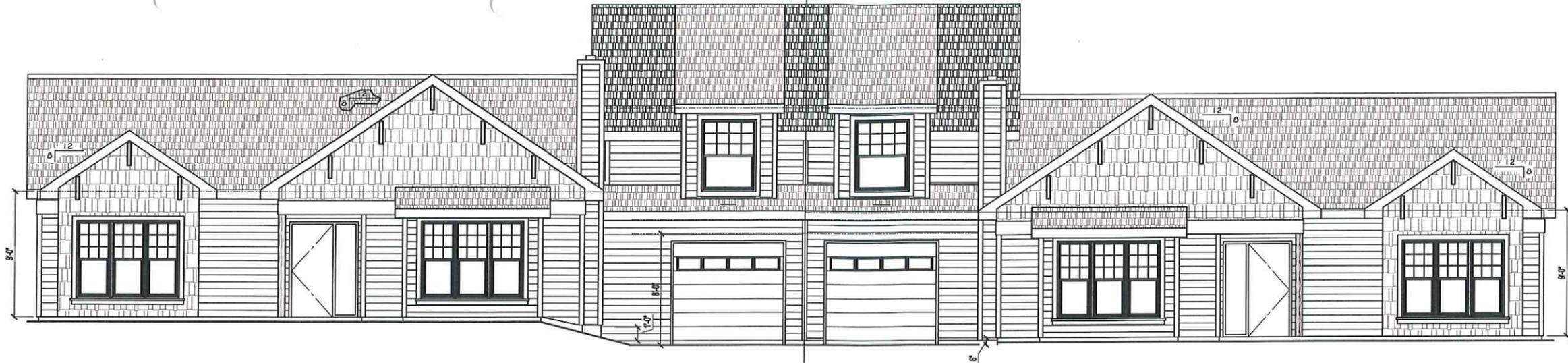
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TREE PLAN

TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

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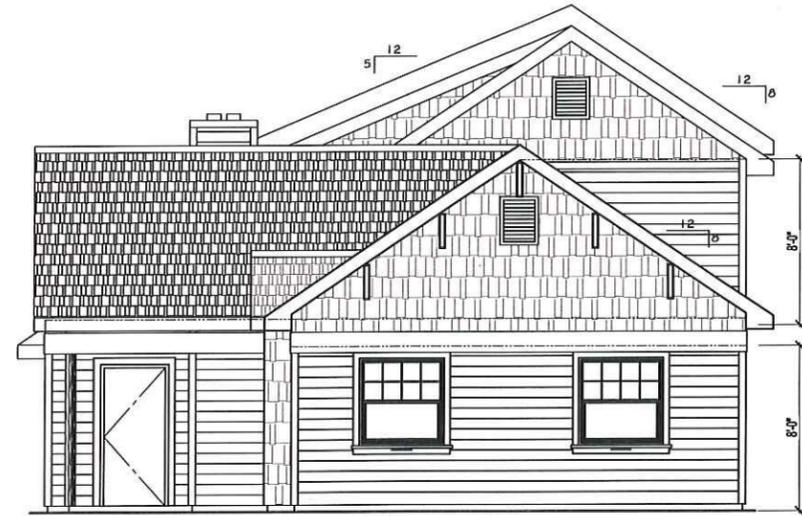




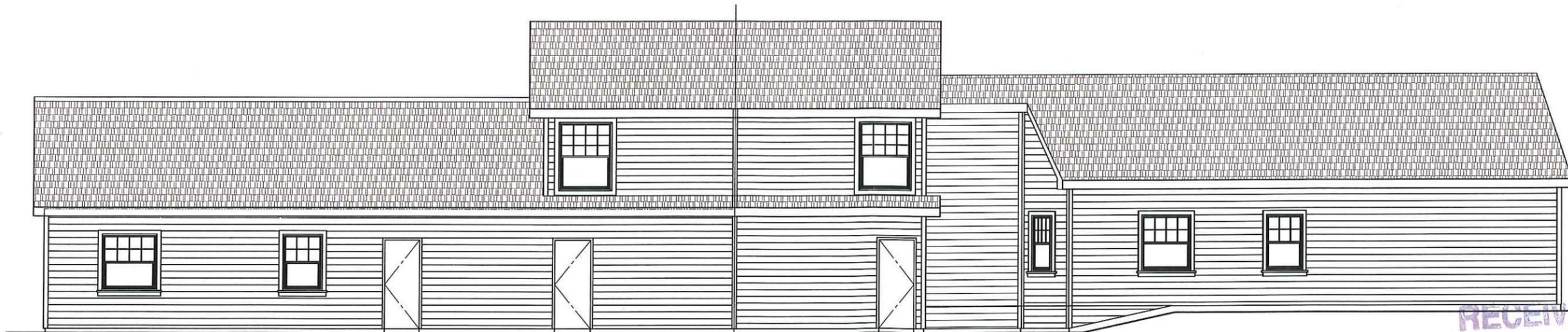
**UNITS 1 AND 2-EAST ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNIT 2-SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNIT 1-NORTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNITS 1 AND 2-EAST ELEVATION**  
SCALE: 1/4" = 1'-0"

TO VERIFY FULL SCALE ——— BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

**OREGON ARCHITECTURE**  
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**SISKIYOU BELLVIEW PHASE II**  
Project Location:  
Raven Woodworks  
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MAP: 39 1E 14CA TAX LOT: 7800

REVISIONS	BY	DATE

PLOT DATE: 8/2/17  
PHASE:  
DRAWN BY: P.M.  
SHEET:

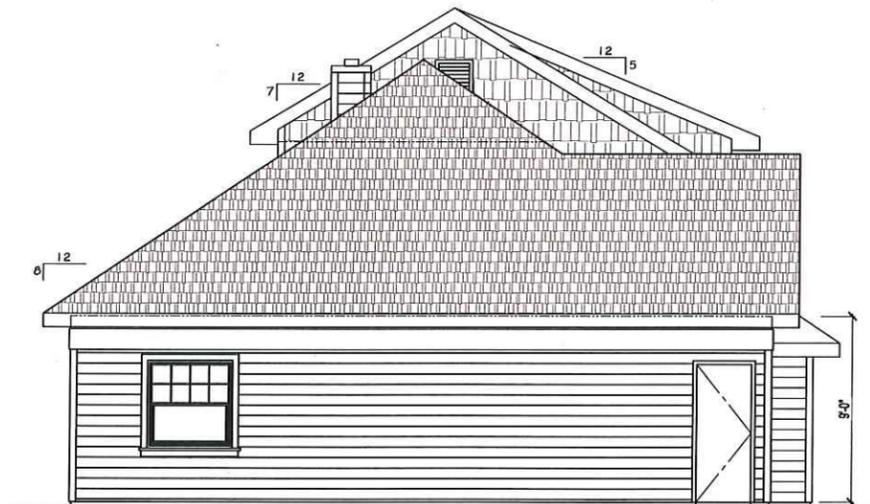
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City of Ashland

**A2.01**  
EXTERIOR ELEVATIONS  
UNITS 1 AND 2

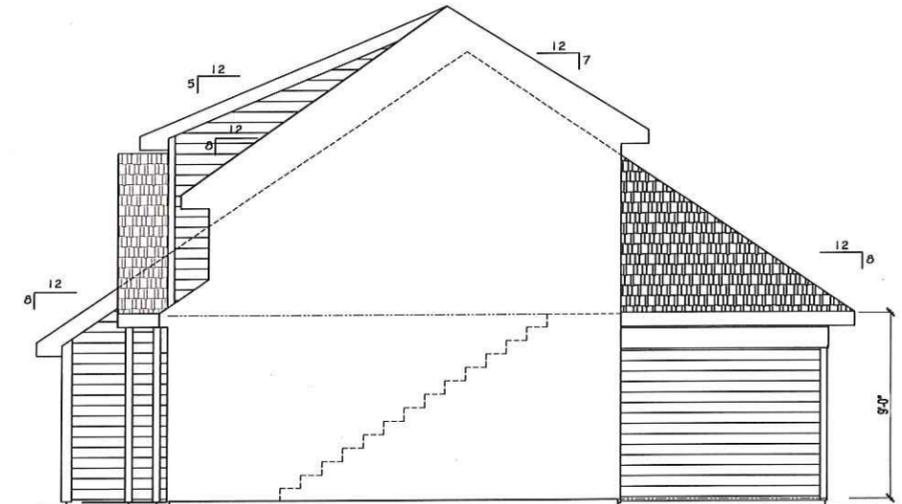
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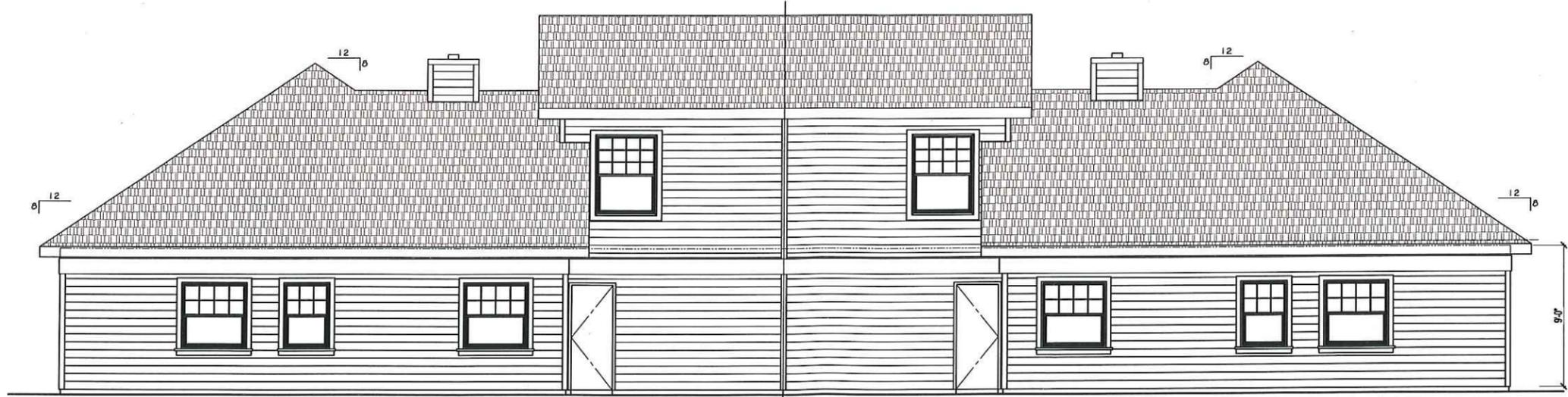
**UNITS 3 AND 4-NORTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNIT 3-EAST ELEVATION (UNIT 4-WEST ELEVATION)**  
SCALE: 1/4" = 1'-0"



**UNITS 3 AND 4-WEST ELEVATION BETWEEN UNITS**  
SCALE: 1/4" = 1'-0"



**UNITS 3 AND 4-SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"

TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

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**SISKIYOU BELLVIEW PHASE II**  
Project Location:  
Raven Woodworks  
2300 Siskiyou Boulevard  
ASHLAND, OREGON 97520  
MAP: 39 1E 14CA TAX LOT: 7800

REVISIONS	BY	DATE

Approval Signature: \_\_\_\_\_ Date: \_\_\_\_\_

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**A2.02**  
EXTERIOR ELEVATION  
UNITS 3 AND 4

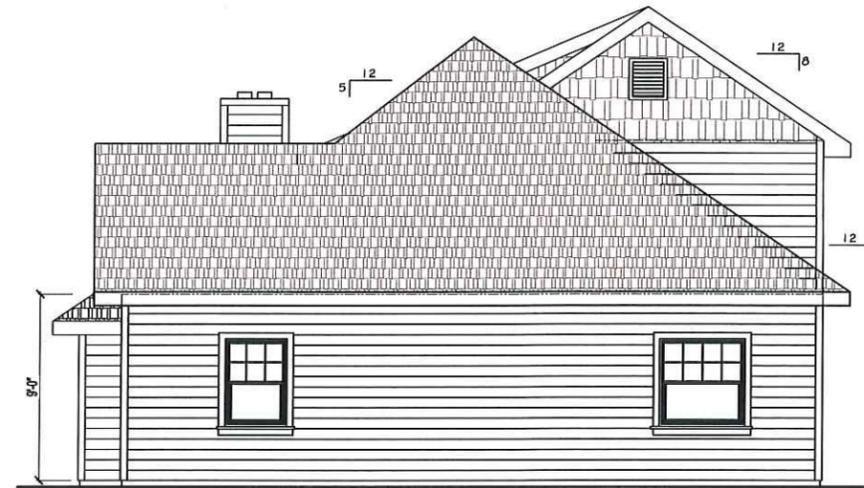
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**UNITS 5 AND 6-WEST ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNIT 6-NORTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNIT 5-SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**UNITS 5 AND 6-EAST ELEVATION**  
SCALE: 1/4" = 1'-0"

TO VERIFY FULL SCALE BAR SHOULD MEASURE ONE INCH BY ONE/SIXTEENTH INCH.

**OREGON ARCHITECTURE**  
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**SISKIYOU BELLVIEW PHASE II**  
Project Location:  
Raven Woodworks  
2300 Siskiyou Boulevard  
ASHLAND, OREGON 97520  
MAP: 39 1E 14CA TAX LOT: 7800

REVISIONS	BY	DATE

APPROVAL SIGNATURE \_\_\_\_\_ DATE \_\_\_\_\_

PLOT DATE: 8/2/17  
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**A2.03**  
EXTERIOR ELEVATIONS  
UNITS 5 AND 6

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**PLANNING ACTION:** PA-2017-01507

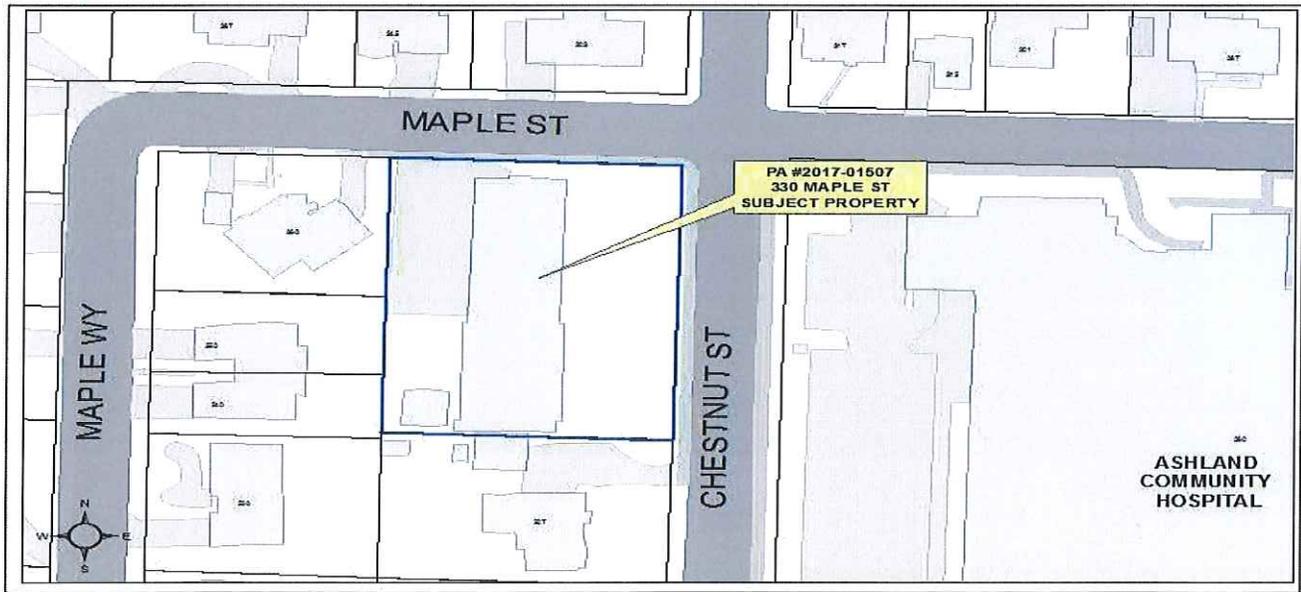
**SUBJECT PROPERTY:** 330 Maple Street

**OWNER/APPLICANT:** Maple LLC/Reitinger & Associates, Inc

**DESCRIPTION:** A request for Site Design Review approval to construct a new 29,400 square foot mixed-use building consisting of basement parking, medical suites on the first floor and two residential units on the second floor for the property located at 330 Maple Street. The application includes: a request for Exception to the Site Development and Design Standards to allow automobile circulation between the building and the street to allow a patient drop-off area from Maple Street; a request for an Exception to Street Standards to allow a smaller than typically-required separation between driveways; and a Tree Removal Permit to remove two trees six-inches in diameter at breast height (d.b.h.) or greater. **COMPREHENSIVE PLAN DESIGNATION:** Health Care; **ZONING:** HC; **ASSESSOR'S MAP:** 39 1E 05DB; **TAX LOT #:** 2000

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **Thursday, September 7, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** *Tuesday, September 12, 2017 at 7:00 PM, Ashland Civic Center, 1175 East Main Street*



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### **18.5.2.050**

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

## **EXCEPTION TO STREET STANDARDS**

### **18.4.6.020.B.1**

Exception to the Street Design Standards. The approval authority may approve exceptions to the standards section in 18.4.6.040 Street Design Standards if all of the following circumstances are found to exist.

- a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.
  - i. For transit facilities and related improvements, access, wait time, and ride experience.
  - ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.
  - iii. For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.
- c. The exception is the minimum necessary to alleviate the difficulty.
- d. The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.

## **TREE REMOVAL PERMIT**

### **18.5.7.040.B**

1. Hazard Tree. A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. Tree That is Not a Hazard. A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
  - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

**ASHLAND PLANNING DEPARTMENT  
STAFF REPORT**

September 12, 2017

**PLANNING ACTION #:** PA #2017-01507

**OWNER/APPLICANT:** Maple LLC/Reitinger & Associates, Inc.

**LOCATION:** 330 Maple Street

**ZONE DESIGNATION:** HC

**COMP. PLAN DESIGNATION:** Health Care

**ORDINANCE REFERENCES:**

(See also <http://www.codepublishing.com/OR/Ashland/#!/LandUse/index.html#NT>)

18.2.4	General Regulations for Base Zones
18.2.5	Standards for Residential Zones
18.2.6	Standards for Non-Residential Zones
18.4.2	Building Placement, Orientation & Design
18.4.3	Parking, Access and Circulation
18.4.4	Landscaping, Lighting & Screening
18.4.5	Tree Preservation and Protection
18.4.6	Public Facilities
18.4.7	Signs
18.4.8	Solar Access
18.5	Application Review Procedures and Approval Criteria
18.5.2	Site Design Review
18.5.7	Tree Removal
18.6.1	Definitions

**APPLICATION DEEMED COMPLETE ON:** September 2, 2017

**REQUEST:** A request for Site Design Review approval to construct a new 29,400 square foot mixed-use building consisting of basement parking, medical suites on the first floor and two residential units on the second floor for the property located at 330 Maple Street. The application includes: a request for Exception to the Site Development and Design Standards to allow automobile circulation between the building and the street to allow a patient drop-off area from Maple Street; a request for an Exception to Street Standards to allow a smaller than typically-required separation between driveways; and a Tree Removal Permit to remove two trees six-inches in diameter at breast height (d.b.h.) or greater.

**I. Relevant Facts**

**1) Background - History of Application**

There are no planning actions of record for the property.

**2) Detailed Description of the Site and Proposal**

***Site Description***

The subject property is a rectangular lot located at the southwest corner of the intersection of Maple and Chestnut Streets, just uphill and across the street from Ashland Community Hospital. The property has an area of 0.63-acres (27,443 square feet) and is zoned HC, a healthcare services zoning district. The two properties immediately to the south, including the contiguous property to the south which is under the same ownership, and properties to the east on and around the Ashland Community Hospital campus are also zoned HC. The property immediately uphill to the west along Maple Street is zoned R-2 (Low Density Multi-Family Residential) while the properties across Maple Street to the north are zoned R-1-7.5 (Single Family Residential).

The property currently contains a 7,056 square foot, one-story medical building near the center of the site which Jackson County tax records identify as the former Ashland View Manor, a 20-room nursing home. In addition, there is a small, single-story storage structure. The application notes that both structures are in poor condition and proposed to be demolished. Use of the existing buildings was discontinued some time ago, and the main building is now boarded up and parking lot access restricted. The existing medical building is setback approximately 55 feet from Chestnut Street, and the area between the sidewalk and the building has slopes of approximately 15-18 percent.

The application includes a tree survey identifying approximately 14 trees greater than six-inches in diameter at breast height (d.b.h.) on and immediately adjacent to the property, and a landscape plan identifies 12 of these as being retained and incorporated in the landscape plan, with only two trees identified for removal on the landscape plan provided.

The property has approximately 180 linear feet of frontage on Chestnut Street, a Neighborhood Collector Street. Chestnut Street is currently improved with paving, curb, gutter, and an approximately four-foot wide curbside sidewalk in place along the property's full frontage. There are currently no parkrow planting strips along Chestnut Street.

The property has approximately 150 linear feet of frontage on Maple Street, a Neighborhood Street. Maple Street is currently improved with paving, curb, gutter, and an approximately four-foot wide curbside sidewalk in place along the property's full frontage. There are currently no parkrow planting strips along Maple Street.

***Current Proposal***

The current application requests Site Design Review approval to construct a new 29,400 square foot mixed-use building consisting of parking in the 13,500 square

foot basement, medical suites on the 13,500 square foot first floor, and two 1,200 residential penthouse suites on the second floor.

The application materials explain that the medical practice developing the project is The Retina and Vitreous Center of Southern Oregon, and that its physicians deal with the deep interior of the eye and diseases which are often blinding or sight-diminishing. 82 percent of patients are over 65 years of age, and 25 percent of patients are over 85 years old. Most patients are temporarily, if not permanently, at least partially disabled due to vision issues and are unable to drive themselves to appointments and instead rely on a combination of family, friends, taxis, medical transport vans and the bus. Approximately 31 percent of patients are dropped off for their appointments by a driver who does not park on site, and are picked up afterwards. Another two percent take the bus, walk or ride to an appointment. The application further explains that typical new patient appointments last from two to three-and-a-half hours due to the time needed for testing, examinations and treatment, and follow-up appointments can take from one-half to one-and-a-half hours. The application notes that almost all retina surgery is outpatient (same day) surgery and in Ashland, all retina surgeries are done at Asante Ashland Community Hospital (AACH). The application notes that AACH is the only hospital in the valley that does ophthalmologic surgery, and that all other eye surgery is done in outpatient surgery centers.

The application also includes a request for Exception to the Site Development and Design Standards to allow automobile circulation between the building and the street to allow a patient drop-off area from Maple Street; a request for an Exception to Street Standards to allow a smaller than typically-required separation between driveways; and a Tree Removal Permit to remove two trees six-inches in diameter at breast height (d.b.h.) or greater.

## **II. Project Impact**

Within the HC zoning district, Site Review for new buildings or additions greater than 15,000 square feet is subject to a “Type II” application procedure which requires a decision by the Planning Commission through a public hearing. Site Design Review approval is considered in light of the approval criteria in AMC 18.5.2.050 as well as the “Building Placement, Orientation and Design” standards for non-residential development in AMC 18.4.2.040.

### **Site Design Review**

The first criterion for Site Design Review approval is that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”* The application materials provided assert that the proposed development complies with applicable provisions for the underlying zone detailed in AMC 18.2 including setbacks, lot area, dimension, density, floor area, building height, building orientation, architecture, and other applicable standards.

With regard to lot coverage, the application indicates that a Variance is requested to allow a 66 percent lot coverage where the HC zone limits coverage to 65 percent. However, the plans provided identify the project impervious surfaces as being limited to 64 percent. Staff believes that the 66 percent coverage proposed was based on an earlier iteration of the plan with a longer service corridor along the west property line, and that the expanded staff lawn area now shown in this location has resolved any coverage issue.

The second approval criterion is that, “*The proposal complies with applicable overlay zone requirements (part 18.3).*” In this instance, the property is located within the Health Care Services District overlay zone. Within this overlay zone, both residential uses and clinics for doctors *including but not limited to opticians* are outright permitted uses. The proposal complies with the building height, setback and coverage requirements of the overlay.

The third criterion for the Site Design Review approval is that, “*The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E (which addresses Exceptions) below.*” In this instance, the applicable standards are those for Basic Site Review found in AMC 18.4.2.040.B. seeking:

- Primary orientation toward a street rather than parking, with automobile circulation and parking not allowed between the building and the street and parking instead located behind or to one side.
- A building façade or multiple building facades occupying a large majority of the street frontage.
- Building entrances oriented to the street and accessed from a public sidewalk, which entrances designed to be clearly visible, functional, and open to the public during all business hours.
- Building entrances located within 20 feet of the street, and where located on a corner lot, entrances oriented to the higher order street or the corner and the building located as close to the intersection corner as practicable.
- Public sidewalks.
- One street tree chosen from the street tree list shall be placed for each 30 feet of frontage.
- Landscaped areas at least ten feet in width shall buffer buildings adjacent to streets.
- Landscaping and recycle/refuse disposal areas provided.
- Noise and glare standards addressed.

In this instance, the building is to be oriented to the corner, with the building facades occupying a large majority of the frontage. A patient drop-off drive with circulation between the building and the street is proposed from Maple Street, but parking is provided in the basement. Sidewalks with street trees are to be provided along Chestnut Street with pedestrian access directly to the corner entry, and landscaping provided in required yard areas along both street frontages. Recycling and refuse disposal areas are screened from the service corridor provided uphill of the building.

The application includes parking calculations indicating that 38.57 parking spaces are required for medical office use, while 3.5 parking spaces are required for the two proposed residential units for a total off-street parking requirement of 42.07 spaces. The applicants propose a mixed-use parking credit of 3.5 parking spaces as the peak parking demand of the

medical and residential office uses would be materially off-set, and a 1.0 parking space credit for providing five additional bicycle parking spaces beyond the nine spaces required for the proposed use. This would reduce the off-street parking demand to 37.57 (38) spaces, and the application illustrates 39 spaces proposed including 36 spaces in the basement parking garage, two parallel spaces off of the delivery driveway, and one space off of the patient drop-off drive. In addition, the application illustrates six available on-street spaces along the subject property's frontage although no off-street parking credit is requested.

The fourth approval criterion for Site Design Review addresses city facilities and requires that, *"The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property."* In consulting staff in the various public utilities, the following has been noted:

- **Electricity:** The Electric Department has noted that while there is available capacity in the adjacent overhead lines, there are no facilities currently in place in the immediate vicinity to serve the subject property. The applicants will need to extend services to the site and provide a new transformer on the property, which is complicated by the proposed on-site stormwater detention, site grades, and the need for the service corridor at the rear to maintain access to the trash and recycling facilities. The applicants are working with the Electric Department to identify the facilities necessary to serve the proposed building and develop a final electric service plan for the site.
- **Water:** The Public Works Department has noted that the property is currently served by six-inch water mains in the adjacent rights-of-way for both Maple and Chestnut Streets. New connections and services will need to be extended by the applicants.
- **Sewer:** The Public Works Department has noted that the property is currently served by six-inch sanitary sewer mains in the adjacent rights-of-way for both Maple and Chestnut Streets. New connections and services will need to be extended by the applicants.
- **Urban storm drainage:** The Public Works Department has noted that the property is currently served by a 12-inch storm sewer main in Maple Street, and that there is currently a catch basin in place at the northeast corner of the lot. Public Works/Engineering staff have noted that the storm drain and catch basin are not currently functioning properly as grades are such that drainage misses the existing catch basin; this is not a capacity issue but rather a problem in the design of the existing infrastructure and staff and the project civil engineer are working to correct this issue with a revised stormwater drainage plan for the site.
- **Paved Access & Adequate Transportation:**
  - **Chestnut Street** - The property has approximately 180 linear feet of frontage on Chestnut Street. Chestnut is a Neighborhood Collector Street and city standard frontage improvements would typically require five- to eight-foot parkrow planting strips with irrigated street trees every 30 feet, and a six- to eight-foot sidewalk. Chestnut Street is currently improved with paving, curb, gutter, and an approximately four-foot wide curbside sidewalk in place along the property's full frontage, and there are no parkrow planting strips in place. The applicants

propose a six-foot sidewalk with a five-foot hardscape parkrow, with street trees to be provided in standard five-foot tree wells with grates. While the application does not propose to rely on on-street parking credits to meet project parking requirements, this more commercial frontage treatment is intended to facilitate the use of on-street parking spaces along the subject property's street frontage.

- **Maple Street** - The property has approximately 150 linear feet of frontage on Maple Street. Maple is a Neighborhood Street, and city standard frontage improvements would typically require seven- to eight-foot parkrow planting strips with irrigated street trees every 30 feet, and five- to six-foot sidewalks. Maple Street is currently improved with paving, curb, gutter, and an approximately four-foot wide curbside sidewalk in place along the property's full frontage, and there are no parkrow planting strips in place. The applicants propose to continue the existing curbside sidewalk configuration on this frontage, with street trees to be provided in the landscaped area proposed directly behind the sidewalk and existing trees to be preserved where possible. The application notes that should the Planning Commission prefer, a standard residential park row planting strip could be accommodated on the Maple Street frontage but the removal of additional trees would be necessary.

The application also includes an assessment by Kelly Sandow of Sandow Engineering, who has determined that: the project will generate no more than 49 vehicle trips during the PM peak hour; is not installing any traffic control devices or geometric improvements; and is not expected to generate more than 20 additional heavy vehicle trips during the day. As such, the proposal does not exceed any of the threshold levels which would trigger a Transportation Impact Analysis (TIA).

- **Trash and Recycling:** The application identifies a service corridor coming off of Maple Street which would share a driveway entrance with the patient drop-off and provide truck access to a screened trash and recycling enclosure. In preliminary review by Recology Ashland, they have indicated that the enclosure may not be large enough, and may require individual rolling cans for the two residential units in addition to the commercial dumpsters proposed. The applicants have been made aware of this potential issue, and advised to contact staff at Recology Ashland.

Conditions have been recommended below to require that final civil engineering plans including electric service and utility plans, street improvement plans, grading and drainage plans be provided for the review and approval of the Staff Advisor and city departments prior to the submittal of a building permit.

#### **Exception to the Site Development and Design Standards**

The application proposes a patient drop-off area to be accessed from a circular driveway from Maple Street, and requests an Exception to the Site Development and Design Standards (AMC 18.4.2.040.B.1.a) to allow automobile circulation between the building and the street.

The approval criteria for an Exception are that, *"1) There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the*

*exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or 2) There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.”*

The application emphasizes that a patient drop-off which is safely off-street and a relatively short transport is essential to a medical practice of this specialty as many patients are too frail to manage more than getting out of their car, being assisted by a wheelchair or staff members, and transported a short distance into the clinic. The application further indicates that the site has a unique sloping conditions for a Health Care zone, and suggests that the solution proposed not only meets the needs of the medical practice but also helps reduce drop-off traffic congestion that would otherwise block the street or adjacent residential driveways.

### **Exception to Street Standards**

The application also includes a request for an Exception to Street Standards (AMC 18.4.3.080.C) to allow a smaller than typically-required separation between driveways. Maple Street here is a neighborhood street and controlled access standards call for a minimum 24-foot separation between driveways for two units or fewer per lot, and a 50-foot separation for three or more units per lot. In this instance, the existing curb cut on Maple Street is 19 feet from the driveway uphill to the west, and the applicants propose to utilize this existing curb cut for access to serve both their service corridor and patient drop-off, noting that the drop-off is necessitated by fully 31 percent of patients are dropped off for their appointments by a driver leaving to do other things during the appointment time, and that the drop-off avoids traffic that would otherwise block Maple Street with drivers trying to make drop-off's from the street.

The applicants note that the proposed configuration will keep larger vehicles off of the street while they are servicing the building for trash, recycling, delivery or other services, and patients using the one-way circulation of the drop-off will exit the property an additional 38 feet to the east. In addition, the applicants note that the existing driveway in this location serves 12 off-street parking spaces, and by shifting these spaces to basement parking they believe that there is an opportunity to positively impact the residential neighbors while retaining the existing curb cut and addressing the need for a patient drop-off on this site.

### **Trees**

The application materials identify 14 trees on and adjacent to the property which are six-inches or greater in diameter at breast height, and note that these are a mix of deciduous and coniferous trees. The application suggests that some trees will be significantly impacted by sidewalk replacements, and those that will not be affected will be protected and preserved. The landscape plan provided identifies only two of the trees for removal: one conifer near the northwest corner of the existing building which is not identified on the tree survey but which will be impacted both by the demolition and by the placement of the new drop-off drive, and the other a ten-inch d.b.h. deciduous tree which will be located just south of the patient drop off drive and which will be impacted by the sidewalk replacement.

As this document is being prepared, the Tree Commission has not yet reviewed the application and a condition is recommended below to make their recommendations conditions of approval.

The application is somewhat unclear with regard to trees in that the narrative provided suggests that more trees are to be removed than are shown to be removed on the landscaping plan provided. In addition, no report from an arborist has been provided either to assess the trees' present conditions or abilities to accommodate the redevelopment of the site or to identify specific measures for their protection.

In staff's preliminary assessment, the two trees identified for removal in the Landscaping Plan are likely necessary to accommodate the demolition of the existing building and redevelopment of the site as proposed, which is in keeping with the Health Care zoning. Staff has recommended that an arborist fully assess all of the site's trees and that a Tree Protection Plan based on the arborist's report be prepared for the review and approval of the Staff Advisor prior to the issuance of a building permit. Should the arborist determine that additional removals are necessary, a modification of the approval would be necessary, and would need to be considered by the Tree Commission.

### **III. Procedural - Required Burden of Proof**

**The criteria for Site Review approval from the Site Design Review Chapter are detailed in AMC 18.5.2.050 as follows:**

- A. ***Underlying Zone:*** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
- B. ***Overlay Zones:*** *The proposal complies with applicable overlay zone requirements (part 18.3).*
- C. ***Site Development and Design Standards:*** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
- D. ***City Facilities:*** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*
- E. ***Exception to the Site Development and Design Standards.*** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.*
  - 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*

2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*

**The criteria for an Exception to Street Standards are described in AMC 18.4.6.020.B.1 as follows:**

- a. *There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.*
- b. *The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.*
  - i. *For transit facilities and related improvements, access, wait time, and ride experience.*
  - ii. *For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.*
  - iii. *For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.*
- c. *The exception is the minimum necessary to alleviate the difficulty.*
- d. *The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.*

**The criteria for Tree Removal Permits are detailed in AMC 18.5.7.040.B as follows:**

1. **Hazard Tree.** *A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
  - a. *The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.*
  - b. *The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*
2. **Tree That is Not a Hazard.** *A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
  - a. *The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints*

- in part 18.10.*
- b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*
  - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
  - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
  - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

#### **IV. Conclusions and Recommendations**

The application notes that the proposed multi-level building serves a low-mobility patient demographic; provides unique solutions to a challenging sloped site and existing parking impacts imposed by the neighboring Ashland Community Hospital; and thoughtfully addresses the transition from healthcare to residential zones.

Staff would concur with the applicant's assessment. Development of the property is challenged both by Maple Street's topography and by adjacent development. The presence of Asante Ashland Community Hospital (AACH) immediately across the street brings with it a considerable parking demand which frequently consumes all of the available on-street parking in the vicinity, including both of the subject property's full street frontages, and which makes it imperative that the subject property accommodate its full off-street parking demand on site rather than relying on on-street parking credits. The property's topography makes this a challenge in that it slopes from Chestnut up Maple by approximately 15 percent, and the applicants must not only accommodate parking on a sloped site but also do so in a way that will serve a clientele with physical and visual impairments. The applicants have responded by taking full advantage of the site's topography, excavating to place proposed parking in the basement beneath the first floor of medical suites while providing a patient drop-off directly to the first floor from Maple Street. Required off-street parking is accommodated entirely on the subject property while allowing for the full redevelopment of this long-dilapidated property in keeping with the underlying Health Care Services District overlay, and by using an existing curb cut on Maple Street to serve not only the drop-off but also a screened service corridor on the uphill side of the building, thereby providing a larger separation between the new building and the existing home above while site topography, associated retaining and proposed vegetative screening buffer the corridor.

In staff's view, the proposal complies with all applicable standards found in the Ashland Land Use Ordinance and merits approval. Should the Commission concur, staff would recommend that following conditions be attached:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in conformance with those approved as part of this application, an application to modify the current Site Design Review approval shall be submitted and approved prior to issuance of a building permit.
3. That permits shall be obtained from the Ashland Public Works/Engineering Division prior to any work in the public right of way. New driveway approaches shall be permitted through the Public Works/Engineering Division and driveway curb cuts shall be installed, inspected and approved prior to the issuance of a certificate of occupancy.
4. That the recommendations from the Tree Commission from its September 7, 2017 meeting shall be conditions of approval where consistent with applicable criteria and standards and with final approval by the Staff Advisor.
5. That a Demolition Permit shall be obtained from the Building Division if deemed necessary by the Building Official prior to any demolition of existing buildings.
6. That a sign permit shall be obtained prior to installation of any new signage. Signage shall meet the requirements of Chapter 18.4.7 and shall meet the requirements of 18.2.4.040 if located in a vision clearance area.
7. That the front entrance at the corner shall remain functional and open to the public during all business hours.
8. That the building permit submittals shall include:
  - a. Building permit plans shall include the identification of all easements, including but not limited to any public or private utility easements.
  - b. Building permit plans shall include solar setback calculations demonstrating that all new construction complies with the applicable solar access standard. Permit submittals shall include elevation drawings or cross sections clearly identifying the highest shadow producing point(s) and their height(s) from natural grade and calculations in the form of  $[(\text{Height} - 6) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$ .
  - c. That a final utility plan shall be submitted for review and approval by the Planning, Engineering, and Building Divisions prior to signature of the final survey plat. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer lines, storm drain lines and electric services.
  - d. That the location and final engineering for all storm drainage improvements associated with the project, shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions prior to signature of the final survey plat. Storm water from all new impervious surfaces and run-off associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.

- e. That the applicant shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric Department prior to the signature of the final survey plat. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department.
- f. That the engineered construction drawings for the public sidewalk along Chestnut Street shall be submitted for review and approval of the Ashland Planning and Engineering Divisions prior to work in the street right-of-way and prior to installation of improvements in the pedestrian corridor. The sidewalk shall be a minimum of six-feet in width with five-foot width hardscape parkrows and street trees in city standard five-foot square tree grates between the sidewalk and the street. All frontage improvements, including but not limited to the sidewalk, street trees, and street lighting, shall be constructed across the entire frontage of the site, inspected and approved prior to the issuance of a certificate of occupancy. The sidewalk shall be constructed to City of Ashland Street Standards. If necessary to provide required frontage improvements, the area necessary for street improvements shall be dedicated as public street right-of-way or a public pedestrian access easement provide. Right-of-way dedications or easements shall be submitted for review and approval of the Planning and Engineering Divisions.
- g. That the engineered construction drawings for the public sidewalk along Maple Street shall be submitted for review and approval of the Ashland Planning and Engineering Divisions prior to work in the street right-of-way and prior to installation of improvements in the pedestrian corridor. The sidewalk shall be a minimum of five-feet in width installed curbside. All frontage improvements, including but not limited to the sidewalk, street trees in the landscaped areas behind the sidewalk, and street lighting shall be constructed across the entire frontage of the site, inspected and approved prior to the issuance of a certificate of occupancy. The sidewalk shall be constructed to City of Ashland Street Standards. If necessary to provide required frontage improvements, the area necessary for street improvements shall be dedicated as public street right-of-way or a public pedestrian access easement provide. Right-of-way dedications or easements shall be submitted for review and approval of the Planning and Engineering Divisions.
- h. That a final Landscaping and Irrigation Plan shall be provided for review and approval. This plan shall include: 1) irrigation details satisfying the requirements of the Water Conserving Landscaping Guidelines and Policies found in AMC 18.4.4; 2) identification of the placement and screening of the required recycling/refuse area; 3) final proposed lot coverage calculations including all building footprints, driveways, parking, and circulation areas. Lot coverage shall be limited to no more than 65 percent as allowed in the HC zoning district; 4) identification of two required mitigation trees.
- i. An arborist report assessing the condition of existing trees and their abilities to accommodate the proposed construction and a Tree Protection Plan prepared by a certified arborist to address trees on the subject property and on adjacent

properties within 15 feet of the property line shall be submitted for the review and approval of the Staff Advisor.

- j. Demonstration that exterior lighting shall be directed onto the property and shall not directly illuminate adjacent properties shall be provided for the review and approval of the Staff Advisor. Exterior lighting details including fixture specifications, placement details and shrouding details (*if necessary*) shall be provided on building permit submittals.
- k. That exterior building materials and paint colors shall be compatible with the surrounding area and consistent with those described in the application materials. Sample exterior building colors shall be provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with the Site Design and Development Standards.
- l. That the building permit submittals shall include verification that required parking spaces and back-up areas meet the dimensional requirements of the land use ordinance.
- m. That the building permit submittals shall identify the required sheltered bicycle parking spaces. The building permit submittals shall verify that the bicycle parking design, spacing and coverage requirements in AMC 18.4.3.070 are met, and all bicycle parking shall be installed in accordance with the approved plan prior to the issuance of the certificate of occupancy.
- n. That the building permit submittals shall identify all proposed mechanical equipment in the elevation drawings, as required in AMC 18.5.2.040.B.4.a., and that these drawings shall include screening meeting the requirements of AMC 18.4.4.030.G.4 to limit the view of all roof-mounted mechanical equipment from public rights-of-way and adjacent residentially-zoned properties through the placement of parapets, walls or other sight-blocking features at least equal in height to the proposed mechanical equipment. Mechanical equipment installation and operation shall be consistent with the provisions of the Ashland Municipal Code, including but not limited to noise attenuation.

9. That prior to the issuance of a building permit:

- a. That the tree protection fencing and other tree preservation measures shall be installed according to the approved plan, inspected and approved by the Staff Advisor prior to any site work, including demolition, staging or storage of materials, or issuance of the building permit. The tree protection shall be chain link fencing six feet tall and installed in accordance with 18.4.5.030.C. and no construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
- b. That all necessary building permits fees and charges, including permits for new electric and water services, and system development charges for water, sewer, storm water, parks, and transportation shall be paid prior to the issuance of a building permit.
- c. The requirements of the Ashland Fire Department, including approved addressing, fire apparatus access, firefighter access pathway, fire hydrant

clearance, fire department connection (FDC), and provisions for a key box must be complied with prior to issuance of the building permit or the use of combustible materials. Fire Department requirements shall be included on the engineered construction documents. If a fire protection vault is required, the vault shall not be located in the sidewalk corridor.

10. That prior to the issuance of a certificate of occupancy:
  - a. That the required bicycle parking shall be installed according to the approved plan, inspected and approved by the Staff Advisor.
  - b. That all landscaping and irrigation shall be installed in accordance with the approved plans, inspected and approved by the Staff Advisor. Replacement trees to mitigate the two trees removed shall be planted and irrigated according to the approved plan.
  - c. That all public improvements including but not limited to the sidewalk, irrigated street trees, and street lighting along Maple and Chestnut Streets shall be installed under permit from the Public Works Department and in accordance with the approved plan prior to the issuance of a certificate of occupancy. All street trees shall be chosen from the adopted Street Tree List and shall be planted in accordance with the specifications contained therein. The street trees shall be irrigated.
  - d. That the screening for the trash and recycling containers shall be installed in accordance with the Site Design and Development Standards prior to the issuance of a certificate of occupancy. An opportunity to recycle site of equal or greater size than the solid waste receptacle shall be included in the trash enclosure in accordance with 18.4.4.040.
  - e. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.

# Maple Street Medical Building

330 Maple Street, Ashland Oregon

Application Proposal  
City of Ashland



**Submitted by:**

Reitinger & Associates, Inc.  
307 N Main Street  
Ashland, Oregon

**Date Submitted:**

July 24, 2017

**Property Owner:**

Maple LLC  
246 Catalina Drive  
Ashland, Oregon

**RECEIVED**

**AUG 03 2017**

**City of Ashland**

## **PREAMBLE**

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This mixed-use medical building will be the first new construction within the hospital district in 30+ years. Its presence will help maintain a medical community that uses & supports the neighboring Asante Ashland Community Hospital (AACH). This multi-level building services a low-mobility patient demographic and will provide unique solutions to: a typically challenging sloped site, existing parking impacts imposed by the neighboring AACH, and thoughtfully addresses the transition from healthcare to residential zones.

## **PROJECT SUMMARY**

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**Project Name:** Maple Street Medical Building

**Description:** Mixed-use building with: medical suite(s) and residential

**Project Address:** 330 Maple Street, Ashland OR

**Map/Tax Lot:** 39 1E 05 DB-2000

**Lot Size:** 0.63 of an acre = 27,443 sq. ft.

**Zoning:** Health Care (HC)

**Building Size:**

Parking Level (below grade)	= 13,500 gross sq ft
Medical Suite(s) (above grade)	= 13,500 gross sq ft
Residential (2 <sup>nd</sup> floor)	= 2,400 gross sq ft
<hr/>	
<b>Total</b>	<b>= 29,400 gross sq. ft.</b>

**Lot Coverage:** 66% percent\* (65% maximum without exception)

**Lot Coverage (by percent):**

Main building (below grade)	= 49%
Screened Trash & Backup Gen.	= 1%
Site Circulation / Driveways	= 14%
<hr/>	
Impervious Area Sub-Total	= 64%
Staff Courtyard	= 6%
Landscaping (total sq. ft. = 4,390)	= 30%
<hr/>	
Pervious Area Sub-Total	= 36%
<b>Total</b>	<b>= 100%</b>

**Parking Summary:** Minimum required for medical use = 38.57 spaces  
Minimum required for residential use = 3.50 spaces

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**Total Required = 42.07 spaces**

Parking Credits:

Bicycle Parking Credit = -1.00 spaces

Mixed Use Parking Credit = -3.50 spaces

Total Parking Credits = -4.50 spaces

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**Net Required = 38 spaces**

Satisfied by:

Off-street – Below Grade

Standard (9' x 18') = 14 spaces

Compact (8' x 16') = 19 spaces

ADA Standard = 2 spaces

ADA Van = 1 space

Off-Street – Above Grade

Parallel (7' x 22') = 2 spaces

Compact (8' x 16') = 1 space

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**Total provided Off-Street = 39 spaces**

On-street (not counted above)

Parallel (7' x 22') = 6 spaces

## **THE APPLICANT**

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While the property is owned in a separate LLC the medical practice developing this project is Retina & Vitreous Center of Southern Oregon, PC. Some helpful practice demographics include:

- Retina physicians and surgeons deal with the deep interior of the eye. Most patients have seen from one to three doctors before ending up at the retina specialists' office.
- Retina diseases are often blinding or sight diminishing
- Most retina patients are unable to drive themselves to their appointments and instead rely on a combination of family, friends, taxis, medical transport vans, and the bus
  - Most patients are temporarily, if not permanently, partially disabled due to vision issues
- 82% of the patients are over 65 years old
- 25% of the patients are over 85 years old
- New patient appointments typically last from 2-3.5 hours due to the time for testing, examination, and treatment
- Returning appointments can be from .5-1.5 hours
- Approximately 31% of all patients are dropped off for their appointment and picked up when they are done.
  - Another 2% take the bus, walk, or ride to an appointment
- Almost all retina surgery is outpatient (same day) surgery and all is done at AAHC
- Before Dr. Rodden, the founding member of this practice, opened in Ashland patients with Retina injuries or diseases were transported to Portland or San Francisco.
- Today a retina patient has a much better chance of retaining their vision due to improved testing, procedures, surgeries, and drugs injected into the eye.

## EXISTING SITE

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### Zoning & Lot Features:

- Tax lot 2000 sits on the corner of Maple Street and Chestnut Street within the Health Care (HC) zone.
- The existing site is currently occupied by a very old single story medical building and a small single story storage structure.
  - Both structures are in poor condition and proposed to be demolished.
- The site is outside of the Historic District, however care and consideration to blend the new structure into the existing surroundings is being honored.
- Neighboring tax lot zones are the:
  - Heath Care Services District (HC),
  - Residential – Low Density Multiple Family (R-2)
  - Residential – Single Family (R-1-7.5)
  - Architectural styles range from commercial to ranch or craftsman-like style homes
- The corner lot is sloped with a grade change from the address side on Maple St to Chestnut Street below
- Chestnut is considered the higher order street for planning and setback purposes



Existing North West Façade at Maple Street



Existing East Façade at Chestnut Street



Neighboring Single Family Homes (North of site)



Neighboring Multi-family Homes (West of site)



Neighboring Ashland Community Hospital (East of site)



Neighboring Medical and Residential (South of site)

## **NEIGHBORHOOD OF SITE**

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The property is located in the HealthCare (HC) Zone Across from Asante Ashland Community Hospital. There are many unique conditions relative to the Health Care Zone in Ashland which include, but are not limited to:

- The only new construction in the HC Zone in the past 30+ years has been done by Ashland Community Hospital in expanding emergency and outpatient services
- The Ashland Community Hospital recently became Asante Ashland Community Hospital (AACH). It is unclear at this time what that will mean for healthcare in Ashland going forward
  - The two biggest surgical services at AACH are Retina and Orthopedics
  - AACH is the only hospital in the valley that does any ophthalmologic surgery. All other eye surgery is done in outpatient surgery centers.

## **PROPOSED BUILDING**

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Proposed mixed-use building features:

- Become an asset to the Healthcare community/neighborhood and a vast improvement to what is there now
- Provide an aesthetic architectural transition between the Healthcare and Residential zones
- Keep the building as low as possible, within the 35-foot height limit, for minimal effect on uphill residential views
- Subterranean parking garage accommodating both residential and commercial needs
- Medical suite(s) on the ground floor
- Two residential penthouse suites on the 2<sup>nd</sup> floor (above the medical suites)
- A prominent Main Entry is positioned at the front corner of Maple and Chestnut Streets to provide an obvious pedestrian cue and vertical circulation to the building.
- Building occupancy type shall be Business (B) and Residential (R-2)
- Elevator access will be available to all levels, originating in the parking garage
- Type II-A fire sprinklered building

#### Main Entry Pavilion:

- Positioned on the Corner of Maple & Chestnut street this strong entry feature is intended as entry focus for people, not for automobiles
- Entry into this foyer from both the parking garage and the street delivering patients to the clinic floor and residents to the residential floor
- The entry column adds an environmentally favorable element in providing a transition between outside temperatures to inside temperatures.

#### The Subterranean parking garage does a number of things for this project:

- Aesthetically a parking garage under the building will keep the most of the parked cars hidden in this mixed Health Care/ Residential neighborhood
  - The parking under the building allows for a functional medical building while minimizing the visual impact of the cars
  - Reducing the visual impact of parking has long been a goal of the City planning team
  - The surface parking in the proposed plan is a 75% reduction from the current twelve surface parking spaces.
- The parking garage will daylight to the East onto Chestnut providing a smooth entrance and exit to Chestnut street making it easy access for staff, patients, and residents

#### The total medical floor sq. ft. shall be 13,500 sq ft:

- There ends up being a sweet spot between number of parking spaces, building size, and lot coverage. 13,500 Square feet is that sweet spot. Due to parking space sizes and parameters making the building any smaller than this 'sweet spot' means that decreasing building size becomes substantial as a percentage and not incremental. This substantial loss makes the project a non-starter for the client.

#### The two penthouse residential units have the following characteristics and advantages:

- ~1200 square feet each unit
- ~2400 square feet total for both units
- 20-30 % of the roof area is proposed for an aesthetic outdoor living area for the two suites
- Remaining rooftop areas will be used to house screened mechanical equipment for the building and maintain visual site lines from the street levels
- Help maintain the residential character of the neighborhood
- Help maintain the security of the medical building and neighborhood by having more constant presence than either an office building (days) or residents (nights and weekends)

#### Site Circulation

- Automobile access to the parking structure from Chestnut Street
- Pedestrian and Bicycle through the front main entry pavilion
- Patient drop-off drive at the Clinic level on the North end of the building
  - **This feature is imperative to the project due to the patient population**
    - Without this feature this project will not happen.
  - 31% of all patients are dropped off by a driver who do not park on site

- This will facilitate ADA and elderly patient access of the many patients with limited mobility and those who do not drive themselves
    - Many have co-morbidities which require access to the clinic level free of prolonged physical effort
    - 5-10% of all patients need clinic staff or wheelchair support to enter
    - This gets patients who are being dropped off away from street traffic and mitigates potential dangers for patients and other drivers
  - One parking space in the drop-off area
    - Some drivers will need to assist patient into the clinic or will go in to get assistance
    - One parking space is also proposed in this area so that cars don't stack up
  - This drop-off will also re-route and traffic from driving up to the dead-end portion of Maple Street that services the neighboring residences
  - Every effort has been made to minimize the impact of this requested exception.
  - If this feature didn't exist patient's drivers would still drive up the hill to see if they could drop off a patient, turn around, and often let them out on the side of the road which could be quite dangerous
- *Request exception to LUO 18.4.2.040.B.1.a. Automobile circulation or off-street parking is not allowed between the building and the street.*
    - *A patient drop off which is safely off-street and relatively short transport is essential to a medical practice of this specialty. Many patients are too frail to manage more than getting out of their car, being assisted by wheelchair or staff members, and transported a short distance into the clinic.*
    - *This site has a unique sloping condition for a Hospital Zone site.*
    - *We believe that this solution not only meets the needs of the practice but also helps reduce the traffic that blocks the street or drives past into the residential area looking for a 'better way to get Mom into the office.'*

Service entrance on the West (back side) of the building

- To utilize the same location of the existing curb cut
- Will double as the entry point to the clinic patient drop-off drive
  - This will keep those larger vehicles off the street while they are servicing the building for trash, recycle, service, and delivery access
  - Those drivers (of patients) who use the patient drop off will actually exit the property further than the 25 feet separation requirement for neighborhood streets.
- *Request exception to LUO 18.4.3.080.C.3 Intersection and Driveway Separation. The distance from a street intersection to a driveway, or from a driveway to another driveway shall meet the minimum spacing requirements for the street's classification in the Ashland Transportation System Plan. Street and driveway access points ... shall be limited to the following. i. Distance between driveways: on collector streets: 75 feet, on neighborhood streets: 24 feet for 2 units or fewer per lot...*
  - *This existing curb cut currently services 12 parking spaces. By retaining the existing curb cut, reducing the number of off-street parking spaces on this level and redirecting that traffic back out to the street without impacting the residential neighbors there is the*

*opportunity both to positively impact the residential neighbors, retain the existing curb cut, and address the unique need for a patient drop off on this site.*

## **PARKING**

### **Parking Allocations:**

Medical Parking Required:	38.57
Residential Parking Required:	3.50
Total Parking Required:	42.07
Parking Credits:	
Bicycle Parking Credit	-1.00
Mixed Use Credit	-3.50
Total Parking Credits	-4.50
<b>NET Required Parking</b>	<b>38.00</b>
Satisfied by:	
Off-Street Underground Parking:	
Standard (9'x18')	14
Compact (8'x16')	19
ADA Standard	2
ADA Van	1
Off-Street Above ground Parking:	
Parallel (7'x22')	3
<b>Total Off-Street Parking spaces provided:</b>	<b>39.00</b>
On-Street Parking (not applied above)	6.00

### **Special Parking Notes:**

- While this site fully meets its parking requirements it may be helpful, in case the Commission has concerns, to review other positive impacts to the area parking created by this proposed building.
  - Off-street Parking was not applied to this project
    - An allowance of the 6 available spaces could be credited to this project
    - Historically the on-street parking was credited to the landowner
  - Underground Parking advantages:
    - Aesthetically far more pleasing for the neighborhood than open paved parking for a large medical building

- It is very costly to provide underground parking but makes sense in this application
    - *While neither the Department nor Commission have a policy on this issue it is likely worthy of a parking credit of at least 5-10% of all provided underground parking*
  - Unusual parking conditions
    - 31% of all patients are dropped off for their appointment with the driver leaving to do other things during the appointment time
      - An internal patient study performed by asking patients (& direct observation) at the clinic whether their driver stayed with them or left during their appointment time to do other things and returned to pick them up when finished produced the following:
        - 27% of patients were delivered by a driver who left
        - 4% of patients were delivered by senior or medical transport van service
        - 2% of patients walk/take the bus/ ride to their appointment
    - Some patients do walk between the hospital for testing and the office and tend to park, if they have a car, at the first location of their day.
      - This is also true for physicians doing surgery
- **Parking In Summary**
  - This site and parking plan provides adequate parking for the use and conditions
    - 100% of the required parking is provided on-site
    - In addition, there are a number of positive parking impacts created by the proposed building on area parking.

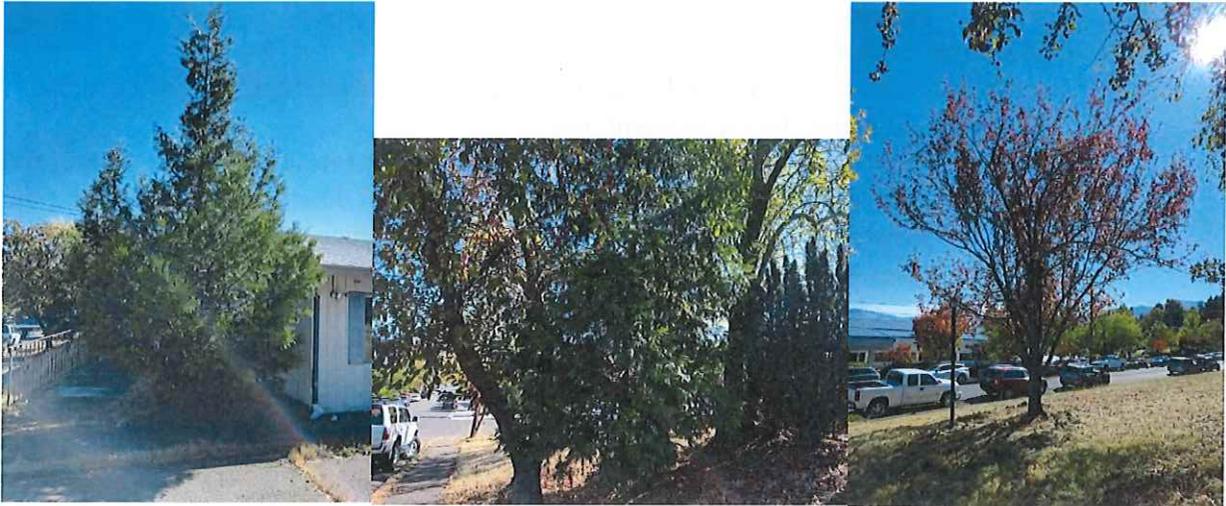
## **PROPOSED LANDSCAPING & SITE DEVELOPMENT**

### **PARK ROWS & COMMERCIAL PARK ROWS**

- Chestnut Street
  - On this property and the south neighboring residential property landscape park rows do not currently exist
  - For Chestnut Street we propose a hardscaped Commercial style park row with street tree wells.
    - Allows for safer pedestrian access and from cars to the curb for the (4) parallel parking spaces along Chestnut Street.
- Maple Street
  - On this property and the west neighboring property landscape park rows do not currently exist
  - No park row is suggested in order to maintain existing street trees, and to reduce the impact of impervious area on the property's lot coverage.
  - However, if the Department or Commission prefer, on Maple Street side a residential park row can be accommodated
    - However, this pushes the sidewalk back partially onto the owner's property increasing impervious lot coverage by 2%

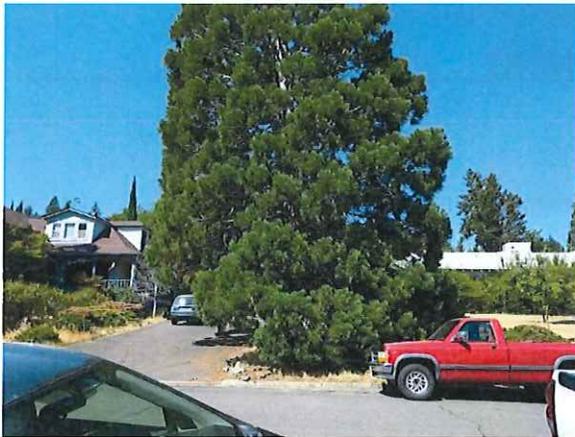
STREET & PROPERTY LINE TREES

Existing North Property Line (Maple) Street Trees

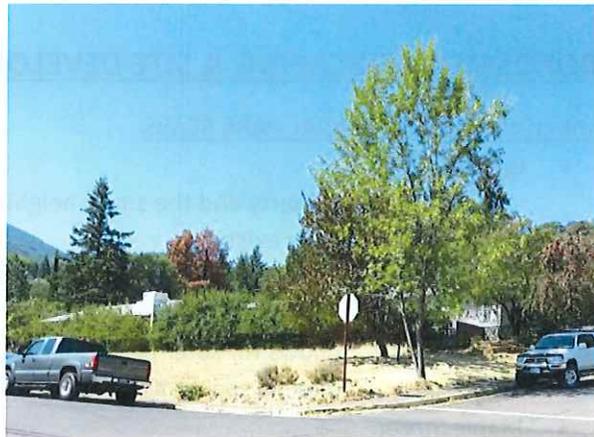


- There are a number of street trees existing on the (Maple St) Northern property line
  - Most will be significantly impacted by the new sidewalk placement and will be removed
  - For trees not affected by the new park row and sidewalk, protection and preservation will be sought

Existing East Property Line (Chestnut) Street Trees



Existing evergreen cedar tree at Southeast corner of site to be protected.



Existing deciduous tree at Northeast corner proposed for protection. Note this tree is stationed in the vision clear area required at the street intersection.

- There are a number of street trees existing on the (Maple St) Northern property line
  - There is a mix of deciduous and coniferous trees proposed for removal and replacement
  - Most will be significantly impacted by the new sidewalk placement and will be removed
  - For trees not affected by the new park row and sidewalk, protection and preservation will be sought
- New Trees will be planted in the new Commercial Park Row

WEST PROPERTY LINE VEGETATION (Back Property Line not bordering a street)

- The existing vegetation at the Western property line bordering the residential zone is in poor health
  - Removal of shrubs or trees within the property line will be replaced with a continuous fence and hedge to meet the requirements for privacy screening
  - Shrubs and trees located on the neighboring properties will be protected from damage



Existing sprawling and overgrown junipers to be removed and replaced with a continuous hedge.



New continuous hedge to be added to fill in gaps within existing vegetation.

Proposed New Street Trees

- Where new street trees are proposed all shall conform to the City of Ashland's, "Recommended Street Tree Guide".
  - A few species currently being considered are:
    - Acer Griseum (Paperbark Maple)
    - Acer Grandidentatum (Bigtooth Maple)
    - Prunus Cerasifera (Flowering Plum).



Paperbark Maple



Bigtooth Maple



Flowering Plum



## **TRAFFIC IMPACT & ARIAL LADDER ACCESS**

- **Traffic Impact Analysis (TIA) Applicability:** Consultation with Engineer, Kelly Sandow of Sandow Engineering, has determined that this project does not exceed the thresholds that would require a TIA. See attached statement and calculations from Sandow Engineering, "Maple Street Medical Building Trip Generation and TIA Applicability".
- **Arial Ladder Access:** Contact has been made with Margueritte Hickman of the Ashland Fire and Rescue, and it was agreed that Arial Ladder Access was no longer a requirement for this project. See attached letter from Ashland Fire & Rescue.

## **SUMMARY**

The Maple Street Medical Building shall cater to both residential and healthcare needs in this Healthcare tax lot bordering Healthcare and Residential zones, by providing medical and residential suites. A subterranean parking structure will alleviate congested streets and give safe and direct access to the facility. It is our hope that a new medical building in the Health Care zone will go a long way to revitalizing Ashland's own medical community. We believe this facility will become an asset to the healthcare district and community for many years to come.

Any comments made by the Planning Department that are not specifically addressed here are considered informational and will be addressed or developed within the permit set forthcoming.

Submitted by:



Mark L. Reitinger  
Reitinger & Associates, Inc.

*Enclosures:*

- *Ashland Fire and Rescue Letter, "Pre-Application Comments, Revised 2/22/17"*
- *Sandow Engineering Letter and Calculations, "Maple Street Medical Building Trip Generation and TIA Applicability"*
- *Maple Street Medical Building Conceptual Drawings (G-001, A-201-204, V-001, C-001 & L-001)*



## Ashland Fire & Rescue

455 Siskiyou Boulevard  
Ashland, OR 97520  
541.482.2770

### Pre-Application Comments

<b>Project Address:</b>	330 MAPLE ST	<b>Date:</b>	2/22/2017
<b>Project Description:</b>	PreApp: 12/7/16 @ 2pm	<b>Permit Number:</b>	PL-2016-02058
<b>Ashland Fire &amp; Rescue Contact:</b>	Margueritte Hickman	541-552-2229	Margueritte.hickman@ashland.or.us

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Fire department comments are based upon the 2014 Oregon Fire Code as adopted by the Ashland Municipal Code:

**Aerial Ladder Access** – Structures exceeding 24 feet in height above the lowest level of fire apparatus access are required to provide access roads capable of accommodating fire department aerial apparatus. These access roads are required to be 26 feet in width in the immediate vicinity of the building. OFC Appendix D 105 as amended by AMC 15.28.070 K & L. ~~The building access route on the west side of the building is required to meet aerial fire apparatus access.~~ Fire apparatus access shall have a 20 foot wide driving surface, must support 60,000 pounds, have a maximum slope of 15 percent, and have vertical clearance of 13' 6". Inside turning radius is at least 20 feet and outside turning radius is at least 40 feet and must be indicated on site plans submitted for building permits. Fire apparatus access is required to be signed as "No Parking-Fire Lane". Fire apparatus access will be accepted from the streets adjacent to the property.

**Addressing** - Building numbers or addresses must be at least 4 inches tall, be of a color that is in contrast to its background, and shall be plainly visible and legible from the street fronting the property. Additional directional signage may be necessary to guide emergency responders down a driveway, path or through a gate. All premises identification, street signs and building numbers, must be in place with temporary signs when construction begins and permanent signage prior to issuance of any occupancy. OFC 505

**Firefighter Access Pathway** – An approved footpath around the structure is required so that all exterior portions of the structure can be reached with the fire hose. Any changes in elevation greater than two feet in height (such as retaining walls) require stairs. The stairs shall be an all-weather surface, and meet the requirements as specified in the Oregon Structural Specialty Code. OFC 503.1.1

**Fire Hydrants Clearance** - Hydrants must have 3 feet of clearance extending from the center nut of the hydrant all the way around. Fences, landscaping and other items may not obstruct the hydrant from clear view. Hydrants must be shown on site plan when submitting for building permits.

**Fire Department Connection (FDC)** – The FDC is required to be a 2 ½” Siamese female connection installed 18” to 48” above finished grade. A single 2 ½” NST female swivel connection with rocker lugs and cap is acceptable if hydraulic calculations are provided that indicate a single 2 ½ “ line will adequately serve the system. Fire flow alarm shall be placed on the FDC. FDC shall be placed in a location approved by the fire department.

**Gates and Fences** – Obstructions such as gates, fences, or any other item which would block or reduce the required fire apparatus access width must be shown on the plans and approved by Ashland Fire and Rescue.

**Key Box** – A Knox Box is required for commercial buildings with fire sprinkler or fire alarms systems. The Knox Box must be a 3200 series or larger with a hinged door and may be either surface mounted or recessed into a wall. The installation location of the Knox Box will be determined by Ashland Fire & Rescue. The Knox Box is required to be installed in accordance with the manufacturer’s instructions. The Knox Box can be ordered at [www.knoxbox.com](http://www.knoxbox.com).

**Vegetation** – Ashland Fire & Rescue recommends the planting and maintenance of fire resistive vegetation throughout the city of Ashland. Here’s how you can create a [www.ashlandfirewise.org](http://www.ashlandfirewise.org) landscape.

**Fire Season** – If work will be completed during fire season, check fire season fire prevention requirements found at [www.ashland.or.us/fireseason](http://www.ashland.or.us/fireseason).

**Final determination of fire hydrant distance, fire flow, and fire apparatus access requirements will be based upon plans submitted for Building Permit review. Changes from plans submitted with this application can result in further requirements. Any future construction must meet fire code requirements in effect at that time. The fire department contact for this project is Fire Marshal Margueritte Hickman. She may be contacted at (541) 552-2229 or [hickmanm@ashland.or.us](mailto:hickmanm@ashland.or.us)**



# SANDOW ENGINEERING

160 MADISON STREET SUITE A • EUGENE, OREGON 97402 • 541.513.3376

March 20, 2017

Karl Johnson, E.I.T., Associate Engineer  
City of Ashland, Public Works/Engineering  
20 East Main St  
Ashland, Oregon 97520



RENEWAL 06 / 30 / 18

RE: Maple Street Medical Building Trip Generation and TIA Applicability

Sandow Engineering has prepared an estimation of the anticipated vehicle trips generated by the proposed Maple Street Medical Building on Maple Street in Ashland, Oregon.

The applicant is proposing the construction of 13,500 sf Medical Office Building with two apartment units totaling 2,400 sf on the second floor and underground parking.

## TRIP GENERATION

The trip generation for the development was estimated using information contained within the Institute of Transportation Engineers (ITE) Trip Generation Manual 9<sup>th</sup> Edition. The site trips are estimated using the data provided for Medical-Dental Office Buildings (LUC 720) and Apartments (LUC 220). The site generated trips for the AM and PM peak hours are illustrated in Table 1.

TABLE 1. TRIP GENERATION ESTIMATE

ITE Land Use	Size	Units	Trip Generation	
			Rate	Trips
<b>AM Peak Hour</b>				
720 – Medical – Dental Office Building	13.5	ksf	2.39	32
220 – Apartments	2	DU	0.51	1
			<b>Total Trips</b>	<b>33</b>
<b>PM Peak Hour</b>				
720 – Medical – Dental Office Building	13.5	ksf	$\ln(T) = 0.90 \ln(X) + 1.53$	48
220 – Apartments	2	DU	0.62	1
			<b>Total Trips</b>	<b>49</b>

As demonstrated, the proposed development is anticipated to generate 33 trips during the AM Peak Hour and 49 trips during the PM Peak Hour.

## TIA APPLICABILITY

Ashland Code Requires a Traffic Impact Analysis when one of the following occurs:

- 1) Addition of 50 newly generated vehicle trips during the adjacent street peak hour

From: Kelly Sandow PE Sandow Engineering  
RE: Maple Street Medical Building-Trip Generation and TIA applicability  
Date: 3/20/17  
Page 2

The expansion is anticipated to generate 49 vehicles trips in the PM peak hour and 29 vehicle trips in the AM peak hour. This criteria is not met for warranting a TIA.

- 2) *Installation of any traffic control device and/or construction of any geometric improvements that will affect the progression or operation of traffic traveling on, entering, or exiting the highway*

The applicant is not installing any traffic control devices or constructing any geometric improvements within the ROW. This criteria is not met for warranting a TIA.

- 3) *Addition of 20 newly generated heavy vehicle trips during the day.*

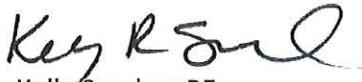
The proposal is for a medical office building with two apartments. There are a limited number of delivery trucks per day for these uses. The development is not expected to not generate more than 20 additional heavy vehicle trips during the day. This criteria is not met for warranting a TIA.

#### FINDINGS

As demonstrated, the proposed development of the Maple Street Medical Building is anticipated to increase traffic to the site by 49 vehicle trips during the PM peak hour. The increase in traffic does not meet the City of Ashland thresholds for triggering a Traffic Impact analysis.

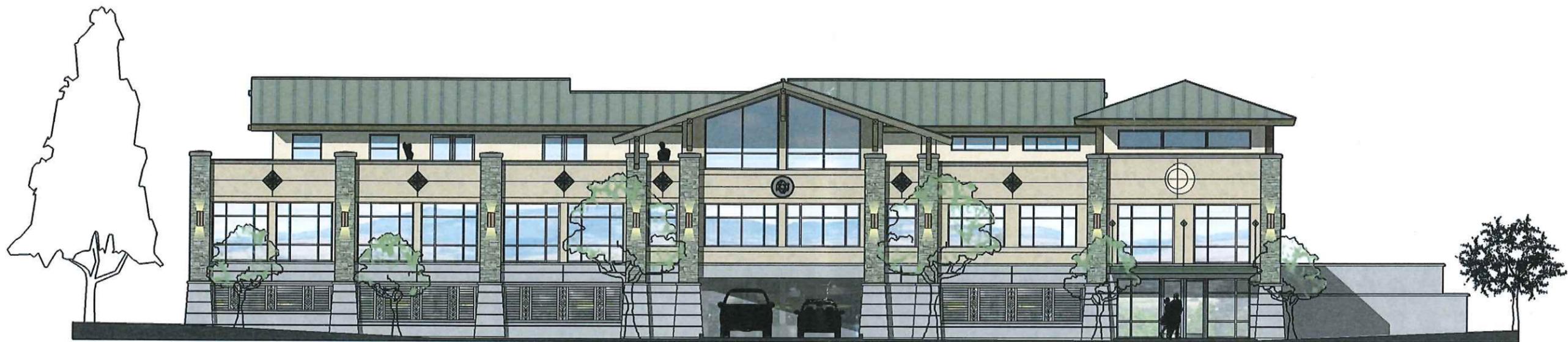
Please feel free to contact me if you have any questions or if you need any additional information.

Sincerely,

  
Kelly Sandow PE

# PROPOSED MAPLE STREET MEDICAL BUILDING

APPLICATION PROPOSAL



EAST CHESTNUT STREETScape CONCEPTUAL ELEVATION

NOT TO SCALE

RECEIVED

AUG 03 2017

City of Ashland

FINAL PLAN ONLY

# PROPOSED MAPLE STREET MEDICAL BUILDING



**REITINGER & ASSOCIATES INC.**  
 MEDICAL MANAGEMENT, SPACE PLANNING, &  
 HEALTHCARE DESIGN CONSULTANTS

360 EAST 9TH STREET #3  
 KETCHIKAN, OR 97540  
 PHONE: 503-753-2129  
 FAX: 503-753-5458

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PROJECT:

## MAPLE STREET MEDICAL BUILDING

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

### 10% SCHEMATICS

DRAWN BY: JMK	DATE: 09/09/16	SHEET SIZE
CHECKED BY: MLR	DATE: 10/21/16	ARCH D
NO.	REVISION	DATE BY
A	REV. PER PRE-APP COMMENTS	04/11/17 JMK
B	REV. PER PRE-APP COMMENTS 2	06/10/17 JMK

CAD FILE: G-001 Cover Sheet.dwg  
 PLOT DATE/TIME: 7/24/2017 - 2:48pm

SHEET TITLE:

COVER SHEET

PROJECT SUMMARY,  
 SITE USE SUMMARY,  
 INDEX OF DRAWINGS,  
 SITE DEMO PLAN,  
 VICINITY MAP,  
 PERSPECTIVE SKETCH

SHEET NUMBER:

G-001

### PROJECT SUMMARY

The Proposed medical building shall house several medical practices and two residential penthouse suites above medical portion of building that is in the current Healthcare zone. The total medical floor square feet shall not exceed 13,500 s.f. There are two proposed residential Penthouse units that would be a total of 2400 s.f. roof top units. The proposed Structure shall provide a subterranean parking garage which will provide the majority of the parking required by the use. The subterranean parking will day light with an entrance and exit to the parking structure off of Chestnut. A secondary entrance is necessary for assisted patient pick up and drop off, and shall be off of the North end of building on Maple Street on the clinic level. Service entrance, delivery, refuse, recycle, backup generator, shall be at rear of building accessed off of Maple Street on the West side of building at the existing curb cut.

Construction of building shall be a Type II-A fire sprinkled building. All levels shall be accessed either by elevator or by stairs as an alternative exit. On the Residential penthouse portion of upper level, a 20 to 30 percent of roof is proposed for an aesthetic outdoor living area for the two .....Penthouse suites. Remaining areas will house screened mechanical of the clinic from residential outdoor living and from visual site lines from the street levels.

The Intent for this proposed structure as it relates to the Healthcare zone and the Residential zone that it borders on, is to blend it in aesthetically with the transitional zoning that occurs at this site. Keeping the building height as low as possible as not to affect or over power uphill residential area views and stay within the 35' height limit. With this intent, we think it will be a welcome asset to the Healthcare community/residential neighborhood and a vast improvement to what is there now.

### SITE USE SUMMARY

<b>PROJECT:</b>	MAPLE STREET MEDICAL BUILDING	<b>PARKING REQUIREMENTS:</b>	1 SPACE PER 350 S.F. OF GROSS FLOOR AREA
<b>MAP NUMBER:</b>	39 1E 05 DB	Medical Parking Required:	38.57
<b>TAX LOT:</b>	2000	Residential Parking Required:	3.50
<b>LOT SIZE:</b>	0.63 OF AN ACRE = 27,443 SQ. FT.	Total Parking Required:	42.07
<b>PROJECT ADDRESS:</b>	330 MAPLE STREET	Parking Credits:	
<b>ZONING:</b>	HEALTH CARE (HC)	Bicycle Parking Credit:	-1.00
<b>OWNER:</b>	MAPLE LLC	Mixed Use Credit:	-3.50
<b>EST. GROSS MEDICAL SQ. FT.:</b>	13,500 GROSS SQ. FT.	Total Parking Credits:	-4.50
<b>MAX. LOT COVERAGE:</b>	65%	NET Required Parking:	38.00
<b>PROPOSED LOT COVERAGE:</b>	66% = ALL IMPERVIOUS SURFACE (VARIANCE REQUEST TO INCREASE)	Satisfied by:	
<b>REQUIRED SET BACKS:</b>	FRONT YARD 20 FT. SIDE YARD 6 FT. REAR YARD 10 FT. PER STORY SIDE YARD OF CORNER LOT 10 FT.	Off-Street Underground Parking:	
<b>MAX. HEIGHT OF BUILDING:</b>	35 FT.	Standard (9'x16)	14
		Compact (9'x16)	19
		ADA Standard	2
		ADA Van	1
		Off-Street Above ground Parking:	
		Parallel (7'x22)	3
		Total Off-Street Parking spaces provided:	39.00
		On-Street Parking (not applied above)	6.00

### DESIGN CRITERIA

**BUILDING CODES**  
 2014 OREGON STRUCTURAL SPECIALTY CODE (OSSC)  
 2014 OREGON ELECTRICAL SPECIALTY CODE (OESC)  
 2014 OREGON PLUMBING SPECIALTY CODE (OPSC)  
 2014 OREGON MECHANICAL SPECIALTY CODE (OMSC)

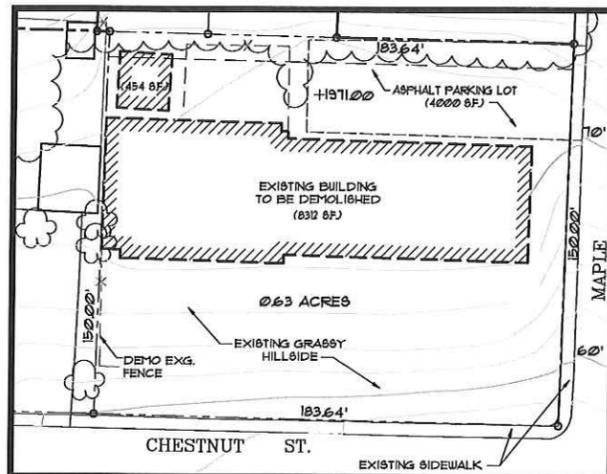
**OCCUPANCY CLASS:**  
 GROUP B, BUSINESS (PROF. SERVICES PHYSICIAN)

**CONSTRUCTION TYPE:**  
 TYPE II-A

**CONSTRUCTION MATERIALS PROPOSED:**  
 POST & TENSION STRUCTURAL CONCRETE PARKING STRUCTURE  
 LOWER LEVEL

### INDEX OF DRAWINGS

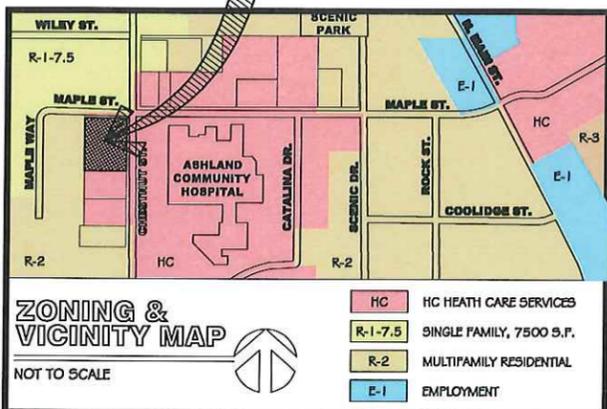
G-001	PROJECT SUMMARY, SITE USE SUMMARY, DESIGN CRITERIA, INDEX OF DRAWINGS, SITE DEMO PLAN, ZONING & VICINITY MAP, EXTERIOR CONCEPT PERSPECTIVE
A-201	PROPOSED EAST EXTERIOR ELEVATION & MATERIAL OPTIONS
A-202	PROPOSED NORTH EXTERIOR ELEVATION & MATERIAL OPTIONS
A-203	PROPOSED WEST EXTERIOR ELEVATION & MATERIAL OPTIONS
A-204	PROPOSED SOUTH EXTERIOR ELEVATION & MATERIAL OPTIONS
V-001	EXISTING SURVEY PLAN
C-001	PROPOSED PARKING PLAN
L-001	PROPOSED LANDSCAPING PLAN



### SITE DEMO PLAN

1/32" = 1'-0"

### PROJECT LOCATION



### VICINITY & ZONING MAP

NOT TO SCALE



### EXTERIOR PERSPECTIVE CONCEPT

NOT TO SCALE

RECEIVED  
 AUG 03 2017  
 City of Ashland

CONCEPTUAL PLAN ONLY



**REITINGER & ASSOCIATES INC.**  
 MEDICAL MANAGEMENT, SPACE PLANNING, &  
 HEALTHCARE DESIGN CONSULTANTS

353 EAST 5TH STREET #3  
 ASTORIA, OR 97103  
 PHONE: 208-728-2237  
 FAX: 208-728-9456

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PROJECT:

**MAPLE STREET  
 MEDICAL  
 BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

10% SCHEMATICS

NO.	REVISION	DATE	BY

CAD FILE: Sht\_02-Exterior Elevation Height Analysis.dwg  
 PLOT DATE/TIME: 7/24/2017 - 1:35pm

SHEET TITLE:

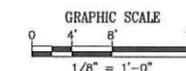
**ARCHITECTURAL**  
 PROPOSED EAST EXTERIOR  
 ELEVATION & MATERIAL OPTIONS  
 BUILDING HEIGHT ANALYSIS

SHEET NUMBER:

**A-201**



EAST ELEVATION (CHESTNUT STREETSCAPE)



**CONCEPTUAL  
 PLAN ONLY!**

**BUILDING HEIGHT ANALYSIS**

BUILDING HEIGHT=  
 $(H1+H2+H3+H4)$   
 $\frac{4}{4}$

MAXIMUM BUILDING HT. FOR ZONE IS 35'

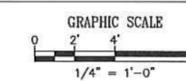
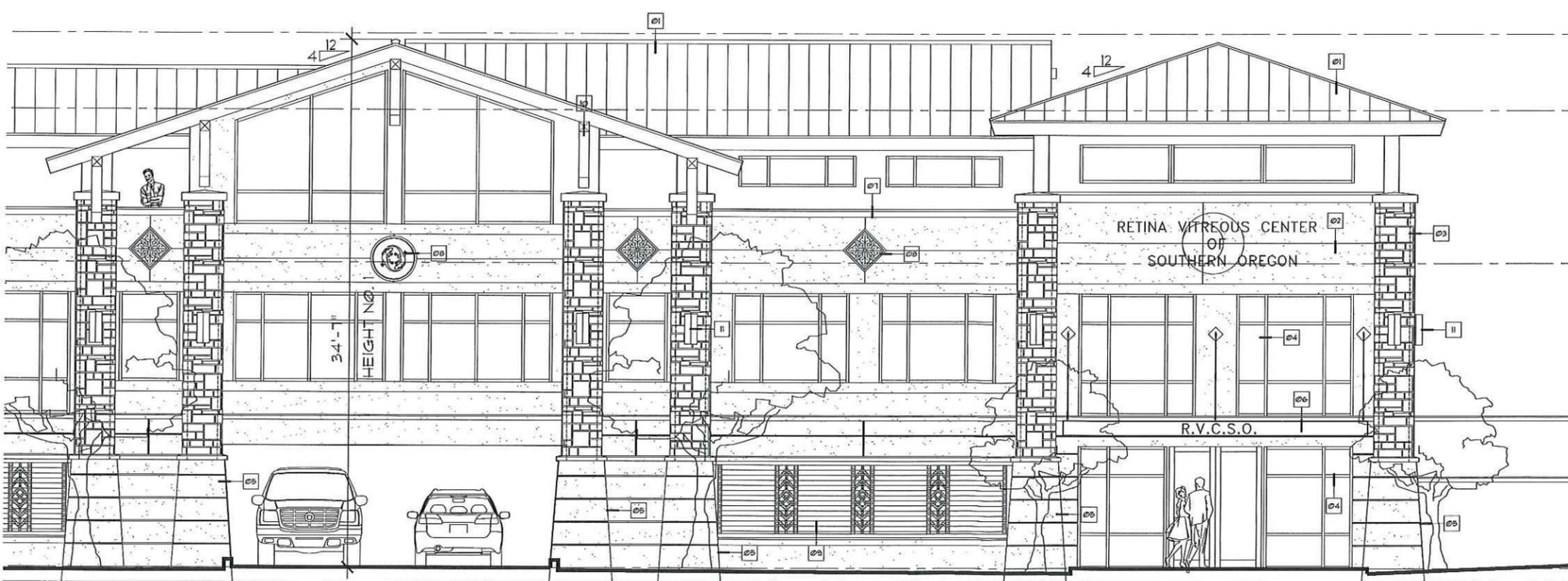
H1 = 34' - 7"  
 H2 = 29' - 6"  
 H3 = 24' - 7"  
 H4 = 26' - 4"

SUM OF H1 + H2 + H3 + H4 = 115  
 $\frac{4}{4}$  = 28' - 9" MEDIAN HT.

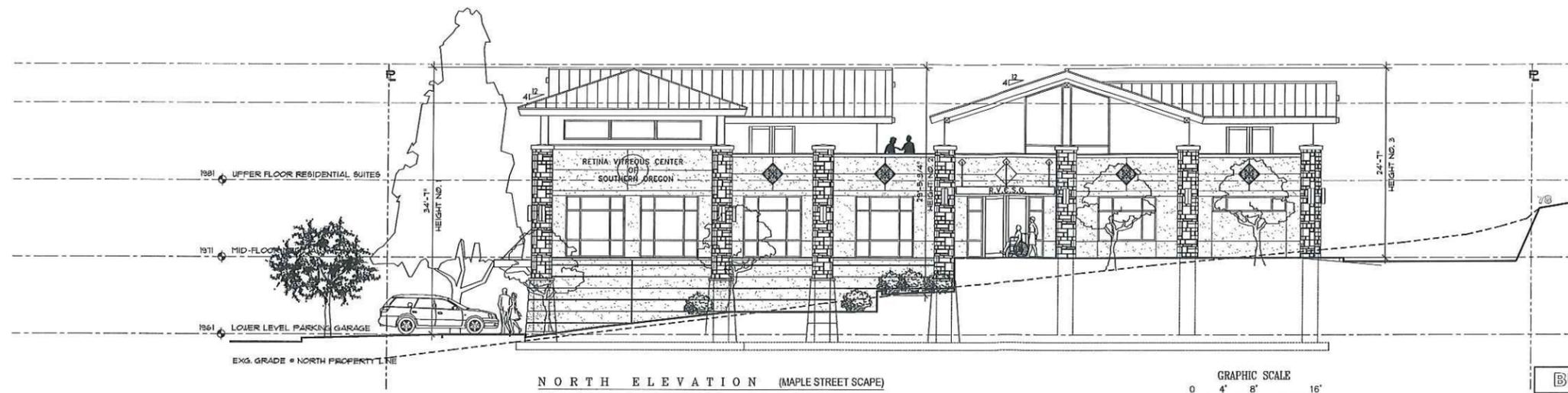
NOTE: THESE HEIGHTS HAVE BEEN DETERMINED BEFORE STRUCTURAL ENGINEERING AND WILL PROBABLY INCREASE, BUT WILL STILL REMAIN BELOW MAXIMUM HT.

**MATERIAL KEYNOTES**

- 01 RAISED SEAM METAL ROOFING
- 02 STUCCO EXTERIOR FINISH W/ CONTROL JOINTS
- 03 MASONRY VENEER OF: BRICK, STONE OR CULTURED STONE
- 04 ALUMINUM STOREFRONT WINDOWS & ENTRY SYSTEMS
- 05 PRE-FORMED/CAST STRUCTURAL COLUMNS FOR POST & TENSION SLAB AT STREETSCAPE ELEVATIONS ONLY
- 06 STEEL ENTRY AWNING W/ ANCHOR STAYS
- 07 PRE-CAST CONCRETE CAPS
- 08 PINEAPPLE GROVE CONC. DECORATIVE MEDALIONS
- 09 GALVANIZED DECORATIVE & SECURITY SCREEN/CABLING FOR PARKING GARAGE
- 10 POWDER COATED STEEL STRUCTURAL KNEE BRACES
- 11 EXTERIOR ACCENT LIGHTING SCENCE



**RECEIVED**  
**AUG 03 2017**  
 City of Ashland



- MATERIAL KEYNOTES**
- 01 RAISED SEAM METAL ROOFING
  - 02 STUCCO EXTERIOR FINISH W/ CONTROL JOINTS
  - 03 MASONRY VENEER OF: BRICK, STONE OR CULTURED STONE
  - 04 ALUMINUM STOREFRONT WINDOWS & ENTRY SYSTEMS
  - 05 PRE-FORMED/CAST STRUCTURAL COLUMNS FOR POST & TENSION SLAB AT STREETScape ELEVATIONS ONLY
  - 06 STEEL ENTRY AWNING W/ ANCHOR STAYS
  - 07 PRE-CAST CONCRETE CAPS
  - 08 PINEAPPLE GROVE CONC. DECORATIVE MEDALIONS
  - 09 CAST IN PLACE CONCRETE COLUMN CAPS
  - 10 GLU-LAM ROOF PURLINS @ RESIDENTIAL GABLE ENDS
  - 11 EXTERIOR ACCENT LIGHTING WALL SCENCE

**BUILDING HEIGHT ANALYSIS**

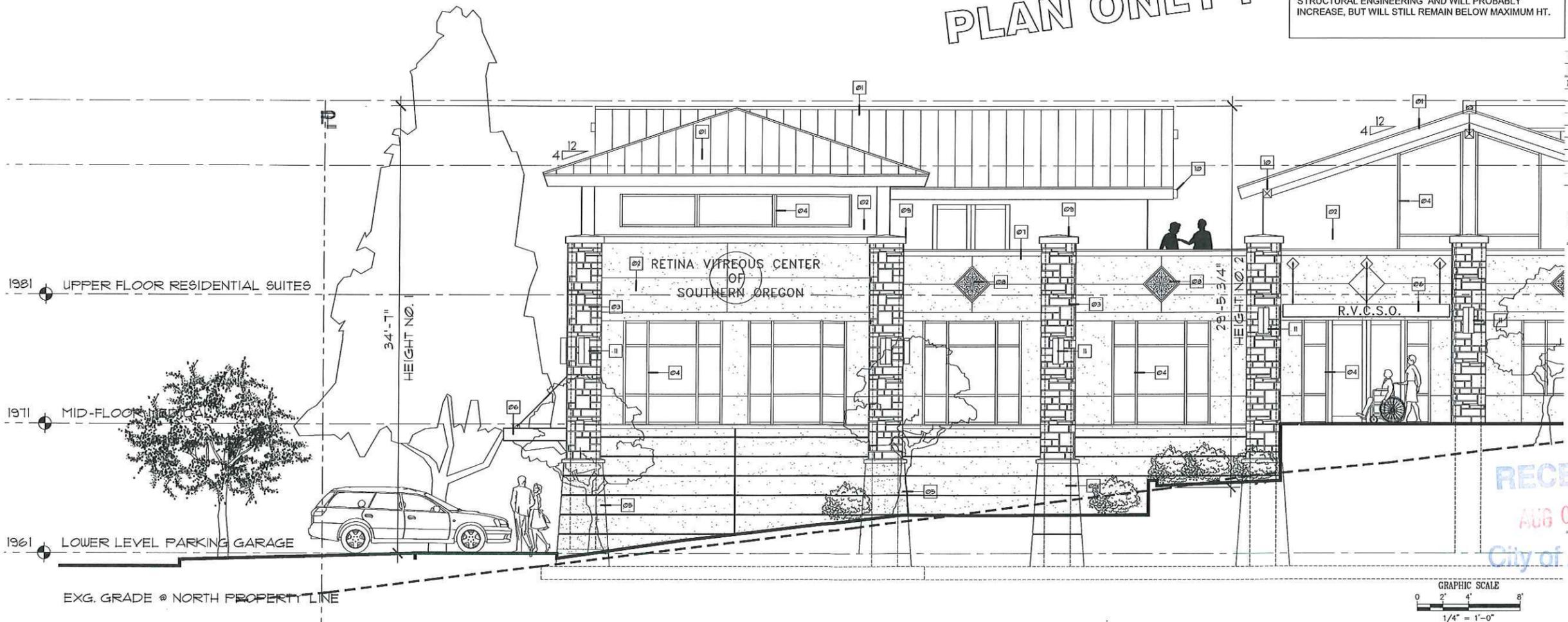
BUILDING HEIGHT=  
 $\frac{(H1+H2+H3+H4)}{4}$

MAXIMUM BUILDING HT. FOR ZONE IS 35'

H1 = 34' - 7"  
 H2 = 29' - 6"  
 H3 = 24' - 7"  
 H4 = 26' - 4"  
 SUM OF H1 + H2 + H3 + H4 = 115  
 $\frac{115}{4} = 28' - 9"$  MEDIAN HT.

NOTE: THESE HEIGHTS HAVE BEEN DETERMINED BEFORE STRUCTURAL ENGINEERING AND WILL PROBABLY INCREASE, BUT WILL STILL REMAIN BELOW MAXIMUM HT.

CONCEPTUAL PLAN ONLY!



**REITINGER & ASSOCIATES INC.**  
 MEDICAL, MANUFACTURING, SPACE PLANNING, &  
 HEALTH CARE DESIGN CONSULTANTS

353 EAST 9TH STREET #3  
 ASTORIA, OR 97103  
 PHONE: 325-728-2237  
 FAX: 325-728-5355

**OWNERSHIP OF DOCUMENTS:**  
 The Architect is a professional corporation and its employees are not authorized to execute or sign any documents on behalf of the Architect or its employees without the written authorization of the Architect.

PROJECT:

**MAPLE STREET  
 MEDICAL  
 BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

10% SCHEMATICS

NO.	REVISION	DATE	BY

CAD FILE: Sht 02-Exterior Elevation Height Analysis.dwg  
 PLOT DATE/TIME: 7/24/2017 - 1:37pm

SHEET TITLE:

ARCHITECTURAL

PROPOSED NORTH EXTERIOR  
 ELEVATION & MATERIAL  
 OPTIONS

BUILDING HEIGHT ANALYSIS

RECEIVED  
 AUG 03 2017  
 City of Ashland

SHEET NUMBER:

A-202



**REITINGER & ASSOCIATES INC.**  
 ARCHITECTURAL, INTERIOR DESIGN, PLANNING, &  
 HEALTHCARE DESIGN CONSULTANTS

303 EAST 5TH STREET #3  
 ASTORIA, OR 97103  
 PHONE: 503-726-2237  
 FAX: 503-726-5155

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PROJECT:

**MAPLE STREET  
 MEDICAL  
 BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

10% SCHEMATICS

NO.	REVISION	DATE	BY

CAD FILE: Sht 02 - Exterior Elevation Height Analysis.dwg  
 PLOT DATE/TIME: 7/24/2017 - 1:41pm

SHEET TITLE:

ARCHITECTURAL

PROPOSED WEST EXTERIOR  
 ELEVATION & MATERIAL OPTIONS

RECEIVED  
 AUG 03 2017  
 City of Ashland

SHEET NUMBER:

A-203

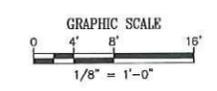
**KEYNOTES**

- 01 RAISED SEAM METAL ROOFING
- 02 STUCCO EXTERIOR FINISH W/ CONTROL JOINTS
- 03 MASONRY VENEER OF: BRICK, STONE OR CULTURED STONE
- 04 ALUMINUM STOREFRONT WINDOWS & ENTRY SYSTEMS
- 05 PRE-FORMED/CAST STRUCTURAL COLUMNS FOR POST & TENSION SLAB AT STREETScape ELEVATIONS ONLY
- 06 STEEL ENTRY AWNING W/ ANCHOR STAYS
- 07 PRE-CAST CONCRETE CAPS
- 08 PINEAPPLE GROVE CONC. DECORATIVE MEDALIONS
- 09 FORMED/CAST IN PLACE CONCRETE COLUMN CAPS
- 10 GLU-LAM STRUCTURAL ROOF PURLINS @ RESIDENTIAL GABLED ENDS
- 11 EXTERIOR ACCENT LIGHTING WALL SCENCE

**BUILDING HEIGHT ANALYSIS**

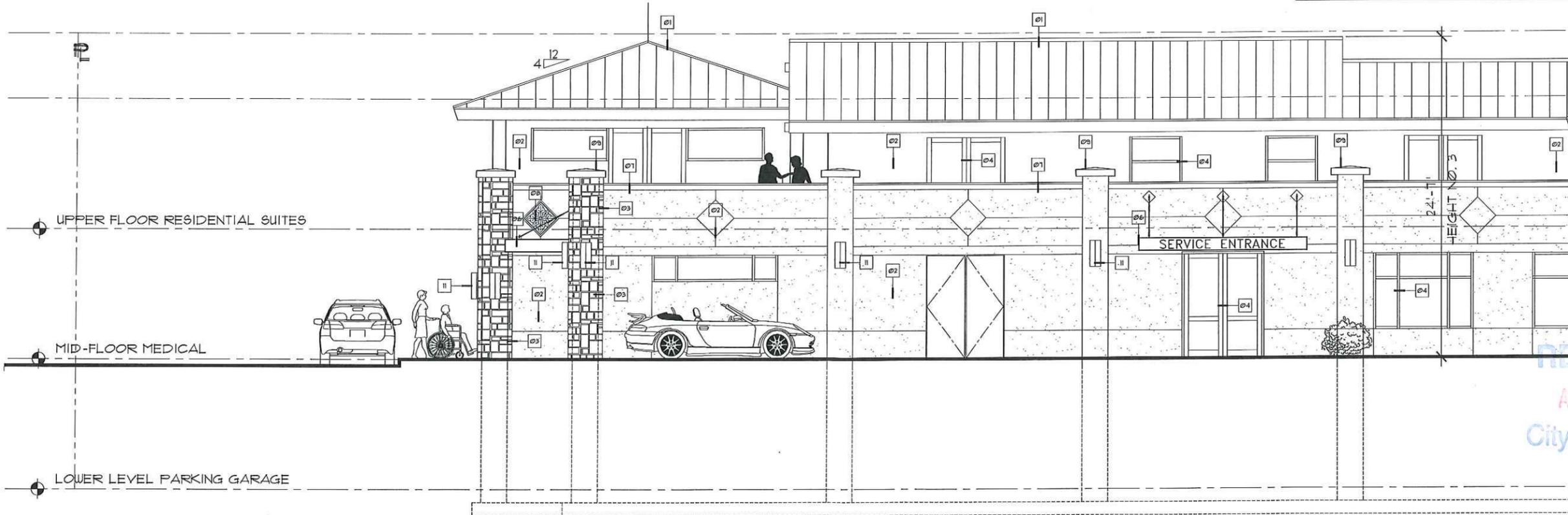
BUILDING HEIGHT=  
 (H1+H2+H3+H4)  
 4  
 MAXIMUM BUILDING HT. FOR ZONE IS 35'  
 H1 = 34' - 7"  
 H2 = 29' - 6"  
 H3 = 24' - 7"  
 H4 = 26' - 4"  
 SUM OF H1 + H2 + H3 + H4 = 115  
 4 = 28' - 9" MEDIAN HT.

NOTE: THESE HEIGHTS HAVE BEEN DETERMINED BEFORE STRUCTURAL ENGINEERING AND WILL PROBABLY INCREASE, BUT WILL STILL REMAIN BELOW MAXIMUM HT.

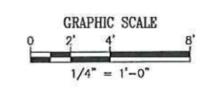


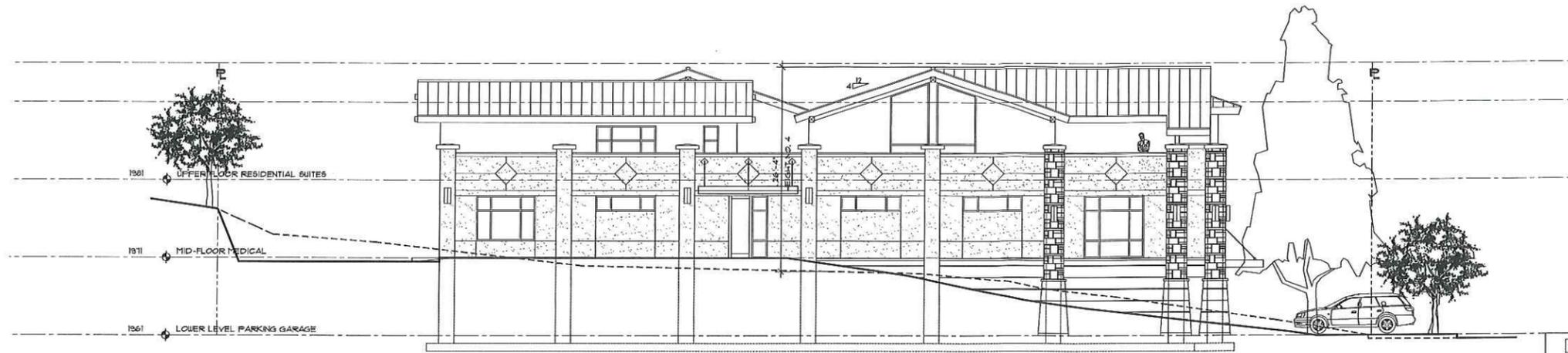
WEST ELEVATION (REAR OF BUILDING)

**CONCEPTUAL  
 PLAN ONLY!**



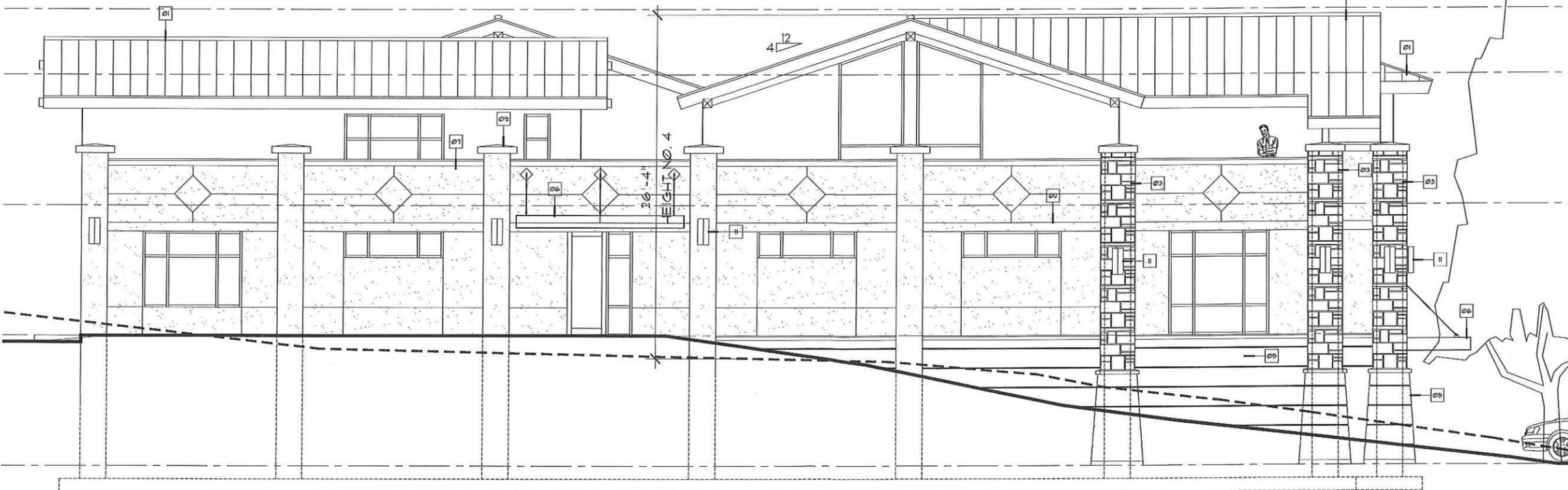
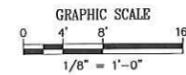
WEST ELEVATION (REAR OF BUILDING)



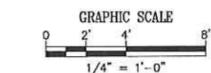


SOUTH ELEVATION

CONCEPTUAL  
PLAN ONLY!



SOUTH ELEVATION



KEYNOTES

- 01 RAISED SEAM METAL ROOFING
- 02 STUCCO EXTERIOR FINISH W/ CONTROL JOINTS
- 03 MASONRY VENEER OF: BRICK, STONE OR CULTURED STONE
- 04 ALUMINUM STOREFRONT WINDOWS & ENTRY SYSTEMS
- 05 PRE-FORMED/CAST STRUCTURAL COLUMNS FOR POST & TENSION SLAB AT STREETSCAPE ELEVATIONS ONLY
- 06 STEEL ENTRY AWNING W/ ANCHOR STAYS
- 07 PRE-CAST CONCRETE CAPS
- 08 PINEAPPLE GROVE CONC. DECORATIVE MEDALIONS
- 09 GALVANIZED DECORATIVE & SECURITY SCREEN/CABLING FOR PARKING GARAGE
- 10 GLU-LAM STRUCTURAL GABLED END ROOF PURLINS
- 11 EXTERIOR ACCENT LIGHTING WALL SCUNCES

BUILDING HEIGHT ANALYSIS

BUILDING HEIGHT=  
(H1+H2+H3+H4)  
4  
MAXIMUM BUILDING HT. FOR ZONE IS 35'

H1 = 34' - 7"  
H2 = 29' - 6"  
H3 = 24' - 7"  
H4 = 26' - 4"  
SUM OF H1 + H2 + H3 + H4 = 115  
4 = 28' - 9" MEDIAN HT.

NOTE: THESE HEIGHTS HAVE BEEN DETERMINED BEFORE STRUCTURAL ENGINEERING AND WILL PROBABLY INCREASE, BUT WILL STILL REMAIN BELOW MAXIMUM HT.



REITINGER & ASSOCIATES INC.

363 EAST 9TH STREET #3  
VICTORIA, B.C. V8S 4G9  
PHONE: 250-726-2020  
FAX: 250-726-8456

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PROJECT:

MAPLE STREET  
MEDICAL  
BUILDING

330 MAPLE STREET  
ASHLAND, OR 97520  
JACKSON COUNTY

DRAWING INFORMATION:

10% SCHEMATICS

NO.	REVISION	DATE	BY

CAD FILE: SM\_02-Exterior Elevation Height Analysis.dwg  
PLOT DATE/TIME: 7/24/2017 - 1:49pm

SHEET TITLE:

ARCHITECTURAL

PROPOSED SOUTH EXTERIOR  
ELEVATION & MATERIAL OPTIONS

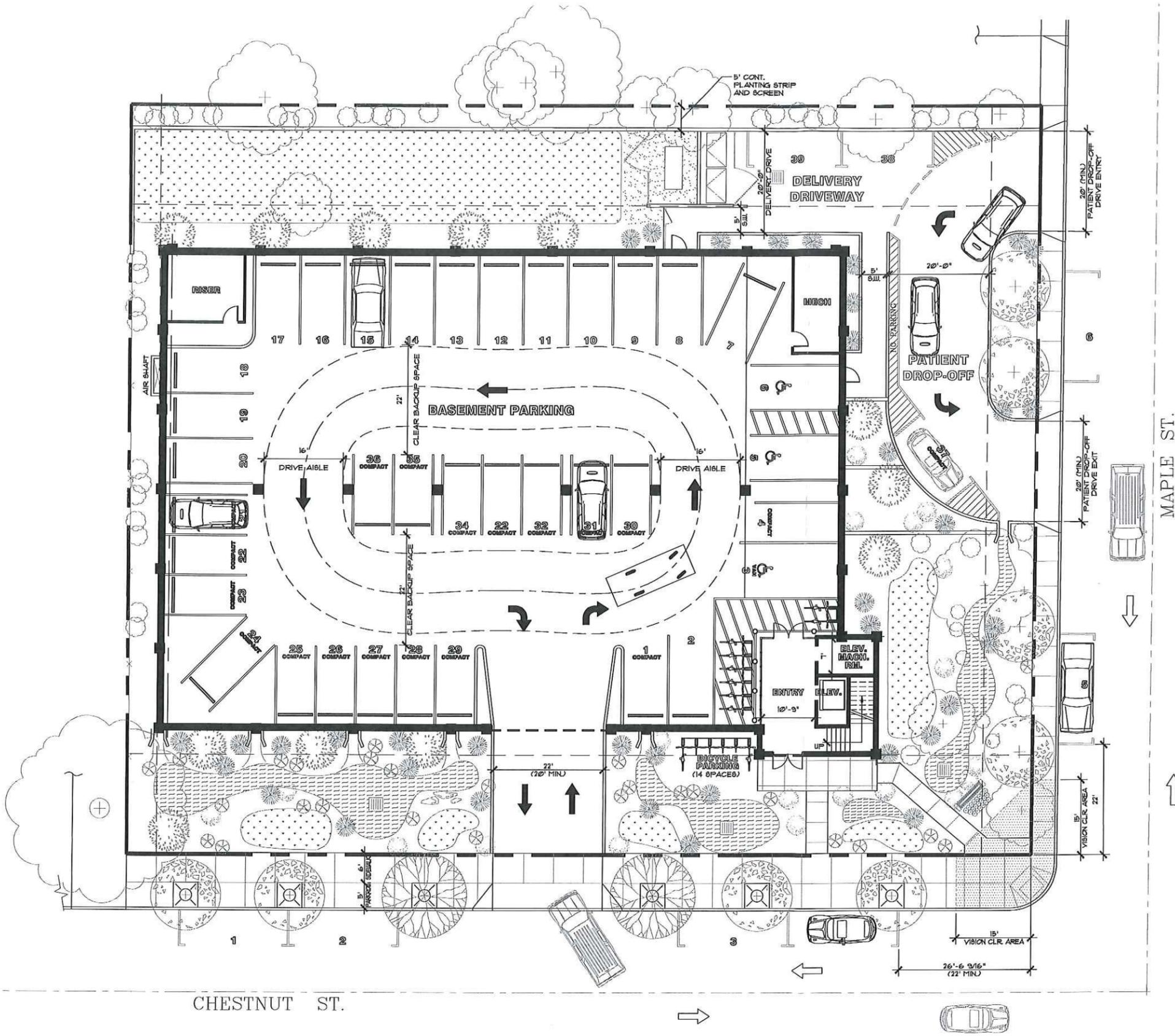
HEIGHT ANALYSIS



SHEET NUMBER:

A-204





PARKING SUMMARY	
Medical Parking Required:	38.57
Residential Parking Required:	3.50
Total Parking Required:	42.07
Parking Credits:	
Bicycle Parking Credit	-1.00
Mixed Use Credit	-3.50
Total Parking Credits	-4.50
<b>NET Required Parking</b>	<b>38.00</b>
Satisfied by:	
Off-Street Underground Parking:	
Standard (9'x18')	14
Compact (8'x16')	19
ADA Standard	2
ADA Van	1
Off-Street Above ground Parking:	
Parallel (7'x22')	3
<b>Total Off-Street Parking spaces provided:</b>	<b>39.00</b>
On-Street Parking (not applied above)	6.00



**REITINGER & ASSOCIATES INC.**  
 MEDICAL MANAGEMENT, SPACE PLANNING, &  
 HEALTHCARE DESIGN CONSULTANTS

300 EAST 9TH STREET #3  
 KETCHIKAN, ID 83340  
 PHONE: 208-728-2237  
 FAX: 208-728-5456

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PROJECT:

**MAPLE STREET  
 MEDICAL  
 BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

**10% SCHEMATICS**

NO.	REVISION	DATE	BY
A	REV. PER PRE-APP COMMENTS	04/11/17	JJK
B	REV. PER PRE-APP COMMENTS 2	06/13/17	JJK

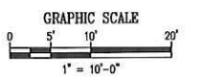
CAD FILE: C-001 Parking Plan.dwg  
 PLOT DATE/TIME: 7/24/2017 - 3:03pm

SHEET TITLE:

CIVIL PLAN

PROPOSED  
 PARKING PLAN

RECEIVED  
 AUG 03 2017  
 City of Ashland

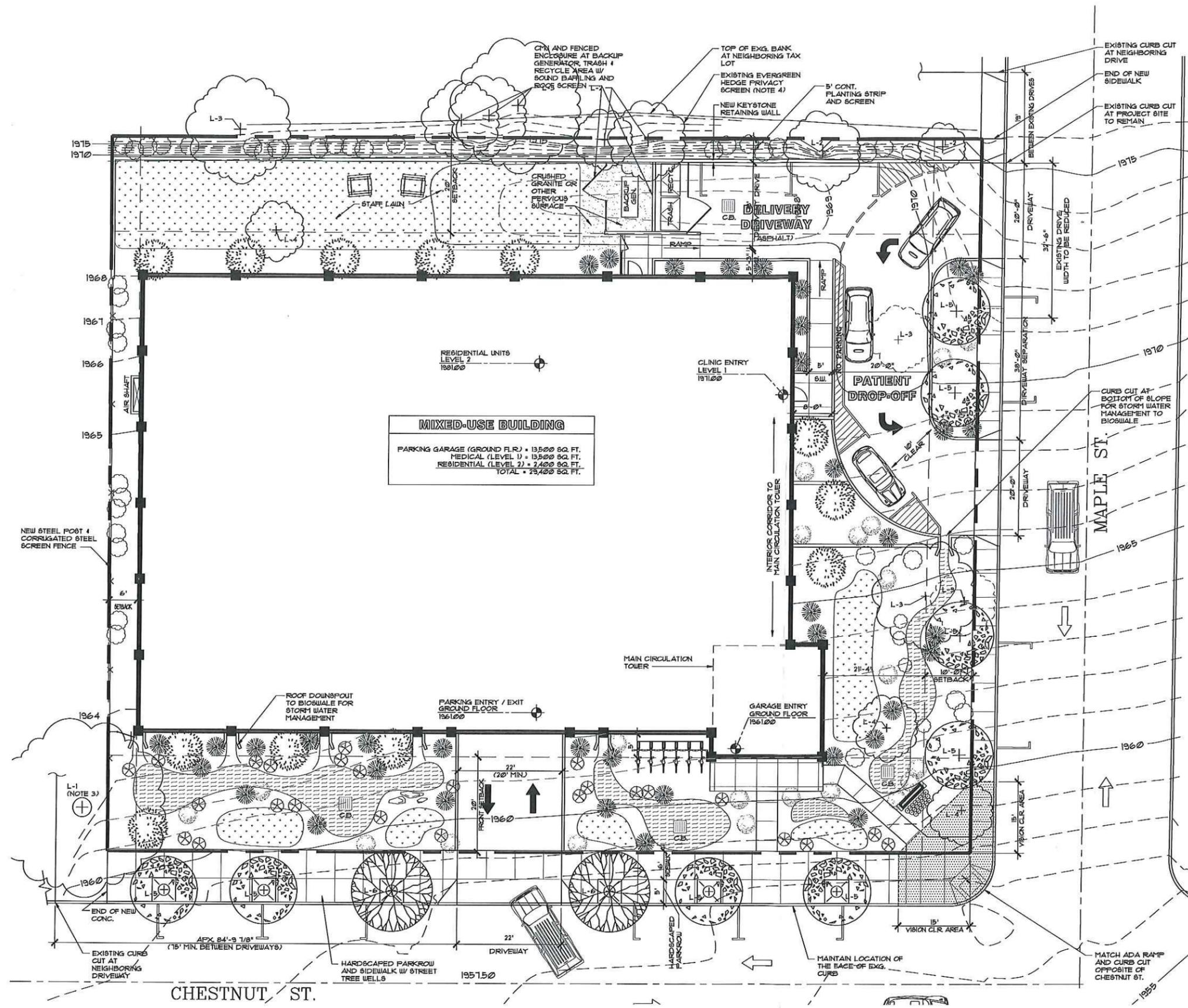


SHEET NUMBER:

C-001

**PROPOSED SITE PARKING PLAN**  
 1" = 10'-0"

**CONCEPTUAL PLAN ONLY**



**MIXED-USE BUILDING**  
 PARKING GARAGE (GROUND FLR) = 13,500 SQ. FT.  
 MEDICAL (LEVEL 1) = 13,500 SQ. FT.  
 RESIDENTIAL (LEVEL 2) = 2,400 SQ. FT.  
 TOTAL = 29,400 SQ. FT.

**GENERAL NOTES**

- NORTH PROPERTY LINE VEGETATION PROPOSAL:** TREE PRESERVATION AND PROTECTION IS PROPOSED FOR TWO EXISTING TREES ON THE NORTH EAST CORNER. THE ADDITION AND MODIFICATION OF THE REQUIRED PARKROW, SIDEWALK, AND CIRCULAR DRIVE WILL REQUIRE REMOVAL OF SOME EXISTING TREES. NEW STREET TREES ARE PROPOSED WITHIN THE NEW PARKROW.
- EAST PROPERTY LINE VEGETATION PROPOSAL:**
  - ALL NEW STREET TREES ON THE EAST PROPERTY LINE SHALL BE PLANTED IN STREET TREE WELLS AT THE HARDSCAPED PARKROW.
  - A HARDSCAPED PARKROW IS PROPOSED FOR EASIER PEDESTRIAN PASSAGE TO AND FROM SITE.
- SOUTH PROPERTY LINE VEGETATION PROPOSAL:** TREE PRESERVATION AND PROTECTION WILL BE PROVIDED FOR THE LARGE EVERGREEN CEDAR ON THE NEIGHBORING PROPERTY TO THE SOUTH.
- WEST PROPERTY LINE VEGETATION PROPOSAL:**
  - A 5-FOOT CONTINUOUS PLANTING STRIP AND FENCE SHALL PROVIDE SCREENING FROM ADJOINING RESIDENTIAL LOTS TO THE WEST.
  - ANY UNHEALTHY SHRUBS WITHIN THE CONTINUOUS PLANTING STRIP, AND WITHIN THE PROPERTY LINES, MAY BE REMOVED AND NEW PLANTINGS SHALL BE INTRODUCED TO PROVIDE CONTINUOUS SCREENING FROM DELIVERY DRIVE AND REFUSE AREA.

**LOT COVERAGE**

1. TOTAL ACRES TO BE DEVELOPED *	0.63 AC
2. LOT COVERAGE BY SQUARE FOOTAGE	
IMPERVIOUS BUILDING (GROSS AT GRADE)	13,500
TRASH REFUSE CIRCULATION (DRIVES + WALKWAYS)	3,885
IMPERVIOUS SUBTOTAL (SF) *	17,385
PERVIOUS STAFF LAWN LANDSCAPING	1,025
PERVIOUS SUBTOTAL (SF) *	8,115
<b>TOTAL (SF) *</b>	<b>25,500</b>

3. LOT COVERAGE BY PERCENTAGE	
IMPERVIOUS BUILDING (GROSS AT GRADE)	49%
TRASH REFUSE CIRCULATION (DRIVES + WALKWAYS)	14%
IMPERVIOUS SUBTOTAL (SF) *	64%
PERVIOUS STAFF COURTYARD LANDSCAPING	6%
PERVIOUS SUBTOTAL (SF) *	30%
<b>TOTAL *</b>	<b>100%</b>

4. **SITE SETBACKS**
- FRONT SETBACK = 20 FT
  - SIDE SETBACK (CORNER LOT) = 10 FT
  - SIDE SETBACK (INTERIOR) = 5 FT
  - REAR (MULTISTORY, 10' PER LEVEL) = 20 FT

**PLANTING LEGEND**

- EXISTING TREE TO REMAIN UNDISTURBED.
- EXISTING TREE TO BE REMOVED OR SALVAGED.
- NEW TREE (SPECIES TO BE SELECTED FROM CITY OF ASHLAND'S RECOMMENDED STREET TREE GUIDE)
- PERENNIAL SHRUBS, T.B.D.
- ANNUALS, T.B.D.
- SPRINKLERED LAWN
- BIOSWALE

**TREE LEGEND**

- L-1 EXG. EVERGREEN DRIP LINE: 30'-35'
- L-2 EXG. EVERGREEN DRIP LINE: 8'-10'
- L-3 EXG. EVERGREEN DRIP LINE: 15'-25'
- L-4 EXG. DECIDUOUS DRIP LINE: 15'-20'
- L-5 NEW DECIDUOUS DRIP LINE: 10'-15'
- L-6 NEW DECIDUOUS DRIP LINE: 20'-25'



**REITINGER & ASSOCIATES INC.**  
 MEDICAL, MANAGEMENT, SPACE PLANNING, & HEALTHCARE DESIGN CONSULTANTS

300 EAST 9TH STREET #3  
 KEVASKA, OR 97330  
 PHONE 503-728-2237  
 FAX 503-728-5458

PROJECT:

**MAPLE STREET MEDICAL BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

DRAWING INFORMATION:

**10% SCHEMATICS**

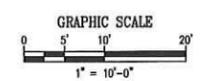
DRAWN BY: JJK	DATE: 03/11/16	SHEET SIZE: ARCH D	
CHECKED BY: MLR	DATE: 10/21/16		
NO.	REVISION	DATE	BY
A	REV. PER PRE-APP COMMENTS	04/11/17	JJK
B	REV. PER PRE-APP COMMENTS 2	06/10/17	JJK

CAD FILE: L-001 Landscaping Plan.dwg  
 PLOT DATE/TIME: 7/12/2017 - 9:45am

SHEET TITLE:

**LANDSCAPING PLAN**

RECEIVED  
 PROPOSED  
 LANDSCAPING PLAN  
 AUG 03 2017  
 City of Ashland



SHEET NUMBER:

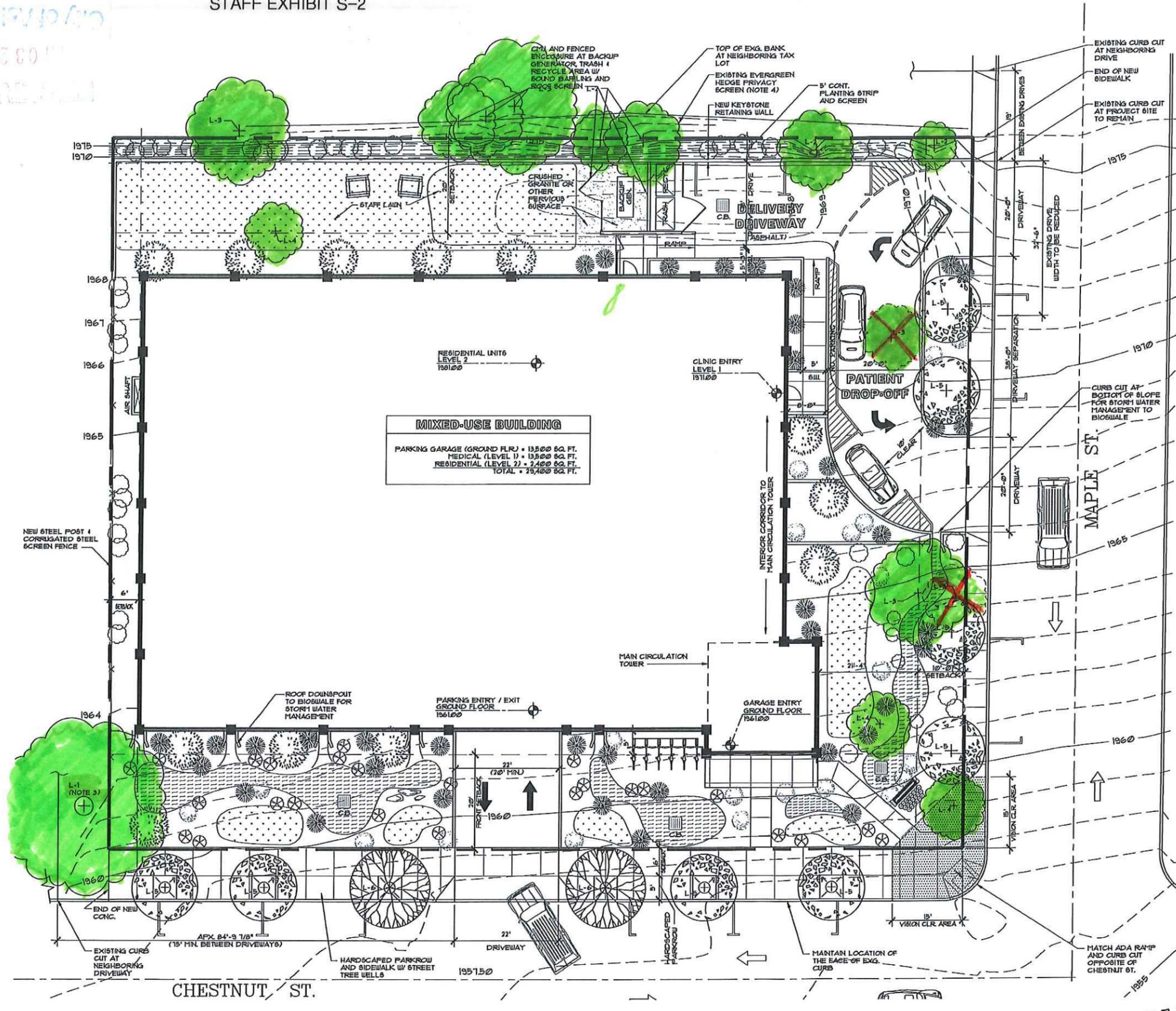
L-001

**PROPOSED LANDSCAPING PLAN**

1" = 10'-0"

**CONCEPTUAL PLAN ONLY**





**MIXED-USE BUILDING**  
 PARKING GARAGE (GROUND FLR) • 13,500 SQ. FT.  
 MEDICAL (LEVEL 1) • 13,500 SQ. FT.  
 RESIDENTIAL (LEVEL 2) • 7,400 SQ. FT.  
 TOTAL • 34,400 SQ. FT.

**GENERAL NOTES**

- NORTH PROPERTY LINE VEGETATION PROPOSAL:** TREE PRESERVATION AND PROTECTION IS PROPOSED FOR TWO EXISTING TREES ON THE NORTH EAST CORNER. THE ADDITION AND MODIFICATION OF THE REQUIRED PARKROW, SIDEWALK, AND CIRCULAR DRIVE WILL REQUIRE REMOVAL OF SOME EXISTING TREES. NEW STREET TREES ARE PROPOSED WITHIN THE NEW PARKROW.
- EAST PROPERTY LINE VEGETATION PROPOSAL:**
  - ALL NEW STREET TREES ON THE EAST PROPERTY LINE SHALL BE PLANTED IN STREET TREE WELLS AT THE HARDSCAPED PARKROW.
  - A HARDSCAPED PARKROW IS PROPOSED FOR EASIER PEDESTRIAN PASSAGE TO AND FROM SITE.
- SOUTH PROPERTY LINE VEGETATION PROPOSAL:** TREE PRESERVATION AND PROTECTION WILL BE PROVIDED FOR THE LARGE EVERGREEN CEDAR ON THE NEIGHBORING PROPERTY TO THE SOUTH.
- WEST PROPERTY LINE VEGETATION PROPOSAL:**
  - A 5-FOOT CONTINUOUS PLANTING STRIP AND FENCE SHALL PROVIDE SCREENING FROM ADJOINING RESIDENTIAL LOTS TO THE WEST.
  - ANY UNHEALTHY SHRUBS WITHIN THE CONTINUOUS PLANTING STRIP, AND WITHIN THE PROPERTY LINES, MAY BE REMOVED AND NEW PLANTINGS SHALL BE INTRODUCED TO PROVIDE CONTINUOUS SCREENING FROM DELIVERY DRIVE AND REFUSE AREA.

**LOT COVERAGE**

1. TOTAL ACRES TO BE DEVELOPED •	0.63 AC
<b>2. LOT COVERAGE BY SQUARE FOOTAGE</b>	
PERVIOUS BUILDING (GROSS AT GRADE)	• 13,500
TRASH REFUSE	• 10
CIRCULATION (DRIVES & WALKWAYS)	• 3,893
PERVIOUS SUBTOTAL (SF) •	17,403
PERVIOUS STAFF LAWN LANDSCAPING	• 1025
	• 815
PERVIOUS SUBTOTAL (SF) •	2,844
<b>TOTAL (SF) •</b>	<b>21,443</b>
<b>3. LOT COVERAGE BY PERCENTAGE</b>	
PERVIOUS BUILDING (GROSS AT GRADE)	• 43%
TRASH REFUSE	• 1%
CIRCULATION (DRIVES & WALKWAYS)	• 14%
PERVIOUS SUBTOTAL (SF) •	64%
PERVIOUS STAFF COURTYARD LANDSCAPING	• 6%
	• 30%
PERVIOUS SUBTOTAL (SF) •	36%
<b>TOTAL •</b>	<b>100%</b>
<b>4. SITE SETBACKS</b>	
FRONT SETBACK •	20 FT
SIDE SETBACK (CORNER LOT) •	10 FT
SIDE SETBACK (INTERIOR) •	6 FT
REAR (MULTISTORY, 10' PER LEVEL) •	20 FT

**PLANTING LEGEND**

- EXISTING TREE TO REMAIN UNDISTURBED.
- EXISTING TREE TO BE REMOVED OR SALVAGED.
- NEW TREE (SPECIES TO BE SELECTED FROM CITY OF ASHLAND'S RECOMMENDED STREET TREE GUIDE.)
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- SPRINKLERED LAWN
- BIOSWALE

**TREE LEGEND**

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- L-5 NEW DECIDUOUS DRIP LINE: 10-15'
- L-6 NEW DECIDUOUS DRIP LINE: 20-25'

350 EAST 8TH STREET #3  
 ASHLAND, OR 97130  
 PHONE: 503-755-2327  
 FAX: 503-755-5438

**PROJECT:**

**MAPLE STREET MEDICAL BUILDING**

330 MAPLE STREET  
 ASHLAND, OR 97520  
 JACKSON COUNTY

**DRAWING INFORMATION:**

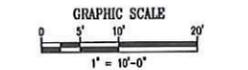
**10% SCHEMATICS**

DRAWN BY: JPK	DATE: 02/11/16	SHEET SIZE: ARCH D	
CHECKED BY: MLR	DATE: 10/21/16	ARCH D	
NO.	REVISION	DATE	BY
A	REV. PER PRE-APP COMMENTS	04/11/17	JPK
B	REV. PER PRE-APP COMMENTS 2	06/02/17	JPK

CAD FILE: L-001 Landscaping Plan.dwg  
 PLOT DATE/TIME: 7/12/2017 - 9:45am

**SHEET TITLE:**  
**LANDSCAPING PLAN**

**PROPOSED LANDSCAPING PLAN**



**SHEET NUMBER:**  
**L-001**

**PROPOSED LANDSCAPING PLAN**  
 1" = 10'-0"

**CONCEPTUAL PLAN ONLY**