

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
JANUARY 10, 2011  
AGENDA**

**I. CALL TO ORDER**

**II. ANNOUNCEMENTS**

**III. CONSENT AGENDA**

**A. Approval of Minutes**

1. December 13, 2011 Regular Meeting

**IV. PUBLIC FORUM**

**V. TYPE II PUBLIC HEARINGS**

**A. PLANNING ACTION: #2011-01576**

**SUBJECT PROPERTY: 1554 Webster Street** (*on the Southern Oregon University campus*)

**APPLICANT: American Campus Community Services**

**DESCRIPTION: A request for Site Review approval to construct a new single-story dining hall near the intersection of Wightman and Webster Streets, two new four-story residence halls near the intersection of Webster and Stadium Streets, two parking lots and associated site improvements on the Southern Oregon University campus at 1554 Webster Street. Also included are requests for Conditional Use Permit approval to allow buildings that exceed the maximum length and vary from the locations identified in the SOU Masterplan and to exceed the 40 foot height allowance in the SO zoning district, and a request for a Tree Removal Permit to remove 24 trees that are 18-inches in diameter-at-breast-height or greater. The application involves the demolition of five residences and their associated accessory structures near the intersection of Webster and Stadium Streets to accommodate the proposed development. COMPREHENSIVE PLAN DESIGNATION: Southern Oregon University; ZONING: SO; ASSESSOR'S MAP: 39 1E 10 CD; TAX LOT: 4200. *(Continued from December 13, 2011 Meeting)***

**B. PLANNING ACTION: #2011-01699**

**SUBJECT PROPERTY: 763 & 777 Jefferson Avenue**

**APPLICANT: Brammo Motorsports LLC**

**DESCRIPTION: A request for Site Review approval to construct two, two-story buildings of 14,857 square feet and 18,805 square feet for the research, design, manufacturing of prototypes, warehousing, storage and administrative offices for the production of Brammo Electric Motorcycles & products. The proposed buildings will be located at 777 Jefferson. A private, dirt test track to further the research and design is proposed for 763 Jefferson. COMPREHENSIVE PLAN DESIGNATION: Employment and Industrial; ZONING: E-1 & M-1; ASSESSOR'S MAP: 39 1E 14AC; TAX LOT: 101 & 102.**

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**VI. OTHER BUSINESS**

**A. Pedestrian Places Update**

**B. Bi-Annual Attendance Report (July – December 2010)**

**VII. ADJOURNMENT**

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
MINUTES  
December 13, 2011

**CALL TO ORDER**

Chair Pam Marsh called the meeting to order at 7:00 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Michael Dawkins  
Eric Heesacker  
Pam Marsh  
Debbie Miller  
Melanie Mindlin

**Staff Present:**

Bill Molnar, Community Development Director  
Derek Severson, Associate Planner  
April Lucas, Administrative Supervisor

**Absent Members:**

Mick Church

**Council Liaison:**

Russ Silbiger, absent

**ANNOUNCEMENTS**

Commissioner Marsh announced the next joint meeting with the Transportation Commission will be held in January. Community Development Director Bill Molnar announced the City Council passed first reading of an ordinance reducing the Planning Commission membership to seven members; and at the next Council meeting the Mayor will be recommending the appointment of a seventh member. Mr. Molnar stated the Pedestrian Places amendments approved by the Council will go into effect on December 15, and noted staff has been asked to go back before Council with an overview of where the changes apply. Mr. Molnar commented on the upcoming meeting schedule and stated the second meeting in January will focus on the Green Code Amendments, and in February the amendments to the Historic Design Standards will come forward. Commissioner Marsh requested they discuss the annual retreat at an upcoming meeting, and provided a brief update of the Chamber breakfast where the TSP Update was discussed.

**CONSENT AGENDA**

- A. Approval of Minutes.
1. October 11, 2011 Regular Meeting.
  2. October 25, 2011 Special Meeting.

Commissioners Miller/Mindlin m/s to approve the Consent Agenda. Voice Vote: all AYES. Motion passed 5-0.

**PUBLIC FORUM**

No one came forward to speak.

**TYPE II PUBLIC HEARINGS**

A. PLANNING ACTION: #2011-01576

SUBJECT PROPERTY: 1554 Webster Street (*on the Southern Oregon University campus*)

APPLICANT: American Campus Community Services

DESCRIPTION: A request for Site Review approval to construct a new single-story dining hall near the intersection of Wightman and Webster Streets, two new four-story residence halls near the intersection of Webster and Stadium Streets, two parking lots and associated site improvements on the Southern Oregon University campus at 1554 Webster Street. Also included are requests for Conditional Use Permit approval to allow buildings that exceed the maximum length and vary from the locations identified in the SOU Masterplan and to exceed the 40 foot height allowance in the SO zoning district, and a request for a Tree Removal Permit to remove 18 trees that are 18-inches in diameter-at-breast-height or greater. The application involves the demolition of five residences and their associated

accessory structures near the intersection of Webster and Stadium Streets to accommodate the proposed development. COMPREHENSIVE PLAN DESIGNATION: Southern Oregon University; ZONING: SO; ASSESSOR'S MAP: 39 1E 10 CD; TAX LOT: 4200

Commissioner Marsh read aloud the public hearing procedures for land use hearings.

### **Ex Parte Contact**

Commissioners Dawkins, Heesacker, Marsh, Miller and Mindlin declared site visits. Commissioner Miller indicated she was in a meeting where someone from SOU mentioned that the street lighting associated with this application would improve Ashland Street.

Commissioner Marsh explained that due to the volume of information, including the newly submitted items, the public hearing for this application will remain open and deliberations will be postponed to the January meeting. She stated tonight they will hear from staff, the applicant, and members of the public; and following the testimony portion Commissioners will have the opportunity to ask questions and issue their requests for additional information. She added that in order to leave here tonight with a good understanding of the application, she has offered to extend the applicant's 15-minute time limit.

### **Staff Report**

Associate Planner Derek Severson addressed the Commission and provided an overview of the application before them. He stated the proposal from Southern Oregon University is to construct two new residence halls off Stadium Street and a new dining hall at the intersection of Wightman and Webster. The dining hall is one-story and 27,500 sq.ft in size; the south residence hall is four-stories and 105,000 sq.ft., and the north residence hall is four-stories and 89,443 sq.ft. Mr. Severson commented on the 2010 SOU Master Plan and how this area was envisioned for development. He stated the Master Plan showed four buildings around a central quad and integrated the Greensprings complex and dining hall to enclose the quad and create a clear residential zone. He stated the current application differs from the Master Plan in that: 1) the dining hall is shifted to the intersection of Webster and Wightman, 2) the four residential hall buildings have been combined into two larger buildings, and 3) the south hall is set back from Ashland Street, and 4) the Greensprings complex no longer fronts the quad. Mr. Severson stated this application also involves the removal of five residential units and their associated accessory structures, the installation of a new parking lot at the corner of Stadium and Webster, and the removal of 22 significant trees on the property.

Mr. Severson summarized the key elements of the application as follows:

- 1) Tree Removal Permits for 22 significant trees.
- 2) Site Review Permits for the dining hall, south residence hall, north residence hall, and two parking lots.
- 3) Conditional Use Permits for the height of the residence halls, length of the residence halls, location of the south residence hall, location of the dining hall, and location of the parking.

Mr. Severson continued his overview of the application and commented on the following elements of the proposal:

#### ***Tree Removal***

Mr. Severson reviewed the tree removal site plan and identified the locations of the trees proposed for removal.

#### ***Dining Hall***

Mr. Severson reviewed the hall's elevations and noted the Master Plan strongly discouraged the use of single story buildings. He stated staff had raised concern regarding this building's sense of entry and how it relates to the street; and the applicant's have responded with revised drawings. He stated staff is generally satisfied with the revised drawings and they appear to comply with the standards.

#### ***Residence Halls***

Mr. Severson stated the buildings are each approximately 400 ft. long and front a central pavilion. He stated at the pre-application stage staff raised concern with the sense of entry, particularly on the south hall and how it relates to Ashland Street, and noted staff's suggestion for an archway treatment similar to Churchill Hall. In addition, he stated staff has concerns with the roofline and believes it should be broken up, and would like to see further design features used to break up the length of the building. Staff also believes the trash should be relocated.

### ***Transportation***

Mr. Severson explained at the Master Plan level there were a number of required studies that were deferred. Specifically, the Master Plan required that the following studies be prepared prior to application: Transportation Impact Analysis and Access Management Strategies, Pedestrian Safety Plan, Transportation Demand Management Strategies, Emergency Vehicle Access, and Parking Requirements for On-Campus Student Housing. Mr. Severson stated due to the level of transportation related issues involved with this proposal, this application has been scheduled for a public hearing before the Transportation Commission on December 15, and staff will report back on their discussion and recommendation(s) at the January meeting.

In summary, Mr. Severson stated the primary areas of focus for staff are:

#### ***Pedestrian Safety:***

- 1) Ensuring there is "big picture" consideration of the circulation changes in shifting 704 beds to the North Campus Village and how they will fit within the fabric of the campus and community. This will need to look at the Gateway Plaza intersection and other un-signalized crossings; treatment of campus circulation routes; and supporting monitoring, education and TDM measures.
- 2) Clarification of what improvements are to be done with the project, by whom, and when.

#### ***Building Design:***

- 1) Sense of entry and relationship to major streets with stronger entries, corresponding site planning to include detailed landscaping treatment, pedestrian connections, and the relocation of the trash compactor.
- 2) Better address length and articulation guidelines in terms of providing functional recesses or courtyard areas, or meaningful offsets to break up the building length.

#### ***Transportation & Parking Management Demand:***

- 1) While new parking areas are proposed, this proposal amounts to a net reduction of 44 parking spaces for the North Campus.
- 2) Impacts to the neighborhood and on-street parking from students and visitors/events.
- 3) North Campus Village location is intended to be walkable to the academic core, and resident students will not be able to park in commuter student lots and elsewhere on campus.
- 4) Gateway Plaza intersection and other crossing treatments.
- 5) Does the Commission believe parking and TDM needs to be further addressed?

#### **Questions for Staff**

Commissioner Miller asked if staff had recommended making the dining hall two-stories. Mr. Severson stated while the Master Plan discourages single-story buildings, the applicant's have stated the building's 40 ft. height is compatible with the residential neighborhood.

Commissioner Mindlin voiced her interest in the difference between the Master Plan and this proposal in terms of pedestrian circulation. She questioned if the new dining hall blocks access to McNeal Pavilion and asked about sidewalks and creating a more direct route along Wightman.

#### **Applicant's Presentation**

**Craig Morris and John Eldridge**/Mr. Morris stated this is the biggest project the University has ever taken on, and believes this will greatly enhance the recruitment and retainment of students. He apologized for the speed in which they are moving this forward, but their goal is to have this housing ready for fall 2013. Mr. Eldridge stated this project will shift 700 beds to the northside of campus, and will diversify their housing stock and allow them to meet their enrollment needs. He stated the Cascade Complex is undesirable to students and renovating it is cost prohibitive. He stated they went door to door and held open houses and the students are very excited about this project; and the feedback from neighbors has been positive as well. Mr. Eldridge stated the two main themes for this project are sustainability and pedestrian safety, and noted their long-standing and positive relationship with the City.

**Kurt Schultz/SERA Architects**/Mr. Schultz stated the Cascade Complex is very inefficient and its removal is what is driving this new campus village, which consolidates the residential and dining facilities. He commented that master plans are intended to be the framework and adjustments are common once development begins. He stated the dining hall outlined in the Master Plan was too

small and too far away, and the original quad was huge, with four downtown Portland city blocks fitting inside. He stated they believe their variations from the Master Plan result in significant improvements to the campus village, and noted they are striving for the highest levels of sustainability and are on track for LEED gold.

Mr. Schultz stated the three main components of this project are the dining hall/community center and the two residence halls. He explained the south hall will contain suite style apartments, and clarified the north hall was recessed back from Ashland Street in order to create a more humanely-scaled quad. He added this modification also allowed them to create a land bank which will be the location of the future mixed-use building. Mr. Schultz explained the decision to create two residence halls instead of four is due to the economic realities that two buildings are more efficient and sustainable. He added four buildings would have resulted in 1/3 higher energy consumption and would not be economically viable. He explained the dining hall was moved in order to make it more centrally located for the students, and stated its new location encourages safe crossings at the designated intersection.

Mr. Schultz commented on the parking element and stated they are proposing a 100-stall parking lot at Stadium and Webster, and a 55 stall lot on South College Way. He stated with the elimination of the parking lot for the dining hall, there is a net loss of parking, however Kittleson's parking analysis shows the parking availability is still acceptable. He also commented on the location of the trash compactor and explained it needs to be close to all three residence halls but also accessible to Recology. Mr. Schultz commented on the style of the buildings and stated all three will be in the same "SOU Mediterranean style" seen throughout campus.

Mr. Schultz reviewed the dining hall and community center layout. He stated they wanted tall ceilings because is it such a big room, and stated the cupola on top will bring in additional daylight and also the mechanical equipment. He stated this building will have a recessed porch with outdoor seating, and believes this lower scale works well with the residential buildings across the street. He also commented on the residence halls and noted the north hall will house upper division students and the south hall will house the lower division. He stated the quad is the social center of the design, and each hall consists of a central two-story lounge, and two wings off each side. He stated they feel this is a very successful design and noted the use of design elements to break up the mass and minimize the appearance of height. He added while he appreciates staff's input, he disagrees with the suggestion to install an arched entryway. Overall, he stated the design is good and they are very proud of what they have put forward, and believes SOU and the City will be proud as well.

**Phil Worth/Kittleson & Associates/**Noted he is also working with the City on their long-range TSP Plan and is aware of the City's transportation goals and needs. Mr. Worth stated of the five required studies, four are already complete and the Emergency Vehicle Access plan will be provided in January. He commented that the current use of the parking supply is well below operational capacity, and when the proposed dorms are fully occupied the University will still be within capacity standards at 84%. Mr. Worth commented on the TDM strategies and stated the strongest measure they are taking is to preserve students on campus. He stated students will find the proposed housing to be attractive well into their upper class years. Regarding pedestrian safety, he stated they take this issue very seriously and have looked closely at all crossings along Siskiyou Blvd., with particular emphasis at the intersection of Wightman and Siskiyou. He stated they have considered a number of treatments and will continue to work with the City to evaluate their options to ensure the crossings are safe for both motorists and pedestrians.

Commissioner Marsh requested Mr. Worth review the pedestrian safety elements. Mr. Worth stated for the un-signalized crossings, their recommendation is to install rapid light beacons, and to use countdown pedestrian crossing signals for the signalized intersections. He stated these two features will reinforce where to cross, and will also let drivers know there are people waiting to cross. He added they are looking at further treatments for Wightman and Siskiyou, since they believe a significant amount of pedestrian activity will occur at this location.

#### **Questions of the Applicant**

Mr. Worth clarified they are looking at the excess of signage along Siskiyou Blvd and are evaluating where signage should be placed. Regarding the traffic data collected for AM and PM peaks, he discounted the accusation that this information was gathered prior to the start of the school year and stated they deliberately held off on collecting this data so that they could gather this once school was in session; and this is the reason why this information was presented so late in the application process.

Comment was made questioning the pedestrian circulation, sidewalks, and lack of parkrows. Mr. Schultz explained how parkrows would impact the well established trees, and stated their intent was to direct the pedestrian traffic to the current tree-lined, wide

walkway. Comment was made that the applicant did not appear to make any efforts to connect this development to the pedestrian place zone across the street.

Comment was made questioning if the applicant has considered an underground crossing to move students across Siskiyou Blvd.

Comment was made that one of the requirements of the Master Plan was to conduct a TDM study and provide a timeline for implementation. Mr. Worth stated the initial draft of this study is included in the packet and the next step is to evaluate the current programs and develop a set of strategies and timeline. Comment was made that this was a requirement of the Master Plan and the Commission will expect for this to be provided when this comes back in January.

Staff's suggestion for an arched treatment on the south residential hall was noted, and it was questioned when the multi-use building might go in. Mr. Morris explained they cannot use state funds for the multi-use building, and therefore they will have to wait until the economic situation improves and can secure a private developer and financing, which may take 10-12 years. He added while an open archway through the building would not make this project viable, they are open to architectural changes that accomplish the intent. Comment was made that since the multi-use building may not come forward for some time, the Commission should consider the visual impacts of the south building from Ashland Street.

Comment was made that while the focus for the students will be the quad, the building facades that face the street should also be attractive; and it was questioned if there is a feasible way to raise the centerline of the roof and adjust the building facades. Mr. Schultz stated that bisecting the buildings with an archway would pose serious issues, but they could consider adding an arch to the façade.

Concern was expressed regarding the information that has been presented on how students will cross the arterials. Comment was made that the Stadium Street crossing is dangerous, and a better crossing should be reconfigured so that students can safely get over to the Market of Choice shopping center.

Comment was made questioning how the loading area for the dining hall will function. Mr. Schultz explained they have three types of delivery trucks. The standard trucks that do daily deliveries as well as the garbage trucks will work just fine; the long semi-trailer trucks will not fit into the loading area, but these deliveries only happen once or twice a week and are typically done early in the morning. He stated for these deliveries the trucks will pull up to the curb and wheel in their goods.

**Commissioners Miller/Miller m/s to continue the hearing past 9:30 p.m. Voice Vote: all AYES. Motion passed 5-0.**

### **Public Testimony**

**Colin Swales/143 Eighth Street/**Stated Siskiyou Blvd does not need to be a four-laned road and shared his experience of seeing a student nearly hit while crossing Ashland Street. Mr. Swales expressed his disappointment that this area was not selected as one of the pedestrian places zones and stated the pedestrian sidewalks along the Ashland Street frontage are totally inadequate in terms of the amount of pedestrian traffic that will be generated. He commented that the proposed residence halls are massive and is disappointed that the drawings provided by the applicant give a birds-eye view instead of a human perspective. He suggested three-dimensional walkthroughs be created to help the commissioners get a better perspective of the proposal. He also commented that while this area is outside the Detail Site Review Zone, these buildings will be visible from a main arterial, and suggested they come back with some type of treatment for the Ashland street frontage.

**Paige Townsend & Nathan Broom/RVTD/**Ms. Townsend stated they all agree that this project is a great value to the community, but they do have concerns about the trip generations caused by the students, both at this site and the campus as a whole. She explained this area is one of the top five trip generator focus areas they have been evaluating and feel the interaction between the University's Commuter Resource Center and RVTD's Transportation Demand Management Resources could be strengthened. Ms. Townsend commented on RVTD's services and programs and noted their desire to regain a bus pass program with the University. She commented on SOU's TDM program and recommended the City act as the enforcement arm of SOU's program; she also commented on ways the University could expand its program on site, including the use of an onsite coordinator, and strengthening their education and encouragement on campus. Ms. Townsend disagreed that the Commuter Resource Center is effective enough to provide the TDM program by themselves; believes there are ways their program can be improved; and would like to see the University work closer with RVTD and enact programs that have longevity.

### Applicant's Rebuttal

Craig Morris/Clarified the University provides monthly bus passes to all faculty and staff who request them, and many of their employees take advantage of this. He stated the students used to pay for a similar program and have to vote on whether to reenact this. He explained the students have expressed concern about the lack of coverage that is currently provided, however now that RVTD is expanding their bus service to Medford there has been interest in going back to this program. Mr. Morris stated students can currently ride the bus to the higher education center in Medford, but they can't get a bus ride back. He added the students who are most at risk for needing passes are not necessarily the students living on campus, but those who have to commute. Mr. Morris also commented on why this project is moving quickly and explained in fall of 2010 they had a 26% increase in student enrollment, and an additional 4% increase this fall, which is unheard of nationally. He stated the majority of these students are coming from out of state and these increases have sped up the timeline for getting this housing on the ground.

Commissioners Mindlin/Dawkins m/s to continue the public hearing to January 10, 2012. Voice Vote: all AYES. Motion passed 5-0.

### Final Questions/Request for Information

Commissioner Mindlin stated her primary interest is the circulation pattern and the entries into the dining hall. Commissioner Marsh noted her disappointment that the University was asking for so many exceptions to the 2010 Master Plan. She stated she is particularly interested in: 1) the sidewalks along Wightman and Stadium, including making these wider and adding parkrows, 2) minimizing the bulk and scale of the proposed residential halls by visually breaking them up and making them more accessible, 3) whether the existing dorms can be better engaged and including the Greensprings complex in the quad, and 4) the TDM component and making sure this relates to the Master Plan and the campus as a whole.

### OTHER BUSINESS

Commissioner Dawkins shared an article that was published 100 years ago in the Mail Tribune regarding the need to produce, cultivate and raise products locally and alleviate the need to import goods into the valley.

### ADJOURNMENT

Meeting adjourned at 10:05 p.m.

*Respectfully submitted,  
April Lucas, Administrative Supervisor*

**TYPE II  
PUBLIC HEARINGS**

---

**PA-2011-01576  
SOU North Campus Village**



**PLANNING ACTION: 2011-01576**

**SUBJECT PROPERTY: 1554 Webster (on the Southern Oregon University campus)**

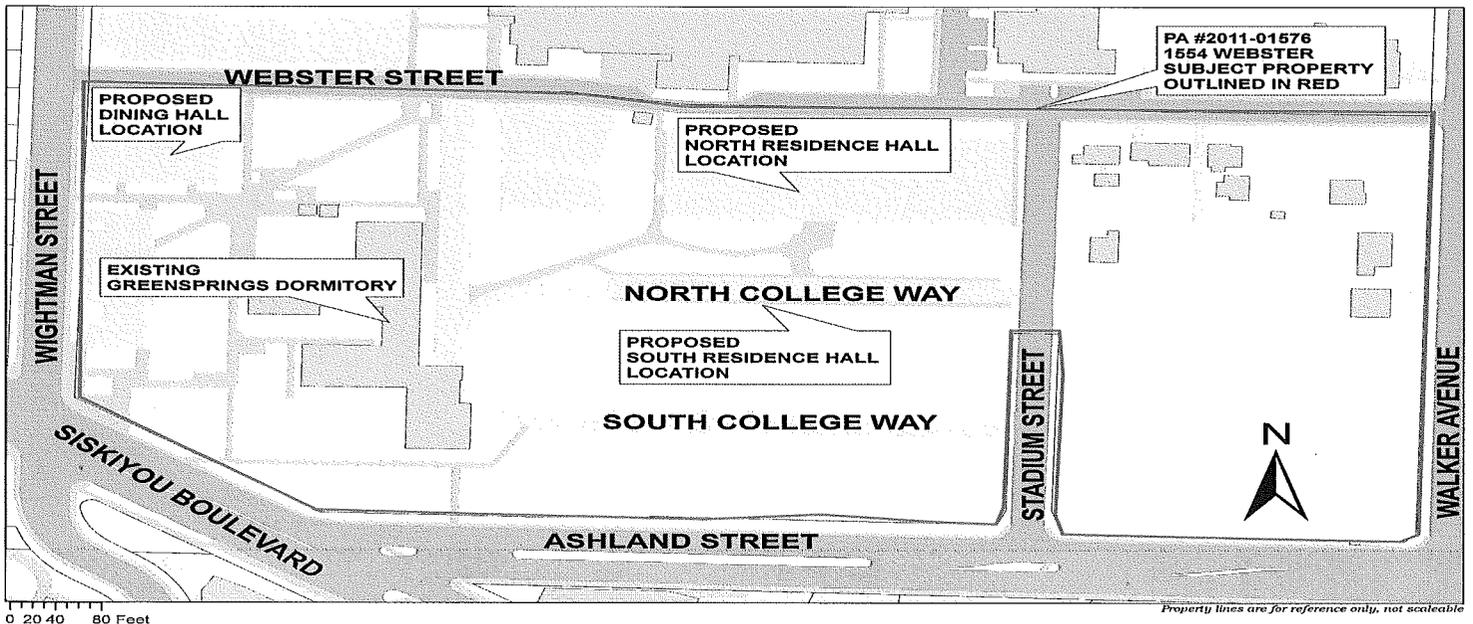
**OWNER/APPLICANT: American Campus Community Services**

**DESCRIPTION: A request for Site Review approval to construct a new single-story dining hall near the intersection of Wightman and Webster Streets, two new four-story residence halls near the intersection of Webster and Stadium Streets, two parking lots and associated site improvements on the Southern Oregon University campus at 1554 Webster Street. Also included are requests for Conditional Use Permit approval to allow buildings that exceed the maximum length and vary from the locations identified in the SOU Masterplan and to exceed the 40 foot height allowance in the SO zoning district, and a request for a Tree Removal Permit to remove 18 trees that are 18-inches in diameter-at-breast-height (d.b.h.) or greater. The application involves the demolition of five residences and their associated accessory structures near the intersection of Webster and Stadium Streets to accommodate the proposed development. **COMPREHENSIVE PLAN DESIGNATION: Southern Oregon University; ZONING: SO; ASSESSOR'S MAP: 39 1E 10 CD; TAX LOT: 4200****

**NOTE:** The Ashland Tree Commission will review this Planning Action on **December 8, 2011 at 6:00 p.m.** in the Community Development and Engineering Services building (Siskiyou Room) located at 51 Winburn Way.

**NOTE:** The Ashland Transportation Commission will review this Planning Action on **December 15, 2011 at 6:00 p.m.** in the Council Chambers building located at 1175 East Main Street.

**ASHLAND PLANNING COMMISSION MEETING: December 13, 2011 at 7:00 PM, Ashland Civic Center**



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### 18.72.070 Criteria for Approval

The following criteria shall be used to approve or deny an application:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.

(ORD 2655, 1991; ORD 2836, 1999)

## CONDITIONAL USE PERMITS

### 18.104.050 Approval Criteria

A conditional use permit shall be granted if the approval authority finds that the proposed use conforms, or can be made to conform through the imposition of conditions, with the following approval criteria.

- A. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
- B. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.
- C. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:
  1. Similarity in scale, bulk, and coverage.
  2. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  3. Architectural compatibility with the impact area.
  4. Air quality, including the generation of dust, odors, or other environmental pollutants.
  5. Generation of noise, light, and glare.
  6. The development of adjacent properties as envisioned in the Comprehensive Plan.
  7. Other factors found to be relevant by the Hearing Authority for review of the proposed use.

## TREE REMOVAL

### 18.61.080 Criteria for Issuance of Tree Removal - Staff Permit

An applicant for a Tree Removal Permit shall demonstrate that the following criteria are satisfied. The Staff Advisor may require an arborist's report to substantiate the criteria for a permit.

- A. Hazard Tree: The Staff Advisor shall issue a tree removal permit for a hazard tree if the applicant demonstrates that a tree is a hazard and warrants removal.
  1. A hazard tree is a tree that is physically damaged to the degree that it is clear that it is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within public rights of way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated or the damage alleviated. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard or a foreseeable danger of property damage to an existing structure and such hazard or danger cannot reasonably be alleviated by treatment or pruning.
  2. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.
- B. Tree that is Not a Hazard: The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:
  1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards, including but not limited to applicable Site Design and Use Standards and Physical and Environmental Constraints. The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and
  2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and
  3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with other provisions of the Ashland Land Use Ordinance.
  4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.

(ORD 2951, 2008; ORD 2883, 2002)

**ASHLAND PLANNING DIVISION  
STAFF REPORT ADDENDUM  
January 10<sup>th</sup>, 2012**

**PLANNING ACTION:** #2011-01576

**APPLICANT:** American Campus Community Services

**LOCATION:** 1554 Webster Street (on the S.O.U. campus)

**ZONE DESIGNATION:** SO

**COMPREHENSIVE PLAN DESIGNATION:** Southern Oregon University

**APPLICATION DEEMED COMPLETE:** December 2, 2011

**120-DAY TIME LIMIT:** March 31, 2012<sup>\*(may be adjusted based on date of last new submittals)</sup>

**ORDINANCE REFERENCE:**

18.61	Tree Preservation and Protection
18.64	SO University District
18.72	Site Design Review
18.92	Off-Street Parking
18.96	Sign Regulations
18.104	Conditional Use Permit
and	The Campus Master Plan Update for Southern Oregon University ("The SOU Plan")

**REQUEST:** A request for Site Review approval to construct a new single-story dining hall near the intersection of Wightman and Webster Streets, two new four-story residence halls near the intersection of Webster and Stadium Streets, two parking lots and associated site improvements on the Southern Oregon University campus at 1554 Webster Street. Also included are requests for Conditional Use Permit approval to allow buildings that exceed the maximum length and vary from the locations identified in the SOU Masterplan and to exceed the 40-foot height allowance in the SO zoning district, and a request for a Tree Removal Permit to remove 24 trees that are 18-inches in diameter-at-breast-height (d.b.h.) or greater. The application involves the demolition of five residences and their associated accessory structures near the intersection of Webster and Stadium Streets to accommodate the proposed development.

**I. Relevant Facts**

**A. Background - History of Application**

The public hearing for this matter was opened and testimony taken at the January 13<sup>th</sup>, 2010 regular meeting of the Planning Commission. During the hearing, staff identified issues relating to pedestrian safety, building design and parking which merited further consideration; staff also noted that a significant amount of new material had been submitted after completion of the staff report, and that the Transportation Commission had not yet had the opportunity to review and comment on the application's

transportation component, which included a significant amount of information which was deferred from the SOU Plan's adoption, where the Transportation Commission would typically have commented. Based on these factors, staff recommended that the hearing be continued until the Planning Commission's next regular meeting. At the conclusion of testimony, the Commission moved to continue the public hearing on the matter to the regular meeting of January 10<sup>th</sup>, 2012.

## **B. Detailed Description of Issues Relating to the Proposal since December meeting**

### **December 13<sup>th</sup> Planning Commission Meeting**

During the course of the December 2011 Planning Commission hearing, staff identified the following issues as needing to be further addressed:

- **Pedestrian Safety**
- **Automobile and Bicycle Parking**
- **Management of Parking and Transportation Demand**
- **Fire Apparatus Access**
- **Building Design**

In addition, in discussion during the hearing the following issues were also identified:

- **Consideration of needed sidewalk improvements along the subject property's street frontages on Wightman, Ashland and Stadium Streets**
- **An implementation timeline for Transportation Demand Management strategies**

### **December 15<sup>th</sup> Transportation Commission Meeting**

The Transportation Commission considered the application at their December 15<sup>th</sup> meeting and made specific recommendations for improvements at a number of intersections and pedestrian crossings along the University corridor after reviewing materials provided by the applicants' transportation consultants, Kittelson & Associates. In addition, the Transportation Commission requested that any approval be conditioned to require that the sidewalks along Ashland Street between Walker Avenue and Siskiyou Boulevard be reconstructed to full city street standards. A more detailed discussion of their specific recommendations is included below in the discussion of the "Project Impact", and copies of their minutes and recommendations will be distributed at the January Planning Commission hearing.

### **January 5<sup>th</sup> Tree Commission Meeting**

The Tree Commission was unable to convene their December meeting due to a lack of quorum; they're now scheduled to review and comment on the application at their regular meeting on January 5<sup>th</sup>, 2012. Because their comments are not available as this staff report is being drafted, a condition has been recommended below to require that all recommendations of the Tree Commission's January 5<sup>th</sup> meeting, where consistent with applicable standards and with final approval by the Staff Advisor, be made conditions of approval for the project. Copies of their January 5<sup>th</sup> recommendations will be distributed at the January Planning Commission hearing.

*(Staff would also note here that a 24-inch Catalpa Tree near the intersection of Wightman Street and Siskiyou Boulevard identified as Tree #71 has been proposed for removal on the subject property in conjunction with a separate land use action. Planning Action #2011-00530 is a conditional use permit application to modify the university's sign program with the addition of four gateway signs at Siskiyou Boulevard's intersection with Mountain Avenue, University Way, Wightman Street, and Indiana Street, as well as a change in the university's logo on all campus signage. Staff has administratively approved this request, and the decision will be final if not appealed by January 4, 2012.)*

## **II. Project Impact**

### **Pedestrian Safety**

In considering the project's impacts as they relate to pedestrian safety and the SOU Plan's requirement that a pedestrian safety plan be provided prior to the shifting of the student residential population across Siskiyou Boulevard, the project's transportation consultants Kittelson & Associates have provided detailed recommendations for the treatment of several of the intersections along the University corridor to provide for the safety of pedestrians crossing between the north and south campus areas while also seeking to minimize the disruption to the flow of vehicular traffic through the corridor. They conclude that "with implementation of the recommended measures" the project can be completed as proposed while maintaining safety at the intersections and acceptable traffic operations. Kittelson's specific recommendations for each of the intersections are detailed below, followed by corresponding recommendations from both city staff and the Transportation Commission, which considered the project at its December 15<sup>th</sup>, 2011 meeting:

#### **Intersections of South Mountain Ave. & Siskiyou Blvd., and of Ashland St. & Siskiyou Blvd.**

- Kittelson's recommendations were to replace the pedestrian signal heads with pedestrian countdown signal heads and add a five-second lead time to the pedestrian phasing of each of these intersections' signals.
- Staff concurred with the recommendation to install pedestrian countdowns signal heads at these intersections, but for both intersections suggested that a five-second lead time would have minimal benefit to pedestrians.
- The Transportation Commission recommended that conditions be attached to the approval to require the replacement of the pedestrian heads with countdown heads and the addition of a five-second leading pedestrian phase for both of these intersections.

#### **Crossing of Siskiyou Blvd. at University Way**

- Kittelson recommended that the existing flashing beacons be replaced with rectangular rapid flash beacons (RRFBs), and that the adjacent street trees be pruned and maintained to improve visibility.
- Staff concurred with the recommendations to install RRFBs in this location, and noted that the Parks Department could prune and maintain the street trees in this location.
- The Transportation Commission recommended that a condition be attached to the approval to require the installation of RRFBs in this location.

#### **Crossing of Siskiyou Blvd. at Avery St.**

- Kittelson recommended that the existing flashing beacons be replaced with rectangular rapid flash beacons (RRFBs), and that the “Stop Here for Pedestrians” sign at the westbound approach be relocated to provide adequate clearance for the crosswalk signage and beacons.
- Staff concurred with the recommendations to install RRFBs in this location, and noted that the city crews could complete the sign relocation.
- The Transportation Commission recommended that a condition be attached to the approval to require the installation of RRFBs in this location.

#### **Crossings of Siskiyou Blvd. at Garfield St. and at Bridge St.**

- Kittelson recommended that the existing flashing beacons be replaced with RRFBs at both of these pedestrian crossings.
- Staff concurred with the recommendations to install RRFBs in these locations.
- The Transportation Commission recommended that a condition be attached to the approval to require the installation of RRFBs at these crossings.

#### **Gateway Intersection of Wightman St., Indiana St. & Siskiyou Blvd.**

- Kittelson recommended the replacements of the existing pedestrian signal heads with pedestrian countdown signal heads; the adjustment of lead times for the pedestrian phasing of the signal to a 36-second “scramble” phase, which will require two additional pedestrian signal heads; and the installation of high visibility markings for a diagonal crossing of the gateway intersection.
- Staff concurred with the recommendation to install pedestrian countdowns signal heads at this intersection, and recommended that prior to implementation of a “scramble phase” that the intersection signalization treatment be designed by Kittelson to minimize the time automobiles are waiting due to the 36-second pedestrian phasing. Staff also concurred with the recommendation to install high visibility pavement markings for a diagonal crosswalk, and indicated that the existing signal cabinet would need to be upgraded to implement these changes.
- The Transportation Commission recommended that conditions be attached to any approval to require that the existing pedestrian signal heads be replaced with pedestrian countdown heads which include an audible indicator of signal timing, that the applicants design and reconfigure the intersection with a single diagonal crosswalk as shown in conceptual drawings presented at the Transportation Commission meeting as new Figure 12; that the existing controllers at both Wightman Street and Highway 66 be upgraded to accommodate the signal modifications; and that high visibility pavement markings for a diagonal crossing be installed.

#### **Crossing of Siskiyou Blvd. at Frances Ln.**

- Kittelson recommended that advance pedestrian signs with RRFBs be installed.
- Public Works/Engineering noted that based on the current and anticipated number of pedestrian crossings in this location, they did not believe that the crossing improvements were merited at this time and recommended that the crossing instead be looked at with future development.
- The Transportation Commission made no recommendation for improvements to this crossing at this time.
- Planning staff believe that the Planning Commission may wish to consider whether

pedestrian safety enhancements at this crossing in the form of signage and RRFB installation would ultimately be beneficial to those students likely to cross to destinations such as Starbucks in this vicinity, as recommended by Kittelson.

#### **Crossing of Ashland St. at Stadium St.**

- Kittelson recommended that advance pedestrian signs with RRFBs be installed.
- Staff concurred with the Kittelson recommendations, and noted that Kittelson should also look more closely at the possibility of other improvements to the existing placement of the crosswalk and whether the existing median layout needed to be altered.
- The Transportation Commission recommended that condition be attached to the approval to require that advance pedestrian signs and pedestrian-activated RRFBs be installed, and that the applicants' team further review the location of the existing crosswalk, and if feasible and warranted the crosswalk be relocated with appropriate pedestrian ways and amenities provided for the new location.

#### **Intersection of Ashland St. and Walker Ave.**

- Kittelson recommended the replacement of the pedestrian signal heads with pedestrian countdown signal heads and the addition of a five-second lead time to the pedestrian phasing of the signal.
- Staff concurred with the Kittelson recommendations, and noted that these improvements would also benefit children using this signal on their way to or from Walker Elementary and Ashland Middle School.
- The Transportation Commission recommended that pedestrian countdown heads with a five-second leading pedestrian phase be installed. The Transportation Commission also recommended that the existing city sidewalk installation on Ashland Street from Walker Avenue to Siskiyou Boulevard be reconstructed to full city street standards for an arterial street.

\* \* \* \*

The Oregon Department of Transportation (ODOT) has reviewed the application and made a determination that the construction of the dining hall and residence halls will not adversely affect state highway facilities (Siskiyou Boulevard or Ashland Street) and thus does not trigger any further ODOT review. However, ODOT has recommended that the city and university update the current crosswalk lighting to replace the existing flashing beacons with rectangular rapid flash beacons (RRFBs) now in use elsewhere around the state, as recommended by Kittelson. ODOT notes that RRFBs have demonstrated effectiveness in improving pedestrian safety in areas where there are conflicts between pedestrians and motor vehicles, particularly at uncontrolled or mid-block crossings, and has also provided a copy of the RRFB section of the Oregon Bicycle & Pedestrian Plan (pg. 5-17) for reference.

Planning staff are generally supportive of the Transportation Commission recommendations; however we believe that the timing of a requirement to reconstruct existing sidewalks on Ashland Street to current city street standards should be carefully considered. Ashland's Street Standards call for a consideration of existing trees in street design, and both the Site Design & Use Standards and Street Standards handbooks require preservation of natural features including existing, established trees to the greatest

extent possible. In this case, reconstruction of the existing sidewalk would necessitate the removal of 29 or more trees along the subject property's Ashland Street frontage, and given the topography present would also require that either a safety railing (*creating physical barrier between the property and the pedestrian streetscape*) or a fill slope (*requiring additional tree removals*) be installed. The applicants have provided a new submittal (Sheet L005 revised December 30, 2011) which identifies the additional tree removals that would likely be necessary to reconstruction the Ashland Street sidewalks. Full reconstruction of the sidewalk in this vicinity could also pose complications to utility installations that would be necessary to serve a future mixed-use building on Ashland Street, as well as any potential for a future reconfiguration of the right-of-way in this vicinity. With this in mind, it is staff's recommendation that sidewalk reconstruction in this vicinity be tied to the future construction of a new mixed-use building fronting on Ashland Street to allow for more careful planning of the relationship of the pedestrian corridor, buildings and plaza space as well as more efficient and coordinated installation of utility infrastructure necessary to serve that future building, while retaining the large, established trees along Ashland Street until their removals are necessary.

The Connectivity Standards detailed in Section II-7 of the Street Standards Handbook provide for the use of off-street pathways connected to the street network as a viable component of the transportation system. These standards explain that such pathways should not be used in lieu of traditional streets with sidewalks, but can be appropriate to supplement traditional streets and sidewalks. In this instance, street improvements including sidewalks are already in place along Ashland Street, and staff believe that the use of a temporary off-street pathway to supplement the sidewalk and accommodate likely pedestrian circulation interior to the site, as envisioned in the Connectivity Standards, is an appropriate treatment until development occurs within the Detail Site Review Zone along the property's Ashland Street frontage. This off-street pathway would have the added benefit of creating a clear relationship between the new South Hall's southern entry and the Ashland Street pedestrian streetscape.

Staff has discussed this issue at some length with the applicants subsequent to the December meetings of the Planning and Transportation Commissions, and believe the more appropriate treatment to accommodate pedestrian circulation in this vicinity until a building is developed along Ashland Street is to require that a new off-street pedestrian connection which would link the existing pathway system from the gateway intersection and Greensprings dorms through the grassy area along Ashland Street, through the parking along South College Way with a materially-distinct crossing to the entrance of the proposed South Hall and out to the sidewalk on Ashland Street near its intersection with Stadium Street and the crosswalk leading to the nearby University District. This pathway would provide a more direct route to the most likely pedestrian destinations arising from the shifting of the resident population to the north campus in a manner which would be better integrated into the existing campus pedestrian circulation system, and would also accommodate coordinated planning of sidewalk improvements with future development at the street. The applicants have provided a revised Landscaped Site Plan (Sheet L001 revised December 30, 2011) which details this pathway installation.

Staff has accordingly proposed conditions below which include all aforementioned recommendations as conditions of approval with the exception of the proposed sidewalk reconstruction on Ashland Street, which we believe would be more appropriately

considered at the time development occurs along the subject property's Ashland Street frontage. A condition is also recommended to require that the applicants provide a detailed pedestrian circulation plan for the new North Campus Village area which identifies an integrated pedestrian pathway system, including a new connection from the southeast corner of the Greensprings to the new South Hall's south entry and to the intersection of Ashland and Stadium Streets, with routes and material treatment of the paths, landscaping including the new gateway plaza treatment, pedestrian scale lighting placement, and way finding measures clearly detailed.

### **Stadium Street Sidewalks**

With the relocation of the dining and residence hall buildings, the installation of 150+ parking spaces in lots not originally envisioned in the SOU Plan, the partial closure of Webster Street to motor vehicles, and the likely eventual construction of a new 50,000 square foot Student Recreation Center addition to McNeal Pavilion, staff believe that Stadium Street will ultimately serve as a gateway street for the developing North Campus Village neighborhood. As such, we believe that despite its being located largely on university property, it merits improvement to city street standards to include full sidewalks and parkrow installation along its length between Webster Street and South College Way, and curbside sidewalks along the remaining length between South College Way and Ashland Street in order to preserve a number of large, established trees near the Ashland Street intersection. With these improvements, staff believes that the street would take some of the burden of vehicular traffic off of the intersection of Siskiyou Boulevard and Wightman Street, and off of Wightman Street itself, thus lessening some of the impacts on the surrounding residential neighborhood from the Dining Hall's relocation, while also providing an attractive and clearly articulated route to better focus pedestrian circulation on the north campus.

Staff has accordingly recommended a condition below to require that new sidewalks and parkrow planting strips with street trees and pedestrian scale street lighting be installed to city street standards along both sides of Stadium Street between Webster Street and South College Way, and to require that curbside sidewalks be installed along both sides of Stadium Street between South College Way and Ashland Street to accommodate preservation of the existing trees on that segment.

### **Wightman Street Sidewalks**

At the December Planning Commission meeting, there was also a good deal of discussion about the existing sidewalks on Wightman Street, particularly in the vicinity of the new Dining Hall. It was suggested that while there are existing, established sycamores in place they are not the best street trees. Ashland's Recommended Street Tree Guide notes that sycamores are trees of great beauty, but also that they have strong surface roots, are subject to disease, can be messy, and generally have a physical form which shades solar installations.

Wightman Street has a narrow, five-foot wide curbside sidewalk in place; just behind the sidewalk there is a row of eight large established sycamores ranging in size from 11 to 21 inches in diameter at breast height. These trees provide a buffer between the sidewalk and adjacent surface parking, and two more rows of sycamores containing a total of 17 trees ranging in size from seven to 20 inches in diameter line an off-street pedestrian circulation pathway that leads roughly from the gateway intersection to the location of

the new Dining Hall. The established curbside sidewalk pattern on the east side of Wightman Street, with only a few small disconnected sections of sidewalk in place along the west side, is carried on all the way to Iowa Street, with a number of large established trees planted very near the back of the sidewalk. The shift to a standard parkrow configuration with wider sidewalks would need to be considered not only for its immediate impact to the established sycamore trees on the subject property, but also to the other trees further up the Wightman Street corridor as development of the north campus continues.

The SOU Plan notes that one of the key opportunities to improve the quality of the campus is to continue to reinforce the main pedestrian spine through new plantings and the development of plazas and new outdoor activity nodes, improved sightlines, and better orientation and articulation of building entries, and also recognizes that the north campus area has substantially less tree canopy established than the southern campus (pp. 25-26). In assessing the north campus area at the pre-application level, staff noted that the existing sycamore-lined pathway had the effect of creating a promenade to draw students from the gateway intersection's plaza area to the new Dining Hall location with an already established canopied corridor, and is one of the few areas on the north campus where the landscaping is already working to reinforce the creation of a main pedestrian spine, in this case in an area where the existing pattern could be easily reinforced to create a strong line of sight connecting the gateway intersection plaza to the new Dining Hall. Staff also noted that these sycamores seem generally healthy, that they have had relatively minimal impacts on the paving in the area, and that the University has recently replaced paving where it was being impacted. When the sycamore street trees and the adjacent sycamore-lined off-street pathway are considered in combination with the established pattern of existing sidewalks and large stature trees further down Wightman Street, staff does not believe a significant reconstruction of sidewalks and the associated removal of established trees is appropriate.

### **Bicycle Parking, Rack Design & Placement**

During December's hearing, staff had noted that additional information was needed to detail bicycle parking to be provided with the proposal. In discussions of bicycle parking with the applicants subsequent to the December meeting, they have indicated that they intend to provide at least the required one bicycle parking space per five required automobile spaces outdoors, with 100 percent of the spaces provided to be covered in racks placed at various points of entry around the proposed buildings. A site plan has been provided illustrating the proposed covered outdoor bicycle parking locations, and the applicants have indicated that their preference would be to continue to use the two-level racks which are in use around campus and have become a *de facto* standard for campus bicycle parking. Finally, the applicants have indicated that they will also provide for indoor parking of bicycles by providing locking bike parking rooms on the lower floors to accommodate indoor parking for students on upper floors, and in-room bicycle parking as well, as part of the requirements for LEED certification.

In staff's view, the provisions made for bicycle parking are consistent with the placement and coverage standards of the land use ordinance and will ultimately exceed the number of bicycle parking spaces required. While the two-level bicycle parking structures will not accommodate all users or bicycle types on their upper levels, the lower level provides options for locking bicycles securely by the frame similar to those provided by the

standard inverted U-rack as detailed in AMC 18.92.060.J, with the upper level providing for a doubling of the number of spaces in the same area. Staff observations are that these racks seem to be well-utilized wherever they have been placed on campus, and for staff, the ready availability of a sufficient amount of convenient, secure bicycle parking will go a long way toward promoting bicycle use by students as a viable alternative to driving.

### **Automobile Parking, Parking Management and Transportation Demand Management**

As noted in the December hearing, the application identifies 570 existing off-street parking spaces on the northern portion of the campus, and explains that in the applicants' studies these spaces currently have a utilization rate of roughly 36 percent. On-street parking currently has a peak utilization rate of 91 percent, and the application suggests that 50 percent of the existing on-street demand is tied to the university. The applicants propose to construct two parking lots – one off of Stadium Street at College Way, where parking is already in place, and another at the corner of Stadium and Webster Streets. The current proposal, with some existing parking removed due to proposed building placement, results in a net reduction of 44 parking spaces, leaving 526 off-street spaces to accommodate demand on the north campus. The application notes that with the proposed north campus development, a utilization rate of approximately 77 percent could be obtained for off-street parking on the north campus, and that there would be a possibility to absorb the additional demand currently associated with on-street parking into the campus.

It was noted during the December hearing that the SOU Plan required the applicants to provide a Transportation Demand Management (TDM) strategy and a timeline for the implementation of specific TDM measures; the applicants have provided a memo as an update on their TDM plans and near-term and long-term action goals, which has been provided in the Planning Commission packets. This memo notes while some actions are entirely within the University's control, such as setting policies on where students and employees park, other items such as a bus pass program or neighborhood parking permit program are dependent upon the expansion of working partnerships between the City, Rogue Valley Transportation District (RVT) and the surrounding neighbors. The memo also notes that the proposed North Campus Village project is a TDM strategy unto itself in that it seeks to make on campus residence more attractive to students who would otherwise commute by car, and provides substantial bicycle parking to support student bicycle commuting. The memo goes on to explain that SOU will implement variable pricing between parking lots when the residence halls open in fall of 2013, re-designate parking lots on the north and south campus areas to ensure that resident students have sufficient parking available on the appropriate side of Siskiyou and that parking resource utilization efficiency is maximized, and would be willing to adjust parking pricing further and pursue other measures such as restricting the sale of parking permits to first year students but believes that a neighborhood parking permit program would be needed prior to implementation to avoid having parking impacts spill over to the surrounding neighborhood streets. In terms of additional transit strategies, the applicants have indicated a willingness to meet with RVT, university staff and administrators and students to discuss transit subsidies, noting that they would like to reinstate a bus pass

program in an appropriate form provided there are changes in transit service to meet student needs such as adequate evening hours to support students who commute to the Higher Education Center in Medford, and will also explore options for providing some sort of express shuttle service for students traveling to the Medford Higher Education Center campus and from outside of Medford as well.

While the application suggests that parking management strategies including parking pricing, parking restrictions, and specific parking lot designations could further reduce demand, in conjunction with transportation demand managements measures such as transit subsidies and a targeted shuttle service, the submittal materials ultimately conclude that because the proposed parking is adequate for the request these measures are not necessary in association with the current proposal. In staff's assessment, the fact that application largely involves a shifting of existing student population from one side of Siskiyou Boulevard to the other merits a large part of the application's focus going to providing adequate parking within a reasonable proximity to the new halls and pedestrian safety enhancements at the likely crossing points for their residents. However without a more detailed explanation of the future Student Recreation Center addition to McNeal Pavilion (*i.e. will this 50,000 square foot building serve only students, or is it likely to also host larger sporting events which draw significant numbers of visitors to the campus?*) which is not proposed for consideration at this time, we believe it should be made clear that a finding cannot be made at this time that the parking in place will be adequate to serve that future construction, and that at the time that project is considered more aggressive parking management and transportation demand management measures such as a neighborhood parking permit program are likely to be necessary to minimize vehicular impacts to the surrounding neighborhood from the combined demand of the North Campus Village housing and Student Recreation Center addition.

### **Fire Apparatus Access**

During the course of the hearing in December staff noted that final fire apparatus access and hydrant installation details remained to be approved by the Fire Marshal. In discussing this issue with the Fire Marshal, she noted that the applicants would need to provide plans detailing required aerial fire truck access and the placement of fire hydrants to serve them prior to building permit approval, while also demonstrating that sufficient access is maintained for the existing buildings including the Greensprings residence hall complex. The applicants' team has been in continued communication with the Fire Marshal in developing these plans, and staff has recommended a condition below to require that the applicants provide a final fire apparatus access plan for the review and approval of the Fire Marshal prior to submittal of the building permit. Because both city standards and the SOU Plan seek to maintain a pedestrian scale to the campus improvements, staff would also recommend that the Planning Commission consider including language which would limit accessways to no more than the minimum required under the driveway standards in AMC 18.92, with any additional width to accommodate fire apparatus requirements to be achieved through alternative treatments such as rolled curbs and materially distinct sidewalks installed next to the driveway paving, or the use of grasscrete or similar alternative treatments which will support fire apparatus access in a manner acceptable to the Fire Marshal and Staff Advisor without requiring wider than necessary swaths of paving.

## **Building Design**

In terms of the building design as it relates to standards for sense of entry, relationship to the street, length and articulation, staff believes that the revisions proposed by the applicants and provided in the Planning Commission's packets have effectively addressed the previously-raised concerns and now satisfy the applicable approval standards. Specifically, the design includes the addition of dormer elements over the bay window projections on the south hall so that the recesses are reflected through the roofline to more effectively break up the building's length and provide greater architectural interest. The applicants have also proposed to revise the street-facing entries on both the north and south halls to incorporate a large arch element over a stronger entry, with a recessed entry area to provide refuge from the elements at this entry, a slightly raised roof on the central building element to more effectively distinguish the massing of the building into three parts, and a pedestrian connection out to Ashland Street which when taken in sum greatly improve the building's presence to the Ashland Street streetscape.

### **III. Procedural - Required Burden of Proof**

**The criteria for Site Review approval are described in 18.72.070 as follows:**

- A. *All applicable City ordinances have been met or will be met by the proposed development.*
- B. *All requirements of the Site Review Chapter have been met or will be met.*
- C. *The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.*
- D. *That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.*

**The criteria for Conditional Use Permit approval are described in 18.104.050 as follows:**

- A. *That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.*
- B. *That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.*
- C. *That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:*
  - 1. *Similarity in scale, bulk, and coverage.*
  - 2. *Generation of traffic and effects on surrounding streets. Increases in pedestrian,*

bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.

3. Architectural compatibility with the impact area.
4. Air quality, including the generation of dust, odors, or other environmental pollutants.
5. Generation of noise, light, and glare.
6. The development of adjacent properties as envisioned in the Comprehensive Plan.
7. Other factors found to be relevant by the Hearing Authority for review of the proposed use.

**The criteria for a Tree Removal Permit are described in AMC 18.61.080.B as follows:**

1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards, including but not limited to applicable Site Design and Use Standards and Physical and Environmental Constraints. The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and
2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and
3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternative to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of the Ashland Land Use Ordinance.
4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.

#### **IV. Conclusions and Recommendations**

The application requests Site Review, Condition Use and Tree Removal permit approvals to construct a new single-story dining hall, two new four-story residence halls, two parking lots and associated site improvements on the Southern Oregon University campus, and to remove 24 significant trees. Conditional Use Permit approval is required because the proposed development includes buildings and parking lots which vary from those conceptually envisioned in the mutually agreed upon SOU Plan, and the Conditional Use Permit process provides a higher level of review which allows for a more thorough consideration of the impact of these variations from the adopted plan's vision.

At last month's hearing, staff raised a number of issues including pedestrian safety, building design, and the management of parking and transportation demand which we believed needed to be further addressed before a decision could be reached. We also had hoped that continuing the hearing until January would provide the time to more

completely review late arriving submittals and allow the Transportation Commission to review and comment on the application.

### **Pedestrian Safety**

With the implementation of the Kittelson recommendations, as supported by the Transportation Commission, signage and pavement markings would be upgraded, trees pruned to improve visibility, and rectangular rapid flash beacons (RRFB's) installed at the four existing crossing on Siskiyou; the crossing at Ashland and Stadium Street would have RRFB beacons and improved pavement markings and signage installed and the crossing and median configuration further considered; the crossings at Mountain Avenue and Walker Avenue would be upgraded with pedestrian countdown signals with audible countdown indicator and the addition of a five second pedestrian lead time; and the gateway intersection at Indiana and Wightmans Streets would have its existing pedestrian signal heads replaced with pedestrian countdown signal heads, a new signal cabinet, and the addition of a 36-second pedestrian scramble phasing lead time and high visibility markings to accommodate a diagonal crossing.

In addition, staff has recommended that sidewalks be installed on both sides of Stadium Street between Webster Street and Ashland Street, and that an additional off-street pedestrian path be installed on campus parallel to Ashland Street between Stadium Street and Siskiyou Boulevard to facilitate the likely new student circulation patterns on the subject property. In staff's view, with the implementation of these measures the application can be found to have adequately addressed pedestrian safety.

### **Management of Parking & Transportation Demand**

The application suggests that parking management strategies applied in conjunction with transportation demand management measures could further reduce demand, but the submittal materials ultimately conclude that because the proposed parking is adequate for the request these measures are not necessary in association with the current proposal. For staff, because the application involves mostly a shifting of existing student population from one side of the boulevard to the other, the application's focus primarily on providing adequate parking within a reasonable proximity to the new halls and pedestrian safety enhancements at likely crossing points for residents is merited. However, we believe it is important to note that a finding cannot be made at this time that the proposed parking will be adequate to serve future construction on the north campus, such as the Student Recreation Center addition to McNeal Pavilion, and that at the time future projects are considered, more aggressive parking management and transportation demand management measures such as a neighborhood parking permit program are likely to be necessary to minimize vehicular impacts to the surrounding neighborhood.

### **Building Design (*Sense of Entry, Relationship to the Street, Length & Articulation*)**

Staff believes that the design revisions proposed by the applicants and provided in the Planning Commission's packets have effectively addressed the previously-raised

concerns and that the proposal can now be found to satisfy the applicable approval standards. Specifically, the design includes the addition of dormer elements over the bay window projections on the south hall so that the recesses are reflected through the roofline to more effectively break up the building's length and provide greater architectural interest. The applicants have also revised the street-facing entries on both the north and south halls to incorporate a large arch element over a stronger entry, with a recessed entry area to provide refuge from the elements at this entry, a slightly raised roof on the central building element to more effectively distinguish the massing of the building into three parts, and an off-street pedestrian pathway connecting the existing pathway system to the building's entry and Ashland Street which when taken in sum greatly improve the building's presence to Ashland Street.

With the additional information provided and modifications proposed since the December hearing, staff is supportive of the application and recommends its approval with the following conditions attached.

- 1) That all proposals of the applicant shall be conditions of approval unless otherwise modified herein, including but not limited to the requirement that the project be certified to at least LEED Silver status within 24 months of issuance of the certificate of occupancy.
- 2) That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify the Site Review and Conditional Use Permit approvals shall be submitted and approved prior to the issuance of a building permit.
- 3) That prior to submittal of the building permit application, the applicants shall consult with Conservation Division staff to allow for the fine-tuning of energy conservation strategies for the proposed buildings.
- 4) That all recommendations of the Ashland Tree Commission from their January 5<sup>th</sup>, 2012 meeting, where consistent with the applicable ordinances and standards and with final approval of the Staff Advisor, shall be conditions of approval unless otherwise modified herein.
- 5) That building permit submittals shall include:
  - a) The identification of all easements, including but not limited to public and private utility easements and fire apparatus access easements.
  - b) The identification of exterior building materials and paint colors for the review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with the requirements of the Site Design and Use Standards, and the colors and materials selected shall be consistent with those approved with the application.

- c) Specifications for all exterior lighting fixtures. Exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
- d) Revised Landscape, Irrigation and Tree Protection Plans shall be provided for the review and approval of the Staff Advisor with the building permit submittals. This plan shall address: 1) the recommendations of the Tree Commission from their January 5<sup>th</sup>, 2012 meeting where consistent with the Site Design and Use Standards and with final approval by the Staff Advisor; 2) the identification of 24 mitigation trees to be planted on site or details of alternative mitigation measures proposes; 3) the required irrigation plans, including the requirements for programmable automatic timer controllers and a maintenance watering schedule with seasonal modifications; 4) the identification of parking lot landscaping equivalent to at least seven percent of the parking surface area, at least one parking lot tree for each seven parking spaces, and required landscape buffers and screening consistent with the Parking Lot Landscaping and Screening Standards. The applicants shall also obtain the required plumbing permits and inspections for installation of the required double-check valve(s) associated with the irrigation system.
- e) That a revised stormwater drainage plan, including any necessary on-site detention measures, shall be provided for the review and approval of the Engineering, Building and Planning Departments with the building permit submittal. The drainage plan shall be designed to ensure that post-development peak stormwater flows are less than or equal pre-development levels as required by the Engineering Division. In addition, the plans shall incorporate above ground stormwater treatment and infiltration facilities consistent with the parking lot design requirements of the SOU Plan for the new parking lots to be constructed.
- f) That a final utility plan for the project shall be provided for the review and approval of the Engineering, Planning and Building Divisions. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins. Any necessary service upgrades shall be completed by the developer at developer's expense.
- g) The applicant shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric, Engineering, Building and Planning Departments prior to the issuance of demolition, excavation or building permits. Transformers, cabinets and vaults shall be located in areas least visible from streets and sidewalks, while considering the access needs of the Electric Department.

- h) That a pedestrian circulation plan for the new North Campus Village area shall be provided which identifies an integrated pedestrian pathway system, including a new connection from the southeast corner of the Greensprings to the new South Hall's south entry and to the intersection of Ashland and Stadium Streets, with routes and material treatment of the paths, landscaping including the new gateway plaza treatment, pedestrian scale lighting, and way finding measures clearly detailed.
  - i) Revised plans to address Fire Code requirements including approved addressing; fire apparatus access, turn-around, angle of approach; fire flows; fire department connections; fire hydrant distance to structures and clearance; stairway access to roof; firefighter access pathway; emergency responder radio coverage provisions; key box; and storage and collection of combustible and recycle materials. If a fire protection vault is required, the vault shall not be located in the sidewalk. In keeping with city standards and the SOU Plan, which both seek to maintain a pedestrian scale for campus improvements, driveways and other access ways shall be limited to no more than the minimum required under the driveway standards in AMC 18.92, with any additional width necessary to accommodate fire apparatus requirements to be achieved through alternative treatments such as a rolled curb and materially distinct sidewalk next to the paving, or the use of grasscrete or similar alternative treatments which will support fire apparatus access in a manner acceptable to the Fire Marshal and Staff Advisor without requiring wider than necessary swaths of paving. Fire apparatus and their width, radius and material treatment shall be clearly identified in the building permit submittals.
- 6) That prior to the issuance of the building, excavation or demolition permits or the commencement of site work or storage of materials:
- a) A Tree Verification Permit shall be obtained, and tree protection measures installed, inspected and approved by Staff Advisor. The Verification Permit is to inspect the identification of trees to be removed and the installation of tree protection fencing for the trees to be retained and protected on and adjacent to the site. Tree protection measures shall be in the form of chain link fencing six feet tall, installed and maintained in accordance with the requirements of AMC 18.61.200.B. The project arborist's recommendations and specifications, as detailed in the submittal materials dated November 1, 2011 from Thomas M. Myers, and subsequent revisions, shall be conditions of approval.
  - b) The approval of a Demolition/Relocation Review and associated permits and inspections shall be obtained from the Building Division prior to demolition of existing structures.
  - c) That prior to the issuance of a building permit, the applicants shall submit civil design drawings for the implementation of improvements detailed in the materials provided by Kittelson & Associates (included as part of the

application in the Supplemental Land Use Information dated December 5, 2011) and modified with this decision for the review and approval of the Public Works, Engineering and Planning Departments and Oregon Department of Transportation. These civil plans are to be reviewed and approved prior to the issuance of building permits, and required improvements are to be completed according to the approved plan, inspected and approved prior to the issuance of a final occupancy permit. The required improvements include public and private street improvements and pedestrian crossing treatments as detailed more completely below:

- Replace existing pedestrian signal heads with countdown heads and the add a five-second leading pedestrian phase to signal timing for the intersections of South Mountain Avenue & Siskiyou Boulevard, and of Ashland Street & Siskiyou Boulevard.
- Replace existing flashing beacons with rectangular rapid flash beacons (RRFBs) for the University Way pedestrian crossing of Siskiyou Boulevard. (*Adjacent street trees be pruned and maintained by the Parks Department to improve visibility.*)
- Replace existing flashing beacons with rectangular rapid flash beacons (RRFBs), and relocate the “Stop Here for Pedestrians” sign at the westbound approach of the Avery Street pedestrian crossing of Siskiyou Boulevard to provide adequate clearance for the crosswalk signage and beacons. (*City Street Department crews to complete the sign relocation.*)
- Replace existing flashing beacons with rectangular rapid flash beacons (RRFBs) at the Garfield Street and Bridge Street pedestrian crossings of Siskiyou Boulevard.
- Replace the existing pedestrian signal heads with pedestrian countdown heads which include an audible indicator of signal timing, and reconfigure the “gateway” intersection of Siskiyou Boulevard with Wightman and Indiana Streets with a single diagonal crosswalk as shown in conceptual drawings presented at the December 15<sup>th</sup> Transportation Commission meeting as new “Figure 12”, and install high visibility pavement markings for a diagonal crossing. The crossing treatment is to be designed to minimize the time automobiles wait due to the 36 second pedestrian-activated “scramble phasing”, and will also require that the applicants upgrade the existing signal cabinets and controllers at both Wightman Street and Ashland Street (Highway 66) to accommodate these signal modifications.
- Install advance pedestrian signs and rectangular rapid flash beacons at the Stadium Street pedestrian crossing of Ashland Street/Highway 66; explore and if warranted and feasible implement alternative crosswalk placement and median configuration to address concerns with pedestrian crossings conflicting with left turning movements from Market of Choice site onto Ashland Street.
- Replace pedestrian signal heads with pedestrian countdown signal heads and add a five-second lead time to the pedestrian phasing of the signal at the intersection of Ashland Street and Walker Avenue.

- Install city standard seven to eight foot width parkrows with street trees, five to six foot sidewalks, and pedestrian scale street lighting on both sides of Stadium Street from Webster Street to South College Way consistent with city standards for a neighborhood street, with the sidewalk improvements to transition to curbside to preserve established trees between South College Way and Ashland Street.
- 7) That prior to the final approval of the project and issuance of a certificate of occupancy:
- a) That all landscape improvements and the irrigation system shall be installed according to the approved plan, inspected, and approved by the Staff Advisor.
  - b) All service and equipment installations shall be completed according to Electric, Engineering, Planning, and Building Departments' specifications, inspected and approved by the Staff Advisor.
  - c) The screening for the trash and recycling enclosure shall be installed in accordance with the approved plan, inspected and approved by the Staff Advisor. An opportunity to recycle site of equal or greater size than the solid waste receptacle shall be identified in the building permit submittals and shall be in place, inspected and approved by the Staff Advisor.
  - d) The requirements of the Ashland Fire Department including approved addressing; fire apparatus access, turn-around, angle of approach; fire flows; fire department connections; fire hydrant distance to structures and clearance; stairway access to roof; firefighter access pathway; emergency responder radio coverage provisions; key box; and storage and collection of combustible and recycle material requirements shall be satisfactorily addressed prior to issuance of a certificate of occupancy.
  - e) All public and private street improvements including but not limited to the installation of sidewalks, parkrows with street trees and standard street lighting on both sides of Stadium Street shall be installed to City of Ashland standards under permit from the Public Works Department in accordance with the approved plan, inspected and approved by the Staff Advisor.
  - f) All hardscape improvements including parking, driveways and off-street pathways shall be installed according to the approved plans prior to issuance of the final certificate of occupancy.
  - g) That the bicycle parking facilities shall be installed according to the approved plans, inspected, and approved by the Staff Advisor prior to the issuance of the certificate of occupancy. The building permit submittals shall verify the design and placement of covered bicycle parking both indoors and outdoors as described in the application.

- h) That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent residential properties.

## April Lucas

---

**From:** Colin Swales [colinswales@gmail.com]  
**Sent:** Tuesday, January 03, 2012 11:22 AM  
**To:** Derek Severson; Brandon Goldman  
**Cc:** April Lucas  
**Subject:** SOU - PA #2011-01576

RECEIVED  
JAN 03 2012

Derek, Brandon,  
(cc April)

At our recent Transportation Commission meeting it was suggested that any member who had individual thoughts on the SOU planning action #2011-01576 should submit them to you for inclusion in the Record. In that spirit, I herewith offer some further suggestions, but only as an interested citizen, and these should not be construed as representing in any way the Ashland Transportation Commission or its membership.

Could you also please print out my *ODOT - Ashland ADTs* spreadsheet at [ [https://docs.google.com/spreadsheet/ccc?key=0AIIUpGpSBunvdFQ1a2RUWUEyc2hLbVFfaOWk5UFNpVIE&hl=en\\_US#gid=0](https://docs.google.com/spreadsheet/ccc?key=0AIIUpGpSBunvdFQ1a2RUWUEyc2hLbVFfaOWk5UFNpVIE&hl=en_US#gid=0) ] and include in the Record, as I am currently out of town and unable to bring into your office a hard copy.

Planning Commissioners,

### 1. Gateway ( also mentioned in my Dec 9th submittal)

The SOU master Plan included some ideas for a highway "gateway" at each end of the SOU Campus. The "gateway" treatment of the Indiana/Wightman junction in my opinion leaves a lot to be desired. This busy junction is invisible from vehicles being driven up Ashland Street from the south. I would suggest that SOU street banners on the university's Ashland Street frontage be used to alert drivers to the existence of busy pedestrian presence.

Changes to the pavement surface treatment ( as suggested in the master plan) should also be installed. Small-unit pavers, green painting of the bike lanes, Sidewalk and parkrow to City Standards along the entire Ashland Street, Siskiyou frontage to SOU should all be seriously considered. Upgrades to the ped crossings on Ashland Street to improve visibility, pedestrian refuges etc should be looked at both at Stadium, Walker and where the proposed new multi-use path exits on Ashland Street near the junction with Siskiyou Bld. Many more students will be crossing here to access the market, Beanery and other stores in the vicinity, but on the opposite side of Ashland Street

### 2. Lighting

After the tragic fatality of a SOU student at the Garfield crossing much discussion ensued about the inadequate lighting. The overhead cobra heads and the pedestrian scale lighting fixtures only glare into the drivers eyes and do little to illuminate pedestrians waiting to cross or actually in the crosswalk. ( Note: more of these were installed by SOU recently to light this dark stretch). This situation is especially bad if pedestrians are wearing dark clothing. Each crosswalk should have a carefully shrouded spotlight that is focused on the actual marked zebra crossing and anyone that is walking across. The current visual blight of the excessive sign pollution only serves to distract drivers from seeing actual people using this area.

The chiarascuro dazzling caused by the low winter sun coming through the gaps in the tall evergreen trees fronting the university I feel was perhaps a major cause of the Jiminez fatality, as she seems to have already crossed 4 of the five lanes yet been totally invisible to the driver that ran her down and threw her 50 ft down the road. I offer no solutions to this daylight dazzle problem except removing the trees, but somehow this

pedestrian invisibility needs to be addressed perhaps by education so that pedestrians crossing Siskiyou Blvd. do not have a false sense of security

### **Multiple Lanes needed?**

ODOT's own historic traffic counts show that the traffic on this arterial through the heart of our university and High school is in fact *decreasing* (Please see attached spreadsheet ODOT - Ashland ADTs).

Siskiyou Boulevard and Ashland Street do not have sufficient traffic to justify the current 4/5 lanes for 25 mph traffic. Should this even be a school zone 20 mph max?

A **road diet** to further calm auto traffic should be considered with the outer lanes perhaps used as a dedicated Bike Boulevard or shuttle bus lane. This would decrease pedestrian crossing distances and eliminate the current very dangerous problem of a driver in the overtaking lane not seeing a pedestrian or noticing the stopped car in the other lane - a common occurrence at present.

### **Pedestrian Place**

It is unfortunate that this planning action just preceded by a few weeks the recent changes that were passed under the rubrik of "Pedestrian Places". (Although I understand that those changes are to be soon looked at again by Council).

The inclusion of the original pedestrian-rich Bridge-Wightman area would have been better than the alternate Tolman area that was substituted. With this new proposal, the **huge increase in pedestrian activity** necessitates that this area is also included for consideration as a "pedestrian place", perhaps by voluntary offerings from SOU.

The proposed "pedestrian scramble" seems to be predicated on the assumption that the auto has the overall priority. This area should indeed be multi-modal and would be perfect for a truly "Shared Space".

ODOT traffic engineers assure me that the City's jurisdiction of this stretch of highway gives them total control over creative options to improve safety for all users of this public right-of-way, without necessarily adhering to the usual straight-jacket of ODOT's own one-size-fits-all rules.

**P.S.** The 2-minute video that I mentioned at the last PC meeting, showing empty-road driving conditions on a typical weekday at 10:00 a.m. in front of SOU and the near-accident I then witnessed at Walker intersection, can now be viewed online at:

[ <http://youtu.be/Z4ecVgovS4I> ]

Respectfully submitted 1/3/12

Colin Swales  
143 Eighth Street, Ashland, OR 97520  
[\(541\) 840 7492](tel:(541)8407492)

RECEIVED

JAN 03 2012

Mile Post		2,001	2,002	2,003	2,004	2,005	2,006	2,007	2,008	2,009	2,010	2010 % below decade peak
<b>Pacific Hwy 1 (INTERSTATE I-5)</b>												
0.00	Oregon-California State Line	13,800	15,000	15,100	14,800	14,700	14,800	14,800	14,800	14,800	14,700	-3%
5.02	0.30 mile south of Siskiyou Interchange	13,900	15,000	15,100	14,700	15,800	14,800	14,700	14,800	14,800	14,600	-3%
11.03	Neil Creek Automatic Recorder, Sta. 15-002	14,700	16,000	16,100	15,800	15,800	15,900	15,800	14,800	15,100	14,900	-7%
13.67	0.50 mile south of Green Springs Highway (ORE66)	13,900	15,000	15,200	14,900	14,900	15,000	15,000	14,100	14,300	14,200	-7%
18.60	0.50 mile south of North Ashland Interchange	24,700	26,800	27,100	27,000	26,000	26,300	26,200	23,500	24,400	24,000	-11%
19.87	0.77 mile north of North Ashland Interchange	37,000	39,800	40,100	41,400	34,900	35,400	35,200	33,400	34,700	34,800	-16%
<b>ROGUE VALLEY HIGHWAY NO. 63 (Hwy 99)</b>												
15.82	0.44 mile northwest of Talent Road	9,400	9,400	9,700	9,400	10,500	9,300				8,800	-16%
17.03	0.01 mile south of Valley View Road	19,100	19,100	18,500	19,900	19,300	18,200	18,700	17,600	18,200	17,400	-13%
18.64	0.02 mile north of Hersey Street	19,800	19,800	19,800	19,400	18,800	17,800	19,400	18,300	18,900	18,300	-8%
	<b>North city limits of Ashland</b>						16,100					
18.70	0.01 mile south of Van Ness Street	17,800	17,800	18,000	17,600	17,100	16,200	16,800	15,800	16,300	15,600	-13%
18.98	0.05 mile north of Helman Street	18,100	18,100	18,200	17,700	17,200	16,200	17,000	16,000	15,500	15,100	-17%
<b>DOWNTOWN Couplet Southbound (One-Way traffic on East Main Street)</b>												
19.06	0.01 mile south of Church Street	13,000	13,000	13,200	12,900	12,500	11,800	11,500	10,800	11,100	10,500	-20%
19.24	0.01 mile south of Pioneer Street	13,700	13,700	13,900	13,600	13,200	12,400	11,200	10,500	10,800	10,400	-25%
19.41	0.01 mile south of 2nd Street	13,400	13,400	13,600	13,300	12,900	12,200	12,000	11,300	11,700	10,200	-25%
19.16	0.01 mile north of Oak Street	15,400	15,400	15,500	15,100	14,600	13,800	12,200	11,500	11,800	11,000	-29%
<b>DOWNTOWN Couplet northbound One-Way traffic on "C" Street (Lithia Way)</b>												
19.22	0.01 mile south of Pioneer Street	13,900	13,900	14,000	13,700	13,300	12,500	11,500	10,800	11,200	10,400	-26%
19.48	0.01 mile south of 3rd Street	13,700	13,700									
<b>RESUME TWO-WAY TRAFFIC On Siskiyou Boule</b>												
19.62	0.01 mile north of Union Street	20,500										
19.88	0.01 mile north of Liberty Street	20,000			19,400			3/1/2008 *	16,785			-16%
								* Ashland City	- not ODOT recorder			
20.16	0.01 mile south of Mountain Avenue	22,800										
20.58	0.01 mile north of Indiana Street	19,300										
20.64	0.01 mile south of Green Springs Highway (ORE66)	11,500										
20.85	0.01 mile south of Walker Avenue	10,100	10,100	10,200	9,900	9,600	9,100	8,900	8,400	8,700	8,300	-19%
21.55	0.02 mile north of Clay Street	7,400	7,400	7,500	7,100	6,900	6,500	6,600	6,200	6,400	6,000	-20%
21.92	South city limits of Ashland, 0.05 mile south of Tolm	3,600	3,600	3,700	3,500	3,400	3,200	3,100	2,900	3,000	2,800	-24%
22.41	0.01 mile south of Crowson Road	1,900	1,900	1,900	1,700	1,600	1,600	1,500	1,400	1,500	1,600	
23.52	0.60 mile north of Pacific Highway (I-5), at South A	1,700	1,700	1,700	1,500	1,500	1,400	1,300	1,200	N/A	N/A	
<b>GREEN SPRINGS HIGHWAY NO. 21 (i.e. Ashland Street starting at Siskiyou Blvd.)</b>												
0.01	0.01 mile east of Rogue Valley Highway (ORE99)	10,500	10,500									
0.19	0.01 mile east of Walker Avenue	13,000	13,000									
0.46	0.01 mile east of Normal Avenue	13,900	13,900									
1.03	0.01 mile west of Tolman Creek Road			13,900	13,800	13,000	12,800	11,800	11,100	11,600	9,500	-31%
1.27	0.07 mile west of Pacific Highway (I-5)	14,100	14,100	14,500	14,900	14,100	13,800	13,400	12,600	13,800	10,800	-26%
1.38	0.04 mile east of Pacific Highway (I-5)	8,700	8,700	8900p	9,100	8,600	8,400	8,900	8,400	9,500	7,700	-19%
1.70	East city limits of Ashland, 0.12 mile west of E. Mai	6,400	6,400	6,600	6,800	6,400	6,300	6,800	6,400	6,700	6,100	-10%

COMMENTS REGARDING THE  
SOU STUDENT HOUSING DEVELOPMENT PLAN\* 2011-2012

TO: ASHLAND PLANNING COMMISSION  
FROM: TOM BURNHAM  
1344 APPLE WAY  
ASHLAND, OR 97520  
Email: [ntburnham@gmail.com](mailto:ntburnham@gmail.com)  
541-482-4467

RECEIVED

JAN 03 2012

City of Ashland  
Field \_\_\_ Office \_\_\_ County \_\_\_

\* THE FOLLOWING COMMENTS REFLECT MY PERSONAL OPINIONS AND NOT NECESSARILY THOSE OF THE ASHLAND TRANSPORTATION COMMISSION OF WHICH I AM A MEMBER.

1. I am concerned about the safe and efficient crossing of Siskiyou Boulevard by pedestrians and cyclists. Siskiyou is a 4 lane Boulevard with some of the busiest vehicular traffic in Ashland. It is wide (4 driving lanes, turn lanes, plus a center median.)

With this new project, Siskiyou Blvd. will be passing through the center of the SOU's campus. Additional campus growth will be on this north side of Siskiyou. The University has a sizeable amount of property that can be developed in this north area. More building means more people.

2. I question the pedestrian counts at the busiest crossing on Siskiyou - the intersection of Siskiyou, Wightman, and Indiana. Has there been an accurate count during the lunch time? The new dining facility, which will be the only major eating facility on campus is proposed for this new development (North side of Siskiyou.)

Have the pedestrian count projections included local non student residents, commuting students and SOU staff that park on the local streets in this particular area and would require crossing Siskiyou?

Does the plan take into consideration the effect of motor vehicle traffic during athletic events that take place in this area. This includes students using local streets for parking and the effect of additional pedestrian use on both sides of Siskiyou. I have not seen any vehicle or pedestrian counts regarding this.

3. Crossing Siskiyou Boulevard via the proposed crosswalk developed by Planning staff appears, at first glance, to be pretty simple and easy to accomplish. I see a problem with pedestrians attempting to get from the north/west corner of Siskiyou and Wightman to the south/east corner of Siskiyou and Indiana (check your drawing).

This would involve: 1. Crossing Wightman's 2 lanes (signalized) 2. Crossing Siskiyou's 5 lanes plus bike lanes (signalized), 3. Crossing Indiana's 3 lanes (signalized). I have no idea how long that would take and if it is a long time, pedestrians and cyclists might decide to "shortcut" this intersection.

4. The idea of a pedestrian/bicycle bridge should still be seriously considered. If not right now, at least it should be put in the plan for the near future by providing the needed space for its construction. This would allow for a continuous flow for pedestrians and cyclists regardless of vehicle traffic and in my opinion a "no brainer". And, vehicle traffic would be able to move at a smoother pace as traffic signals would not have to contend with pedestrians. I understand that cost is an issue. SOU, ODOT, and the City should start to work on grants or other sources to fund this.

We all realize that the north side of Siskiyou will be where SOU will eventually grow more.

5. I have not seen any plans for a campus shuttle service. Getting from one end of campus to the other certainly deserves this. It could cut pedestrian walking and move folks in an efficient manner. I would prefer to see as much walking as possible, but sometimes that can't happen.

Overall the planning process was scheduled according to State statutes, but adequate time to review the project has not been given to both the Planning Commission and Transportation Commission. After all, this is a fairly large project with many future implications

Thank you for taking the time to review and listen to these concerns.

Tom Burnham

Copy: Transportation Commission

**RECEIVED**

JAN 03 2012

City of Ashland  
Field\_\_\_\_Office\_\_\_\_County\_\_\_\_

*Attention: Derrick: please put in packets for Jan. 10 meeting*

590 Glenview Drive  
Ashland, Oregon 97520  
December 27, 2011

Dear Ashland Planning Commission:

As an alumni of the Planning Commission, serving eight years ending in 2005, I feel obliged to offer an opinion regarding the proposed student housing on the SOU campus. (I attended all the citizen meetings during the SOU 10 year plan proposals.)

PLEASE DO NOT PERMIT THE "CONDITIONAL USE" REQUESTS IN REGARDS TO THE PROPOSED LENGTH AND HEIGHTS OF THE DORM STRUCTURES.

If 250' length for residential campus uses is allowed, the 400' length proposal is far beyond an acceptable "conditional use". Also, if the height limit is 40' and the proposed "average" heights of the hip roof is 49', it is not acceptable.

The backside facades of the two structures which face the courtyard present an unbroken 400' mass that have the character of Russian apartment blocks, or, the NYC "projects" which have proven unacceptable living spaces.

Architectural tweaks might mitigate the problem. I'll be so bold as to make a suggestion: If the 400' facades were broken by end "wings" that match the courtyard facades, or by a central entrance "wing", (or all three wings), the unacceptable length would be mitigated. Furthermore, if the "wings" were not full height, but two stories instead, the "average" allowable height would be mitigated. (Adding the wings on the 400' facades could make up floor space lost to the lower wing heights). Additionally, perhaps the small hip gables over the "bump outs" could be straight gables with additional tall windows. Mainly, this feature would break up the enormous mass of the red roof.

*See attached drawings*

The effect would present structures that seem to grow organically on the site and rise like the beautiful foothills behind. It is more like Frank Lloyd Wright's principle: "of the site, not on it". This mitigation would increase the structural cost, but our existing ordinances would be much more closely followed.

When I was on the Planning Commission, setting a precedent was avoided. Our ordinances yield the small town atmosphere touted in our Comprehensive Plan. These dorms could last a hundred years and absolutely must fit into the site. PRIDE OF PLACE never should be overlooked.

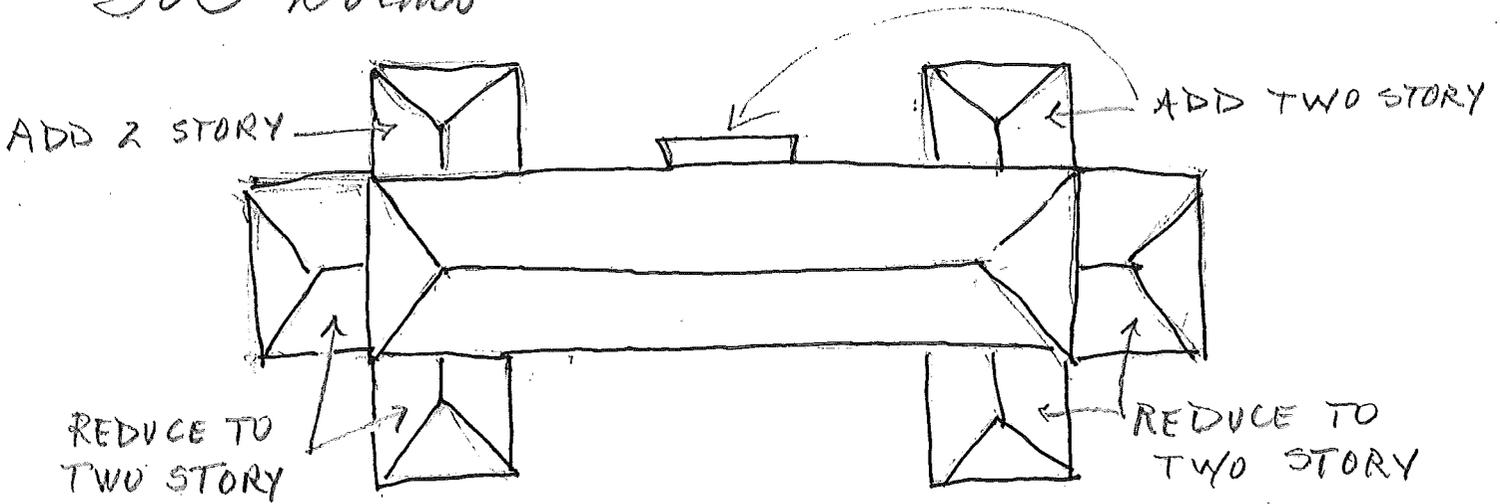
*Respectfully,  
Marilyn Briggs*

RECEIVED

DEC 28 2011

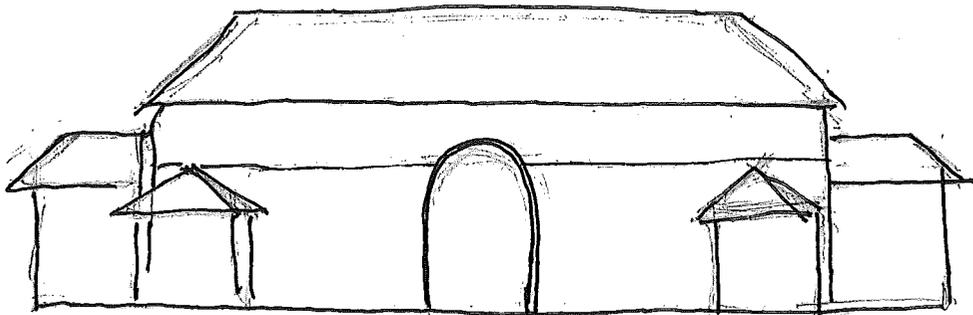
City of Ashland  
Field \_\_\_\_\_ Office \_\_\_\_\_ County \_\_\_\_\_

Derrick: Please do computer scale drawings  
 Suggested Modifications to Proposed  
 SOC Dorms

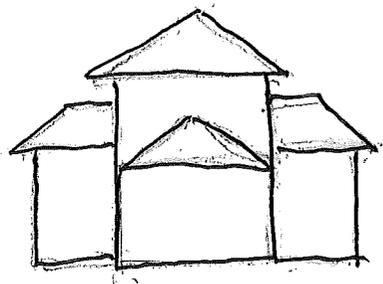


ROOF PLAN

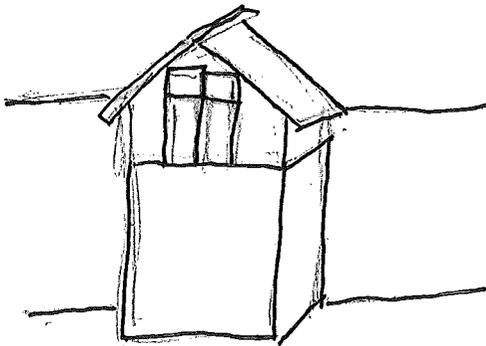
(THIS IS DORM ACROSS FROM GYM, REVERSE PLAN OPPOSITE DORM)



NORTH OR SOUTH ELEVATION



EAST OR WEST



GABLE BAY  
 (INSTEAD OF HIP)

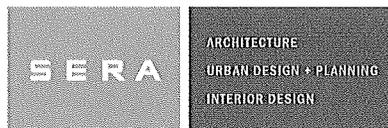
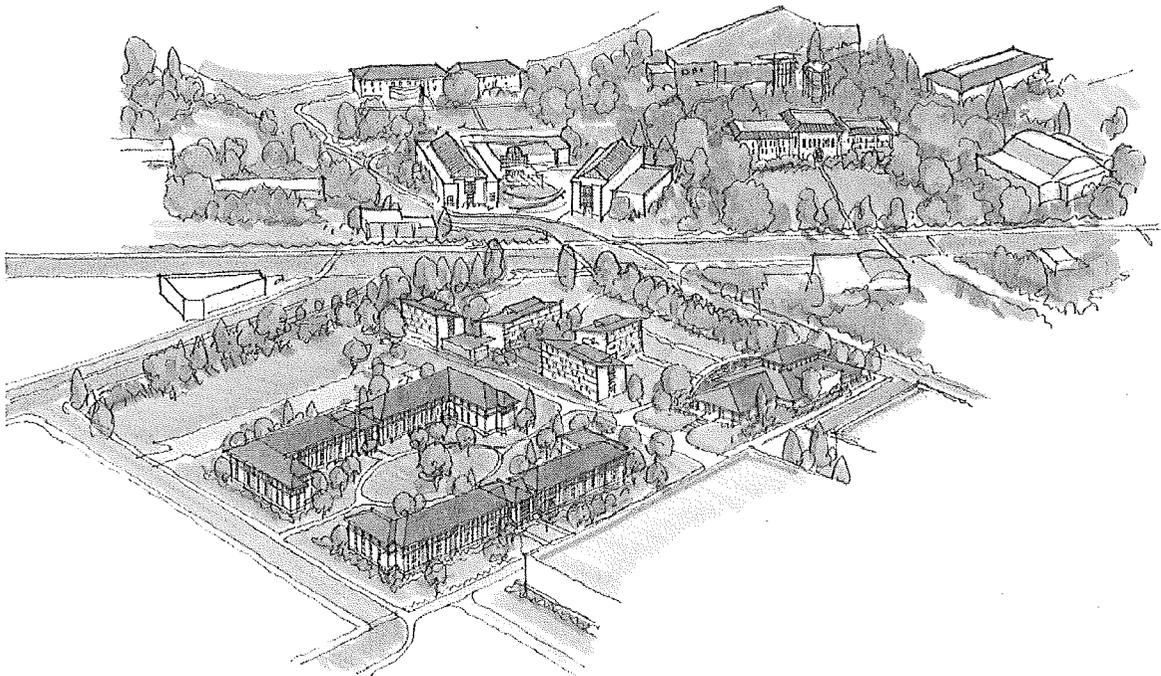
**RECEIVED**

DEC 28 2011

City of Ashland  
 Field \_\_\_ Office \_\_\_ County \_\_\_

# SOUTHERN OREGON UNIVERSITY STUDENT HOUSING DEVELOPMENT

Revised Site Review Application  
Conditional Use Permit Application  
December 29, 2011



RECEIVED

DEC 29 2011

City of Ashland

## SITE REVIEW REVISED SUBMISSION

### Plan Requirements

The following information is being provided in response to the initial Planning Commission Hearing comments for consideration:

### Drawings (SEE ATTACHED SUBMITTAL)

SHEET L001 (revised):

*Update showing Stadium St parkrow, asphalt sidewalk in south lawn connecting to Stadium*

SHEET L002 (revised):

*Additional trees inventoried along Ashland St*

SHEET L003 (revised):

*Planting updates for Stadium St parkrow, trash compactor screening*

SHEET L004 (revised):

*Planting updates for Stadium St parkrow*

SHEET L005 (new):

*New exhibit showing tree removal along Ashland St if a new parkrow is required*

RENDERING 1 (new):

*South elevation perspective view of South Residence Hall*

RENDERING 2 (new):

*Enlarged elevation of South Residence Hall building entry pavillion*

SITE RENDERING (revised):

*Updated to show Stadium St improvements, asphalt sidewalk in south lawn connecting to Stadium, and landscape improvements as outlined above*

### Written Material

1. Southern Oregon University Transportation Demand Management Plan Update, Dated December 28, 2011
2. Revised SOU Tree Inventory, Dated December 26, 2011.

RECEIVED

DEC 30 2011

City of Ashland

Tree									
Tree #	Species	DBH in	Height in	Crown	Condition	Tolerance	Tree protection	Notes	Remove
1		16	40	15	Good	moderate	12		Remove
2	Norway spruce	11	25	8	Good	moderate	8.25		Remove
3	Blue spruce	12	30	12	Good	moderate	9		Remove
4	Black pine	14	40	12	Good	good	7		Remove
5	Shore pine	20	35	11	Fair	good	10	Leaning	Remove
6	Blue spruce	15	29	6	Good	moderate	11.25		Remove
7	Honey locust	12	30	12	Fair	good	6	Trunk wound	Remove
8	Black locust	7	35	9	Poor	good	3.5	Multiple stemmed	Remove
9	Honey locust	10	25	13	Fair	good	5	Included bark co	Remove
10	ponderosa	20	40	17	Good	good	10		Remove
11	Black pine	11	30	12	Good	good	5.5		Remove
12	Shore pine	18	38	15	Fair	good	9	Severe lean	Remove
13	Silver maple	36	32	45	Fair	moderate	36	Broken scaffold	Protect
14	Shore pine	21	40	14	Good	good	10.5		Remove
15	Himalayan	19	40	15	Good	good	9.5		Remove
16	Ponderosa	15	30	14	Good	good	7.5		Remove
17	Port orford	14	30	13	fair	good	7	Clorotic	Remove
18	Siberian elm	37	38	36	Fair	good	18.5	Broken scaffold	Remove
19	Ash	8	24	15	Good	moderate	6		Protect
20	Red Maple	16	28	18	Good	moderate	12		Protect
21	Red Maple	11	27	15	Good	moderate	8.25		Protect
22	Red Maple	7	20	11	Good	moderate	5.25		Protect
23	Red Maple	9	24	13	Good	moderate	6.75		Protect
24	Red Maple	8	23	12	Fair	moderate	6	Trunk wound	Protect
25	Blue spruce	7	28	5	Good	good	5.25		Remove
26	Shore pine	9	28	7	Good	good	6.75		Remove
27	ponderosa	10	31	9	Fair	good	7.5	Clorotic	Remove
28	Shore pine	9	27	10	Good	good	6.75		Remove
29	Red Maple	6	27	5	Good	moderate	4.5		Remove
30	Pagoda Tree	12	26	14	Good	moderate	9		Protect
31	Deodar cedar	18	42	20	Good	good	13.5		Remove
32	Red Maple	15	15	25	Good	moderate	11.25		Remove
33	Red Maple	10	10	28	Good	moderate	7.5		Remove
34	Silver maple	17	16	32	Good	moderate	12.75		Remove
35	Tulip Tree	11	10	36	Good	good	8.25		Remove
36	Deodar cedar	28	28	40	Good	good	28.75		Protect
37	English laurel	10	12	14	Good	good	5		Remove
38	Plum	5	6	15	fair	moderate	3.75		Remove
39	Plum	4	6	15	fair	moderate	3		Remove
40	Plum	4	6	15	fair	moderate	3		Remove
41	Red maple	15	38	15	good	moderate	11.25		Remove
42	Red maple	12	28	12	fair	moderate	9		Remove
43	Red maple	11	36	11	Fair	moderate	8.25		Remove
44	Red maple	14	36	15	Good	moderate	10.5		Remove
45	Plum	10	22	9	Good	moderate	7.5		Remove
46	Bartlet pear	6	22	6	Good	moderate	4.5		Remove
47	Shore pine	15	20	15	Good	good	11.25		Remove
48	Hawthorne	7	18	7	Good	moderate	5.25		Remove
49	Shore pine	10	24	9	fair	good	5		Remove
50	Birch	6	26	6	Good	moderate	4.5		Remove
51	Sycamore	20	32	30	Good	moderate	20		Remove
52	Siberian elm	24	35	26	Good	good	18		Remove
53	Siberian elm	25	32	27	Good	good	18.75		Remove
54	Deodar cedar	18	40	21	Good	good	13.5		Remove
55	Deodar cedar	16	40	18	Good	good	12		Remove
56	Deodar cedar	14	40	15	Good	good	10.5		Remove
57	Austrian pine	17	34	14	Good	good	12.75		Remove
58	Austrian pine	14	32	12	Good	good	10.5		Remove
59	Austrian pine	13	30	13	Good	good	9.75		Remove
60	Zelkova	13	22	15	Good	good	9.75		Protect
61	Pagoda Tree	8	24	9	Good	moderate	6		Protect
62	Austrian pine	11	32	12	Good	good	8.25		Protect
63	Leland cypress	20	52	15	Good	good	15		Protect

Tree #	Species	DBH in	Height in	Crown	Condition	Tolerance	Tree protection	Notes	Remove
64	Plum	7	22	6	Good	moderate	5.25		Protect
65	Leland cypress	18	42	15	Good	good	13.5		Protect
66	Leland cypress	27	38	22	Good	good	20.25		Protect
67	Leland cypress	27	44	23	Good	good	20.25		Protect
68	Leland cypress	25	42	21	Good	good	18.75		Protect
69	Sycamore	20	40	28	Good	moderate	15		Protect
70	Giant	44	44	22	Good	moderate	33		Protect
71	Catalpa	25	39	21	Good	moderate	18.75		Protect
72	Sycamore	20	42	22	Good	moderate	15		Protect
73	Sycamore	19	40	19	Good	moderate	14.25		Protect
74	Sycamore	10	36	18	Good	moderate	7.5		Protect
75	Sycamore	12	35	12	Good	moderate	9		Protect
76	Sycamore	15	36	15	Good	moderate	11.25		Protect
77	Sycamore	10	35	10	fair	moderate	7.5		Protect
78	Sycamore	10	36	11	Good	moderate	7.5		Protect
79	Sycamore	7	26	9	fair	moderate	5.25		Protect
80	Sycamore	12	35	12	Good	moderate	9		Protect
81	Sycamore	10	28	9	Good	moderate	7.5		Protect
82	Sycamore	11	36	12	Good	moderate	8.25		Protect
83	Sycamore	9	30	11	Fair	moderate	6.75		Protect
84	Sycamore	12	35	13	Good	moderate	9		Protect
85	Sycamore	9	30	11	Fair	moderate	6.75		Protect
86	Sycamore	11	35	12	Good	moderate	8.25		Protect
87	Sycamore	9	29	10	Fair	moderate	6.75		Protect
88	Sycamore	9	30	12	Good	moderate	6.75		Protect
89	Sycamore	7	32	12	Fair	moderate	5.25		Remove
90	Sycamore	11	34	15	Good	moderate	8.25		Remove
91	Sycamore	10	33	12	Good	moderate	7.5		Remove
92	Sycamore	10	34	12	Good	moderate	7.5		Remove
93	Sycamore	10	33	12	Good	moderate	7.5		Remove
94	Sycamore	13	33	12	Good	moderate	9.75		Remove
95	Sycamore	21	32	16	Good	moderate	15.75		Remove
96	Sycamore	13	32	18	Fair	moderate	9.75		Protect
97	Sycamore	13	32	18	Fair	moderate	9.75		Protect
98	Sycamore	13	32	20	Fair	moderate	9.75		Protect
99	Sycamore	12	42	30	Fair	moderate	9		Protect
100	Sycamore	12	33	15	Fair	moderate	9		Protect
101	Sycamore	11	30	13	Fair	moderate	8.25		Protect
102	Sycamore	11	36	12	Good	moderate	8.25		Protect
103	Sycamore	15	33	17	Good	moderate	11.25		Protect
104	Red maple	13	32	13	Fair	moderate	9.75		Remove
105	Red maple	11	30	14	Good	moderate	8.25		Remove
106	Red maple	14	36	20	Good	moderate	10.5		Remove
107	Red maple	11	40	12	Good	moderate	8.25		Remove
108	Red maple	12	38	12	Good	moderate	9		Remove
109	Red maple	12	33	12	Good	moderate	9		Remove
110	Red maple	13	35	17	Good	moderate	9.75		Remove
111	Tulip tree	11	42	7	Good	good	5.5		Remove
112	Tulip tree	9	40	6	Good	good	4.5		Remove
113	Tulip tree	20	44	16	Good	good	10		Remove
114	Tulip tree	17	45	15	Good	good	8.5		Remove
115	Black pine	11	30	15	Good	good	5.5		Remove
116	Black pine	22	30	21	Good	good	16.5		Remove
117	Red oak	23	42	21	Good	good	17.25		Remove
118	Red oak	26	45	33	Good	good	19.5		Remove
119	Box Elder	24	38	28	Good	moderate	18		Protect
120	Arbor Vitea	8	21	5	Good	moderate	6		Remove
121	Arbor Vitea	9	22	6	Good	moderate	6.75		Remove
122	Golden locust	16	30	17	Fair	good	8	Topped	Remove
123	Black locust	7	31	12	Fair	good	3.5	Codominant stems	Remove
124	Black locust	8	32	11	Fair	good	4	Codominant stems	Remove
125	Blue spruce	18	26	9	Fair	moderate	13.5	Topped	Remove
126	Siberian elm	14	38	15	Fair	good	10.5	Codominant stems	Remove
127	Plum	7	15	6	Fair	moderate	5.25		Remove

Tree #	Species	DBH in	Height in	Crown	Condition	Tolerance	Tree protection	Notes	Remove
128	Black locust	21	48	15	Fair	good	10.5	Codominant top	Remove
129	Black locust	21	48	15	Fair	good	10.5		Remove
130	Apple	6	18	6	Poor	good	3		Remove
131	Red maple	13	33	18	Good	good	6.5		Remove
132	Incense cedar	21	48	9	Good	moderate	15.75		Remove
133	Incense cedar	21	48	9	Good	moderate	15.75		Remove
134	Norway maple	8	36	14	Fair	moderate	6	Multitrunk	Remove
135	Douglas fir	15	30	12	Poor	moderate	11.25	Dead top	Remove
136	Mulberry	9	33	18	Fair	good	4.5		Remove
137	Mulberry	10	35	14	Good	good	5		Remove
138	Ponderosa	7	25	5	Good	good	3.5		Remove
139	Birch	13	33	15	Dead	moderate	9.75		Remove
140	Big leaf Maple	6	21	5	Good	poor	4.5		Remove
141	Incense cedar	37	62	14	Good	moderate	27.75		Remove
142	Black locust	7	33	9	Fair	good	3.5	Stump sprout	Remove
143	Norway spruce	13	45	15	Good	moderate	9.75		Remove
144	Black oak	15	45	16	Good	moderate	11.25		Remove
145	Black locust	8	42	12	Good	good	4		Remove
146	Black locust	9	42	14	Good	good	6.75		Remove
147	Black locust	12	41	15	Good	good	9		Remove
148	Apple	9	26	10	Good	good	6.75		Remove
149	Apple	12	26	10	Good	good	9		Remove
150	Incense cedar	6	21	5	Good	moderate	4.5		Remove
151	Black locust	8	38	8	Fair	good	4	Stump sprout	Remove
152	Black locust	10	38	8	Fair	good	5		Remove
153	Black locust	14	44	15	Fair	good	7		Remove
154	Black locust	14	44	18	Poor	good	7		Remove
155	Black locust	6	33	9	Poor	good	3	Stump sprout	Protect
156	Black locust	27	42	25	Poor	good	13.5	Trunk wound	Protect
157	Siberian elm	16	40	15	Good	good	8		Protect
158	Siberian elm	17	40	15	Good	good	8.5		Protect
159	Incense Cedar	14	38	7	Good	moderate	10.5		Protect
160	Incense Cedar	11	38	8	Good	moderate	8.25		Protect
161	Incense Cedar	6	20	5	Good	moderate	4.5		Protect
162	Incense Cedar	12	40	7	Good	moderate	9		Protect
163	Incense Cedar	17	40	9	Good	moderate	12.75		Protect
164	Incense Cedar	20	42	12	Good	moderate	15		Protect
165	Incense Cedar	23	45	14	Good	moderate	17.25		Protect
166	Incense Cedar	19	44	12	Good	moderate	14.25		Protect
167	Incense Cedar	9	42	7	Good	moderate	6.75		Protect
168	Incense Cedar	9	41	7	Good	moderate	6.75		Protect
169	Incense Cedar	9	36	7	Good	moderate	6.75		Protect
170	Incense Cedar	9	38	7	Good	moderate	6.75		Remove
171	Incense Cedar	12	36	7	Good	moderate	9		Remove
172	Incense Cedar	10	37	7	Good	moderate	7.5		Remove
173	Incense Cedar	10	35	7	Good	moderate	7.5		Remove
174	Black walnut	26	38	24	Good	moderate	19.5		Protect
175	Black poplar	42	48	45	Good	moderate	31.5		Protect
200	Redwood	61	65	30	Good	good	45.75		
201	Incense cedar	35	35	24	Fair	moderate	35		
202	Pagoda tree	11	22	15	Good	moderate	11		
203	Incense cedar	31	42	20	Good	moderate	31		
204	Incense cedar	25	40	17	Good	moderate	25		
205	Chamaecypari	21	24	17	Fair	good	21		
206	Pear	9	13	10	Poor	moderate	9		
207	Black oak	23	40	21	Fair	moderate	23		
208	Black oak	24	42	27	Fair	moderate	24		
209	Douglas fir	14	43	15	Fair	moderate	14		
210	Black oak	30	44	30	Good	moderate	30		
211	White oak	28	41	25	Good	good	28		
212	White oak	10	9	18	Poor	good	10		
213	Red maple	6	18	8	Fair	moderate	4.5		
214	Black oak	6	14	7	Fair	moderate	4.5		

Tree #	Species	DBH in	Height in	Crown	Condition	Tolerance	Tree protection	Notes	Remove
215	English holly	10	14	7	Fair	moderate	10		
216	Black oak	25	40	15	Fair	moderate	25		
217	Douglas fir	23	45	18	Good	moderate	23		
218	Black oak	29	45	28	Good	moderate	29		
219	Red maple	5	18	6	Poor	moderate	5		
220	Plum	10	23	18	Fair	moderate	10		
221	Red maple	5	18	5	Poor	moderate	3.75		
222	Plum	8	18	12	Poor	moderate	6		
223	Cymacypress	13	30	21	Good	good	13		
224	Red maple	5	18	4	Fair	moderate	5		
225	Red maple	5	18	5	Fair	moderate	5		
226	Red maple	6	17	5	Fair	moderate	6		
227	Poplar	5	22	5	Fair	moderate	5		
228	Cherry	13	24	12	Fair	moderate	13		
229	Pear	8	13	6	Fair	moderate	8		
230	Red maple	8	26	8	Fair	moderate	8		
231	Redwood	50	58	16	Good	good	37.5		
232	Ash	14	38	13	Good	good	10.5		
233	Ash	7	32	15	Good	good	3.5		
234	Ash	8	33	15	Good	good	4		
235	Leland cypress	14	44	21	Good	good	7		
236	Leland cypress	15	44	21	Good	good	7.5		
237	Ash	10	34	12	Good	good	5		
238	Leland cypress	15	44	18	Good	good	7.5		
239	Leland cypress	12	44	21	Good	good	6		
240	Redwood	41	48	15	Good	good	30.75		
241	Ash	9	33	10	Good	good	4.5		
242	Ash	8	32	9	Good	good	4		
243	Ponderosa	10	36	9	Fair	good	5		
244	Leland cypress	23	36	18	Good	good	11.5		
245	Ash	7	30	7	Good	good	3.5		
246	Leland cypress	13	36	15	Good	good	6.5		
247	Ash	7	31	6	Good	good	3.5		
248	Leland cypress	14	40	12	Good	good	7		
249	Leland cypress	13	41	15	Good	good	6.5		
250	Ash	7	28	6	Good	good	3.5		
251	Ash	7	32	7	Good	good	3.5		
252	Ash	8	35	8	Good	good	4		
253	Ash	8	35	8	Good	good	4		
254	Ash	11	37	12	Good	good	5.5		
255	Ash	8	35	8	Good	good	4		
256	Ash	12	37	12	Good	good	6		
257	Black pine	12	30	9	Good	good	6		
258	Liquidambar	13	38	12	Good	good	9.75		
259	Liquidambar	16	40	18	Good	good	12		
260	Liquidambar	23	46	15	Good	good	17.25		
261	Liquidambar	14	43	16	Good	good	10.5		
262	Black pine	9	38	9	Good	good	6.75		
263	Black pine	17	38	15	Good	good	12.75		
264	Red maple	7	30	9	Good	moderate	3.5		
265	Leland cypress	16	42	15	Good	good	12		

RECEIVED

DEC 30 2011

City of Ashland



RECEIVED

DEC 30 2011

City of Ashland

December 28, 2011

TO: City of Ashland Planning Commission  
Derek Severson, Planner

FROM: Southern Oregon University

RE: ***Southern Oregon University Transportation Demand Management Plan Update***

Thank you very much for your consideration of SOU's current application to construct new student housing and a dining commons. This application is consistent with our City-approved Master Plan and will allow us to provide additional on-campus housing options for our students. As you know, the City has placed conditions on the approval of the Master Plan for SOU to prepare an updated transportation demand management (TDM) plan. We are committed to further detailing our TDM plan and identifying near-term and long-term actions and goals. This is consistent with our own sustainability goals and is also supportive of future campus expansions.

The City has already received evidence of our efforts toward updating the TDM plan. Work conducted by Kittelson & Associates, Inc. (KAI) has summarized many of our ongoing TDM efforts and their results, expressed in such terms as the adequacy of on-campus parking resources and the safety and operational adequacy of the surrounding transportation system. Nonetheless, SOU intends to go further in its TDM efforts. Several actions are entirely within our control, such as policies controlling where students and employees park. Other potential actions are largely dependent upon the City's vision and on expanding our working partnerships with the City, our surrounding neighbors, and Rogue Valley Transit District (RVTD).

The proposed student housing facility is, in and of itself, a TDM strategy for SOU, helping to convert what are now student auto commuters into on-campus pedestrian and bicycle commuters. Significant bicycle parking is planned with this project and transportation outreach efforts by housing staff will be expanded over the coming year.

Our resident students already have a very low automobile ownership rate (0.27 vehicles per resident student) and while there are some things we could do to further reduce this rate, our TDM program should primarily focus on commuter students and employees. These users have the largest impact on the transportation system, our surrounding neighbors, and our own parking needs.

SOU's current TDM programs and strategies are aimed at reducing the number of drive-alone trips to campus. These activities are run through our Commuter Resource Center (CRC), which educates and promotes travel options such as walking, biking, and transit to students and employees. The CRC also facilitates ridesharing, helps people find carpool matches, and hosts a weekly commuter connections networking group for people who commute from Grants Pass.

Part of SOU's TDM program is parking pricing. Annual parking permits are \$118/year for residence hall and commuter students and \$125/year for faculty and staff. Visitor parking is \$1 per hour. We recognize that increasing the price of parking is an important tool in managing travel demand.

SOU is open to increasing parking pricing; however, we are concerned about the impacts this may have on our surrounding neighbors by increasing the demand for on-street parking. Any substantial increase in parking permit rates will only be successful in reducing single-occupant travel to campus if the City of Ashland concurrently implements an area parking

permit program to help protect our neighbors from inappropriate parking by those who may try to avoid the higher parking fees. Thus, this strategy would require a significant partnership and planning between SOU, the neighborhoods, and the City. SOU will need to rely on the City's vision and planning efforts—as well as feasibility studies of a neighborhood parking program—which in turn will dictate the opportunity and timing of implementation for all involved.

SOU is interested in implementing many of the TDM strategies recommended in Kittelson & Associates report. The following provides a summary of those recommendations and SOU's intended actions:

#### Parking Management Strategies

- ☞ Parking Pricing: implement adjustments to the existing parking pricing structure to provide “variable pricing” between different lots.
  - *SOU anticipates implementing variable pricing between lots when the proposed housing facility opens in fall of 2013 and already does this for hourly/daily parking in Lot 41.*
- ☞ Parking Restrictions: implement additional parking restrictions, such as restricting the sale of permits to first-year students.
  - *SOU is interested in exploring this type of strategy, but believes a neighborhood parking permit program is necessary prior to implementing such restrictions to avoid inappropriate neighborhood parking spillover.*
- ☞ Parking Designations: re-designate existing surface parking lots within the north and south campus areas to better distribute parking demands across the SOU system.
  - *SOU will be modifying our parking designations in the north and south campus areas to ensure that resident hall students have sufficient parking on the appropriate side of Siskiyou Boulevard and to better utilize existing parking resources.*

#### Transit Strategies

- ☞ Transit Subsidies: reinstate transit pass subsidies for students to provide access to the transit system during all days and hours of operation, concurrent with improved transit service for SOU.
  - *SOU is scheduling a meeting with RVTD staff, administrators, and students in January 2012 to discuss transit pass subsidies. We would like to reinstate this program in an appropriate form, but feel it must be connected to changes in transit service that meet the needs of our students, such as adequate evening hours to support our Higher Education Center in Medford. Even with planning expansion of evening hours, RVTD will not meet the HEC-Ashland student transit needs, as many SOU courses at the HEC do not end until well after 9p.m.*
- ☞ Shuttle Service: target service to places and/or for special user groups that may be unavailable via regular transit services.
  - *SOU will give this consideration, pending our discussions with RVTD on a transit pass program. To explore this idea, we will map our student and employee addresses to identify potential shuttle service routes, estimate costs to operate a shuttle system, and propose it to our student body, as it would need to be at least partially funded through student fees. An ‘express’ shuttle between the HEC and Ashland Campus that can guarantee transit in a reasonable (30 minutes or less) amount of time, coupled with a ‘park & ride’ concept for those commuting from outside of Medford, may reduce single-occupant vehicle traffic considerably, but the cost of such a program will need to be weighed carefully. This type of program may be a necessary component of a neighborhood permit program and higher parking fees.*

There are two critical issues that need to be addressed in order to further develop a more detailed TDM program. The first is establishing a transit pass subsidy program with RVTD that meets the needs of student and employee commuters and can be supported by the student body. The second is establishing some type of neighborhood parking permit area surrounding the campus. We are committed to working on both of these issues with RVTD, our neighbors, and the City of Ashland in advance of the completion of the North Campus Village housing project.

1250 Siskiyou Boulevard  
Ashland, Oregon 97520-5045  
T: 541.552.6221  
F: 541.552.8324

DEC 30 2011  
City of Ashland



SOUTH HALL

RECEIVED

DEC 30 2011

City of Ashland

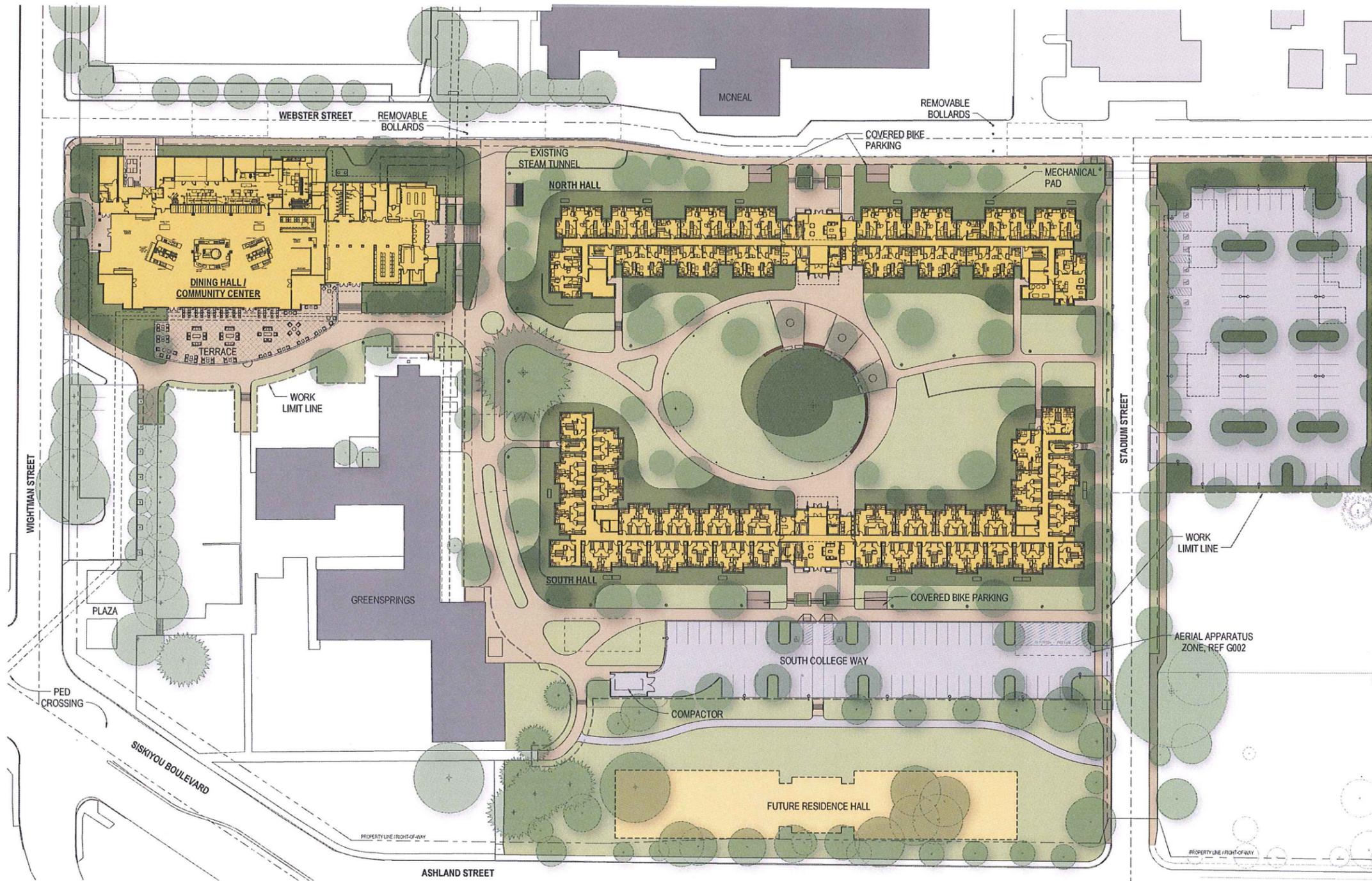


SOUTH HALL ENTRY PAVILION

RECEIVED

DEC 30 2011

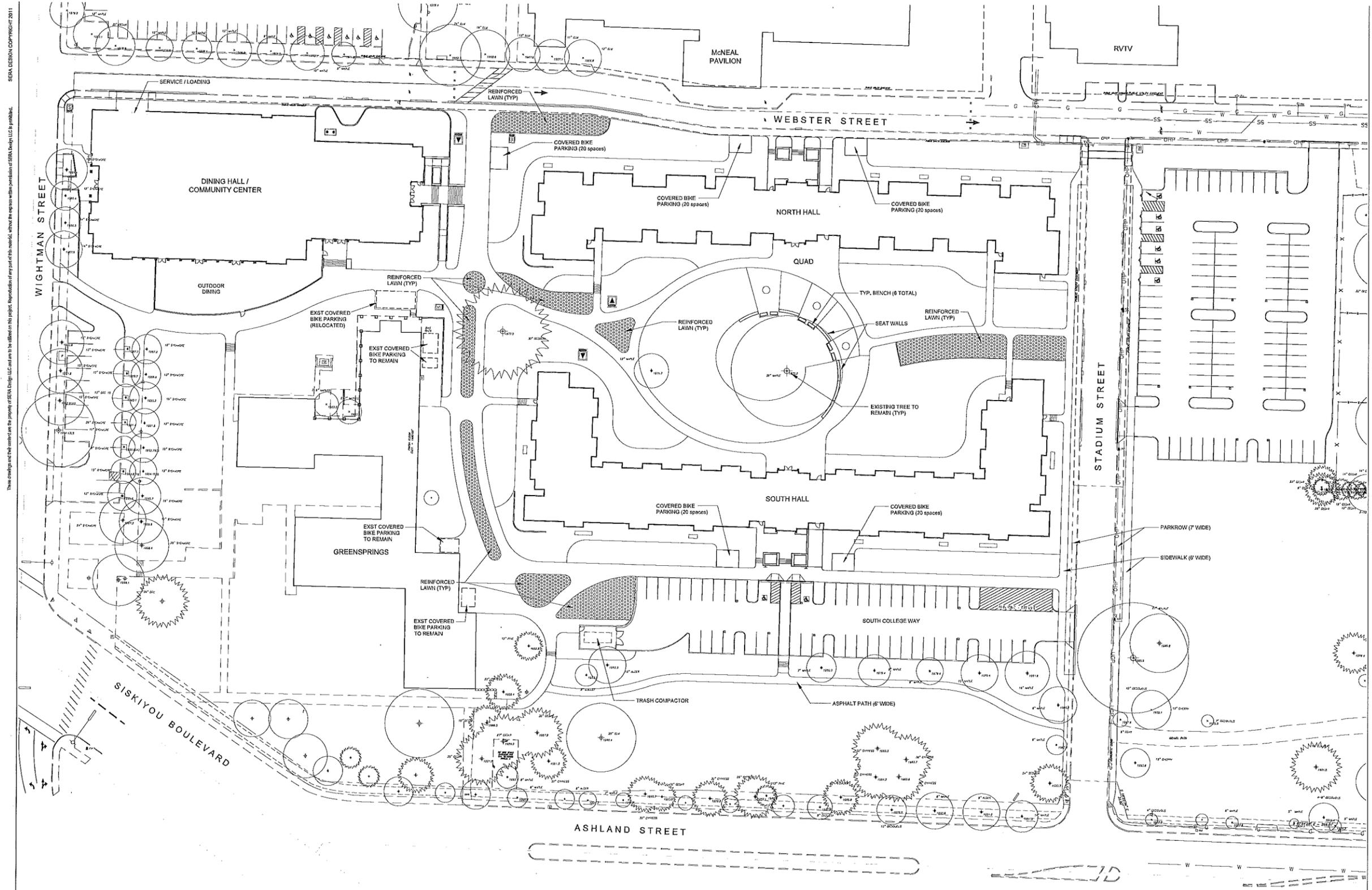
City of Ashland



RECEIVED

DEC 30 2011

City of Ashland



SERA DESIGN COPYRIGHT 2011  
 These drawings and their contents are the property of SERA Design, LLC and are to be utilized on this project. Reproduction of any part of this material, without the express written permission of SERA Design, LLC is prohibited.

Notice of Alternate Billing Cycle: The Contract will allow the Owner to require the submission of billings or estimates in billing cycles other than 30-day cycles. Billing or estimates shall be submitted monthly ending on the last day of the month.  
 Notice of Extended Certificate Provisions: The Contract will allow the Owner to early billings and estimates within 30 days after the billings and estimates are received from the original contractor.  
 Notice of Extended Payment Provisions: The Contract will allow the Owner to make payment within 30 days after the date a billing or estimate is received.

**SERA**  
 ARCHITECTURE  
 URBAN DESIGN + PLANNING  
 INTERIOR DESIGN  
 334 NW 5TH AVENUE  
 PORTLAND OREGON 97209  
 P: 503.445.7372  
 F: 503.445.7336  
 SERA@SERA.COM

**CoveyPartee**  
 LANDSCAPE ARCHITECTS  
 202 LAKE WASHINGTON ST  
 SEASIDE, OREGON 97138  
 541.523.3335  
 info@coveypartee.com

**SOU North Campus Village**  
 AMERICAN CAMPUS COMMUNITIES  
 1554 WEBSTER STREET, ASHLAND, OR 97120

REVISIONS:  
 02 DEC 2011  
 30 DEC 2011

CHECKED BY: GTC  
 ISSUE DATE: 04 NOV 2011  
 PROJECT NO.: 091641

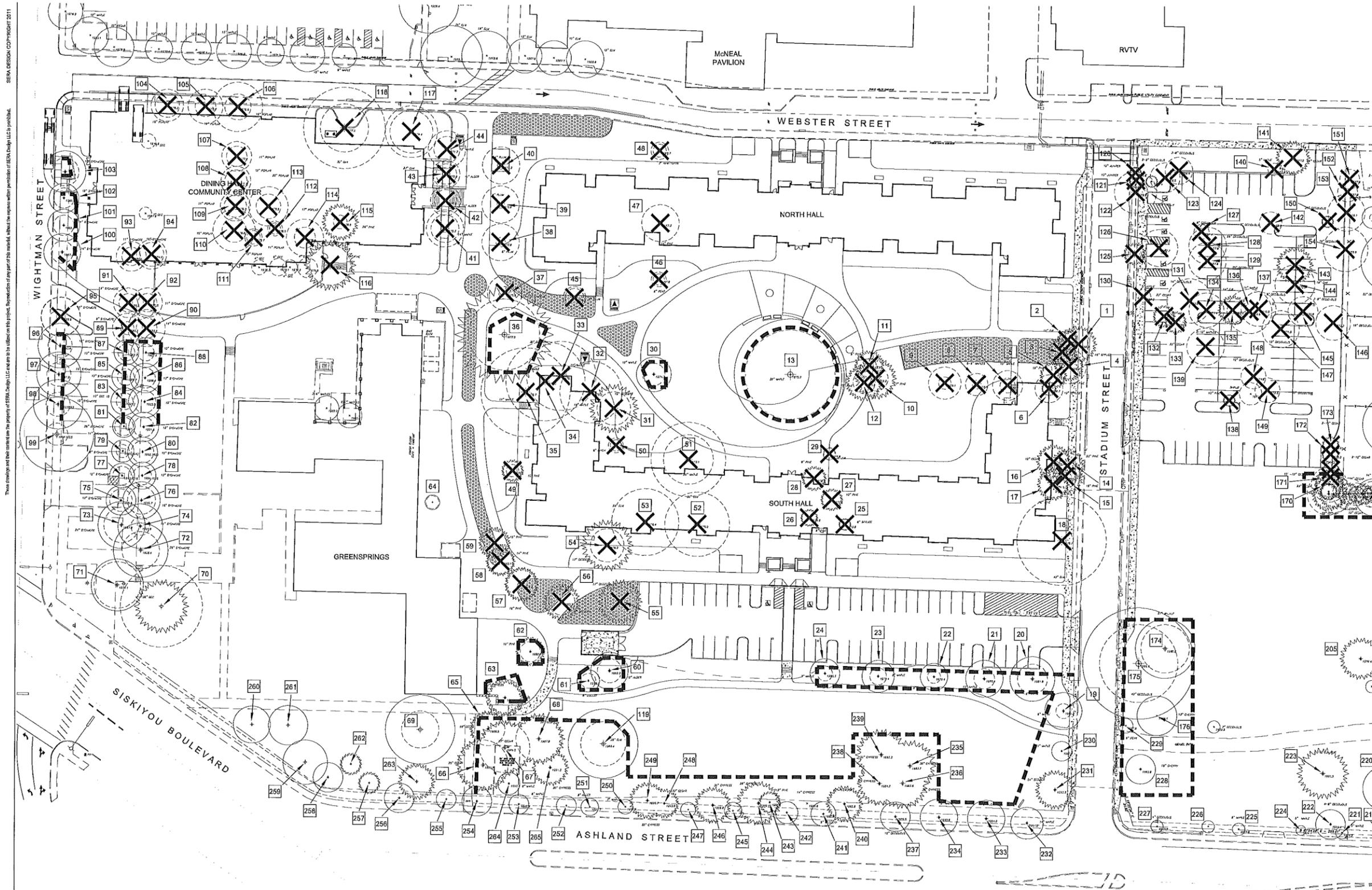
LANDSCAPE  
 SITE PLAN

**L001**

LAND USE

RECEIVED  
 DEC 30 2011  
 City of Ashland





SERA DESIGN COPYRIGHT 2011  
These drawings and their contents are the property of SERA Design, LLC and are to be utilized on this project. Reproduction of any part of this material, without the express written permission of SERA Design, LLC is prohibited.

**LEGEND**

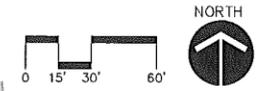
- EXISTING TREES TO BE REMOVED
- TYP. TREE PROTECTION ZONE
- EXISTING TREES TO REMAIN
- EXSTG. TREE NUMBER  
(See Certified Arborist's Tree Inventory)
- TREE PROTECTION FENCING  
(See Note 4.)

**NOTES**

1. Topographic survey provided by Polaris Land Surveying, LLC.
2. Tree Inventory provided by Tom Myers, Certified Arborist. Refer to Tree Inventory for additional information regarding tree species, size, protection zone, and condition.
3. Where discrepancies exist between the existing tree species and size labels on the survey and the information contained in the Tree Inventory, the Tree Inventory shall be deemed correct.
4. Final location of tree protection fencing shall be determined by a certified arborist as described in Tree Protection Plan.

DEC 30 2011

City of Ashland



**SERA**  
ARCHITECTURE  
URBAN DESIGN • PLANNING  
INTERIOR DESIGN  
330 NW 5TH AVENUE  
PORTLAND OREGON 97209  
P. 503.445.1372  
F. 503.445.1399  
SERADOC.COM

**CoveyPardee**  
LANDSCAPE ARCHITECTS  
2020 EAST WASHINGTON STREET  
SUITE 100  
ASHLAND, OREGON 97102  
503.261.1111

**SOU North Campus Village**  
AMERICAN CAMPUS COMMUNITIES  
1554 WEBSTER STREET, ASHLAND, OR 97102

REVISIONS  
02 DEC 2011  
30 DEC 2011

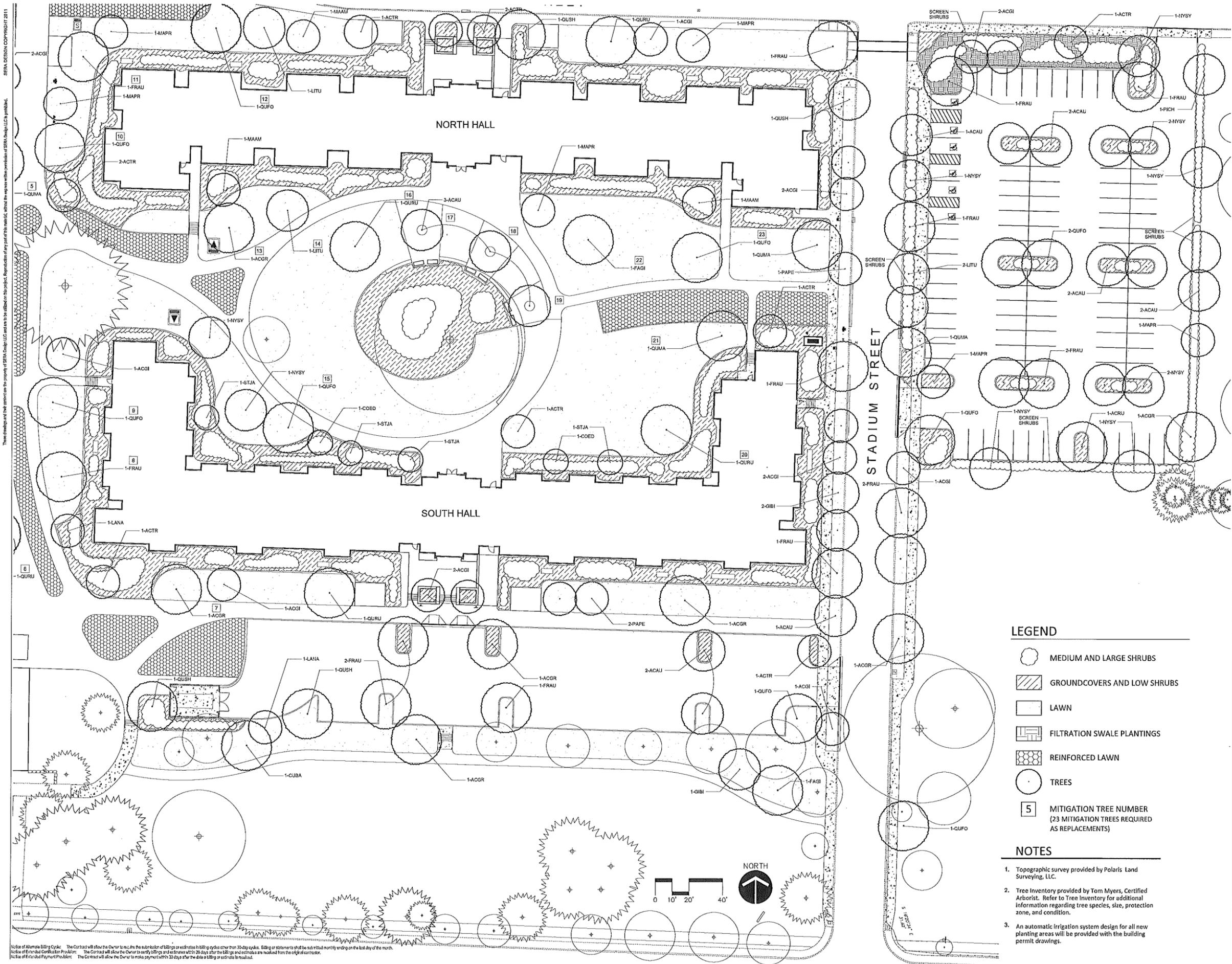
CHECKED BY: GTC  
ISSUE DATE: 04 NOV 2011  
PROJECT NO.: 091841

TREE PROTECTION  
& REMOVAL PLAN

**L002** LAND USE

Notice of Alternate Billing Cycle: The Contract will allow the Owner to require the submission of billings or estimates in billing cycles other than 30-day cycles. Billing or estimates shall be submitted monthly ending on the last day of the month.  
Notice of Extended Certification Provision: The Contract will allow the Owner to certify billings and estimates within 20 days after the billings and estimates are needed from the original contractor.  
Notice of Extended Payment Protection: The Contract will allow the Owner to make payment within 30 days after the date a billing or estimate is received.





SERA DESIGN COPYRIGHT © 2011  
 These drawings and their contents are the property of SERA Design LLC and shall not be published or otherwise used in any project without the express written permission of SERA Design LLC.

Notice of Automatic Billing Cycle: The Contractor shall allow the Owner to require the administration of billings or estimates in billing cycles other than 30-day cycles. Billing or statements shall be submitted monthly ending on the last day of the month.  
 Notice of Extended Completion Period: The Contractor shall allow the Owner to extend billings and estimates within 30 days after the billing and estimates are received from the original contractor.  
 Notice of Extended Payment Period: The Contractor shall allow the Owner to make payment within 30 days after the date a billing or estimate is received.

**SERA**  
 ARCHITECTURE  
 URBAN DESIGN + PLANNING  
 INTERIOR DESIGN  
 330 NW 37th AVENUE  
 PORTLAND OREGON 97209  
 P. 503.445.1372  
 F. 503.445.1356  
 SERA@SERA.COM

CoveyPardee  
 LANDSCAPE ARCHITECTS  
 200 EAST HANCOCK STREET #6  
 ASTORIA, OR 97103  
 503.325.0000

**SOU North Campus Village**  
 AMERICAN CAMPUS COMMUNITIES  
 1504 WEBSTER STREET, ASTORIA, OR 97103

REVISIONS  
 01 DEC 2011  
 30 DEC 2011

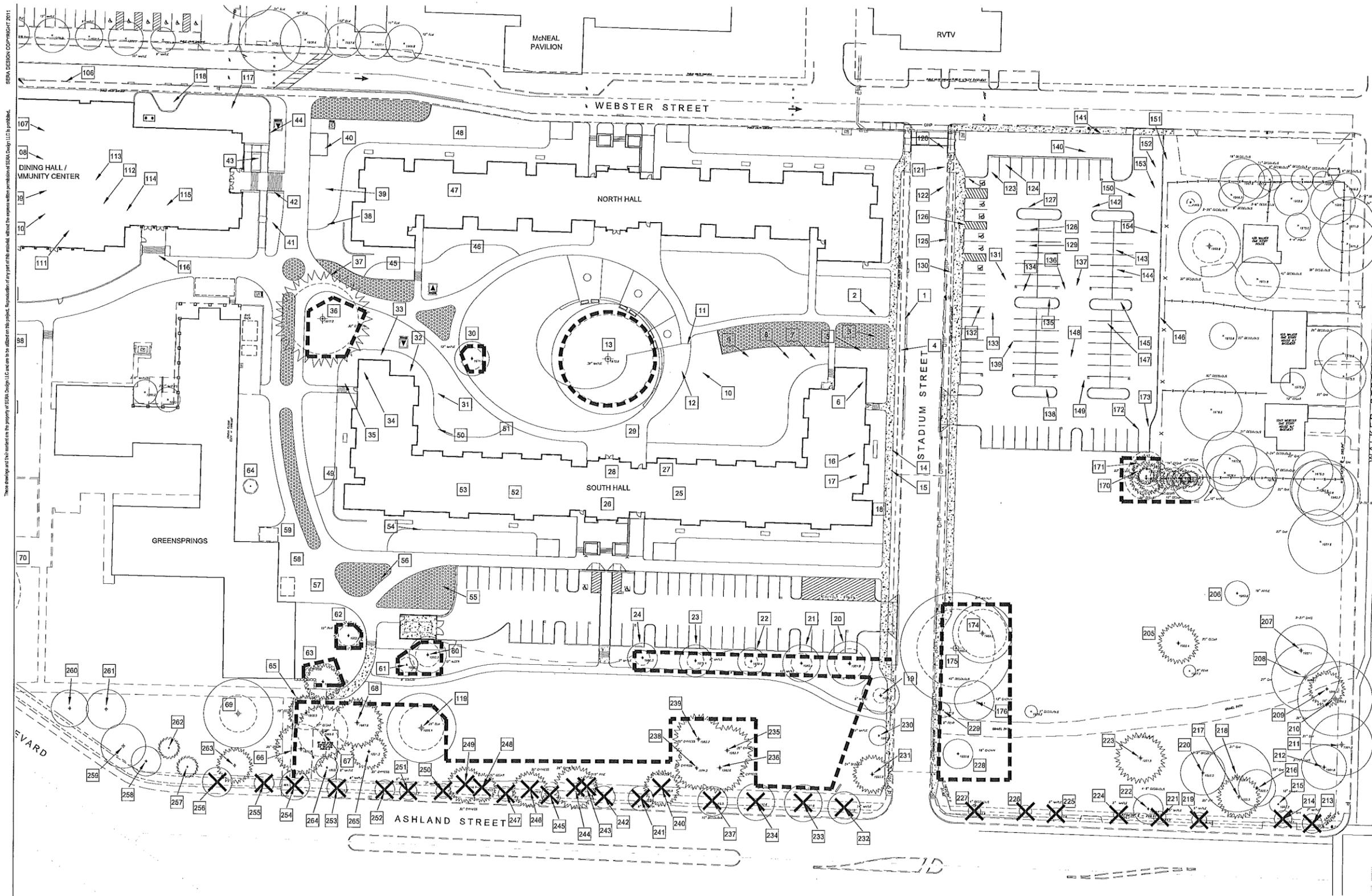
CHECKED BY: OTC  
 ISSUE DATE: 04 NOV 2011  
 PROJECT NO.: 091641

SITE REVIEW  
 PLANTING PLAN  
 EAST

**L004** LAND USE

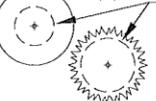
DEC 8 2011

City of Astoria



SERA DESIGN COPYRIGHT 2011  
 These drawings and the content on the property of SERA Design LLC and are to be used only for the project. Reproduction of any part of this manual, without the express written permission of SERA Design LLC is prohibited.

**LEGEND**

-  EXISTING TREES TO BE REMOVED
-  TYP. TREE PROTECTION ZONE
-  EXISTING TREES TO REMAIN
-  TREE PROTECTION FENCING (See Note 4.)
-  EXSTG. TREE NUMBER (See Certified Arborist's Tree Inventory)

**NOTES**

1. Topographic survey provided by Polaris Land Surveying, LLC.
2. Tree Inventory provided by Tom Myers, Certified Arborist. Refer to Tree Inventory for additional information regarding tree species, size, protection zone, and condition.
3. Where discrepancies exist between the existing tree species and size labels on the survey and the information contained in the Tree Inventory, the Tree Inventory shall be deemed correct.
4. Final location of tree protection fencing shall be determined by a certified arborist as described in Tree Protection Plan.

Notice of Alternate Billing Cycle: The Contract will allow the Owner to require the submission of bills or estimates to billing cycles other than 30-day cycles. Billing or estimates shall be submitted monthly ending on the last day of the month.  
 Notice of Extended Completion Period: The Contract will allow the Owner to extend the completion date of the project and estimates within 30 days after the billing and estimates are received from the original contractor.  
 Notice of Extended Payment Period: The Contract will allow the Owner to make payment within 30 days after the date a billing or estimate is received.

**SERA**  
 ARCHITECTURE  
 URBAN DESIGN + PLANNING  
 INTERIOR DESIGN  
 338 NW 5TH AVENUE  
 PORTLAND OREGON 97209  
 P. 503.445.1372  
 F. 503.445.1339  
 SERAOR.COM

**CoveyPardee**  
 LANDSCAPE ARCHITECTS  
 2000 NE 15TH ST  
 PORTLAND, OR 97232  
 503.255.1310  
 COVEYPARDEE.COM

**SOU North Campus Village**  
 AMERICAN CAMPUS COMMUNITIES  
 1554 WEBSTER STREET, ASHLAND, OR 97130

REVISIONS  
 03 DEC 2011  
 30 DEC 2011

CHECKED BY: GTC  
 ISSUE DATE: 04 NOV 2011  
 PROJECT NO.: 091541

TREE REMOVAL  
 FOR ASHLAND ST  
 PARKROW

**L005** LAND USE



DEC 30 2011

CoveyPardee

CITY OF  
**ASHLAND**  
TRANSPORTATION COMMISSION  
Thursday, December 15, 2011  
Council Chambers, 1175 East Main Street

**DETAIL AGENDA FOR ITEM V**

- I. Ordinance Review (staff)
- II. Open Public Hearing
- III. Staff Presentation
  - a. Planning
  - b. Public Works
  - c. TC Questions to Staff
- IV. Applicant Presentation (15-20 min)
- V. TC Questions to Applicant
- VI. Public Comment (3 min per person)
- VII. Close Public Hearing
- VIII. TC Deliberation and Subsequent Vote

**2.13.030 Powers and Duties - Generally**

The Transportation Commission will review and make recommendations on the following topics as it relates to all modes of Transportation:

1. Safety: will develop, coordinate and promote transportation safety programs;
2. Planning:
  - \* Will review and serve as the primary body to develop recommendations to the City's long range transportation plans.
  - \* Will review and make recommendations in Type III Planning Actions during the pre-application process.
3. Funding: will make recommendations to the City's transportation section of the Capital Improvements Program;
4. Advocacy: will advocate and promote all modes of transportation to make modal equity a reality.
  - \* Facilitate coordination of transportation issues with other governmental entities.
  - \* Select one or more member liaisons to attend and participate in meetings with other transportation related committees in the Rogue Valley.
  - \* Examine multi-modal transportation issues.

(Ord 2975, 2008; Ord 3003, 2010)

# Memorandum

---

DATE: December 14, 2011  
TO: Mike Faught, Jim Olson  
FROM: Karl Johnson   
RE: SOU Student Housing

Below are my comments regarding each of the intersection proposals that have been presented by Kittelson & Associates, Inc. in their document titled *Southern Oregon University Student Housing Development Supplemental Land Use Information* dated December 5, 2011

- **S. Mountain Avenue/Siskiyou Boulevard**
  - **Proposal** – Replace Pedestrian Heads with Pedestrian Countdown Heads
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Add a 5 second leading pedestrian phase
    - I'm not sure what the reason for this would be. The north and south legs of S. Mountain Avenue are independent of each other and with the right hand turn lane that is built on the south-side, the only conflict that exists between pedestrians and automobiles is right turns from the north-side onto Siskiyou Boulevard.
  
- **University Way/Siskiyou Boulevard**
  - **Proposal** – Replace flashing beacons with Rectangular Rapid Flash Beacons
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Maintain street trees to ensure crosswalk and flashing beacons are visible
    - This would be a good task for the City of Ashland Parks Department to keep up on.
  
- **Garfield Street/Siskiyou Boulevard**
  - **Proposal** – Replace flashing beacons with Rectangular Rapid Flash Beacons
    - This appears to be a good idea that could be implemented.
  
- **Avery Street/Siskiyou Boulevard**
  - **Proposal** – Replace flashing beacons with Rectangular Rapid Flash Beacons
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Relocate “Stop Here for Ped” sign at the westbound approach to provide adequate clearance for the crosswalks sign and flashing beacons.
    - This seems like a task that the City of Ashland could review and complete the proposed relocations if necessary.



- **Bridge Street/Siskiyou Boulevard**
  - **Proposal** – Replace flashing beacons with Rectangular Rapid Flash Beacons
    - This appears to be a good idea that could be implemented.
  
- **Wightman Street/Indiana Street/Siskiyou Boulevard**
  - **Proposal** – Replace Pedestrian Heads with Pedestrian Countdown Heads
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Add a 36 second pedestrian “scramble” phase (requires 2 additional pedestrian signal heads and pushbuttons)
    - If this option is going to be implemented with this project, Kittelson should provide a design to help limit the time that automobiles are waiting due to the added 36 seconds. The existing signal cabinet should be upgraded as part of this proposal.
  - **Proposal** – Install high visibility pavement marking for diagonal crosswalk
    - This appears to be a good idea that could be implemented if the diagonal crosswalk is approved/required.
  
- **Ashland Street/Siskiyou Boulevard**
  - **Proposal** – Replace Pedestrian Heads with Pedestrian Countdown Heads
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Add a 5 second leading pedestrian phase
    - I'm not sure what the reason for this would be. The only conflict that exists for pedestrians at this intersection is for automobiles turning right on red from Ashland Street onto Siskiyou Boulevard. The other crossings have definite stops and so adding 5 seconds would not do anything to improve an existing situation.
  
- **Frances Lane/Siskiyou Boulevard**
  - **Proposal** – Install advance pedestrian signs with Rectangular Rapid Flash Beacons.
    - This intersection doesn't seem to need this treatment at this time with the current number of pedestrians that cross there. These improvements could be installed in the future if it is found to be something that is necessary with future development.
  
- **Stadium Street/Ashland Street**
  - **Proposal** – Install advance pedestrian signs with Rectangular Rapid Flash Beacons.
    - This appears to be a good idea that could be implemented. Kittelson should look at the possibility of other improvements to the existing placement of the crosswalk and whether the existing median layout needs to be altered.
  
- **Walker Avenue/Ashland Street**
  - **Proposal** – Replace Pedestrian Heads with Pedestrian Countdown Heads
    - This appears to be a good idea that could be implemented.
  - **Proposal** – Add a 5 second leading pedestrian phase



- This appears to be a good idea that could be implemented for crossing Ashland Street and would help the children that use this signal to go to Walker Elementary and Ashland Middle School.
- **Additional Comments**
  - Kittelson has said that the Wightman Street/Indiana Street/Siskiyou Boulevard intersection does not meet warrants for a pedestrian bridge and that there would be issues with pedestrians wanting to use it. I still feel that a pedestrian bridge should be investigated more to help increase safety for pedestrians crossing Siskiyou Boulevard at this intersection and there should be ways in which pedestrians could be enticed to use the bridge instead of existing crosswalks





CITY OF  
**ASHLAND**  
TRANSPORTATION COMMISSION

**RECOMMENDED CONDITIONS OF APPROVAL FOR PLANNING ACTION 2011-01576**

**S. Mountain Avenue/Siskiyou Boulevard**

- 1) Replace Pedestrian Heads with Audible Pedestrian Countdown Heads
- 2) Provide a 5 second leading pedestrian phase

**University Way/Siskiyou Boulevard**

Replace flashing beacons with Audible Rectangular Rapid Flash Beacons

**Garfield Street/Siskiyou Boulevard**

Replace flashing beacons with Audible Rectangular Rapid Flash Beacons

**Avery Street/Siskiyou Boulevard**

Replace flashing beacons with Audible Rectangular Rapid Flash Beacons

**Bridge Street/Siskiyou Boulevard**

Replace flashing beacons with Audible Rectangular Rapid Flash Beacons

**Wightman Street/Indiana Street/Siskiyou Boulevard**

- 1) Replace Pedestrian Heads with Audible Pedestrian Countdown Heads
- 2) Design and Reconfigure the intersection with single crosswalk as shown conceptually on new Figure 12 (see attached); upgrade existing controller at both Wightman and Hwy 66
- 3) Install high visibility pavement marking for diagonal crosswalk

**Ashland Street/Siskiyou Boulevard**

- 1) Replace Pedestrian Heads with Audible Pedestrian Countdown Heads
- 2) Provide a 5 second leading pedestrian phase

**Stadium Street/Ashland Street**

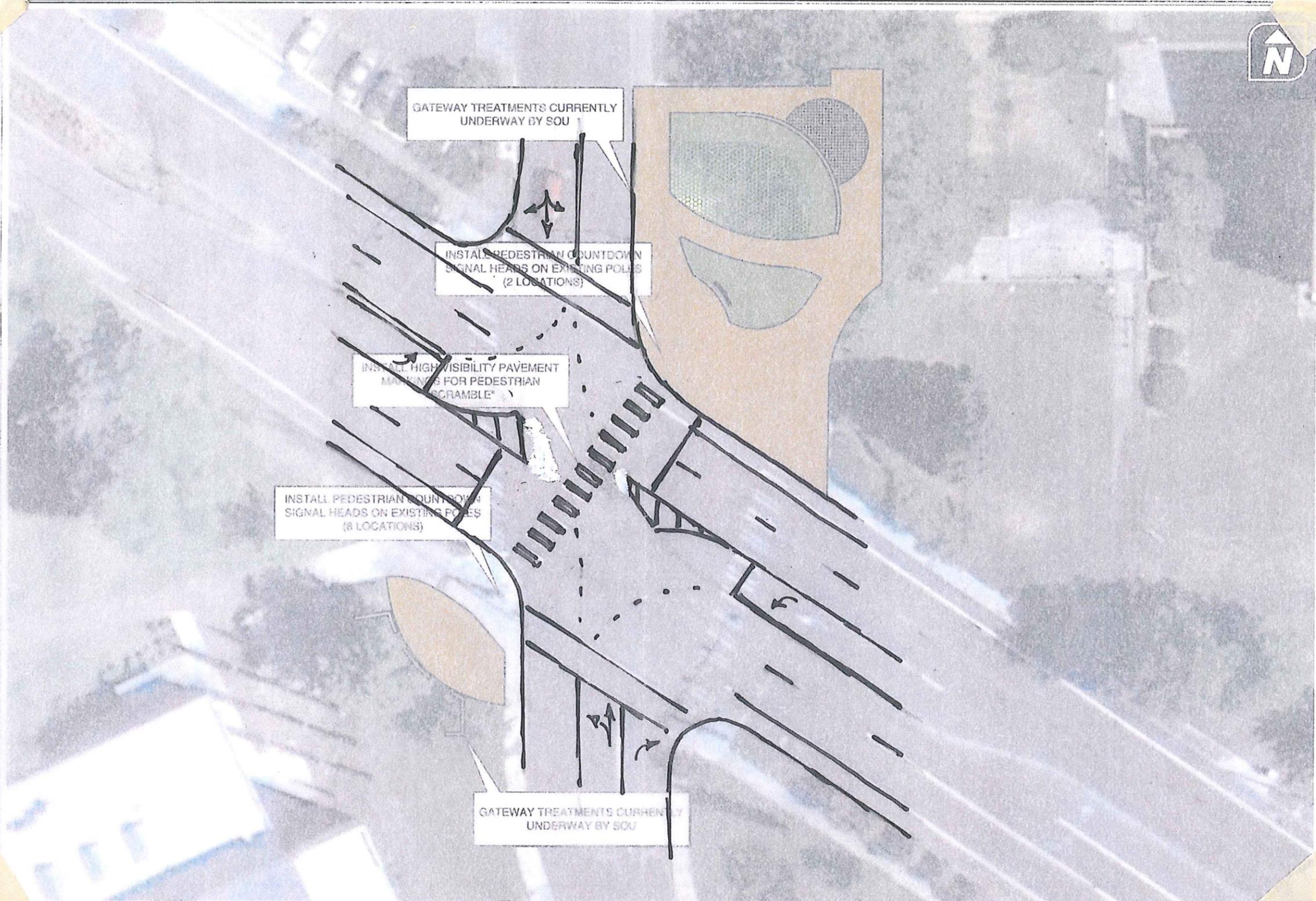
- 1) Install advance pedestrian signs
- 2) Install Audible Rectangular Rapid Flash Beacons
- 3) Review location of existing crosswalk, and if conditions warrant relocate crosswalk with appropriate pedestrian ways and pedestrian amenities from the new facility

**Walker Avenue/Ashland Street**

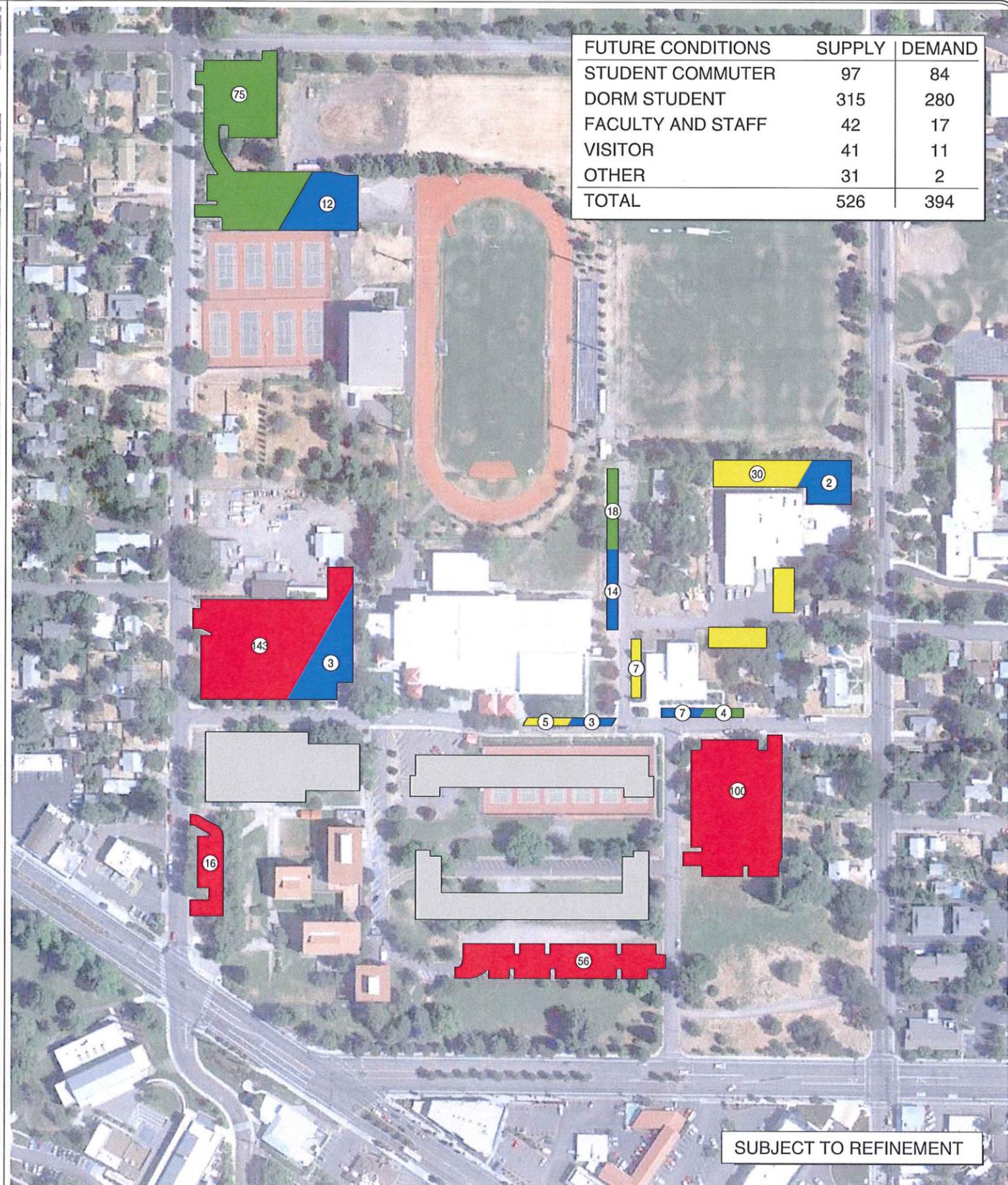
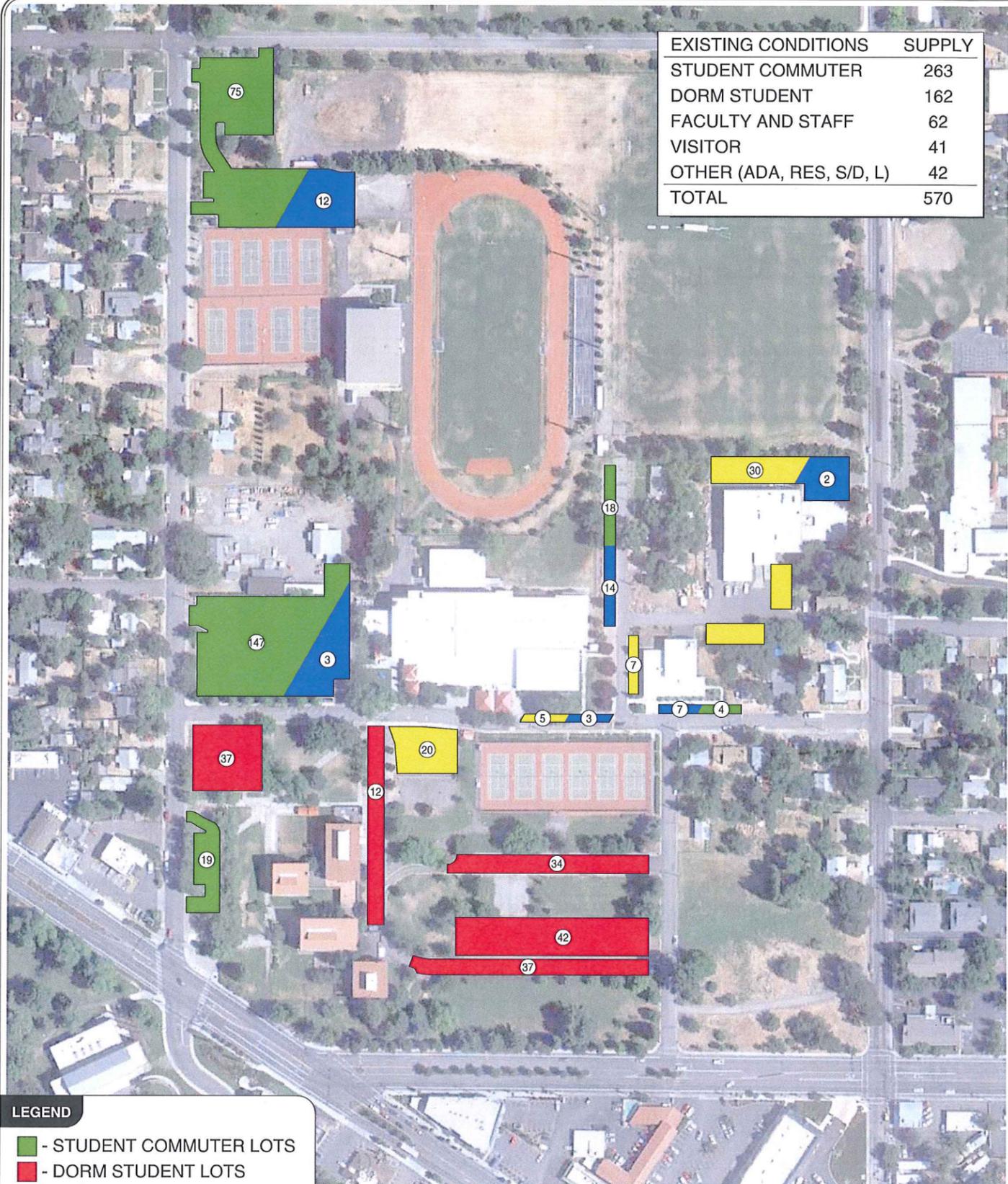
- 1) Replace Pedestrian Heads with Audible Pedestrian Countdown Heads
- 2) Provide a 5 second leading pedestrian phase
- 3) Pedestrian amenities on SOU campus frontage



S:\Projects\2011\12\120111001\120111001.dwg Date: 01/02/12 09:53:00 AM User: jkittelson



**RECOMMENDED PEDESTRIAN IMPROVEMENTS AT THE WIGHTMAN-INDIANA STREET INTERSECTION  
ASHLAND, OREGON**



**LEGEND**

- - STUDENT COMMUTER LOTS
- - DORM STUDENT LOTS
- - FACULTY AND STAFF LOTS
- - METERED PARKING LOTS

SUBJECT TO REFINEMENT

RECOMMENDED PARKING DESIGNATION AND SUPPLY  
ASHLAND, OREGON

H:\profile\11854 - SOU Student Housing\dwgs\figs\11854\FIG04.dwg Dec 15, 2011 - 9:57am - mbell Layout Tab: Fig1

**TYPE II  
PUBLIC HEARINGS**

---

**PA-2011-01699  
Brammo Motorsports**



**PLANNING ACTION: #2011-01699**

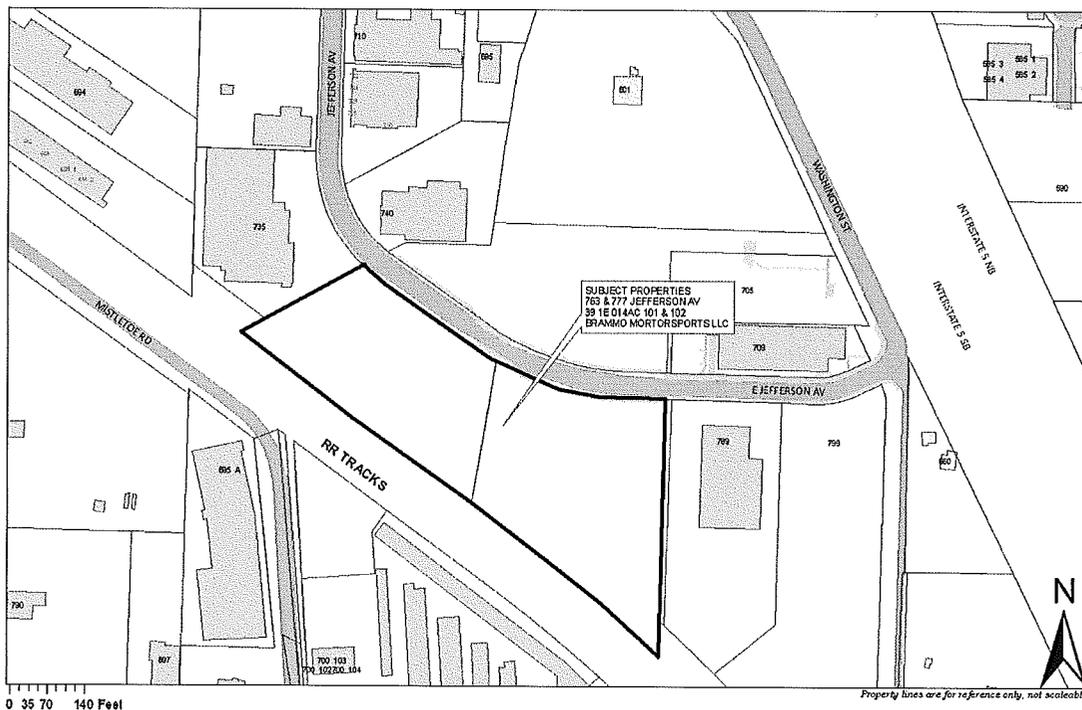
**SUBJECT PROPERTY: 763 & 777 Jefferson Avenue**

**OWNER/APPLICANT: Brammo Motorsports LLC**

**DESCRIPTION: A request for Site Review approval to construct two, two-story buildings of 14,857 square feet and 18,805 square feet for the research, design, manufacturing of prototypes, warehousing, storage and administrative offices for the production of Brammo Electric Motorcycles & products. The proposed buildings will be located at 777 Jefferson. A private, dirt test track to further the research and design is proposed for 763 Jefferson. COMPREHENSIVE PLAN DESIGNATION: Employment and Industrial; ZONING: E-1 & M-1; ASSESSOR'S MAP: 39 1E 14AC; TAX LOT: 101 & 102.**

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **January 5, 2012 at 6:00 p.m.** in the Community Development and Engineering Services building (Siskiyou Room) located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING: January 10, 2012 at 7:00 PM, Ashland Civic Center**



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## **SITE DESIGN AND USE STANDARDS**

### 18.72.070 Criteria for Approval

The following criteria shall be used to approve or deny an application:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.

**ASHLAND PLANNING DIVISION  
STAFF REPORT  
January 10, 2012**

**PLANNING ACTION:** PA-2011-01699

**APPLICANT:** Brammo Motorsports LLC

**LOCATION:** 763 & 777 Jefferson

**COMPREHENSIVE PLAN DESIGNATION:** Manufacturing & Employment

**APPLICATION DEEMED COMPLETE:** January 3, 2012

**120-DAY TIME LIMIT:** May 2, 2012

<b>ORDINANCE REFERENCE:</b>	18.12	District Boundaries
	18.40	E-1 Employment District
	18.52	M-1 Industrial District
	18.61	Tree Preservation and Protection
	18.63	Water Resources Protection Zone
	18.72	Site Design and Use Standards
	18.92	Off-Street Parking
	18.112	Enforcement

**REQUEST:** A request for Site Review approval to construct two, two-story buildings of 14,857 square feet and 18,805 square feet for the research, design, manufacturing of prototypes, warehousing, storage and administrative offices for the production of Brammo Electric Motorcycles & products. The proposed buildings will be located at 777 Jefferson. A private, dirt test track to further the research and design is proposed for 763 Jefferson.

**I. Relevant Facts**

**A. Background - History of Application**

In May 2006, an 8.43-acre parcel was annexed into the city limits. The annexed land allowed for the completion of Jefferson Avenue. As part of the annexation request, a Site Review approval with an Administrative Variance for a new 41,000 square foot manufacturing facility; a Physical and Environmental Constraints Review for the Knoll Creek crossing and tree removal request were included.

In November 2006, a modification of the approved Site Review was approved. That modification split the single 41,000 square foot building into two smaller buildings and modified the parking lot layout.

In May 2008, the large single lot that was annexed was split into three smaller parcels with Jefferson Avenue bisecting the property. One parcel was to the north of Jefferson Avenue and two parcels; the subject sites were to the south of Jefferson Avenue.

Following the annexation, Jefferson Avenue was completed to city street standards with bio-swale park rows and a crossing of Knoll Creek. The street was accepted and adopted by the city in July 2011.

## **B. Detailed Description of the Site and Proposal**

The subject properties are a combined 5.17 acres and are located at 763 and 777 Jefferson Avenue in southeast Ashland. Tax lot 101, at 763 Jefferson is 2.47 acres and Tax lot 201 at 777 Jefferson is 2.70 acres. The properties are vacant and are divided by Knoll Creek. Jefferson Avenue is paved with curb, gutter, parkrow and sidewalk along the entire frontage of the subject properties frontage. The properties are zoned Industrial (M-1). To the east of the subject property is Oak Street Tank and Steel which was annexed in 1999. To the north is a 2.59 acre parcel that was the part of the original 8.43 acres that were annexed in 2006 and is zoned Employment (E-1). West of the property at 735 Jefferson is Ashland Racquet and Fitness. The railroad right-of-way and E-1 zoned properties are to the south.

The subject properties have an approximately three percent slope to the northwest, toward Jefferson Avenue and Knoll Creek from the rear of the parcel. Natural features on the sites include Knoll Creek, dividing the two properties. Knoll Creek is considered an intermittent or ephemeral stream and has a 20-foot conservation easement in place. A delineated wetland (WD#2008-05950) is nearly centered on the lot at 763 Jefferson Avenue. The creek and the wetland are both subject to the requirements of the Water Resources Protection Zone Ordinance (AMC 18.63). The remainder of the property has grasses, oak and cottonwood trees. The application materials provided identify 19 deciduous trees six-inches in diameter at breast height (d.b.h.) or greater along the southern property line and adjacent to Knoll Creek.

### **1. Site Review**

The proposal includes the construction of two structures at 777 Jefferson. Building A is proposed to be a two-story, 19,197 square foot building. Building B is proposed to be two-story, 25,195 square feet. The buildings are both to be a combination of office, research, design, manufacturing and storage. The buildings are oriented toward Jefferson Avenue. The fronts of the structures are both setback 25-feet from the front property line. Because of the building design and the strong building streetwall projection, the actual front entrance of Building A is 50-feet from the front property line and Building B is 58-feet from the front property line. The projection emphasizes the interesting rooflines proposed and the buildings have a modern, European industrial aesthetic. Building A will have a inverted gable, butterfly roof and Building B is proposed to have an peak roof line. The roof design allows for the collection of rainwater. Solar arrays will be

installed in the future. The average height of both buildings is proposed to be 40-feet. The applicant has also proposed to construct two 'stylized' bioswale planters at the front of the building. Clerestory windows run the length of the east and west sides of the buildings. Each building is proposed to have a loading ramp, two bay door on the west side of Building A and three bay door on the west side of Building B. Fifty-two automobile and fleet vehicle parking spaces are proposed to be provided between the buildings, at the rear of Building B and on the west side of Building A. Fourteen bicycle parking are to be provided under the covered front entry area.

The exterior building materials include steel siding, standing seam metal siding, zinc-alum roofing and aluminum doors and windows. The colors of the exterior building materials are grays, black and oxide red steel panels.

## **2. Test Track**

The property at 763 Jefferson is proposed to have dirt test track constructed for the use of Brammo Motorsports Research and Development. There is a wetland centered on the property, the test track is proposed to encircle the wetland and the wetland buffer. A new driveway curbcut will be installed on Jefferson Avenue to access the site. The track is not for public use and will have a gate installed to prevent unauthorized entry.

## **3. Tree / Landscaping**

The applicant has proposed to remove two dead cottonwood trees which are located in the southeast corner of the 777 Jefferson property near the railroad tracks. These trees are not subject to any land-use requirements as they are exempt from the code. The remaining oaks and cottonwoods are to be retained. One street tree in front of 763 Jefferson will likely require removal to permit the installation of the site access. This tree is also not subject to the land-use requirements.

Jefferson Avenue was recently improved to city street standards and has the required number of street trees installed across the frontage of the property.

The applicant has proposed to retain 19,900 square feet of the parcel at 777 Jefferson as landscaping.

## **II. Project Impact**

The project requires Site Review approval since it involves the construction of a new buildings in the M-1 zoning district. Site Review procedures require a public hearing before the Planning Commission, for structures greater than 10,000 square feet.

## **A. Site Review**

### **Requirements of the Employment and Industrial Zoning Districts**

All of the building square footages are proposed as office, research, design, development, warehouse and light manufacturing of prototypes, which are permitted uses in the M-1 zones. The M-1 zoning district has a minimum front setback of 25-feet from the front property line. As proposed, the buildings design satisfies this requirement with 25-foot front setbacks. The proposed average building heights are 40 feet, which is the maximum building height in the zoning district. The proposal will result in 17 percent of the site being landscaped, which exceeds the 10 percent minimum landscaping requirement of the M-1 zoning district and the 15 percent minimum landscaping requirement for the E-1 zoning district.

### **Site Design and Use Standards**

The subject property is located within the Basic Site Review Zone. The proposed buildings are designed to provide a primary orientation toward Jefferson Avenue. Each building will have a covered entry and walkway to the sidewalk. Parking is proposed to be located to the sides and rear of the buildings. Internal sidewalks for circulation from the parking lots to the building, parking lot shade trees and landscaped parking lot 'fingers' are to be installed. The applicant has also provided three on-site bioswale / stormwater detention ponds. Two are proposed to be located at the front of the buildings and a larger detention pond is proposed along the southern property line. The bioswales in front of the building are at the lowest elevation of the site and the runoff will be filtered and piped underground back to the creek. A landscape plan has been provided; the existing trees that are to be preserved have been incorporated, parking lot landscaping and a landscape area between the building and the street are identified.

Basic Site Review Standards (II-C-1) require that buildings shall have their primary orientation toward the street and accessed from a public sidewalk. Additionally the standards call for building entrances to be located within 20 feet of the public right-of-way to which they are oriented unless the building is not to be accessed by pedestrians, such as warehouses or industrial buildings without attached office space. The required front setback in the M-1 zone is 25-feet from the front property line. In this instance, the structure, which has an approximately 20-foot architectural projection without windows or entrance doors is 25-feet from the front property line and the building's entrance is proposed to be placed approximately 50 and 58 feet from the right-of-way. These buildings comply with the basic setback of the zone, but exceed that required in the Site Design and Use Standards. According to Enforcement section of the Ashland municipal Code, 18.112.070, where conditions imposed by a provision of this title are less restrictive than comparable conditions, imposed by...any other ordinance...or regulation, the more restrictive shall govern. This means that the site is subject to the 25-foot front setback of the M-1 zone, not the 20-foot setback of the Site Design and Use Standards.

The topography of the subject property, the building design and the zone setback all push the structure away from the sidewalk and street. Orientation of buildings to the street and providing pedestrian access from the public sidewalk are fundamental requirements that have shaped commercial developments for the last 30 years. However, staff believes that the building can be found to meet the applicable standards because the standard regarding the more restrictive standard regulates, that the use of the property is in accordance with the zone and will be industrial nature and function. Additionally, the buildings are interesting architecturally, with varied mass, roof forms, that create a strong sense of entry and comply with the standards for Basic Site review standards for site layout and design.

### **Parking**

The materials submitted note that based on 46,442 square feet, 46 parking spaces are required at the warehouse standard of one parking space per 1,000 square feet. According to staff's calculations the combined square foot area of the structures is 44,392 square feet. The would require 45 spaces at the warehouse standard. The applicant also has two fleet vehicles. The total number of spaces permitted including the permitted 10 percent overage allows for 50 spaces. There are also five on-street parking credits available to serve the two parcels. The site plan shows 52 parking spaces. Given the nature of the proposed use and because a portion of the each portion of the new buildings area is to provide manufacturing process storage space without a commensurate increase in the number of employees and no on-site retail component, staff believes that calculating the parking requirements based on an industrial standard rather than the office standard is appropriate.

As noted in the Site Design and Use Standards, "*[b]ecause parking areas are usually large in size to accommodate cars and trucks they are insensitive to the human scale. Additionally, noise, light, heat, and exhaust odors are commonly associated with parking areas. The ill effects associated with parking areas can be mitigated through good design and well placed landscaping.*" These concerns are the underlying basis for Ashland's parking lot landscaping and screening requirements, and at least part of the reason for the Off-Street Parking Chapter (AMC 18.92) limitations which cap parking provided at no more than ten percent above what is required for the use. This limitation avoids an auto-centric focus in development, while reducing the overall amount of paving installed, thus reducing the heat-island effect of summer sunlight reflecting off of asphalt while also allowing precipitation to be absorbed directly into the ground rather than creating excessive run-off and the associated water quality impacts. In staff's view, the parking proposed is more than necessary for the uses proposed, and should be reduced to include the 29 spaces between the buildings and the 7 spaces to the west of Building A. The remaining parking area currently identified could be reserved in landscaped / natural area to allow a phased future installation of the additional spaces should the demand or the nature of the use in place change. A condition requiring a revised site plan reflecting this phased installation for the site's parking is recommended below.

## **B. Test Track**

The proposed dirt test track will be located entirely on Tax lot 101 at 763 Jefferson. The track is proposed to be constructed of dirt, raked and watered to reduce dust. The products that Brammo Motorsports develops are electric motorcycles which make virtually no sound so noise ordinance issues will not be a problem. The applicant has proposed to gate the access to the test track and to sign it "No Trespassing" so as to prevent unauthorized use. A driveway curb cut will be installed to permit the bikes to be rolled from the adjacent parcel or driven for off-site locations. There will be no creek crossings permitted nor have they been proposed.

A delineated wetland (WD#2008-05950) is to be centered in the track with a minimum 20 foot buffer. The applicant has proposed a Conservation Easement. Staff is concerned that a Conservation Easement would unduly restrict the developable M-1 zoned property in the future as removal of conservation easements is a difficult process. Within the Water Resource Protection Zone code 18.63 there are provisions in place to address wetlands at the time of the parcels development that include reduction in the buffer widths, relocation of the wetland or a Variance if it can be shown that the application of this chapter unduly restricts the development or use of the lot and renders the lot not buildable. At this point unless the applicant insists on a Conservation Easement, providing the required 20-foot buffer and a management plan for the wetland and buffer zone will suffice for protection of the wetland consist with the code while not limiting the future development potential. A condition to this effect has been suggested.

## **C. Landscaping**

Two dead trees are proposed to be removed. They are exempt for the land use requirements. The remaining trees are proposed to be preserved and protected during construction. The trees are largely on the perimeter of the project at the southeast property line and along Knoll Creek.

A significant number of new trees (39) and shrubs are identified for planting on the landscape plans provided.

In considering the application, staff noted that there is one large Oak to be retained at the rear of the property near a proposed retaining wall. Given its proximity to the proposed construction, staff recommends that the arborist's recommendations be incorporated and implemented in accordance with the Tree Preservation and Protection Plan to include any additional necessary protective measures which would ensure the trees ongoing viability. Additionally, any disturbance to the Water Resource Protection zone and Conservation Easement for Knoll Creek shall be mitigated in accordance with the proposed landscaping plan.

### **III. Procedural - Required Burden of Proof**

The criteria for Site Review approval are described in 18.72.070 as follows:

The following criteria shall be used to approve or deny an application:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options. (Ord. 2655, 1991; Ord 2836 S6, 1999)

### **IV. Conclusions and Recommendations**

The application includes a request for Site Review approval since it involves the construction of new buildings in the M-1 zoning district. Site Review approvals for structures greater than 10,000 square feet require a public hearing before the Planning Commission.

Staff generally believes the proposed building design is appropriate for the area's manufacturing designation, and will complement the buildings in the vicinity. The proposed building is fairly consistent with the City's Basic Site Review Standards. The materials proposed reflect a utilitarian design comprised of metal roofing and siding appropriate to the zoning district. The proposal effectively executes these materials to create an attractive building which relates well to its surroundings. The generous number of clerestory windows provided will supply natural light to the interior spaces, while adding variation to the exterior. The requested Site Review complies with the applicable approval standards and the proposal is consistent with the zoning designation of the property. Brammo Motorsports is a locally-owned internationally-known business that currently employs 90 people.

Staff has raised a few issues in the body of this report, including the need to reduce the amount of parking initially provided through a phased parking installation. Overall, however, we believe that the application merits approval and that these issues can be satisfactorily addressed through conditions of approval. We accordingly recommend approval of the application with the following conditions attached:

- 1) That all proposals of the applicant shall be conditions of approval unless otherwise modified here.

- 2) That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this Site Review approval shall be submitted and approved prior to issuance of a building permit.
- 3) That a sign permit shall be obtained prior to installation of any new signage. Signage shall meet the requirements of Chapter 18.96.
- 4) That the track shall be maintained in accordance with the findings and shall not be used for competitions, shows or other public events without a Modification of this Site Review approval.
- 5) That prior to the submittal of a building permit:
  - a) That a stormwater drainage plan, including details of on-site detention for storm water and necessary water quality mitigation, shall be submitted for the review and approval of the Planning, Building, and Engineering Divisions. Post development peak flows shall be demonstrated to be less than or equal to pre-development levels.
  - c) A final utility plan for the project shall be submitted for the review and approval by the Planning, Engineering and Building Divisions prior to issuance of a building permit. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins. Utility installations, including any necessary fire protection vault, shall be placed outside of the pedestrian corridor, and necessary public utility easements on the property shall be shown on the building permit submittals.
  - d) The applicant shall submit an electric distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan shall be reviewed and approved by the Planning, Engineering and Electric Departments prior to building permit submittal. Transformers and cabinets shall be located outside of the pedestrian corridor, in those areas least visible from the right-of-way while considering the access needs of the Electric Department.
  - e) The building permit plan submittals shall include lot coverage calculations including all building footprints, driveways, parking, and circulation areas. These plans shall demonstrate that at least 15 percent of the site is surfaced in landscaping, and that at least seven percent of the parking lot

area is provided in required parking lot landscaping, as required in the Site Design & Use Standards.

- f) The building permit plan submittals shall include and sample exterior building colors and materials for review and approval of the Staff Advisor. The exterior building materials and paint colors shall be compatible with the surrounding area and consistent with those described in the application materials.
- 6) That prior to the issuance of a building permit:
- a) That a Verification Permit in accordance with 18.61.042.B shall be applied for and approved by the Ashland Planning Division prior to site work, storage of materials and/or issuance of a building permit. The Verification Permit is to inspect the identifications the installation of tree protection fencing to protect the trees to be retained. The tree protection fencing shall be installed according to the approved Tree Protection and Removal Plan, inspected and approved by the Staff Advisor prior to site work, storage of materials and/or issuance of a building permit.
  - b) That silt fencing or other erosion control methods to protect the Knoll Creek corridor and the wetland shall be installed and inspected by the Staff Advisor prior to the issuance of the building permit or prior to any site work. The silt fencing shall be installed in accordance with best management practices.
  - c) The applicant shall provide the approved Landscape/Irrigation Plan which addresses the Water Conserving Landscaping Guidelines and Policies of the Site Design and Use Standards, including irrigation controller requirements to allow multiple/flexible calendar programming with the building permit submittals.
  - d) All exterior lighting shall be appropriately shrouded so as not to permit direct illumination of any adjacent land. Lighting details, including a scaled plan and specifications detailing shrouding, shall be submitted to the Staff Advisor for review and approval with the building permit submittals.
  - e) At the time of building plan submittal, the bike rack details and shelter details shall be submitted for review and approval by the Staff Advisor. The building permit submittals shall verify that the bicycle parking design, spacing, and coverage requirements are met in accordance with 18.92.040.I.
  - f) Mechanical equipment shall be screened from view from Jefferson Avenue. The locations of mechanical equipment and any associated

screening shall be shown on the site plan and elevations in the building permit submittals.

- g) That the buildings shall meet Solar Setback B in accordance with 18.70.040.B. The building permit submittals shall demonstrate compliance with Solar Setback B.
  - h) The requirements of the Ashland Fire Department shall be satisfied, including that all addressing shall be approved prior to being installed; that fire apparatus access be provided and necessary fire apparatus easements identified and recorded; that adequate fire flow be provided and maintained; that fire sprinklers, hydrants, a Knox box, and/or an FDC be installed; and that an approved walkway to accommodate fire fighter access around and between both buildings be maintained. Including a Knox Box for the gate at the test track driveway entrance.
  - i) A revised site plan detailing a phased parking installation shall be provided for the review and approval of the Staff Advisor. In the initial phase, parking to be installed for occupancy of the building would be limited to the lot between the buildings and the seven spaces west of Building A. The remaining 18 parking spaces in the southeastern portion of the site are to be reserved in landscaped area pending future installation in a second phase should the parking demand or the nature of the use change to necessitate their installation.
  - j) That a management plan for the long-term preservation of the wetland and the required 20-foot wetland buffer consistent with 18.63.120 on Tax lot 101 at 763 Jefferson shall be provided for review and approval by the staff advisor.
- 7) That prior to the issuance of a certificate of occupancy:
- a) That the screening for the trash and recycling enclosure shall be installed in accordance with the Site Design and Use Standards.
  - b) All required parking areas excepting the spaces directly to the south of Building B shall be paved and striped. The additional spaces up to the maximum of 40 maybe installed when they become necessary.
  - c) All landscaping and the irrigation systems shall be installed in accordance with the approved plan, inspected and approved by the Staff Advisor prior to the issuance of a certificate of occupancy. That at the time of planting and prior to the issuance of a final certificate of occupancy, not less than two inches of mulch shall be added in all non-turf landscaped areas in the developed area after the installation of living plant materials.
  - e) That required bicycle parking spaces according to the approved plan and

in accordance with design and rack standards in 18.92.040.I and J, inspected, and approved by the Staff Advisor prior to issuance of a certificate of occupancy. Inverted u-racks shall be used for the bicycle parking.

## PROJECT NARRATIVE/FINDINGS

12.02.11

**PROJECT NAME:** Brammo Inc., Research and Design Facility

**TYPE OF PLANNING ACTION:** A request for a Site Review Approval(Chapter 18.72) for (2) Commercial/Industrial buildings in the E-1/M-1 Zone District(Chapter 18.40).

### **PROJECT INFORMATION:**

**OWNER:**

Brammo Inc.  
550 Clover Lane  
Ashland, OR 97520

**ARCHITECT:**

Ray Kistler, KSW Architects  
545 A St.  
Ashland, OR 97520

**SURVEYOR:**

Polaris Land Surveying, LLC  
PO Box 459  
Ashland, OR 97520

**LANDSCAPE ARCHITECT:**

Laurie Sager  
Laurie Sager and Assoc. Landscape Architects  
700 Mistletoe Road, Suite 201  
Ashland, OR 97520

**CIVIL ENGINEER:**

Jim Higday  
Hardey Engineering & Associates, Inc.  
2870 Nansen Drive  
Medford, OR 97501-0063

**PROJECT ADDRESS:**

763 & 777 Jefferson Avenue

**LEGAL DESCRIPTION:**

39-1E-14AC Tax Lot 101,102

**COMPREHENSIVE PLAN DESIGNATION:**

Industrial Manufacturing/Employment

**ZONING DESIGNATION:**

M-1/E-1

RECEIVED

DEC 02 2011

City of Ashland  
Field \_\_\_\_\_ Office \_\_\_\_\_ County \_\_\_\_\_

**SITE DATA:**

Area of Property: Lot 101 107,593 S.F.(2.47 acres)  
Lot 102 117,612 S.F.(2.70 acres)

Lot 101 to be utilized as Electric Motorcycle Dirt Test Track

Development of Lot 102:  
Pavement(Parking/Walkways/Plaza) 40,888 S.F. 34%  
Landscape/Recreation 19,900 S.F. 17%  
Building Footprint 33,692 S.F. 28%  
↳ Building A Footprint [14,857 S.F. of 33,662 S.F. = 44%]  
Building B Footprint [18,805 S.F. of 33,662 S.F. = 56%]

**PARKING REQUIRED:**

Total Spaces Required: 46,442 total square footage

Industrial Uses: 1 space/1,000 S.F. = 46 Spaces  
+ 10% of total spaces = 4.6 Spaces  
+ company fleet vehicles = 2 Spaces

**Total Spaces Allowable = 52 Spaces**

**PARKING PROVIDED:**

Total Spaces Provided:

Off-Street Spaces(Including ADA)= 52 Spaces  
Jefferson Street On-Street Credit= 5 Spaces(8'-0"x24'-0")

**Total Spaces Provided = 57 Spaces**

**BICYCLE PARKING REQUIRED/PROVIDED:**

Total Spaces Required:

1 per 5 req. Parking Spaces = 9 Spaces

**Total Spaces Required = 9 Spaces  
50% Sheltered**

Total Spaces Provided:

**Total Spaces Provided = 14 Spaces  
100% Sheltered**

**PUBLIC SPACES:**

**1,432 S.F. Provided at Front of Building(4.2%)**

**SITE DESCRIPTION:**

**LAND USE:**

In the general area of the subject property there currently exist a variety of industrial & manufacturing occupancies and uses.

These range from Modern Fan, Yerba Prima, Ashland Tennis & Fitness Club, Maranatha Foods and Oak Street Tank & Steel(see Vicinity/Zone Map and Aerial Photograph).

There is also a combination of zoning districts which include E-1, M-1, and the zoning subsets included in the Crowman Mill Overlay Area.

Uses and Zoning Districts immediately adjacent to the subject property are as follows:

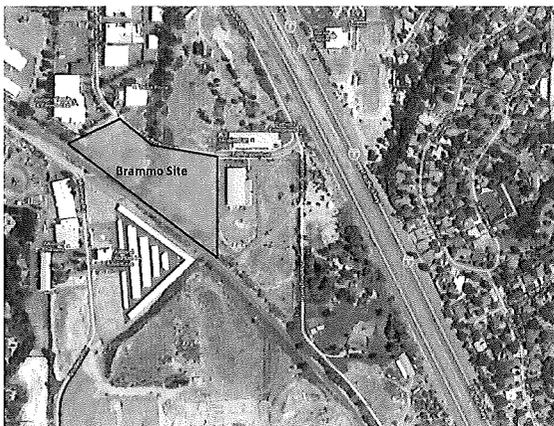
- North-Zoned E-1
- South-Zoned M-1
- West-Zoned M-1/E-1
- East- Zoned E-1

The subject parcels are Zoned E-1/M-1, at a ratio of proportionally 30% E-1/70% M-1.

The purpose of the E-1 District is to provide for a variety of uses such as office, retail, and manufacturing in an aesthetic environment while having a minimal impact on surrounding uses.

The purpose of the M-1 District is designed to encourage sound industrial development, while providing a protective environment exclusively for such development.

There are no yard requirements for this Zone Designation except those required in the Site Review, General Provisions and Solar Access chapters, specifically a five foot landscaped strip along the south property line, and no further yard requirements. The maximum allowable height of a building is 40 feet.



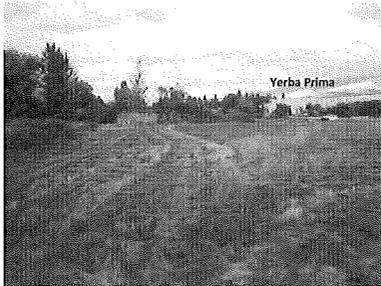
1.01 Aerial Photograph



1.02 Zone Map

**PHYSICAL CHARACTERISTICS:**

The proposed parcels, located at 763 & 777 Jefferson Ave., have a combined area of approximately 225,205 S.F.(5.17 acres) , and are currently undeveloped. The property slopes down to the northwest at a 3.4% fall,(refer to Topographic Survey). Views from Lot 102:



2.01 View from Southern Extent to North-West



2.02 View from Southern Extent to North

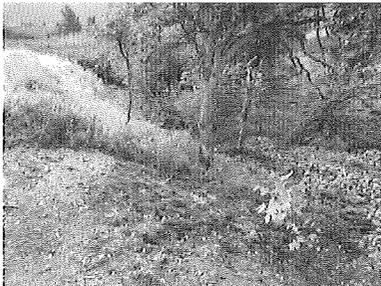
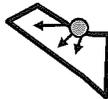


2.03 View from Southern Extent to North-East

**RIPARIAN CORRIDOR:**

There is a seasonal creek bed dividing the proposed parcels, and a 20'-0" conservation easement is observed on each parcel(refer to Topographic Survey). Stormwater mitigation facilities, i.e. bioswales and detention methods, will be utilized to clean and dose all stormwater runoff before returning it to the creek bed, thus minimizing the effect on city storm sewers. There is also a delineated wetland centered on Lot 101 and subject to a Water Resources Protection Zone ordinance and a 20'-0" buffer.

Views of Riparian Corridor:



2.01 View of East Riparian Embankment



2.02 View of West Riparian Corridor/Culvert



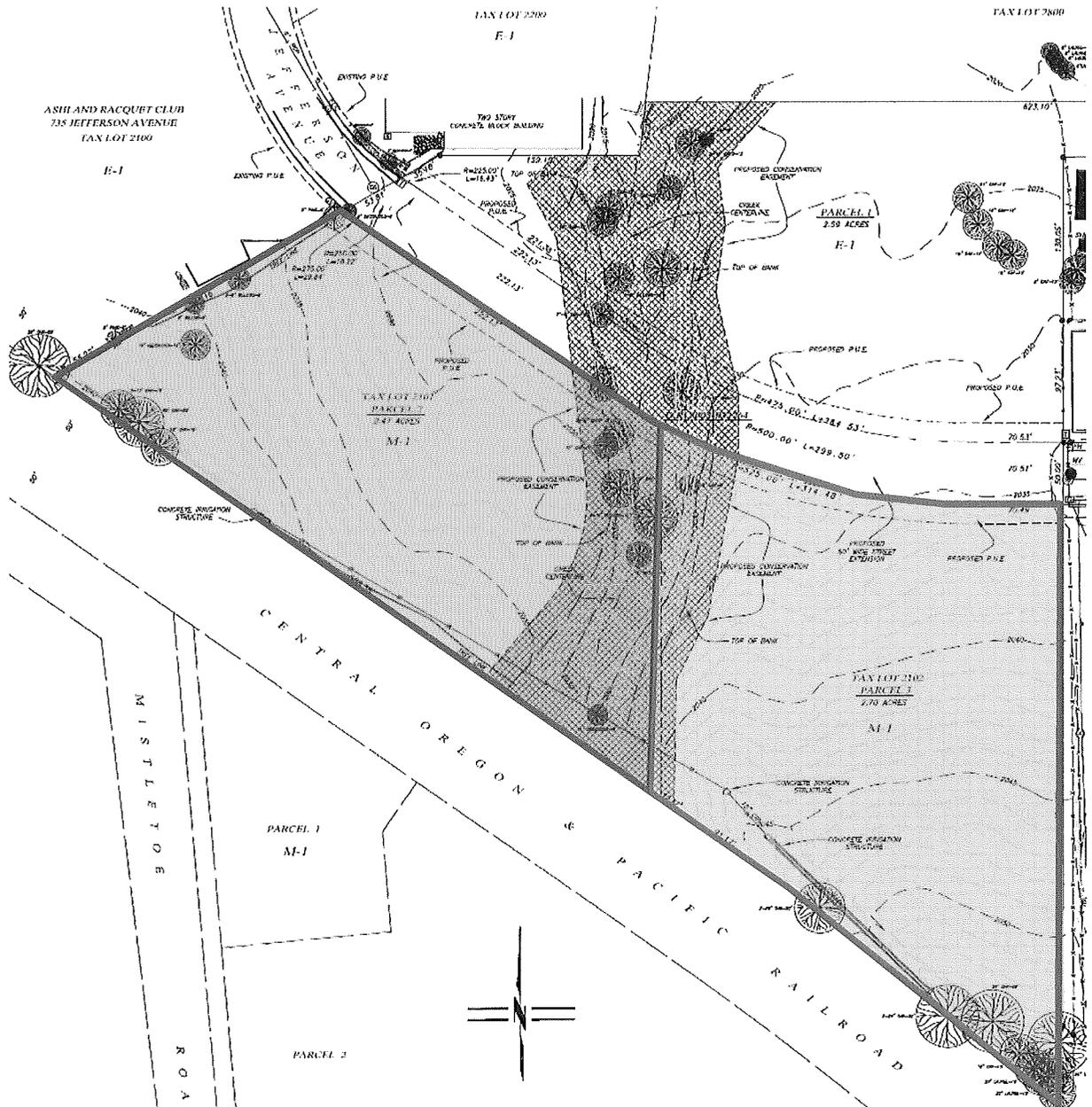
2.03 View of Wetland(Blackberry Bush) T.L.#101

**ACCESS TO TAX LOT 101:**

Vehicle access to the proposed Test Track on T.L. 101 will be via Jefferson Avenue. Motorcycles being transported to/from the adjacent property via Jefferson Avenue will either be walked(if not street legal), driven, or loaded onto an adequate transport vehicle.

**ACCESS TO TAX LOT 102:**

Vehicle access to the Brammo Research & Design Facility on T.L. 102 will be via Jefferson Avenue. There are two entry/exit aprons that will allow for access to the facility and subsequent turn-around for fire-safety vehicles. As the facility will not be used for manufacturing, traffic impact will be minimal as exhibited in the Traffic Impact Analysis, submitted as a part of these findings.

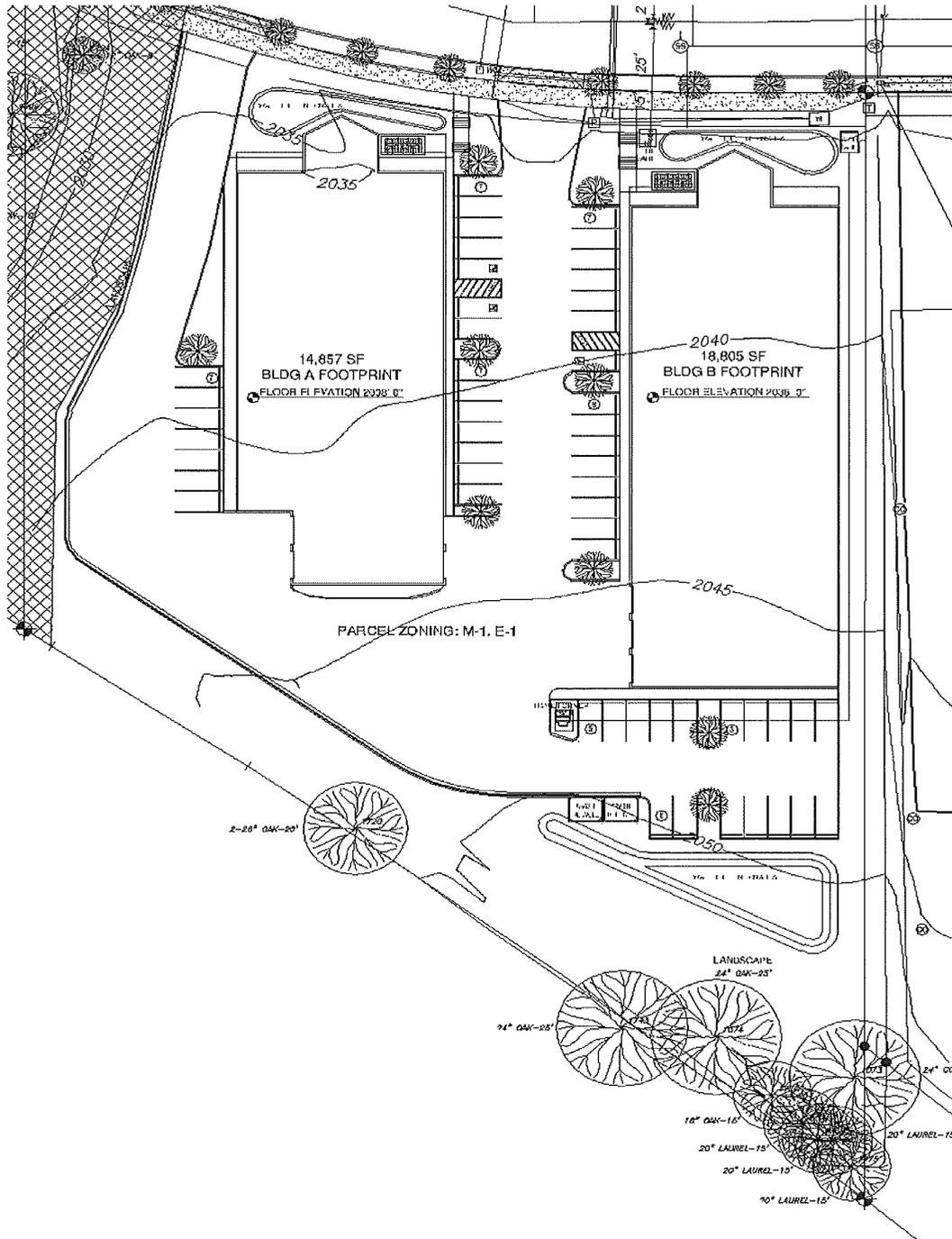


3.01 Survey of Existing Taxlots 2101 & 2102

**PROJECT DESCRIPTION:**

**REQUEST:**

The applicant seeks approval to utilize Tax Lot 2101 as a Prototype Test Track for the development and practical testing of electric motorcycles. Tax Lot 2102 would be utilized for the site of (2) Research & Design Facilities. These structures would primarily be used for product design development and staging of manufacturing assembly process. The ground floor of each structure would house a product showroom space, with primarily office, light manufacturing, and storage, comprising of the rest of the facilities.

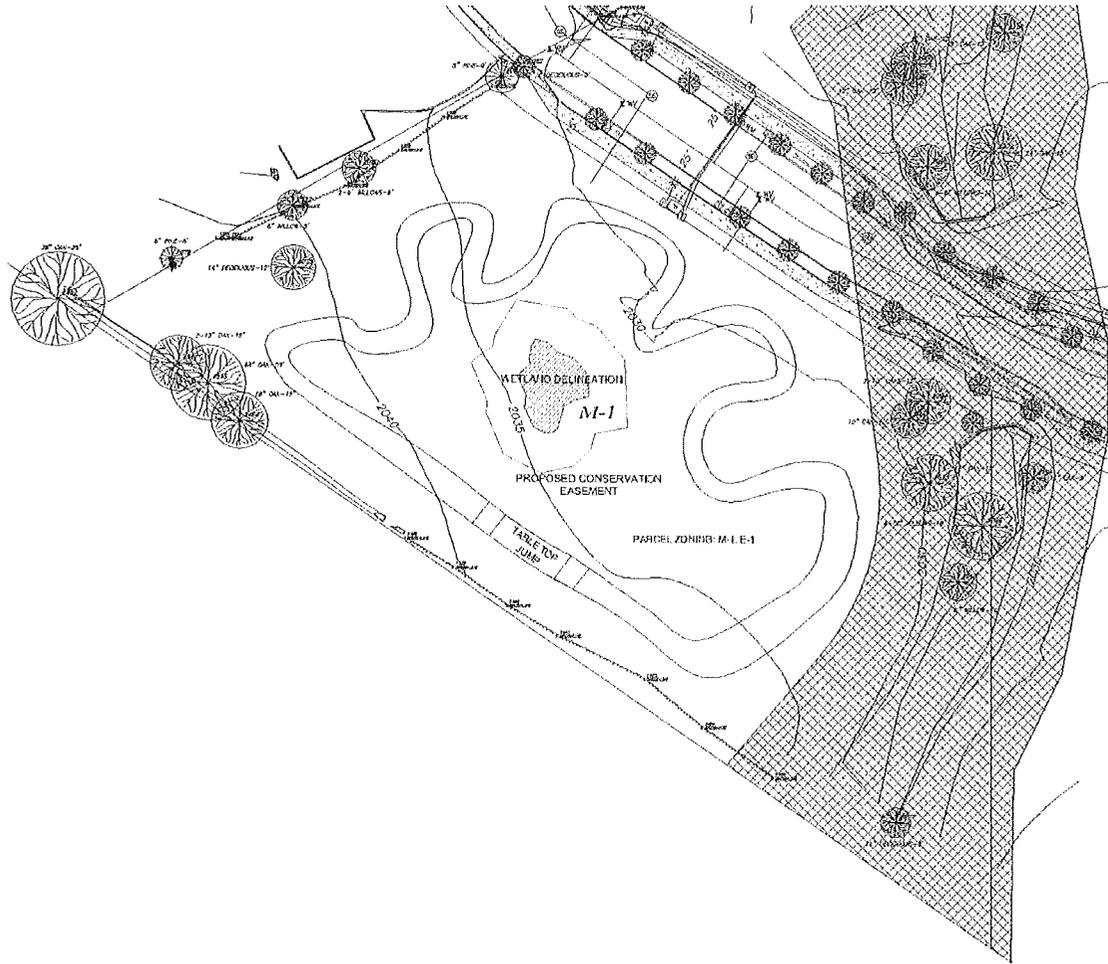


4.01 Site Plan of Proposed Brammo Research & Design Facilities

**BRAMMO RESEARCH & DESIGN FACILITIES**

**MANUFACTURING / EMPLOYMENT DISTRICT ZONING COMPLIANCE:**

The proposed usage of the Brammo Research & Design Facilities are in compliance with the M-1/E-1 Zone District(Chapter 18.40) in regard to permitted uses , setbacks, lot coverage, and height. Combined the buildings have a total lot coverage of 33,692 s.f., and will have a combined use of office space, research, light duty manufacturing, and storage, all in accordance with the statutes of M-1/E-1 zoning.



5.01 Site Plan of Proposed Brammo Research & Design Facilities Test Track

## BRAMMO RESEARCH & DESIGN DIRT TEST TRACK

### MANUFACTURING / EMPLOYMENT DISTRICT ZONING COMPLIANCE:

The proposed usage of the Brammo Research & Design Test Track is in compliance with the M-1/E-1 Zone District (Chapter 18.40) in regard to permitted uses, setbacks, lot coverage, and height. A driveway curbing cut will allow for access to the Test Track from Jefferson Avenue, and the Track will not infringe upon either the protected conservation easement surrounding the creek or the wetland delineation (WD#08-0595) found in the center of the site. The track will be maintained with a water truck to mitigate any dust, prior to usage, and after use the track will be watered and raked to nominal state.

The Brammo Electric Motorcycle in essence, does not create noise. The motorcycles using the track will be entirely electric, and therefore concerns of sound from the track will be non-existent, the motorcycles make no more sound than a conventional bicycle. The track will be utilized for private research only, and therefore will only have small groups of observers recording and analyzing the performance of the motorcycles in a practical application. The motorcycles will be walked from the R&D facility to the track site along Jefferson Avenue, and access to the private test track will be gated to prevent unauthorized use of the facility.

**ACCESS/PARKING:**

Access to the Brammo Motors Research & Design Test Track consists of one entrance/exit from Jefferson Avenue. The Research & Design Facility has two entrance/exit aprons both serving Jefferson Avenue.

The off-street parking ordinance requires 1 space/1,000 s.f. for industrial/manufacturing or 1 space/2 employees, whichever is less. Currently there is accommodation for a total of 52 spaces on the site, assessed at the lesser of the standards(1 space/1,000 s.f.) allocates 34 spaces. The 10% Max. Allowable Parking increase, and company fleet vehicle parking(1 space/company vehicle) allows for the additional 18 spaces. Three of these spaces are ADA compliant, as required per Section 18.92.030.

There are 5 additional On-Street parking spaces available, thus bringing the available parking to a sum total of 57 spaces. The Owner may seek to perform a phased installation of the parking, installing the surfacing for expanded parking areas at a later date, based on the growth of the business.

**LANDSCAPE/OPEN SPACE:**

The Preliminary Landscape Plan for this application provides all required shade trees for the proposed parking areas. This plan also designates appropriate plantings for the landscape areas adjacent to Knoll Creek and the existing bio-swale along Jefferson Avenue. In addition, the Landscape Plan for the proposed detention areas within the property boundaries shows a combination of plant material that will help to provide filtration for the captured on-site storm water and will provide an aesthetically pleasing landscape for the overall site. The plant material chosen for this project was selected to be drought tolerant, low maintenance and aesthetically appropriate for the project location, the proposed building architecture and its surrounding natural features including views and Knoll Creek.

**UTILITIES:**

Existing sewer, water, and storm sewer lines are located within the ROW of Jefferson Avenue and have been stubbed out to the public utility easement. Service connections will be made within the easement for the water and sewer connections. An additional water meter will be installed for the easterly building along with an FDC vault onto the existing water stub. One water meter currently exists which will serve the west building. The electrical/CTV/telephone utilities are located within the public utility easement. The storm drain system will be detained, filtered through bio-swales and then enter Knoll Creek via a energy dissipater.

3-Phase electrical service will be provided from existing infrastructure stubbed in to the northern border of the site, adjacent to Jefferson Avenue. In preliminary meetings with Dave Tygerson, a vault would be installed adjacent to Jefferson Avenue, and underground service lines would then run along a public utility easement on the Eastern Property Line and connect through the parking corridor to a transformer installed at the southern border of the parking area. This installation would allow for independent 3-Phase service to each of the proposed buildings.

**TRAFFIC IMPACT ANALYSIS:**

<FOLLOWING ATTACHMENT PER JOHN HARDEY, HEA ENGINEERING>

# Hardey Traffic Engineering Group, LLC

P.O. Box 876  
Medford, OR 97501  
541.855.8677 tel

Email:hardeytrafficgroup@clearwire.net

November 22, 2011

Jim Higday  
Hardey Engineering & Associates, Inc  
PO Box 1625  
Medford, Oregon 97501

## RE: Brammo Site Plan - Traffic Impacts

Dear Jim,

Our initial work on this project was to determine if a Traffic Impact Analysis or Traffic Impact Study would be needed for the planning application. The City of Ashland has established three criteria to determine this (noted in italics below). Following each criteria are our comments.

### *1. Trip Generation Threshold: 50 newly generated vehicle trips (inbound and outbound) during the adjacent street peak hour;*

The project as presented to us, has 2 new buildings which will access Jefferson Avenue. The purpose of the project is to manufacture motorcycles. Building "A" will have 21,247 square feet, and 60 employees. Building "B" will have 25,195 square feet, and 30 employees. Using the ITE Trip Generation Manual for Manufacturing (ITE Code 140) Weekday A.M. and P.M. Peak Hour of Generator for both building square footage and employees, the following table was generated.

<b>Manufacturing (140) Average Vehicle Trip Ends</b>			
<b>Peak Hour</b>	<b>Average Trip Rate x</b>	<b>Factor =</b>	<b>Total Trip Ends</b>
AM Peak	0.40	90 Employees	36
PM Peak	0.36	90 Employees	32.4
AM Peak	0.73	46.442 Thousand SF	33.9
PM Peak	0.74	46.442 Thousand SF	34.4

For either AM or PM Peak Hour, the project will not generate more than 50 newly generated vehicle trips during the adjacent street peak hour, therefore does not meet the criteria set forth in Threshold 1.

### *2. Mitigation Threshold: Installation of any traffic control device and/or construction of any geometric improvements that will affect the progression or operation of traffic traveling on, entering, or exiting the*

highway;

The project does not have direct access onto any highway and we are not anticipating installation of any traffic control devices or geometric improvements to any highways therefore the project does not meet the criteria set forth in Threshold 2.

**3. Heavy Vehicle Trip Generation Threshold: 20 newly generated heavy vehicle trips (inbound and outbound) during the day;**

According to information provided to us by the client, this project will only have “2 trucks a day fed-ex/ups deliveries” which will not meet the criteria set forth in Threshold 3.

Our work indicates that none of the criteria established in the Thresholds noted above, have been met, therefore no further traffic analysis is warranted.

If you have any questions or need additional clarification, please let me know.

Sincerely,



Hardey Traffic Engineering Group LLC  
John Hardey, PE, PLS  
Owner

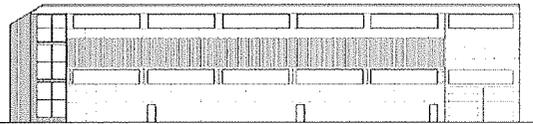


File:C:\Documents and Settings\John Hardey\My Documents\Corel User Files\John Files\Hardey Traffic Engineering Group\brammo.wpd

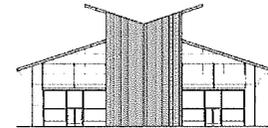
**ARCHITECTURE:**

The site of the proposed building is located within the E-1/M-1 zone of Jefferson Avenue. The applicant has provided digital renderings as well as exterior design elevations for the proposed building.

It is the intention of the applicant to provide a building that references both the character and industrial design aesthetic of European manufacturing facilities, while also utilizing current technologies and methods of sustainable design. Brammo, Inc. is focused on the progression of renewable energy and building systems, in keeping with the development of a carbon-free transportation solution, the building which houses their Research & Development strives to reflect this innovation and desire for excellence.



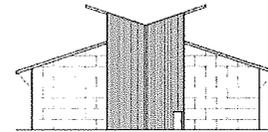
6.01 Building 'A' West Elevation



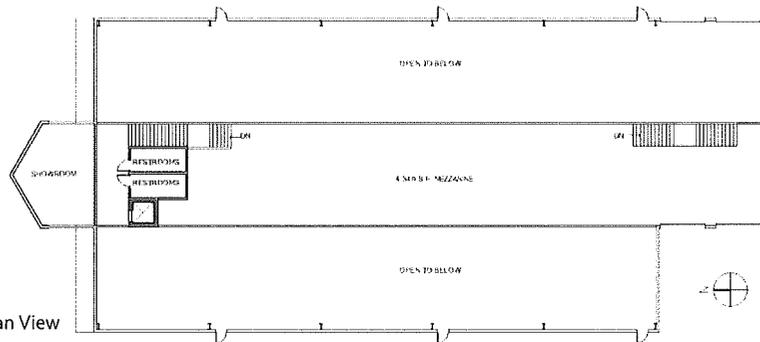
6.02 Building 'A' North Elevation



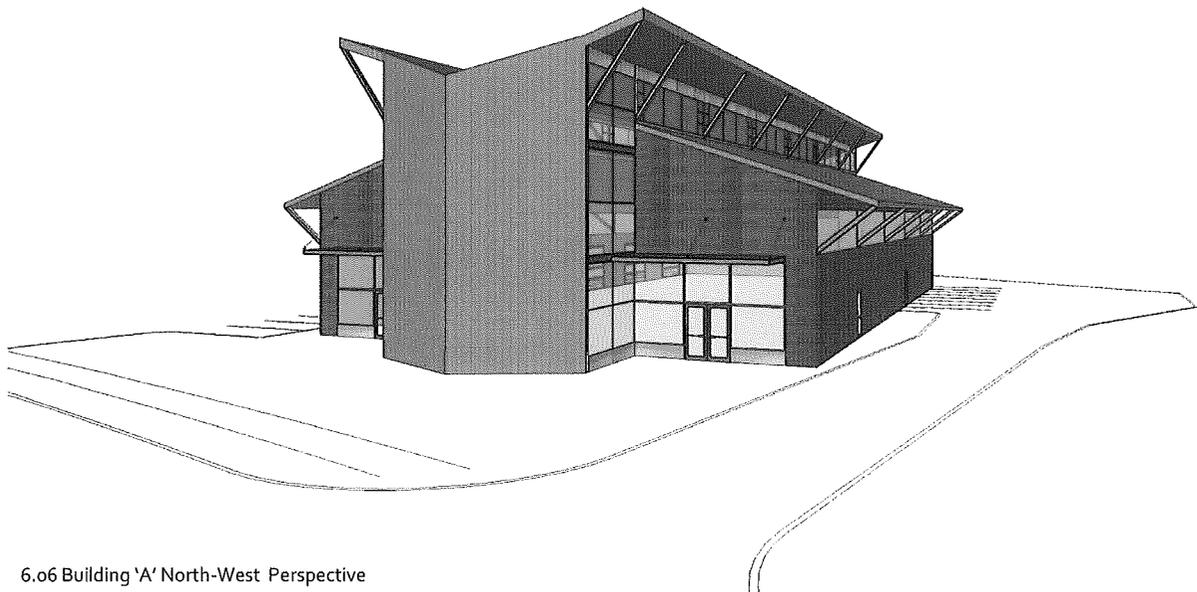
6.03 Building 'A' East Elevation



6.04 Building 'A' South Elevation



6.05 Building 'A' Plan View

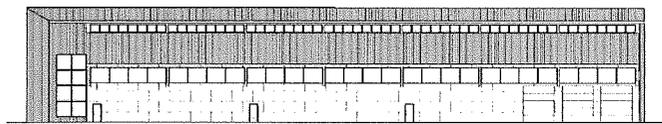


6.06 Building 'A' North-West Perspective

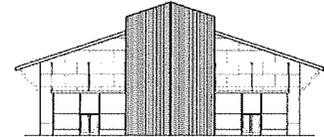
**USAGE:**

The Brammo Research & Development facilities will function as a multi-tiered space in regards to the development of electric motor technologies, product showroom, office/workspace, and low-impact manufacturing. The ability to design and test the motorcycles on site is essential, as the nature of the Brammo process is experimental and the direct application of theory to real-world performance is necessary for safe and efficient product development.

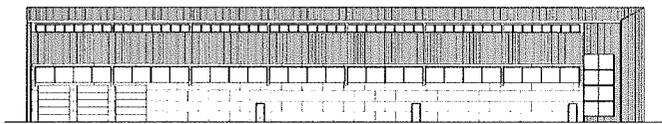
Building A & B, both allow for service truck access and are open in floor plan, allowing for modular development of the interior space, and maximum flexibility depending on the necessary use.



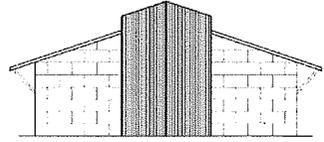
7.01 Building 'B' West Elevation



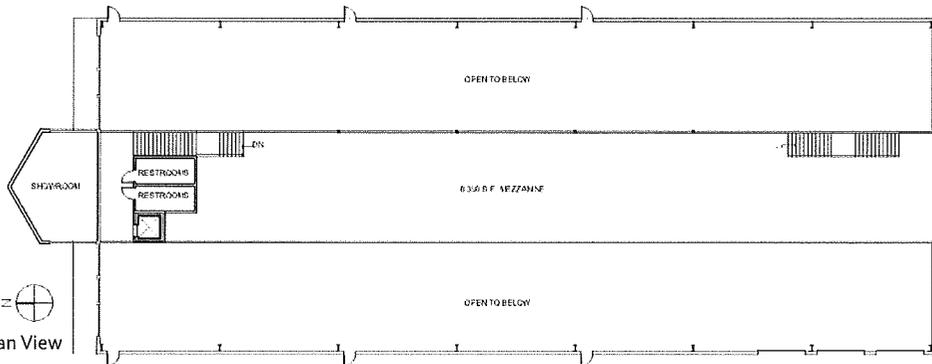
7.02 Building 'B' North Elevation



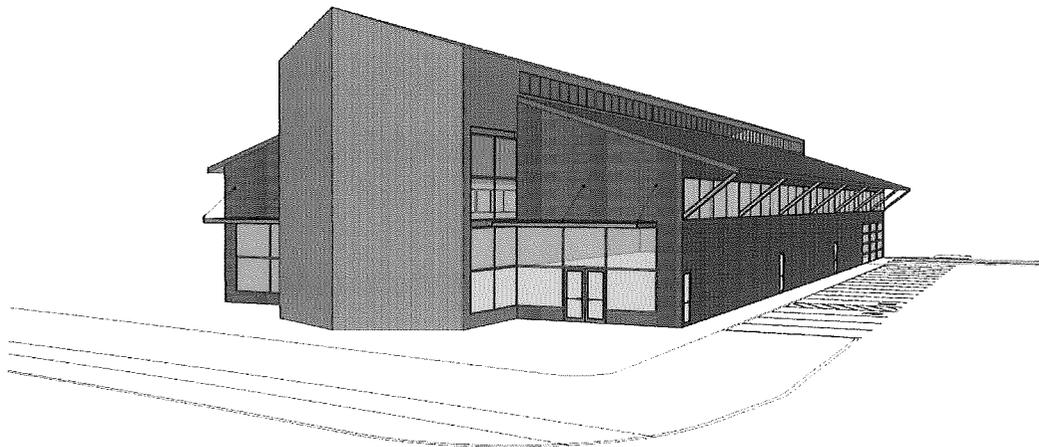
7.03 Building 'B' East Elevation



7.04 Building 'B' South Elevation



7.05 Building 'B' Plan View



7.06 Building 'A' North-West Perspective

## **FINDINGS:**

### **SITE REVIEW(Chapter 18.72)**

The planning Staff/Commission can approve a Site Review when the following criteria have been addressed:

**A. All applicable City Ordinances have been met or will be met by the proposed development.**

### **EMPLOYMENT ZONE (CHAPTER 18.40) & MANUFACTURING ZONE DISTRICT(CHAPTER 18.52)**

#### **PERMITTED USES**

The City's Comprehensive Plan and Zoning Map shows the subject property to be roughly 30% Employment E-1 and 70% Manufacturing M-1.

*"This district is designated to provide for a variety of uses such as office, retail or manufacturing in an aesthetic environment and having a minimal impact on surrounding uses."*

*"This district is designed to encourage sound industrial development in the City by providing a protective environment exclusively for such development."*

The applicant is proposing (2) buildings with 46,442 S.F. of office/manufacturing/storage space with industrial mezzanines on the second floor. This proposal is consistent with the City's designation indicated on the Comprehensive Plan Map which is subsequently zoned Employment/Manufacturing. The proposed project is also in conformance with the City's E-1 Zoning Designation, Chapter 18.40.020 which permits outright office, retail, and manufacturing uses.

#### **SETBACKS**

As shown on the Site Plan, the proposed project meets or exceeds the area, width, and yard requirements of the Zoning Designation. A five foot landscaped strip is installed along the abutting property line to the

#### **MAXIMUM BUILDING HEIGHT**

As shown on the Preliminary Exterior Elevations, the average height of the proposed buildings is 40'-0", which is in accordance with the maximum height requirement of 40'-0".

### **GENERAL REGULATIONS (CHAPTER 18.68)**

#### **FENCES**

There are no fences proposed on tax-lot 2102. However, there will be a gated entry to the test track and the Jefferson Avenue street frontage will be inaccessible to the general public.

#### **VISION CLEARANCE**

The site plan shows the vision clearance at the driveway apron entrances to Jefferson Avenue to allow for the clear vision triangle requirements.

## SPECIAL SETBACK REQUIREMENTS

The site has no specific setback requirements as an E-1 zoned lot.

## OFF-STREET PARKING/DRIVEWAY STANDARDS (CHAPTER 18.92)

The off-street parking ordinance for industrial/manufacturing usage requires 1 spaces/1,000 S.F. or 1 space/2 employees, whichever is less. In this case, Brammo currently has 90 employees requiring 45 parking spaces, and the total square footage of the proposed development is 44,392 ( 1 space/1,000 S.F. for industrial/manufacturing), therefore the lesser of the two being 44 spaces based on total square footage. The applicant is also utilizing the 10% maximum overage clause allowing for an additional 4 parking spaces, and parking for 4 fleet vehicles, thus bringing the total number of parking spaces to 52. The required ADA spaces for a parking lot of 51-75 spaces is 3, and the applicant is providing 3 ADA spaces each with van accessibility.

14 covered bicycle parking spaces are being provided(1 bicycle space/5 req. auto spaces), which is in accordance with the required statutes of 18.92.040. The applicant is providing the covered bicycle parking spaces at the north entry corner of each building.

## SOLAR ACCESS (CHAPTER 18.70)

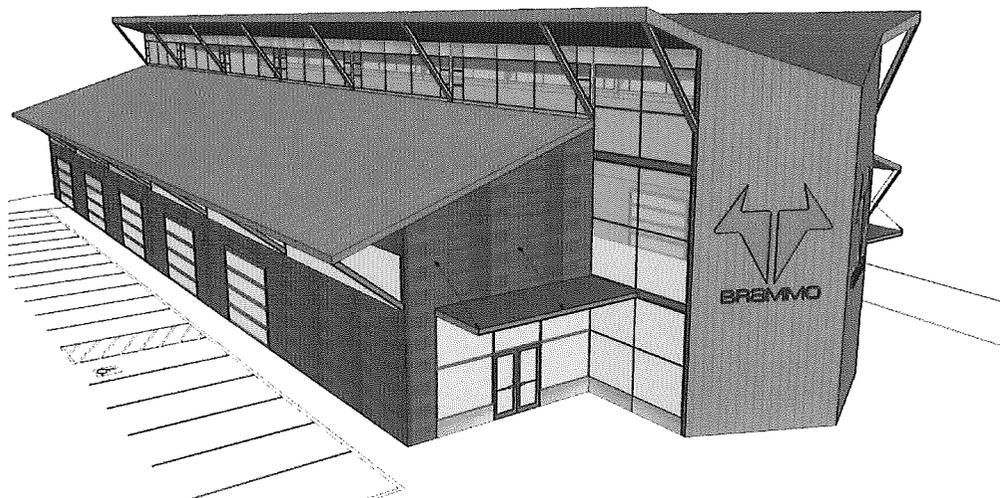
The buildings are subject to Solar Setback B, and are in accordance with the required setback as demonstrated:

$$40'-0''(\text{Height})-16'-0''/.445+(-.03)= \text{Required Solar Setback of } 58'-0''$$

Both of the proposed buildings on T.L. 2102 have a northern property line adjacent to Jefferson Avenue, and therefore the buildings do not infringe upon the required setback statute that shadows are greater than 16'-0'' at the Northern property line.

## SIGNAGE (CHAPTER 18.96)

The proposed signage will be incorporated into the entry of the plaza through the usage of a transparent building facade with a metal frame infrastructure referencing the logo of the Brammo logo. The intention is for the signage to be visible within the context of the entry, and allow for flexibility of display options.



8.01 Building 'A' Signage

**B. All requirements of the Site Review Chapter have been met or will be met.**

#### **SUBMITTAL REQUIREMENTS**

The applicant is providing a Narrative and Findings in addition to exhibits for written documentation. The extent of the Graphic Document includes: Site/Landscape/Utility Plans, Floor Plans, Proposed Colors and Materials, and Building Elevation drawings. This information is provided in the Application Package and addresses the submittal requirements of Chapter 18.72.060.

#### **LOT COVERAGE/LANDSCAPE/OPEN SPACE**

The Site and Landscape plans delineate the paving, buildings and landscape areas. The area of paved surfaces is approximately 40,888 S.F.[34%] with 33,692 S.F.[28%] of the site being utilized as building footprint. The E-1 zone district requires a minimum of 15% of the lot area to be landscaped, and the M-1 zone district requires a minimum of 10% total lot area to be landscaped. As the project is primarily in the M-1 zone[70%+ zoned M-1], it still retains 30% of its zoning in the E-1 classification. As such the applicant is providing 19,900 S.F. of landscaping[17%] to fulfill the requirements of both zones.

#### **TRASH/RECYCLING**

The applicant has located (2)trash/recycle areas adjacent to the southern parking area and the 5'-0" buffer zone on the south property line, see fig. 4.01. The area will be screened from view by a 5'-0" tall metal / wood sliding gate.

#### **LIGHT AND GLARE**

Pole lights will be installed at 14'-0" maximum height will have fixtures directing light downward, per city specifications.

**C. The development complies with the Site Design Standards adopted by the City Council for the implementation of this Chapter.**

#### **II-C-1.BASIC SITE REVIEW STANDARDS**

**Approval Standard: Development in all commercial and employment zones shall conform to the following development standards:**

##### **II-C-1a. ORIENTATION AND SCALE**

*Buildings shall have their primary orientation toward the street rather than the parking area. Building entrances shall be functional, and shall be accessed from a public sidewalk. Public sidewalks shall be provided adjacent to a public street frontage.*

*Buildings that are within 30 feet of the street shall have an entrance for pedestrians directly from the street to the building interior. This entrance shall be designed to be attractive and functional, and shall be open to the public during all business hours.*

As shown on the Preliminary Landscape Site Plan, Floor Plan, and Elevations, the entrance(s) are oriented toward Jefferson Avenue and the public sidewalk. The applicant seeks to preserve the sense of scale in the transition from Jefferson Avenue to the building, and provide an entry area for pedestrians and building inhabitants complete with bicycle parking and sheltered entrances to the building.

It is important to the applicant that the site and building be designed to preserve the pedestrian friendly feeling of the street and maintain the integrity of the bioswale element prevalent on the Jefferson Avenue streetscape. The stylized detention ponds and plantings between the proposed buildings and the streetscape will further enhance the overall pedestrian friendly theme that exists in this zone by creating an interesting and aesthetically friendly design for observation as people move past and through the site.

#### **II-C-1b. ORIENTATION AND SCALE**

*1. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street.*

Street trees currently exist on Jefferson Avenue as shown on the Preliminary Landscape Plan.

#### **II-C-1c. LANDSCAPING**

*1. Landscaping shall be designed so that 50% coverage occurs after one year and 90% coverage occurs after 5 years.*

*2. Landscaping design shall use a variety of low water deciduous and evergreen trees and shrubs and flowering plant species.*

*3. Buildings adjacent to streets shall be buffered by landscaped areas at least 10 feet in width, except in the Ashland Historic District. Outdoor storage areas shall be screened from view from adjacent public right-of-ways, except in M-1 zones. Loading facilities shall be screened and buffered when adjacent to residentially zoned land.*

*4. Irrigation systems shall be installed to assure landscaping success.*

*5. Efforts shall be made to save as many existing healthy trees and shrubs on the site as possible.*

The Landscape Plan has been designed so that 50% coverage occurs after one year and 90% coverage occurs after 5 years. The Landscape Plan has also been designed in accordance with low-water usage standards, which have in effect reduced the potable water use for irrigation by 50% from a calculated mid-summer baseline case. This will put the landscape irrigation system well within the standards established by the City of Ashland.

There are currently 10 existing trees located within the area proposed for construction at 777 Jefferson Avenue including; 6 – *Populus trichocarpa*, 3 – *Quercus kelloggii* and 1 – *Quercus garryana*. In addition, there is 1 – *Quercus kelloggii* - #9, just outside of the Southern property line which has been identified for protection on the Preliminary Tree Protection and Removal Plan. The design team has gone to extensive effort to preserve all trees identified and has intentionally avoided any unnecessary work within the banks of Knoll Creek. There are a number of naturalized trees within the Knoll Creek banks which have not been identified individually, which will not be impacted by the proposed project.

In addition to the trees noted above, there are 8 existing trees located at 763 Jefferson Avenue including; 4 – *Quercus kelloggii*, 3 – *Populus trichocarpa*, and 1 - *Pinus densiflora*. These trees are to be protected and preserved.

#### **II-C-1d. PARKING**

- 1. Parking areas shall be located behind buildings or on one or both sides.*
- 2. Parking areas shall be shaded by deciduous trees, buffered from adjacent non-residential uses and screened from non-residential uses.*

As shown in fig. 4.01, the parking areas are located adjacent to the proposed buildings with access off of Jefferson Avenue. There will be deciduous trees planted within the proposed parking islands and along the western edge of the parking area adjacent to Knoll Creek. In addition, there are two proposed retaining walls with a 5'-0" wide planter area(buffer) along the southern boundary, adjacent to the existing property line.

#### **II-C-1f. NOISE AND GLARE**

- 1. Special attention to glare (AMC18.72.110) and noise (AMC9.08.170(c) & AMC9.08.175) shall be considered in the project design to insure compliance with these standards.*

The proposed usage of the Brammo Research & Design Test Track is in compliance with the M-1/E-1 Zone District(Chapter 18.40) in regard to permitted uses , setbacks, lot coverage, and height. A driveway curb-cut will allow for access to the Test Track from Jefferson Avenue, and the Track will not infringe upon either the protected conservation easement surrounding the creek or the wetland delineation(WD#08-0595) found in the center of the site. The track will be maintained with a water truck to mitigate any dust, prior to usage, and after use the track will be watered and raked to nominal state.

The Brammo Electric Motorcycle in essence, does not create noise. The motorcycles using the track will be entirely electric, and therefore concerns of sound from the track will be minimal. The track will be utilized for private research only, and therefore will only have small groups of observers recording and analyzing the performance of the motorcycles in a practical application. The motorcycles will be walked from the R&D facility to the track site along Jefferson Avenue, and access to the private test track will be gated to prevent unauthorized use of the facility. In regards to potential glare from the test track, all test bikes will be a matte black finish, and thus negate any issues with surrounding properties and potential glare from the motorcycles while in the testing phase.

#### **II-C-1g. EXPANSIONS OF EXISTING SITES AND BUILDINGS**

- 1. For sites which do not conform to these requirements, an equal percentage of the site must be made to comply with these standards as the percentage of building expansion, e.g., if building area is to expand by 25%, then 25% of the site must be brought up to the standards required by this document.*

Not applicable.

## **D. PARKING LOT LANDSCAPING AND SCREENING STANDARDS**

**Approval Standard:** All parking lots, which for purposes of this section include areas of vehicle maneuvering, parking, and loading shall be landscaped and screened as follows:

### **II-D-1. SCREENING AT REQUIRED YARDS**

*1. Parking abutting a required landscaped front or exterior yard shall incorporate a sight obscuring hedge screen into the required landscaped yard.*

*The screen shall grow to be at least 36 inches higher than the finished grade of the parking area, except for required vision clearance areas.*

*The screen height may be achieved by a combination of earth mounding and plant materials.*

*Elevated parking lots shall screen both the parking and the retaining wall.*

The Preliminary Landscape Plan complies with the required parking lot screening as outlined above. Prudent use of plant material to screen the proposed parking lots has been incorporated into the proposed design. While the parking areas are at a higher elevation than Jefferson Avenue, retaining walls have not been used to elevate these lots. Natural topography requires that the building and parking elevations be raised. Due to the elevated prominence of the lots, the proposed planting has been designed to adequately screen all parking areas to a minimum height of 36 inches.

### **II-D-2. SCREENING ABUTTING PROPERTY LINES**

*1. Parking abutting a property line shall be screened by a 5'-0" landscaped strip. Where a buffer between zones is required, the screening shall be incorporated into the required buffer strip, and will not be an additional requirement.*

The Site and Landscape Plans show a 10'-0" wide buffer (plus 6" curb) adjacent to the east property line. The proposed landscape has been designed to both screen and buffer the parking area from adjacent development.

### **II-D-3. LANDSCAPE STANDARDS:**

*Parking lot landscaping shall consist of a minimum of 7% of the total parking area plus a ratio of 1 tree for each seven parking spaces to create a canopy effect.*

*The tree species shall be an appropriate large canopied shade tree and shall be selected from the street tree list to avoid root damage to pavement and utilities, and damage from droppings to parked cars and pedestrians.*

*The tree shall be planted in a landscaped area such that the tree bole is at least 2 feet from any curb or paved area.*

*The landscaped area shall be planted with shrubs and/or living ground cover to assure 50% coverage within 1 year and 90% within 5 years.*

**B. All requirements of the Site Review Chapter have been met or will be met.**

#### **SUBMITTAL REQUIREMENTS**

The applicant is providing a Narrative and Findings in addition to exhibits for written documentation. The extent of the Graphic Document includes: Site/Landscape/Utility Plans, Floor Plans, Proposed Colors and Materials, and Building Elevation drawings. This information is provided in the Application Package and addresses the submittal requirements of Chapter 18.72.060.

#### **LOT COVERAGE/LANDSCAPE/OPEN SPACE**

The Site and Landscape plans delineate the paving, buildings and landscape areas. The area of paved surfaces is approximately 40,888 S.F.[34%] with 33,692 S.F.[28%] of the site being utilized as building footprint. The E-1 zone district requires a minimum of 15% of the lot area to be landscaped, and the M-1 zone district requires a minimum of 10% total lot area to be landscaped. As the project is primarily in the M-1 zone[70%+ zoned M-1], it still retains 30% of its zoning in the E-1 classification. As such the applicant is providing 19,900 S.F. of landscaping[17%] to fulfill the requirements of both zones.

#### **TRASH/RECYCLING**

The applicant has located (2)trash/recycle areas adjacent to the southern parking area and the 5'-0" buffer zone on the south property line, see fig. 4.01. The area will be screened from view by a 5'-0" tall metal / wood sliding gate.

#### **LIGHT AND GLARE**

Pole lights will be installed at 14'-0" maximum height will have fixtures directing light downward, per city specifications.

**C. The development complies with the Site Design Standards adopted by the City Council for the implementation of this Chapter.**

#### **II-C-1.BASIC SITE REVIEW STANDARDS**

**Approval Standard: Development in all commercial and employment zones shall conform to the following development standards:**

##### **II-C-1a. ORIENTATION AND SCALE**

*Buildings shall have their primary orientation toward the street rather than the parking area. Building entrances shall be functional, and shall be accessed from a public sidewalk. Public sidewalks shall be provided adjacent to a public street frontage.*

*Buildings that are within 30 feet of the street shall have an entrance for pedestrians directly from the street to the building interior. This entrance shall be designed to be attractive and functional, and shall be open to the public during all business hours.*

As shown on the Preliminary Landscape Site Plan, Floor Plan, and Elevations, the entrance(s) are oriented toward Jefferson Avenue and the public sidewalk. The applicant seeks to preserve the sense of scale in the transition from Jefferson Avenue to the building, and provide an entry area for pedestrians and building inhabitants complete with bicycle parking and sheltered entrances to the building.

*Landscaped areas shall be evenly distributed throughout the parking area and parking perimeter at the required ratio.*

*That portion of a required landscaped yard, buffer strip or screening strip abutting parking stalls may be counted toward required parking lot landscaping but only for those stalls abutting landscaping as long as the tree species, living plant material coverage and placement distribution criteria are also met. Front or exterior yard landscaping may not be substituted for the interior parking stalls.*

The proposed impervious parking area has been calculated at x S.F., and the proposed parking lot landscaping has been calculated at x S.F. This proposed parking lot landscaping is x% of the total parking lot area. This design complies with the 7% minimum requirement. The proposed landscape areas have been located to provide planting throughout the parking lots. The trees, shrubs, and groundcovers selected for this project comply with the above standards and will provide the coverage required over the allotted amount of time noted above.

#### **II-D-4. RESIDENTIAL SCREENING:**

*Parking areas adjacent to a residential dwelling shall be set back at least 8 feet from the building, and shall provide a continuous hedge screen.*

Not applicable.

#### **II-D-5. HEDGE SCREENING:**

*The required hedge screen shall be installed as follows:*

- 1. Evergreen shrubs shall be planted so that 50% of the desired screening is achieved within 2 years, 100% within 4 years.*
- 2. Living groundcover in the screen strip shall be planted such that 100% coverage is achieved within 2 years.*

The Landscape Plan shows compliance with this finding where appropriate.

#### **II-D-6. OTHER SCREENING:**

- 1. Other screening and buffering shall be provided as follows:*

***Refuse Container Screen:*** *Refuse containers or disposal areas shall be screened from view by placement of a solid wood fence or masonry wall from five to eight feet in height. All refuse materials shall be contained within the refuse area.*

***Service Corridor Screen:*** *When adjacent to residential uses, commercial and industrial service areas shall reduce the adverse effects of noise, odor and visual clutter upon adjacent residential uses.*

***Light and Glare Screen:*** *Artificial lighting shall be so arranged and constructed as to not produce direct glare on adjacent residential properties or streets.*

The Site and Landscape Plans show a trash/recycle area to be located to the South of the parking lot, along the Southern Parking Area. This area will be screened with a masonry wall, and steel gate . All wall mounted lighting within the site will be directed towards the ground.

## **E. STREET TREE STANDARDS**

**APPROVAL STANDARD:** All development fronting on public or private streets shall be required to plant street trees in accordance with the following standards and chosen from the recommended list of street trees found in this section.

### **II-E-1. LOCATION FOR STREET TREES**

*Street trees shall be located behind the sidewalk except in cases where there is a designated planting strip in the right-of-way, or the sidewalk is greater than 8 feet wide. Street trees shall include irrigation, root barriers, and generally conform to the standards established by the Department of Community Development.*

N/A Street trees for this project already exist along Jefferson Avenue.

### **II-E-2. SPACING, PLACEMENT, AND PRUNING OF STREET TREES**

*All tree spacing may be made subject to special site conditions which may, for reasons such as safety, affect the decision. Any such proposed special condition shall be subject to the Staff Advisor's review and approval. The placement, spacing, and pruning of street trees shall be as follow:*

- 1. Street trees shall be placed at the rate of one tree for every 30 feet of street frontage. Trees shall be evenly spaced, with variations to the spacing permitted for specific site limitations, such as driveway approaches.*
- 2. Trees shall not be planted closer than 25 feet from the curb line of intersections of streets or alleys, and not closer than 10 feet from private driveways (measured at the back edge of the sidewalk), fire hydrants, or utility poles.*
- 3. Street trees shall not be planted closer than 20 feet to light standards. Except for public safety no new light standard location shall be positioned closer than 10 feet to any existing street tree, and preferably such locations will be at least 20 feet distant.*
- 4. Trees shall not be planted closer than 2 ½ feet from the face of the curb except at intersections where it shall be 5 feet from the curb, in a curb return area.*
- 5. Where there are overhead power lines, tree species are to be chosen that will not interfere with those lines.*
- 6. Trees shall not be planted within 2 feet of any permanent hard surface paving or walkway. Sidewalk cuts in concrete for trees shall be at least 10 square feet; however, larger cuts are encouraged because they allow additional air and water into the root system and add to the health of the tree. Space between the tree and such hard surface may be covered by permeable non-permanent hard surfaces such as grates, bricks on sand, or paver blocks.*
- 7. Trees, as they grow, shall be pruned to provide at least 8 feet of clearance above sidewalks and 12 feet above street roadway surfaces.*
- 8. Existing trees may be used as street trees if there will no damage from the development which will kill or weaken the tree.*

*Sidewalks of variable width and elevation may be utilized to save existing street trees, subject to approval by the Staff Advisor.*

N/A Street trees for this project already exist along Jefferson Avenue.

**II-E-3. Replacement of Street Trees**

*Existing street trees removed by development projects shall be replaced by the developer with those from the approved street tree list. The replacement trees shall be of size and species similar to the trees that are approved by the Staff Advisor.*

N/A Street trees for this project already exist along Jefferson Avenue.

**II-E-4. Recommended Street Trees**

*Street trees shall conform to the street tree list approved by the Ashland Tree Commission.*

N/A Street trees for this project already exist along Jefferson Avenue.

**SECTION 18.61.080 Criteria for Issuance of Tree Removal Permit.**

*An applicant for a Tree Removal Permit shall demonstrate that the following criteria are satisfied. The Staff Advisor may require an arborist's report to substantiate the criteria for a permit.*

*B. Tree that is Not a Hazard: The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:*

- 1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards, including but not limited to applicable Site Design and Use Standards and Physical and Environmental Constraints. The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and*
- 2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and*
- 3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property.*

The applicant requests a Tree Removal Permit for the 2 – *Populus trichocarpa* trees, which are dead, and have been identified on the Preliminary Tree Protection and Removal Plan. The design team has worked hard to avoid disturbance to existing live trees and has made every effort to avoid any construction impact to these trees. Tree protection fencing is also shown on the Preliminary Tree Protection and Removal Plan.

## **SECTION 18.63.080 Water Resource Protection Zone Reductions**

*A Water Resource Protection Zone may be reduced by up to 25% through a Type I land use procedure, and by greater than 25% and up to 50% through a Type II land use procedure to allow alteration within the Water Resource Protection Zone based upon findings that the following approval criteria have been satisfied.*

*A. The proposed use or activity is designed to avoid intrusion into the Water Resource Protection Zone through the use of up to a 50% reduction of any dimensional standards (e.g. required front, side and rear yard setbacks; required distance between buildings) to permit development as far outside or upland of the Water Resource Protection Zone as possible. Such adjustment to any applicable dimensional standards shall be reviewed as part of the requested reduction, and shall not be subject to a separate Variance application under Chapter 18.100. Reductions to dimensional standards may not be used to reduce required Solar Access setbacks without evidence of agreement by the effected property owner(s) to the north through a concurrent Solar Access Variance application as described in section 18.70.060.*

*B. The alteration of the Water Resource Protection Zone is the minimum necessary to efficiently perform the proposed activity and/or use. The proposed development shall minimize disturbance to the Water Resource Protection Zone by utilizing the following design options to minimize or reduce impacts of development.*

- 1. Multi-story construction shall be considered.*
- 2. Parking spaces shall be minimized to no more than that required as a minimum for the use.*
- 3. Pavement shall be minimized, and all pavement used shall be installed and maintained in a pervious paving material.*
- 4. Engineering solutions shall be used to minimize additional grading and/or fill.*

*C. The application demonstrates that equal or better protection for identified resources will be ensured through restoration, enhancement and mitigation measures. The structures, functions and values of the Water Resource will be restored through the implementation of a restoration and enhancement strategy set forth in a mitigation plan prepared in accordance with the standards and requirements described in section 18.63.120.*

*D. Long term conservation, management and maintenance of the Water Resource Protection Zone shall be ensured through preparation and recordation of a management plan as described in section 18.63.120.C, except a management plan is not required for residentially zoned lots occupied only by a single-family dwelling and accessory structures.*

The Brammo Research & Design Facilities and Test Track, are observing all necessary conservation easements surrounding the creek and the delineated wetland(WD#08-0595). Currently there is a minimum 20'-0" conservation easement surrounding wetland WD#08-0595, and a 25'-0" conservation easement surrounding the creek. Both the proposed R&D facilities and practice track are in observance and accordance with the Water Resource Protection zone and not seeking a reduction in size or scope.

## **SECTION 18.63.090 Hardship Variances**

*Hardship Variances shall be processed as a Type II land use procedure. Hardship Variances are not subject to the Variance requirements of Chapter 18.100. The approval authority may approve or approve with conditions a request for a Hardship Variance based upon findings that the following approval criteria have been satisfied.*

*A. The application of this chapter unduly restricts the development or use of the lot, and renders the lot not buildable.*

*B. The proposed activity or use of land would have been permitted prior to the effective date of this ordinance.*

*C. The applicant has explored all other reasonable options available under this chapter and throughout the Ashland Land Use Ordinance to relieve the hardship.*

*D. Adverse impacts on the structures, functions or values of the resource including water quality, erosion, or slope stability that would result from approval of this Hardship Variance have been minimized and will be mitigated to the greatest extent possible through restoration and enhancement of the Water Resource Protection Zone in accordance with a mitigation plan prepared in accordance with the standards and requirements in section 18.63.120.*

*E. Long term conservation, management and maintenance of the Water Resource Protection Zone shall be ensured through preparation and recordation of a management plan as described in section 18.63.120.C, except a management plan is not required for residentially zoned lots occupied only by a single-family dwelling and accessory structures.*

The Brammo Research & Design Facilities and Test Track, are observing all necessary conservation easements surrounding the creek and the delineated wetland(WD#08-0595). Currently there is a minimum 20'-0" conservation easement surrounding wetland WD#08-0595, and a 25'-0" conservation easement surrounding the creek. Both the proposed R&D facilities and practice track are in observance and accordance with the Water Resource Protection zone and not seeking a variance regarding the Water Resource Protection Zone.

# BRAMMO, INC.

## RESEARCH AND DEVELOPMENT FACILITY

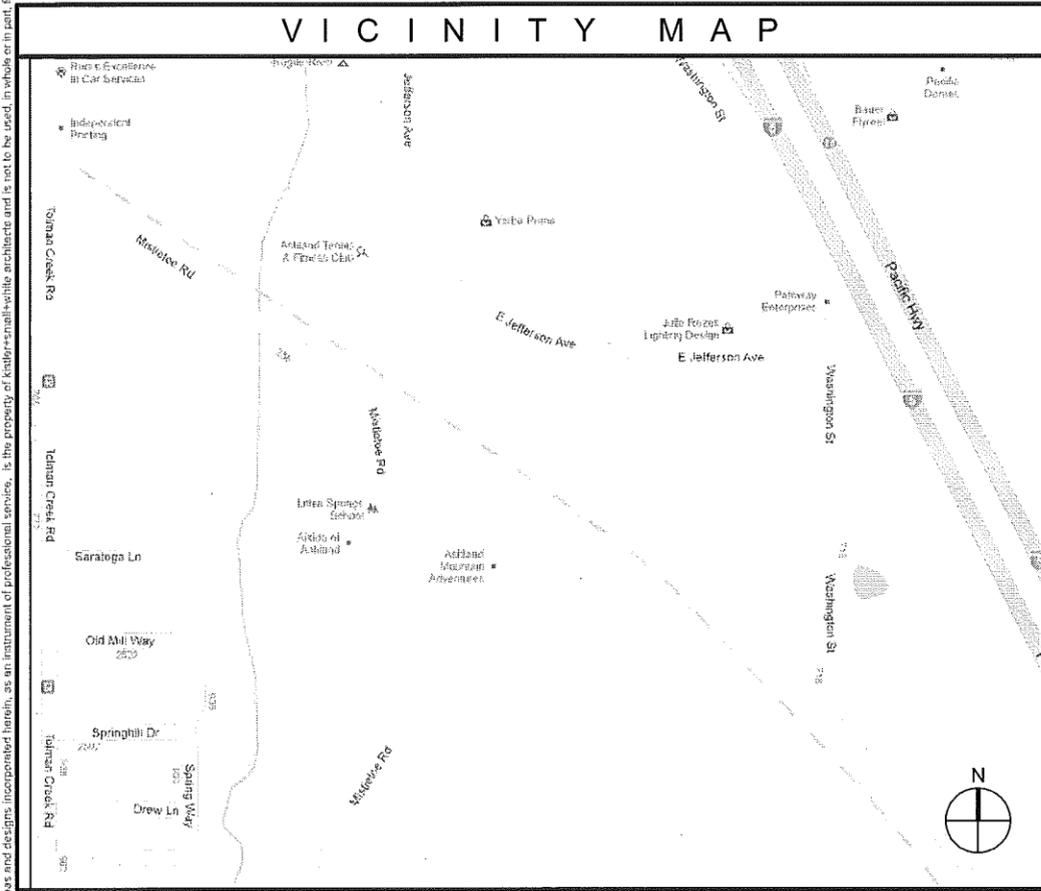
kistler+  
small  
+white  
architects

545 A STREET  
ASHLAND, OR  
97520

TEL.: 541.488.8200

This document, and the ideas and designs incorporated herein, are an instrument of professional service, is the property of kistler+small+white architects and is not to be used, in whole or in part, for any other project without the written authorization of kistler+small+white architects.

OWNER	ARCHITECT	CIVIL ENGINEER	LANDSCAPE ARCHITECT	STRUCTURAL ENGINEER	MECHANICAL ENGINEER	ELECTRICAL ENGINEER
<b>BRAMMO, INC.</b> CONTACT: CRAIG BRAMSCHER 550 CLOVER LANE ASHLAND, OR (541) 482-9555 (P) (541) 552-0414 (F) cbramscher@brammo.com	<b>kistler small + white architects</b> CONTACT: RAY KISTLER 545 A STREET SUITE 3 ASHLAND, OREGON 97520 (541) 488-8200 (P) (541) 552-9512 (F) ray@kistlersmallwhite.com (E)	<b>HARDEY ENGINEERING</b> CONTACT: JIM HIGDAY 2870 NANSEN DRIVE MEDFORD, OR 97501-0063 (541) 772-6880 (P) jhigday@hea-inc.com	<b>LAURIE SAGER &amp; ASSOC.</b> CONTACT: LAURIE SAGER 700 MISTLETOE ROAD, SUITE 201 ASHLAND, OR 97520 (541) 488-1446 (P) (541) 488-0636 (F) lauriesager@lauriesager.com	XXX CONTACT: XX ADDRESS LINE 1 ADDRESS LINE 2 (541) XXX-XXXX (P) (541) XXX-XXXX (F) EMAIL ADDRESS	XXX CONTACT: XX ADDRESS LINE 1 ADDRESS LINE 2 (541) XXX-XXXX (P) (541) XXX-XXXX (F) EMAIL ADDRESS	XXX CONTACT: XX ADDRESS LINE 1 ADDRESS LINE 2 (541) XXX-XXXX (P) (541) XXX-XXXX (F) EMAIL ADDRESS



### LEGENDS AND SYMBOLS

<p><b>DETAIL</b>                  DETAIL IDENTIFICATION                  DWG. NO. WHERE DETAIL IS DRAWN</p> <p><b>WALL CROSS SECTION</b>                  SECTION IDENTIFICATION                  DWG. NO. WHERE SECTION IS DRAWN</p> <p><b>BUILDING CROSS SECTION</b>                  SECTION IDENTIFICATION                  DWG. NO. WHERE SECTION IS DRAWN</p> <p><b>EXTERIOR ELEVATION</b>                  ELEVATION IDENTIFICATION                  DWG. NO. WHERE SECTION IS DRAWN</p> <p><b>INTERIOR ELEVATION</b>                  ELEVATION IDENTIFICATION                  DWG. NO. WHERE SECTION IS DRAWN</p> <p><b>GRID LINE</b>                  TOP TO BOTTOM: LETTERS                  LEFT TO RIGHT: NUMBERS</p> <p><b>OFFICE</b>                  ROOM IDENTIFICATION                  ROOM NAME                  ROOM NUMBER</p> <p><b>NORTH ARROW</b>                  ARROW INDICATES PLAN NORTH</p>	<p><b>DOOR IDENTIFICATION</b>                  NO: ROOM NUMBER OF DOOR                  LETTER: DOOR IDENTIFICATION</p> <p><b>KEY NOTE</b>                  KEY NOTE NUMBER</p> <p><b>WINDOW IDENTIFICATION</b>                  WINDOW NUMBER                  GLAZING TYPE</p> <p><b>RELITE IDENTIFICATION</b>                  RELITE TYPE</p> <p><b>PARTITION IDENTIFICATION</b>                  PARTITION TYPE</p> <p><b>O.F.C.I. ITEM</b>                  ITEM NUMBER</p> <p><b>CABINET DIMENSION</b>                  W.I.C. CABINET TYPE NUMBER                  CABINET LENGTH (FIELD VERIFY)</p> <p><b>CABINET DEPTH</b></p> <p><b>ELEVATION TAG</b>                  LEVEL LINE = ELEVATION</p> <p><b>CENTER LINE</b></p> <p><b>LOUVER IDENTIFICATION</b>                  LOUVER NUMBER</p> <p><b>HVAC EXHAUST</b></p> <p><b>HVAC RETURN</b></p> <p><b>HVAC SUPPLY</b></p> <p><b>LIGHTING SYMBOL</b></p> <p><b>LIGHTING SYMBOL</b></p> <p><b>LIGHTING SYMBOL</b></p>
---	--

SEE INDIVIDUAL SHEETS FOR ADDITIONAL LEGENDS AND SYMBOLS

### ABBREVIATIONS

NOTE: ABBREVIATIONS BELOW MAY NOT INCLUDE ALL FOUND IN THE DRAWINGS

ABV ABOVE AC ASPHALTIC CONCRETE ACT ACOUSTIC CEILING TILE AFF ABOVE FINISHED FLOOR ALT ALTERNATE ASW AREA SEPARATION WALL BD BOARD BLDG BUILDING B.O. BOTTOM OF BM BEAM BS BACKSPASH CAB CABINET CFCI CONTRACTOR FURNISHED CONTRACTOR INSTALLED CH COAT HOOK CJ CONTROL JOINT CLG CEILING CLR CLEAR CMU CONCRETE MASONRY UNIT COL COLUMN CONC CONCRETE CONF CONFERENCE CONT CONTINUOUS CORR CORRIDOR CPT CARPET CR CLASSROOM CTB COMPUTER TERMINAL BOARD CTSK COUNTERSINK DBL DOUBLE DIA DIAMETER DIMS DIMENSIONS DF DRINKING FOUNTAIN DS DOWNSPOUT DW DISHWASHER DP DISABLED PERSON E EXISTING EA EACH EJ EXPANSION JOINT ELE ELECTRICAL ELECT ELECTRICAL ELEV ELEVATOR EM ENTRANCE MATTING EMT ELECTRICAL METALLIC TUBING EQ EQUAL EX EXISTING (E) EXISTING EXT EXTERIOR FD FLOOR DRAIN FE FIRE EXTINGUISHER FEC FIRE EXTINGUISHER & CABINET FEC FIRE EXTINGUISHER CABINET FF FINISHED FLOOR FFE FINISHED FLOOR ELEVATION FG FINISH GRADE FHMS FLAT HEAD SHEET METAL SCREWS FIN FINISH (ED) FND FEMININE NAPKIN DISPENSER F.O.C. FACE OF CONCRETE F.O.C. FACE OF COLUMN F.O.FIN. FACE OF FINISH F.O.M. FACE OF MASONRY F.O.S. FACE OF STUD / STRUCTURE FP FIREPLACE FR FIRE RESISTANT FRMG FRAMING FRZR FREEZER FTG FOOTING GA GAUGE GA GYPSUM ASSOCIATION GALV GALVANIZED GYP BD GYPSUM BOARD GB GRAB BAR GI GALVANIZED IRON GS GALVANIZED STEEL HS HALL STATION (ELEVATOR) H HIGH HDBD HARDBOARD HDR HEADER HDWD HARDWOOD HDT HEIGHT HR HOUR HVAC HEATING/VENTILATION/ AIR CONDITIONING HW HOT WATER HC HANDICAP INSUL INSULATION IR INSIDE RADIUS JST JOIST KS KNEE SPACE LAV LAVATORY LPG LIQUID PROPANE GAS	M MIRROR MAX MAXIMUM MACH MACHINE MB MACHINE BOLT MB MARKER BOARD MDL MODEL MECH MECHANICAL MED MEDIUM MWC MEDFORD WATER COMMISSION MFR MANUFACTURER MIW MAKE IT WORK MIN MINIMUM MISC MISCELLANEOUS MPH MILES PER HOUR MTL METAL MW MICROWAVE (N) NEW NIC NOT IN CONTRACT O/ OVER OCC OCCUPANCY OCF OZONE CHEMICAL FEED BUILDING OPNG OPENING OC ON CENTER OD OUTSIDE DIAMETER OFCI OWNER FURNISHED OFCI CONTRACTOR INSTALLED OFCI OWNER FURNISHED OFCI OWNER INSTALLED P PAINT P LAM PLASTIC LAMINATE PSF POUNDS PER SQ. FOOT PWC PLASTIC WALL COVERING PWP PLASTIC WALL PANEL PT PRESSURE TREATED RD ROOF DRAIN REF REFRIGERATOR RM ROOM RO ROUGH OPENING SD SOAP DISPENSER SEP SEPARATION SF SQUARE FEET SQ FT SQUARE FEET SHTG SHEATHING SIM SIMILAR SM SHEET METAL SMS SHEET METAL SCREWS SPECS SPECIFICATIONS STB SINGAL TERMINAL BACKBOARD STL STEEL STRUCT STRUCTURAL SS STAINLESS STEEL SST STAINLESS STEEL TPD TOILET PAPER DISPENSER TRSFR TRANSFORMER/TRANSFER TS TUBE STEEL TTB TELEPHONE TERMINAL BACKBOARD TVB DATA TERMINAL BACKBOARD TYP TYPICAL TH THRESHOLD TO TOP OF TC TOP OF CONCRETE / CURB TW TOP OF WALL UL UNDERWRITER'S LABORATORY UTIL UTILITY UNO UNLESS NOTED OTHERWISE UON UNLESS OTHERWISE NOTED VCT VINYL COMPOSITION TILE VERT VERTICAL VEST VESTIBULE V-A TYPE A, RATED V-B TYPE B, NON-RATED VWC VINYL WALL COVERING WBG WALL BUMPER GUARD WC WATER CLOSET WD WOOD WDW WINDOW WH WATER HEATER WTP WATER TREATMENT PLANT WF WIDE FLANGE WS WOOD SCREW W.I.C. WOODWORK INSTITUTE W/ WITH
--	--

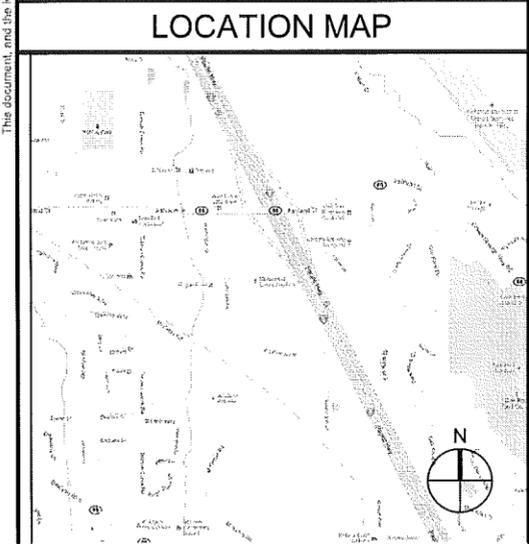
### DRAWING INDEX

GENERAL:  
 A0.1 GENERAL INFORMATION

CIVIL:  
 C1.0 GRADING & UTILITY PLAN

ARCHITECTURAL:  
 A1.1 SITE PLAN  
 A1.2 SITE PLAN  
 A1.3 BUILDING 'A' ELEVATIONS  
 A1.4 BUILDING 'A' ELEVATIONS  
 A1.5 BUILDING 'B' ELEVATIONS  
 A1.6 BUILDING 'B' ELEVATIONS

LANDSCAPE:  
 L1.0 PRELIMINARY TREE PROTECTION/REMOVAL PLAN  
 L1.1 PRELIMINARY TREE PROTECTION/REMOVAL PLAN  
 L2.0 PRELIMINARY LANDSCAPE SITE PLAN  
 L3.0 PRELIMINARY LANDSCAPE PLAN



### GOVERNING CODES

THE DESIGN OF THIS PROJECT IS BASED ON THE FOLLOWING CODES:

- OREGON STRUCTURAL SPECIALTY CODE, 2010 ed.
- OREGON MECHANICAL SPECIALTY CODE, 2010 ed.
- OREGON PLUMBING SPECIALTY CODE, 2008 ed.
- OREGON ELECTRICAL SPECIALTY CODE, 2008 ed.
- OREGON FIRE CODE, 2010 ed.
- NFPA 72: NATIONAL FIRE ALARM CODE, 2010 ed.
- NFPA 13: AUTOMATIC SPRINKLER SYSTEMS, 2010 ed.

### MATERIALS LEGEND

<p><b>EARTH</b></p> <p><b>GRAVEL / ROCK</b></p> <p><b>SAND / GROUT</b></p> <p><b>CONCRETE</b></p> <p><b>CMU</b></p> <p><b>BRICK</b></p> <p><b>FRAMED WALL - NEW</b></p> <p><b>(E) CONCRETE</b></p> <p><b>(E) CMU</b></p>	<p><b>BATT INSULATION</b></p> <p><b>RIGID INSULATION</b></p> <p><b>CEILING / ACOUSTIC TILE / PANEL</b></p> <p><b>METAL</b></p> <p><b>DIMENSIONAL LUMBER</b></p> <p><b>BLOCKING / SHIM</b></p> <p><b>FINISH WOOD</b></p> <p><b>PLYWOOD</b></p>
--	---

NOTE: ALL MATERIAL SYMBOLS ABOVE MAY NOT NECESSARILY BE USED IN THIS PROJECT. SEE INDIVIDUAL SHEETS FOR ADDITIONAL LEGENDS AND SYMBOLS

BRAMMO, INC.  
 RESEARCH AND DEVELOPMENT FACILITY  
 OWNER: CRAIG BRAMSCHER

REVISIONS

GENERAL INFORMATION

PROJECT: 11-025

ISSUE DATE: 12-02-11

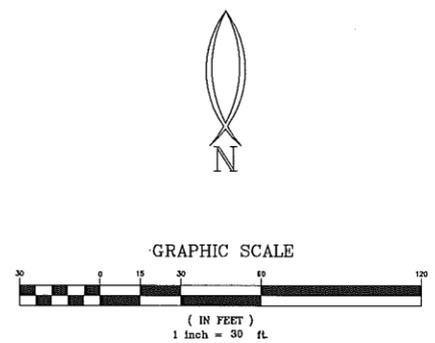
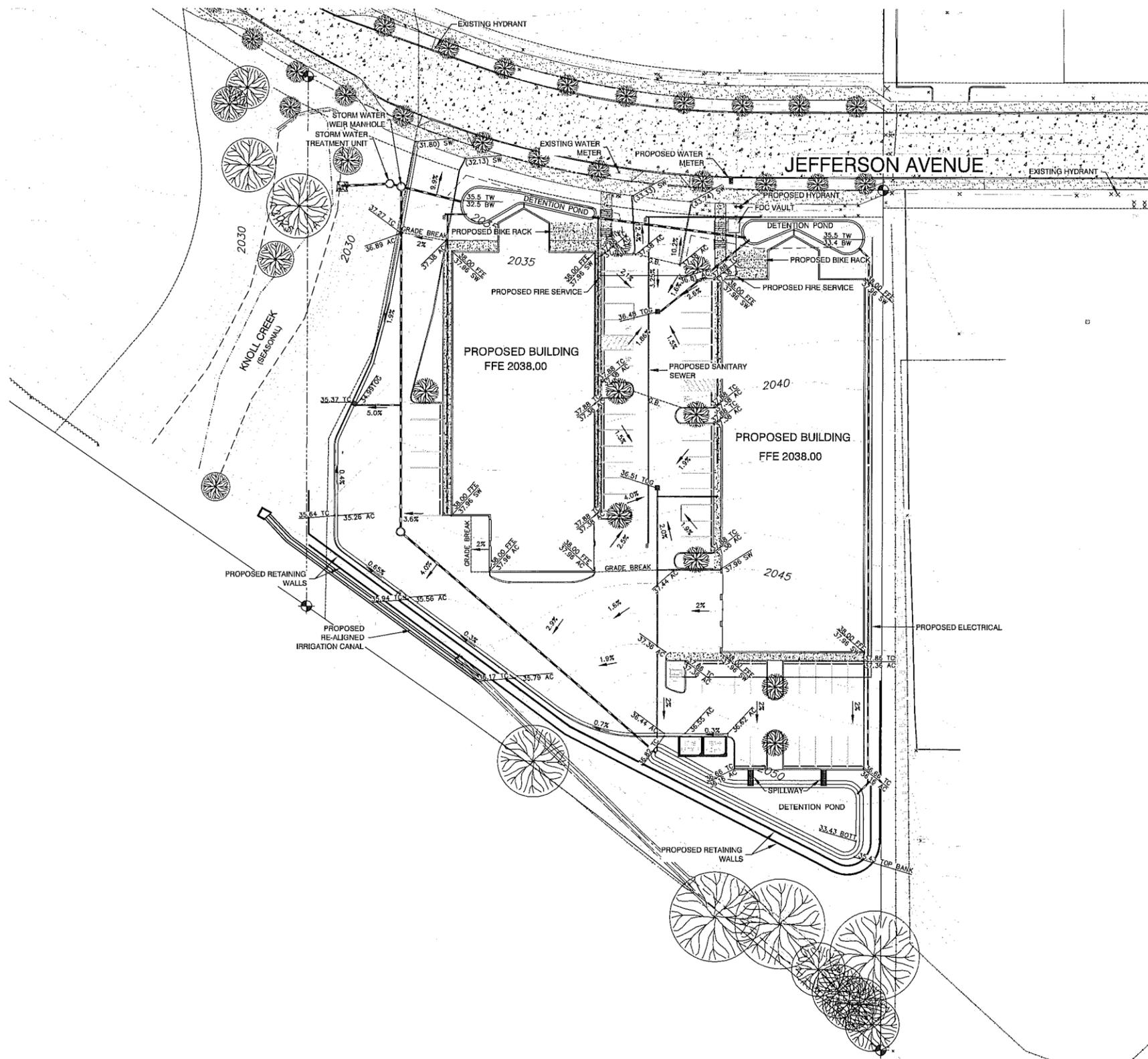
SHEET:

**A0.1**

RECEIVED

DEC 02 2011

City of Ashland  
 Field Office County



SW 1/4 OF THE  
 NE 1/4 OF SECTION 14  
 T 39 S, R 1 E, W.M.  
 MAP: 39-1E-14AC

TAX LOT 102  
 ASHLAND, OREGON  
 DEC 2, 2011

- LEGEND:
- AC - ASPHALT CONCRETE
  - TC - TOP OF CURB
  - DRAINAGE FLOW
  - TW - TOP OF WALL
  - BW - BOTTOM OF WALL
  - FFE - FINISH FLOOR ELEVATION
  - X.XX - SLOPE
  - ( ) - EXISTING GRADE

**FEA**  
 ENGINEERING INTEGRITY  
 Hardey Engineering &  
 Associates, Inc.  
 P.O. BOX 1625  
 MEDFORD, OREGON 97501  
 PHONE: 541-772-3530  
 FAX: 541-772-3533  
 EMAIL: info@fea-inc.com



GRADING & UTILITY PLAN  
 FOR  
 BRAMMO SPORTS

ASHLAND, OREGON

BRAMMO SPORTS

PROJECT	BRAMMO SPORTS
DATE	12/2/11
BY	JH
CHECKED BY	JH
DATE	12/2/11
PROJECT NO.	12345
CITY OF ASHLAND	
BRAMMO SPORTS	
GRADING PLAN	
SHEET	C1
OF 1	

RECEIVED

City of Ashland  
 Field \_\_\_ Office \_\_\_ County \_\_\_

This document and the data herein are the property of FEA, Inc. and are not to be used, in whole or in part, for any other project without the written authorization of FEA, Inc.

This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler+Small+White architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler+Small+White architects.

**PLANNING SUMMARY:**

GOVERNING AUTHORITY: CITY OF ASHLAND  
**APPLICABLE CODES:**  
 BUILDING: OREGON STRUCTURAL SPECIALTY CODE, 2010 ed.  
 MECHANICAL: OREGON MECHANICAL SPECIALTY CODE, 2010 ed.  
 PLUMBING: OREGON PLUMBING SPECIALTY CODE, 2008 ed.  
 ELECTRICAL: OREGON ELECTRICAL SPECIALTY CODE, 2008 ed.  
 FIRE: OREGON FIRE CODE, 2010 ed.

- 1 PROPOSED 10'-0" P.U.E.
- 2 PROPOSED GAS UTILITY EXTENSION & CONNECTION TO BUILDING
- 3 PROPOSED FIRE HYDRANT LOCATION & CONNECTION TO WATER MAIN
- 4 PROPOSED 1 1/2" WATER METER

CONSTRUCTION TYPE: II-B  
 PARCEL NUMBER: 391E 14AC, LOT 101,102  
 ZONING CLASSIFICATION: M1 W/ E1 OVERLAY  
 OCCUPANCY GROUPS: F1, B  
 OCCUPANCY SEPARATION: NO  
 SPRINKLERS: YES  
 ALLOWABLE AREA: TABLE 503  
 SPRINKLER INCREASE: F1 = 15,500 SF PER BUILDING  
 200% = 31,00 SF PER BUILDING

**PROPOSED PARKING**

STANDARD: 49  
 HANDICAPPED: 3  
 VANPOOL: 3 (1 REQUIRED)  
 TOTAL PROVIDED: 52  
 BIKE PARKING: 14

**BUILDING AREA TOTAL:**

44,392 SF	REQ'D PARKING
44,392 SF	44 (ONE SPACE PER/1000 SF)
	+4 (10% INCREASE PER 18.92)
	48 + (4) FLEET VEHICLES = 52 TOTAL

**SITE AREA:**

117,612 SF (2.70 ACRES)  
 STRUCTURE AREA: 33,692 SF (28%)  
 LANDSCAPE AREA: 19,900 SF (17%) > 9,539 SF (10% M-1 LANDSCAPE REQ'T)  
 PAVED AREA: 40,888 SF (34%)  
 DEVELOPED: 74,580 SF (63%)  
 BUFFER STRIP: N/A

**ENERGY CONSERVATION:**

ENVELOPE: PRESCRIPTIVE PATH -  
 HVAC: COMPLEX ANALYSIS  
 ELECTRICAL: SPACE BY SPACE

**STRUCTURAL DESIGN CRITERIA**

ZIP CODE: 97520  
 LOCATION: ASHLAND, OREGON  
 WIND EXPOSURE: 25 PSF  
 SEISMIC CATEGORY: D1  
 LIVE LOAD: "B" 80 MPH  
 FORCE RESISTING SYSTEM: LIGHT FRAME BEARING WALL  
 FROST DEPTH: 12"

**NARRATIVE:**

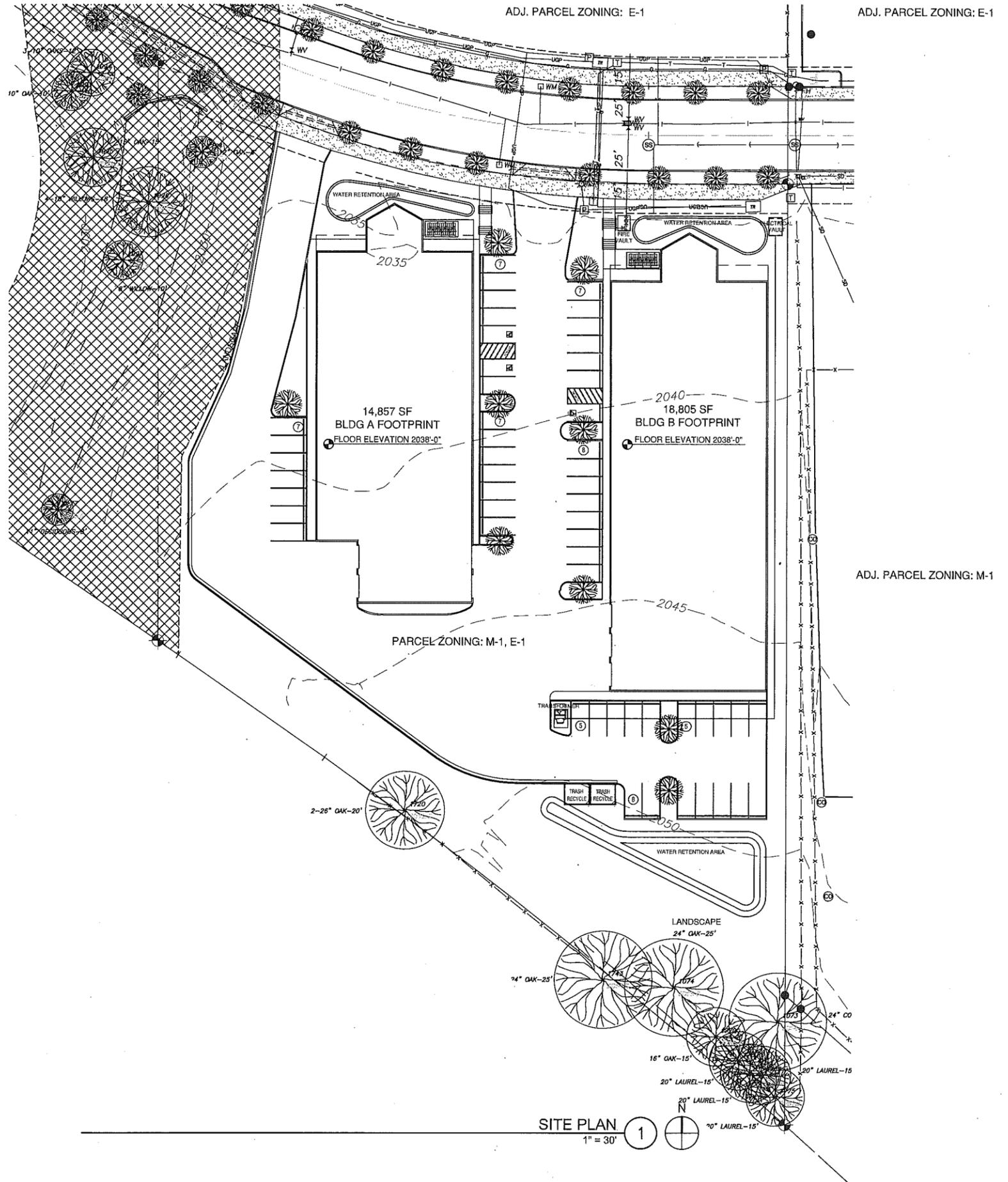
THE 2.19 ACRE SITE IS COMPRISED OF LIGHT BRUSH AND VEGETATION WITH A TOTAL OF 9 TREES ON THE SOUTH/SOUTH-WEST AREA OF THE SITE RANGING FROM 16" D.B.H. TO 18-24" D.B.H. THE PROPOSED PLAN CALLS FOR THE REMOVAL OF 2 DEAD COTTONWOOD TREES WITH THE REMAINDER UNDISTURBED.

THE PROPOSED SITE IS ADJACENT TO A SEASONAL CREEK WITH A FLOOD PROTECTION ZONE EXTENDING INTO THE SITE ON THE WESTERN BOUNDARY LINE.

THE FOLLOWING APPLICATION PROPOSES THE CONSTRUCTION OF (2) NEW BUILDINGS ON THE SITE IN ADDITION TO PARKING FOR 68 VEHICLES. IN KEEPING WITH THE E-1 ZONING, THE PROPOSED BRAMMO OFFICE AND MANUFACTURING FACILITY WILL PROVIDE A SIGNIFICANT NUMBER OF JOBS FOR LOCAL RESIDENTS IN THE DESIGN-DEVELOPMENT PROCESS OF ELECTRIC MOTORCYCLES. A DIRT TEST TRACK IS ALSO BEING PROPOSED ON THE ADJACENT TAX LOT 101, ALSO OWNED BY BRAMMO MOTORS.

IT IS THE INTENT OF THIS PROJECT TO PAY HOMAGE TO THE UNIQUE INDUSTRIAL DESIGN AESTHETIC ASSOCIATED WITH EUROPEAN MANUFACTURING FACILITIES, AND TO PROVIDE A HUB FOR BOTH EMPLOYMENT RESOURCES AND SUSTAINABLE TECHNOLOGY BASED IN THE CITY OF ASHLAND.

PROPOSED TEST TRACK, SEE ATTACHED PAGE  
 ADJ. PARCEL ZONING: M-1, E-1



**kistler+small+white**  
 architects  
 545 A STREET  
 ASHLAND, OR  
 97520  
 TEL.: 541.488.8200

**BRAMMO, INC.**  
 RESEARCH AND DEVELOPMENT FACILITY  
 OWNER: CRAIG BRAMMSCHER

REVISIONS


SITE PLAN

PROJECT: 11-025  
 ISSUE DATE: 12-02-11  
 SHEET:

**A1.1**

This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler+Small+White architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler+Small+White architects.

kistler+  
small  
+white  
architects

545 A STREET  
ASHLAND, OR  
97520

TEL.: 541.488.8200

BRAMMO, INC.  
RESEARCH AND DEVELOPMENT FACILITY  
OWNER:  
CRAIG BRAMSCHER

REVISIONS

TRACK PLAN

PROJECT: 11-025

ISSUE DATE: 12-02-11

SHEET:

A1.2

ADJ. PARCEL ZONING: E-1

**PLANNING SUMMARY:**

GOVERNING AUTHORITY: CITY OF ASHLAND

**APPLICABLE CODES:**

BUILDING: OREGON STRUCTURAL SPECIALTY CODE, 2010 ed.

MECHANICAL: OREGON MECHANICAL SPECIALTY CODE, 2010 ed.

PLUMBING: OREGON PLUMBING SPECIALTY CODE, 2008 ed.

ELECTRICAL: OREGON ELECTRICAL SPECIALTY CODE, 2008 ed.

FIRE: OREGON FIRE CODE, 2010 ed.

CONSTRUCTION TYPE: II-B

PARCEL NUMBER: 391E 14AC, LOT 101,102

ZONING CLASSIFICATION: M1 W/ E1 OVERLAY

OCCUPANCY GROUPS: N/A

PROPOSED BRAMMO FACILITY, SEE ATTACHED PAGE  
ADJ. PARCEL ZONING: M-1, E-1

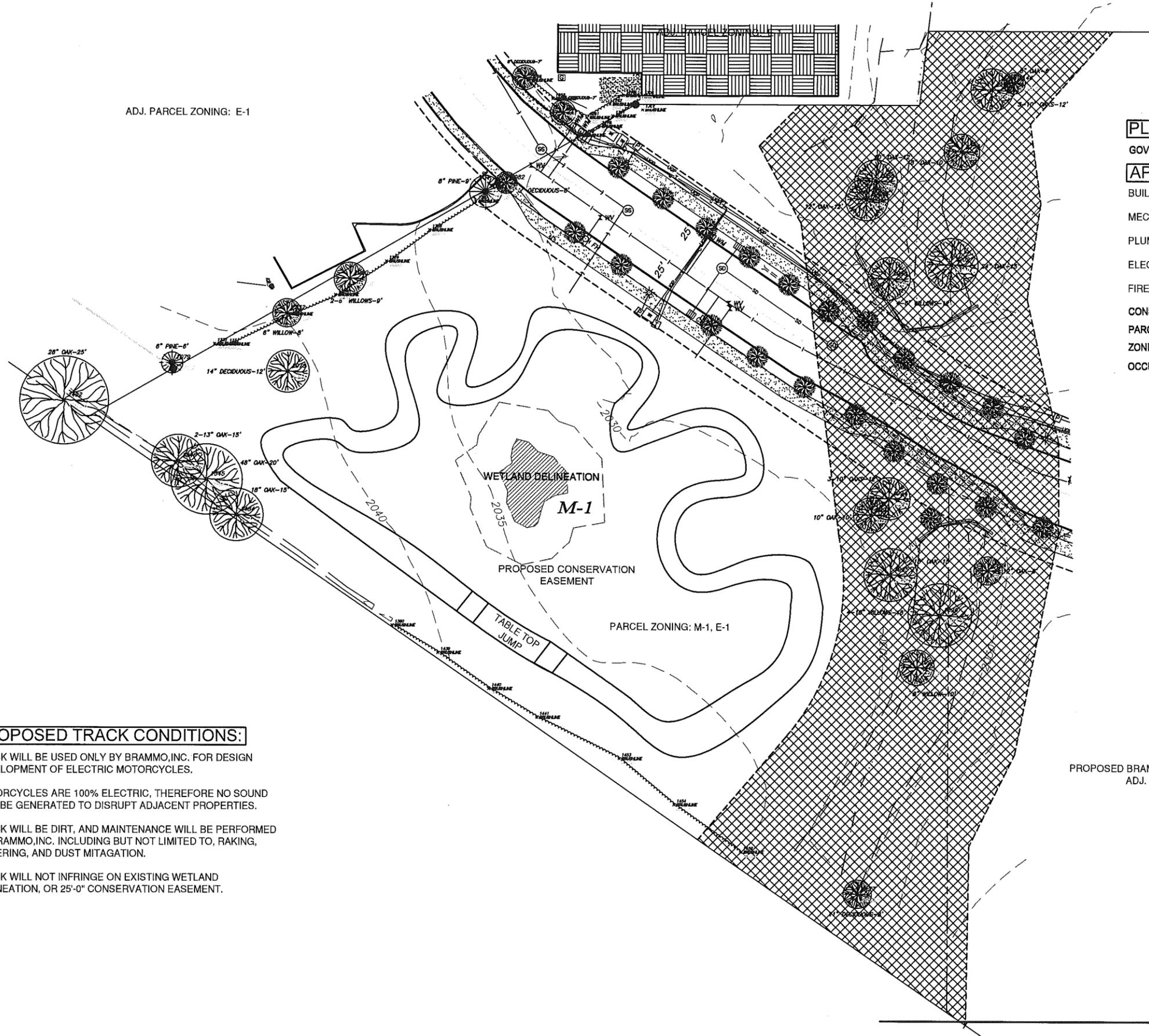
RECEIVED

DEC 02 2011

City of Ashland  
Field \_\_\_ Office \_\_\_ County \_\_\_

SITE PLAN

1" = 30'



**PROPOSED TRACK CONDITIONS:**

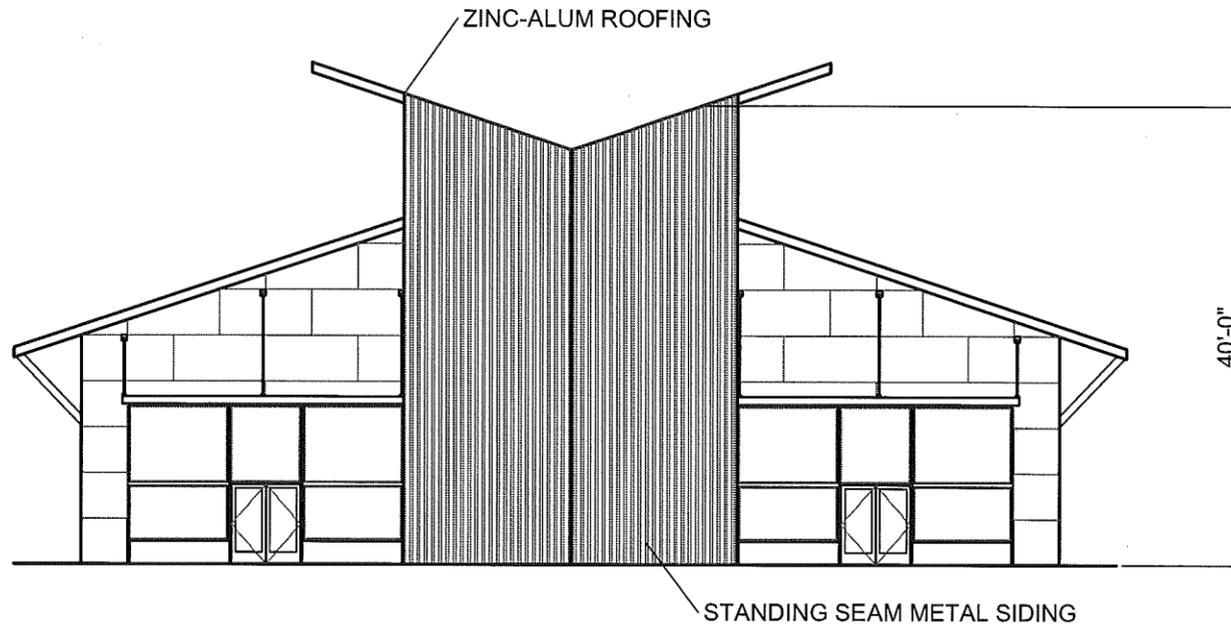
TRACK WILL BE USED ONLY BY BRAMMO, INC. FOR DESIGN DEVELOPMENT OF ELECTRIC MOTORCYCLES.

MOTORCYCLES ARE 100% ELECTRIC, THEREFORE NO SOUND WILL BE GENERATED TO DISRUPT ADJACENT PROPERTIES.

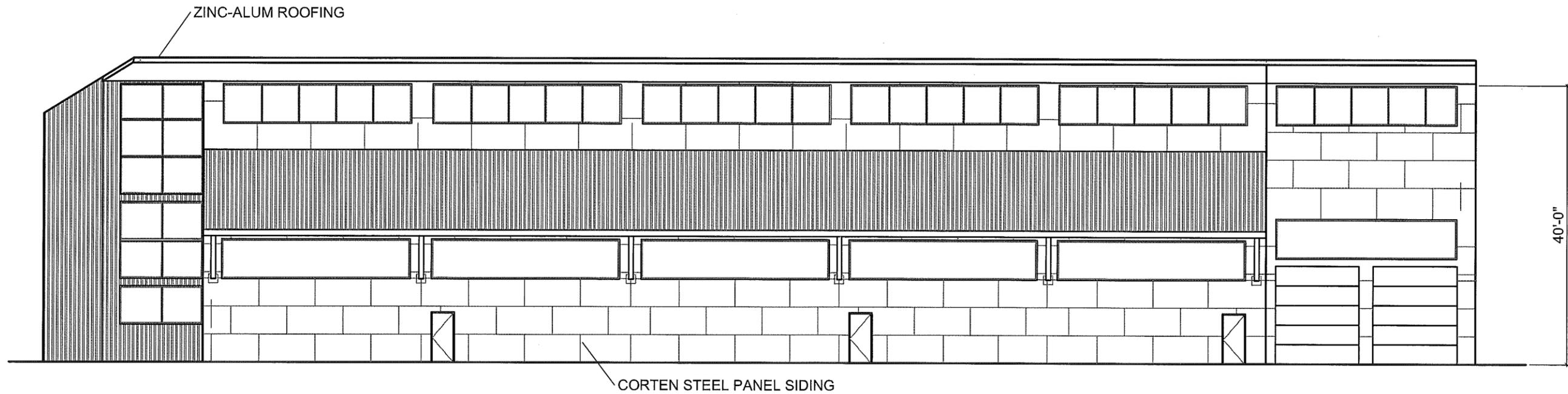
TRACK WILL BE DIRT, AND MAINTENANCE WILL BE PERFORMED BY BRAMMO, INC. INCLUDING BUT NOT LIMITED TO, RAKING, WATERING, AND DUST MITIGATION.

TRACK WILL NOT INFRINGE ON EXISTING WETLAND DELINEATION, OR 25'-0" CONSERVATION EASEMENT.

This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler+Small+White architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler+Small+White architects.



BUILDING 'A' NORTH ELEVATION 1  
1/8" = 1'-0"



BUILDING 'A' WEST ELEVATION 2  
1/8" = 1'-0"

kistler+  
small  
+white  
architects

545 A STREET  
ASHLAND, OR  
97520

TEL.: 541.488.8200

BRAMMO, INC.  
RESEARCH AND DEVELOPMENT FACILITY  
OWNER:  
CRAIG BRAMSCHER

REVISIONS

BUILDING 'B'  
ELEVATIONS

PROJECT: 11-025

ISSUE DATE: 12-02-11

SHEET:

A1.3

RECEIVED

DEC 02 2011

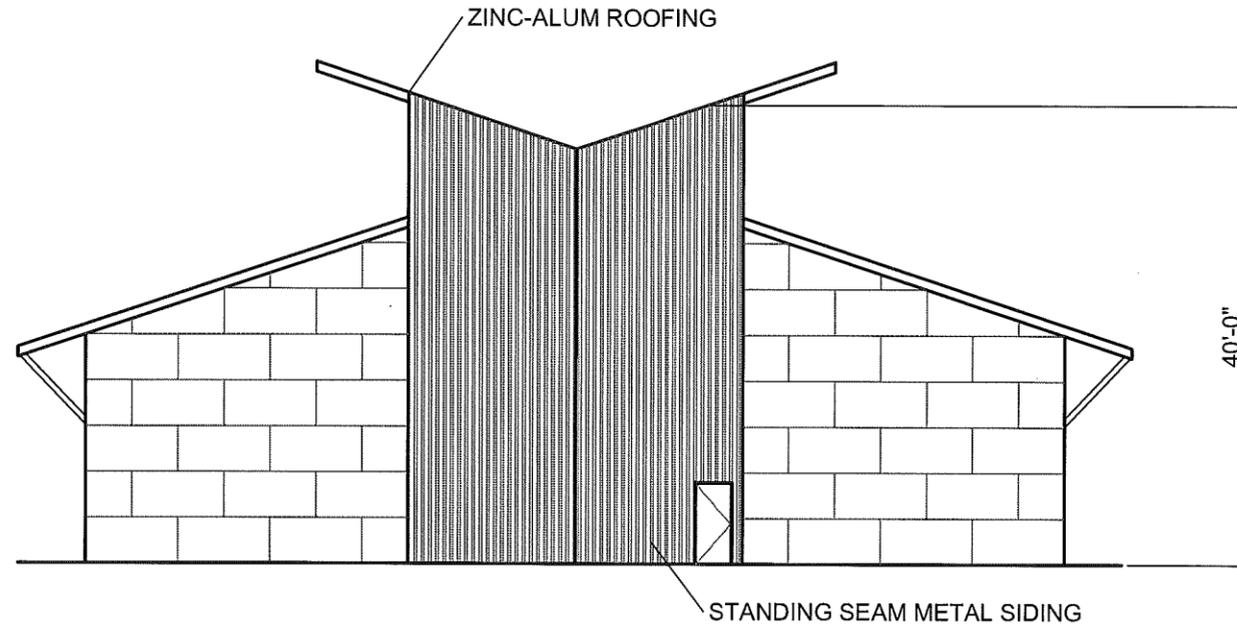
City of Ashland  
Field \_\_\_ Office \_\_\_ County \_\_\_

This document and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler+Small+White architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler+Small+White architects.

kistler+  
small  
+white  
architects

545 A STREET  
ASHLAND, OR  
97520

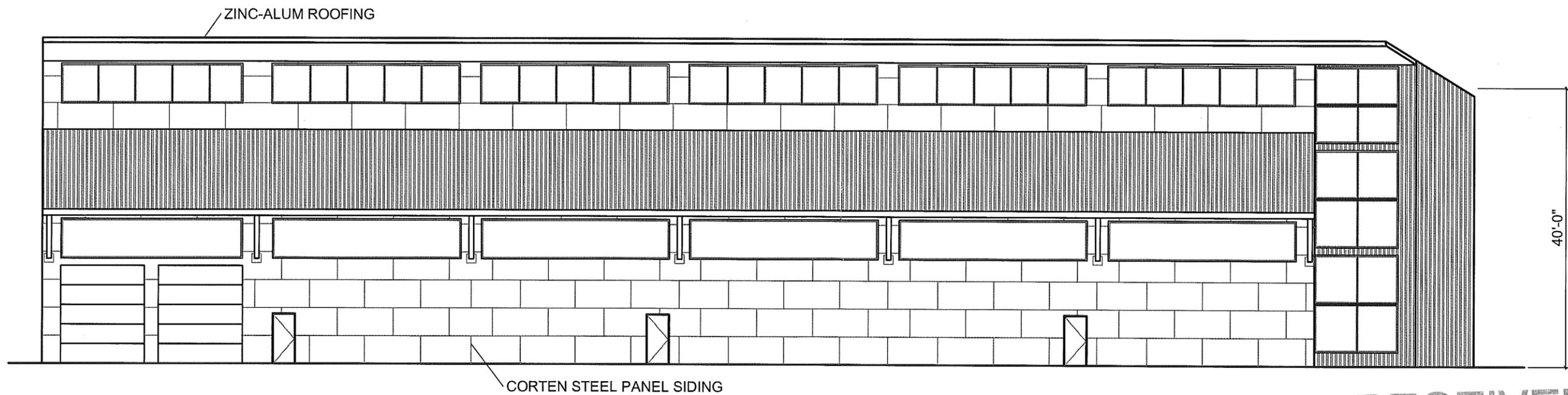
TEL.: 541.488.8200



BUILDING 'A' SOUTH ELEVATION

1/8" = 1'-0"

3



BUILDING 'A' EAST ELEVATION

1/8" = 1'-0"

4

BRAMMO, INC.  
RESEARCH AND DEVELOPMENT FACILITY  
OWNER:  
CRAIG BRAMSCHER

REVISIONS

BUILDING 'B'  
ELEVATIONS

PROJECT: 11-025

ISSUE DATE: 12-02-11

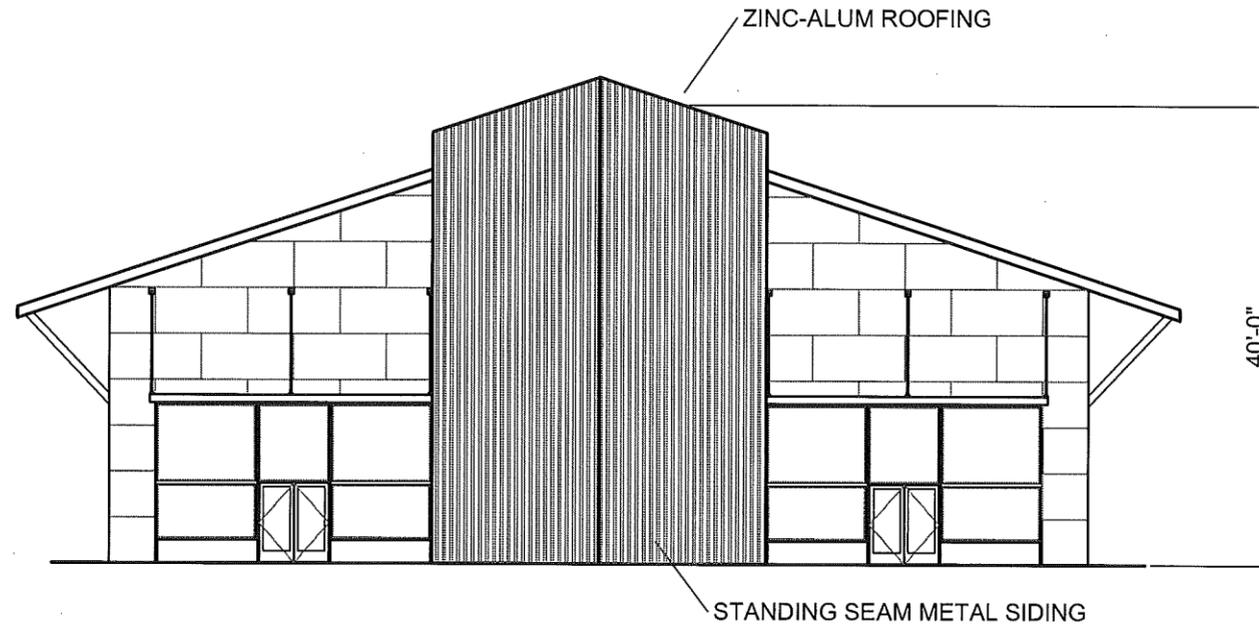
SHEET:

A1.4

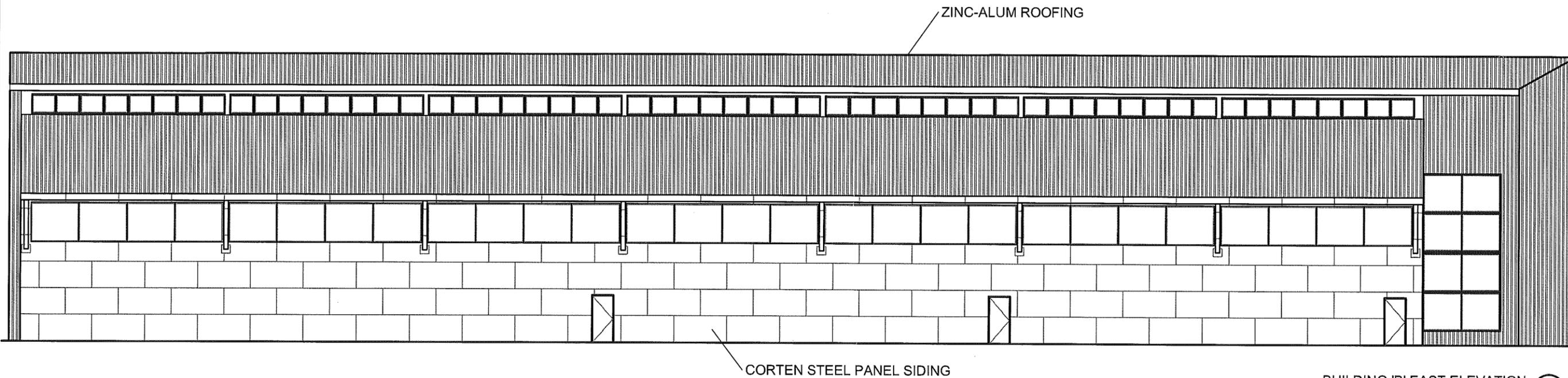
DEC 02 2011

City of Ashland  
Field \_\_\_ Office \_\_\_ County \_\_\_

This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of kistler+small+white architects and is not to be used, in whole or in part, for any other project without the written authorization of kistler+small+white architects.



BUILDING 'B' NORTH ELEVATION 1  
1/8" = 1'-0"



BUILDING 'B' EAST ELEVATION 2  
1/8" = 1'-0"

RECEIVED

DEC 02 2011

City of Ashland  
Field \_\_\_ Office \_\_\_ County \_\_\_

kistler+  
small  
+white

architects

545 A STREET  
ASHLAND, OR  
97520

TEL.: 541.488.8200

BRAMMO, INC.  
RESEARCH AND DEVELOPMENT FACILITY  
OWNER:  
CRAIG BRAMSCHER

REVISIONS

BUILDING 'B'  
ELEVATIONS

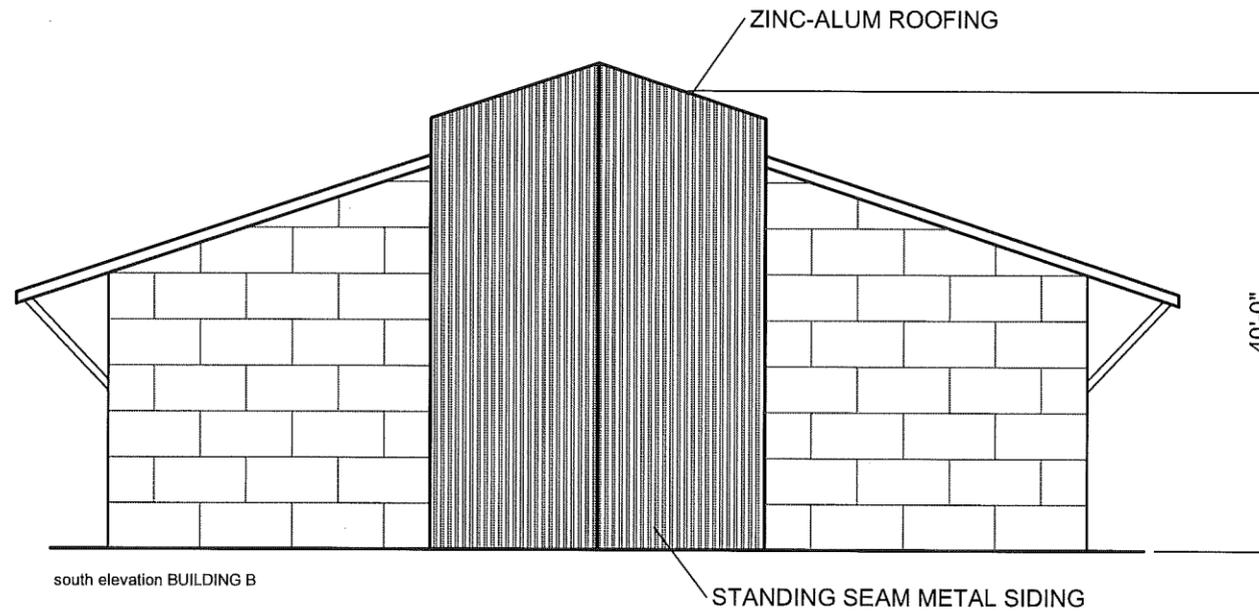
PROJECT: 11-025

ISSUE DATE: 10-03-11

SHEET:

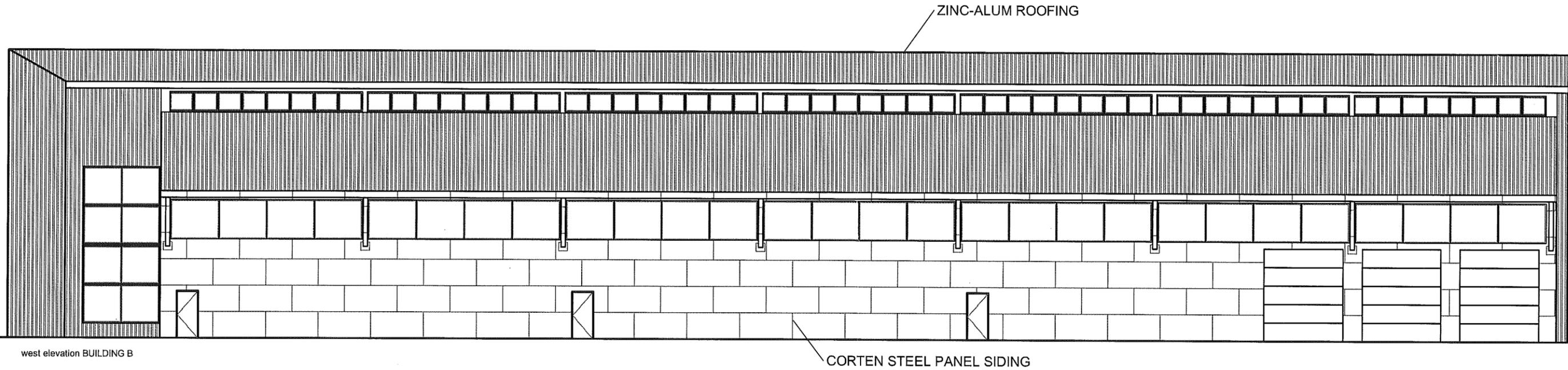
A1.5

This document, and the ideas and designs incorporated herein, are an instrument of professional service, is the property of Kistler+Small+White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler+Small+White Architects.



south elevation BUILDING B

BUILDING 'B' SOUTH ELEVATION 3  
1/8" = 1'-0"



west elevation BUILDING B

BUILDING 'B' WEST ELEVATION 4  
1/8" = 1'-0"

RECEIVED

DEC 02 2011

City of Ashland  
Field Office County

kistler+  
small  
+white

architects

545 A STREET  
ASHLAND, OR  
97520

TEL.: 541.488.8200

BRAMMO, INC.  
RESEARCH AND DEVELOPMENT FACILITY  
OWNER:  
CRAIG BRAMSCHER

REVISIONS

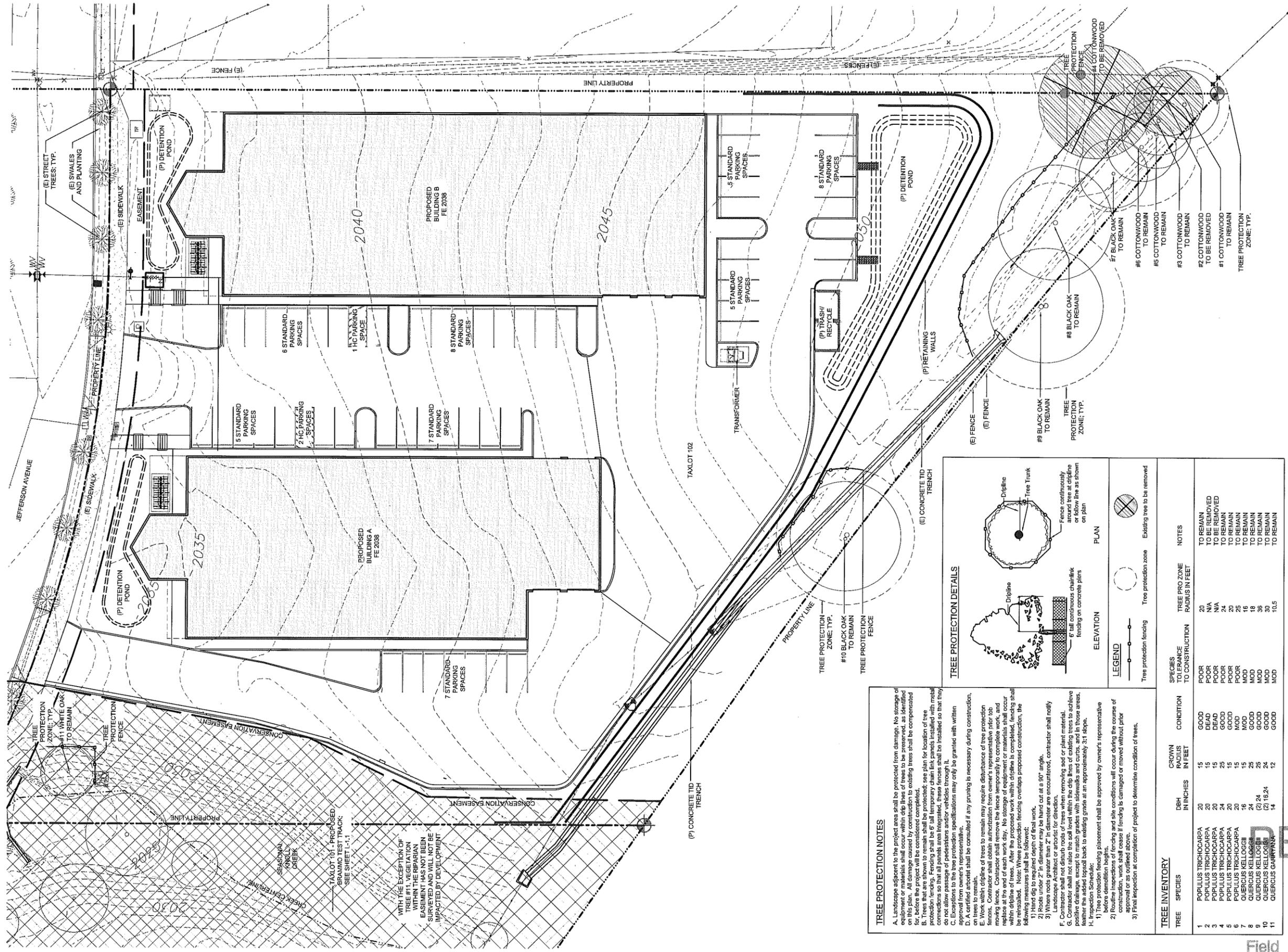
BUILDING 'B'  
ELEVATIONS

PROJECT: 11-025

ISSUE DATE: 10-03-11

SHEET:

A1.6



**TREE PROTECTION NOTES**

A. Landscape adjacent to the project area shall be protected from damage. No storage of equipment or materials shall occur within drip lines of trees to be preserved, as identified on this plan. Existing trees shall be compensated for before the project will be considered completed.

B. Trees that are shown to remain shall be protected; see plan for location of tree protection fencing. Fencing shall be 6' tall temporary chain link panels installed with metal connections so that all panels are integrated, these fences shall be installed so that they do not allow passage of pedestrians and/or vehicles through it.

C. Exceptions to the tree protection specifications may only be granted with written permission from the owner.

D. A certified arborist shall be consulted if any pruning is necessary during construction, on trees to remain.

E. Work within drip lines of trees to remain may require disturbance of tree protection fences. Contractor shall obtain authorization from owner's representative prior to moving fence. Contractor shall remove the fence temporarily to complete work, and replace at the end of each work day. No storage of equipment or materials shall occur within drip lines of trees. After any proposed work within drip line is completed, fencing shall be replaced. Tree protection fencing overlaps proposed construction, the following measures shall be followed:

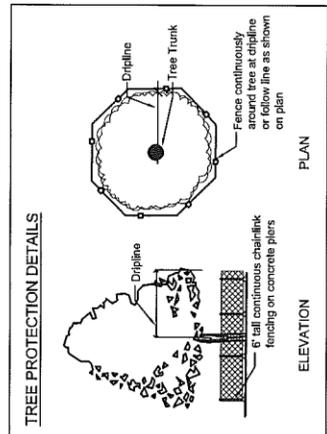
- 1) Hand dig to required depth of final work.
- 2) Rocks under 2" in diameter may be hand cut at a 90° angle.
- 3) Where roots greater than 2" in diameter are encountered, contractor shall notify Landscape Architect or arborist for direction.

F. Contractor shall not disturb roots of trees when removing sod or plant material.

G. Contractor shall be responsible for creating and installing a plan to achieve positive drainage, except to match grades with side walks and in those areas, the added topsoil back to existing grade at an approximately 3:1 slope.

H. Inspection Schedule:

- 1) Tree protection fencing placement shall be approved by owner's representative before demolition begins.
- 2) Routine inspections of fencing and site conditions will occur during the course of the project. If fencing is damaged or moved without prior approval or as certified arborist, the contractor shall be responsible for repair.
- 3) Final inspection at completion of project to determine condition of trees.



TREE SPECIES	DBH IN INCHES	CROWN RADIUS IN FEET	CONDITION	SPECIES TO CONSTRUCTION	TREE PRO ZONE RADIUS IN FEET	NOTES
1 POPULUS TRICHOCARPA	20	15	GOOD	POOR	20	TO REMAIN
2 POPULUS TRICHOCARPA	20	15	DEAD	POOR	N/A	TO BE REMOVED
3 POPULUS TRICHOCARPA	20	15	DEAD	POOR	N/A	TO BE REMOVED
4 POPULUS TRICHOCARPA	24	25	GOOD	POOR	24	TO REMAIN
5 POPULUS TRICHOCARPA	20	15	MOD	POOR	20	TO REMAIN
6 POPULUS TRICHOCARPA	16	15	MOD	POOR	16	TO REMAIN
7 QUERCUS KELLOGGII	24	25	GOOD	MOD	18	TO REMAIN
8 QUERCUS KELLOGGII	(2) 24	(2) 25	GOOD	MOD	36	TO REMAIN
9 QUERCUS KELLOGGII	(2) 18, 24	(2) 18, 24	GOOD	MOD	30	TO REMAIN
10 QUERCUS KELLOGGII	14	12	GOOD	MOD	10.5	TO REMAIN
11 QUERCUS GARRYANA						



PRELIMINARY TREE PROTECTION AND REMOVAL PLAN



Laurie Sager  
 AND ASSOCIATES LANDSCAPE ARCHITECTS INC  
 700 MISTLETOE ROAD, SUITE 201  
 ASHLAND, OREGON 97520

Revision Date:  
 Drawn By: KAG  
 Scale 1" = 40'-0"

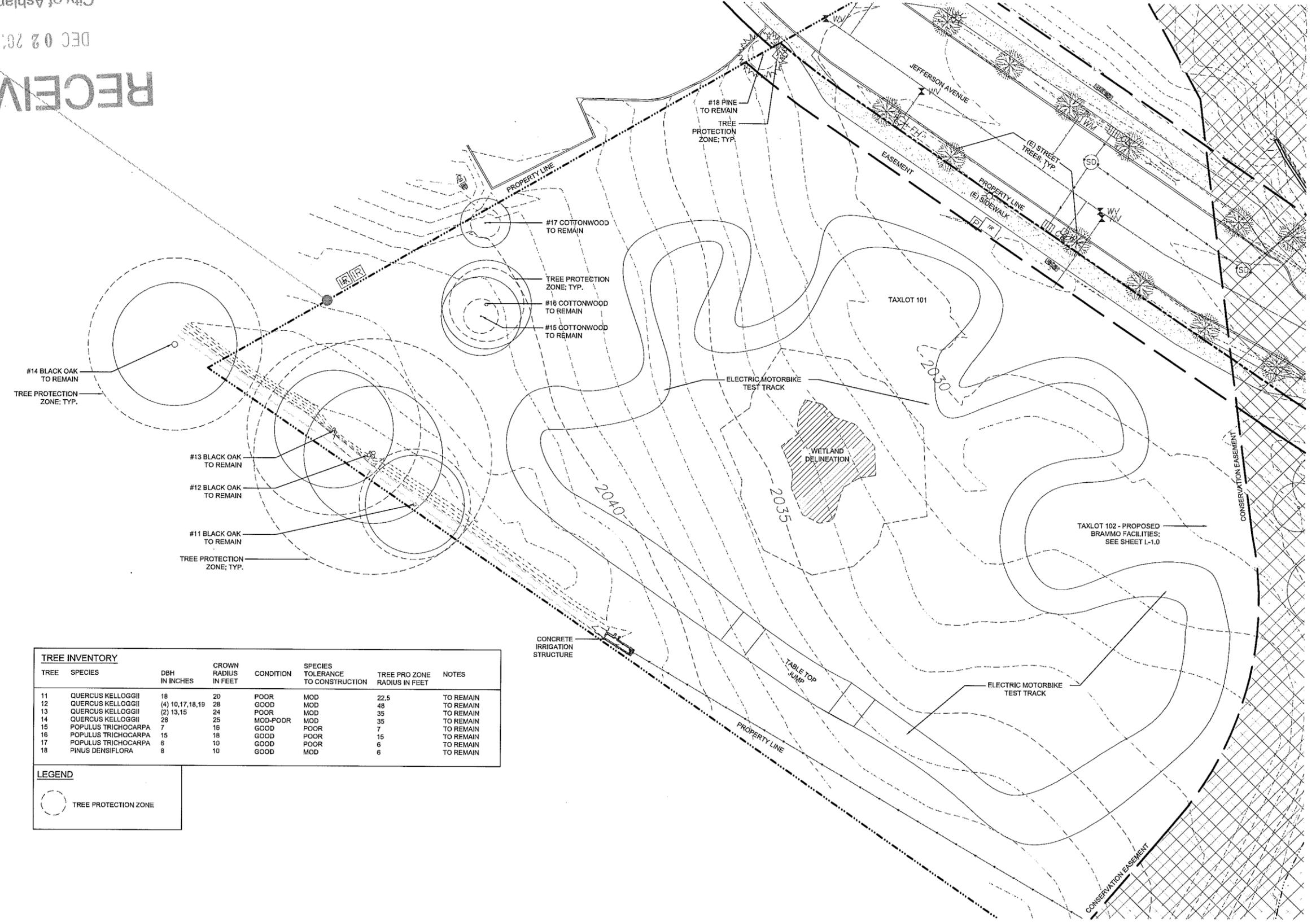
BRAMMO, INC.  
 RESEARCH AND DEVELOPMENT FACILITY  
 177 JEFFERSON AVENUE  
 ASHLAND, OREGON

December 2, 2011

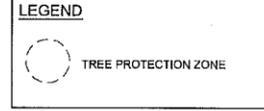
RECEIVED  
 DEC 02 2011  
 City of Ashland  
 Field Office County

L-1.0

RECEIVED  
 DEC 02 2011  
 City of Ashland  
 Field Office  
 County



TREE INVENTORY							
TREE	SPECIES	DBH IN INCHES	CROWN RADIUS IN FEET	CONDITION	SPECIES TOLERANCE TO CONSTRUCTION	TREE PRO ZONE RADIUS IN FEET	NOTES
11	QUERCUS KELLOGGII	18	20	POOR	MOD	22.5	TO REMAIN
12	QUERCUS KELLOGGII	(4) 10,17,18,19	28	GOOD	MOD	48	TO REMAIN
13	QUERCUS KELLOGGII	(2) 13,15	24	POOR	MOD	35	TO REMAIN
14	QUERCUS KELLOGGII	28	25	MOD-POOR	MOD	35	TO REMAIN
15	POPULUS TRICHOCARPA	7	16	GOOD	POOR	7	TO REMAIN
16	POPULUS TRICHOCARPA	15	18	GOOD	POOR	15	TO REMAIN
17	POPULUS TRICHOCARPA	8	10	GOOD	POOR	6	TO REMAIN
18	PINUS DENSIFLORA	8	10	GOOD	MOD	6	TO REMAIN



LAURIE SAGER  
 AND ASSOCIATES LANDSCAPE ARCHITECTS INC  
 700 MISTLETOE ROAD, SUITE 201  
 ASHLAND, OREGON 97520

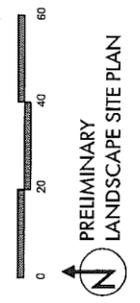
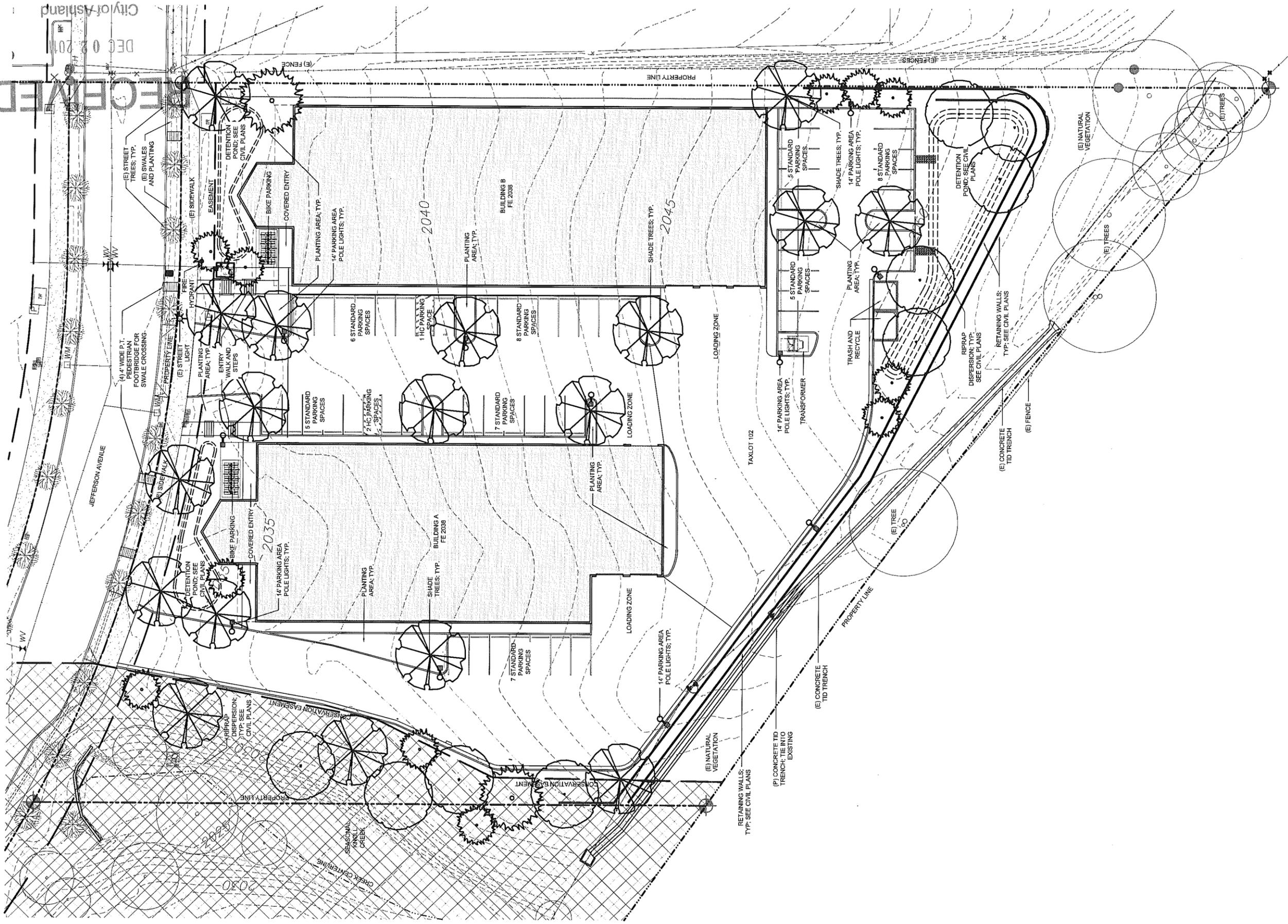


Revision Date:  
 Drawn By: KAG  
 Scale 1" = 40'-0"

BRAMMO, INC.  
 RESEARCH AND DEVELOPMENT FACILITY  
 763 JEFFERSON AVENUE  
 ASHLAND, OREGON

December 2, 2011

RECEIVED  
 DEC 02 2011  
 City of Ashland  
 Field Office  
 Ashland County



PRELIMINARY  
 LANDSCAPE SITE PLAN



Revision Date:

Drawn By:  
 KAG  
 Scale 1" = 40'-0"

BRAMMO, INC.  
 RESEARCH AND DEVELOPMENT FACILITY  
 177 JEFFERSON AVENUE  
 ASHLAND, OREGON

December 2, 2011

L2.0

LAURIE SAGER  
 AND ASSOCIATES LANDSCAPE ARCHITECTS INC  
 700 MISTLETOE ROAD, SUITE 201  
 ASHLAND, OREGON 97520





# **OTHER BUSINESS**

---

# Memo

---

DATE: January 10, 2012

TO: Ashland Planning Commission

FROM: Maria Harris, Planning Manager

RE: Re-review of Pedestrian Places Ordinance Revisions

---

At the December 6, 2011 City Council meeting, a question was raised regarding the ordinance amendments from the Pedestrian Places Project that apply throughout the City. It is staff's understanding that the Council passed the ordinance revisions without fully realizing that some of the provisions apply outside of the Pedestrian Place Overlay Zone. At the December 20, 2011 meeting, the City Council directed the Planning Commission to review the ordinance changes made city wide and to the Detail Site Review, and make recommendations to Council for changes as appropriate.

The attached outline was prepared for the December 20 Council meeting, and groups the amendments by those that apply throughout the City, those that apply in the Pedestrian Overlay and Detail Site Review Zone, and those that apply exclusively to the Pedestrian Overlay. Also attached are the map of the Detail Site Review Zone, and the minutes from the December 20 Council meeting.

The Planning Commission held study sessions on March 29, August 23 and September 13 of 2011 on the proposed ordinance amendments designed to implement ideas, proposals and recommendations evaluated as part of the Pedestrian Places Project, and on October 11, 2011 held a public hearing and made a recommendation to approve the ordinance amendments. The City Council held a public hearing and approved first reading of the ordinance amendments on November 1, 2011, and approved second reading of the amendments on November 15, 2011. The ordinance amendments became effective on December 16, 2011.

This is an informational item to update the Planning Commission, and no decision is required.

## **Next Steps:**

The public hearing for the re-review of the Pedestrian Place Ordinance Revisions is scheduled for the January 24 Planning Commission meeting. The meeting will be noticed as a public hearing.

## **Attachments:**

Applicability of Ordinance Amendments by Location, Pedestrian Places Project, December 20, 2011  
Detail Site Review Zone Map  
December 20, 2011 City Council Minutes

## Applicability of Ordinance Amendments by Location Pedestrian Places Project

### I. Amendments that apply to Pedestrian Overlay and throughout the entire City

#### GENERAL REGULATIONS – 18.68

##### 1. SECTION 18.68.050 Arterial Street Setback Requirements.

###### Previous Standard

- Required an approximately 20-foot front yard setback along arterial roads.

###### New Standard

- Requires a 20-foot yard or a setback equal to the width required to install sidewalk improvements (park row and sidewalk), whichever is less.

#### SITE DESIGN REVIEW – 18.72

##### 2. SECTION 18.72.090 Exception to the Site Design and Use Standards.

###### Previous Standard

- Described (4) approval criteria for allowing an exception.

###### New Standard

- Describes (4) approval criteria for allowing an exception; or
- Allows an exception for proposals that “will result in a design that equally or better achieves the stated purpose of the Site Design and Use Standards.”

## OFF-STREET PARKING CHAPTER – 18.92

### 3. SECTION 18.92.050 ~~Credit for On-street Automobile~~ Parking Management Strategies.

#### Previous Standard

- Through implementing specific strategies, allowed up to a 35% reduction in required parking; 50% in a Historic District.

#### New Standard

- Through implementing specific strategies, allows up to a 50% reduction in required parking.
- Permits the City to require a study by a qualified professional to provide supporting justification for offsets in parking demand, access, circulation and other transportation impacts.

### 4. SECTION 18.92.050 On-Street Parking Credit.

#### Previous Standard

- Allowed one off-street parking space credit for every two on-street spaces up to four credits, thereafter one space credit for each on-street parking space.
- Parallel parking = 24 feet of uninterrupted curb.
- 45 degree diagonal = 13 feet of uninterrupted curb

#### New Standard

- Allows one off-street parking space credit for each **one** on-street parking space.
- Parallel parking = **22** feet of uninterrupted curb.
- 45 degree diagonal = **12** feet of uninterrupted curb.

### 5. SECTION 18.92.050 Alternative Vehicle Parking.

#### New Standard

Allows alternative vehicle parking facilities to be substituted for up to 25 percent of the required parking spaces on site.

1. Motorcycle or scooter parking. Permits one off-street parking space credit for four motorcycle or scooter parking spaces.

2. Bicycle parking. Permits one off-street parking space credit for five additional, non-required bicycle parking spaces.

**6. SECTION 18.92.050 Shared Parking.**

**New Standard**

Allows one off-street parking space credit for every space constructed in designated off-site shared parking areas, or payment of in-lieu-of-parking fees for a common parking.

**7. SECTION 18.92.050 TDM Plan Credit.**

**New Standard**

Allows implementation of an individual Transportation Demand Management (TDM) plan that demonstrates a reduction of long term parking demand by a percentage equal to the credit requested.

**8. SECTION 18.92.050 Transit Facilities Credit.**

**New Standard**

States that projects that require at least 20 spaces and abut a street with transit service are eligible for a 10% reduction in parking through meeting the following standards:

1. Construct a pedestrian and transit supportive plaza
2. Transit service shall have a minimum of 30-minute peak period transit service frequency.
3. Existing parking areas may be converted to take advantage of these provisions.
4. The plaza must be adjacent to and visible from the transit street. .
5. The plaza must be at least 300 square feet in area The plaza must include all of the following elements:
  - a. A plaza that is open to the public.
  - b. A bench or other sitting area
  - c. A shelter or other weather protection.

**9. SECTION 18.92.080 Addressing Environmental and Microclimatic Impacts of Surface Parking**

**New Standard** (currently applies to *Croman Mill zone*)

Requires that parking areas of more than seven parking spaces meet the following standards:

- a. Use at least one of the following strategies for the surface parking area, or put 50% of parking underground.
  - i Use light colored paving materials with a high solar reflectance (Solar Reflective Index (SRI) of at least 29) to reduce heat absorption for a minimum of 50% of the parking area surface.

- ii. Provide porous solid surfacing or an open grid pavement system that is at least 50% pervious for a minimum of 50% of the parking area surface.
- iii. Provide at least 50% shade from tree canopy over the parking area surface within five years of project occupancy.
- iv. Provide at least 50% shade from solar energy generating carports, canopies or trellis structures over the parking area surface.
- b. Design parking lots and other hard surface areas in a way that captures and treats runoff with landscaped medians and swales.

**10. SECTION 18.92.080 Site Circulation**

**New Standard**

Requires that new development provides a circulation system that accommodates expected traffic on the site, with street-like features such as sidewalks, accessible curb ramps, trees and pedestrian scale lighting.

**11. SECTION 18.92.090 Pedestrian Access and Circulation (new section)**

**New Standards**

**A.** Requires projects to design a walkway system through the project that addresses the following:

- 1. The walkway system shall extend throughout the site and connect to all future phases of development.
- 2. The walkway shall be safe, direct, and convenient with connections between primary building entrances and all adjacent streets.
- 3. Walkways within developments including multiple buildings shall include connections between building entrances, and provide walkway connections between parking area and recreational facilities and other common areas.
- 4. Install protected raised walkways large parking area (>50 spaces)

**B.** Sets standards for Walkway Design and Construction, which address the following:

- 1. Vehicle/Walkway Separation.
- 2. Crosswalks.
- 3. Walkway Surface and Width.
- 4. Accessible routes.
- 5. Pedestrian Scale Lighting

## **II. Amendments that apply to Pedestrian Overlay and Detail Site Review Zone** (Commercial & Employment zoned land adjacent to major streets)

### **1. Minimum Floor Area Ratio (II-C-2 DETAIL SITE REVIEW STANDARDS)**

#### **Previous Standard**

- Minimum .35 FAR; Maximum FAR .5

#### **New Standard**

- Minimum .5 FAR; No Maximum FAR. (FAR).

In addition, an allowance is added for a shadow plan to phase in FAR requirement for projects including existing buildings or greater than 1/2 acre.

### **2. Building Setback from Sidewalk (II-C-2 DETAIL SITE REVIEW STANDARDS )**

#### **Previous Standard**

- A building setback of no more than 20 feet from a public sidewalk unless the area is used for pedestrian activities such as plazas or outside eating areas.

#### **New Standard**

- A building setback of no more than 5 feet from a public sidewalk unless the area is used for pedestrian activities such as plazas or outside eating areas, or for a required public utility easement.

### **3. Landscape Buffer (II-C-2 DETAIL SITE REVIEW STANDARDS)**

#### **Previous Standard**

- A 10-foot wide buffer required between buildings and the street, except in the Ashland Historic District.

#### **New Standard**

- A 10-foot wide buffer required between buildings and the street, except in the Ashland Historic District and Detail Site Review Zone.

### III. Amendments that apply to Pedestrian Overlay (exclusively)

#### 1. SECTION 18.18.56.040 Special Permitted Uses

##### **New Standard**

Commercial uses such as professional offices, stores and restaurants up to 2,500 square feet permitted in *residential zones* in Pedestrian Overlay if minimum housing density requirements are met.

#### 2. SECTION 18.56.040 Solar Setback

##### **Previous Standard**

- New buildings and additions required to meet the solar access setback which limits the height of the shadow permitted at the north property line, and thereby limits the height of the building.

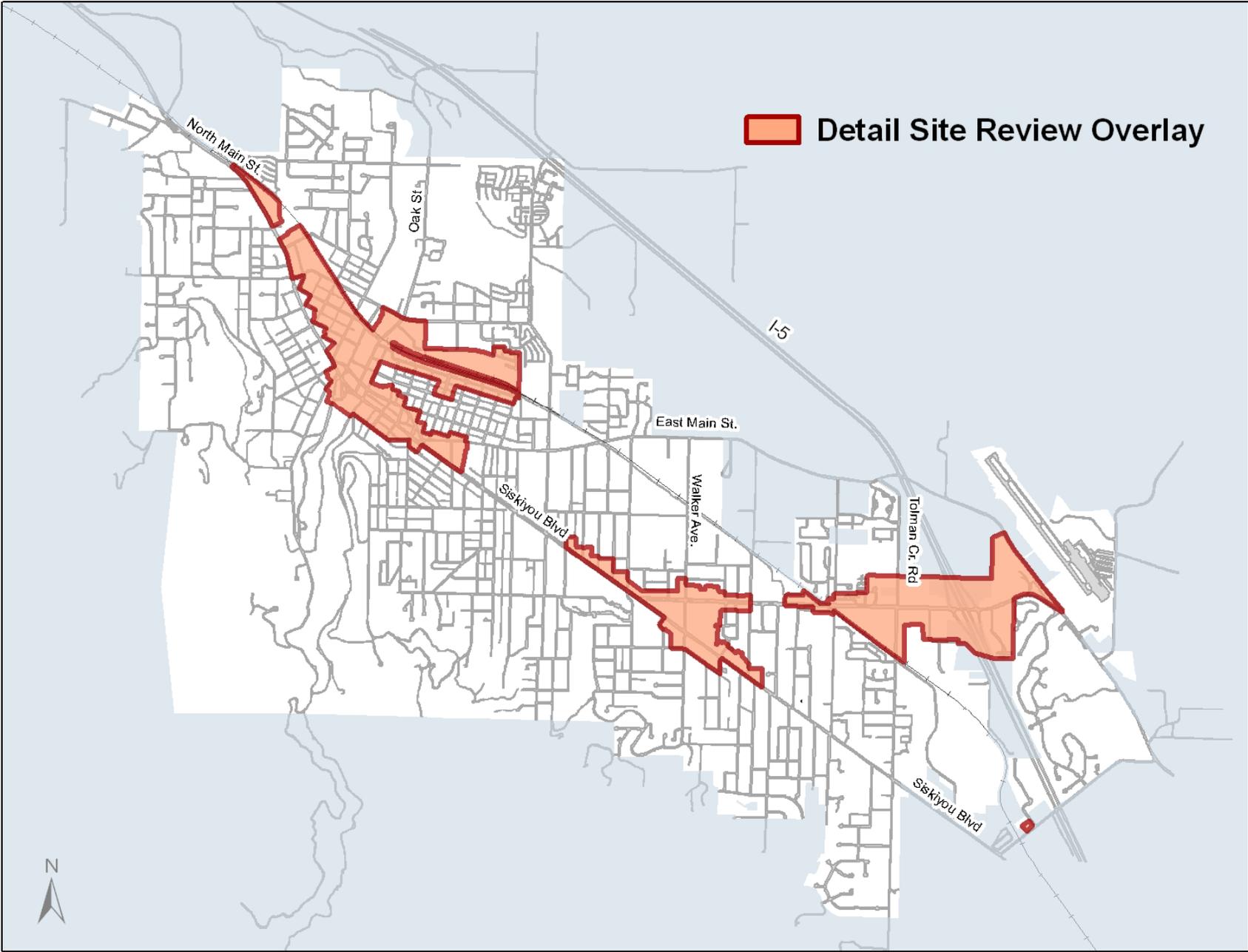
##### **New Standard**

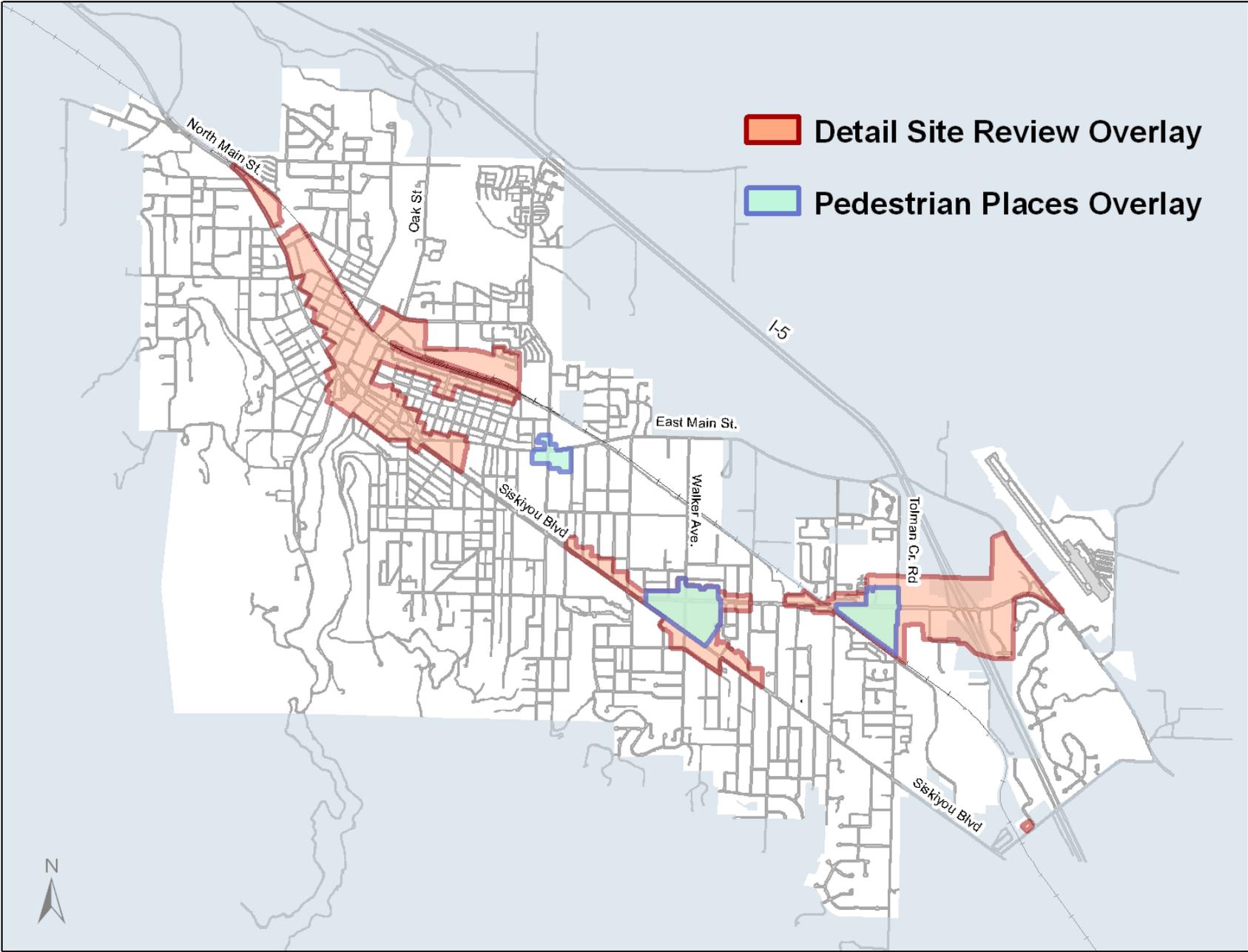
- The solar access setback applies only to those new buildings and additions which have residential zones located to the north.

#### 3. SECTION 18.56.040 Plazas and Landscaping Ratio

##### **New Standard**

Outdoor seating areas, plazas and other useable paved surfaces may count for up to 50% of the required landscape area.





Interim City Administrator Lee Tuneberg explained there was not a formal policy for opting out and the City had received less than 12 requests not to use the meters in their area. Alternately, more people were opting in because it eliminated leaving gates open, having City staff enter personal property and alleviated concerns regarding their pets. Currently 43% of electric meters used radio frequency emitters.

**Councilor Silbiger/Morris m/s to approve Consent Agenda item #5. DISCUSSION:**

Councilor Silbiger noted this was a long-standing conversion. There was no plausible or measurable health problem related to the transmitters and they used the same frequency as non-cellular telephones. Councilor Voisin thought the City should research the possible health effects related to the transmitters and would not support the motion. Mayor Stromberg suggested the City provide a policy for customers to opt out and charge accordingly. Councilor Chapman added the meters were low powered and outside. Staff could look into purchasing first generation meters or reprogramming the new meters to a standby mode. Councilor Morris did not think the new meters were potentially harmful but wanted a policy to opt out. Councilor Lemhouse supported delaying the order to address concerns and possible options. Councilor Voisin reiterated the need to delay until they received more information. Councilor Chapman explained the company could decide on reprogramming or using first generation meters while the order was processed. Mr. Tuneberg recommended digital meters without the Electronic Radio Transmitter (ERT). They were more accurate and incapable of transmission.

**Voice Vote: Councilor Morris, Chapman and Silbiger, YES; Councilor Voisin and Lemhouse, NO. Motion passed 3-2.**

Council directed staff to explore a policy for opting out and associated costs.

**PUBLIC HEARINGS**- None

**PUBLIC FORUM**

**Pam Vavra/457 C Street/**Spoke on the behalf of the Local Chapter of the Civil Liberties Defense Center regarding homelessness and the Center's founder, Ralph Temple who had passed. She noted the escalation nationwide of Police coming down on homeless people and that homelessness had increased dramatically since 2008. The Center worked to protect the public by educating them on their rights and reaffirmed the Center's intention to continue Mr. Temple's work.

**UNFINISHED BUSINESS**

**1. Will Council approve a resolution for an interim rate increase for solid waste collection?**

City Administrator Larry Patterson provided background on the 7.5% increase across the board for all users with a 20% increase on commercial bins Council approved during the December 6, 2011 meeting. Recology adjusted the rates that equated to an overall increase of 11.2%. General Manager Steve DiFabion explained the rate increase with the current calculations would help Recology break even.

**Councilor Voisin/Chapman m/s to approve Resolution #2011-34. DISCUSSION:** Councilor Voisin thought the rate increase was reasonable and a good beginning. Councilor Lemhouse did not see the evidence that justified the rate increase and would not support the motion. Councilor Chapman noted some of the commercial rates were higher because it was an average and would hurt more customers than he had anticipated. Mayor Stromberg was not comfortable with the concept of cost-plus as the basis for rates because it gave Recology the incentive that more costs equaled more profit and put the City at a disadvantage. Councilor Silbiger agreed with the Mayor and added approving the rate change was not an acceptance of Recology's request to build a 10% profit margin into the replacement Franchise Agreement. Councilor Voisin wanted a Study Session to address the 23.3% increase, review an audit of Recology's financials, and then start negotiations. Mayor Stromberg clarified the next steps included getting expert help, analyzing the figures, and interacting with Recology. **Roll Call Vote: Councilor Silbiger, Voisin, Chapman and Morris, YES; Councilor Lemhouse, NO. Motion passed 4-1.**

**2. Does Council have questions regarding the applicability of recent ordinance amendments designed to implement ideas, proposals and recommendations evaluated as part of the Pedestrian Places Project?**

Interim City Administrator Larry Patterson explained Council passed the Pedestrian Places ordinance without fully realizing that some of the provisions in the Overlay Zone applied citywide. At Council's request, staff prepared an outline of amendments to the ordinance that clarified the changes. Council was concerned with the transit stops and the blanket assessment of arterial setbacks and wanted the Planning Commission to review all the amendments.

Community Development Director Bill Molnar addressed **Section 18.92.050 Transit Facilities Credit**, and explained the changes to the off street-parking chapter sought to identify additional

parking management strategies to utilize parking more efficiently. This change applied to projects with parking requirements of at least 20 spaces. Applicants providing a transit plaza were eligible for a 10% reduction in the parking requirement. The City would also have the ability to require additional study by a professional.

He went to explain the staff recommendation to remove the 20-foot arterial setback in the city and apply it to commercial or employment areas only. This change primarily affected Ashland Street, Siskiyou Boulevard, and parts of North Main. An applicant could reduce the 20-foot setback no less than the City's new sidewalk standards. In a commercial zone, that was anywhere from 13-15 feet back from the curb. In addition, a 5-foot setback could be greater. Even with the front yard setback removed along commercial arterials, the building setback would be 15-20 feet. The residential 20-foot front yard setback remained unchanged. Mr. Molnar added the Planning Commission was aware of the amendments and understood it would apply to the greater extent of Ashland Street beyond the small Pedestrian Overlay.

Mayor Stromberg thought the amendments were major impacts to areas of Ashland and did not think the Planning Commission had discussed them in depth. They also passed through Council unnoticed and needed review by both groups.

Councilor Silbiger shared his personal experience discovering the amendments and addressed the outline of changes. The first four pages affected the entire city. There was one page for the Pedestrian Overlay and Detailed Review Zone and only half a page for Pedestrian Places. The changes did not appear to be an oversight and he found it disturbing. The majority of changes were good but the Planning Commission and Council should have discussed them in detail prior to approval. If the intent and purpose beyond the Pedestrian Places was to increase density along certain corridors, those standards were now in areas they should not be.

Councilor Morris supported most of the changes but expressed concern they were presented as Pedestrian Places. He had mentioned the special setback requirement had changed during the hearing and that it was not a Pedestrian Places location. In addition, there were items in the ordinance not covered in the public meetings.

Councilor Voisin thought the changes were good and that Council frequently gave mixed messages to staff. Mayor Stromberg added that when writing new ordinance language that had a specific purpose there was a tendency to address other issues relating to the ordinance. However, some of the issues in the Pedestrian Places ordinance did not go through the proper land use process and had a different context. The people affected by the changes were most likely unaware they existed.

**Colin Swales/143 8<sup>th</sup> Street/**Noted he had spoken to these issues at the Planning Commission and Council meetings. The outline of changes was more extensive than he originally thought and he had read the ordinance carefully. When working with arterial setbacks there was a need to preserve both the public space and the property rights of those who abutted the arterials.

**Councilor Silbiger/Chapman m/s to direct the Planning Commission to review the ordinance changes made citywide and to the Detailed Site Review and make recommendations to Council for changes as appropriate. DISCUSSION:** Councilor Silbiger explained in addition to reading the minutes, he had confirmed with a Planning Commissioner they had not discussed changes outside of the Pedestrian Places Overlay, nor had Council. It was appropriate for the Planning Commission to go through the review process. **Roll Call Vote: Councilor Silbiger, Voisin, Lemhouse, Morris and Chapman, YES. Motion passed.**

**3. Will Council authorize the City Administrator to finalize and sign the Charging Site Host Agreement with ECOtality North America to participate in a pilot project for the installation and use of Electric Vehicle (EV) charging units in two City owned parking facilities in the downtown area?**

Interim Assistant City Administrator Lee Tuneberg explained the pilot program would have two charging stations in the downtown area. Project Manager Adam Hanks added the City Administrator would designate the spaces for EV charging only. Currently there was 7% use of these spaces. The pilot project would help determine how pervasive use was and how quickly it would ramp up.

Mr. Hanks explained enforcement was the same as changing a 15-minute parking zone to two hours and fell below the threshold of an ordinance. Additionally, the ordinance authorized the City Administrator to make changes at that level. Initial energy use would be insignificant, eventually fall to time of use, and load management. When the pilot program ended, they would cap the charging pedestal without the City incurring removal costs.

# Memo

DATE: January 10, 2012  
 TO: Planning Commission  
 RE: Planning Commission Attendance Report

Pursuant to AMC 2.10.025, below is the Planning Commission's attendance record for July through December 2011.

Meeting Date	Meeting Type	Absences
July 12, 2011	Regular Meeting	1 – Melanie Mindlin
July 26, 2011	Joint Study Session	1 – Melanie Mindlin
August 9, 2011	Regular Meeting	0
August 23, 2011	Special Meeting	1 – Melanie Mindlin
September 13, 2011	Regular Meeting	1 – Pam Marsh
September 27, 2011	Joint Study Session	0
October 11, 2011	Regular Meeting	0
October 25, 2011	Special Meeting	0
November 7, 2011	Joint Study Session	0
November 29, 2011	Joint Study Session	0
December 13, 2011	Regular Meeting	1 – Mick Church

**AMC 2.10.025**

*All members are expected to attend all regularly scheduled meetings, study sessions and special meetings, when applicable. If a member will be absent from a meeting the member must notify the chair or the staff liaison at least two hours prior to the meeting. Any member who has two or more unexcused absences in a six month period [i.e. January 1– June 30 or July 1 - December 31] shall be considered inactive and the position vacant. Further any member not attending a minimum of two-thirds (2/3) of all scheduled meetings (inclusive of study sessions and special meetings) shall be considered inactive and the position vacant. Attendance shall be reviewed by the commission or board during the regularly scheduled meetings in January and July, with a report sent to the Mayor and City Council advising of the need for appointment or re-appointment, if necessary.*

