

CITY OF ASHLAND

HISTORIC PRESERVATION ADVISORY COMMITTEE AGENDA Community Development Building – 51 Winburn Way December 7, 2022, 6PM

- I. 6:00PM - REGULAR MEETING – CALL TO ORDER
- II. READING OF LAND ACKNOWLEDGEMENT
"We acknowledge and honor the aboriginal people on whose ancestral homelands we live, —the Ikirakutsum Band of the Shasta Nation, including the original past indigenous inhabitants, as well as the diverse Native communities who make their home here today. We also recognize and acknowledge the Shasta village of K'wakhakha — "Where the Crow lights"—that is now the Ashland City Plaza."
- III. (6:05) APPROVAL OF AGENDA (5 min)
 - Commissioner suggested amendments to Agenda
- IV. (6:10) APPROVAL OF MINUTES (5 min)
 - Historic Commission electronic meeting of November 2, 2022
- V. (6:15) PUBLIC FORUM (15 min)
- VI. (6:30) LIAISON REPORTS (5 min)
 - Council report (Moran)
 - Staff report (Severson)
- VII. (6:35) PLANNING ACTION REVIEW (20 min)
PLANNING ACTION: PA-T1-2022-00199
SUBJECT PROPERTY: 748 Siskiyou Blvd.
APPLICANT: Rogue Development for American Band College
DESCRIPTION: A request for Site Design Review and Conditional Use Permit to change the use of an existing, non-conforming site occupied by a church/religious institution structure to an administrative office space and storage facility for the American Band College. The request includes the removal of a mature Catalpa tree in the parkrow to allow the installation of the new driveway approach. **COMPREHENSIVE PLAN DESIGNATION:** Multi Family Residential;
ZONING: R-2 **ASSESSOR'S MAP:** 39 1E 09 DB; **TAX LOTS:** 100
- IX. (6:55) BUILDING PERMIT REVIEW (10 min)
125 E. Main – Kyle Taylor
- X. (7:05) DISCUSSION ITEMS (10 min)
 - Mural project at Elks Lodge (Artist John Pugh)
 - Agenda for HPAC retreat
- XI. (7:15) INFORMATION ITEMS (10 min)
- XII. ADJOURNMENT

"In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-9200), or by email at planning@ashland.or.us. Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1)."

**ASHLAND HISTORIC COMMISSION
MEETING
DRAFT (Action) Meeting Minutes
November 2, 2022**

Community Development/Engineering Services Building – 51 Winburn Way

6:00PM CALL TO ORDER

Chair Hovenkamp called the meeting to order at 6:00pm.

As this was Commissioner Scharen's first meeting, there was a brief round of introductions.

Commissioners Present:	Council Liaison:
Shostrom	Shaun Moran
Hovenkamp	Staff Present:
Emery	Derek Severson; Senior Planner
Von Chamier	
Bonetti	Public Present:
Whitford	Steve Hoxmeier
Repp (via Zoom)	
Scharen (via Zoom)	
Skibby (via Zoom)	
Commissioners Absent:	All present

READING OF LAND ACKNOWLEDGEMENT

Land Acknowledgement was read by Hovenkamp

(6:05) APPROVAL OF AGENDA (5 min)

Commissioner had no suggested amendments to the agenda.

(6:10) APPROVAL OF MINUTES (5 min)

Shostrom/Whitford m/s to approve the minutes of October 5, 2022. ALL AYES. Motion passed.

(6:15) PUBLIC FORUM (15 min)

There was no one in the audience wishing to speak.

(6:30) LIAISON REPORTS (5 min)

Councilor Moran gave liaison report. Items discussed were:

- On-going Council discussion of Standing Advisory Committees and Management Advisory Committees, with Moran noting that the Council had decided to make the Transportation Committee a Standing Advisory Committee going into the Transportation System Plan Update for the sake of transparency and an inclusive public process.
- The Council continues to discuss how to support and encourage multi-family residential development and is looking at the process for collecting system development charges.
- The Council is also looking at options for protecting and preserving existing manufactured housing parks.

Severson noted:

- CLG on-line workshop is scheduled via Zoom on November 16th from 1:00 p.m. to 6:30 p.m. It is free but requires pre-registration. Severson passed out an agenda and will email everyone a copy of Kuri Gill's e-mail with the registration link.
- The project for the Ashland Food Co-Op's bottle drop that was considered by the Committee last month has been approved by the Staff Advisor.
- The denial of the Magnolia Terrace project at 165 Water Street was appealed to the Council, however the applicant requested a postponement of the appeal hearing. That hearing will be scheduled before the City Council in the coming months, and staff will keep the Committee in the loop as the appeal moves forward.

(6:35) PLANNING ACTION REVIEW (20 min)

PLANNING ACTION: PA-T1-2022-00196

SUBJECT PROPERTY: 485 A Street

APPLICANT: Steve & Kathy Hoxmeier

DESCRIPTION: A request for Site Design Review approval to add a two story 400 sq. foot addition to the existing mixed use commercial building located at 485 A Street. The proposed addition will be in the same fashion as the existing building with matching siding, trim, roof, and eaves. The addition adds a new bathroom to the existing apartment and a new commercial kitchen below. **COMPREHENSIVE PLAN**

DESIGNATION: Employment; **ZONING:** E-1 **ASSESSOR'S MAP:** 39 1E 04AB; **TAX LOTS:** 6500

Severson gave a brief staff report.

Steve Hoxmeier/435 B Street noted that he would like to re-use the awning currently in place on the upstairs rather than the type shown on the drawings, noting that shifting it to the downstairs would provide a more commercial feel. He also explained that the downstairs kitchen space was not intended for public access but would serve the outdoor grill which has a previous approval.

Shostrom commented that the proposal seemed compatible with the existing building, fits the architecture and is symmetrical and seemed like a great addition. Hovenkamp, Skibby, Scharen and Repp concurred.

Shostrom/Skibby m/s to recommend approval of this application as submitted. Voice vote: All AYES. Motion passed.

(6:55) DISCUSSION ITEMS (10 min)

Severson noted that the new member orientation would be rescheduled for the later part of November.

After some discussion of the potential for an annual retreat, it was decided that the retreat would be held on Friday, January 6th from 9:30 a.m. to 12:30 p.m. in the Siskiyou Room. Members asked that the agenda be discussed at the December 7th meeting. There was brief discussion of the state's Climate Friendly & Equitable Communities rulemaking, which has several new requirements including substantial changes to cities' abilities to require parking and will also require that cities identify climate friendly areas that could accommodate at least 30 percent of their population and require that these areas be re-zoned to allow additional building height (up to 50 feet) and density (up to 15 dwelling units per acre). The state envisions that this will likely include downtowns. Staff suggested that the Commission could consider the potential implications of increased height allowances during the retreat to be prepared for the discussions to come.

(7:05) INFORMATION ITEMS (10 min)

None to discuss

ADJOURNMENT

Next meeting is scheduled for December 7, 2022, at 6:00pm at Community Development, 51 Winburn Way.

There being no other items to discuss, the meeting adjourned at 7:37 pm

Respectfully submitted by Regan Trapp

748 Siskiyou

October 25, 2022

**Site Design Review and Conditional Use Permit Review
To change use and modify the non-conforming site to provide a parking area**

Property Owner: Western International Band Clinic Inc.
407 Terrace Street
Ashland, OR 97520

Applicant: Rogue Planning & Development Services, LLC
Amy Gunter
1314-B Center Dr., PMB 457
Medford, OR 97501

Engineering: Civil West Engineering Service, Inc.
830 O'Hare Parkway, Suite 102
Medford, OR 97504

Subject Property

Property Address: 748 Siskiyou Boulevard
Map & Tax Lot: 39 1E 09DB; Tax lot 100

**Comprehensive
Plan Designation:** Low Density Multiple Family Residential
Zoning: R-2
Adjacent Zones: R-1-7.5 & R-2
Siskiyou-Hargadine Historic District

Request:

Request for Site Design Review and Conditional Use Permit to change the use of an existing, non-conforming site occupied by a church/religious institution structure to an administrative office space and storage facility for the American Band College.

The change of use necessitates the inclusion of code required site improvements to bring the site closer to compliance. In this case, there is a narrow driveway curb cut and apron but there is not parking for any of the site uses. The proposal includes installation of a new apron, parking area and pathways for accessible access to both levels of the structure.

On the Siskiyou Boulevard frontage there is also a solid, wood door to the basement level accessed from the sidewalk. There is a large picture window with double hung windows on the right side of the door. This door is covered with a poorly attached, deteriorating marquee type of awning. There are brick planters with dirt in direct contact with the structure. Retaining walls are present along the property lines. The retaining is a mixture of poured concrete, stacked block and stacked rocks. The wall varies between about 4.5-feet to 12-inches.

The property has a driveway curb cut of approximately 7-feet wide from Liberty Street. There is a concrete sidewalk from the apron to the residential unit and the stair/ramp entry for the church. Until recently, with the current tenants and users of the space parking in the yard, there is no on-site parking area.

Siskiyou Boulevard is improved with a public sidewalk, and a grass park row. A pedestrian scaled streetlight and an RVTB bus stop are located along the frontage of the property. To the west, there is an on-street parking bay.

Liberty Street is paved with curb and gutter. There is a landscape park row and public sidewalk. As noted, there is a narrow driveway apron from Liberty Street, accessing the site. In the park row, there are two street trees. A Catalpa Tree and a Cedar Tree. These trees restrict the driveway apron width to less than the minimum required width of an apron per the engineering standards. The trees have also caused substantial sidewalk damage and the water meter requires replacement. A separate street tree removal permit has been requested.

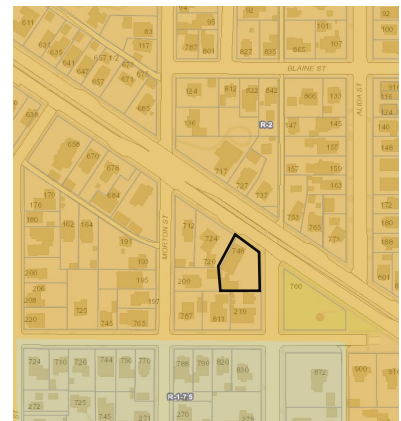


Figure 2: Zoning Map

The subject property and those immediately adjacent are zoned R-2, Low Density, Multi-Family Residential. To the south, across Iowa Street, the properties are zoned single family residential.

Detailed Request:

The request seeks to change the use of the property from church with detach residential to office and storage with detached residential for the use of the American Band College (ABC). ABC is a master’s degree program where nearly 200 band directors from middle school and high school levels, from 40+ states, and a few foreign countries take classes at Ashland High School facilities. ABC provides master educators from various universities to teach the ABC students during the 3-week period in the summer.

The classes are all presented in the Ashland Highschool (AHS) Theater from mid-June to early July. ABC presents two black-tie concerts during the ABC session (June at the Craterian Theater and the Ashland Pops concert at the Ashland Football Field on July 4). ABC is proud to support the Ashland Community by renting classrooms/theater at AHS, sharing music with the AHS band director, renting hotel rooms, eating in local restaurants, shopping at local stores/grocery stores, dining out and enjoying all Ashland has to offer. ABC also enjoys working with the Ashland Chamber of Commerce as they co-host our 4th of July concert and part of the proceeds to help the city with the celebrations each year.

Historically, the ABC has rented space from the Ashland School District for storage of musical and band equipment and sheet music and music books. Due to spatial constraints at the local schools, ABC's storage areas are needed by the schools leading to ABC seeking property where a large area of conditioned storage is provided but its not for daily office or business use. This led ABC to the purchase of the subject property. The subject property was on the market for a period of time after being vacant for many months, and with the previous uses of the site as a church with office, the change to an administrative office and storage of equipment that requires a large area of climatized space for the large percussion related musical instruments that do not travel with students. These include things like tympani's, bass drums, large drum sets, marimbas and xylophones, chimes, etc. Additionally, ABC stores the metal music stands, and stage risers that make up the performance stage and conductor stand for the off-site performances.

A large part of the need for space is for the music that is used by the musicians that attend the ABC program while in Ashland and for performances. The music is stored within a sliding rack system that is constructed within the former sanctuary space. The racks of music are heavy and required structural bracing because it was found that the floor structure of the building was inadequate for the weight of people let alone paper storage.

The existing site development site does not currently comply with several applicable site development standards including parking requirements. There are no improved on-site parking spaces. There is an area in the lawn that is where cars have parked over the years but it is not a defined parking area.

The use of the site as general office and storage is less of an intensity and far fewer occupants than the church's occupancy rating thus not triggering a variance to parking where none exists. Additionally, the change of use for the ABC use is less intense of a use than the permitted density of the property with three to four residential dwellings.

The existing frontage improvements do not comply with the standards as the Siskiyou Boulevard improvements require street trees in the landscape park row and a slightly wider sidewalk.

The proposal is to change the use of structure which requires addressing site deficiencies. In this case, the lack of any on-site parking is proposed to be provided in a limited fashion to allow for the reuse of the site as a low intensity single office and professional music and musical equipment storage. The proposed use is similar to the previous low intensity uses on the site as a religious institution.

The proposed change of use is specific to the ABC use and investment into the site. The change of use will not have a greater adverse effect on the livability of the immediate impact area. The uses in the area include single family and multi-family residential, a public park and a religious institution.

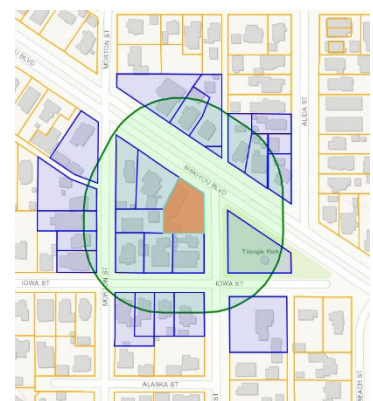


Figure 3: Impact Area

The property has no parking and the modification to the non-conforming site necessitates a conditional use permit. The use change requires a conditional use permit.

The target use of the R-2 zone for the site is 3.6 residential dwelling units, required open spaces, vehicular and bicycle parking and a limited building area due to Maximum Permitted Floor Area in the historic district. The R-2 zone allows for religious institutions and uses such as professional offices are permissible with a Conditional Use Permit.

Parking for residential units is largely based on the area and number of bedrooms assuming there are two additional units of with three bedrooms in addition to the existing residence, there would be six parking spaces required. There are no parking spaces on the site for the residence or the church use.

The previous use as a church with office would have required substantially more than six parking spaces. The number of parking spaces would have been based on the number of seats in the pews. It is not known how many seats or what the previous occupancy of the building was. It can be assumed that some parking would be required.

The proposal seeks to remove the street trees, install a driveway apron that provides access to a two-space parking area. One of the parking spaces is required to be accessible and a van isle is proposed. Because of the proximity of the driveway to the intersection of Liberty Street and Siskiyou Boulevard, a hammerhead is proposed. This is to allow for vehicles to back into the hammerhead and exit the property in a forward manner. The existing retaining wall is proposed to shift away from the property line to allow for a pathway to be created that will provide pedestrian access to the lower floor level from the parking area.

Landscape revisions to the substantial lawn area is proposed. The plan is to relandscape the areas of disturbances following the relocation of the retaining walls, installation of the parking area and street trees.

Additionally, the proposal adds architectural features such as a replacement marquess awning with a pergola over the door on the Siskiyou Boulevard façade, and a gable pediment treatment. The solid wood door is proposed to be widened slightly to a true 36-inch opening and replaced with a solid wood door. The brick planters on either side of the door will be moved. There is no barrier between the dirt and the walls leaving moisture against the wall.

The current accessible route is through the door facing Liberty Street. This entry is presently a residential type, wood framed, divided light patio door. This door is proposed to be replaced with an aluminum frame, commercial style door.

The route begins in the lawn area where there is a concrete path to a short run of stairs leading to a small landing. That landing is also accessible from a narrow ramp that is less than 36-inches wide and

more than a four percent slope. The change of use requires removal of barriers to accessibilities. To achieve this, in addition to the ADA, van accessible parking space, a clear route to the Liberty Street entry is provided with a change to the direction of the walkways and improvements to the ramp. The door threshold will be lowered with the door replacement and the restroom is proposed to be altered to provide accessibility.

The proposal provides parking where presently none existed, provides ADA compliant parking and pathways to access the building. Allows for parking to turn around and exit in a forward manner at the intersection, proposes reduction in lawn area in introduction of drought tolerant landscaping and plants street trees to replace those proposed for removal on the Liberty Street frontage, and to install one on Siskiyou Boulevard baring street tree planting spacing standards or concerns from the Public Works Dept. regarding proximity to intersection, signage for crosswalk, parking bay location, or other visual or physical conflicts on locating a street tree near a busy intersection.

The permitted use of the property as three to four residential dwellings can be found to have a higher intensity impact than the proposed use of the property for the operations of the ABC as an administrative office and musical library and musical equipment climatized storage facility. It can be found that the proposed introduction of parking and accessible routes complies with the Site Review Standards for the change of use to a low intensity use. The changes to the structure are minimally structural, with the replacement of the poor condition marquess with a pergola, and the gable end feature (gable pediment) and the changes to the doors are largely cosmetic but improvement structures overall contribution to the historic district.

The actual historic structure on the site is the small stucco residence, there are no modifications to that building proposed and where no parking was provided, an on-site parking space will be available. This parking will be shared similarly to how the property has operated since the construction of the church building. The preservation of the residence and changes to the site that do not prevent future residential use while allowing a change for a unique use, the American Band College administrative headquarters and storage allows for the revitalization of a large area corner lot. With the further of addition of parking, the structure could be converted to a higher intensity office or residential use than proposed herein which furthers climate, energy goals of preservation of a structures embodied energy and the preservation of historic structures.

Criteria from the Ashland Land Use Ordinance
FINDINGS ADDRESSING CONDITIONAL USE PERMIT APPROVAL CRITERIA

The subject property at 748 Siskiyou Boulevard is noted as a church since 1960s. The 5,108 square feet, one- and one-half story structure with a full basement. The structure has been in the same location and the same shape, orientation, scale, and massing since 1964. The existing site development occurred before the creation of the land use ordinance.

The non-conforming site development is not increasing with the proposal. There are parking spaces proposed for the residential unit and one for the office space, thus increasing conformity. On-site parking has never been provided in conjunction with the development of the site that we can determine, and the proposed use of the structure as an office with storage is not an increase in the non-conforming development of the site.

The structure is considered a non-historic, non-contributing according to the Historic Resources Assessment of the Skidmore-Academy District. The change of use of the site and the creation of parking area requires a Site Design Review. The building use as an administrative office and storage in the multi-Family zone requires a Conditional Use Permit.

The proposal seeks to change the use of the structure from the church occupancy to a single administrative office of less than 500 square feet. The remainder of the building is used as musical document storage including sheet music and music composition books for the use of the students of the American Band College. The students do not attend classes or perform concerts at the building. It is a storage facility for when the college is not in session and off-site with the musical equipment and musical documents for the current educational sessions. The things in the space are relocated offsite hence the need for site alterations to the landscape to accommodate a pathway system and reduced lawn area.

To bring the site closer to compliance with the site design standards, specifically provision of a functional parking area for the residential unit and to provide useable area of the site for vehicle parking when the office is in use and when the equipment is loaded onto a moving van and relocated to the practice and/or concert venue.

The proposal includes the replacement of the existing marquee type overhang on the Siskiyou Boulevard façade the door and adding a pergola structure. The proposed pergola structure will provide the same function as the present overhang, but the design also reduces the feeling of the mass and scale of the structure and add interest to a flat facade. A new, 36-inch solid door is proposed on the Siskiyou Boulevard facade. This door leads to the basement storage area. It is recessed from the street

with a concrete pad between the building and the public sidewalk. The brick planters that have dirt and water in direct contact with the structure will be removed.

The divided light patio door is proposed to be replaced with an aluminum store front style commercial door is proposed on the Liberty Street facade door.

The 11,761 square foot property is zoned low density multi-family residential. There is a small residential structure that is historical in age that will remain on site. The church structure replaced a residential unit that appears in historical mapping and photos.

The primary orientation of the structure was the large gable end facing Siskiyou Boulevard. A new pergola type structure is proposed to break up the mass and scale of the large gable end. The proposed architectural addition will replace a poorly constructed overhang.

The existing structures and concrete surfaces occupy approximately 25 percent of the site. The additional concrete areas for parking and walkways adds 1475 square feet for a total coverage of 4,277 square feet which is less than the allowed 65 percent in the zone.

The existing use of 748 Siskiyou Boulevard as a church sanctuary, including administrative office, a pre-school, community meeting space, event space, and community gathering space, would have required a substantial parking spaces.

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with 1 relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.

Finding:

The proposed use of the property is an administrative office and storage for the American Band Academy. The use is allowed with a conditional use permit. The use is in conformance with the standards of the zoning district and the use allows for the adaptive reuse of a large former church structure.

The use conforms to the relevant comprehensive plan policies and the change of use, retains the existing residential and does not prevent residential development. The adaptive reuse of the property is further supported by the Comprehensive Plan Chapter Seven, The Economic Element, of Ashland's Comprehensive Plan provides for Policies and Goals of the City regarding strong economic support through all decision making.

Goal (7.07.03) To ensure that the local economy increases in its health, and diversifies in the number, type, and size of businesses consistent with the local social needs, public service capabilities, and the retention of a high-quality environment.

Policy #5 - The City shall encourage economic development of the local resources and enhance employment opportunities for existing residents. The City's policy is that economic development shall always have as its primary purpose the enhancement of the community's economic health.

The proposed change of use of the former church structure to an administrative office and storage allows for a long-standing economic partnership between the ABC, Ashland School District, and the Ashland Chamber of Commerce.

2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.

Finding:

There are adequate public facilities that service the property. The water meter is present on Liberty Street. The sanitary sewer is in Liberty Street. A stormwater sewer line is present in Siskiyou Boulevard. The removal of the street trees will allow for the sunken water meter to be reset. Other than that, no changes to the property that would impact the public facilities are proposed.

Both Liberty Street and Siskiyou Boulevard are public streets improved with curb, gutter, sidewalk, storm drain, landscape park row, and street trees.

The Cedar tree and the Catalpa tree in the Liberty Street Park row are proposed for removal. A separate street tree removal permit has been requested.

3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.

Finding:

The use of residentially zoned property as an office and storage use in the low density multi-family residential zone to replace a religious institution, will not have a greater adverse effect on the livability of the immediate impact area. The target use of the zone for the site is 3.6 dwelling units. This would require approximately six parking spaces and per the current regulations,

The proposal is to retain the existing structure and convert to a less intensive use than the existing religious institution or the target use of the site for the purposes of comparison of the impacts of the conditional use permit.

If the entire structure was proposed as office use, there would be eight (8) parking spaces required and two for the residential use for a total of ten (10).

If the property were occupied by the allowed 3.6 – 4 residential dwellings as permitted in the zone, there would be between four to eight parking spaces required on-site. Presently, there are no on-site parking spaces. The existing uses and the non-conforming site development requires a conditional use review.

- a. Similarity in scale, bulk, and coverage.

Finding:

The proposal is to change the use and provide parking for the existing residence to be shared with the ABC administrative office use of the building. Additionally, architectural addition of a modernized, structurally accurate marquee awning and the addition of a gable end treatment, provides interest to the rather dull façade. modernize and update an existing, architecturally lacking, historic structure. The building is a non-contributing structure in the historic resources inventory and the site development is non-conforming.

The proposed additional is appropriate to the scale and mass of the structure. The proposal does not increase the bulk, or coverage of the site. Then proposed cosmetic changes to the Siskiyou Boulevard façade improves the site orientation to the higher order street.

The use of the structure will not change the scale, bulk or coverage of the site to be non-conforming. The addition of the parking area and accessible walkways increases lot coverage, but the coverage is less than permitted in the zone.

- b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.

Finding:

An analysis of parking a traffic was not conducted because the use proposed is less than the allowed residential use of the site as three to four dwellings and is less than the previous use as a religious organization. The American Band College is only in session a few weeks a year and the administrative office is 500 square feet or smaller.

There are public sidewalks along both frontages. There is a striped bike lane along Siskiyou Boulevard.

- c. Architectural compatibility with the impact area.

Finding:

The proposed exterior additions are architecturally compatible with the historic interest area. The majority of the nearby properties are occupied by residential uses. The nearby church on has a similar architectural detail in the gable end.

- d. Air quality, including the generation of dust, odors, or other environmental pollutants.

Finding:

The proposed modification to the use of the structure and the continued use of the property for residential use, will not generate additional air quality issues, dust, odors or other environmental pollutants.

- e. Generation of noise, light, and glare.

Finding:

The proposed use as small office and storage will not generate noise, light, or glare beyond what is expected in a multifamily residential zone. The target use of the site as three to four residential units would have the propensity to generate more noise, light or glare than the proposed use.

- f. The development of adjacent properties as envisioned in the Comprehensive Plan.

Finding:

The proposal will not have any impact on the development of adjacent properties as envisioned in the Comprehensive Plan.

- g. Other factors found to be relevant by the approval authority for review of the proposed use.

Finding:

It is unclear what other factors would be relevant.

- 4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.

Finding:

Office use is a permitted in the zone with a conditional use permit.

- 5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.

c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.

Finding:

The residential density of the property is 3.6 dwelling units. Those would require between four to eight parking spaces, and at least 950 square feet of open space. The proposal to change the use of the structure to storage and office is less of an impact to the livability of the zone than 3.6 residential dwellings would be.

Site Development Design Standards Approval Criteria:

18.5.2.050 Approval Criteria

An application for Site Design Review shall be approved if the proposal meets the criteria in subsections A, B, C, and D below.

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

Finding:

The 11,761 square foot property is zoned low density multi-family residential. There is a 796 square foot, two-bedroom residence structure that is historical in age that will remain on site and is a permitted use. The proposal is to convert the 4,312 square foot religious structure to an office and storage area. The proposed uses are allowed with a conditional use permit.

The primary orientation of the structure was the large gable end facing Siskiyou Boulevard. A new marquess awning structure and a gable end treatment. Both the gable end treatment and the awning are proposed to break up the mass and scale of the large gable end. The proposed marquess will replace a poorly constructed overhang.

There is no parking for the existing uses. The proposal seeks to provide two parking spaces. When creating parking in conjunction with the commercial type of use at least one must be ADA with van off-loading zone. The parking complies or can comply with conditions to meet the standards. There is also a turnaround area to allow for existing in a forward manner near the intersection of Liberty and Siskiyou.

The existing structures and concrete surfaces occupy approximately 32 percent of the site. The additional concrete areas for parking and walkways adds 2,668 square feet for a total coverage of 6461.25 square feet, 55 percent of the property which is less than the allowed 65 percent coverage in the zone. There are no changes to setbacks of the structures.

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

Finding:

The property is subject to the Historic District Standards for exterior additions. As evidenced in the findings below it can be found that the proposed development complies with the historic site development standards and the Secretary of the Interior Standards for the rehabilitation of the non-historic, non-contributing structure.

C. Site Development and Design Standards. The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

18.4.2.040 Non-Residential Development

Finding:

The building proposed for a change of use is an existing, non-conforming, non-residential structure in the residential zone. The proposed exterior modifications will have a positive impact on the streetscape by improving the structures mass, scale and orientation towards Siskiyou Boulevard. The changes to the Liberty Street facade provide on-site parking, and ADA accessible parking and access to the building where previously lacking in meeting the standards. Though two parking spaces is less than required for the proposed use, the parking is adequate for the proposed use of the building as largely storage and only a small office. The parking will continue to be shared with the tenant(s) that are more frequent users of the parking.

B. Basic Site Review Standards.

1. Orientation and Scale.

Finding:

The existing structure is physically oriented towards Siskiyou Boulevard. This façade though facing the higher order street, the Liberty Street façade is functionally the front due to the location of the entry lobby area of the church structure. The tall street façade, has a solid door and large picture window that face Siskiyou Boulevard. Over this door is a poorly constructed marquee type overhang and then there is 17-feet of unadorned flat, vertical siding facade wall where the church cross previously was hung. A truss to create a gable pediment treatment is proposed to break up the mass of the tall, blank wall facing the public street. These proposed changes decrease the mass of the building and increases the orientation to the public street. The door on the Liberty Street façade is proposed to be changed from a residential style patio door to a commercial storefront type of door which will improve the orientation of this façade to the street.

The lower level is the space where the storage occurs. This space will have access from the parking area to allow for ease of access from storage and in the event of a change of use to more intensive, there is adequate ADA access to each level of the structure.

Accessible pathways from the parking area and the public sidewalk are proposed to be extended to the Siskiyou Boulevard facing door and the Liberty Street facing door.

The proposed entry treatment and door changes orients the structure to the streets and provides direct access from the public sidewalk to the entrances. The Siskiyou facing door is not presently and for the present use will not be accessible to the public during business hours. This level is used for storage and consistent with AMC 18.4.2.040.B.1.g. this door is not accessible for pedestrians due to the use. The office is accessible from the Liberty Street entry.

There are no improved on-site parking or vehicle access areas. The proposal provides for two, surface parking spaces to the side of the primary structure on the site (Siskiyou oriented church building) and are not between the façade of the Liberty Street oriented residence. Though not formally provided, this is the area of the existing lawn parking and access to the Liberty Street facing door.

The proposed parking spaces are accessible from the widened Liberty Street driveway apron. The parking spaces provide an accessible route to the Siskiyou Boulevard façade with the proposed walkway. There is also a hammerhead proposed to allow for the head-in parked vehicle to back up and turn to forward facing to allow for better site line of the Liberty Street and Siskiyou Boulevard intersection.

The parking area is located in the only site accessible area which is between Liberty Street and the entry door into the lobby area of the structure. The location of the buildings on the site and the driveway apron location, grade of the property, and the inability to relocate the sites vehicular access, require the parking between the building and Liberty Street.

2. Streetscape.

Finding:

There is a grass park row on Siskiyou Boulevard. There is an RVTD bus stop (flag stop, not covered). There is also a marked pedestrian crossing of Siskiyou Boulevard, street signage and a pedestrian scaled streetlight. Due to these things and the required spacing standards, it's an inappropriate location to plant street tree. Which is a vertical element to conceal pedestrians. If required, a street appropriate for the narrow park row will be planted.

There are two street trees on Liberty Street that are proposed for removal under separate permit. These trees will be replaced with appropriate street trees that are spaced more appropriate than those proposed for removal.

3. Landscaping.

Finding:

The landscaping is pre-existing. There is a large lawn area. Along the property boundaries is a series of retaining walls made of poured in place concrete, stacked block and rock is some dead vegetation. The proposal provides a landscape plan that includes revegetation of the areas of disturbance along the frontage and along the proposed retaining walls. The areas adjacent to the parking spaces will be revegetated with lawn area.

4. Designated Creek Protection.

Finding:

Not applicable

5. Noise and Glare.

Finding:

Additional light and glare beyond what is standard in the multi-family residential zone are not anticipated. The proposed use is not noisy beyond typical residential uses allowed as permitted or conditional uses.

Any new artificial lighting will comply with the standards of 18.4.4.050.

6. Expansion of Existing Sites and Buildings.

Finding:

The existing site is non-conforming in that it does not have any on-site vehicle or bicycle parking for the residential use nor the previous religious institution use. The area of the existing structure proposed change of use does not expand the non-conforming situation and proposes two parking spaces, one of which is a van accessible ADA parking space. The parking area is to the side of the structures and is separated from the residential structure by eight-feet. There is more than five-feet of landscaping between the parking spaces and the property lines.

The addition of the gable pediment treatment does not expand or enlarge the building or the site. The replacement awning structure does not expand or enlarge the building or the site.

The existing structures areas above grade complies with the maximum permitted floor area of the site. Maximum permitted floor area if the structures were all residential is 3,161.36 square feet. The main level of the building proposed to be changed in use and the 796 square foot detached residence is 2,812 square feet which is less than the permitted MPFA in the zone.

18.4.2.050 Historic District Development

Finding:

The property is located within the Siskiyou Hargadine Historic District.

The proposal is to change the use of the non- historic, non-contributing structure and to add a gable pediment, repair the poorly constructed awning over the Siskiyou Boulevard door. The doe not impact the form or shape of the existing structure. The proposed alterations will have a positive impact through visual relief to reduce the mass and scale facing the public street. on the scale, form, and mass.

It can be found that the proposed exterior alterations are architecturally compatible with the historic district design standards for a renovated commercial structure.

The proposal seeks to modify the Liberty Street façade by installing a commercial storefront type door.

B. Historic District Design Standards.

1. Transitional Areas.

Finding:

The property is zoned residential but has been occupied by a quasi-commercial type use with the church structure occupying the majority of the property. The proposed cosmetic alterations to the structure will provide interests and reduce the mass and scale of the tall, blank wall. Gable pediment treatments and corbels are a feature of the residences and the church building to the northeast across Siskiyou Boulevard.

2. Height.

Finding:

No modifications to the height.

3. Scale.

Finding:

The scale of the structure is not is not altered. The proposed gable pediment treatment provides a reduction is the scale of the blank wall and reduces the mass.

4. Massing.

Finding:

The mass of the structure is reduced with the rehabilitated awning cover and the gable pediment. The addition of the features

The existing exterior treatments provide vertical and historical rhythms. The continued use of horizontal siding is consistent with the historical finish of the structure and the new glazing for the entry and the new windows provides vertical elements to the design.

5. Setback.

Finding:

No change.

6. Roof.

Finding:

No change.

7. Rhythm of Openings.

Finding:

There are no changes in the rhythm of openings. The replacement of the wood framed, divided light, patio door with a commercial style storefront door is an appropriate replacement to provide security and the door has a more accessible threshold height than a residential type of installation that exists. The door type allows for the adaptive reuse of the building.

8. Base or Platforms.

Finding:

No change.

9. Form.

Finding:

No change.

10. Entrances.

Finding:

The replacement of the wood framed, divided light, patio door with a commercial style storefront door is an appropriate replacement

11. Imitation of Historic Features.

Finding:

Not applicable.

12. Additions:

Finding:

Not applicable.

13. Garage:

Finding:

Not applicable.

C. Rehabilitation Standards for Existing Buildings and Additions.

2. Rehabilitation Standards. In addition to the standards of part [18.4](#), the approval authority uses the following standards for existing buildings and additions within the Historic District Overlay. These standards apply primarily to residential historic districts, residential buildings in the Downtown Historic District, and National Register-listed historic buildings not located within the Historic District Overlay. The purpose of the following standards is to prevent incompatible treatment of buildings in the Historic District Overlay and to ensure that new additions and materials maintain the historic and architectural character of the district.

Finding:

The proposed exterior modifications are improvements to the façade of the non-historic, non-contributing structure that is within the residential historic district overlay.

The existing structure is not historically significant and lacks character. The structure was built for the utilitarian use as a religious institution. The structure is a tall, 26' 3" façade setback 15' from the Siskiyou Boulevard property line. There is an overhang facing Siskiyou which is in poor condition that will be replaced with a pergola structure. Within the gable, a gable pediment is proposed. This craftsman type detail is reflected in numerous structures in the Siskiyou Hargadine historic district. The use of beams, corbels, gable trim and pediment details, decorative eaves and similar are found on the structures in the impact area. The building has vertical siding that adds to the mass and scale of the façade of the tall, flat facade structure facing the street. The proposed pergola and gable detail provide relief and reduction in mass and scale by increasing the scale of the the horizontal element facing Siskiyou.

The findings address how the proposed exterior modifications are improvements to the use of, the functionality of the site, and improve the Siskiyou Boulevard façade of the non-historic, non-contributing, non-descript structure.

- a. Historic architectural styles and associated features shall not be replicated in new additions or associated buildings.

Finding:

Not applicable.

- b. Original architectural features shall be restored as much as possible when those features can be documented.

Finding:

Not applicable.

- c. Replacement finishes on exterior walls of historic buildings shall match the original finish. Exterior finishes on new additions to historic buildings shall be compatible with, but not replicate, the finish of the historic building.

Finding:

Not applicable.

- d. Diagonal and vertical siding shall be avoided on new additions or on historic buildings except in those instances where it was used as the original siding.

Finding:

Not applicable, no change proposed.

- e. Exterior wall colors on new additions shall match those of the historic building.

Finding:

Not applicable.

f. Imitative materials including but not limited to asphalt siding, wood textured aluminum siding, and artificial stone shall be avoided.

Finding:

Not applicable.

g. Replacement windows in historic buildings shall match the original windows. Windows in new additions shall be compatible in proportion, shape and size, but not replicate original windows in the historic building.

Finding:

Not applicable.

h. Reconstructed roofs on historic buildings shall match the pitch and form of the original roof. Roofs on new additions shall match the pitch and form of the historic building, and shall be attached at a different height so the addition can be differentiated from the historic building. Shed roofs are acceptable for one-story rear additions.

Finding:

Not applicable.

i. Asphalt or composition shingle roofs are preferred. Asphalt shingles that match the original roof material in color and texture are acceptable. Wood shake, wood-shingle, tile, and metal roofs shall be avoided.

Finding:

Not applicable.

j. New porches or entries shall be compatible with, but not replicate, the historic character of the building.

Finding:

Not applicable

k. New detached buildings shall be compatible with the associated historic building and shall conform to the above standards.

Finding:

Not applicable

1. The latest version of the Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings shall be used in clarifying and determining whether the above standards are met.

Finding:

The building is not a historic structure.

D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Finding:

There are adequate public facilities that service the property. There are no changes that impact the existing public utilities.

A street tree removal permit has been requested.

E. Exception to the Site Development and Design Standards. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1, 2, or 3, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty;

Finding:

An exception to the standards regarding location of parking areas between the building and the street and the public pedestrian access to the building entrance shall be functional, and shall be open to the public during all business hours. (AMC 18.4.2.040.B.1.a. and AMC 18.4.2.040.B.1.c.).

The exceptions are necessary to meet the Site Development Standards for non-residential development for the existing structure due to the location of the existing structure, existing site access from Liberty

Street, accessible level of the structure that is not a basement storage area is from the Liberty Street façade and not the basement level, Siskiyou Boulevard façade.

There are no improved on-site parking or vehicle access areas. The only location to provide parking is between the structure entry facing Liberty Street which is the primary building access and the street.

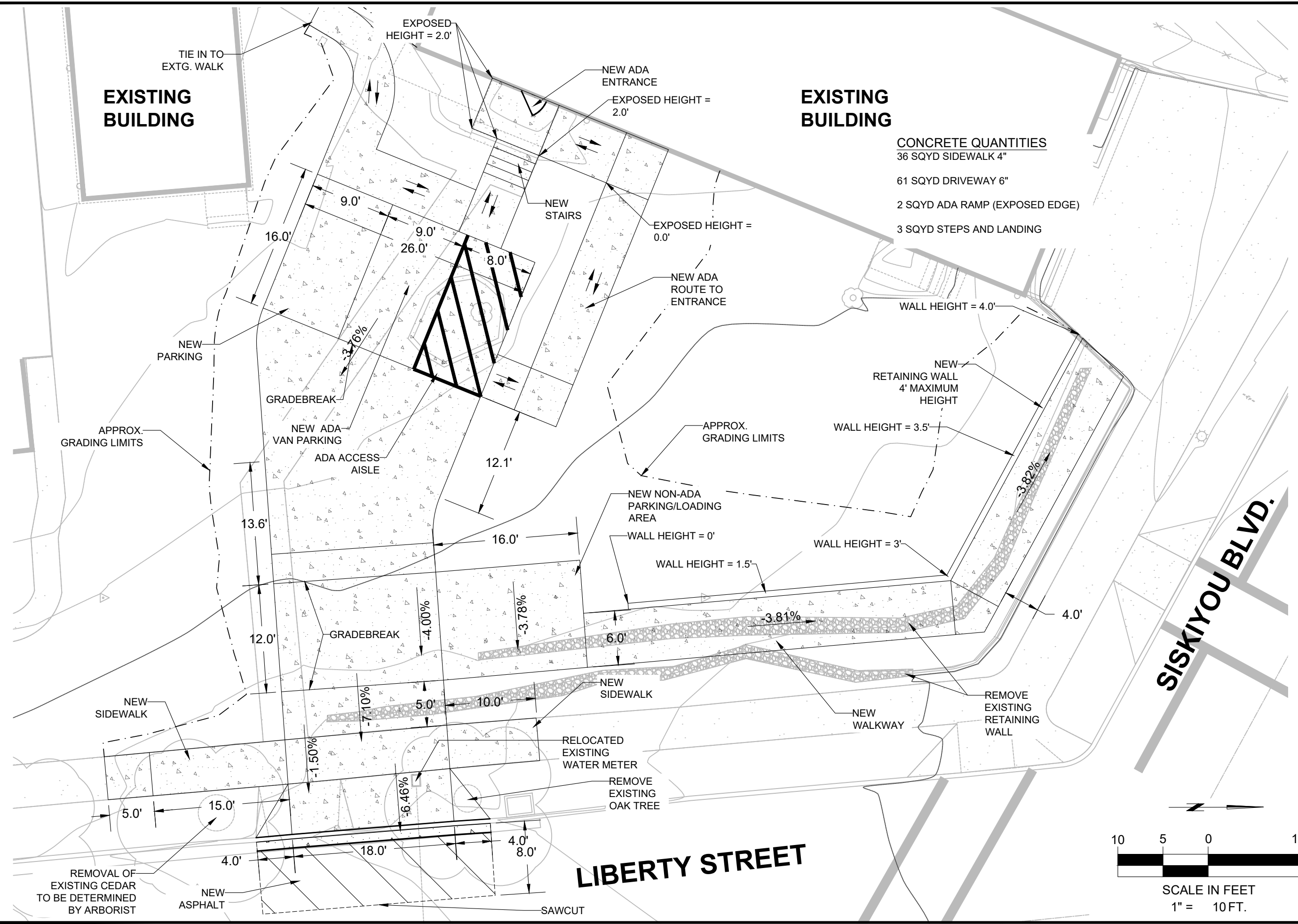
Though not formally provided, this is the area of the existing lawn parking and access to the Liberty Street facing door. The driveway access to the site is from Liberty Street with none permitted from Siskiyou Boulevard.

The parking area is located in the only site accessible area which is between Liberty Street and the entry door into the lobby area of the structure. The location of the buildings on the site and the driveway apron location, grade of the property, and the inability to relocate the sites vehicular access, require the parking between the building and Liberty Street.

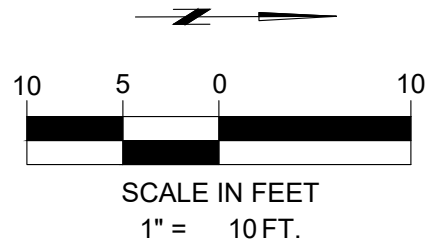
The request is the minimum necessary to alleviate the difficulty and the two, surface parking spaces are not between the façade of the Liberty Street oriented historic residence.

The use of the site utilizes the level of the building accessible from the Siskiyou sidewalk as storage thus no pedestrian access. The exception is to allow for this door to not be considered an accessible access to the structure due to the existing functionality and proposed use. The building is being rehabilitated structurally that will allow for future use of the space as a more intense use if desired by a different user and the Siskiyou entry could be utilized. Thus the request is the minimum necessary to address the standard.

DATE: 9/13/22 FILE: O:\CW_Projects\2204_Misc_Private_Engineering_Work\2204-213 - American Band College - WIBC - 748 Siskiyou Conditional Use Permit Drawings\04 Final Design\Drawings\Exhibits\2204-213 AB College WIBC-Siskiyou Conditional Use Permit-Exhibit 3 (ada parking).dwg



CONCRETE QUANTITIES
36 SQYD SIDEWALK 4"
61 SQYD DRIVEWAY 6"
2 SQYD ADA RAMP (EXPOSED EDGE)
3 SQYD STEPS AND LANDING



PRELIMINARY



PREPARED BY: **Civil West**
Engineering Services, Inc.

830 O'HARE PARKWAY, SUITE 102
MEDFORD, OR 97504
541-266-8601
www.civilwest.com

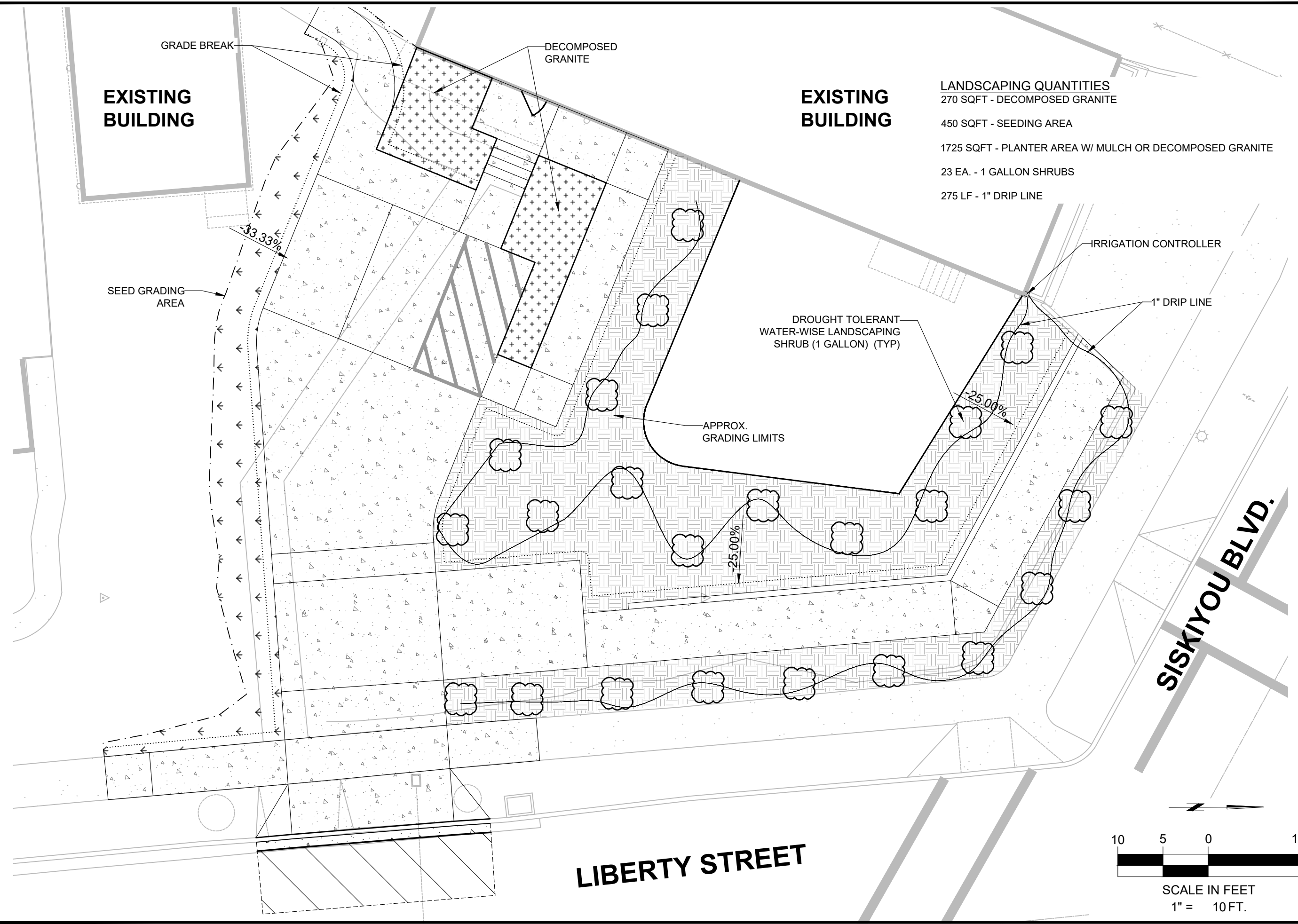
REV.	DATE	DESCRIPTION	BY

DESIGNED BY: DWT DRAWN BY: DWT CHECKED BY: MB
PROJECT NO: 2204-213

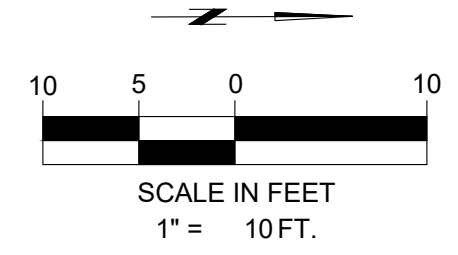
CITY OF ASHLAND
JACKSON COUNTY, OREGON
AMERICAN BAND COLLEGE
SISKIYOU CONDITIONAL USE PERMIT
CONCEPT EXHIBIT

SHEET NO. **EXH-3**

DATE:9/14/22 FILE:O:\CW_Projects\2204_213 - American Band College - 748 Siskiyou Conditional Use Permit Drawings\04 Final Design\Drawings\Exhibits\2204-213 AB College WIBC-Siskiyou Conditional Use Permit-Exhibit 4 Landscaping.dwg



- LANDSCAPING QUANTITIES**
- 270 SQFT - DECOMPOSED GRANITE
 - 450 SQFT - SEEDING AREA
 - 1725 SQFT - PLANTER AREA W/ MULCH OR DECOMPOSED GRANITE
 - 23 EA. - 1 GALLON SHRUBS
 - 275 LF - 1" DRIP LINE



PRELIMINARY



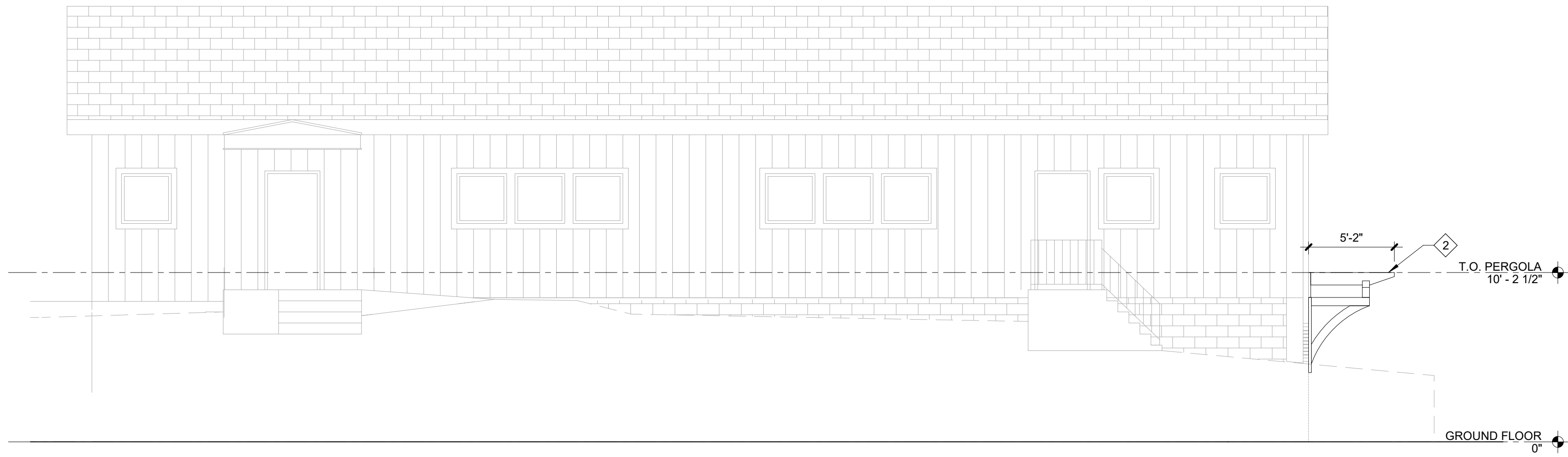
830 O'HARE PARKWAY, SUITE 102
MEDFORD, OR 97504
541-266-8601
www.civilwest.com

REV.	DATE	DESCRIPTION	BY

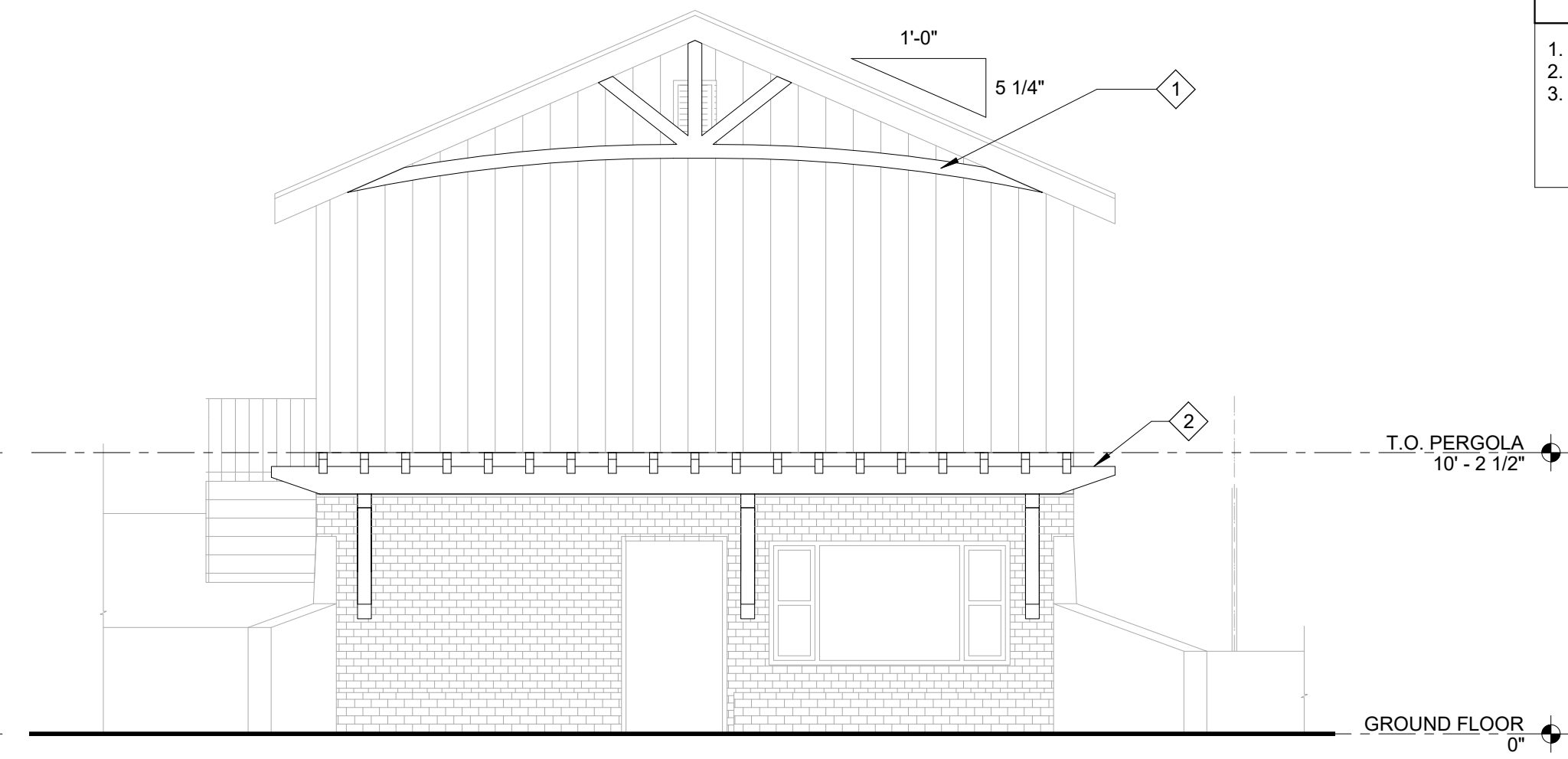
DESIGNED BY: DWT	DRAWN BY: DWT	CHECKED BY: MB
PROJECT NO: 2204-213		

CITY OF ASHLAND
JACKSON COUNTY, OREGON
AMERICAN BAND COLLEGE
SISKIYOU CONDITIONAL USE PERMIT
LANDSCAPING

SHEET NO. **EXH-3**

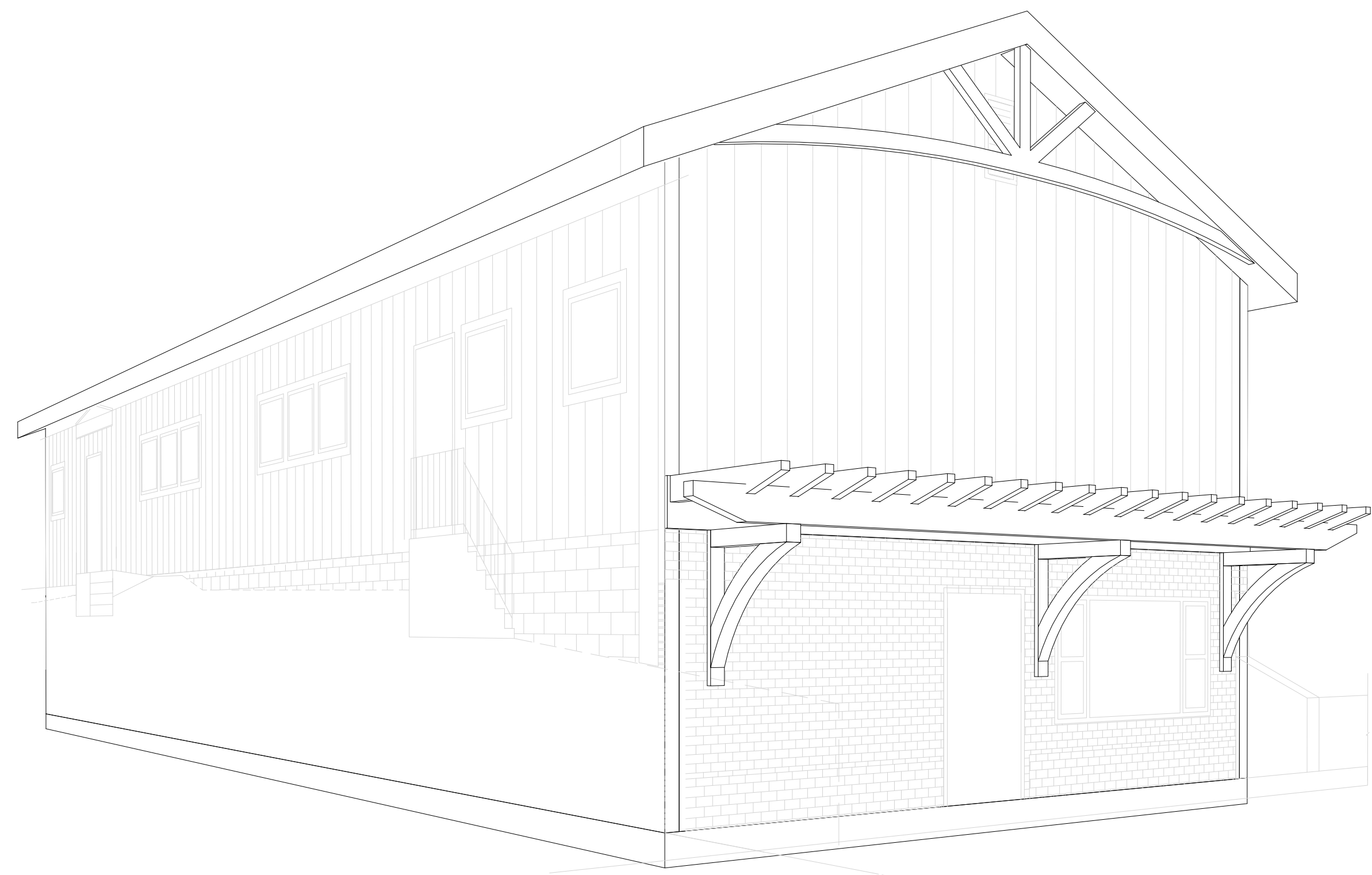


EAST ELEVATION 2
3/16" = 1'-0"

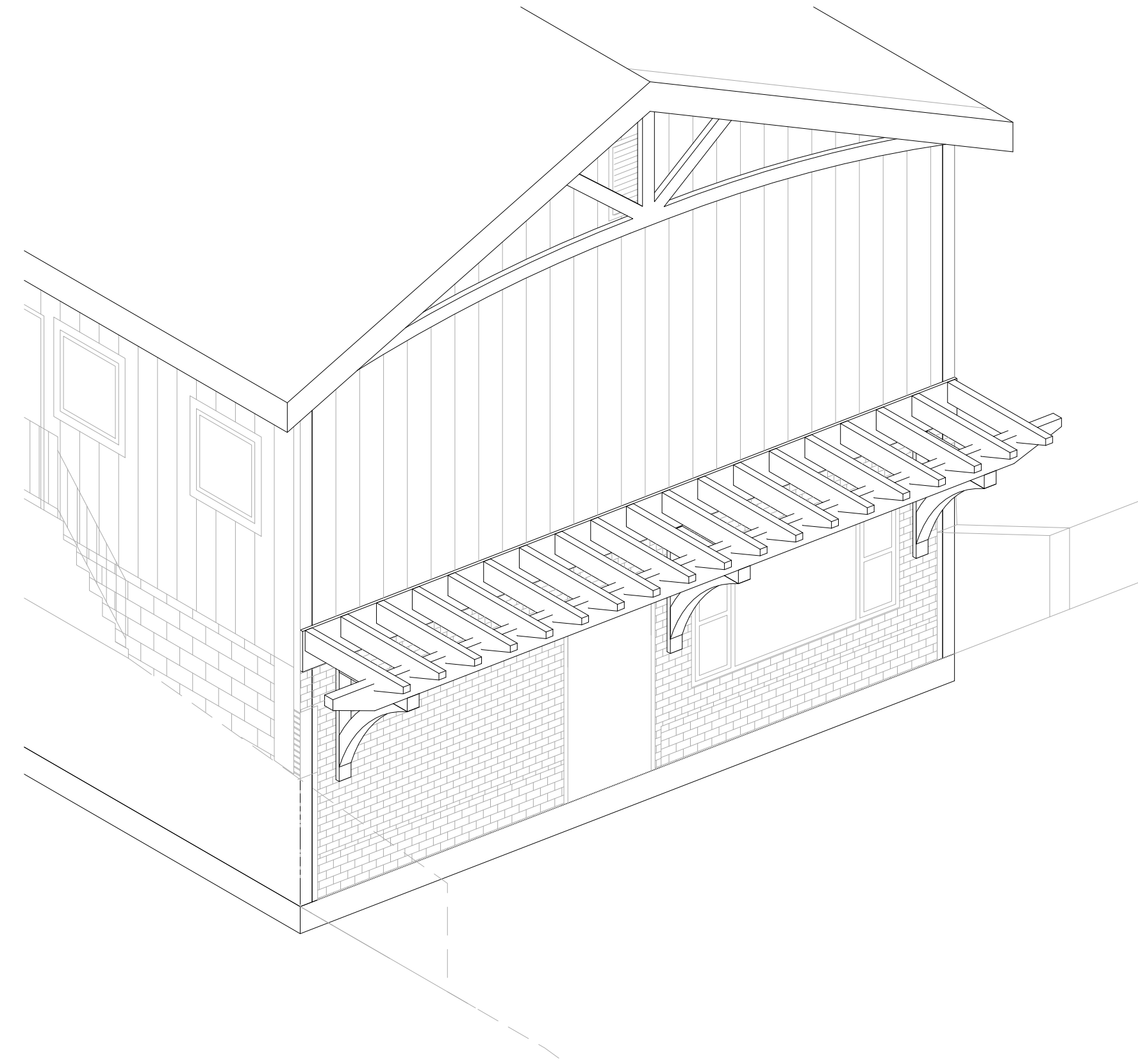


NORTH ELEVATION 1
3/16" = 1'-0"

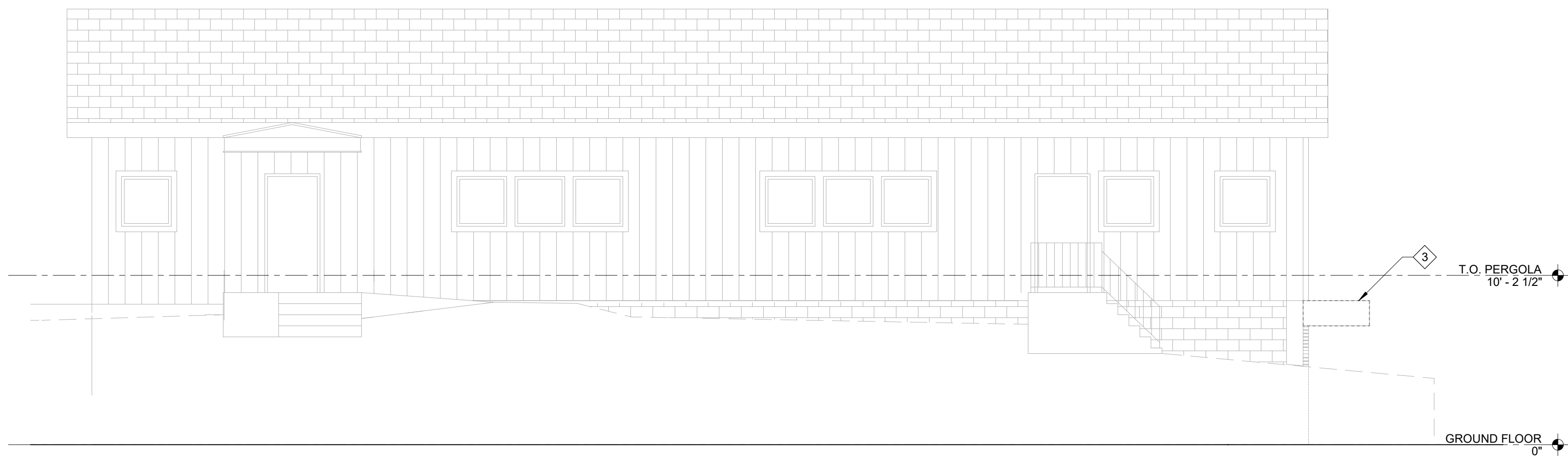
KEYNOTES	
1.	NEW GABLE TRIM
2.	NEW PERGOLA
3.	DEMO EXISTING OVERHANG



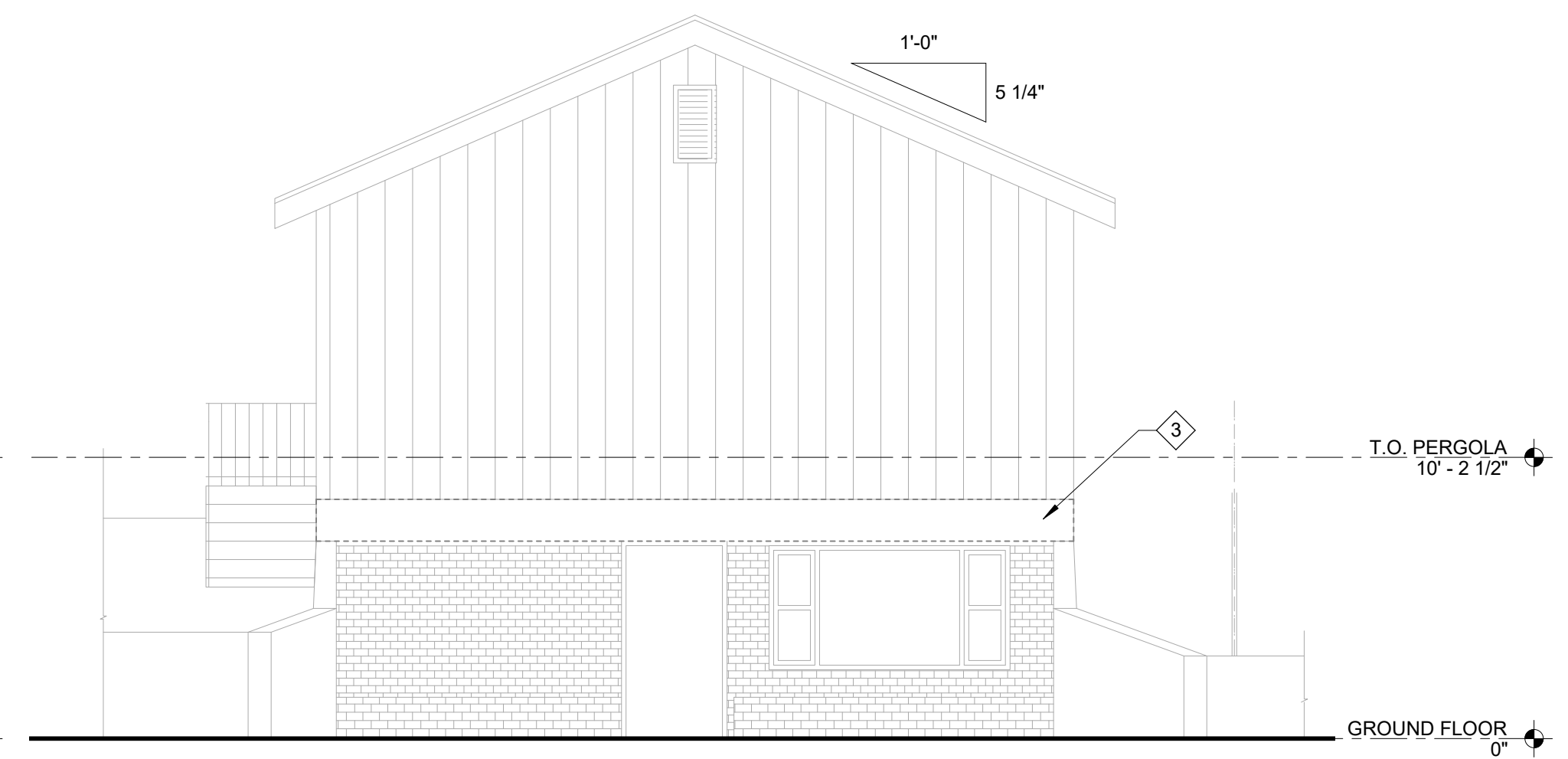
3D VIEW 4



BIRDS VIEW 3



(E) EAST ELEVATION_DEMO 6
3/16" = 1'-0"



(E) NORTH ELEVATION_DEMO 5
3/16" = 1'-0"

ONE INCH EQUALS FULL SCALE

AMERICAN BAND COLLEGE
ASHLAND OR

REVISION ID:	DATE:

PROJECT NO. 005
DRAWN:
DATE:

PERGOLA DESIGN

A6

SCHEMATIC DESIGN

CITY OF ASHLAND

JACKSON COUNTY, OREGON

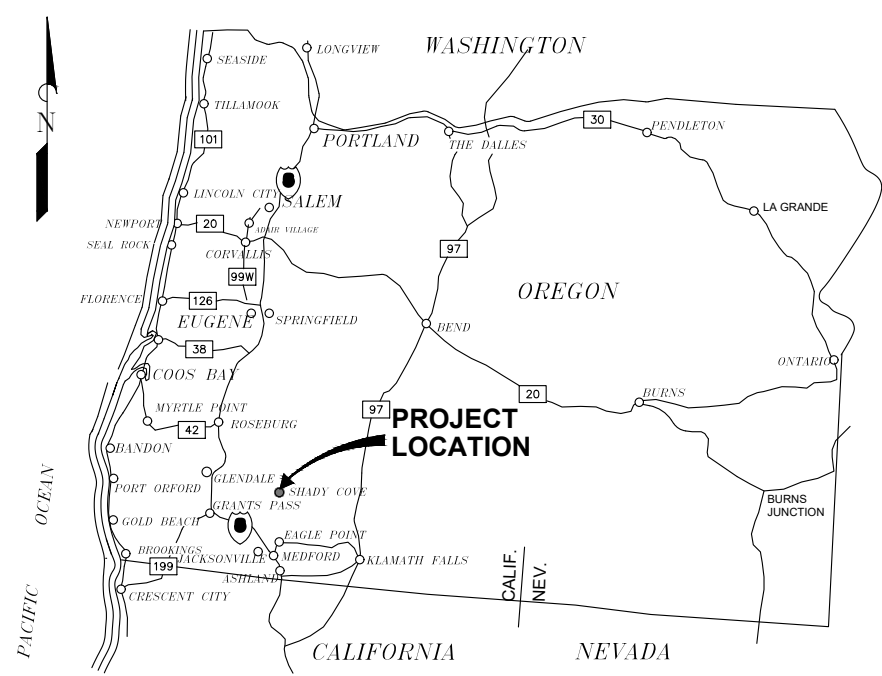
AMERICAN BAND COLLEGE HIGH DENSITY STORAGE

PROJECT NO. 2204-162
OCTOBER 2022

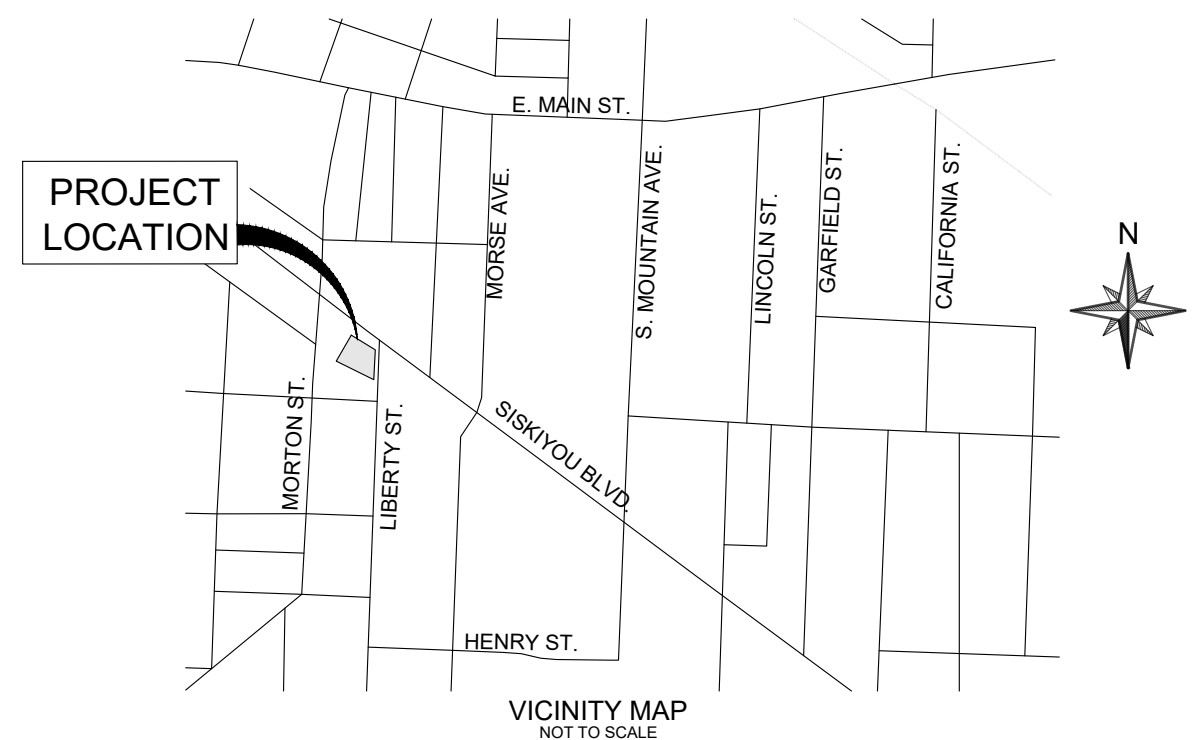


EXPIRATION DATE: 06/30/23

PREPARED BY: **Civil West**
Engineering Services, Inc.
830 O'HARE PARKWAY, SUITE 102
MEDFORD, OR 97504
541-266-8601
www.civilwest.com



LOCATION MAP



VICINITY MAP
NOT TO SCALE

GENERAL NOTES

- ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH 952-001-0090. YOU MAY OBTAIN A COPY OF THE RULES BY CALLING THE CENTER. NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503) 232-1987. STAT. AUTH.: ORS 757.542 THROUGH ORS 757.562 AND ORS 757.993.
- THE CONTRACTOR SHALL CONTACT 'ONE CALL' FOR UTILITY LOCATES PRIOR TO EXCAVATION. (1-800-332-2344)
- ALL OVERHEAD ELECTRICAL DISTRIBUTION SYSTEMS MAY NOT BE SPECIFICALLY INDICATED ON THE DRAWINGS BUT DO EXIST ALONG THE PIPELINE ROUTES. CONTRACTOR IS TO TAKE ALL NECESSARY PRECAUTION AROUND EXISTING POWER LINES AND MAINTAIN SAFE DISTANCES DURING THE CONSTRUCTION PROCESS.
- ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO ALL GOVERNING AGENCIES REGULATORY REQUIREMENTS. OWNER/CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY AND COUNTY PERMITS RELATES TO THE CONSTRUCTION OF IMPROVEMENTS SHOWN HEREON.
- PROPERTY AND RIGHT OF WAY LINES SHOWN IN THIS PLAN SET ARE APPROXIMATE AND BASED ON BEST AVAILABLE INFORMATION. CONTRACTOR SHALL OBTAIN TEMPORARY CONSTRUCTION ACCESS OR PERMISSION IF NECESSARY FROM PRIVATE LAND OWNERS PRIOR TO ENTERING PRIVATE PROPERTY.

SHEET INDEX

SHEET#	SHEET NAME
G1.0	COVER SHEET
S1.0	GENERAL STRUCTURAL NOTES
S1.2	ELEVATIONS
S1.3	SHEAR WALL AND HOLD SCHEDULE
S2.0	FLOOR AND FOUNDATION PLAN
S2.1	2ND FLOOR IMPROVEMENT PLAN
S2.2	PERGOLA PLAN
S2.3	PERGOLA CONNECTION DETAIL
S3.0	SECTIONAL ANDS DETAILS
S3.1	CONNECTION AND FOOTING DETAILS



CITY OF ASHLAND
JACKSON COUNTY, OREGON
AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
COVER SHEET

DATE/SHEET NO. **G1.0**
10/07/22

DATE: 9/30/22 FILE: O:\CW_Proj\2204-162\Drawings\2204-162-AS-Structural\2204-162-AS-Structural.dwg, 9/30/2022, 9:52:26 AM, A1

PROJECT STRUCTURAL NOTES: (ASHLAND, OREGON)

GENERAL INFORMATION

- GOVERNING CODE IS THE 2018 OREGON STRUCTURAL SPECIALTY CODE, ASCE7-16, ACI 318-19, AWC-NDS-18, AWC-SPDWS, IBC-18.
- THE PROJECT WAS DESIGNED FOR THE FOLLOWING LOADS:
LOADS:

a. ROOF LIVE	20 PSF SNOW LOAD
b. LIVE LOAD	40 PSF
c. ROOF DEAD	20 PSF
d. 2ND FLOOR	20 PSF
e. HD-STORAGE	167 PSF
f. WIND LOAD	120 MPH
	EXPOSURE B
	IW = 1.0
g. SEISMIC LOAD	SEISMIC DESIGN CATEGORY D
	SEISMIC SITE CLASS D
	S _s = 0.693
	S ₁ = 0.421
	V = 5.1 K(R=8)
- THE GENERAL CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS PRIOR TO COMMENCING WORK.
- ALL FEATURES OF CONSTRUCTION NOT FULLY SHOWN SHALL BE OF THE SAME TYPE AND CHARACTER AS SHOWN FOR SIMILAR CONDITIONS SUBJECT TO REVIEW OF THE ENGINEER OF RECORD.
- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION MEANS AND METHODS. RESPONSIBILITY SHALL INCLUDE BUT IS NOT LIMITED TO DEMOLITION AND CONSTRUCTION MEANS AND METHODS, TECHNIQUES, SEQUENCING, AND SAFETY REQUIRED TO COMPLETE CONSTRUCTION.
- ALL ERECTION BRACING, TEMPORARY SHORING AND CONSTRUCTION SEQUENCING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- RESIDENCE HAS BEEN DESIGNED BASED ON THE ASSUMPTION THAT THE BUILDING LOT IS FLAT. G.C. TO CONTACT ENGINEER IF RETAINING WALLS ARE REQUIRED.
- ALL WATERPROOFING, DAMP PROOFING, AND WEATHERPROOFING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

SPECIAL INSPECTION

- SPECIAL INSPECTION REQUIRED SHALL BE PROVIDED PER OSSC CHAPTER 17 AND AS REQUIRED BY LOCAL JURISDICTION.

FOUNDATIONS

- FOUNDATION SIZES ARE BASED ON THE TOTAL LOAD BEARING PRESSURE OF 1500 PSF. NO GEOTECHNICAL EVALUATION HAS BEEN PERFORMED FOR THIS PROJECT. G.C. SHALL COORDINATE EFFORTS TO VERIFY PRIOR TO CONSTRUCTION.
- ALL FOOTINGS SHALL BE PLACED ON A MINIMUM 12" OF IMPORTED GRANULAR STRUCTURAL FILL. IMPORTED GRANULAR SHALL BE PLACED IN LIFTS RESTRICTED TO 6 INCHES IN LOOSE THICKNESS, MOISTURE CONDITIONED TO ABOUT 2% ABOVE OPTIMAL, AND COMPACTED TO A MINIMUM 95% COMPACTION IN ACCORDANCE WITH ASTM D698. ALL FOUNDATIONS FOR ANY PARTICULAR STRUCTURE SHALL BE FOUNDED ON THE SAME MATERIAL.
- BOTTOM OF ALL FOOTINGS TO BE EMBEDDED BELOW FROST DEPTH.

REINFORCING STEEL

- ALL REINFORCING STEEL SHALL CONFORM TO ASTM A 615 GRADE 60.
- WELDED WIRE FABRIC TO BE PER ASTM A185
- PLACE 2-0 X 2-0 BARS AT CORNERS AND INTERSECTIONS.
- ALL REINFORCING STEEL SHALL BE DETAILED, FABRICATED, AND PLACED IN ACCORDANCE WITH ACI DETAILING MANUAL 315.
- LAP ALL REINFORCING BARS MINIMUM 30" (UNLESS NOTED PER ACI 318-19).
- SECURELY TIE ALL REINFORCING STEEL PRIOR TO PLACING CONCRETE. ALL TIE WIRE SHALL BE MIN. #16 ANNEALED STEEL.
- PROVIDE 3" CLEARANCE FOR ALL REINFORCING STEEL IN CONCRETE CAST AGAINST EARTH. 2" FOR CONCRETE EXPOSED TO EARTH AND WEATHER, AND 1-1/2" FOR ALL INTERIOR EXPOSURE AGAINST GROUND, AND 3/4" FOR ALL OTHER INTERIOR EXPOSURE.
- ALL REINFORCING STEEL SHALL BE SUPPORTED ON WELL CURED CONCRETE BLOCKS OR APPROVED METAL CHAIRS, AS SPECIFIED BY THE CRSI MANUAL OF STANDARD PRACTICES, MSP 2009.

CONCRETE

- ALL CONCRETE SHALL BE DEVELOPED A MINIMUM 28-DAY LAB CURED COMPRESSIVE STRENGTH OF 4,000 PSI. ALL FOOTINGS WERE DESIGNED WITH F'C = 4,000 PSI.
- ALL EXPOSED CONCRETE TO HAVE 5% PLUS OR MINUS 1% AIR, BY VOLUME, CONFORMING TO ASTM C260.
- ALL CONCRETE SHALL HAVE A MAXIMUM SLUMP OF 4".
- ALL CONCRETE SHALL BE CURED PER ACI 315-005 STANDARD
- THE CURING AND JOINTING SPECIFICATION IN THIS SUBMITTAL MEET THE INTEND OF ACI RECOMMENDATIONS TO MINIMIZE UNCONTROLLED SLAB CRACKING. CONTRACTOR SHALL REVIEW THE SLAB AND SLAB JOINT SPECIFICATIONS AND APPROVE PRIOR TO CONCRETE PLACEMENT. CIVIL WEST WILL NOT BE RESPONSIBLE FOR MITIGATION OF SLAB CRACKING THAT OCCURS OUTSIDE THE CRACK CONTROL GRID.

FRAMING LUMBER

- ALL FRAMING LUMBER SHALL BE DOUGLAS FIR-LARCH AND SHALL BE GRADED UNER THE MOST RECENTLY ADOPTED RULES OF THE WEST COAST LUMBER INSPECTION BUREAU (WCLIB).
- ALL BEAMS AND JOISTS SHALL BE NUMBER 2 (UNLESS NOTED OTHERWISE).
- ALL STUDS AND BLOCKING SHALL BE NUMBER 2.
- ALL LUMBER IN CONTACT WITH CONCRETE OR EXPOSED SHALL BE PRESSURE TREATED IN ACCORDANCE WITH AWPA STANDARD C-2 AND SHALL BEAR THE AWPA QUALITY MARK.
- DOUBLE ALL JOISTS OR BCI LADDER FRAMING @ 24" O.C. UNDER WALL PARTITIONS, AND PROVIDE BLOCKING BETWEEN JOISTS WHERE BEARING WALLS ARE PERPENDICULAR TO JOISTS.
- ALL GLULAM BEAMS TO BE 24F-V4 TYPICAL, 24F-V8 FOR CANTILEVERED OR CONTINUOUS SPAN, U.N.O.
- ALL LVL LUMBER TO BE BOISE CASCADE VERSA-LAM OR APPROVED EQUAL.
- ALL FRAMING CONNECTIONS TO BE MADE WITH SIMPSON FRAMING HARDWARE. COORDINATE SELECTION W/ ENGINEER PRIOR TO CONSTRUCTION.

PLYWOOD SHEATHING

- ALL PLYWOOD SHALL BE C-D GRADE WITH EXTERIOR GLUE MANUFACTURED IN ACCORDANCE WITH THE UNITED STATES PRODUCT STANDARD PS 1-83/ANSI A199.1 "FOR CONSTRUCTION AND INDUSTRIAL PLYWOOD" AND SHALL CONFORM TO UBC STANDARD 23-2 AND SHALL BEAR THE TRADEMARK OF THE APA.
- PLYWOOD SHALL BE LAID WITH END JOINTS STAGGERED.
- BLOCK ALL SHEAR WALL SHEATHING WITH 2X BLOCKING AT ALL EDGES.
- ROOF SHEATHING TO BE UNBLOCKED 5/8" C-D 24/0 PLY WITH 8D AT 6" O.C. PANEL EDGE NAILING AND 12" O.C. FIELD NAILING TYPICAL, U.N.O.
- FLOOR SHEATHING TO BE UNBLOCKED 7/8" T&G C-D PLY WITH 10D AT 4" O.C. PANEL EDGE NAILING AND 12" O.C. FIELD NAILING TYPICAL, U.N.O.
- EXTERIOR WALLS TO BE 7/16" EXPOSURE I, C-D PLY OR OSB SHEATHING WITH 10D AT 6" O.C. PANEL EDGE NAILING AND 12" O.C. FIELD NAILING (OR EQUIVALENT), U.N.O. SEE SHEARWALL CALLOUTS AND SCHEDULE.
- OSB MAY BE SUBSTITUTED FOR PLYWOOD WITH SAME SPAN RATING.

GANG-NAILED TRUSSES

- SHOP DRAWINGS, STAMPED BY A REGISTERED ENGINEER IN THE STATE OF OREGON ARE REQUIRED ON THE PRE-MANUFACTURED GANG-NAIL TRUSSES, SUBMIT 3 SETS OF DRAWINGS AND CALCULATION FOR REVIEW.
- TRUSSES SHALL COMPLY WITH ALL PROVISIONS OF THE DESIGN SPECIFICATION FOR LIGHT METAL PLATE CONNECTED WOOD TRUSSES OF THE TRUSS PLATE INSTITUTE.
- ALL TRUSS AND COMPRESSIVE MEMBER STAY BRACING AND CONNECTIONS SHALL BE MANUFACTURED BY THE TRUSS SUPPLIER.
- MEMBERS SHALL NOT EXCEED A LIVE LOAD DEFLECTION OF L/360 FOR ROOFS, L/600 FOR FLOORS.
- SHOP DRAWINGS SHALL CLEARLY SHOW ERECTION PLAN, ALL STAY BRACING FOR THE TRUSS COMPRESSIVE MEMBERS, AND REQUIRED CONNECTIONS. TRUSS DESIGNS SHALL INCLUDE ALL MEMBER FORCES AND COMBINED STRESSES FOR VERIFICATION. SUBMITTAL SHALL ALSO INCLUDE ALL ICBO APPROVALS.
- THE TRUSS MANUFACTURER SHALL SUBMIT CERTIFICATES FROM AN INDEPENDENT INSPECTION AGENCY VERIFYING THAT ALL TRUSSES DELIVERED TO THE PROJECT CONFORM TO THE APPROVED SHOP DRAWINGS. COST OF ALL INSPECTIONS SHALL BE THE RESPONSIBILITY OF THE SUPPLIER.
- CONNECT ALL TRUSSES TO SUPPORTING MEMBER WITH (1) SIMPSON H1 CLIP AND (2) 10D TOE NAILS.
- SEE PLAN NOTES FOR REQUIRED DRAG FORCES AND CONNECTION REQUIREMENTS TO BE INCLUDED IN THE TRUSS DESIGN.
- CIVIL WEST ENGINEERING SERVICE, INC. IS NOT RESPONSIBLE FOR ANY INTERIOR FINISH DAMAGE (NAIL HEAD PROTRUSIONS, JOINT SEPARATIONS, DRYWALL CRACKING, ETC.) DUE TO TRUSS MOVEMENT AT INTERIOR NON-LOAD BEARING WALLS AND PARTITIONS THAT FALLS UNDER THE PRE-MANUFACTURED TRUSS PACKAGE.

TIMBER FASTENERS

- ALL TIMBER MATERIAL SHALL BE FASTENED PER OSSC TABLE 2304.9.1 "FASTENING SCHEDULE" U.N.O.

MASONRY CONSTRUCTION

- CONCRETE MASONRY UNITS SHALL COMPLY WITH ASTM C90 WITH A MINIMUM NET AREA COMPRESSIVE STRENGTH OF 1,900 PSI.
- ASSEMBLIES SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF F'M = 2,000 PSI AS VERIFIED BY PRISM TESTS BEFORE AND DURING CONSTRUCTION.
- CONCRETE MASONRY WALLS SHALL BE REINFORCED AS SHOWN ON THE PLANS AND DETAILS.
- MORTAR SHALL BE TYPE 'M' OR 'S', WITH A MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS OF 1,900 PSI AND SHALL CONFORM TO OSSC SECTION 2103.
- GROUT SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 2,000 PSI AT 28 DAYS AND SHALL CONFORM TO OSSC SECTION 2105.
- ALL MASONRY WALLS SHALL BE CONSTRUCTED WITH MAXIMUM LIFTS OF 4'-0". TOP COURSE SHALL BE A MINIMUM 32".

STRUCTURAL STEEL

- ALL STRUCTURAL STEEL SHALL BE AS FOLLOWS:
 - WIDE FLANGE SHAPES - ASTM A992, GRADE 50
 - CHANNELS, PLATES AND ANGLES (EXCEPT AS NOTED) - ASTM A36
 - HOLLOW STRUCTURAL SECTIONS (TUBES) - ASTM A500, GRADE 'B' (FY = 46 KSI)
- ALL WELDING SHALL CONFORM TO AWS (CURRENT EDITION) SPECIFICATIONS.
- ALL WELDS TO BE E70 ELECTRODE. ALL WELDS TO BE 1/16" SMALLER THAN THINNEST MATERIAL TO BE WELDED U.N.O.
- ALL FABRICATION, ERECTION, IDENTIFICATION AND PAINTING SHALL CONFORM TO AISC SPECIFICATIONS. 'BP-X' REFERENCES COLUMN BASE PLATES, REFER TO DETAILS FOR 'BP-X' REFERENCES AND REQUIREMENTS.



EXPIRATION DATE: 06/30/23



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541-266-8601
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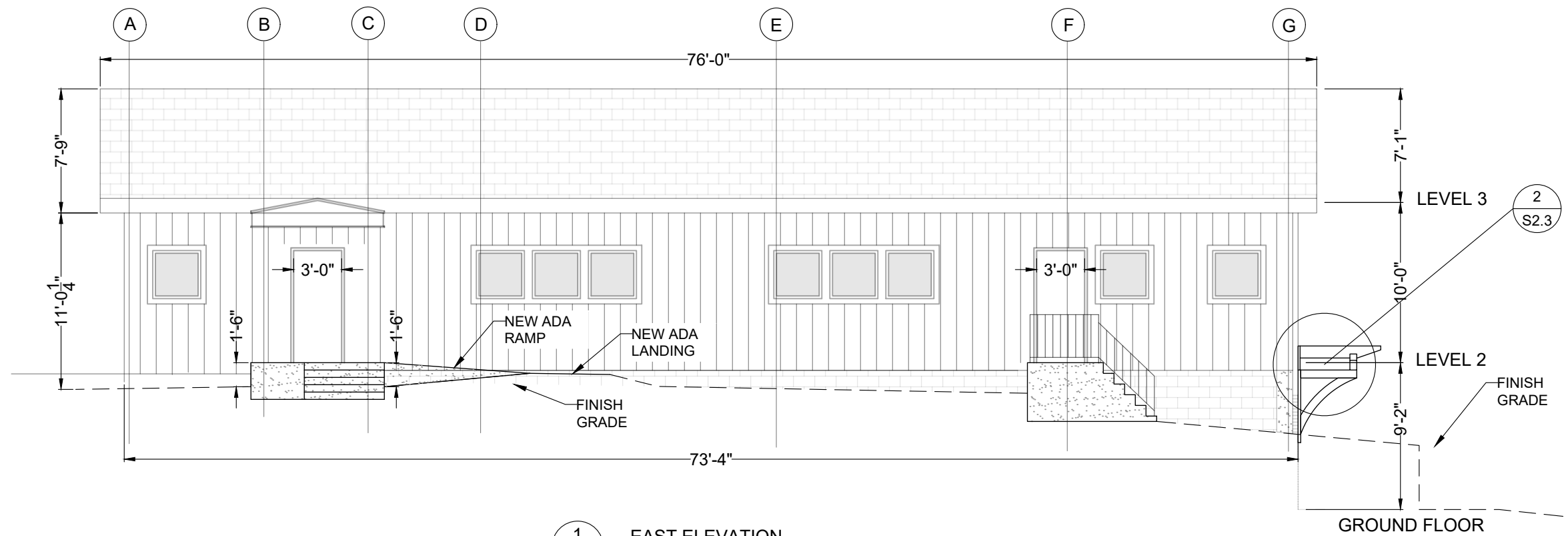
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DRAWN BY: DCK
PROJECT NO: 2204-162

CITY OF ASHLAND JACKSON COUNTY, OREGON	AMERICAN BAND COLLEGE HIGH DENSITY STORAGE	GENERAL NOTES
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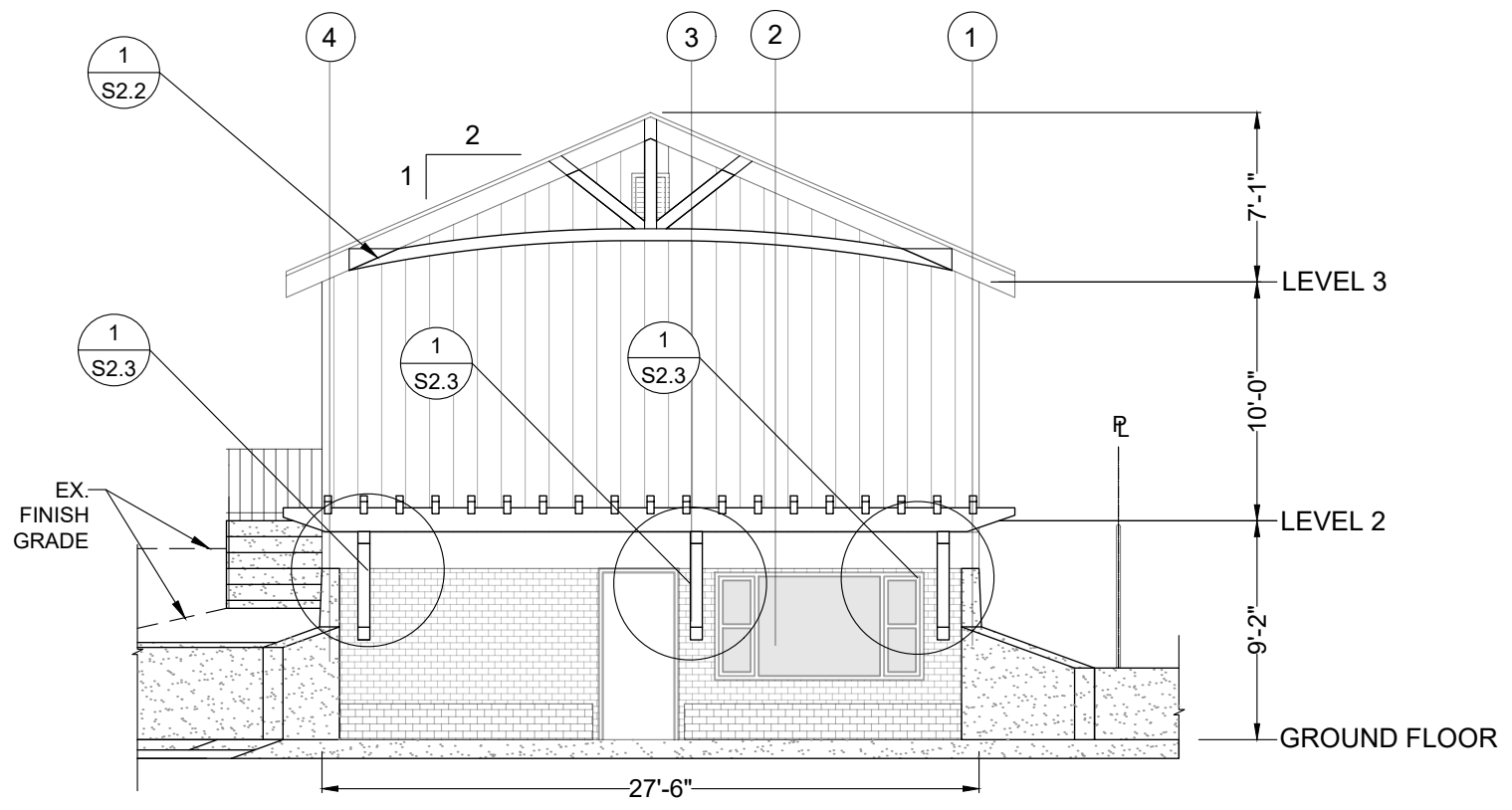
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DATE SHEET NO.	10/07/22

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1 EAST ELEVATION
S1.2 SCALE: 1/8" = 1.0'



1 NORTH ELEVATION
S1.2 SCALE: 1/8" = 1.0'



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

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PROJECT NO: 2204-162

CITY OF ASHLAND
JACKSON COUNTY, OREGON
AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
ELEVATIONS

DATE/SHEET NO. S1.2
10/07/22

PROJECT STRUCTURAL NOTES: (ASHLAND, OREGON)

-  EXISTING WALLS
-  PROPOSED SHEAR WALLS
-  CONCRETE
-  BEAMS, HEADERS, JOIST, RAFTERS
-  FOUNDATION
-  SAW CUT
-  SHEATHING
-  WOOD POST
-  SIMPSON STRONG WALL

HOLDOWN SCHEDULE							
Y	SIMPSON MODEL	ALLOWABLE LOAD (LBS.)	MINIMUM POST THICKNESS	FASTENERS	CONCRETE ANCHORS	MASONRY ANCHORS	REMARKS
1	HDU2-SDS2.5	3075	3 x 3-1/2	(6)SDS 1/4 x 2-1/2 SCREWS	SSTB16	N/A	REFER TO SIMPSON CATALOG FOR INSTALLATION REQUIREMENTS
2	HDU4-SDS2.5	4565	3-1/2 x 3-1/2	(10)SDS 1/4 x 2-1/2 SCREWS	SSTB16	N/A	REFER TO SIMPSON CATALOG FOR INSTALLATION REQUIREMENTS

1. NAILS ARE TO COMMON WIRE NAILS U.N.O.
2. HARDWARE IS TO BE SIMPSON U.N.O.
3. HOLDOWN HARDWARE CAN BE EXTENDED WITH A307 THIRD ROD AND COUPLER.
4. ALIGN ALL HOLDOWNS FOR THE FULL HEIGHT OF STRUCTURE.
5. ALL HARDWARE TO BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS.
6. HOLDOWN ANCHOR BOLTS ARE IN ADDITION TO TYPICAL SILL PLATE ANCHOR BOLTS.
7. EXTEND THREADED ROD TO WITHIN 3" CLEAR OF BOTTOM OF FOOTING.

SHEARWALL SCHEDULE													
X	SHEATHING	SHEAR (PLF)	NAIL SIZE	EDGE NAIL (O.C.)	FIELD NAIL (O.C.)	MAX STUD SPACING	PANEL EDGE STUD DIMENSION	BOTTOM PLATE DIMENSION	MUDSILL DIMENSION	BOT. PLATE FASTENING (O.C.)	TOP PLATE A35 (O.C.)	5/8" x 10" A.B. (O.C.)	REMARKS
A	19/32" APA RATED, ONE SIDE	230	6d	6"	12"	16"	2x	2x	2x	16d @ 6"	2'-0"	1'-0"	STAGGER EDGE NAILING
B	5/8" GYPSUM, TWO SIDE	500	6d	*	12"	16"	2x	2x	2x	16d @ 6"	2'-0"	1'-0"	STAGGER EDGE NAILING

1. **AS AN ALTERNATIVE FOR TOP PLATE TIES, SIMPSON LTP5 MAYBE USED IN LIEU OF A35.**
2. ALL PLYWOOD TO BE APA RATED STRUCTURAL 1 EXTERIOR SHEATHING.
3. ALL NAILS TO BE COMMON OR GALVANIZED BOX TYPE.
4. ATTACH BLOCKING TO SHEARWALL AS INDICATED IN TABLE ABOVE
5. ALL WALL SHEATHING TO EXTEND FULL HEIGHT OF WALL, TOP PLATE TO BOTTOM PLATE.
6. ALL SHEARWALLS AND HOLDOWNS MUST HAVE CONTINUOUS LOAD PATH TO FOUNDATION.
7. USE 3" x 3" x 1/4" PLATE WASHER TYPICAL AT ALL ANCHOR BOLTS.
8. ALL SHEAR WALLS TO BE FULLY BLOCKED U.N.O BLOCKING TO MATCH REQUIREMENTS FOR PANEL EDGE STUDS.
9. #9 AND #10 SD SCREWS MAYBE BE USED IN LIEU OF 10d AND 16d NAILS, RESPECTIVELY.



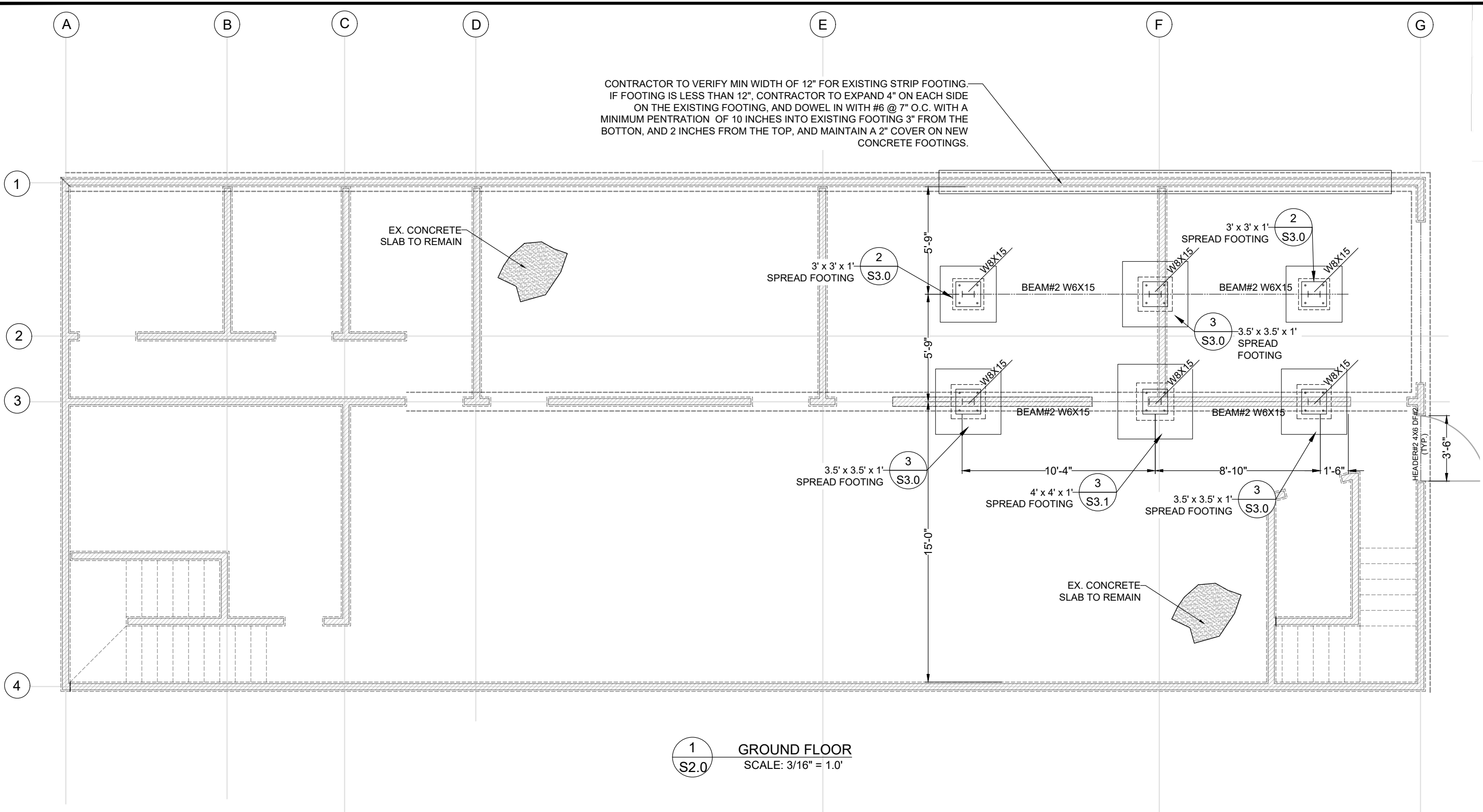
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PROJECT NO: 2204-162											

CITY OF ASHLAND
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AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
GENERAL NOTES

DATE/SHEET NO.
S1.3
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CONTRACTOR TO VERIFY MIN WIDTH OF 12" FOR EXISTING STRIP FOOTING. IF FOOTING IS LESS THAN 12", CONTRACTOR TO EXPAND 4" ON EACH SIDE ON THE EXISTING FOOTING, AND DOWEL IN WITH #6 @ 7" O.C. WITH A MINIMUM PENETRATION OF 10 INCHES INTO EXISTING FOOTING 3" FROM THE BOTTOM, AND 2 INCHES FROM THE TOP, AND MAINTAIN A 2" COVER ON NEW CONCRETE FOOTINGS.

EX. CONCRETE SLAB TO REMAIN

EX. CONCRETE SLAB TO REMAIN

1
S2.0
GROUND FLOOR
SCALE: 3/16" = 1.0'

- FOUNDATIONS AND FRAMING NOTES:**
1. (V) INDICATES HOLDDOWN TYPE AND LOCATION AT END OF SHEARWALL
 2. BOTTOM OF FOOTINGS TO BE PLACE BELOW FROST DETPH
 3. ALL FOOTINGS ARE TO BE CENTERED UNDER COLUMNS U.N.O.
 4. IF HIGHLY EXPANSIVE MATERIAL (CLAY/ADOBE) IS ENCOUNTERED DURING FOOTING OR SLAB EXCAVATION, THE MATERIAL IS TO BE REMOVED AND REPLACED WITH IMPORTED GRANULAR STRUCTURAL FILL. OVER EXCAVATION DEPTH SHALL BE DETERMINED BY PROJECT ENGINEER ONSITE.
 5. IF EXISTING FOUNDATION ARE BEARING ON A SECTION OF GRANULAR, NEW FOUNDATIONS SHALL MATCH THAT OF THE EXISTING GRANULAR SECTION.
 6. ALL HEADERS ARE BOX HEADERS PER R602.7.3 U.N.O.
 7. ALL NEW FOOTINGS ARE TO BE FOUNDED AT ABOUT THE SAME ELEVATION.
 8. NEW FOOTINGS SHALL BE FOUNDED ON THE SAME MATERIAL.

REGISTERED PROFESSIONAL
ENGINEER
93841PE
OREGON
JULY 10, 2018
DAVID C. KUNG
EXPIRATION DATE: 06/30/23

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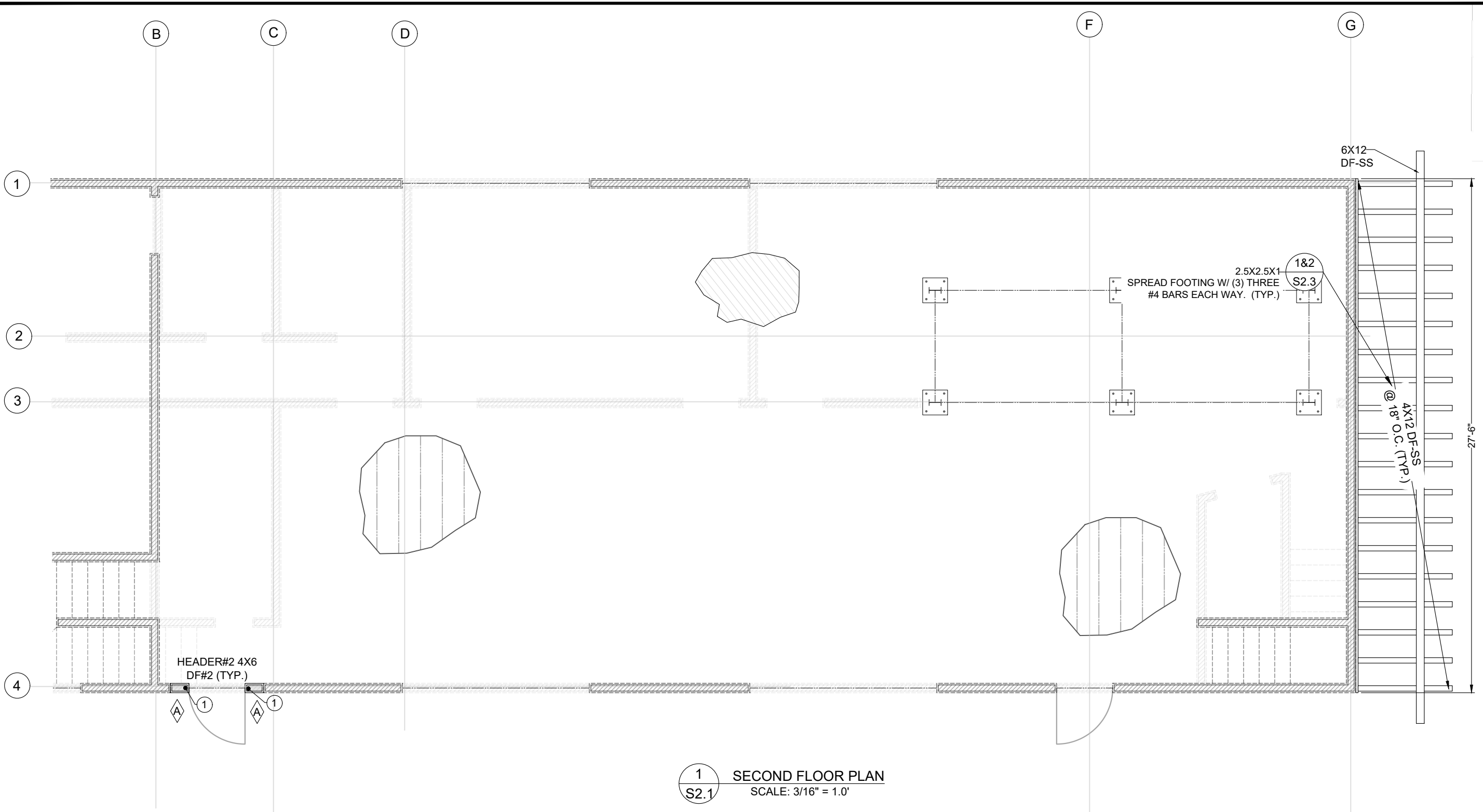
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CITY OF ASHLAND
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AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
GROUND FLOOR AND FOUNDATION
PLANS

DATE SHEET NO.
S2.0
10/07/22

DATE: 9/30/22 FILE: O:\CW_Proj\2024\09\2024-09-26-2022.dwg - Structural Plan 9-26-2022.dwg, 9/30/2022 9:56:17 AM, A1



1 SECOND FLOOR PLAN
S2.1 SCALE: 3/16" = 1.0'

FOUNDATIONS AND FRAMING NOTES:

1. (V) INDICATES HOLDDOWN TYPE AND LOCATION AT END OF SHEARWALL
2. BOTTOM OF FOOTINGS TO BE PLACE BELOW FROST DETPH
3. ALL FOOTINGS ARE TO BE CENTERED UNDER COLUMNS U.N.O.
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5. IF EXISTING FOUNDATION ARE BEARING ON A SECTION OF GRANULAR, NEW FOUNDATIONS SHALL MATCH THAT OF THE EXISTING GRANULAR SECTION.
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8. NEW FOOTINGS SHALL BE FOUNDED ON THE SAME MATERIAL.



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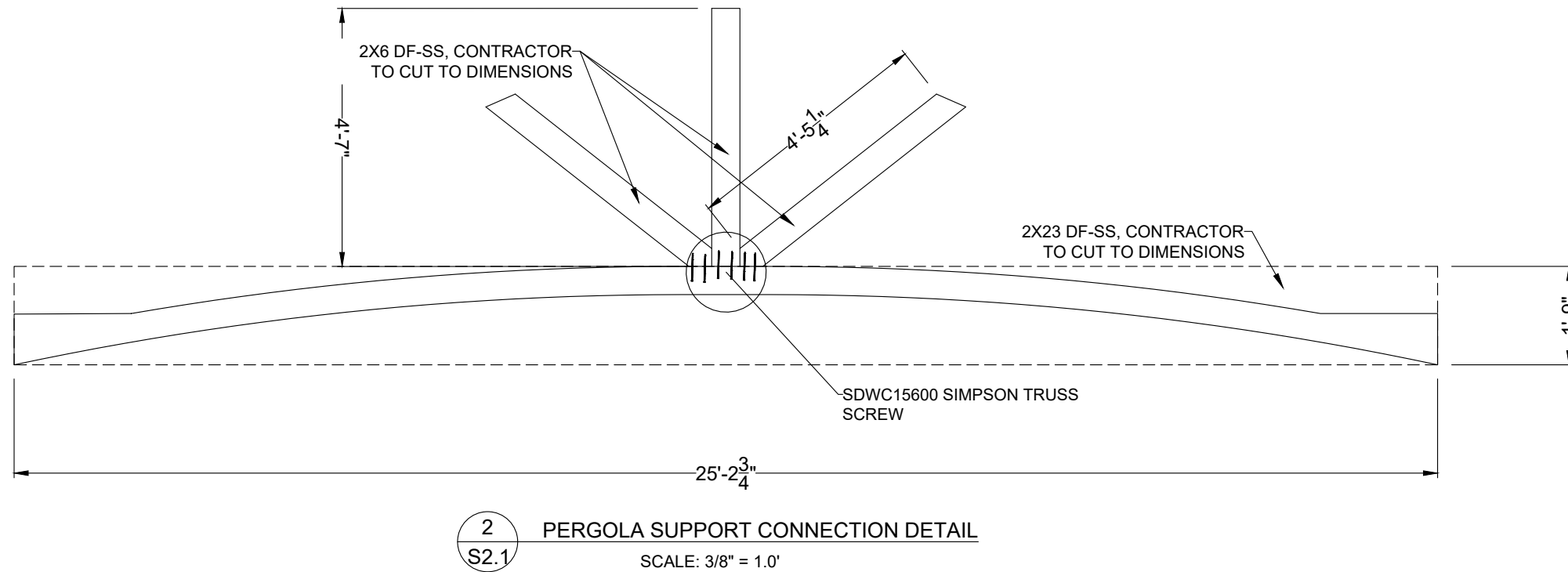
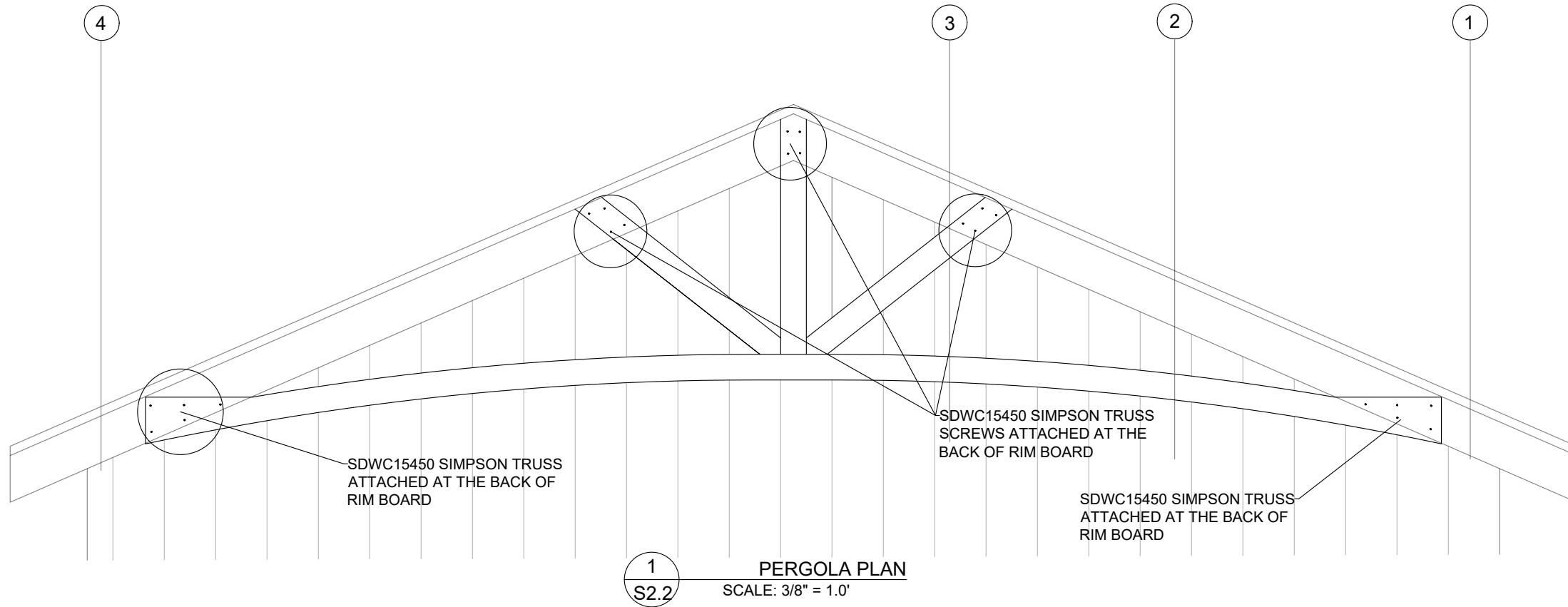
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HIGH DENSITY STORAGE
2ND FLOOR IMPROVEMENT PLAN

DATE/SHEET NO. S2.1
10/07/22

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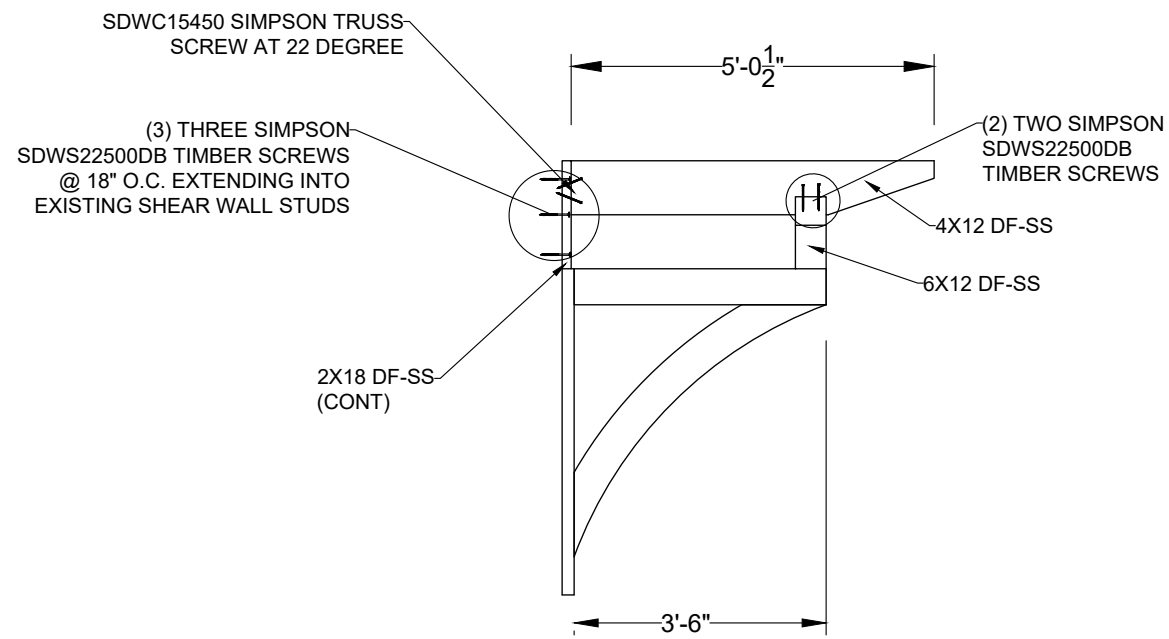
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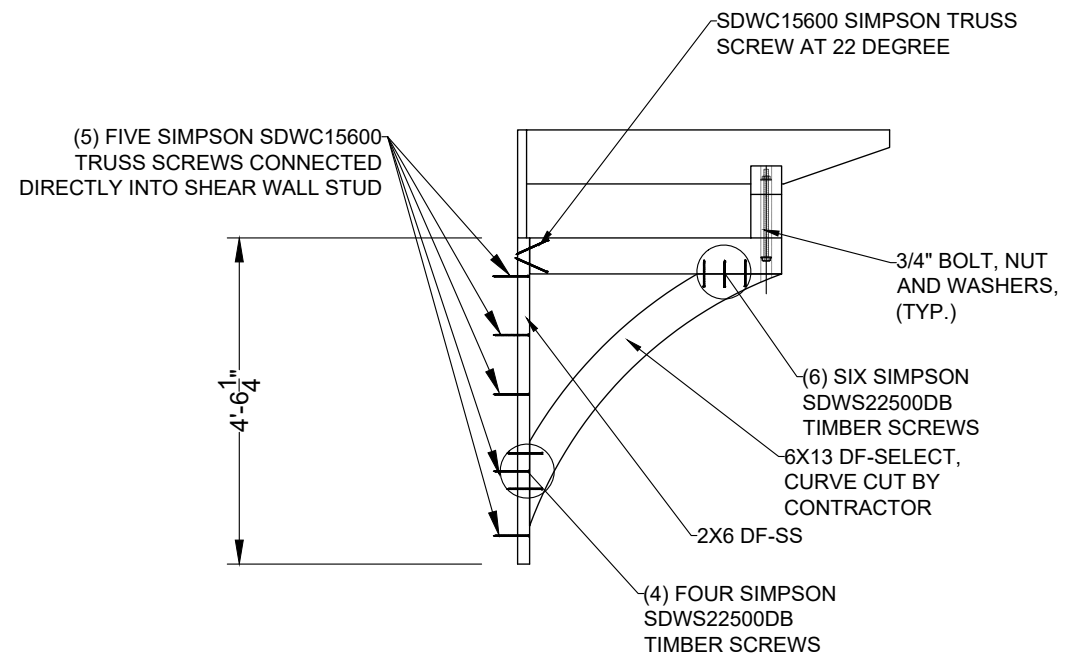
CITY OF ASHLAND
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AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
PERGOLA PLAN

DATE/SHEET NO. S2.2
10/07/22

DATE: 9/30/22 FILE: O:\CW_Projects\2022\09\09\2022\09\09\2022.dwg, 9/30/2022, 9:56:45 AM, AI



1 PERGOLA JOIST SYSTEM CONNECTION
 S2.3 SCALE: 3/8" = 1.0'



2 PERGOLA SUPPORT CONNECTION DETAIL
 S2.3 SCALE: 3/8" = 1.0'



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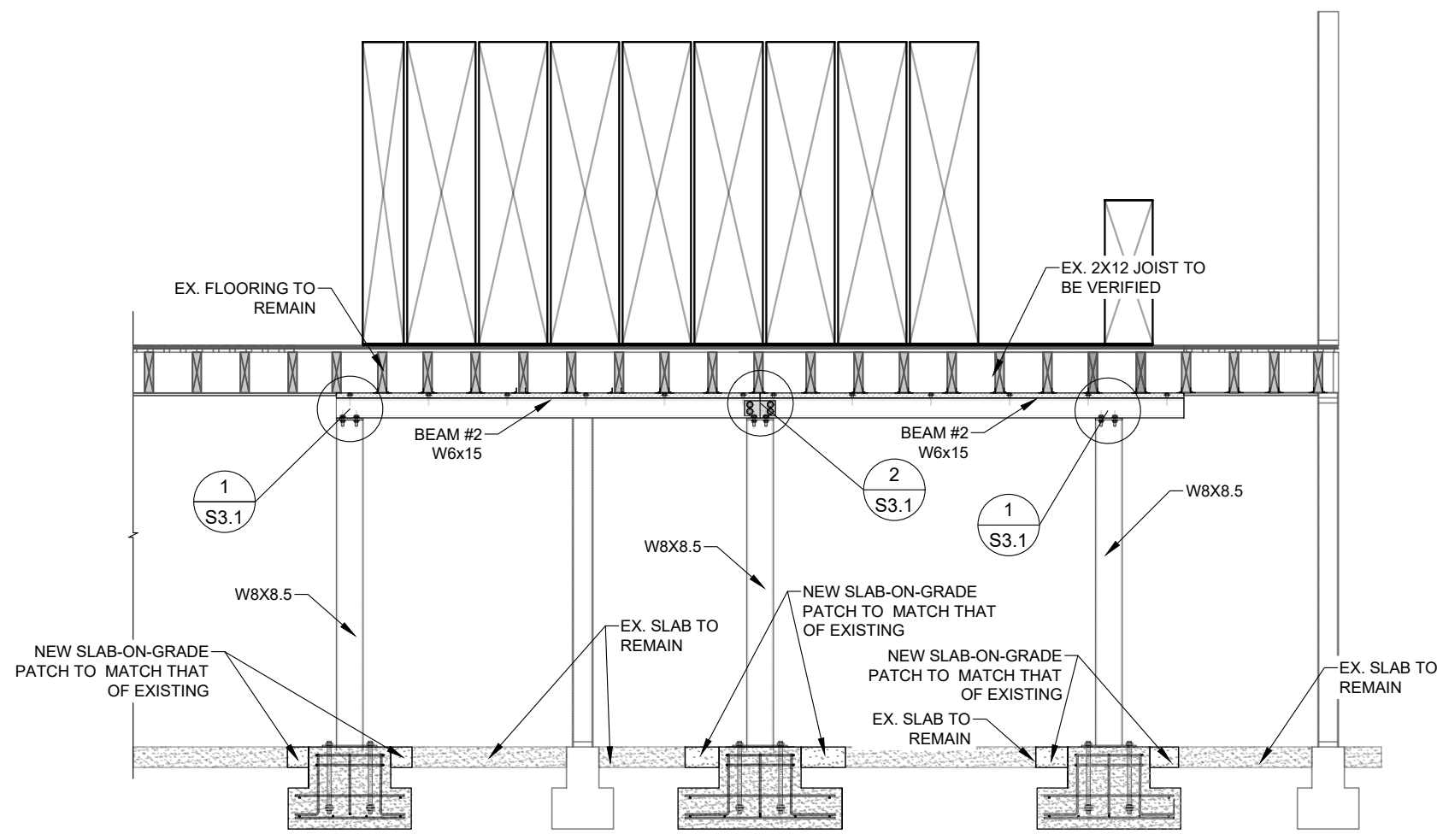
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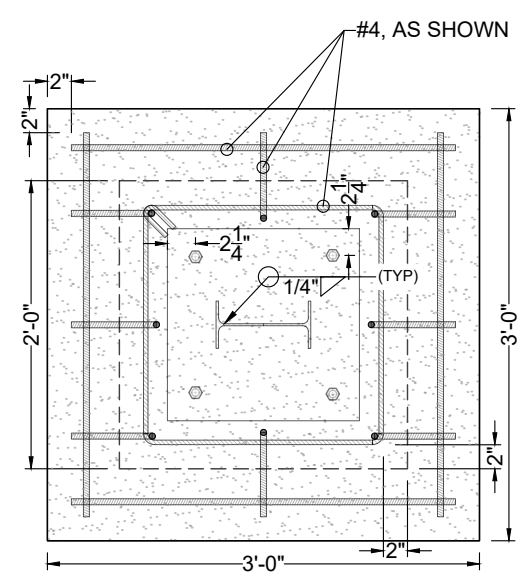
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 HIGH DENSITY STORAGE
 PERGOLA CONNECTION DETAIL

DATE SHEET NO.
S2.3
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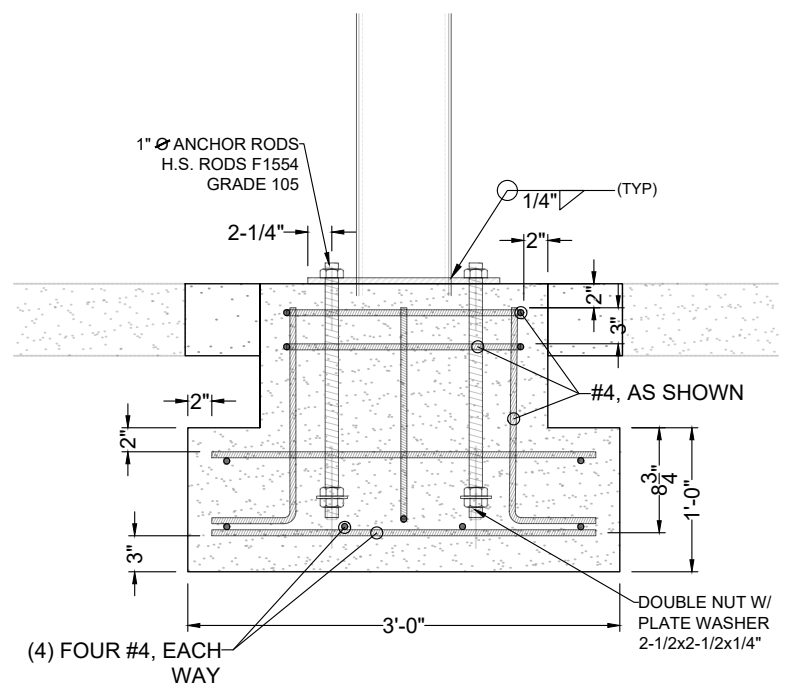
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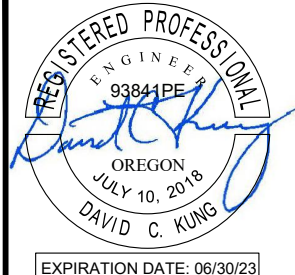
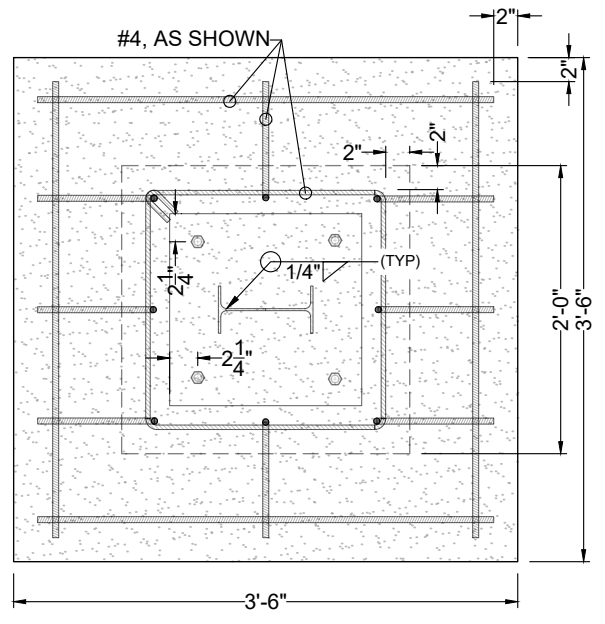
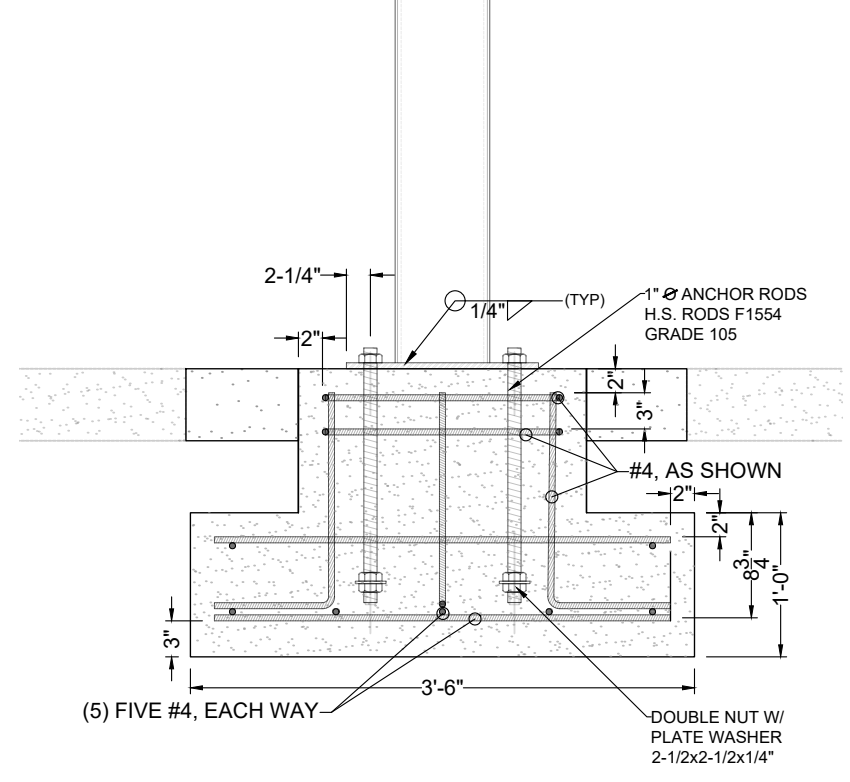
1
SECTIONAL DETAIL
SCALE: 1" = 1.0'



2
36" SPREAD FOOTING DETAIL
SCALE: 3/4" = 1.0'



3
42" SPREAD FOOTING DETAIL
SCALE: 3/4" = 1.0'



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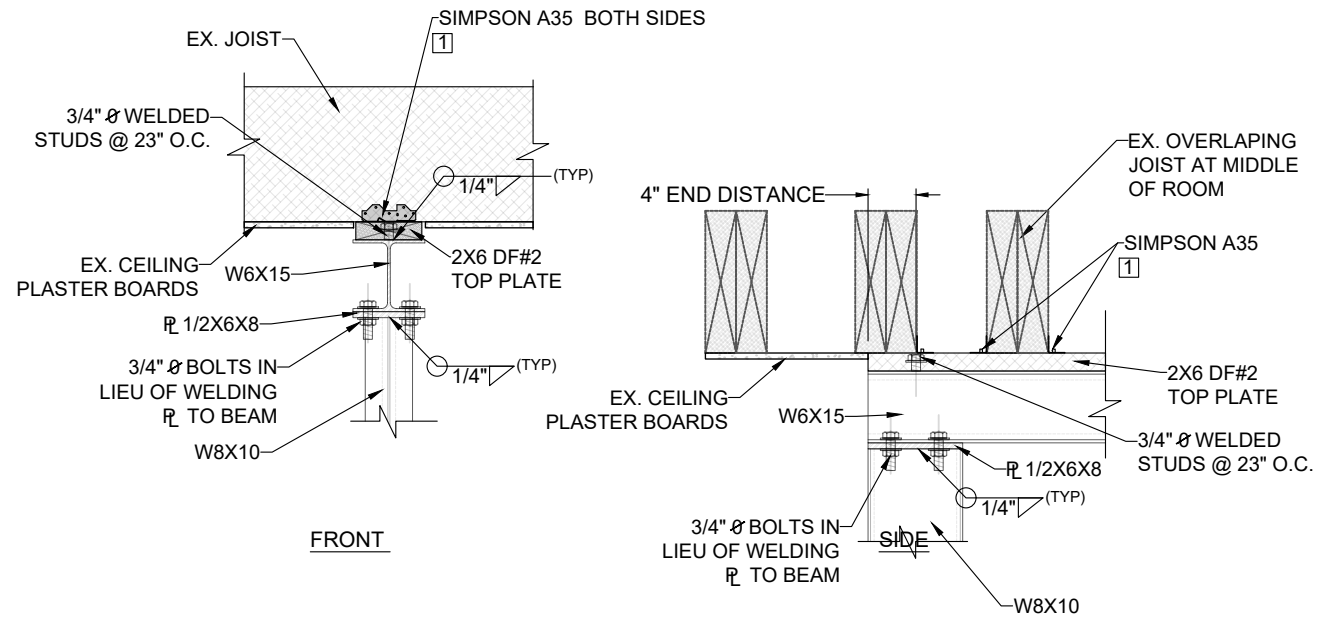
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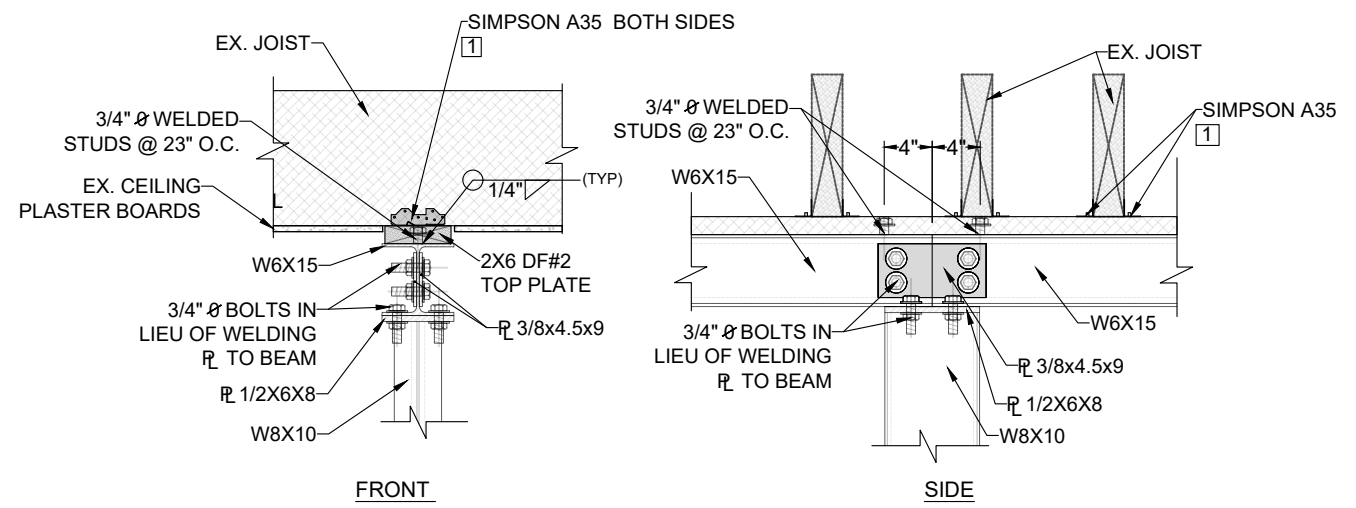
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HIGH DENSITY STORAGE
SECTIONAL AND DETAILS

DATE/SHEET NO.
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10/07/22

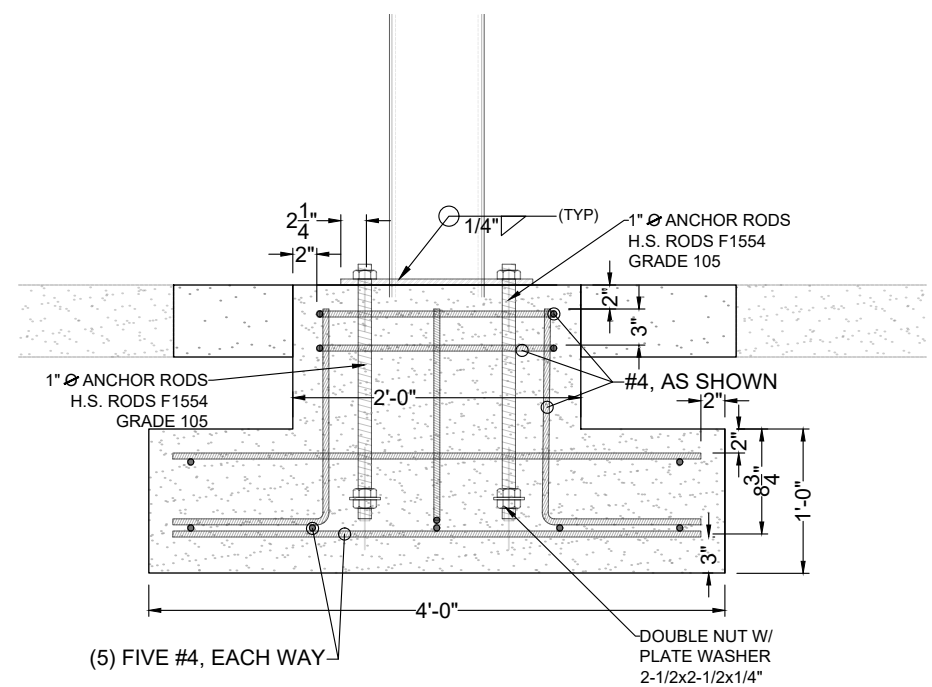
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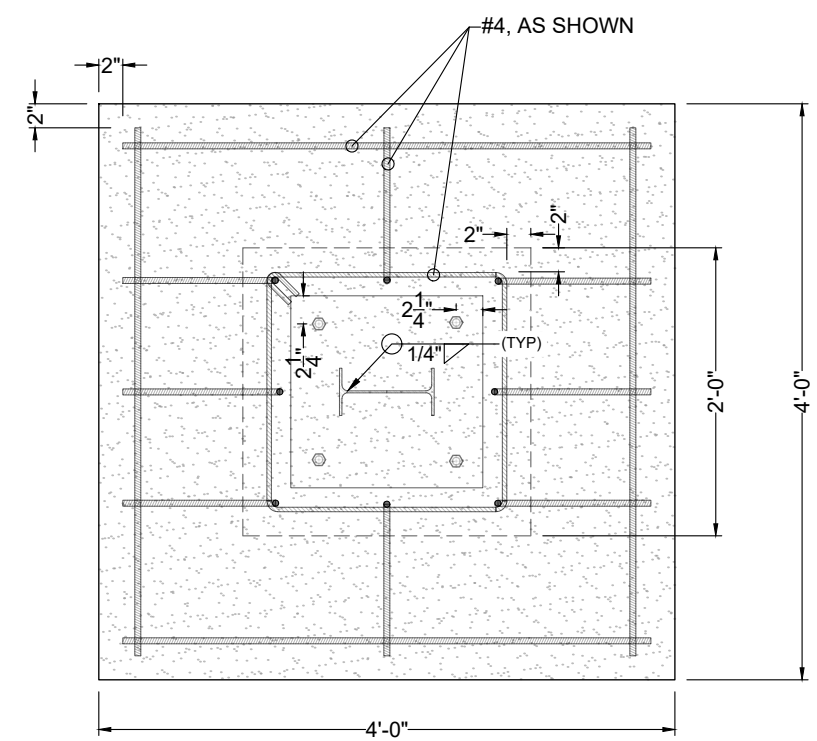
1 END CONNECTION DETAIL
S3.1 SCALE: 3/4" = 1.0'



2 MID SPAN CONNECTION DETAIL
S3.1 SCALE: 3/4" = 1.0'



3 48" SPREAD FOOTING DETAIL
S3.1 SCALE: 3/4" = 1.0'



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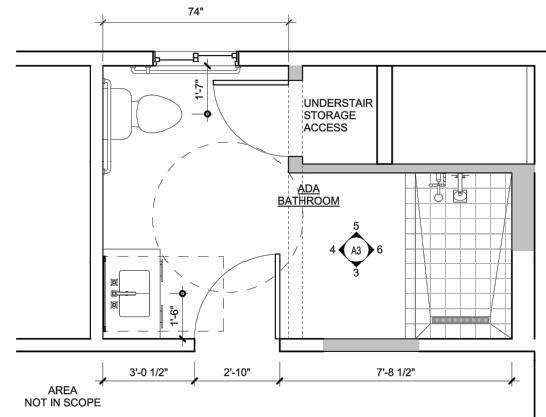
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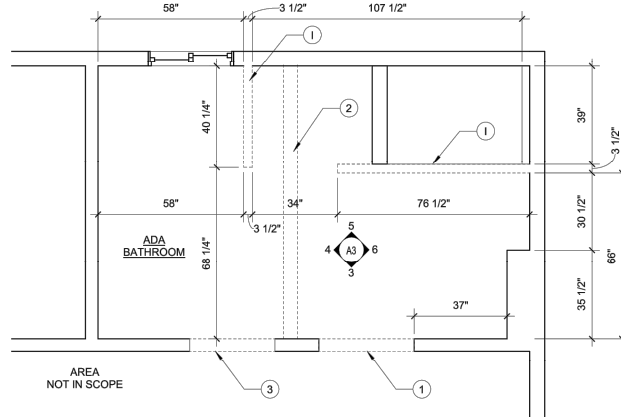
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AMERICAN BAND COLLEGE
HIGH DENSITY STORAGE
CONNECTION AND FOOTING DETAILS

DATE/SHEET NO. **S3.1**
10/07/22

DATE: 9/14/22 FILE: O:\CW_Projects\2204_Misc_Private_Engineering_Work\2204-213 - American Band College_WBC - 748 Siskiyou Conditional Use Permit Drawings\04 Final Design\Drawings\Exhibits\2204-213 AB College WBC-Siskiyou Conditional Use Permit-Exhibit 4-bathroom.dwg



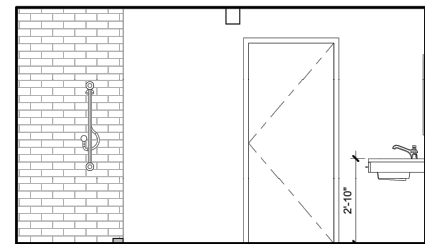
PROPOSED FLOOR PLAN
3/8" = 1'-0" ②



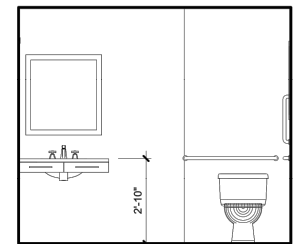
EXISTING FLOOR PLAN
3/8" = 1'-0" ①

KEYNOTES	
1.	DEMO WALL
2.	BEAM ABOVE
3.	DEMO WALL FOR A NEW DOOR
4.	DEMO PARTIAL WALL

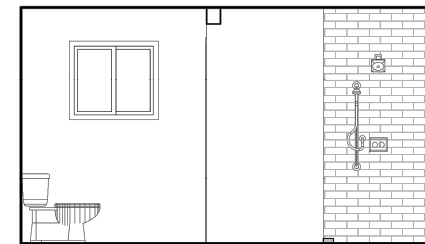
WALL LEGEND	
	EXISTING
	PROPOSED
	TO BE DEMOLISHED



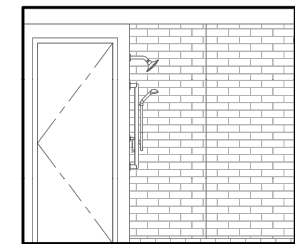
EAST INTERIOR ELEVATION
3/8" = 1'-0" ③



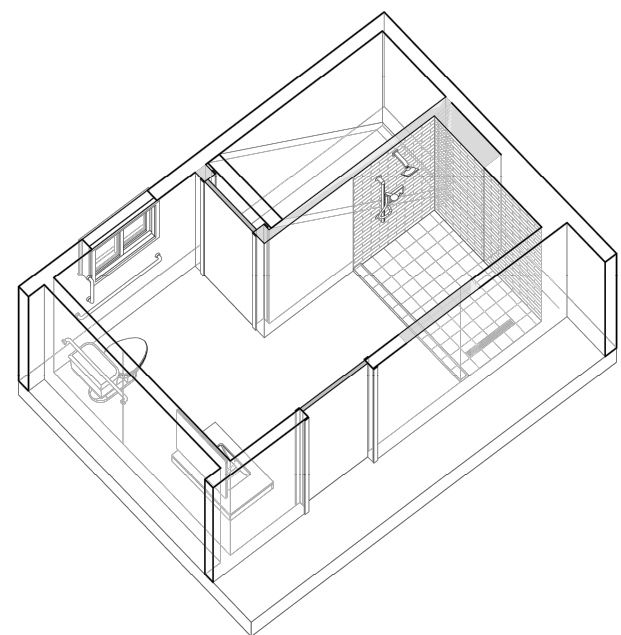
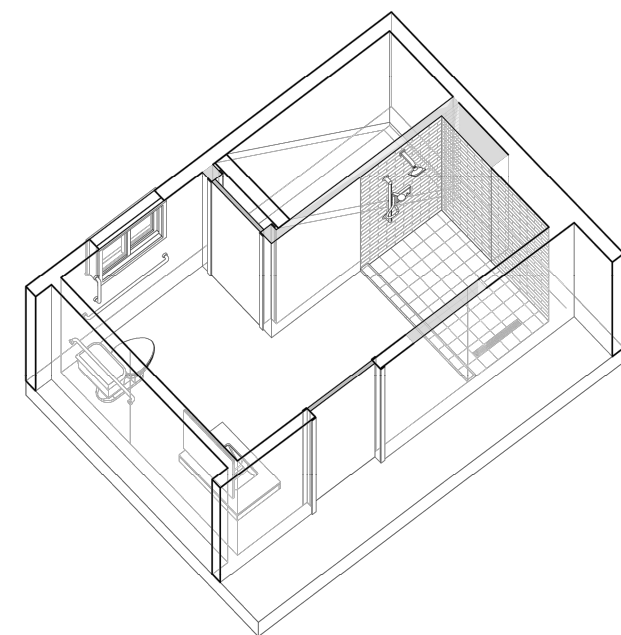
SOUTH INTERIOR ELEVATION
3/8" = 1'-0" ④



WEST INTERIOR ELEVATION
3/8" = 1'-0" ⑤



NORTH INTERIOR ELEVATION
3/8" = 1'-0" ⑥



BIRDS EYE VIEW ⑦

REVISION ID:	DATE:

PROJECT NO. 005
DRAWN:
DATE:

ADA BATHROOM
ASHLAND OR

A3

SCHEMATIC DESIGN

PRELIMINARY

Civil West
Engineering Services, Inc.

830 O'HARE PARKWAY, SUITE 102
MEDFORD, OR 97504

541-266-8601
www.civilwest.com

REV.	DATE	DESCRIPTION	BY

DESIGNED BY: DWT	DRAWN BY: DWT	CHECKED BY: MB
PROJECT NO: 2204-213		

CITY OF ASHLAND	ADA BATHROOM
JACKSON COUNTY, OREGON	
AMERICAN BAND COLLEGE SISKIYOU CONDITIONAL USE PERMIT	

SHEET NO. **EXH-4**

125 E. Main

Historically Compatible Commercial, Interior Renovation/Exterior Restoration

Paddington Station

125 East Main Street

Owner: Donald & Pamela Hammond

Architect: Robert Saladoff

Contractor: Robert Davis Construction



Paddington Station, 125 East Main Street

This historic mercantile building built in 1903 has been an integral part of the commercial downtown Ashland landscape through many generations. The owners undertook this comprehensive renovation and restoration project to not only modernize the out-of-date and inefficient interior retail spaces, but to restore the character of the original façade on East Main Street, while making the building safer for its customers.

The major aesthetic aspect of this federally certified historic project included the restoration of the front façade, bringing back the original intricate detail at the cornice, the maintenance of the metal, wood, and glass storefront, and the reintroduction of a corrugated metal awning. The interior renovation involved several major architectural elements including the refinishing of the original Douglas fir floors, the restoration of the mezzanine and stairs, the repair of the original metal ceiling tiles, and the renovation of the basement space for additional retail space.

The majority of the expenditures and attention, however, was attributed to the seismic upgrade of the building, the complete renovation of the building's electrical, mechanical, and lighting systems, and the significant upgrade to the buildings accessibility. All of these major building systems were completed with little or no compromise to the individual architectural elements and the historic building as a whole.



Award Ceremony – (L to R) Historic Commission Chairman Dale Shostrom; City Councilor, Kate Jackson; Donald & Pam Hammond;; Rob Saladof

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section Number: 7 Page: 19

Ashland Downtown Historic District, Ashland, OR

ID# 13.0

SUNDIN BUILDING

116 C STREET

Architect: Patterson, Philip C.

Modern Period: Commercial

1979

391E09BA 11200

Builder: Taylor, Bob

Non-Compatible, Non-Historic, Non-Contributing

This two-story masonry structure was constructed in 1979 for developer Judith A. Sundin. The building has an open ground floor corridor, creating a mall-type arrangement between C Street and Will Dodge Way, the alley that bisects this block. Small retail and professional spaces and a coffee shop are the current occupants. While somewhat compatible volumetrically and in its zero setback, the Sundin Building does not contribute to the historic character of the downtown.

ID# 14.0 *Survey #265*

DODGE J.P. BUILDING

125 MAIN ST E

Architect: Clark Frank Chamberlain

Other: Vernacular [Storefront Commercial]

1904

391E09BA 11201

Primary Contributing

Construction of J. P. Dodge's new building on East Main began in 1904, the first of a group of three new business blocks built adjoining the Opera House between 1904 and 1905.

"Forty feet of the S. M. Byers frontage of 120 feet on Main street, adjoining the opera house, has been purchased by J. P. Dodge...[who] contemplates the erection of a brick building on the property at an early date." (*Tidings*, 13-Jun-1904, 3:3)

Construction of the building was largely completed by August at a cost of \$5,500. John Page Dodge arrived in Ashland in 1883 and operated a long successful furniture company in partnership with two sons, Louis and Will.¹³ Active in civic affairs, J. P. Dodge served on the Ashland City Council in 1892-93 and was elected Mayor in 1896. Dodge also is connected to Site #16, used as a funeral home related to the furniture company. John Page Dodge died in 1928. His sons continued the business after his death.

According to historic postcard images, the original exterior of the J. P. Dodge Building was of exposed red brick with a series of corbelled brick arch details lining the multi-part brick parapet. In 1947, as an element of the modernization of this area of East Main street, the Dodge building was stucco-clad, obscuring some of the original detailing.

Complete remodeling of the front of the J. P. Dodge and Sons...[building] is underway. When complete the store fronts will have been modernized and finished in white stucco, according to Robert Dodge. (*Tidings*, 16-Jul-1947, 1:7)

¹³ In the early 1990s the city-owned alley that runs behind the J. P. Dodge Building and the J. P. Dodge and Sons Funeral Home (Site 16.0) was named "Will Dodge Way" in honor of this pioneer Ashland businessman.

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Section Number: 7 Page: 20

Ashland Downtown Historic District, Ashland, OR

Today, the stucco-covered arched details of the simple cornice remain visible and the first floor storefront, below the transom band, substantially reflects the 1947 remodeled appearance of the Dodge Building although first floor elements of the original 1904 design do remain. The J. P. Dodge retains sufficient integrity to relate its development history during the period of significance.

ID# 15.0 Survey #266

GANIARD OPERA HSE (portion)

105 MAIN ST E

Other: Vernacular [Storefront Commercial]

1889/1912

391E09BA 11300

Historic/Non-Contributing in Current Condition

The Ganiard Opera House, erected in 1889 by Oscar Ganiard, an important local land owner and member of a pioneer family, was originally a substantial three-story brick building. "The Ganiard Block is an old landmark, being one of the first brick structures in Ashland." (*Tidings*, 8-Aug-1912, 1:3-4) the building had cut sandstone columns at street level, framing both the corner entry and the primary entrance on Main Street, centered under a 2½ story arched stairwell opening that led to large theater and the office spaces of the upper floors. In 1912 the building was seriously damaged in a spectacular fire

Very little was saved from the burning building. The upper floor except for a few books and instruments of Dr. Songer, was a total loss. Harrison Brothers, who operated a moving picture show in the opera house, estimate their loss at \$1200...W. Stephenson, owner of the building...says he will not rebuild, but will sell the property as it stands...(*Tidings*, 8-August-1912, 1:3-4)

Following the fire, the severely damaged building was immediately condemned by the city's Fire and Water Committee. "It is understood the report will recommend that the upper two stories be torn down, they being considered unsafe by the committee. It will be remembered that this block was condemned some time ago but the walls were bolstered up and the building was allowed to be used." (*Tidings*, 8-August-1912, 1:2) By the end of the month, the damaged building was sold to E. E. Estes for \$13,500, who announced a plan to rebuild. "Mr. Estes is today engaged in tearing down the chimneys and other menacing parts of the old structure [in preparation] to the erection on the lot of a modern two-story structure for stores and offices." (*Tidings*, 29-aug-1:1) For unknown reasons, Estes' plans failed to materialize and eventually the remaining first floor elements of the Ganiard Opera House were rebuilt and used for various retail and professional spaces. In the late 1940s the site was occupied by the Greyhound bus terminal and an electric shop and the 1964 Polk City directory lists Western Auto at the corner-facing space. A series of financial institutions were located here from the 1970s through the early 1990s when the building underwent a renovation that opened up the basement area and created a series of small storefronts along the Pioneer Street elevation.

From: [Kyle Taylor](#)
To: [building](#)
Subject: 125 E Main - Window Glass Replacement
Date: Wednesday, November 16, 2022 1:05:54 PM
Attachments: [image003.png](#)
[125 E Main Narrative.pdf](#)
[Commercial Permit Application Form Updated 11 2022.pdf](#)
[Consent Form.pdf](#)
[W1.pdf](#)
[W2.pdf](#)

[EXTERNAL SENDER]

Good afternoon,

Per my conversation with Brandon, attached is the information I have for a building permit for 125 E. Main. This is just a window replacement for the storefront glass at Paddington Station. It's a little unique that they have fairly old glass that is no longer available, so we are installing a new storefront system provided by Southern Oregon Glass. It is 7" aluminum channel with double pane low E glass. It will be similar in overall look to what is there now, just a new version of the old design. The only plans that we have are from the glass company showing the layout of the glass panels, all trims and other design will be fit to match existing as close as possible.

Let me know what other information you need.



Kyle Taylor

President

Office: 541.690.1617

Mobile: 541.621.8095

Fax: 541.608.0468

kyle@tayloredelements.com

www.tayloredelements.com

New Construction / Remodels / Finish Carpentry / Project Mgmt / Energy Efficiency



Project Narrative

125 Main St, Ashland OR 97520 – Paddington Station

Storefront Glass Replacement

Overview:

Contractor to provide all labor, material and cleanup associated with the replacement of the storefront glass on the frontage of 125 Main St. Work includes sidewalk closure permitting, scaffold installation, curtain walls, mitigation of potential hazards, removal of sidewalk frontage glass (not including entry door or glass above entry door, or display window to the right of the entry door), overnight plywood security installation and new storefront style glass wall using Aluminum partitions. Work is scheduled to begin January 9th, 2023. Critical that all work be done to match existing Historic pattern as possible with new style glass installation.

Structure Features & Site Improvements

- **Site Setup:** Including All permits, and sidewalk traffic plan and closure procedures. Work zone containment plastic walls and security safeguards
- **Demolition and Mitigation:** Lead paint mitigation and removal of existing glass and all nonstructural trims/framing associated
- **Trim:** Paint grade 1x2 trims on outside of new window units to allow for transition to existing structure
- **Windows:** New curtain wall 2 ¼" x 7" metal and 1" IG Low E Units with "Dark Bronze" aluminum partitions
- **Masonry:** Minor mortar repairs as needed (match existing, no changes to existing masonry)

Interior Trim

- **Hardware Finish:** "Dark Bronze" aluminum partitions
- **Window Trim:** 1x1 or 1x2 paint grade trims as needed as transition to fit into existing opening
- **Baseboard:** Remove and reinstall as needed to allow access to existing window trims

Paint Features

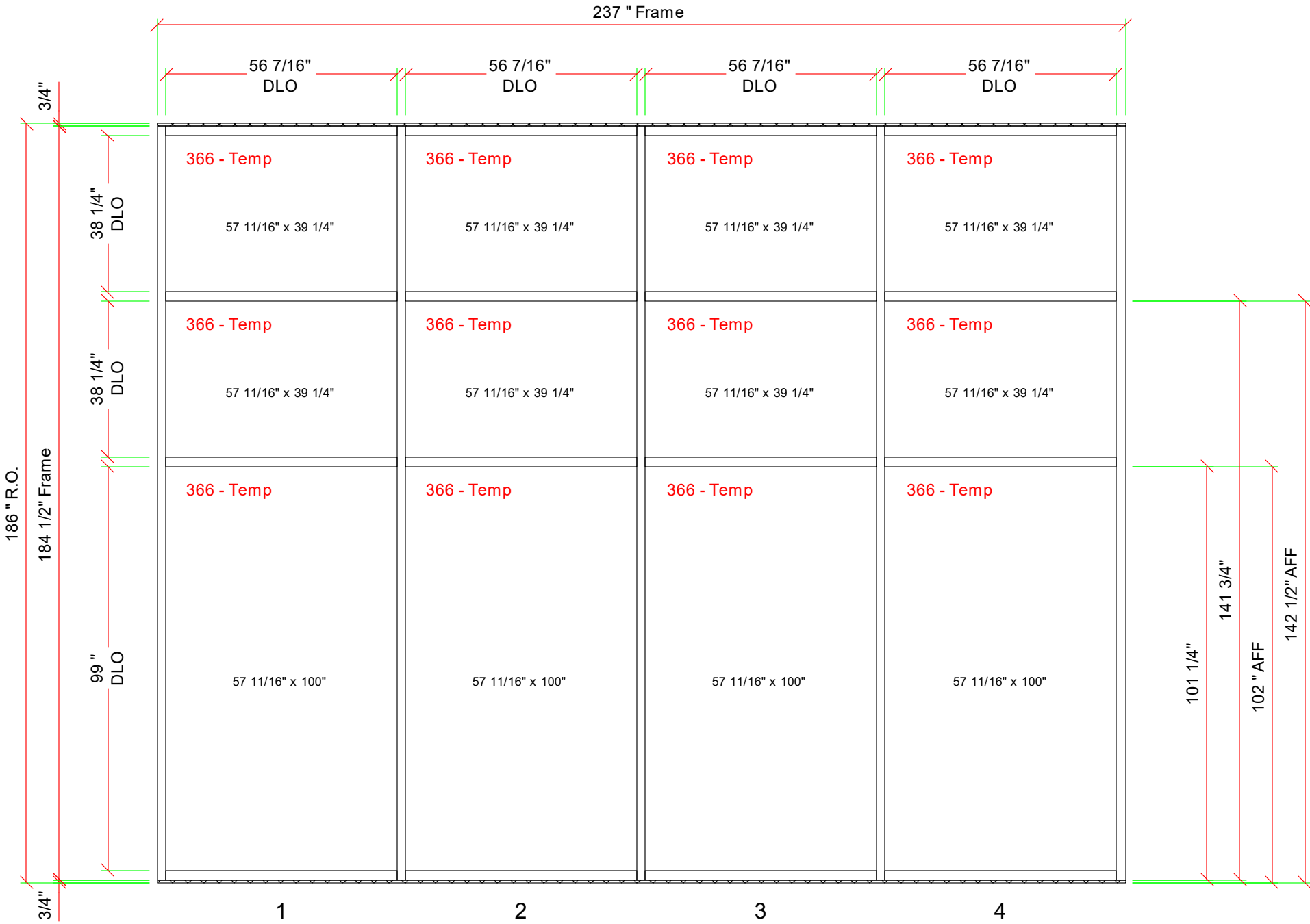
- **Interior Paint:** One color to match existing
- **Exterior Paint:** New trims and existing support posts and beams as needed to accommodate new window units
- **Exterior Color:** One color (matching metal color as close as possible), **Color: TBD**

Kyle Taylor

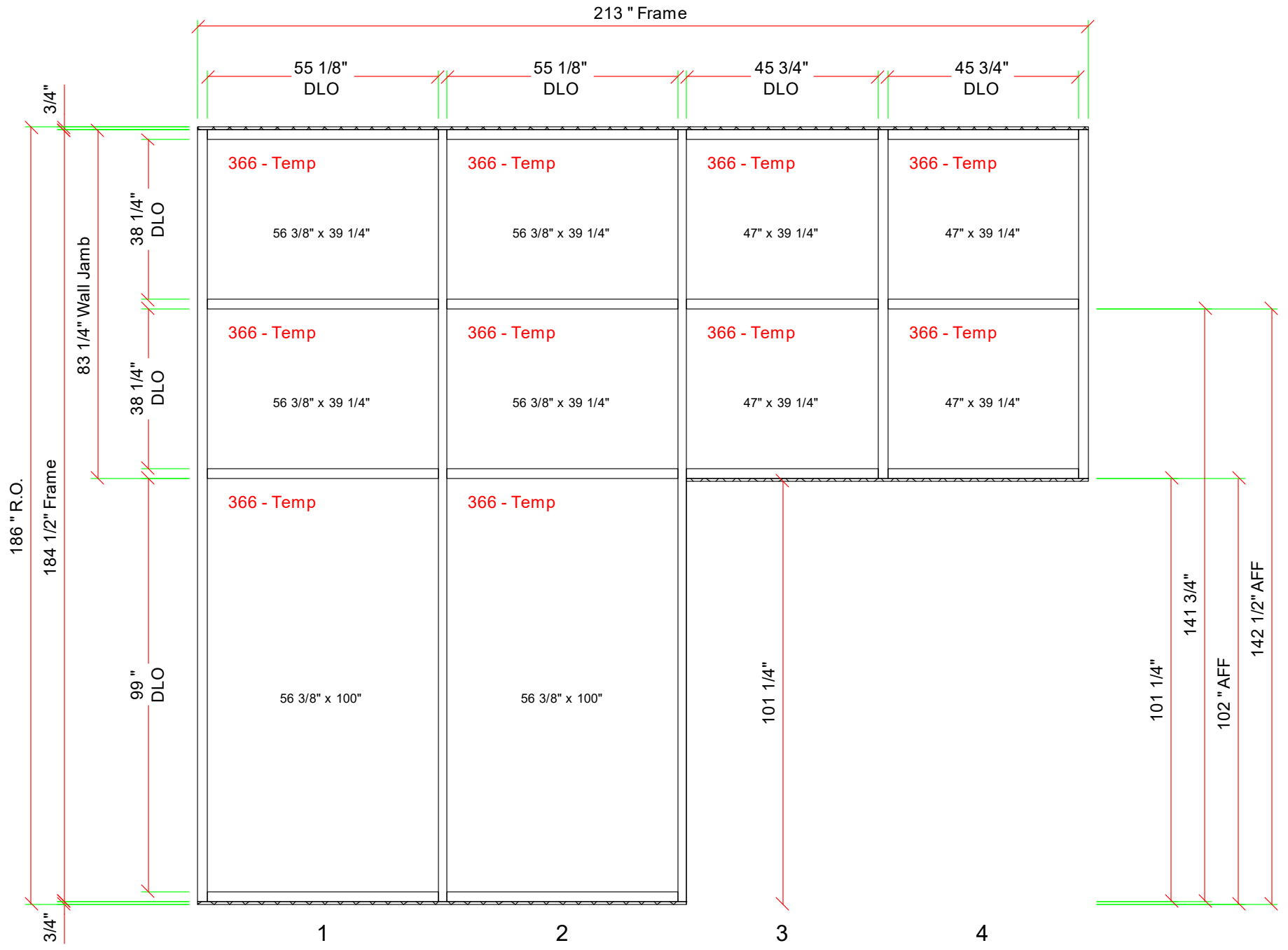
President

541-621-8095

kyle@tayloredelements.com



Paddington - 1 - 001 - W1 (1 Thus)
 Frame: (BRONZE) T500 2-1/4 x7 OPG1900 SSG Vert



Paddington - 2 - 002 - W2 (1 Thus)
 Frame: (BRONZE) T500 2-1/4 x7 OPG1900 SSG Vert

ELKS MURAL PROJECT

**ASHLAND HISTORIC COMMISSION
ELECTRONIC MEETING
Meeting Minutes
April 6, 2022**

Community Development/Engineering Services Building – Electronic Meeting

CALL TO ORDER:

Hovenkamp called the electronic meeting to order at 6:00pm.

Commissioners Present:	Council Liaison:
Shostrom	Shaun Moran
Whitford	Staff Present:
Emery	Brandon Goldman; Planning Manager
Hovenkamp	Regan Trapp; Permit Technician II
Swink	
Von Chamier	
Commissioners Absent:	Skibby

APPROVAL OF MINUTES:

Shostrom/Emery m/s to approve minutes for March 2, 2022. Voice vote. ALL AYES. Motion passed.

PUBLIC FORUM:

Elk's building mural project - Peter Finkle and John Pugh

John Pugh (local muralist) presented his ideas regarding the mural to the Commission. John showed the Commission some of his work that he has done in many places. He likes to create illusions with his art to create the human experience. The proposal encompasses the original Chautauqua Dome, painted blue with Native American vibe and the idea that Ashland represents a spiritual energy. The name of the painting is called "Enchantment" which depicts and tells the story of Ashland. There is no time frame on this project yet because he wants to make sure that everyone is on the same page and approves of the design. He will be working with the Elks to raise money for the entire project.

Commission feedback:

- This is creating interest with the 3D design and allows it to feel more real.
- Loves the whimsical way of the design on one of the most historic buildings in Ashland.
- Work is spectacular and will be a nice addition to downtown. Wonderful to have indigenous representation.
- This type of mural will stand out and looks very provocative and fun.
- Will dominate the whole historic street which could be controversial in a negative way.
- The mural will engage people to ask questions about the community and invite visitors to see what Ashland has to offer.

COUNCIL LIAISON REPORT:

Councilor Moran gave the liaison report. Items discussed were:

- New City Manager Joe Lessard - Making necessary personnel hires and dealing with fiscal issues.
- New Assistant City Manager Sabrina Cotta welcomed.
- Departments within the general fund will have to cut 5-10%.
- Council working with SOU on community survey regarding City services.

PLANNING ACTION REVIEW

Public Art Mural Packet

Thank you for your interest in the process for executing exterior murals on public or private property in within the City of Ashland. The attached documents are intended to assist applicants through the approval and installation process.

- Guidelines and Process for executing/installing an exterior mural in the City of Ashland
- Public Art Mural Application
- Art Agreement between City of Ashland and Property Owner



Public Art Murals Guidelines and Process

The Ashland Municipal Code requires that exterior murals must be approved by the Public Art Commission (PAC) whose role is to ensure that each project aesthetically enhances its location and surroundings. The costs associated with developing and executing/installing a mural are the responsibility of the applicant and/or property owner of the wall where the proposed mural will be installed.

To simplify this document, the word 'mural' refers to artwork that is painted on an exterior wall and other works of art affixed to an exterior wall.

Overview

- The approval process for executing/installing murals on public or private property within the City of Ashland is administered by the City of Ashland staff liaison to the Public Art Commission.
- Applicants without professional mural experience may apply but should partner with a professional muralist.
- Applicant must provide a budget for the project and if the project is approved funding must be in place before work can begin.
- Proposed murals are reviewed by the PAC, by the Historic Commission if required, and if recommended by the PAC, approved by the City Council.
- Murals shall not be considered for installation on building facades with a public entrance in historic districts.
- Murals may be considered for installation on building with a public entrance outside historic districts.
- Murals shall not be proposed for installation on an unpainted façade surface (natural brick, stone) of a historic building.
- All property owners must sign an Art Agreement to be included with the Public Art Mural application agreeing to transfer ownership of the mural to the City pending approval of the proposed mural by the City Council.
- All murals approved through this process become part of the City's public art collection for as long as the Art Agreement remains in effect.

- The number of murals per block may be limited.
- Historically significant murals (including historic advertisements) shall not be painted over, even if faded.
- To the extent practicable, murals shall be applied only to the flat planes of walls.
- Imitative materials including but not limited to asphalt siding, wood textured aluminum, and artificial stone should be avoided on murals within historic districts.
- All applicants are required to meet with the staff liaison at least one month prior to submitting an application. To schedule an appointment, contact Public Arts Commission Staff Liason at 541-488-5305.

Murals on Historic Buildings

- Murals proposed for installation on the exterior of structures listed on the National Register of Historic Places or to a contributing property within a Historic District on the National Register of Historic Places will be forwarded to the Historic Commission for review.
- The Historic Commission will review the proposal using criteria standards stated in the Ashland Municipal Code and provide their comments to the City Council and to the Public Art Commission.

Criteria for Approval of Wall Murals

The mural should be a professionally designed, original work of exceptional quality with consideration of the following criteria:

- Work that is of enduring value for including in the City's public art collection.
- Visual imagery that enhances the aesthetic experience within the City and the character and nature of the site.
- Visual imagery that is appropriate for all audiences (not reflecting partisan politics or containing sexual or religious content or expressing a commercial aspect, etc.).
- Artwork that is appropriately designed for all view points to the mural (by pedestrians, from moving vehicles, seated audiences, etc.).
- Artwork that is appropriately sited for directional exposure to minimize fading of colors.
- Suitability of the wall surface to receive all materials that are to be used to execute the mural including the wall preparation material.
- Work that is appropriate in scale to the building and to the site.
- All installation and technical issues.

Mural Design Application

Applicants (artist, property owner, etc.) intending to execute/install a mural on an exterior wall that is visible from a public-right-of-way and within the boundaries of the City of Ashland must apply for approval through the following process. Applicant shall:

- a. Schedule an appointment and meet with the staff liaison to the PAC for an informational overview of the process and initial review of the proposed project.
- b. Complete and submit a Public Art Mural application.
- c. Submit a signed Art Agreement from the property owner.
- d. Prepare a mural presentation package as described in *Mural Design Presentation and Review*.
- e. Schedule an appointment for PAC review of mural package at a public Commission meeting.
- f. Submit a complete Mural Presentation package to staff 10 days prior to PAC review. Only packages that are totally complete will be accepted for review.

Mural Design Presentation and Review

The proposed mural application will be presented to the Public Art Commission at their monthly public meeting.

▪ **Initial PAC Presentation Meeting**

Presentation materials for the initial meeting must include:

- a. Photos of the proposed location of the mural including all wall features and features immediately adjacent to the proposed mural site; complete wall measurements.
- b. Professional portfolio of the lead artist's mural work including examples of the artist's demonstrated ability from prior projects to carry out the project as designed.
- c. A color drawing at ½ inch scale that adequately illustrates the proposed mural including actual color, finishes and materials samples with their locations designated on the mural drawing,
- d. Verbal explanation of imagery concept including:
 - how the artwork enhances the existing character of the site through scale, color, material, texture, and content,
 - how the mural considers the social dynamics of the location, and
 - how the artwork considers the historical, geographical and cultural features of the site as well as its relationship to existing architecture and landscaping.
- e. Statement regarding the durability of the artwork and its potential to require ongoing maintenance.
- f. Art Agreement signed by the property owner.

▪ **Preliminary Design Approval**

Generally, the PAC review and preliminary approval for the applicant to move forward with the proposed mural concept occurs at the regularly scheduled monthly PAC meeting

following the applicant's initial presentation. Staff will notify the applicant of the Commission's decision and if necessary, schedule a date for the second design meeting.

*NOTE: If the mural is proposed for installation on the exterior of structures listed on the National Register of Historic Places or to a contributing property within a Historic District on the National Register of Historic Places the proposal will be forwarded to the Historic Commission for review. See **Murals on Historic Buildings** above.*

▪ **Second PAC Presentation Meeting**

Following preliminary approval of the mural concept, the applicant may be asked to attend a meeting to present the following:

- a. A color elevation drawn and gridded to ½ inch scale to illustrate how the mural concept will be translated to the site wall.
- b. Any additional information requested by the PAC.

Design Approval by City Council

- Following final design approval by the PAC and review by the Historic Commission, if required, the Commission will forward the mural concept to the City Council for approval.
- Once final approval is granted by the City Council, the applicant must:
 - a. Provide staff with the installation schedule. Applicant will be responsible for implementing all safety requirements per direction from staff (if work is occurring within the public right of way).
 - b. Provide a \$500 deposit to the City, if required. This deposit is held in reserve until the completed mural receives final installation approval by the PAC.
 - c. Enter into a contract between the applicant and the City of Ashland.

Review of Project during Installation

In order to facilitate timely PAC reviews throughout the mural prep and painting process, the applicant must provide staff a schedule of dates for completion of each mural stage addressed below.

- The PAC will review the project three separate times during the mural installation. The applicant must notify staff at the end of each of the following steps:
 - a. After the wall is prepped and ready for application. At this step, the PAC will also review paint colors and medium as well as other materials to be applied to the wall to ensure they are the same colors and materials approved during the review process and the medium is appropriate and durable.
 - b. Halfway through the application process for review of compliance with the drawings, materials and finishes.
 - c. Within seven days of completion.

- Following the third review, the PAC will determine if the completed mural is in compliance with the approved documents, drawings, materials and finishes.

Note: If the PAC finds that there are areas of the mural that are not rendered according to the approved design documents, the PAC may request the applicant adjust the mural to comply with the approved design. The PAC also recognizes that an artist may wish to make minor changes during the process that deviate from the approved concept but that enhance the overall project. The PAC and artist will agree on any changes to the approved design drawing.

- Once the PAC and applicant are satisfied that the mural is complete, the mural must be coated with a clear UV protectant paint to protect the mural from graffiti and ultra violet rays.
- The PAC will vote to accept the mural into the City's public art collection and forward their recommendation to the City Council for approval.

Other Things to Know

- The City will contract with the applicant for the execution/installation of the mural.
- The contract will require the applicant to submit proof of liability insurance.
- The Art Agreement will be in place for a period of five years. At the expiration of the five years, the Art Agreement may be terminated or extended by either party upon 30-day written notice.
- The City retains the right to remove the mural if the mural is not executed according to the approved concept documents.
- The City is responsible for the maintenance of the mural during the existence of the Art Agreement.

CITY OF ASHLAND

Public Art Commission

Mural Application

Applicant (City contracts with)

Applicant Name: JOHN PUGH
Applicant phone and email: 408 835-4341
Applicant Address Line 1: 143 VAN NESS AVE.
City: ASHLAND State: OR Zip: 97520

Lead Artist

Artist Name: SAME AS ABOVE
Artist phone and email:
Artist's Address Line 1:
Artist's Mailing Address (if different):
City: State: Zip:
Artist website: ARTofjohnpugh.com

Proposed Mural Building

Name of Property Owner of proposed mural building (if different from applicant): ELKS (944)
Owner phone and email: 541 482-3911 secretary@elks944
Proposed Mural Building Street Address: ASHLAND ELKS LODGE #944
255 E. MAIN ST.
Property Owner mailing address:
City: ASHLAND State: OR Zip: 97520

Dimensions of proposed mural wall:

Has the owner given permission for a mural to be painted on the proposed wall and is the owner willing to enter into an Agreement with the City? **YES**

The wall is:

brick cinderblock stucco wood other

Questions

1. Please describe the project, the specific location of the mural and why a mural will enhance the area. **MURAL TO BE LOCATED ON WEST-FACING WALL, PERPENDICULAR TO TRAFFIC ON MAIN ST. MURAL DESIGN WILL BE CROWD-PLEASING AND DYNAMIC, WILL EXPLORE ASHLAND HISTORY; INCLUDING**
2. Can the wall be seen from the public right of way (e.g. sidewalk, alley, street etc.)? **SHASTA VILLAGE.**
YES; AUTO & PEDESTRIAN TRAFFIC
3. Describe the process you used to select a professional mural artist?
WHAT CAN I SAY? HE IS REALLY GOOD.
4. Describe the theme/image you envision for this mural if known at this time.
WITH A PRODUCTION SET THEME, WE EXPLORE THE STORY OF DOWNTOWN ASHLAND'S LITHA PARK.
5. Why do you want a mural at this location? How will the mural benefit the neighborhood? Community? **IT'S A HIGHLY VISIBLE DOWNTOWN LOCATION - WITH A STRONG ILLUSION CORRIDOR. MURAL PROVIDES SENSE OF PLACE, HISTORICAL HERITAGE, PRIDE OF PLACE, AS WELL AS A PLACE-MAKING ATTRACTON.**
6. Please attached a detailed budget for the project. What funding do you have for the project? **MEMBERS OF THE ART COMMUNITY (INCLUDING SOU) WILL BE INVITED TO PARTICIPATE. WILL SERVE TO EDUCATE VISITORS DOWNTOWN.**
7. Describe the ground in front of the wall (condition, debris etc.) and surrounding features.

SEE ATTACHED

Mural Application **GROUND IN FRONT OF MURAL** Page 2
IS THE ROOF TOP OF CHASE BANK

CITY OF ASHLAND

Public Art Mural Agreement

The Agreement is between Elks Lodge (Property Owner) and the City of Ashland (City).

RECITALS

1. The City has adopted a process for the placement of public art murals (Mural) on public and private buildings throughout Ashland.
2. The Property Owner owns the real property situated at 255 E. Main St. (physical address) and is willing to make an exterior wall (Wall) available for a public art mural.
3. After final approval by the PAC of the installed mural, it becomes the property of the City of Ashland and may be removed by either party after a period of five years.

The parties agree as follows:

This agreement is in effect for at least five years. After the five year minimum, the agreement may be terminated by either party upon 30 days written notice. Upon termination, the Wall will be restored to its prior condition at the expense of the party who initiated the termination.

The City is responsible for the maintenance and if necessary repair of the Mural during the life of the agreement. The City shall have the right to access the Mural and the Wall for maintenance purposes. The City may remove the Mural, if in the sole judgement of the City, the Mural cannot be maintained.

In the event of any dispute in any manner relating to this agreement, the parties shall submit the dispute to be resolved by binding arbitration. The arbitration award shall be final and binding on the parties on the parties in the same manner as the final judgment of a court.

City of Ashland, OR

Name and Title: Ronald J. Corallo CHAIR BOD

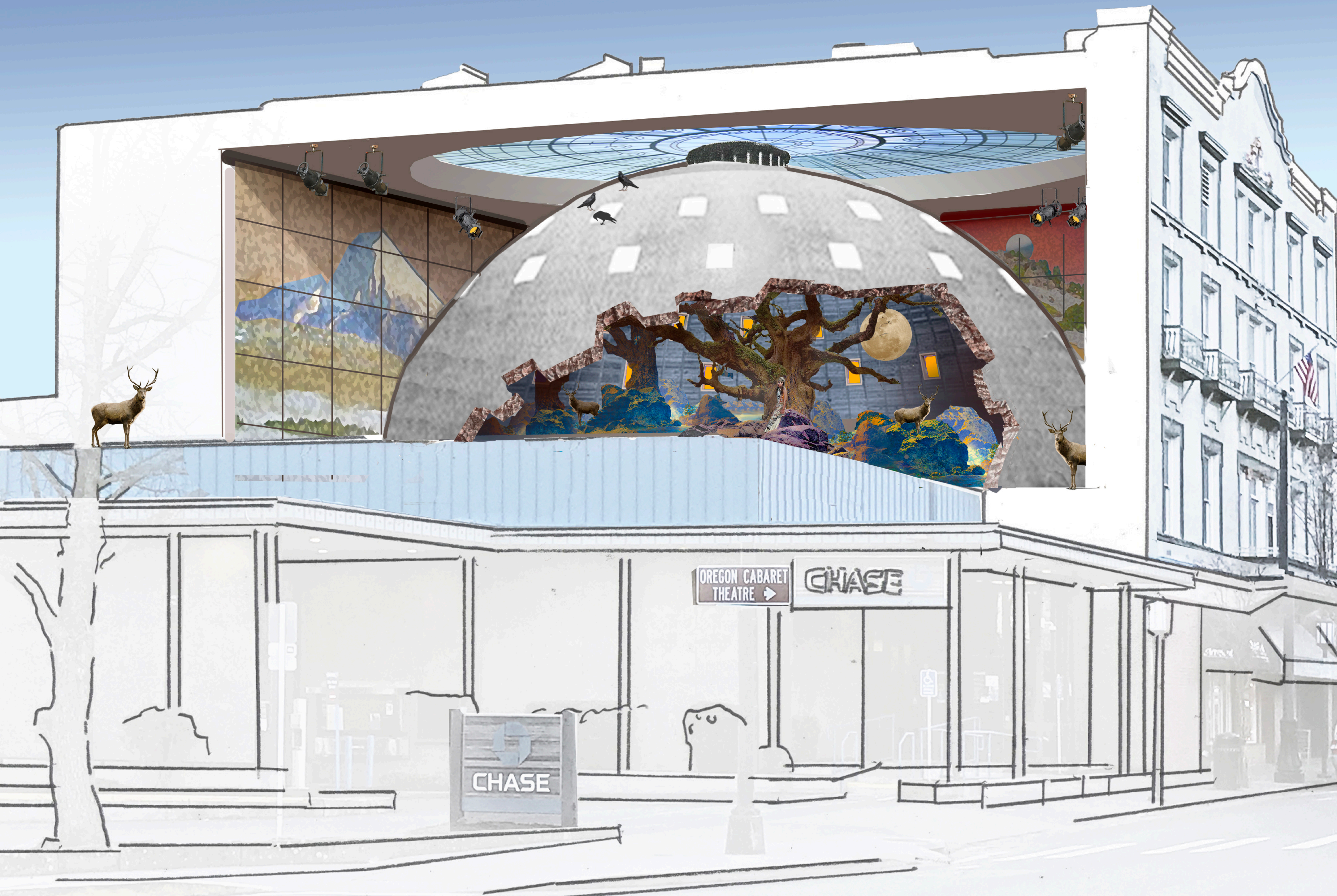
Date: 7/06/2022

Property Owner

Name: Grand Lodge Mailing Address: Chicago Ill

Phone: 541-482-3911 Email Address: Secretary@elks944

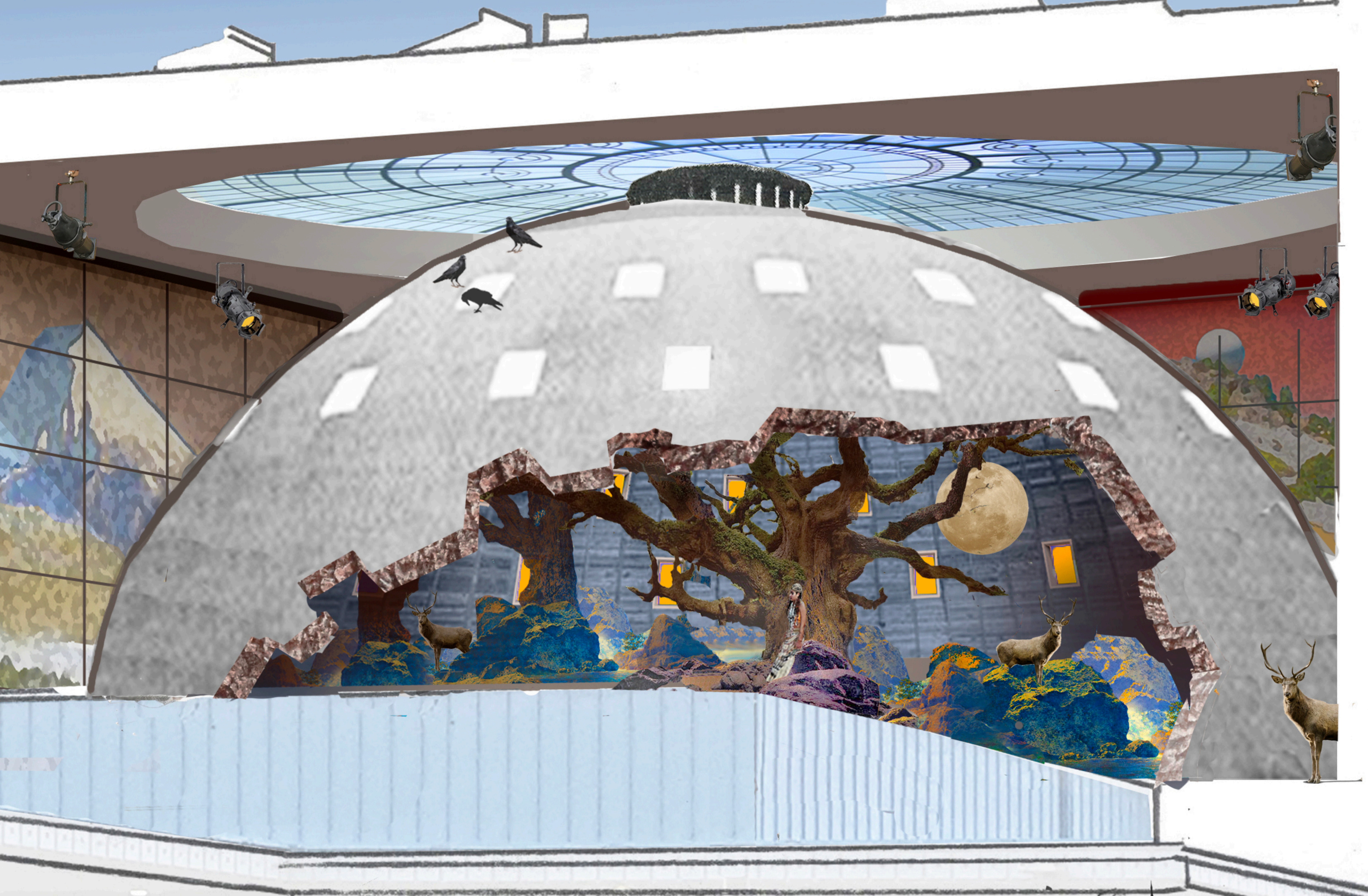
Date: 7/06/2022



ASHLAND ELKS

MAIN STREET

JOHN PUGH



Downtown Ashland Elks Mural Proposal - John Pugh

Itemized Budget Estimate

The project budget of \$200,000 will include all labor, materials, studio expenses, scaffolding, and general liability insurance. This project will be fabricated and mostly painted in the studio, with about 35% of the process saved for the on location installation and integration.

Paint and Materials

Studio Overhead (based on five months studio time):	\$16,000
Nova Color Paints, Varnishes, and Gels for installation:	\$11,000
Prepared PolyTab (outdoor mural canvas):	\$3,000

Wall Prep and Skim Coating

Pressure Washing	\$2,500
Wall Plastering - includes Weldcrete bonder, Skim Brown Coat, Acrylic Fine Sand Finish Texture (two coats), and Galvanize Accessory Reinforcement Metal:	\$35,000
Prime Wall:	\$2,500

Final Treatment - B-72: \$5,000

Mural Lighting

LED Flood Fixtures (4)	\$2,000
Wiring, connection parts to power supply, and labor :	\$4,000

On Site Expenses

\$2,000,000 General Liability Insurance (pro-rated):	\$1,000
Scaffolding (including special rigging over narrow areas):	\$23,000
Safety / Other Rental Equipment,	\$2,000

Artist Fees

Artist Fee (includes Artist Time, Research Expenses / Materials, Photography, Model Making):	\$65,000
Additional professional artist (in studio & on location)	\$20,000
Two Artist assistants (in studio & on location):	\$8,000

Estimated Total: \$200,000

Mural Durability and B-72

B-72 is a state-of-the-art new approach to making a mural last generations. It's completely resistant to the elements and is so strong that even if the mural gets tagged, any strong solvents can be used to remove the graffiti without damaging the mural.

When sprayed on the mural, this product penetrates and replaces the acrylic binding without disturbing the pigment. Murals that look faded or even "gone" can be attributed to the oxidation of the acrylic binding. When acrylic binding oxidizes - usually from a combination of sun, water, especially sea water - it creates micro fractures in the binding (like a cracked windshield). This obscures the mural to the point of eventually appearing fading beyond recognition.

On a quality acrylic mural the life expectancy for a south / west facing exposure (northern hemisphere) could be from about 10 to 15 years, and a north / east facing exposure could be from 15 to 30 years. Yet assuming that the mural artist used only the finest color-fast pigments, the mural will, with simple maintenance, become a permanent public art form, and last 100 years. Like mosaics or bronze sculpture, B-72 breaks the stigma or paradigm that murals do not have long lives.

Rick Briley and Wayne Winiiecki of 'Mural Medics' are experts on B-72 and it's application. I've worked with Rick and Wayne over the years, and believe they are the go-to-guys for B-72 and for spraying the coating for mural protection. They've used this product on murals that seemed to be faded beyond recognition, and the results were like magic. Some murals were restored to the point that they looked like the day they were painted.

For further information and spraying application of B-72, Please contact Rick Briley at 760 668-1744 or email at: rick.briley@muralmedic.com

John Pugh Mural Commissions (Partial Listing)

- 2021 Bijou Theater Mural, City of Hermosa Beach, CA
- 2020 Denver Zoo / City Park, Denver Arts Commission, CO
Aquarium on the Boardwalk - *Branson, MI*
- 2019 Montclair Mural Project, City of Oakland, CA
Central Life Building, Ottawa Mural Program, IL
- 2018 Theatre in the Round - *Minneapolis, MN*
City Walk Mural Project, Government of UAE - *Dubai, UAE*
- 2017 Quetzalcoatl Mural Project, Government of Mexico - *Mexico City*
Jean Cocteau Theater, George RR Martin - *Santa Fe, NM*
- 2016 State Library, Wisconsin Historical Society - *Madison, WI*
Promega Corporation - *Madison, WI*
- 2015 Lennox Mural with Elastic TV Production - *Santa Monica, CA*
City of La Crosse, National Endowment for the Arts - *La Crosse, WI*
- 2014 Sacramento Water Tank Project, Sacramento Arts Commission, CA
GE Wonderground Mural Project - LA, Boston, SF, Chicago, and NYC
- 2013 Wonderworks Science Museum, Syracuse, NY
City of Hermosa Beach - *LA, CA*
- 2012 Speightstown Mural Project – *Speightstown, Barbados*
Universal Studios – *Tokyo, Japan*
- 2011 City of New Plymouth - *New Zealand*
Beach Boardwalk – *Santa Cruz, CA*
- 2010 Westside Recreation Centre, Calgary Arts Commission - *Calgary, Canada*
Pepsi Cola International - *New York, NY*
Skyline College, Art in Public Places – *San Bruno, CA*
- 2009 Santa Cruz Redevelopment Agency – *Santa Cruz, CA*
Aquatic Center, City of Fremont Arts Commission – *Fremont, CA*
Del Oro Theater, Redevelopment Agency, *Grass Valley, CA*
- 2008 Mana Nalu Plaza, Honolulu Arts Commission - *Honolulu, HI*
Madera Police Station - *Madera, CA*
Juvenile Hall Lobby, Sacramento Metropolitan Arts Commission, CA
- 2007 Dimond High School, Anchorage Arts Commission – *Anchorage, AK*
Star Reacher, Private, *Carmel, CA*
- 2006 Downtown Breezeway, Chandler Arts Commission – *Phoenix, AZ*
Wonderworks Science Museum, *Gatlinburg, TN*
Palo Alto Medical Foundation (Two Projects)– *Palo Alto, CA*
- 2005 Rotorua Public Library, Arts Commission – *Rotorua, New Zealand*
Sarasota Health Department, Florida Art in State Buildings – *Sarasota, FL*

- 2004 Bishop Mural Society (two projects) – *Bishop, CA*
 Crossroads Mural Project, Dublin Arts Commission – *Dublin, CA*
- 2003 Mainplace Merced, Redevelopment Agency – *Merced, CA*
 Berryessa Community Center, San Jose Arts Commission – *San Jose, CA*
- 2002 El Camino Hospital – *Mountain View, CA*
 Opa Locka Health Services, Florida Art in State Buildings – *Miami, FL*
 Hayward City Hall, Hayward Arts Commission – *Hayward, CA*
- 2001 Debra Winger - (private) *New York, NY*
 Global Mural Conference Project – *Twentynine Palms, CA*
- 2000 University of North Florida, Florida Art in State Buildings – *Jacksonville, FL*
 Levi Strauss – *Dayton, NV & Jacksonville, OR*
- 1999 University of Alaska, 1% For Art Commission – *Fairbanks, AK*
 Kaiser Permanente Hospital – *Santa Clara, CA*
- 1998 Victor Valley College Library, Arts Commission – *Victorville, CA*
 Standing on a Corner in Winslow, Arizona Park – *Winslow, AZ*
- 1997 Downtown Breezeway, South San Francisco Arts Commission, *CA*
 Barbi Benton – (private) - *Honolulu, HI*
 ICTV, California Design International – *San Francisco, CA*
 Silicon Graphics, Inc. – *Mountain View, CA*
- 1996 Pleasant Company – *Taipei, Taiwan*
 Moore Technologies – *San Jose, CA*
- 1995 Park Meadows Shopping Center – *Denver, CO*
 Crossroads Center, Palm Desert Arts Commission – *Palm Desert, CA*
- 1994 -1980 County Parking Facility, Sacramento Metropolitan Arts Commission, *CA*
 Stanford Shopping Center, Stanford University – *Palo Alto, CA*
 California State University, Chico

Awards Distinguished Alumnus – California State University, Chico, CA
 Most Successful Mural Artist – California Mural Symposium 2006
 Master Muralist, Mural Art Center Awards – San Francisco, CA

Publicity John Pugh has completed over 250 murals, and his work has appeared in articles / media world wide including; Time, L.A. Times, New York Times, USA Today, Good Morning America, London Sun and Mail, Tokyo Mainichi, BBC World News, Art Business News, Public Art Review, Artweek, and Southwest Art.

The book '*Murals of John Pugh; Beyond Trompe L'oeil*' (Random House) is available at Barnes and Noble, and Amazon.

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Ashland Downtown Historic District, Ashland, OR

The "attractive" fountain built by the Jordan Sash and Cabinet Works remained in the building as a part of the drug store use until the late 1970s. "The fountain, in operation for more than 40 years at the same location...pharmacist Jack Sabin removed the last of the counters and the familiar swivel stools on Wednesday...signaling an end to the drug store soda fountain in Ashland. (*Tidings*, 16-Feb-1978, 1:2-5)

While altered from its original design, the Hershey Building retains high integrity to its c1931 remodeled appearance and effectively relates its development history during the period of significance.

ID# 77.0 *Survey #316*

ASHLAND ELKS LODGE #944
247-255 MAIN ST E

Architect: Clark, Frank Chamberlain

Academic Classicism: American Renaissance

1910
391E09BD 4400

Primary Contributing

An impressive four story concrete volume, construction of the Ashland Elks Lodge began in 1908 and was completed in 1910. The Elks Building is one of the largest single structures in the Ashland Downtown. In May 1908 the Elks' had moved the Lindsay Applegate home which had stood on this site in preparation for construction. (*Tidings*, 4-May-1908) The initial announcement of the project credits Charles Burgraff, a prominent architect from Albany, Oregon as the designer. (*Tidings*, 2-Mar-1908, 3:3) Before actual construction began, however, design of the project had been given over to Frank Chamberlain Clark. "F. C. Clark, of San Francisco, a former resident of Ashland, arrived in town Friday evening. Mr. Clark is an architect and plans submitted by him for the new Elks' temple here have been accepted." (*Tidings*, 18-Jun-1908, 3:2)

The elevations show about the finest block in Ashland. The dimensions of the building are 75 x 100 feet with three (sic) and a full basement...The first floor will be given over to store rooms for rental. The second floor, as well as the third, will be entirely devoted to lodge purposes... (*Tidings*, 22-June-1908, 1:3)

Construction progress was slow and it was not until 1910 that the building was finally completed. The formal dedication ceremony was held over a three day period in mid-October 1910 and the building was hailed as "...the finest between San Francisco and Portland." (*Tidings*, 17-Oct-1910, 1:3-4)

The Elks leased the ground floor retail spaces to various stores, most notably the local J. C. Penney department store, which was located here for more than five decades. In 1968 a fire damaged the building's lower floors, causing an estimated \$100K of damage. (*Medford Mail Tribune*, 17-Aug-1968)

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Ashland Downtown Historic District, Ashland, OR

Still used by the Elks, the first floor retail spaces of the Elks Building have been somewhat altered and modernized to suit a series of specialty retail uses. At present the building offers three retail storefronts, plus the access to the Elks' uses of the upper floors. While modified at street level, the upper three floors of the stucco-clad building retain very high integrity, with the project balconies, wrought-iron railings, integral window surrounds and elegant stepped parapet. A significant visual landmark in the eastern portion of the District, the Elks Building effectively and accurately conveys its appearance during the period of significance and the associations which make it significant within the development of downtown Ashland.

ID# 77.100 *Survey #316*

ASHLAND ELKS PARKING LOT
0 C STREET
No Style

391E09BD 3900
Vacant: Improved Parking

Sanborn Maps of this area in 1928 show the parcel with a wood-frame dwelling and a "auto repair" garage lining First Street. Acquired by the Elks, the site is now an improved parking lot for their lodge facility.

ID# 78.0

CRATER NATIONAL BANK BLDG
243 MAIN ST E
Architect: Keeney, Robert
Modern Period: Commercial

1971
391E09BD 4500
Builder: Salter, Harold
Non-Compatible, Non-Historic, Non-Contributing

This modern masonry structure was complete in 1971 on a site previously associated with the Lithia Theater and a gas station. (Ashland Permit 4030) Built of non-historic materials and set back on the site to allow for drive-through banking, it is not consistent with the traditional character of the downtown.

ID# 79.0 *Survey #317*

CITIZEN'S BANKING AND TRUST CO. BUILDING [WEST]
232 MAIN ST E
Architect: Bowen, William F.
American L. 19th C: Chicago School

1910
391E09BD 4600
Builder: Wentworth, H. T.
Primary Contributing [NR-Listed]

This fine two story buff-colored brick volume with granite detailing was completed in 1910 as the home of Citizen's Banking and Trust Company, a local financial institution founded during this booming period of Ashland's history. The bank retained architect W. F. Bowen to design the building and visually combine it into a single volume with the adjacent structure, already under construction for Clyde Payne. (See ID# 79.100, below). Upon completion, the bank occupied the prominent corner storefront, with a grocery and barber shop on the main floor of the eastern, Payne, portion. The combined second floor, accessed via a shared entry, provided quality spaces for a number of Ashland professionals. Initially successful, Citizen's Bank failed during the depression and the organization's assets were liquidated after it closed its door for good in 1932. Historic photographs document