



CITY OF ASHLAND



TREE COMMISSION AGENDA October 11, 2018

- I. **CALL TO ORDER**
6:00 p.m. in the Siskiyou Room of the Community Development and Engineering Services Building located at 51 Winburn Way.
- II. **APPROVAL OF MINUTES**
Approval of September 6, 2018 meeting minutes.
- III. **ANNOUNCEMENTS & LIAISON REPORTS**
 - Council Liaison
 - Parks & Recreation Liaison
 - Community Development Liaison
- IV. **PUBLIC FORUM**
- V. **DISCUSSION**
 1. Oregon Shakespeare Festival Oak Tree Replacement
 2. Tree Removal and Landscape updates to Public Parking Lot at 130 N Pioneer St
- VI. **TYPE I REVIEWS.**
 - A. **PLANNING ACTION: PA-T1-2018-00021**
SUBJECT PROPERTY: 898 Morton St
APPLICANT: Kerry Kencairn
OWNER: Ryan Schnobrich
DESCRIPTION: A request for a Tree Removal Permit to remove two trees from the property located at 898 Morton Street. Both trees are Ponderosa pines between approximately 14 and 17-inches diameter at breast height (dbh) and over 40 feet tall. The property is steeply sloped and located in the Hillside Lands overlay. The trees were slated to be retained in the original subdivision approval. The application materials provided include a statement from the Fire Adapted Communities Coordinator recommending the removal of the trees because of their proximity to the existing house and deck. COMPREHENSIVE PLAN DESIGNATION: Rural Residential; ZONING: RR-.5; ASSESSOR'S MAP: 39 1E 16AC; TAX LOT: 421
 - B. **PLANNING ACTION: PA-TREE-2018-00029**
SUBJECT PROPERTY: 899 Cypress Point Loop
APPLICANT/OWNER: Geoffrey Ball
DESCRIPTION: A request for a Hazardous Tree Removal Permit to remove a black oak located in the Oak Knoll subdivision open space but functioning as part of the backyard at 899 Cypress Point Loop. The tree, approximately 40-inches diameter at breast height, was recommended for removal by two arborists and found to have root disease, rot, fungus, and die off. COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-10; ASSESSOR'S MAP: 39 1E 13CB; TAX LOT: 5300
- VII. **TYPE II REVIEWS**
 - A. **PLANNING ACTION: PA-T2-2018-00003**
SUBJECT PROPERTY: 188 Garfield Street
APPLICANT: Rogue Planning & Development Services, LLC
OWNER: Spartan Ashland Rivergate Real Estate, LLC
DESCRIPTION: A request for Site Design Review approval to construct a 72-unit studio apartment community ("The MidTown Lofts") for the properties located at 188 Garfield Street. The application also includes requests for a Tree Removal Permit to remove 15 trees that are more than six-inches in diameter at breast height (d.b.h.); an Exception to the Site Development and Design Standards to treat stormwater run-off in a combination of bio-swales, underground

treatment facilities and detentions ponds rather than in landscaped parking lot medians and swales; and for Exceptions to Street Standards to retain the existing curbside sidewalk system along the frontage of the property and for the location of the driveway curb cut on Quincy Street, which is proposed to be shared with the property to the east and which would exceed the maximum driveway curb cut width for residential developments. (All of the proposed units are studio units that are less than 500 square feet in gross habitable floor area and each counts as $\frac{3}{4}$ of a unit for purposes of density calculation; density bonuses are requested for conservation housing, outdoor recreation space and major recreation facilities.)

COMPREHENSIVE PLAN DESIGNATION: High Density Multi-Family Residential; ZONING: R-3;
ASSESSOR'S MAP: 39 1E 15AD; TAX LOT: 2100 & 2101

B. PLANNING ACTION: PA-T2-2018-00004

SUBJECT PROPERTY: 1661 Ashland Street

APPLICANT: Rogue Credit Union & Columbia Care Services, Inc.

OWNER: Columbia Care Services, Inc./Jerome White, KSW Architects

DESCRIPTION: A request for a Land Partition and Site Design Review to create three parcels from the parent property located at 1661 Ashland Street. Parcel 1 would be 33,278 square feet in area and would contain the proposed "Rogue Ridge" development consisting of 30 multi-family dwelling units including one resident manager's unit and 29 affordable housing units in a 26,146 square foot, three-story building in two connected wings; Parcel 2 would be 9,913 square feet in area and would contain a future two-story commercial building; and the third parcel would be 22,462 square feet and would contain the existing Rogue Credit Union building. (25 of the 30 proposed units are studio units that are less than 500 square feet in gross habitable floor area and each counts as $\frac{3}{4}$ of a unit for purposes of density calculation; the five remaining units are two- and three-bedroom and count as full units for density purposes).

COMPREHENSIVE PLAN DESIGNATION: Commercial; ZONING: C-1;

ASSESSOR'S MAP: 39 1E 10DC; TAX LOT: 8700 & 9201

VIII. STREET TREE REMOVAL PERMITS

A. PLANNING ACTION: PA-TREE-2018-00027

SUBJECT PROPERTY: Right of Way at intersection of Scenic and Grandview

APPLICANT/OWNER: City of Ashland Streets Division

DESCRIPTION: A request for a Hazardous Tree Removal Permit to remove a volunteer tree that is damaging the curb. The tree has multiple 6 inch stems and an approximate height of 30 feet. It is near 195 Scenic Drive. COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-10;

B. PLANNING ACTION: PA-TREE-2018-00028

SUBJECT PROPERTY: Right of Way at intersection of Orchard and Westwood St

APPLICANT/OWNER: City of Ashland Electric Dept

DESCRIPTION: A request for a Street Tree Removal Permit to remove a dead tree at the intersection of Orchard and Westwood Street. The tree is approximately 30 inches in diameter at breast height and 70 feet tall. The tree is located approximately between 460 and 450 Orchard St. COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-10;

C. PLANNING ACTION: PA-TREE-2018-00030

SUBJECT PROPERTY: 365 Strawberry Ln

APPLICANT: Regensis Ecological Design

OWNER: William Potts

DESCRIPTION: A request for a Street Tree Removal Permit to remove a tree in the right of way at 365 Strawberry Ln. The tree is a 10 inch DBH lodgepole pine that is approximately 20 feet in height. The tree is on an eroding cut slope and has exposed structural roots. In order to accommodate further foundation drainage improvements further root cutting is required. COMPREHENSIVE PLAN DESIGNATION: Rural Residential; ZONING: RR-.5; ASSESSOR'S MAP: 39 1E 08AC; TAX LOT: 602

D. PLANNING ACTION: PA-TREE-2018-00031

SUBJECT PROPERTY: 166 N Laurel St

APPLICANT/OWNER: Vadim Agakhanov

DESCRIPTION: A request for a Street Tree Removal Permit to remove a tree located on the alley between 166 N Laurel and 148 N Laurel St. The tree is a 12 inch DBH Juniper approximately 20 feet in height. The tree is damaging the foundation of a structure and leaning out over the alleyway. COMPREHENSIVE PLAN DESIGNATION: Low Density Multi-Family; ZONING: R-2; ASSESSOR'S MAP: 39 1E 04CC; TAX LOT: 3400

IX. ADJOURNMENT

Next Meeting: November 8, 2018



CITY OF ASHLAND



TREE COMMISSION MINUTES Draft September 6, 2018

Commissioners Present:	Parks Liaison
Christopher John	Peter Baughman
Russell Neff	
Asa Cates	Staff Present:
Mike Oxendine	Nathan Emerson
Council Liaison	
Steven Jensen	

CALL TO ORDER

Chair Christopher John called the meeting to order at 6:01 p.m. in the Siskiyou Room of the Community Development and Engineering Services Building located at 51 Winburn Way.

APPROVAL OF MINUTES

Cates/John m/s to approve the minutes of August 9, 2018 with the correction that Commission Mike Oxendine was present and needed to be added to the roll. Voice vote. ALL AYES. Motion passed

ANNOUNCEMENTS & LIAISON REPORTS

- **Council Liaison** - Council Liaison Steven Jensen introduced himself. He previously served three terms as Chair of the Ashland Forest Land Commission. Mr. Jensen is a retired South Medford High School Science Teacher. The Commissioners introduced themselves to Mr. Jensen.
- **Parks & Recreation Liaison**
Peter Baughman announced that a new Parks Superintendent has been hired. That person is Tree Commissioner Mike Oxendine. Because of Mike's new position this meeting will be his last.

PUBLIC FORUM

No one spoke at public forum.

The Agenda was adjusted and Planning Action Tree-2018-00021 was moved to the first planning action item.

TYPE I REVIEWS

PLANNING ACTION: TREE-2018-00021

SUBJECT PROPERTY: 898 Morton St

APPLICANT: Kerry Kencairn

OWNER: Ryan Schnobrich

DESCRIPTION: A request for a Tree Removal Permit to remove two trees from the property located at 898 Morton Street. Both trees are Ponderosa pines between approximately 14 and 17 inches' diameter at breast height (dbh) and over 40 feet tall. The property is steeply sloped and located in the Hillside Lands overlay. The trees were slated to be retained in the original subdivision approval. The application materials provided include a statement from the Fire Adapted Communities Coordinator recommending the removal of the trees because of their proximity to the existing house and deck. **COMPREHENSIVE PLAN DESIGNATION:** Rural Residential; **ZONING:** RR-.5; **ASSESSOR'S MAP:** 39 1E 16AC; **TAX LOT:** 421.

Commissioner John brought to the attention of the group that he had pruned trees on that property in the past but does not have any future prospective work at that location. After a brief discussion the Commissioners agreed it would not be a conflict of interest and he could participate.

Emerson gave a short staff report explaining that normally trees in this zone and of that size do not require a tree removal permit. Because they were originally in the subdivision plan this is considered a modification.

A neighbor, Tom Besich of 880 Morton introduced himself. Mr. Besich was interested in learning the process and recommended mitigation for the removal of the trees. Mr. Besich would like to see that other trees on the property are protected as well. He specifically called attention to three trees that shaded his property. He was interested in ensuring that these trees were maintained in good health. Mr. Besich commented on the presence of other fire hazards at 898 Morton Street.

The Commissioners discussed the trees and the nature of fire hazard. Jensen questioned if it was possible to require that the fire mitigation plan that was submitted with the application be executed. It was determined there is no code provision that would allow the Commission to require that to be so. Cates mentioned that the Ponderosas' requested for removal are most likely to be the most fire resistant trees on the entire property. John believes this will come up more often in the future with the concern for Wildfire safety.

Cates/Oxendine m/s to deny the application and recommend that the applicant prune branches ten feet from the roof and deck and any other part of the structure. Voice Vote: All Ayes.

PLANNING ACTION: PA-T2-2018-00002

SUBJECT PROPERTY: 880 Park Street

APPLICANT/OWNER: Tudor Properties, LLC/Kistler Small + White, LLC (Architects)

DESCRIPTION: A request for Site Design Review approval to construct a 15-unit apartment complex consisting of six apartment buildings, a separate 221 square foot laundry facility and a 30-space parking lot for the property at 880 Park Street. The application includes requests for Exception to the

Street Standards to retain the existing asphalt multi-use path along Siskiyou Boulevard and to construct a meandering sidewalk along the subject properties Park Street frontage rather than installing new city standard sidewalks and parkrow planting strips, and for a Tree Removal Permit to remove five trees greater than six-inches in diameter at breast height (d.b.h.), including two Green Ash (*Fraxinus pennsylvanica*), one Modesto Ash (*Fraxinus velutina*), and two Redwoods (*Sequoia sempervirens*) including a multi-trunked cluster with five trunks of diameters ranging from eight- to 14-inches d.b.h. Note: An existing approximately 895 square foot shop building on the southeastern portion of the property would be demolished as part of the proposal.

COMPREHENSIVE PLAN DESIGNATION: High Density Multi-Family Residential; ZONING: R-3; ASSESSOR'S MAP: 39 1E 15AD; TAX LOT: 3402.

Chair John recused himself from the discussion of this project and left the room. He provided service to the applicant for this proposal.

Emerson gave a brief staff report and handed out a memo from Senior Planner Derek Severson. See exhibit A at the end of the minutes. The main concern for the Tree Commission is the Redwoods. Emerson reminded the Commissioners that the code says you cannot deny tree removal if it prevents the legal density (amount of units) for that site to be developed.

Sue Williams, who lives at 836 Park Street across Siskiyou Boulevard was present and spoke for public comment. She advocated to save the trees on the property.

The applicant, Matt Small of Kistler, Small and White Architects was present along with project engineer Ryan Beugli and landscaper Eric Simpson. Mr. Small gave his summary of the project and his belief that it was impossible to save either the large Redwood or the Redwood cluster without a complete redesign of the project. He added there are many requirements of the code and meeting them all led to the current design.

Oxendine asked the applicant to explain what affordable housing meant in this context. Mr. Small explained that the units are small and able to rent by the bedroom, leading to rents less than the normal rate. Emerson gave an explanation of what affordable means in Ashland Land Use Code which requires deed restrictions and income caps. The units being proposed does not meet the technical code definition.

The Commissioners discussed the project. Oxendine stated the mitigation plan was great and would result in more trees overall. He agreed with the need for more affordable housing in the City of Ashland. Oxendine believed that the multi-stemmed Redwood had been removed in the past and sprouted again. He also commented that the multi-stems would need to be removed in the future because of form and both he and Commissioner Neff agreed the location was inappropriate for that site.

The following motion was made

Oxendine/Cates m/s to approve the planning action with the following recommendations.

- *That half of the Raywood Ash trees proposed as street trees on the landscape plan be replaced by*

an oak variety of 3 ½ inches caliper or greater.

- *That the retained Ash trees have a tree protection zone established with fencing, watered twice a week during growing season while the project occurs. And signs be posted on the tree protection fencing stating it must remain during the project, and that an arborist be present when cutting in the critical root zone.*
- *The tree #7, the Redwood cluster, not be retained.*

LANNING ACTION: TREE-2018-00023

SUBJECT PROPERTY: 154 B Street

APPLICANT/OWNER: Ashland Property Management/Jayne Dutra & Gordon Huff

DESCRIPTION: A request for a Tree Removal Permit to remove two trees from the property located at 154 B Street. Both trees are Box Elders (*acer negundo*) and are 23- to 24-inches diameter at breast height (dbh) and 45- to 50-feet tall. The application materials provided by the project arborist indicate that one tree is mostly dead with root rot and conks growing on its trunk, and the second tree is showing signs of decline and believed to have root rot as well. The application also notes that if one tree loses limbs, it would impact the power lines serving the home. **COMPREHENSIVE PLAN**

DESIGNATION: Low Density Multi-Family Residential; **ZONING:** R-2;

ASSESSOR'S MAP: 39 1E 09 BA; **TAX LOT:** 12300.

Emerson gave a brief staff report explaining this is a multi-family property and both trees are over 6" DBA which is why a tree removal permit is required. The applicant is applying for a hazard tree removal and must meet that criteria. The arborist report was included in the application.

The Commissioners discussed how Tree #2 did not meet any of the hazard criteria. In order to recommend removal, the applicant would need to meet either hazard or non-hazard criteria. The Commissioners also recommended that any Tree removals be mitigated by a new Tree.

The following motion was made;

John/Neff m/s to approve removal of Tree #1 but not Tree #2. Voice Vote; All Ayes.

DISCUSSION

Tree of the Year Discussion – Emerson gave a quick report on the plans for the Tree of the Year Contest. The Commissioners were generally supportive but wanted to include Trees nominated on Public Property.

Cates/John m/s that the Tree Commission would allow both Private and Public Trees be included in the Tree of the Year nominations. Voice Vote; All Ayes.

ADJOURNMENT

The meeting was adjourned at 7:20 p.m. Emerson noted that the next regular meeting would be held on Thursday, October 4, 2018 at 6:00 pm. in the Siskiyou Room of the Community Development and Engineering Services Building located at 51 Winburn Way.

Respectfully submitted by, Carolyn Schwendener



P.O. Box 158
15 South Pioneer Street
Ashland, Or 97520

541 482 2111
541 482 0446 fax
541 482 4331 box office

www.osfashland.org

September 26, 2018

City of Ashland Tree Commission
c/o Ashland Department of Community Development
51 Winburn Way
Ashland, OR 97520

VIA EMAIL

Dear Commissioners:

The Oregon Shakespeare Festival requests the Commission's input in an advisory capacity regarding the removal and replacement of a dead canyon live oak (*Quercus chrysolepis*) which graced the southern corner of OSF's central courtyard ("the Bricks") for many decades.

The tree was in failing health for many months, but its death was confirmed by Southern Oregon Tree Care earlier in 2018. The recent renovation of the Bricks may have contributed to the tree's demise, despite tree protection measures put in place, but the tree was clearly of a very advanced age and had started to decline prior to the construction project.

OSF plans to engage Southern Oregon Tree Care to remove the tree and stump on October 29, 2018, after OSF's 2018 season concludes. This date was picked so as to create as little disturbance to OSF staff and patrons as possible, as well as to allow for planting of a replacement tree during the optimal time of year. We intend to plant a 2" to 3"-diameter replacement tree as soon as possible after the old tree has been removed and in the same location.

OSF has considered many species of replacement trees, all in the oak family, as the "old oak tree" was a campus icon and it feels important to continue this legacy with a similar replacement tree. We are seeking a balance of drought and heat tolerance, gracious shape and generous shade, ability to thrive in an urban setting, and a relatively expeditious growth trajectory. Current contenders include the Shumard oak (*Quercus shumardii*), willow oak (*Quercus phellos*), Nuttall oak (*Quercus nuttallii*), or another red oak species to be determined, but we invite the Commission's suggestions. We hope to buy locally and are somewhat constrained by the inventory of local nurseries, none of which appear to carry canyon live oaks.

Thank you for your consideration and advice,

A handwritten signature in blue ink, appearing to read "Ted DeLong", with a long, sweeping flourish extending to the right.

Ted DeLong
General Manager





Planning Division
51 Winburn Way, Ashland OR 97520
541-488-5305 Fax 541-488-6006

STREET TREE REMOVAL PERMIT

A tree that is located in any public street right-of-way or other public property may not be removed until a Street Tree Removal Permit has been submitted according to the Application Submission Requirements, below, and reviewed and approved by the City of Ashland.

An application for street tree removal must demonstrate that the tree is an emergency, hazard, or dead tree as outlined below in the Application Submission Requirements.

Application Submission Requirements. An application for a street tree removal permit shall include all of the following information.

1. **Application Form and Fee.** The application must include the information requested on the Street Tree Removal Permit form provided by the City of Ashland and the permit application fee. Only those property owners of a lot adjoining the street tree location or homeowners' associations responsible for street trees in their development or subdivision may apply to remove an adjoining street tree. If a tree is located in front of more than one property, each property owner or homeowners' association official must sign the Street Tree Removal Permit form.
2. **Site Plan.** A site plan of the property drawn to scale containing the following information. The scale of the site plan must be at least one inch equals 50 feet or larger.
 - a. North arrow and scale.
 - b. Property boundaries including dimensions of all lot lines and driveway locations.
 - c. Location and width of all public streets, planting strips, and sidewalks adjoining the site.
 - d. Size, species, and location of the tree(s) proposed to be removed.
3. **Written Statement.** A written statement explaining how the proposed street tree removal satisfies one of the following approval criteria. The Community Development director may require additional information to demonstrate that the proposed removal satisfies one of the following approval criteria including: 1) a written statement to be prepared by an arborist licensed by the State of Oregon Landscape Contractors Board of Construction Contractors Board and certified by the International Society of Arboriculture or American Society of Consulting Arborists; and 2) an International Society of Arboriculture (ISA) Basic Tree Risk Assessment Form to be completed by an arborist.

Street Tree Removal Approval Criteria

- a) **Emergency Tree Removal.** The tree presents an immediate danger of collapse and represents a clear and present hazard to persons or property. Immediate danger of collapse is defined as a tree that may already be leaning, with the surrounding soil heaving, and/or there is a significant likelihood that the tree will topple or otherwise fail and cause damage before a tree removal permit could be obtained through the non-emergency process.
- b) **Hazard Tree Removal.** The tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. A hazard tree is a tree that is physically damaged to the degree that it is clear the tree is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within a public right-of-way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated.
- c) **Dead Tree.** The tree is dead. A dead tree is lifeless. Such evidence of lifelessness may include unseasonable lack of foliage, brittle dry branches, or lack of any growth during the growing season.

Replacement and Stump Removal. Applicants for approved Street Tree Removal Permits are required to remove any stumps and replace the tree. Stump removal and replacements for approved street tree removals shall meet the following requirements.

1. Any street tree removed shall be removed at ground level or lower. If a tree is removed below ground level, the surface must be restored to finish grade and any regrowth which occurs shall be promptly removed.
2. All street trees shall be an appropriate species selected from and planted according to the City of Ashland Recommended Street Tree List.
3. The minimum size for a replacement tree is eight feet in height or one inch in caliper measured at 12 inches above the root crown.
4. Applicants for a Street Tree Removal Permit may be required to replace the tree or trees being removed with a tree or trees of comparable value.
5. If a street tree is determined to be dead or dying, then the replacement need be no larger than the minimize size described above.

Type of Tree(s) Raywood ash (x2), live oak (x1), and smoke tree (x1)

Approximate Diameter at breast height See Exhibit A "Summary" Height See Exhibit A "Summary" Canopy See Exhibit A "Summary"

Location of Tree Planter strip along northern property line

Reason for Request See Exhibit A "Reason for Request"

Are there underground utility lines and/or overhead power lines present? yes

If yes, please list which lines are present underground power lines

Is there sidewalk damage? no If yes, has a Public Works permit been issued? _____

OVER ►►

DESCRIPTION OF PROPERTY

Street Address 130 N. Pioneer St., Ashland, OR 97520
Assessor's Map No. 39 1E 09BA Tax Lot(s) 11500
Zoning C-1, detailed site review zone 5 Comp Plan Designation Commercial

PROPERTY OWNER

Name City of Ashland Phone 541 488 5587 E-Mail kaylea.kathol@ashland.or.us
Address 20 E Main St City Ashland Zip 97520
Name _____ Phone _____ E-Mail _____
Address _____ City _____ Zip _____

PROFESSIONAL PERFORMING THE TREE REMOVAL (e.g., tree service)

Name See Attachment A "Professional Performing Tree Removal" Phone _____ E-Mail _____
Address _____ City _____ Zip _____

ARBORIST, LANDSCAPE ARCHITECT, OTHER

Title landscape architect Name Covey Pardee Landscape Architects Phone 541 552 1015 E-Mail greg@coveypardee.com
Address 295 E Main St City Ashland Zip 97520
Title landscape architect Name Covey Pardee Landscape Architects Phone _____ E-Mail alan@coveypardee.com
Address _____ City _____ Zip _____

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner. I hereby certify that the statements and information contained in this application are in all respects, true and correct. I further understand that if this request is subsequently contested, the burden will be on me to establish:

- 1) *that I produced sufficient factual evidence to support this request;*
- 2) *that the information contained in this application are adequate; and further*
- 3) *that all trees, structures, or improvements are properly located on the ground.*

Property Owner's Signature (required)

Date

STAFF DECISION:

Permit is hereby (circle one): Approved Approved with Conditions Denied

Conditions of Approval _____

Is the tree 18" d.b.h or greater? NO YES Has the City Administrator has been notified: NO YES

Community Development Director/Planning Manager Signature Date

RECEIVED
SEP 05 2018
City of Ashland

Exhibit A: Supplemental Information for Tree Removal Permit Application

City of Ashland Engineering Division Project No. 2017-03

Summary

Public Works proposes the removal of four trees located in the planter strip along the northern property line of the parking lot at the corner of N. Pioneer Street and Lithia Way. The trees of concern are summarized in TABLE.

Type	DBH (inches)	Height (feet)	Canopy Width (feet)
Raywood ash	12	25	20
Live oak	12	30	15
Raywood ash	14	30	25
Smoke tree	4	10	8

Reason for Request

City Council has directed Public Works to address the concerns of a Stan Potocki, who owns property adjacent to the City-owned parking lot at the corner of N. Pioneer Street and Lithia Way. Mr. Potocki has reported to Council that the inadequate fence and landscaping in the planter strip separating his parcel from Pioneer Parking Lot does not sufficiently protect his property from trespassing, loitering and littering, nor does it provide a sufficient visual or sound screen of unsavory activities that occur in the City's parking lot. The Council Communication and Mr. Potocki's complete report is provided in **Exhibit B**.

Staff commissioned Covey Pardee Landscape Architects (CPLA) to design a masonry wall, landscape, and lighting solutions to protect Mr. Potocki's property and privacy and discourage loitering, camping, public urination, etc. within the planter strip. While many of the existing trees in the planter strip can be salvaged, the root systems of the four identified above are located directly within the (substantial) foundation of the masonry wall. There is no way to salvage these four trees without compromising the structural integrity of the wall, so they must be removed and replaced. CPLA's plans, as well as the structural plans provided by CPLA's structural engineering subcontractor, are provided in **Exhibit C**.

Professional Performing Tree Removal

Public Works will not award this project until it is able to secure authorization to remove the four trees described above. Because this project will involve several complex components, including demolition, electrical work, masonry work, and landscaping, the City will award the project to a prime contractor who will subcontract tree removal to a professional arborist.

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SEP 05 2018

City of Ashland

Council Communication

January 9, 2017, Study Session

Discussion of approaches to behavior issues at the Lithia Way/Pioneer Street parking lot

FROM:

City Administration, administration@ashland.or.us

SUMMARY

Councilor Marsh requested a study session discussion of behavior issues in the City-owned parking lot at Lithia Way and Pioneer Street, as described in a letter to the Council by a neighboring property owner. The described issues include open consumption of alcohol, smoking, dog fighting, public urination and large numbers of people congregating in parking spaces.

BACKGROUND AND POLICY IMPLICATIONS:

In an October 26, 2016, letter to the Council, Stan Potocki, the owner of the property abutting the City-owned parking lot at Lithia Way and Pioneer Street, described a number of issues in the parking lot that are negatively affecting his property and the use of the parking lot itself. His letter requested that the City take a number of actions to address these issues, including:

- Extend the boundaries of the downtown smoking ban to include the parking lot.
- Enact an ordinance that limits the use of the parking lot to actively parking.
- Eliminate landscaping buffer areas (specifically the 5' buffer along the northern property line).
- Increase police patrols in the parking lot.
- Install better lighting and video surveillance.

The City has previously discussed means by which to mitigate the impact of the parking lot on Mr. Potocki's property and the following actions are the planning/estimating phase of development:

- Replace wooden fence along northern boundary line with an 8' concrete or block wall.
- Replace the pedestrian lights along the northern boundary with high-intensity LED cobra head lamps.
- Improve draining in the landscape strip along the northern boundary.

COUNCIL GOALS SUPPORTED:

N/A

FISCAL IMPLICATIONS:

Designs and costs of the mitigation projects described above are still being developed.

STAFF RECOMMENDATION AND REQUESTED ACTION:

N/A. This item is for discussion only.

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City of Ashland



CITY OF
ASHLAND

SUGGESTED MOTION:

N/A

ATTACHMENTS:

October 26, 2016 letter from Stan Potocki

RECEIVED

SEP 05 2018

City of Ashland



VOCATIONAL RESOURCE CONSULTANTS, LLC
P.O. Box 217, Ashland, Oregon 97520 (541) 482-8888

October 26, 2016

City of Ashland

- Re: (1) Please extend the downtown smoking ban to the City of Ashland parking lot (Lithia Way, Pioneer St.) as soon as possible.
(2) Please enact ordinances as soon as possible to curb the significant and daily misuse of the parking lot.

Greetings,

To those of you who do not know me, I will provide a very brief introduction. My name is Stan Potocki. I have resided in Ashland for almost 40 years.

I am the manager of a vocational rehabilitation company. I own the property at 150 N. Pioneer St. which is directly adjacent to the City of Ashland parking lot at the corner of Lithia Way and Pioneer St.

There are two homes on the property. My office is located in the downstairs floor of the front house.

While preparing this letter I realized that I likely have the single most first-hand knowledge of what takes place in the parking lot. This is because my ground floor office window, where I have worked for the past 27 years, looks out directly to the parking lot.

I have never seen the parking lot be as bad as it is. What goes on daily is exponentially worse than ever before. This has unfolded over the past few months.

A large part of that is attributable to the downtown smoking ban.

I am in the process of working with the City of Ashland in order to mitigate the impact of the parking lot to my property next door. That is essentially a separate issue. I believe that there are a number of things that can be done to "reclaim" the parking lot.

I recognize that it would not be possible for each person reading this letter to fully grasp what transpires in the parking lot. I will therefore provide very specific concrete information. I also believe that a picture is worth a thousand words. Therefore, I have attached some photographs taken over a couple of weeks during this past month.

This involves a combination of the following: drug and alcohol use; public drinking with empty

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SEP 05 2018

City of Ashland

alcohol containers strewn throughout the parking lot; very loud arguments laced with expletives; public urination along the fence line; people sleeping in the parking lot; people hopping the fence, drinking alcohol, and leaving empty alcohol containers in my backyard; large groups of people and dogs literally occupying the handicap parking spaces; people placing their jackets on the handicap parking signs as coat hangers; people sitting in and occupying other parking space areas; car repairs on multiple vehicles being performed, groups of 15 to 20 people congregating in the middle of the parking areas with dog fights and screaming, etc.

Please take a moment to briefly look at the photographs which I have attached as I believe that will help to provide better understanding of daily problem issues.

I will provide specific recommendations which will greatly improve matters as follows:

- (1) Please extend the downtown smoking ban to the city owned parking lot. The recent ordinance has driven a large number of individuals to the parking lot. That has now suddenly become a large congregation place.
- (2) Please enact an ordinance that limits use of the parking lot to **actively** parking vehicles (i.e., driving into the parking lot, parking a vehicle, leaving the parking lot). This is a compelling common sense factor. A parking lot is intended to.....park vehicles. That is the very purpose of its existence. When you drive to the airport and enter the main terminal area, there is an **active** pickup and drop off area for common sense reasons. Common sense ordinance: 2 or 3 minutes to come and go while parking.
- (3) Please eliminate the landscaping buffer areas. They have evolved into havens for public drinking, drug use and loitering. With the current ordinances, my understanding is that the police essentially have their hands tied to a very significant degree. In contrast, my understanding is that the police can enforce not having the actual parking spaces occupied. One specific solution therefore simply involves eliminating areas which cannot be fully enforced (i.e. landscaping areas). A landscaping area is intended to be a buffer. Instead, it is now completely the opposite. Those landscaping areas are now used for sleeping, drinking beer, public urination, etc. It would be profoundly better to have concrete and an area that can be enforced.
- (4) Please specifically eliminate the 5' landscaping buffer on the north side of the parking lot right next to my property. For reasons referenced, the landscaping buffer is actually a tremendous detriment.
- (5) Please provide better funding for the police department so that they can increase downtown patrol. The police officers and the chief of police with whom I have interacted are very pleasant and professional. I appreciate that they are, from my viewpoint, stretched thin and limited in what they can do due to current ordinances.
- (6) Please install better lighting and video cameras in the parking lot. I am in the process of following up with city administrative personnel regarding mitigation involving replacing the 27 year old wooden fence with a wall. It would be ideal

for the police to be able to remotely monitor and/or view criminal activities by having some video cameras and better lighting.

In summary, a parking lot should be a parking lot, not a public park. Without question, the City of Ashland cannot want to have handicap parking spaces literally occupied by groups of people. Beer bottles strewn throughout the landscaped areas and in the parking lot create a very negative impression. A lack of consequence for this condones ongoing negative criminal behavior.

Again, a common sense factor, if groups of people congregate, eat food, and drink beer or other alcohol, there absolutely will be issues involving public urination or defecation. I cannot tell you the number of times that individuals walk right up to the fence line, 5' from my office window and pee on the fence. The City of Ashland truly needs to regulate and take responsibility for what it allows to take place in the parking lot which it owns. It has gotten to the point of being truly ridiculous.

I firmly believe that the specific recommendations I have provided will drastically improve matters for the community.

For those of you who do not know me, I would simply like to note that I absolutely would not be taking the time to write if the situation had not degenerated to such a level.

I am blessed to be busy at work and I have a family and a busy life. I would much prefer not writing. But, there is no practical alternative other than watching a bad situation continue to become worse.

Please contact me (541-890-4339) if you have questions regarding any of the information I have outlined. Please take a moment to see the attached photographs which depict daily parking lot problem issues during this past month.

Sincerely,



Stan Potocki

cc: Ashland City Council, Greg Lemhouse, greg@council.ashland.or.us; Pam Marsh, pam@council.ashland.or.us; Michael Morris, mike@council.ashland.or.us; Rich Rosenthal, rich@council.ashland.or.us; Carol Voisin, carol@council.ashland.or.us; Stefani Seffinger, steffani@council.ashland.or.us
City of Ashland Mayor: John Stromberg, john@council.ashland.or.us; Dave Kanner, dave.kanner@ashland.or.us; Tighe O'Meara, tighe.omeara@ashland.or.us; Bill Molnar, bill.molnar@ashland.or.us; Mike Faught, faughtm@ashland.or.us
Planning Commission: Troy Brown, Jr., tbrownpc@gmail.com; ; Deborah Miller, hmiller@jeffnet.org; Melanie Mindlin, sassetta@mind.net; Haywood Norton,

flnorton527@gmail.com; Roger Pearce, pearcer22@gmail.com; Lynn Thompson,
lkthompson@hotmail.com

sp\$ltr city of ashland

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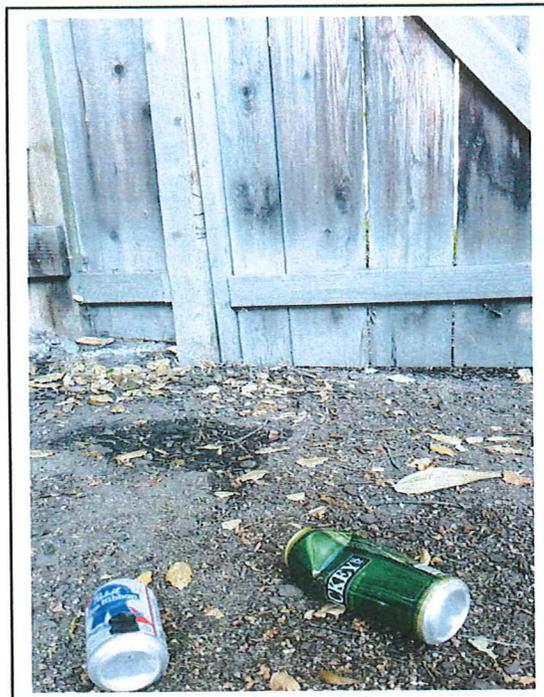
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City of Ashland

October 7, 2016: Handicap parking space area occupied on the corner of Lithia Way and Pioneer Street. Disabled individuals are not going to pull into that area and ask people to leave.

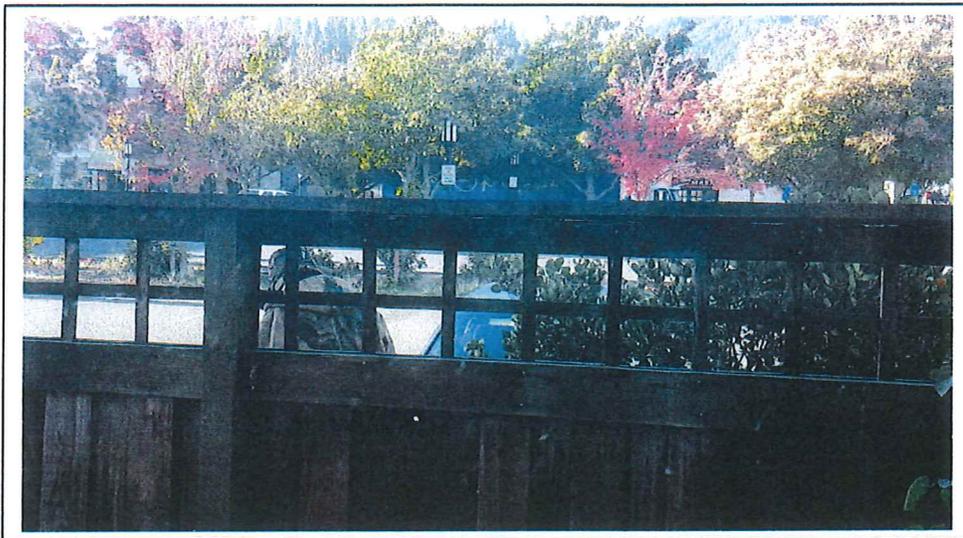


October 8, 2016: Empty alcohol containers in my backyard, trespassing involving people hopping the fence to drink in my backyard, both a trespassing and a safety issue.

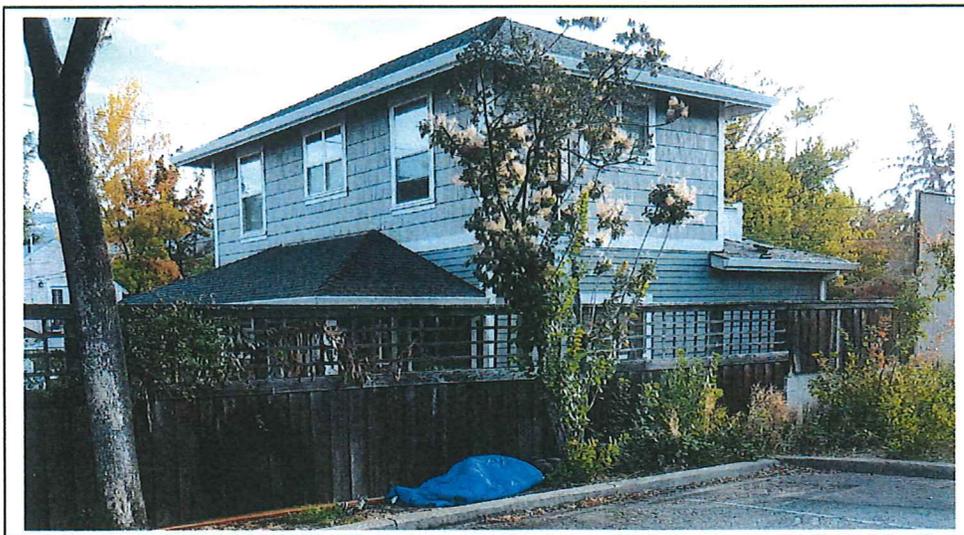


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October 10, 2016: View from my backyard to parking lot; this person just finished peeing on the fence and is walking back to main parking lot area.



October 12, 2016: Person sleeping in landscaping buffer area right next to my property.



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October 14, 2016: Empty alcohol containers in my backyard below kitchen window, people trespassing onto property from parking lot and actually drinking in my backyard (police report, trespassing, attached).



October 17, 2016: People congregate and take up parking spaces.



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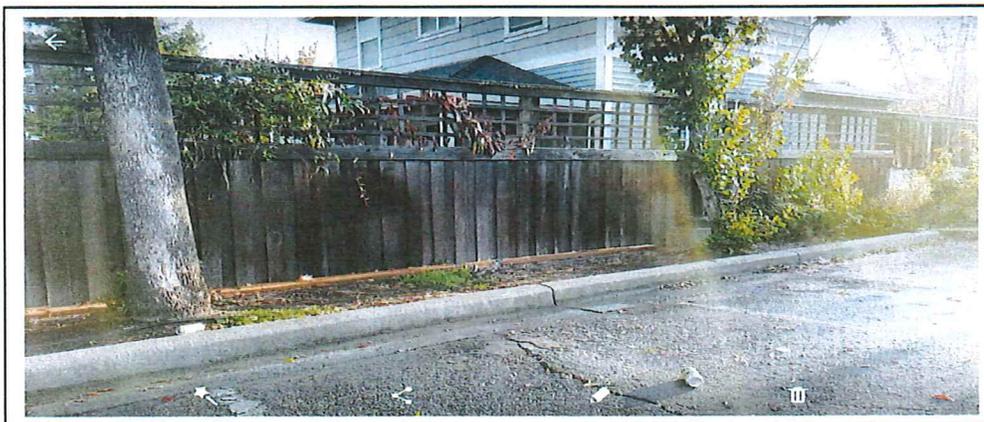
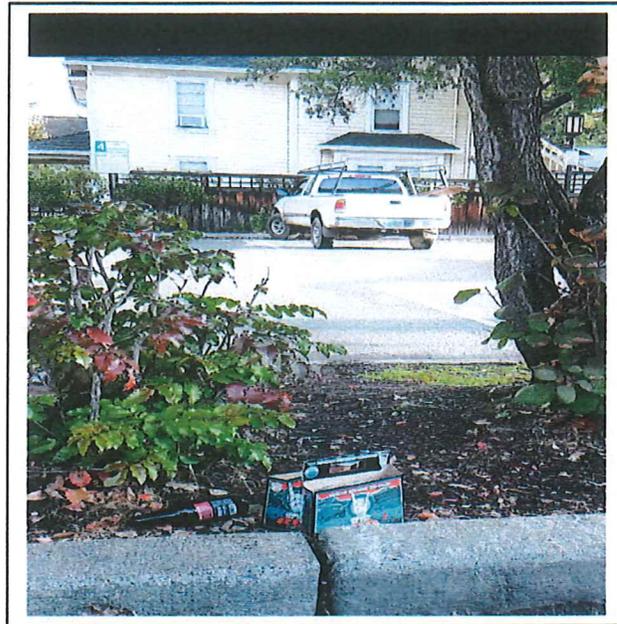
October 18, 2016: People occupying handicap parking space with gear and pets. Person on the left is juggling.



October 19, 2016: People occupying parking spaces, car hoods open and vehicle repairs are in progress.



October 20, 2016: Beer cans strewn throughout parking lot, beer can and clothing left next to the handicap parking area.



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Detailed History for Police Event #J162890474 As of 10/21/2016 15:00:02

Output for: A50355

Priority:4 Type:TRES - Trespass
 Location:150 N PIONEER ST, AS btwn LITHIA WAY and B ST
 Map:5627B

Created:	10/15/2016 14:31:06	CAD16	EC1267
Entered:	10/15/2016 14:34:02	CAD16	EC1267
Dispatch:	10/15/2016 14:34:25	CAD08	EC1234
Enroute:	10/15/2016 14:35:33	AM3	A45185
Closed:	10/15/2016 15:20:49	AM3	A45185

ICUnit: PrimeUnit:515 Dispo:NR Type:TRES - Trespass
 Agency:APD Group:APD Beat:APD Block:A50001 Detail

14:31:06pdt CREATE Location:150 N PIONEER ST, AS Type:TRES Name:POTOCKI,STAN RPaddr:2020 CRESTVIEW DR, AS Phone:541/890-4339 Group:APD Area:A50001 TypeDesc:Trespass LocDesc:btwn LITHIA WAY and B ST Priority:2 Agency:APD Map:5627B LocType:S ReqCont:YES ContType:BY PHONE

14:34:02 ENTRY Urgency:None-->R Priority:2-->4 Response:None-->APIPAT Comment:PEOPLE ARE TRESPASSING ON COMPS PROPERTY DURING THE NIGHT AND DRINKING IN THE BACKYARD AND LEAVING BEHIND TRASH. LOC IS COMPS BUSINESS.

14:34:02 -PREMIS Comment:PPR

14:34:04 NOMORE

14:34:17 SELECT

14:34:17 -SGGEST Unit:514 Comment:Standard

14:34:25 DISP [515](#) Operator:A45185 OperNames:EVANS, LISA

14:34:25 -PRIU [515](#)

14:35:33 *ENRTE [515](#)

15:11:13 *MISC [515](#) Comment:advised where the transients are drinking, down the ally way on the back left corner of the house. ex pat

15:20:49 *CLEAR [515](#) Dispo:NR

15:20:49 -CLEAR

15:20:49 *CLOSE

CONTACT INFO:

Name	Phone	RPaddr	RP-Dob	ReqCont	ContType	AltPhone
POTOCKI,STAN	541/890-4339	2020 CRESTVIEW DR, AS		YES	BY PHONE	

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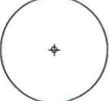
PIONEER PARKING LOT
BOUNDARY FENCE IMPROVEMENTS
130 NORTH PIONEER STREET
ASHLAND, OREGON 97152

ISSUE/REV.	DATE
90% CD REVIEW	04/13/18

SHEET TITLE	
EXISTING CONDITIONS & DEMOLITION PLAN	
DATE	13 APRIL 2018
DRAWN	ADP
CHECKED	GTC
JOB	Pioneer Parking Lot

SHEET
L101
FILE

LEGEND

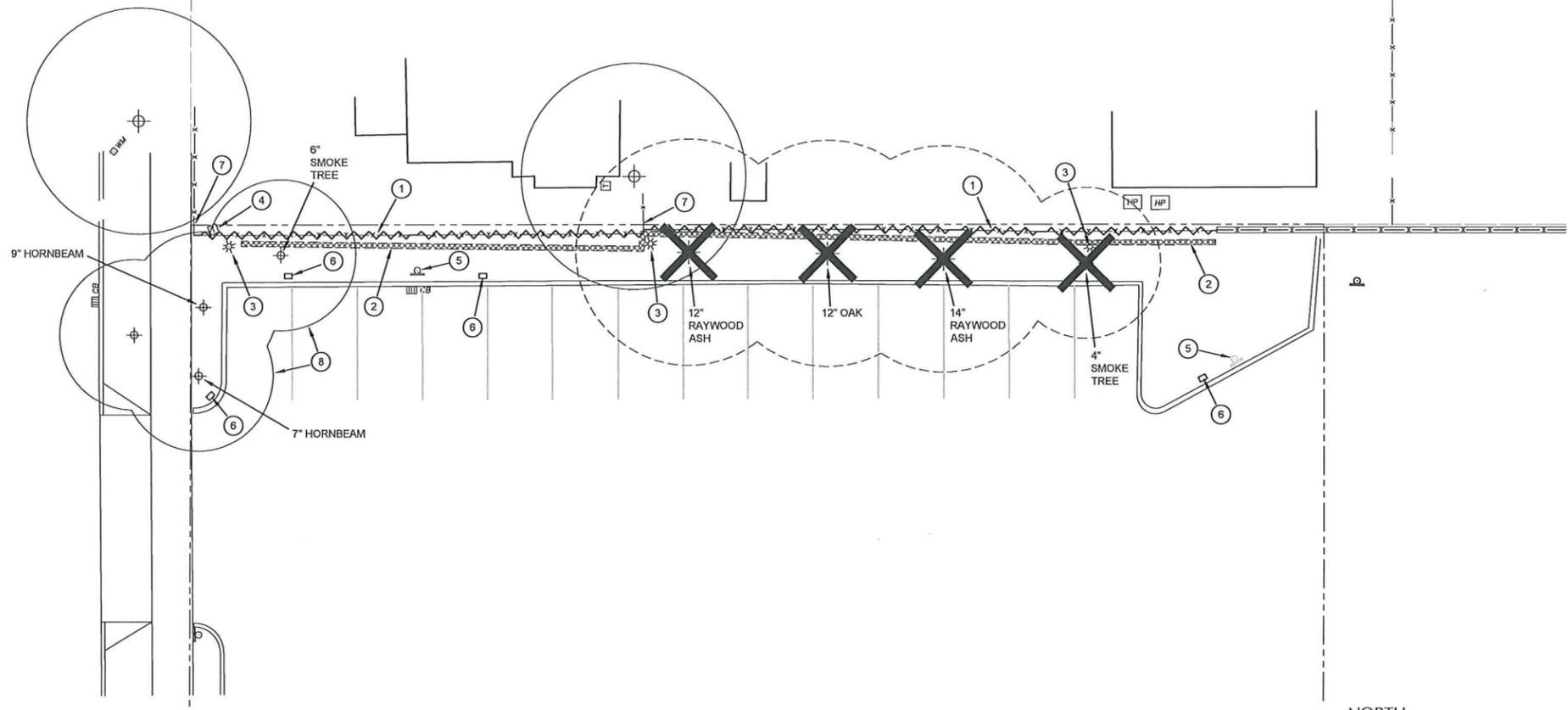
-  REMOVE FENCE
-  REMOVE RETAINING WALL
-  EXISTING TREE TO REMAIN
-  REMOVE EXISTING TREE

GENERAL NOTES

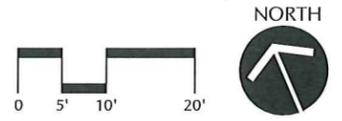
- A. VERIFY LOCATIONS OF ALL BELOW GRADE UTILITIES PRIOR TO BEGINNING WORK. CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL UTILITY LOCATES.
- B. TOPOGRAPHIC SURVEY PROVIDED BY POLARIS LAND SURVEYING, LLC.
- C. PROTECT ALL EXISTING TREES TO REMAIN.
- D. REMOVE ALL SHRUBS FROM NEW PLANTING AREAS.
- E. REMOVE EXISTING IRRIGATION VALVES, HEADS, AND PIPE NOT SCHEDULED FOR REUSE. DELIVER SALVAGEABLE EQUIPMENT TO PARKS DEPARTMENT.

KEY NOTES

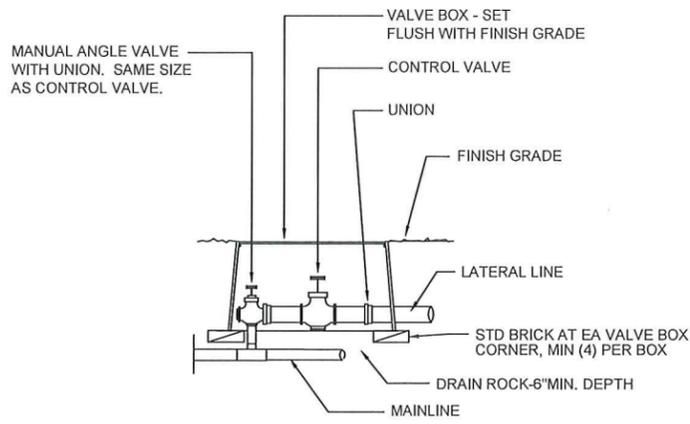
- ① REMOVE WOOD FENCE AND FOOTINGS.
- ② REMOVE RETAINING WALL AND FOOTINGS.
- ③ REMOVE LIGHT POLE AND FOOTING, AND DELIVER TO CITY OF ASHLAND (COA).
- ④ RELOCATE TELEPHONE PEDESTAL. COORDINATE WITH PHONE COMPANY.
- ⑤ EXISTING SIGN TO REMAIN.
- ⑥ REMOVE PLAQUE AND DELIVER TO COA.
- ⑦ FENCE TO BE REMOVED BY ADJACENT PROPERTY OWNER AS REQUIRED FOR NEW CONSTRUCTION. COORDINATE WITH COA.
- ⑧ EXISTING TREES TO REMAIN. (PROTECT THROUGHOUT PROJECT.)



EXISTING CONDITIONS & DEMOLITION PLAN 1
1" = 10' @ 22x34"

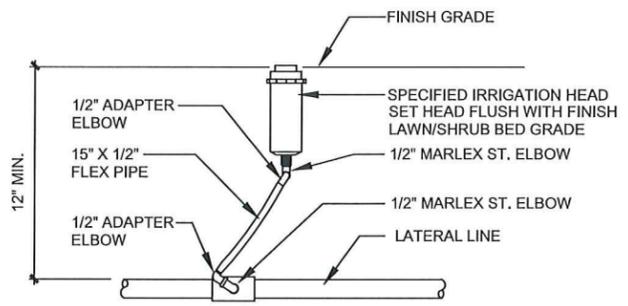


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City of Ashland

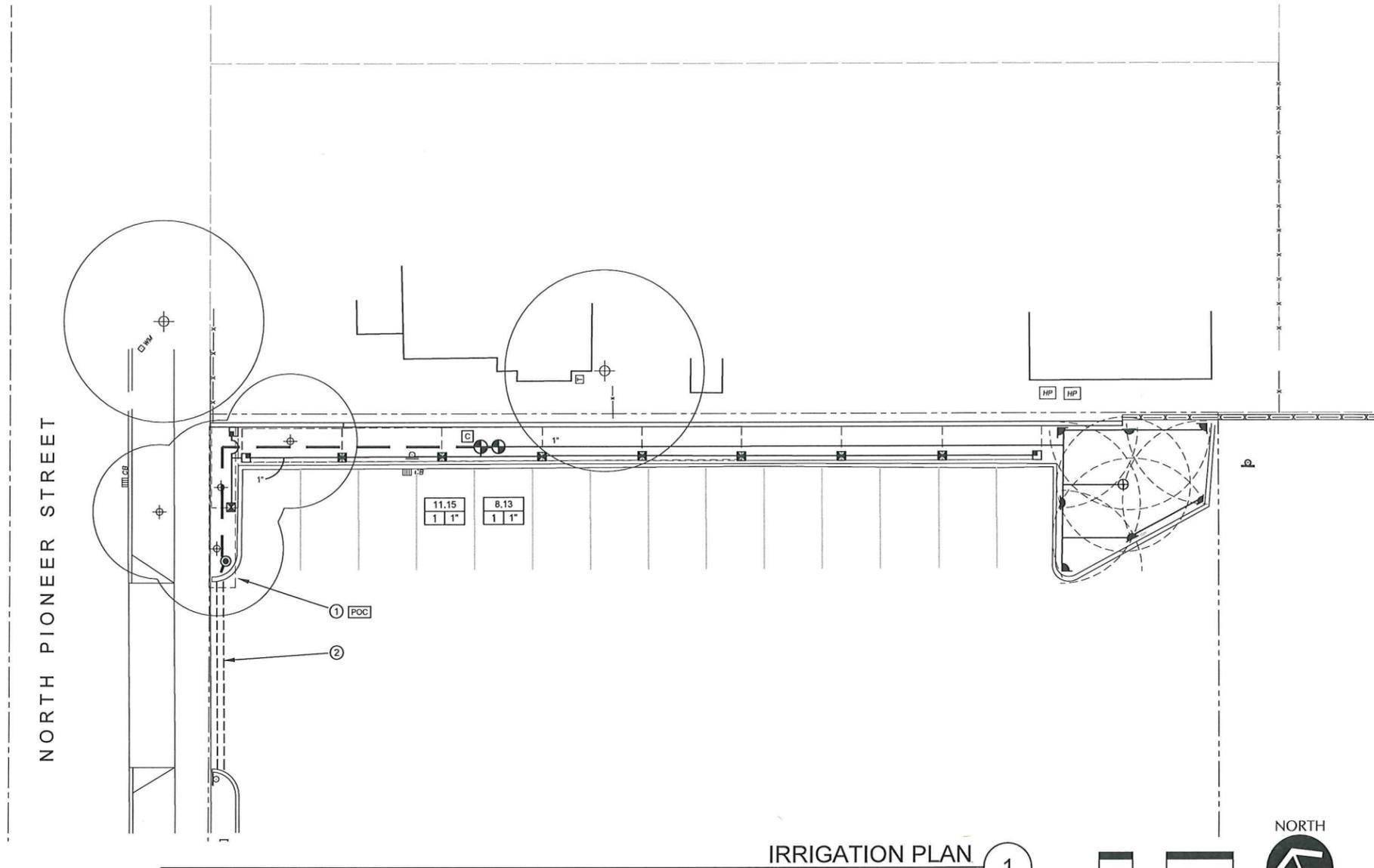


1 IRRIGATION CONTROL VALVE
- NOT TO SCALE

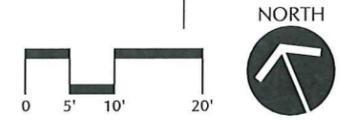
NOTE:
INSTALL IRRIGATION HEADS PERPENDICULAR TO THE SLOPE OF THE FINISH GRADE.



2 SPRAY HEAD ASSEMBLY
- NOT TO SCALE



IRRIGATION PLAN 1
1" = 10' @ 22x34"



IRRIGATION LEGEND

SYMBOL	RADIUS	ARC	GPM	PSI	MODEL
	8'-12"	90°	0.31	45	Rain Bird 1800-SAM-P45 Series w/ R-VAN14 Nozzle (8'-14"). See notes for pop-up size.
	8'-12"	120°	0.41	45	"
	8'-12"	180°	0.61	45	"
	10'-12"	360°	1.96	45	Rain Bird 1800-SAM-P45 Series w/ R13-18F Nozzle (13'-18"). See notes for pop-up size.
	4x15'	15LCS	0.49	30	Rain Bird 1800-SAM-PRS w/ 15 Strip Series MPR Nozzle. See notes for pop-up size.
	4x15'	15RCS	0.49	30	"
	4x30'	15SST	1.21	30	"
SYMBOL	DESCRIPTION				
	RAIN BIRD PEB-PRS-D SERIES CONTROL VALVE				
	MAIN LINE ISOLATION VALVE, AS SPECIFIED				
	MAIN LINE, SCH 40 PVC (MINIMUM 1" DIA. UNLESS OTHERWISE NOTED)				
	LATERAL LINE, SCH 40 PVC.				
	SLEEVE, SCH 40 PVC, MIN. 4" DIA. UNLESS OTHERWISE NOTED. COORDINATE WITH GENERAL CONTRACTOR.				
	POINT OF CONNECTION: Connect to existing irrigation waterlines at location indicated, coordinate with Owner.				
	RAIN BIRD TBOS-II TWO STATION BATTERY OPERATED CONTROL SYSTEM				

GENERAL NOTES

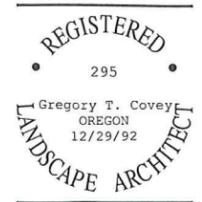
- A. The landscape contractor shall inspect the site and verify conditions and dimensions prior to construction.
- B. Install irrigation system to comply with the codes and ordinances of all jurisdictional agencies.
- C. Irrigation plans are schematic. Place irrigation lines in common trench whenever possible. Field adjust lines to avoid conflict with utilities.
- D. Verify backflow prevention device is operational and has been approved by the appropriate authority.
- E. All valves shall be placed in valve boxes in a manner which facilitates access for maintenance. Locate valve boxes in shrub beds.
- F. All components of irrigation system shall be installed and adjusted to provide complete coverage. Contractor is responsible for providing a complete working system.
- G. Verify minimum static water pressure of 50 psi at point of connection to existing irrigation water line. Notify the Landscape Architect and Owner's representative if actual field data differs from this information.
- H. System is designed to operate with a minimum of 45 psi at the furthest head from the point of connection. Head layout and zones are based on this data, and specifications shown in the irrigation legend. Notify the Landscape Architect and owner if actual field data differs from this information.
- I. Irrigation laterals are sized starting at valve and continuing in direction of flow. Reductions in pipe size are labeled beginning downstream of nearest fitting. All laterals not sized are minimum 1 inch or same size as nearest adjacent pipe.
- J. **Valve Key**

23.0	G.P.M.
7	1"

 Zone number Valve size
- K. Install all irrigation pipe in PVC sleeves below all paved surfaces as specified in Section 328424, Irrigation.
- L. Provide 12" pop-up sprinkler heads for all shrub beds unless otherwise indicated on the plans.
- M. Multi-strand control wire not allowed. Use 14 gauge wire as specified in Section 328424, Irrigation.

KEY NOTES

- ① CONNECT TO EXISTING MAINLINE AT THIS LOCATION AND EXTEND TO NEW VALVES. INSTALL ISOLATION VALVE AT CONNECTION POINT. CHECK OPERATION OF EXISTING QCV AT THIS LOCATION, AND REPORT ANY PROBLEMS TO THE LANDSCAPE ARCHITECT AND OWNER
- ② EXISTING SLEEVE. CHECK INTEGRITY OF SLEEVE AND IRRIGATION LINE WITHIN SLEEVE TO ASSURE IT IS IN PROPER WORKING ORDER. INFORM LANDSCAPE ARCHITECT AND OWNER OF ANY PROBLEMS



CoveyPardee
LANDSCAPE ARCHITECTS
205 EAST MAIN, SUITE 8
ASHLAND, OR 97520
eng@coveypardee.com

PIONEER PARKING LOT
BOUNDARY FENCE IMPROVEMENTS
130 NORTH PIONEER STREET
ASHLAND, OREGON 97520

ISSUE/REV.	DATE
90% CD REVIEW	04/13/18

SHEET TITLE
IRRIGATION PLAN

DATE	13 APRIL 2018
DRAWN	ADP
CHECKED	GTC
JOB	Pioneer Parking Lot

SHEET
L201
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City of Ashland

PIONEER PARKING LOT
BOUNDARY FENCE IMPROVEMENTS
130 NORTH PIONEER STREET
ASHLAND, OREGON 97520

ISSUE/REV.	DATE
90% CD REVIEW	04/13/18

SHEET TITLE
PLANTING PLAN

DATE 13 APRIL 2018

DRAWN ADP

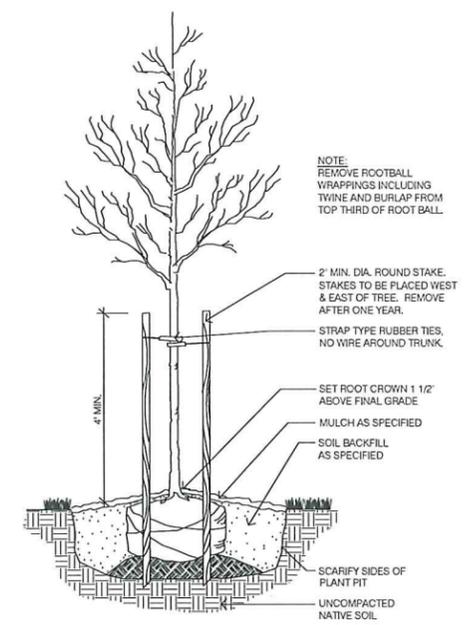
CHECKED GTC

JOB Pioneer Parking Lot

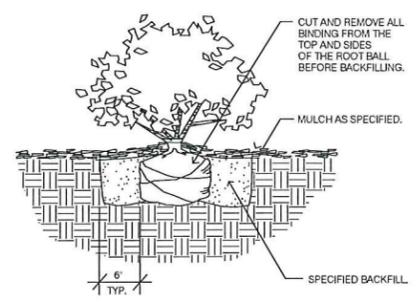
SHEET

L301

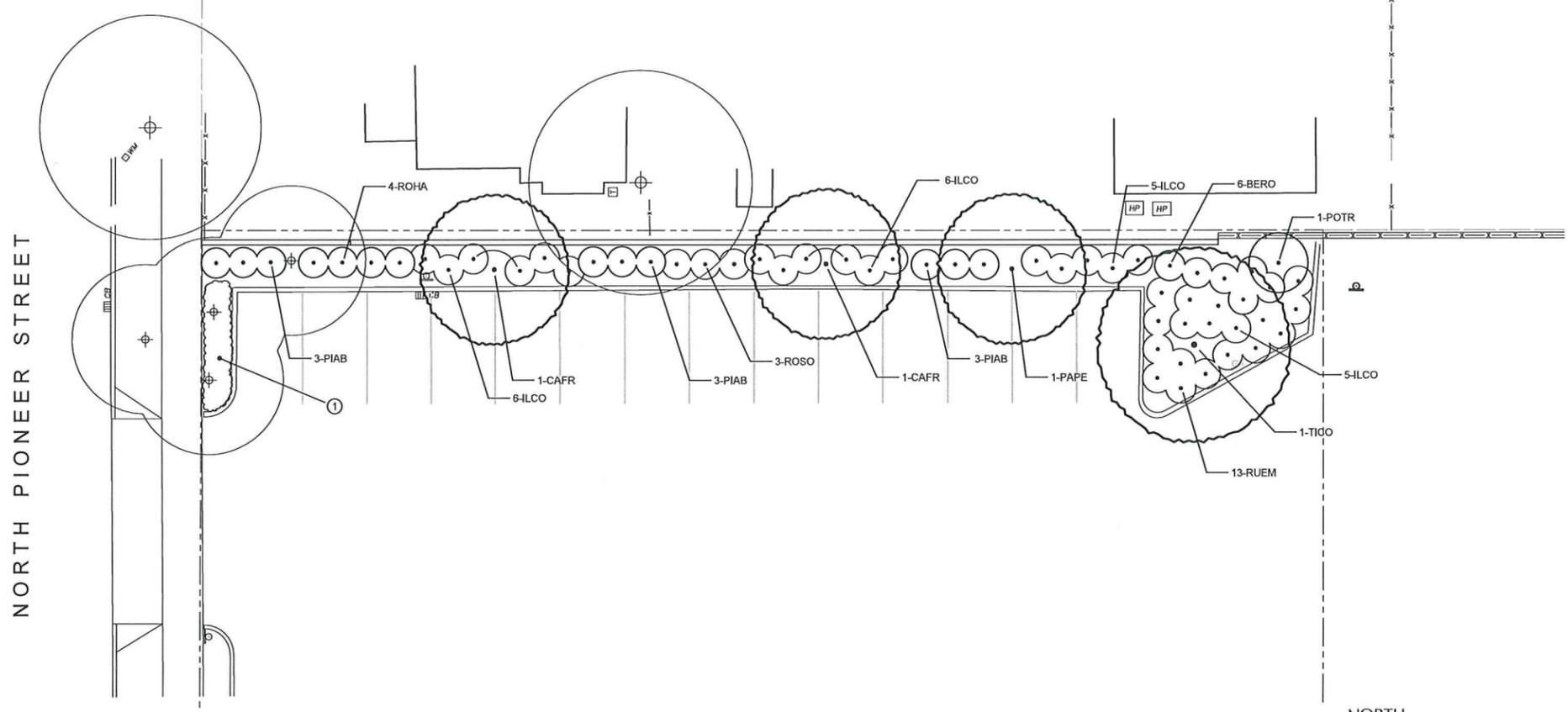
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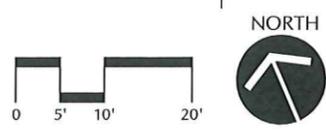
1 TREE PLANTING
NOT TO SCALE



2 SHRUB PLANTING
NOT TO SCALE



PLANTING PLAN 1
1" = 10' @ 22x34"



KEY NOTES

- 1 RETAIN EXISTING TREES (SEE SHEET L101) AND EXISTING MAHONIA 'COMPACTA' GROUNDCOVER.

PLANT LIST

KEY	TREES		
PAPE	PARROTIA PERSICA 'RUBY VASE'	RUBY VASE PERSIAN IRONWOOD	2" CAL B&B
TICO	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN	2" CAL B&B
CAFR	CARPINUS 'FRANS FONTAINE'	'FRANS FONTAINE' HORNBEAM	2" CAL B&B
SHRUBS			
BERO	BERBERIS 'ROSE GLOW'	ROSE GLOW BARBERRY	5 GAL
ILCO	ILEX CORNUTA 'BURFORDI NANA'	COMPACT BURFORD HOLLY	1 GAL
PIAB	PICEA ABIES 'NIDIFORMIS'	BIRD'S NEST SPRUCE	5 GAL
POTR	PONCIRUS TRIFOLIATA	HARDY ORANGE	5 GAL
ROHA	ROSA 'HANSA'	HANSA HYBRID RUGOSA ROSE	5 GAL
GROUND COVERS			
ROSO	ROSA 'SOLERO VIGOROSA'	YELLOW GROUNDCOVER ROSE	1 GAL
RUEM	RUBUS C. 'EMERALD CARPET'	CRINKLE LEAF RASPBERRY	1 GAL

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SEP 05 2018

City of Ashland

July 25, 2018

To Whom It May Concern:

On June 6, 2016, Ashland Fire & Rescue performed a free Firewise Assessment of 898 Morton Street. During this assessment, it was noted that two ponderosa pine trees growing close to the house were recommended to be removed due to their proximity to the deck and house. It was also noted that it is imperative for homes within that neighborhood maintain at least a 10 foot spacing between conifer tree branches and the structures.

898 Morton Street is within the Park Estates Home Owners Association which became Firewise USA recognized in 2017. Recognized Firewise USA neighborhoods are actively working to reduce their wildfire through a grassroots approach. Ashland Fire & Rescue supports proactive wildfire reduction work in this neighborhood.

Please find the attached assessment from 2016 and let me know if you have any questions.

Best,
Alison

Fire Adapted Communities Coordinator
Ashland Fire & Rescue
alison.lerch@ashland.or.us

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City of Ashland



ASHLAND FIRE & RESCUE

455 Siskiyou Boulevard
Ashland, OR 97520
(541) 482-2770 • Fax (541) 488-5318
TTY: 800-735-2900



ASHLAND FIRE & RESCUE HOME IGNITION ZONE ASSESSMENT

Date of Assessment: 6/6/2016 Resident Name: Risa Littman & Ryan Schnobrich

Property Address: 898 Morton Street Phone Number: 805-660-2900

Property Owner: SAME Email Address: risalittman@gmail.com

ASSESSMENT ITEMS	MITIGATION RECOMMENDATIONS
1. OVERVIEW OF SURROUNDINGS	
How is the structure positioned in relationship to severe fire behavior?	The house is situated in the Wildfire Hazard Zone on a hillside adjacent to a heavily wooded drainage. Homes have overlapping Home Ignition Zones (100 ft apart)
Type of construction:	Multi story house with composite siding
Address visible?	Yes
2. CHIMNEY TO EAVES:	
Inspect the roof – noncombustible? Shingles missing? Shingles flat with no gaps? Pitch?	Roof is in great condition.
Gutters – present? Noncombustible?	Present and noncombustible.
Litter on roof, in gutters, and crevices? Tree limbs pruned away from roof?	Trees are pruned away from the roof, however there are several large Ponderosa pine trees whose stumps are within 5-15 feet of the structure.
3. EAVES TO FOUNDATION:	
Attic, eave, soffit vents, and crawl spaces:	All areas are vented.
Inspect windows and screen – metal screens? Multi-paned windows? Picture windows facing vegetation?	Windows and screens are in good condition.
Walls and attachments: noncombustible? Will they collect litter? Planters and hose reels?	Non-combustible walls made of composite siding.
Decks – combustible materials?	½ TREX decking ½ Wood construction.
Fences:	No fences.

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Flammable material next to or under the structure:	The area next to or under the house is mostly free of flammable material. Keep the area underneath the 2 nd story deck free of anything flammable and you will be in good shape.
Combustible materials near or on the structure where walls meet roof or decking surfaces:	There are needles/leaves underneath the deck/stairs near the front door. Although the space is small, it is recommended to put small 1/8 inch screening underneath decks to keep debris out and the potential of embers to get trapped.
Nooks and crannies and other small spaces – condition? Protected?	Protected.
4. FOUNDATION TO IMMEDIATE LANDSCAPED AREA (within 30ft of home):	
Landscaped/managed vegetation – separation distances, maintenance, plant selection? Firewise landscaping zones? Sprinklers recessed in lawn?	The house is free of hazardous ladder fuels like blackberry. However, there are several mature Ponderosa pine trees that are leaning towards the house. It is recommended to remove 2 of the Ponderosa pines that are leaning over the deck and are within 10 feet. In addition, removal all dead trees on the property. A few smaller conifer trees that were planted close to the house are also recommended to be removed. Landowner can revegetate with fire resistant species if looking for a privacy screen from the neighbors. Some of these trees are on the border of the neighbor’s property line.
Propane tanks?	If a grill is present, keep propane tank in off position unless in use. If an evacuation is in order, remove the propane tank from the grill and leave it as far away from your house as possible.
Vehicle and RV use and parking, lawnmowers, etc.:	N/A
5. IMMEDIATE LANDSCAPED AREA TO EXTENT OF THE HOME IGNITION ZONE (within 100ft – 200ft of home):	
Inspect vegetation clearance and crown separation:	The neighborhood is within a mature Ponderosa pine and Douglas fir stand. The continuation of vegetation poses a threat to the homes in the event of a wildfire. Maintaining at least 30 feet of defensible space with no ladder fuels and conifer tree branches pruned at least 10 feet from the home will aid to the homes survivability.
Wood piles at appropriate distance? Dead plants and brush?	No wood piles currently present. If a pile is to be created, keep it at least 30 feet away from the home.
Driveway trimmed and cleared for 14ft?	Driveway is trimmed to 14ft, however it is short. There is a fire hydrant at the turn to the driveway.
Adjacent homes and yards:	Adjacent neighbors are good stewards to their yard.

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City of Ashland

TOPOGRAPHIC SITE SURVEY

LOCATED AT

898 Morton Street
Ashland, Oregon

LYING SITUATE WITHIN

NORTHEAST QUARTER OF SECTION 16,
TOWNSHIP 39 SOUTH, RANGE 1 EAST, W.M.
CITY OF ASHLAND, JACKSON COUNTY, OREGON

FOR

Ryan Schnobrich
898 Morton Street
Ashland, Oregon 97520

LEGEND

- ▲ SURVEY CONTROL POINT, AS DESCRIBED
- IRON PIN MONUMENT
- BRASS CAP MONUMENT
- SUBJECT PROPERTY LINE
- BOUNDARY LINE
- CENTERLINE
- BASEMENT LINE
- FENCELINE
- WATER LINE
- BURIED NATURAL GAS LINE
- BURIED TELEPHONE LINE
- BURIED CABLE TV LINE
- BURIED IRRIGATION LINE
- STORM DRAIN LINE
- SANITARY SEWER LINE
- OVERHEAD POWER LINE
- BURIED POWER LINE
- ROCK WALL
- CONTOUR LINE
- GUY ANCHOR
- PP POWER POLE
- PT POWER TRANSFORMER
- PC POWER PEDESTAL/CABINET
- EM ELECTRIC METER
- HF HEAT PUMP
- ★ AL AREA LIGHT
- WM WATER METER
- WV WATER VALVE
- FH FIRE HYDRANT
- CB CATCHBASIN
- CI CURB INLET
- SM STORM SEWER MANHOLE
- SS SANITARY SEWER MANHOLE
- CN CLEANOUT
- TP TELEPHONE PEDESTAL
- GM GAS METER
- GV GAS VALVE
- IB IRRIGATION BOX
- ▨ CONCRETE SURFACE
- ▨ ASPHALT SURFACE
- ▨ BUILDING
- ▨ COMPOSITE DECKING
- ☼ CONIFER TREE (AS DESCRIBED)
- ☼ DECIDUOUS TREE (AS DESCRIBED)

SURVEY NOTES

THE BASIS OF VERTICAL CONTROL FOR THIS SURVEY IS SURVEY CONTROL POINT 10, BEING A CONCRETE NAIL IN ASPHALT 20' WESTERLY OF THE NORTHWEST PROPERTY CORNER AT 898 MORTON STREET, HAVING AN ASSUMED ELEVATION OF 1023.05'.

EXPOSED UTILITY STRUCTURES SHOWN HEREON WERE FIELD LOCATED DURING THE PERFORMANCE OF THIS SURVEY. BURIED UTILITY LOCATIONS WERE DETERMINED BY UTILIZING A COMBINATION OF FIELD SURVEYED PAINT MARKS AND "AS-BUILT" RECORD DRAWINGS FURNISHED BY THE RESPECTIVE UTILITY COMPANY REPRESENTATIVES, ARE APPROXIMATE AND SHOWN HEREON FOR GRAPHIC PURPOSES ONLY. FIELD VERIFICATION OF ALL BURIED UTILITIES MUST BE PERFORMED PRIOR TO ANY EXCAVATION OR CONSTRUCTION ACTIVITIES.

REGISTERED
PROFESSIONAL
LAND SURVEYOR

OREGON
JULY 14, 1999
SHAWN KAMPMANN
2003 15

RENEWAL DATE: 6/30/2019

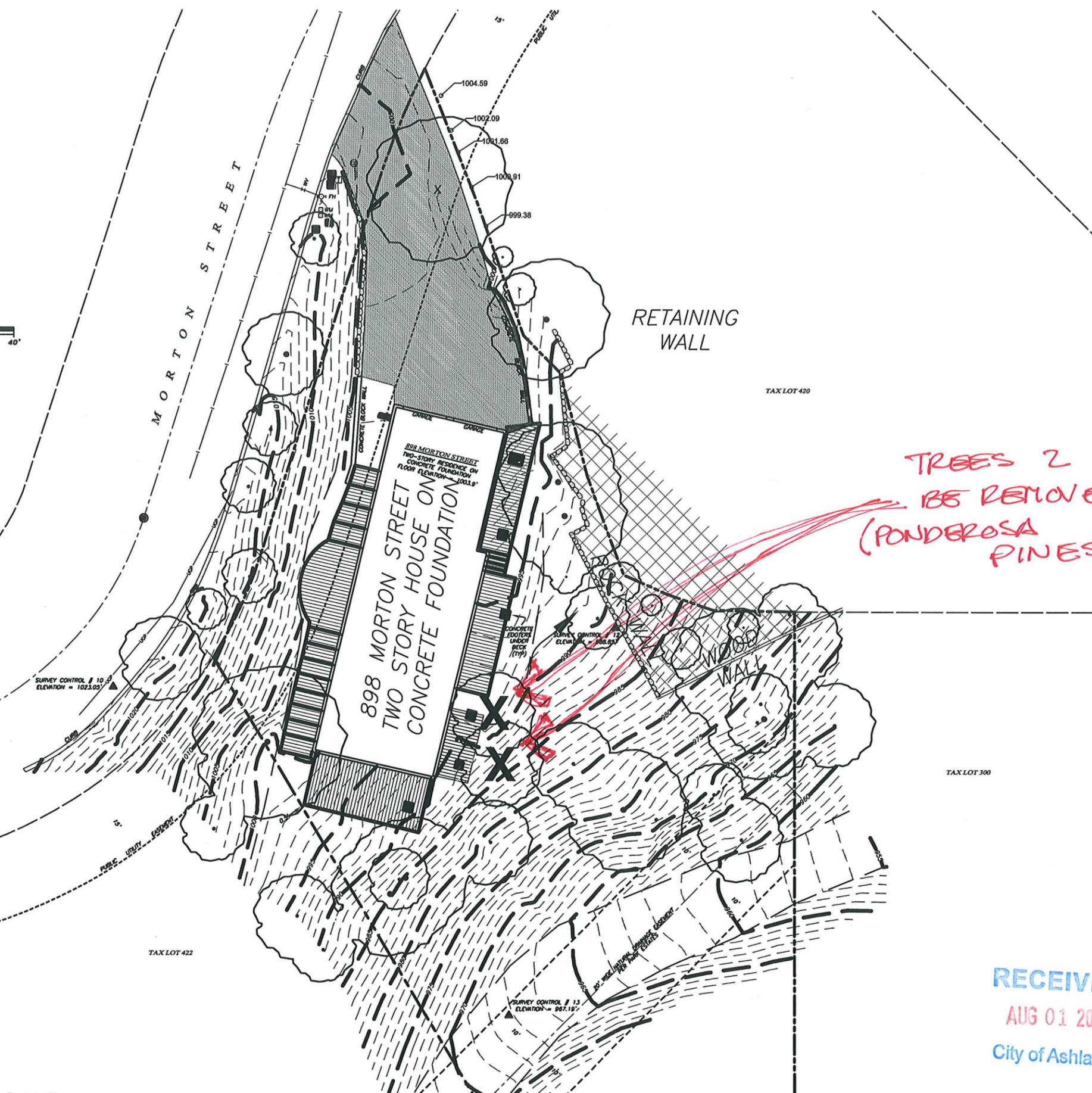
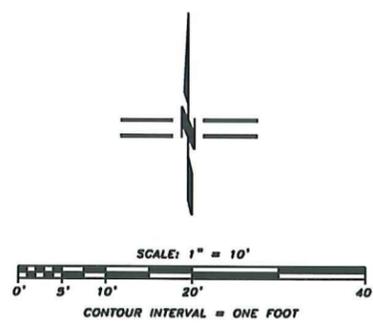
SURVEYED BY:

POLARIS LAND SURVEYING LLC
P.O. BOX 459
ASHLAND, OREGON 97520
(541) 482-8008

DATE: DECEMBER 20, 2017
PROJECT NO. 1045-16

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City of Ashland

POLARIS LAND SURVEYING



Nathan Emerson

From: Kerry KenCairn <kerry@kencairnlandscape.com>
Sent: Wednesday, August 22, 2018 10:26 AM
To: Nathan Emerson
Subject: RE: 898 Morton St Hazard Tree Removal Information

Sur both trees are between 14 and 17 dbh and over 40 feet in height.

From: Nathan Emerson [mailto:nathan.emerson@ashland.or.us]
Sent: Wednesday, August 22, 2018 10:07 AM
To: kerry@kencairnlandscape.com
Subject: 898 Morton St Hazard Tree Removal Information

Hello Kerry,

I'm prepping the notice for the 898 Morton Street Tree removal application.

Can you tell me the approximate DBH and height of the two Ponderosa pines proposed for removal?

Thanks,

Nathan Emerson
Assistant Planner
City of Ashland, Community Development Department
20 E. Main St., Ashland, OR 97520
541.552.2052 Tel 800.735.2900 TTY

This email transmission is official business of the City of Ashland and it is subject to Oregon Public Records Law for disclosure and retention. If you have received this message in error, please contact me at (541) 552-2052. Thank you.



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Comments on Planning Action "Tree-2018-00021"

Provided by Judy Conner and Tom Besich, 880 Morton Street.

It is our understanding that the Ashland Tree Commission has reviewed the proposed removal of two approximately 80 feet tall ponderosa pine trees on the 898 Morton Street property and has found no basis for removing the trees for fire prevention. In fact, the Ashland Tree Commission believes the subject trees present no fire threat at all and only need to be trimmed of their lower branches. Neither the Applicant nor the property owners attended that September 6 Ashland Tree Commission meeting to support their Tree Removal Permit request.

We live on the adjacent property down-hill to the east at 880 Morton. Our request is that the City of Ashland -- in response to the subject application, regardless of whether or not the City grants the tree removal permit -- protect, as "mitigation", three (3) other existing smaller fir trees on the 898 Morton property as identified on the attached map.

During the last winter, the 898 Morton property owners removed a very large [22-23" DBH, 80+ feet tall] fir tree out near the street so they could widen their driveway. We obtained that tree removal permit from the City in August 2016 as part of a settlement. Their present request to remove two (2) more mature trees, both about 80 feet tall ponderosa pines, would make 3 very mature trees they will have removed. Our request is that the City "mitigate" the loss of these trees, and potentially other trees, by protecting three (3) smaller [4-6" DBH, 25-35 feet tall] fir trees as identified on the attached map.

The Applicant for the owners of 898 Morton identifies the two tall ponderosa pine trees as fire hazards to their house. However, the Ashland Tree Commission at its Sept 6 meeting disagreed with this assumption and saw no fire basis for removing the two trees. In support of the alleged fire danger, the Applicant attached a June 6, 2016 "Ashland Fire & Rescue Home Ignition Zone Assessment" report. On the 2nd page of that 6-6-2016 report are three "assessment items" that imply that concern for fire might not be the motivation for removing the two ponderosa pines.

- 1) Under "flammable material next to or under the structure", the 6-6-2016 report says to "keep the area underneath the 2nd story deck free of anything flammable", yet the property owners have filled that area with quite a bit of old wood railing/fencing removed from the original driveway retaining wall. This conflicts with the 6-6-2016 report.
- 2) Under "combustible materials near or on the structure where walls meet roof or decking surfaces", the report says to "put small 1/8 inch screening underneath decks to keep debris out and the potential of embers to get trapped." It does not appear the property owners have done that.
- 3) Also, near the bottom of that 2nd page, under "wood piles at appropriate distance", the 6-6-2016 report says "if a (wood) pile is to be created, keep it at least 30 feet away from the home." Yet, the property owners have a fire wood pile right on top of their upper floor deck by their back door.

Based on these 3 fire prevention problems, the Applicant does not appear to have much credibility regarding fire hazard. Or, perhaps the property owners didn't tell the Applicant.

The three small fir trees identified as "mitigation" appear to be far enough from the house to pose no fire danger, and their branches are trimmed up about 8-10 feet from the ground. Although the property

owners failed to cut the now dried weeds on the slope below the house and subject trees [an annual June 15 deadline?], there is no ladder fuel under any of these trees. Many fir trees were lost about 20-25 years ago due to a bug infestation, so it seems important to retain these trees. It is our understanding that such trees shade the ground and other plants, thereby helping to retain moisture.

The name "Tree Removal Permit" implies that when the trees are cut down, they will be "removed" from the property. We mention this because, as the City's "Notice" states, "the property is steeply sloped" AND the bottom of the immediate slope is a ravine with 2 over-lapping easements: one for "drainage" [20 feet wide] and the other "riparian" [60 feet wide]. Over the years the bottom of that ravine has been collecting lots of logs. We do not know if this is considered a problem [fire, bugs, interference with the easements, etc]. If these 2 large ponderosa pines are merely cut [as 5-foot logs?] and dropped into that ravine, they could add considerably to any existing and/or future problems.

But we recognize that it might be more expensive to instead remove the logs from the property. The Jackson County Fuels ["JCF"] people on Eagle Mill Road at Valley View near I-5 north of town say such logs would have to be cut into 16 inch sections [versus 5 feet] for JCF to be willing to pick them up. Due to this summer's forest fires, JCF says it's now 2 months behind doing such pick-ups, and there's no guarantee JCF would be willing to carry the 16" sections up the slope to the street.

Regarding the "notice of application" format, we recommend that the City drop the wide-scale map and instead return to using an aerial photo of the subject property. The general reaction to this "notice" was "which trees?" Because the "notice" is posted in front of the property and is sent to only those properties within 200 feet, the map is useless compared to the aerial photo.

Respectfully submitted,



Judy Conner

880 Morton Street



Tom Besich

880 Morton Street

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Flammable material next to or under the structure:	The area next to or under the house is mostly free of flammable material. Keep the area underneath the 2 nd story deck free of anything flammable and you will be in good shape.
Combustible materials near or on the structure where walls meet roof or decking surfaces:	There are needles/leaves underneath the deck/stairs near the front door. Although the space is small, it is recommended to put small 1/8 inch screening underneath decks to keep debris out and the potential of embers to get trapped.
Nooks and crannies and other small spaces – condition? Protected?	Protected.
4. FOUNDATION TO IMMEDIATE LANDSCAPED AREA (within 30ft of home):	
Landscaped/managed vegetation – separation distances, maintenance, plant selection? Firewise landscaping zones? Sprinklers recessed in lawn?	The house is free of hazardous ladder fuels like blackberry. However, there are several mature Ponderosa pine trees that are leaning towards the house. It is recommended to remove 2 of the Ponderosa pines that are leaning over the deck and are within 10 feet. In addition, removal all dead trees on the property. A few smaller conifer trees that were planted close to the house are also recommended to be removed. Landowner can revegetate with fire resistant species if looking for a privacy screen from the neighbors. Some of these trees are on the border of the neighbor's property line.
Propane tanks?	If a grill is present, keep propane tank in off position unless in use. If an evacuation is in order, remove the propane tank from the grill and leave it as far away from your house as possible.
Vehicle and RV use and parking, lawnmowers, etc.:	N/A
5. IMMEDIATE LANDSCAPED AREA TO EXTENT OF THE HOME IGNITION ZONE (within 100ft – 200ft of home):	
Inspect vegetation clearance and crown separation:	The neighborhood is within a mature Ponderosa pine and Douglas fir stand. The continuation of vegetation poses a threat to the homes in the event of a wildfire. Maintaining at least 30 feet of defensible space with no ladder fuels and conifer tree branches pruned at least 10 feet from the home will aid to the homes survivability.
Wood piles at appropriate distance? Dead plants and brush?	No wood piles currently present. If a pile is to be created, keep it at least 30 feet away from the home.
Driveway trimmed and cleared for 14ft?	Driveway is trimmed to 14ft, however it is short. There is a fire hydrant at the turn to the driveway.
Adjacent homes and yards:	Adjacent neighbors are good stewards to their yard.

old wood railing/fence now stored under deck
no apparent screening

wood pile is on deck

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Large fir removed in winter 2017-18
for driveway widening
(tree removal permit Aug 29, 2016)

3 small fir trees to be
preserved as "mitigation"

TREES 2
BE REMOVED
(PONDEROSA
PINES)

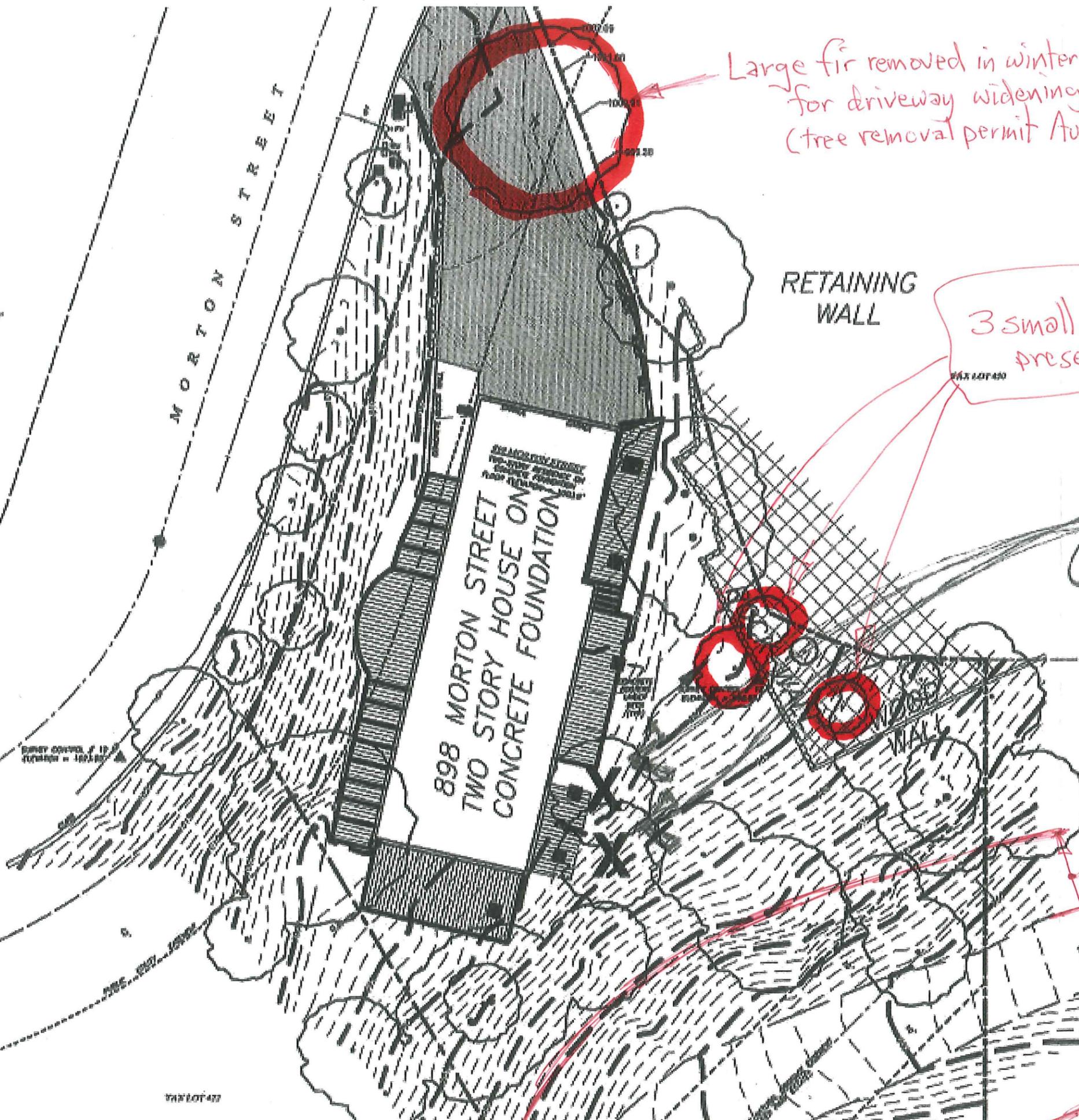
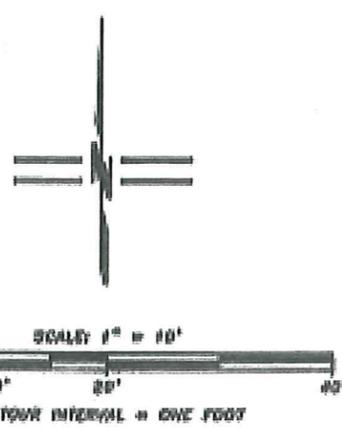
Easements:
"riparian" (60' wide)

"drainage" (20' wide)

898 MORTON STREET
TWO STORY HOUSE ON
CONCRETE FOUNDATION

RETAINING
WALL

MORTON STREET



THIS PLAN
SHOWN IN
ASHELAND
RESPONSE
DURING
WORK IN
PARENT M
RESPECT
SITING
STANDARD
CONCRETE

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Nathan Emerson

From: GEOFFREY BALL <gbrjfs@pacbell.net>
Sent: Thursday, September 13, 2018 9:59 AM
To: Nathan Emerson
Cc: earthstewards@hotmail.com; John Hunt
Subject: Emergency Tree Removal
Attachments: 899 Cypress Point Loop.pdf

Nathan

Having never submitted a permit before please let me know if you need any additional information, application attached. I did not see if there is a submittal fee associated with this application.

We have had two arborist come to inspect the tree. Casey Roland and Peter Lunde. When Casey was here a few weeks ago, I asked that if he would cut 1/3 of the tree to at least protect the house. (the base of the tree is 15-18 feet from the edge of the roof and the tree is over 65 feet tall) So that when part of the main trunk limb falls it will take down a large portion of the home. Casey said that the tree is in such a condition that he would not attempt to take part of it and it needs to be dropped all at one time. Peter Lunde confirmed this as well. Peter's contact number is 541-646-1864 and Casey's phone number is 541-488-0782 and you are welcome to ask them confirming this condition.

As a home owner we are concerned about the safety of our home and since the tree could fall into the common area HOA walking trail we are concerned about public safety of our neighbors as well.

Feel free to contact me if you have any further questions.

Best regards,
Geoff and Bonnie Ball

Picture 1 shows a very recent break in the root structure

Picture 2 shows a very recent crack now forming on 1/3 of the tree which entangles the house

Picture 3 shows the lean of the tree

Picture 4 shows the size of the right main trunk (1/3 of the tree) that puts the house at risk.

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Attached is an application for tree



removal.

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SEP 13 2018

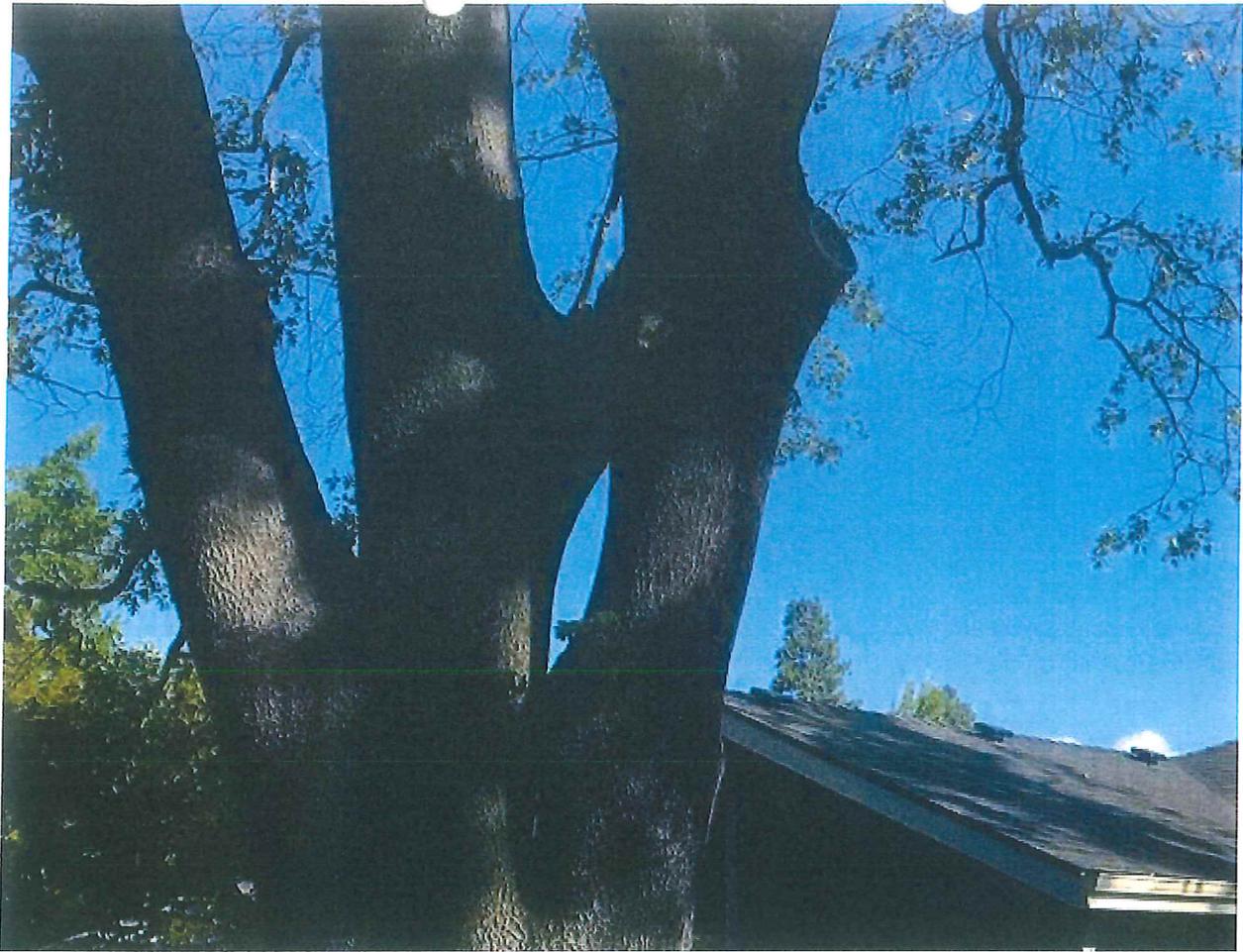
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Earth Stewards Tree Care Specialists

PO Box 807

Talent, OR 97540

541-646-1864

earthstewards@hotmail.com

Location: 899 Cypress Point Loop, Ashland, OR

Species: Black Oak (*Quercus kelloggii*) 40" in diameter

Nature of Work: Request permit to remove diseased tree.

Reason for Removal: Tree has severe case of Phytophthora root disease causing major die back. 1/3 of tree already dead.

Condition: Tree has been declining for years from excessive irrigation around trunk and root area. Tree has severe case of Phytophthora root disease causing major die back of canopy and already killing 1 main trunk about 1/3 of the trees canopy. Decay fungus has been documented on the trees trunk flare and buttress roots. There is evidence of advanced root rot as documented by photographs. Tree canopy extends over private property including house and patio as well as public walking paths used by HOA residents. Targets and extent of disease makes removing the tree the only viable option to keep human life and property safe from harm.

Peter Lunde

Owner Earth Stewards, LLC

ISA Certified Arborist #PN-8425A

CCB# 183967



Applicant's Statement of Completeness

(To be completed by the Applicant and returned to the City of Ashland)

Date Received
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(to be completed by staff)

Re: PA-T2-2018-00003, 188 Garfield St.

Date Application Expires: **January 5, 2019**

Pursuant to an Incompleteness Determination, I, the undersigned applicant or agent for the applicant, elects one of the three options below by initialing:

(APL) 1. **Submit All of the Missing Information**
(Initial if elected)

I am submitting all of the information requested in the Incompleteness Determination letter.

Unless checked below, I am requesting that the City of Ashland Planning Division review this additional information within 30 days of submission to determine whether the application is complete. I understand that this 30-day review for completeness period for the new information preserves my opportunity to submit additional materials, should it be determined that the application is still incomplete after the second review. *(Note: The 120-day period for the City of Ashland's final determination of compliance with applicable criteria does not commence until the additional review for completeness period is completed.)*

Check if desired



I waive further review of the information submitted for completeness and direct review of the information submitted for compliance with the Community Development Code criteria, regardless of whether the application is, in fact, later determined by the staff to be incomplete.

I understand that by checking the above statement the application will be evaluated based upon the material submitted and no notice of any missing information will be given. If material information is missing from the application, the application will fail to meet the burden of showing that all criteria are met, and the application will be denied.

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2. **Submit Some of the Requested Information:**
(Initial if elected) **Decline to Provide Other Information**

I am submitting some of the information requested and declining to submit other information requested in the Incompleteness Determination letter. I understand that by declining to submit all information the City of Ashland believes necessary, the Ashland Planning Division may conclude that the applicable criteria are not met and a Denial will be issued or recommended.

3. **Decline to Provide any of the Requested Information**
(Initial if elected)

I decline to provide any of the information requested. I understand that the Community Development Department may conclude that the applicable criteria are not met and a Denial will be issued or recommended.

Signed and Acknowledged
(Applicant or Applicant's Agent)

Date

Return to:
City of Ashland, Planning Division
Attn: Derek Severson, *Senior Planner*
c/o City Hall, 20 East Main Street
Ashland, OR 97520

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Spartan Properties

MidTown Lofts

kistler+small+white Architects
Covey Pardee Landscape Architects
Rogue Planning & Development Services



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ROGUE PLANNING & DEVELOPMENT SERVICES, LLC

**Site Design Review Approval
for 72-unit loft apartment development**

AMENDED FINDINGS

Property Address: 188 GARFEILD STREET
Map & Tax Lots: 39 1E 10CB Tax Lots: 2100 & 2101

Property Owner: Spartan Properties
66 Water Street, Suite 200
Ashland, OR 97520

Architecture: kistler + small + white Architects
Raymond Kistler
66 Water Street, Suite 101
Ashland, OR 97520

Landscape Architecture: Covey/Pardee Landscape Architects
295 East Main Street, # 8
Ashland, OR 97520

Engineering Services: Marquess and Associates
Jim Higday, Principal
1120 E Jackson Street
Medford, OR 97504

Surveyor: Polaris Land Surveying
PO BOX 459
Ashland, OR 97520

Planning Consultant: Rogue Planning & Development Services, LLC
Amy Gunter
33 North Central Ave. Suite 213
Medford, OR 97520

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Request:

The request is for Residential Site Design Review to allow for the development of the properties located at 39 1E 10CB #2100 and 2101; aka 188 Garfield Street as a 72-unit, studio apartment community. The MidTown Lofts.

A tree removal permit is requested to remove 15 trees that are more than six-inches in diameter at breast height.

An exception to Street Standards is requested to retain the existing curbside sidewalk system with established trees along the frontage of the property. A second exception to Street Standards for the relocation of the driveway apron accessing the site from Quincy Street is required. The proposed driveway apron is 40-feet wide.

Property Description:

The vacant, 2.10-acre property extends the entire block face on the west side of Garfield Street between Iowa Street and Quincy Street. The property has 414.09-feet of frontage on Garfield Street. The property extends 215.16-feet east along the south side of Quincy Street, and 150.80 of frontage on the north side of Iowa Street. See Figure 1 below.

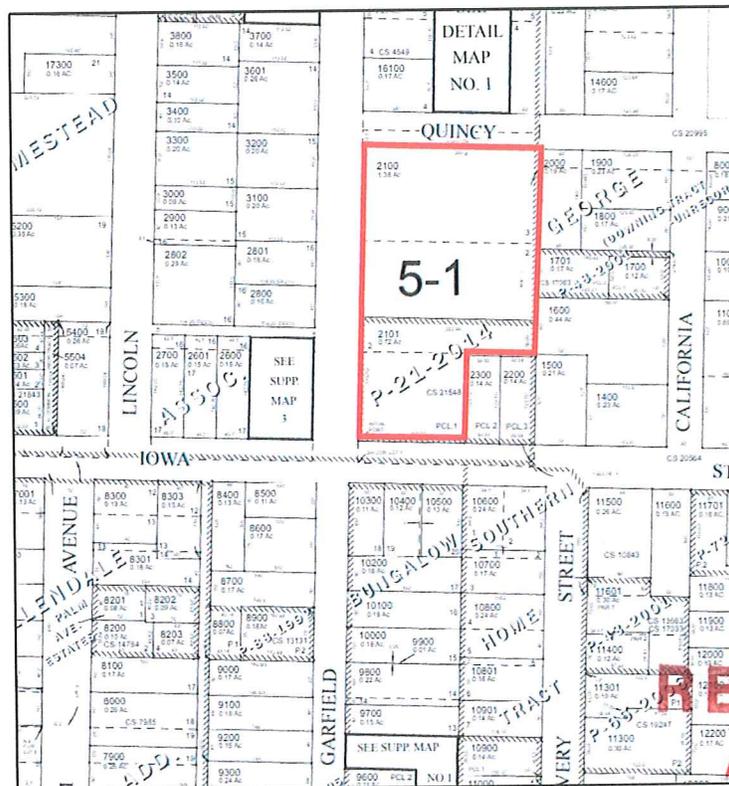


Figure 1: Assessor's Map

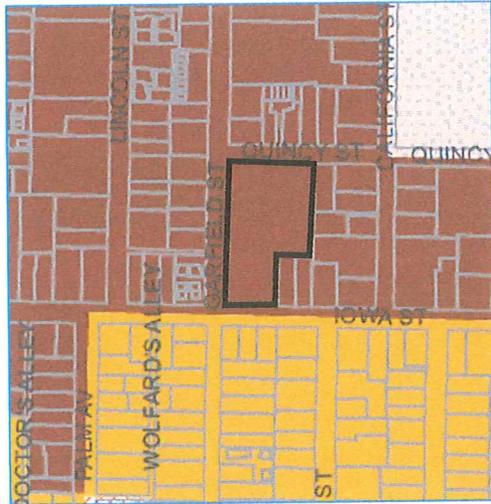


Figure 2: Zoning Map

The subject properties and the adjacent properties to the north, east and west are zoned R-3, High-Density Multi-Family Residential. The properties to the south, across Iowa Street are zoned R-2, Low Density Multi-Family Residential. Approximately 200-feet to the northeast, the property is zoned SO, Southern Oregon University.

The subject property had historically been the location of a large church, its parking area and other site improvements. The site is presently vacant of structures.

The adjacent properties are developed with single family residences, multi-family residences, apartment complexes and condominium developments.

There are 26-trees six-inches in diameter at breast height and larger on and directly adjacent to the property (there were 30-trees previously on the property, with four removed since the Tree Survey was completed). Of the 26 trees on site, 20 of the trees are directly adjacent too or near the existing curbside sidewalk, these established trees function as street trees. The other trees on the site consist of Spruce, Siberian elm, Mulberry, Sweet Gum, Purple Leaf Plum, Norway Maple, and Cherry.

The property has an approximately four percent average slope downhill from Iowa Street to Quincy Street.

The property is bound by Iowa Street along the south property line. According to the street classification from the Transportation System Plan (TSP), Iowa Street is a 2-Lane Avenue or Major Collector. Avenues have a required right-of-way (ROW) width of between 59 – 86 feet. Iowa Street has a 60-feet of ROW along the frontage of the property. Iowa Street is not improved to Avenue Standards, it has curb, gutter, sidewalk, but lacks parkrow and dedicated bike lanes.

Garfield Street is classified as a Neighborhood Street in the TSP. Neighborhood Streets that allow parking on both sides have a required ROW of 50 – 57 feet. Garfield Street has a 60-foot wide right of way. Garfield Street is improved with pavement, curb and gutter, there is an existing curbside sidewalk along the entire frontage of the property.

Along the south property line is Quincy Street. Quincy Street is also a Neighborhood Street. Quincy Street has a 50-foot wide ROW and is improved with pavement, curb, gutter, landscape park row and sidewalk along the frontage of the property.

The property has four existing driveway curb cuts, one accessing the property from Iowa, two on Garfield and one on Quincy.

A vehicular access easement extends from Quincy Street, 140-feet along the west property line benefiting the property located at 181 California Street. The easement is generally 20-feet wide.

There is overhead power present along the south side of Iowa Street, on the west side of Garfield Street, and along the property frontage on Quincy Street. There is one cobra head style street light on the south side of Iowa Street, near the intersection of Garfield and Iowa Streets.

A water meter serves the property on Garfield Street. There is a fire hydrant at the intersection of Garfield Street and Iowa Street. Another fire hydrant is present across Quincy Street from the subject property. Water mains are present in Iowa Street (six-inch main), Garfield Street (four-inch main), and in Quincy Street where there is a four-inch main.

Sanitary sewer services are present in Iowa Street, Garfield Street and in Quincy Street. Each has a six-inch sanitary sewer main.

There are no storm drainage facilities on the subject property. There are 12-inch storm sewer mains present on Garfield Street and Quincy Street.

Detailed proposal:

The proposed development of the property is for the 72-unit, MidTown Lofts. This new, residential apartment development is proposed as nine, two-story, eight-plexes with four units per floor. The MidTown Lofts are proposed to be an architecturally interesting, Danish inspired, modern apartment complex with extensive common and private outdoor space.

Each eight-plex is proposed to consist of 496-square foot habitable floor area, studio-bedroom units. There are four units on the ground level and four units above. Each group of four is connected via a covered central walkway that accesses the primary entrances of the ground level units and provides the access to the stairway leading to the entrances of second story units. As required by building codes, two of the ground level units are proposed as ADA accessible.

All units also have a semi-private patio or elevated deck area that also provides for a strong orientation for the structures towards the public streets and the open space. Each building has a prominent orientation towards the public street. With a recessed entry along the fronts of the units as well as the clearly defined walkway between the buildings leading to the entries.

The new, energy efficient units are proposed to be developed to the Earth Advantage Multi-Family Standards. High efficiency HVAC systems, Low E windows and insulation with high R values will be provided. The proposed thermal envelopes will provide for more comfortable and stable room temperature. LED lighting will be utilized both interior and throughout the property to further reduce energy consumption.



According to the City of Ashland Housing Needs Analysis, and the Rental Needs Analysis, the present needed housing type in Ashland are 500-square foot and less, apartment dwellings. The proposal provides for a much-needed housing type.

In addition to the individual, 80 – 100 square feet of semi-private open space area devoted to each unit, a large common courtyard area is proposed. Seven of the nine residential structures are adjacent to the courtyard area. Within the courtyard, elements intended to enhance the common area and encourage the use of the common area by the tenants are provided. These include a barbeque area, tables with chairs and/or benches, a large covered area with pathways connecting the units to the open space. To further enhance the recreational amenities of the courtyard, an active, flexible outdoor activity space is proposed. Designated area is provided in the courtyard for “lawn” games such as, badminton, spike ball, cornhole, croquet, ladder golf and others are proposed. The covered seating area, the fire pit, BBQ kitchen area, lawn games, substantial landscape buffers, all provide for an optimal apartment development layout while providing ample recreational opportunities that are appropriate for an urban, studio-unit development.

Parking, Access, Circulation:

The proposed development requires 72 parking spaces. The required parking is proposed as a combination of on-site parking in a 67-space surface parking lot. Along the more than 600-feet of street frontage, where 30, on-street parking spaces are available, the proposal seeks an on-street credit for five parking spaces.

Accessible via driveways from Garfield Street and Quincy Streets, the 67-space, surface parking lot is proposed. The number of curb cuts is proposed to be reduced from four to two. The Garfield Street driveway is proposed to be realigned with a public alley on the west side of Garfield. The Quincy Street driveway is in generally the same location as the existing driveway curb cut. This driveway access is proposed to be shifted slightly towards the west property line where a driveway serving the adjacent property further west is located. The existing driveway location does not comply with minimum driveway separation standards. The proposed location attempts to mitigate the lack of separation by combining the curb cut with the adjacent curb cut to the north (power pole may require protection or relocation) but provides for a better pedestrian and vehicular environment by reducing the number of curb cuts on the street. This location also aligns the proposed driveway with the driveways accessing the properties to the north on Quincy Street, improving visibility for vehicles backing out of the properties across Quincy onto the street.

The 67-space parking lot provides for three, ADA accessible parking spaces. Half of the parking spaces are proposed as compact. The parking area is accessed via a 22-foot wide drive aisle. The proposed parking lot and drive isle is designed with the Vehicle Parking Area Design Standards in mind. The final engineering documents will demonstrate compliance with either the use of light colored paving

materials that has a high solar reflectance or by providing 50 percent of the parking surface area as porous solid surface such as brick, or open grid paving system.

Large canopy shade trees are provided in the landscape islands. The proposal requests to utilize permeable paving techniques and fewer, larger bioswale detention ponds versus utilizing bioswale technology in the parking lot landscape medians. comply with the Rogue Valley Stormwater Quality Design Manual Standards updated January 2018. The goal of the storm water management system is to mimic a natural hydrologic process during a rain event with minimal impact on local surface waters. According to the project engineer, Jim Higday, P.E., Marquess and Associates, the creation of a combination facility, with bioswale detention ponds and below grade detention vault is a substantially better project design for stormwater quality and quantity.

The larger surface area detention swales allow for initial lower upfront engineering cost, and lower lifetime system operation and maintenance. Additionally, the bioswale type of planter is less compatible in a parking lot area where the landscape medians is often utilized as a walkway or vehicle access point. The parking lot landscape plan provides for parking lot shade trees, one for every seven parking spaces, evenly distributed throughout the parking area. Additional landscape areas for the parking lot is along the east and south property lines (abutting adjacent residential property at 1273 Iowa). These trees will be planted in the required five-foot landscape buffer that will be provided against the adjacent property lines. The five-foot planter provides adequate planter dimensions to encourage the growth of large stature trees because there is not a physical barrier to growth of these trees on the adjacent properties which will also benefit from the shade created.

The parking area is divided into separate areas by a group of buildings, landscape areas and a five-foot wide walkway that connects the units fronting on Iowa Street across the parking area to the common courtyard area.

This walkway is part of the connected walkway system that leads through the development in order to provide a safe walking routes from the units too and through the parking areas, and to the public sidewalk that is present along all frontages of the property. The existing sidewalk system connects to the further expanded neighborhood sidewalk system.

The property is within walking distance of numerous destinations such as Southern Oregon University, Ashland Street Shopping Center, the bike path along the railroad tracks is approximately a quarter of a mile away. The property is less than a quarter mile away from the bike lanes and public transportation on Siskiyou Boulevard. The property is within a 20-minute walk or 10-minute bicycle ride of many of Ashland's commercial areas.

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Density:

The property is 2.10 acres. The R-3 zones allows for the development of the property with 20-dwelling units per acre. The proposed density of 72 units less than 500-square feet in area ($72 \times .75 = 54$), with a proposed 29 percent increase, complies with the allowed density standards found in AMC 18.2.5.080. The proposed project requests a density bonus for Conservation Housing, for increased outdoor recreation space above the minimum eight percent required by the code and inclusion of major recreation facilities.

Allowed Density 18.2.5.080: $2.1 \times 20 = 42$ units

When units are less than 500-square feet of gross habitable floor area, each unit is equivalent to .75 of a unit.

The following density bonuses are sought with the proposal.

Conservation Housing – max 15%: $42 \times .15 = 6.3$

All of the units are proposed to have an energy efficient envelope. The units are proposed to have LED and low electric usage appliances. All of the proposed units will comply with Earth Advantage Multi-Family Standards.

Outdoor Recreation Space – max 10%: $42 \times .10 = 4.2$

The required eight percent outdoor recreation space for the 91,549.78 square foot parcel is 7,323.924 square feet. The proposed outdoor recreation space for the property is a combination of semi-private patios and balconies and the larger open space with the lawn areas, large common patio area with table and chairs, community BBQ, propane fueled fire pit, and a shade structure. The recreational elements of the open space for the MidTown Lofts with the built-in areas and provided equipment for lawn games enhances the recreational amenities of the open space. The total lot area devoted to outdoor recreation area for the MidTown Lofts “community” is 21,643 square feet in area or 23.6 percent.

Each unit also has a semi-private outdoor space that is either a deck or a patio area which accounts for 5,616 square feet or 14.6 percent. The common outdoor recreation area accounts for 5,754 square feet or nine percent. The density bonus standard requires that a minimum open space area of 18 percent be provided to comply with the density bonus standards. The proposed 23.6 percent outdoor recreation area exceeds the minimum required outdoor recreation space and exceeds the minimum additional area to allow for the density bonus. The remaining landscape areas provide for 10,273 square feet of incidental open space areas that are not factored into the required outdoor recreation space.

Major Recreation Facility: $42 \times .10 = 4.2$ (REQUESTED (.04 = 1.68)

For each percent of the total project budget devoted to major recreational facilities, a six percent density bonus is permitted. A four percent bonus is sought. The proposed the recreational lawn game areas, BBQ area, including built in outdoor kitchen, covered outdoor area, fire pit and seating areas around the



common outdoor recreational area account for more than one percent of the total project budget devoted to recreational facilities. The financial breakdown of the proposed recreational amenities and the project budget are attached. Open space area is a requirement, providing additional land area to obtain the openspace density bonus is achievable in many situations. That open space must be functional. Minimal lawn area and some walkways can achieve this standard. What is different with this proposal, is that its location allows for the numerous major recreation facilities that are found in public spaces less than ¼ of a mile from the property and a substantial amount of the project budget can be devoted to making the provided outdoor recreational space highly functional, desirable space.

There are increased construction, maintenance costs, and insurance ramifications for swimming pools that increase the rents to cover those costs. Basketball or tennis courts have the potential for generation of substantial noise and increase lot coverage greatly. The development is not intended for families so a playground was not considered.

The proposed large, functional, recreational area meets the needs of millennials, young professionals, active seniors, college students, etc. and the outdoor gathering areas combined with the activity spaces, foster a social environment for the tenants to gather or to have friends visit. The increased density requested is less than two, small units, but the return to the Mid-Town Lofts community with having the additional amenities provides for a comfortable, urban, modern, environmentally conscious development.

Public Utilities:

There are adequate public utilities provided in the adjacent public rights-of-way to service the proposed development. Areas for utility vaults are provided to allow for the undergrounding of the electric service for the development. The proposal demonstrates that the area for on-site detention and treatment of storm water facilities has been provided for on site. Sanitary sewer laterals have been provided on the civil plan. Water meter and fire department connection for sprinklers are shown on the plans.

Findings addressing the relevant criteria from the Ashland Municipal Code are provided herein. For clarity, the criteria is in Times New Roman font and the applicant's findings in Calibri font.

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**Site Development Design Standards Approval Criteria:
Ashland Municipal Code 18.5.2.050**

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

Applicant's Finding:

The subject property is zoned R-3, High Density Multiple Family Residential. The parcel is 91,474 square feet (2.1 ac) and exceeds minimum lot area and minimum lot dimensions in the R-3 zone.

The proposal for the property is to be developed with nine groups of two-story, eight plex units that consist of 496-square foot, studio-bedroom units. Four units on the ground level are proposed with four units above. The fourplexes will be connected via a stair and deck walkway. Through the incorporation of strong architectural elements, all of the units have designed to have a strong orientation to the public street.

The proposed dwellings are a modern, Danish inspired architecture. Interesting angles and mixtures of materials commonly found in the local environment such as wood, stucco, metal and concrete will bring an element of design reminiscent of when apartments were built for enjoyment and pride of tenancy, not just a place to live.

There are varying roof forms, pitches and heights to break up the mass of the structures. The walkway between the buildings is clearly defined with the strong roof form that invites residents and visitors into the walkways that provide entrance to the units. Each unit also has a clear sense of entry in the semi-private patio area that is oriented towards the public street frontages. The patio doors are not intended as the front entrance, with that provided off the common walkway. The proposed private patio access can function as a front entrance or as a secondary entrance while providing a strong orientation to the street.

The solar setback standards are met with the development because Quincy Street, which has a 50-foot wide right-of-way is to the north of the subject property. For the purposes of determining required solar setback, the structures on the subject property are allowed to shade the entire right-of-way. The proposed structural setback and height will not cast a shadow beyond the width of the Quincy Street right-of-way.

Density: *The proposed density complies with the allowed density standards found in AMC 18.2.5.080. The proposed project also requests density bonuses for Conservation Housing, for increased outdoor recreation space above the minimum eight percent required by the code and inclusion of major recreation facilities.*

Allowed Density 18.2.5.080: $2.1 \times 20 = 42$ dwelling units

Maximum allowed density with density bonuses: $+60\% (42 \times .60 = 25.4)$

Potential Density: 67.2 dwelling units

Proposed Density: The proposal seeks density bonuses for Conservation Housing, Open Space and Major Recreational Facilities for a requested increase of 29 percent to allowed density ($42 + 29\% = 54$ units). Per AMC 18.2.5.080.F., each unit when less than 500 square feet is .75 of a unit for the purposes of density. The proposal is for 72 units, with 496-square feet in floor area. This is equal to 54 dwelling units greater than 500 square feet ($72 \times .75 = 54$).

Conservation Housing – max 15%: $42 \times .15 = 6.3$

All of the units are proposed to have an energy efficient envelope. The units are proposed to have LED and low electric usage appliances. All of the proposed units will comply with Earth Advantage and Energy Star Requirements for new construction.

Outdoor Recreation – max 10%: $42 \times .10 = 4.2$

The required eight percent outdoor recreation space for a 91,549.78 square foot parcel is 7,323.924 square feet. The proposed outdoor recreation space for the property is a combination of semi-private patios and balconies and the larger open space with the lawn areas, large patio area with table and chairs, community BBQs and fire pit and a shade structure. The recreational elements of the open space for the MidTown Lofts with the built-in areas and provided equipment for lawn games enhances the recreational amenities of the open space. The total lot area devoted to outdoor recreation area for the MidTown Lofts “community” is 21,643 square feet in area or 23.6 percent.

Each unit also has a semi-private outdoor space that is either a deck or a patio area which accounts for 5,616 square feet or 14.6 percent. The common outdoor recreation area accounts for 5,754 square feet or nine percent. The density bonus standard requires that a minimum of 18 percent to comply with the density bonus standards. The remaining areas provide for 10,273 square feet of incidental openspace areas. The proposal exceeds the minimum required outdoor recreation space.

Major Recreation Facility: $42 \times .10 = 4.2$ (REQUESTED (.04 = 1.68))

For each percent of the total project budget devoted to major recreational facilities, a six percent density bonus is permitted. A four percent bonus is sought with this proposal. The proposal is to have numerous recreational amenities including dedicated lawn game areas, built in outdoor BBQ kitchen, covered patio area, fire pit and extensive seating areas in the common outdoor recreational area. These amenities create a substantially more desirable outdoor area with major recreation areas. The provided amenities account for more than one percent of the total project budget devoted to recreational facilities.

The project team finds that the proposed recreational amenities meet the current recreational trends in new, studio-unit developments. The proposed recreational amenities are environmentally sensitive and have limited paved or concrete surfaces. There are limited amenities listed in the Major Recreation

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Facilities found in section AMC 18.2.5.080.F.3.c. The suggested facilities include tennis courts, swimming pools, or playgrounds, or similar. The major recreational facilities suggested in the code all generate or have the potential to generate significant noise impacts in addition to having negative environmental impacts from the required solid surface to accommodate court games. The small size of the units are not typically occupied by families or parents with child or children so a playground would not be a beneficial use of the space. The code does provide for the "or similar" use as a major recreational facility. The proposed amenities including the dedicated lawn game areas provides active area for the envisioned demographic. The proximity of the property to conveniently located off-site major recreational facilities further reduces the need for a tennis court or basketball court.

Within less than one quarter of a mile there are a number of public major recreational facilities provided. These include Garfield Park which is 620 feet to the north on Garfield Street, where basketball, volleyball, play structure, lawns and splash pad area are found. Roughly one quarter mile to the east on Quincy and Wightman Street, tennis courts, ballfields and wide open spaces are found on the SOU campus. Additional recreational area is found on Lincoln Street at the Lincoln Street ballfield. Within one mile of the property even more major recreational facilities are found at the Daniel Meyer Pool on Holmes Avenue in the Hunter Park.

The proposed lawn games, and substantial open space improvements, though not listed as major recreational facilities, the proposed unit size, demographics of Ashland and in particular, the demographic of Ashland that seeks a 496-square foot studio unit are not seeking basketball courts, swimming pools or tennis courts. Those type of amenities are less desirable than well designed, beautifully landscaped, comfortable, outdoor spaces with fun amenities. The finances of the project and the amount of the project budget devoted to the recreation facilities demonstrate that that AMC 18.2.5.080.F.3.c. The project team believe that the Planning Commission can find that the financial aspects of the proposal are met, that the amenities provided comply with the purpose and intent of the density bonus for major recreational facilities.

Lot Coverage: Proposed impervious areas including building footprints, pathways, parking areas, patios and decks is 41,747 SF, for a total lot coverage of 55.5 percent, substantially less than the maximum of 75 percent in the zone.

Parking: The proposed 67-space parking lot provides for three, ADA accessible parking spaces. Half of the parking spaces are proposed as compact. The parking area is accessed via a 22-foot wide drive aisle. The proposed parking lot and drive isle is designed in compliance with the Vehicle Parking Area Design Standards. The parking spaces will be clearly marked and where necessary, wheel stops will be provided. The final engineering documents will demonstrate compliance with either the use of light colored paving materials that has a high solar reflectance or by providing 50 percent of the parking surface area as

porous solid surface such as brick, or open grid paving system. Landscape islands that have parking lot shade trees are proposed. More than seven percent of the area devoted to outdoor parking is landscaped areas that are uniformly distributed throughout the parking area. Irrigation, curbing, trees, shrubs, ground cover and mulch are provided in the parking area. A six-foot tall privacy fence will be provided along the property lines to screen the parking lot area from the adjacent properties to the south and east of the parking area.

One covered bicycle parking space is required for each unit. In order to provide for bicycle security, a hanging bicycle rack for a single bicycle will be provided within each unit except the two A-Type (ADA accessible) units. The hanging rack has a nook provided for the bike hanger, the A-type units require a larger bathroom and doorways that eliminate the area for the bike hanger. Outside of the units, in covered areas as stand-alone structures, found near the parking area that parallel Iowa Street, inverted U-racks in groups of six providing for 12 spaces for visitors, or tenants that chose to park outside of their unit.

The new development has a circulation system that accommodates expected traffic on-site. The layout has street-like features. Pedestrian connections through the site and to the adjacent sites.

Energy Usage: All of the units within the proposed development will be constructed to the Earth Advantage and Energy Star Standards. A detailed analysis of the actual energy consumption has not been determined but due to the high energy efficiency standards of the two programs the units will require substantially less energy to operate than typical construction. The units will be high performance, using the best practices and innovative construction technologies to gain efficiencies in design, energy systems, and materials for increased energy efficiency, superior indoor air quality, lower water usage and responsible use of natural resources.

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

Applicant's Finding:

There are no applicable overlay zones for the subject property.

C. Site Development and Design Standards.

Applicant's Finding:

The proposed site development complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

The proposed parking is directly adjacent to the residential dwellings. There are short travel distances from the parking spaces to the entrances of the dwellings. The surface parking spaces are visible from

the windows of the units and open space areas. The layout and design does not provide for vulnerable areas that are not visible from the units and open space. Low level landscape lighting for the paths will be provided throughout the open space. Each unit will have a shrouded yard light that provides down-lighting and security for the unit but will not directly illuminate adjacent properties. No plant materials are proposed that prevent surveillance of the open space or the semi-private patios and balconies.

The eight-plexes are proposed to have 21.5-foot separations between the two structures on Iowa Street, a 12-feet of separation between the eight-plexes on Garfield and Quincy Streets. This separation complies with the maximum required building separations. The area between the fourplex units that make up each half of the eight-plexes provide a visual division in the façade of the structure and a walkway through. In addition to this area, there is a 12-foot landscape buffer between the eight-plexes. This area provides a clear line of sight from the public right-of-way into the development with glimpses of the recreational open space visible from the streets.

The proposed 19.5-foot wide building with slight separation of nearly 8-feet for the walkway between the connected buildings and the 12-foot separation between the individual eight-plexes is similar to the massing of the structures in the vicinity. For example, the most recent development is a condominium development across Garfield Street from the southern half of the property that has more than 35-feet of structure with minimal open space, separation between the structures. Additionally, the developments to the south across Iowa Street are multi-family residentially zoned lots that are primary occupied by 20 – 30-foot-wide structures separated from the adjacent structure by 10 – 14 feet.

Building Orientation.

Building Orientation to Street. Dwelling units shall have their primary orientation toward a street. Where residential buildings are located within 20 feet of a street, they shall have a primary entrance opening toward the street and connected to the right-of-way via an approved walkway.

Applicant's Finding:

All residential units that front upon the public street are oriented towards the street. All ground floor units are proposed to have a semi-private patio area that has a clear entrance to the unit. The upper story units will also have a deck area that provides additional visual interest and clear orientation to the public street. The actual primary entrances to the unit are within the semi-covered, connected walkway. With the large, marquee style awning extending beyond the façade of the eight-plex increases the building strong orientation to the public street. The apartments adjacent to the parking area are more than 20-feet from Iowa, Garfield or Quincy Streets and are generally oriented towards the common open space.

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Limitation on Parking between Primary Entrance and Street. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or on one or both sides.

Applicant's Finding:

No parking is proposed between the buildings and the street. All parking is located to the side and rear of the structures.

Build-to Line. Where a new building is proposed in a zone that requires a build-to line or maximum front setback yard, except as otherwise required for clear vision at intersections, the building shall comply with the build-to line standard.

Applicant's Finding:

Both Iowa Street and Quincy Streets are treated as front facades with the front yard setback from Iowa Street at 21.5-feet to provide adequate protection for the preserved trees. The frontage along Quincy Street has a 16-foot front yard setback for the units. The ground floor patio and un-covered decks above extend six-feet into the front yard setbacks. Garfield Street is also treated like a front façade vs. a side yard and a 15-foot front yard setback to the façade of the structure and nine-feet to the patio and uncovered porches above. Substantial vision clearance triangles are provided at each street intersection.

Garages. Alleys and Shared Drives. Where a lot abuts a rear or side alley, or a shared driveway, including flag drives, the garage or carport opening(s) for that dwelling shall orient to the alley or shared drive, as applicable, and not a street.

Applicant's Finding:

There are no garages proposed.

Setback for Garage Opening Facing Street. The minimum setback for a garage (or carport) opening facing a street is 20 feet. This provision does not apply to alleys.

Applicant's Finding:

There are not garages proposed.

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Building Materials. Building materials and paint colors should be compatible with the surrounding area. Very bright primary or neon-type paint colors, which attract attention to the building or use, are unacceptable.

Applicant's Finding:

The building design and materials are more modern than the historical apartment complex development in Ashland, which has been largely non-existent for the past 30 years. The materials are commonly found in modern architectural and found in the Ashland's historic districts. The materials are compatible with the surrounding area. The units are proposed to have clear coated wood siding, corrugated metal siding, standing seam metal roofing, metal railings and vinyl windows. The exact paint colors have not been selected but they will not be bright primary or neon colors.

Streetscape. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

Applicant's Finding:

There are some existing street trees that are to be preserved. In addition to the existing street trees, new street trees that comply with the City of Ashland Street Tree planting standards will be provided. The draft landscaping plan provide the species chosen for the street trees.

Landscaping and Recycle/Refuse Disposal Areas. Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

Applicant's Finding:

A common refuse area will be provided in a screened area adjacent to the parking area. The common refuse area will comply with the City's standards for screening and to meet the needs of Recology.

18.4.4.030 Landscaping and Screening

Applicant's Finding:

The proposed landscaping plan and the irrigation plan that will be submitted with the building permits complies with the Irrigation and Water Conserving Landscaping requirements of the City of Ashland and the standards to meet Earth Advantage point requirements. The conceptual landscaping plan submitted with the application has been designed so that plant coverage of 90 percent within five years of planting is met. Two-inches of mulch will be provided in all non-turf areas after planting. Turf areas are considered as synthetic lawn. The selected plan species include low to very low water usage plant materials. The landscaping in the Storm Water treatment facilities will be planted with water-tolerant species. The

proposed landscaping has been designed for crime prevention and defensible space to allow for natural surveillance.

The code requires five-foot landscape buffers along the property lines. The code also requires parking lot shade trees. There is not a barrier to growth and development of shade trees proposed to be planted in the five-foot wide landscape buffer. According to the Landscape Architect, appropriately sized and planted large canopy shade trees are proposed that are not known for their infrastructure destructing properties. There is not a substantial difference between five and seven feet of planting area with respect to tree growth, and that appropriate plant selection, correct planting and irrigation techniques is more important. The conceptual landscape plan demonstrates compliance with the standards.

The lighting is proposed in a manner that will provide for pedestrian safety, property identification, and crime prevention. The standards are such that no direct illumination onto adjacent residential properties is anticipated. Light poles of not more than 14-feet in height for the pedestrian facilities will be used within the development.

Tree Preservation, Protection, and Removal

18.4.5.030 Tree Protection:

Applicant's Finding:

The trees shown on the proposed Tree Protection plan will be preserved with six-foot fencing, placed at the dripline of the trees, protected throughout the duration of construction. See the attached tree removal and tree protection plan. There are 30 trees shown on the Tree Plan due to the trees not appearing to have obtained permit for removal between the 2016/2017 Rivergate Church application and this proposal.

18.5.7 Tree Removal:

B. Tree Removal Permit.

- a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.

Applicant's Finding:

There were 30 trees greater than six-inches in diameter at breast height (DBH) on or directly adjacent to the property. Of these, 20 trees are proposed for removal. Four trees are included on the tree inventory that were previously removed.

The trees proposed for removal were carefully considered but their location or condition were not conducive to retaining the trees.

b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.

Applicant's Finding:

The removal of the trees will not have impacts on erosion, soil stability, flow of surface waters, and protection of adjacent trees or existing windbreaks. The trees were primarily planted as landscape trees for the former property occupants, the Rivergate Church. The site will be fully developed upon removal and all surfaces will be manipulated and either constructed or landscaped as part of the development. All trees that are in preservable locations and that contribute to the streetscape and the property layout, desired densities and setbacks, have been preserved and the revised tree plan provides preservation details.

c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.

Applicant's Finding:

There are a significant number of deciduous and conifer trees within 200-feet of the property. The removal of the three trees will not have a negative impact on the densities, sizes, canopies or species diversity.

The canopy increase on the property will replace the canopy removed as part of this request.

d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.

Applicant's Finding:

The proposal complies with residential densities. The removed trees have no significant environmental benefits that will not be achieved in the near future with the proposed replacement trees.

e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

Applicant's Finding:

Numerous mitigation trees are proposed to be planted throughout the project site. Due to the nature of the development, high-density multi-family, no conifer trees are proposed. There are 12



trees proposed in the open spaces. There are 15 parking lot shade trees proposed and 11 new street trees. The proposed number of deciduous trees, 38 total, is more than double the required mitigation ratio. The trees will be planted and maintained per the specifications of the Recommended Street Tree Guide.

D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Applicant's Finding:

Adequate city facilities exist to service the proposed development.

Water: A water meter serves the property on Garfield Street. There is a fire hydrant at the intersection of Garfield Street and Iowa Street. Another fire hydrant is present across Quincy Street from the subject property. Water mains are present in Iowa Street (six-inch main), Garfield Street (four-inch main), and in Quincy Street where there is a four-inch main. A single service for the units, a service for the openspace and fire connections are proposed on the north side of the driveway accessing the site from Garfield Street.

Sanitary Sewer: Sanitary sewer services are present in Iowa Street, Garfield Street and in Quincy Street. Each has a six-inch sanitary sewer main. There is adequate capacity in the lines to service the new units.

Electrical: There is overhead power present along the south side of Iowa Street, on the west side of Garfield Street, and along the property frontage on Quincy Street. There is one cobra head style street light on the south side of Iowa Street, near the intersection of Garfield and Iowa Streets. All electrical service on the site will be served by single electrical transformer installed on the property from the primary poles across Garfield Street from the subject property. The transformer will be to the north of the relocated driveway, the exact location has not been determined but, there is adequate open space and lot coverage area to allow for the placement without negative impacts to lot coverage areas.

The power pole within the proposed single driveway apron on the Quincy Street side will be relocated to meet the needs of Ashland electric dept.

Storm Sewer: There are no storm drainage facilities on the subject property. There are 12-inch storm sewer mains present on Garfield Street and Quincy Street.

All street frontages are paved with curb, gutter, sidewalk, street trees. The proposed development has paved driveway and parking areas and walkways connecting the units to the public sidewalks adjacent to the development.

E. Exception to the Site Development and Design Standards. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or

2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

Applicant's Finding:

An exception 18.4.3.080.B.5.b.to not have the parking lots designed in a way that captures and treats runoff with landscaped medians and swales. The proposed bioswales and underground treatment and detention ponds treat the hard surfaces and the parking lot surface. The proposed methods are a more efficient, cost effective stormwater detention and treatment facility.

Since the parking lot medians are often walked upon by tenants entering and existing vehicles, a traditional, walkable ground cover is a better use of the space than a variable grade, rocky and or sloped landscape buffer with a grate system and possibly filled with water.

It can be found that the proposal to include light colored, some pervious paving techniques, larger bioswales outside of the area where vehicle entry and exiting occurs, is a superior low impact development design than the parking lot median bioswales.

The parking lot landscape buffer and parking lot landscape peninsulas are provided that are sized and design with species selected that will do well in the parking lot while achieving the purpose and intent of the Site Design Standards as they relate to landscape buffers.

Public Facilities

18.4.6.020

B. Exceptions and Variances.

1. Exception to the Street Design Standards.

a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.

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Applicant's Finding:

The location of the public infrastructure, and street trees along the frontage and at the intersections, would require relocation of the sidewalk to create a parkrow at a high cost to the property owner. Installing street improvements that comply with the standards for sidewalk and park row width including curb return at the intersection are cost prohibitive and would require the removal of established street trees.

The widened curb cut is in generally the same location as the existing Quincy Street driveway accessing the site that is less than 24-feet than the adjacent property to the west. The shared curb cut is in more conformance with the standards for reduced curb cuts and requirements for shared access than requiring a 24-foot separation for a new, separate curb cut.

A recorded ingress / egress access easement for the property at 181 California Street (flag lot with vehicular access from subject property only). This lot does not have access through 1274 Quincy Street the proposed shared curbcut and access must be retained for the property at 181 California Street. The proposal retains that lot's legal access through the property and retains the existing driveway curbcut for the adjacent property.

The proposed combined curbcut is 36-feet (20-feet proposed, 16-feet existing). This is larger than the maximum of 18-foot cut with six-foot apron flares for a total of 24-feet. The proposed 36-foot combined is better than two separate cuts that total 48-feet. The enlarged cut allows for access to each property and doesn't put the burden of providing parking for in inadequate layout for the three units at 1274 Quincy Street.

b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.

Applicant's Finding:

The connectivity of the property and the neighborhood will have superior transportation facilities through the installation of additional street trees and an overall reduction in the number of curb cuts and accesses from the site to the public right-of-way.

- i. For transit facilities and related improvements, access, wait time, and ride experience.
Not applicable
- ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.
Iowa, Garfield and Quincy Streets are all 'shared' streets without dedicated bicycle lanes. The proposal will not have a negative impact on the bicycle facilities.
- iii. For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.

The proposal is to retain the existing sidewalks and street trees along all the street frontages and to have a wider than standard, shared driveway curbcut. In addition to the larger stature street trees that exist and are proposed for preservation, new street trees are proposed on all street frontages. A truncated dome and accessible cross walk is proposed for the intersection of Garfield and Quincy Streets. The proposed improvements improve the comfort level of walking along the street and provides a safer crossing of Quincy Street.

The wider than standard driveway curbcut is better for the pedestrian environment because the total number of curbcuts is reduced and the width of a single curbcut is widened.

c. The exception is the minimum necessary to alleviate the difficulty.

Applicant's Finding:

The exception is the minimum necessary to alleviate the difficulty of improving to full city standards because the existing pedestrian environment is established, and the exception allows for the preservation of larger stature, existing, healthy street trees.

A single, larger than standard driveway curbcut and apron is the minimum necessary to alleviate the difficulty of having two, separate curbcuts.

Requiring a 24-foot separation prevents the property at 181 Garfield from having access via their existing access easement. A 24-foot separation places the curbcut on the subject property in line with proposed Units 1 – 4. This would create an island of virtually unusable area between the driveways. Placement of residence or open space on the east side of the driveway amongst the parking spaces would not be a good, efficient use of the land.

d. The exception is consistent with the Purpose and Intent of the Street Standards in subsection [18.4.6.040.A](#).

Applicant's Finding:

The purpose and intent contain standards for street connectivity and design as well as cross sections for street improvements including installation of new streets and improvements to existing streets. The preservation of the existing sidewalk along the frontage while preserving a large number of existing, healthy shade producing street trees and maintaining connectivity is consistent with the standards.

The driveway width being wider than allowed by standards provides for a better pedestrian environment by reducing the number of curbcuts and the consolidation of the driveway allows for the driveway to be located across from existing driveways to the north of property on Quincy Street. There is access and parking for multiple modes of transportation. The driveway curbcut

does not decrease pedestrian safety and increases the amount of level sidewalk that is free of curbcuts. The proposal also retains the greatest amount of publicly available on-street parking.

Conclusion:

In conclusion, the project team finds that the proposed development will be a welcome addition in the high-density, multi-family residential neighborhood. According to the City of Ashland Housing Needs Analysis and the Rental Needs Analysis, the present needed housing type in Ashland is 500-square foot and less, apartment dwellings.

It can be found that the parking lot layout, design and construction all complies with the purpose and intent of the Comprehensive Plan and the Ashland Municipal Code to achieve desired densities that promote the urbanization of the multi-family zone. That the provision of rental housing that complies with and exceeds energy efficiency standards is a needed development pattern in Ashland.

There are 72, small, energy efficient units combined with the generous site amenities including the large, open and inviting common area which will provide a unique recreational opportunity for the community of future tenants.

The applicant finds that all of the applicable City of Ashland requirements have been met or can be met through the imposition of conditions of approval.

Attachments:

- 1) Financial Document re. Major Recreational Space density bonus
- 2) Tree Removal Permit Request
- 3) Traffic Engineer, Trip Generation and TIA Applicability Letter
- 4) Street View: AP 0.0
- 5) Site Plan: AP 0.1
- 6) Preliminary Civil Engineering: C1
- 7) Landscape Plans: L 0.1 – 0.2
- 8) Tree Removal and Protection Plan: L 0.3
- 9) City of Ashland Electric Distribution Map

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City Of Ashland

July 7, 2018

Value of Recreation Space for Ashland Urban Lofts

Amy,

Cost of Building Project \$10,800,000. x 1%	= \$108,000.
Est. Value of the recreation amenities (activity equipment and surfaces) w/ land value of dedicated rec space	= \$150,000.
Est. Value of BBQ / outdoor kitchen area/ Fire Pit	= \$ 28,000.
Est. Value of cover	= \$ 75,000.
Est. Value of seating areas	= <u>\$ 12,000.</u>
Total Estimate of Recreation Amenities	= \$164,000.

If you have any further questions regarding our planned Recreation Amenities, please don't hesitate to call.

Sincerely,

Raymond Kistler, Principal Architect

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SANDOW ENGINEERING

160 MADISON STREET SUITE A • EUGENE, OREGON 97402 • 541.513.3376

August 2, 2018

Karl Johnson, E.I.T., Associate Engineer
City of Ashland, Public Works/Engineering
20 East Main St
Ashland, Oregon 97520



RENEWAL 06/30/20

RE: Mid-Town Urban Lofts Trip Generation and TIA Applicability

Sandow Engineering has prepared an estimation of the anticipated vehicle trips generated by the proposed Mid-Town Urban Lofts located at the southwest corner of Garfield Street and Quincy Street in Ashland, Oregon.

TRIP GENERATION

The applicant is proposing the construction of a 72-unit studio apartment complex. The trip generation for the development was estimated using information contained within the Institute of Transportation Engineers (ITE) Trip Generation Manual 10th Edition. The site trips are estimated using the data provided for Multifamily Housing Low-Rise (LUC 220). The site generated trips for the AM and PM peak hours are illustrated in Table 1.

TABLE 1. TRIP GENERATION ESTIMATE

ITE Land Use	Size	Units	Trip Generation	
			Rate	Trips
AM Peak Hour				
220 – Multifamily Housing Low-Rise	72	DU	0.46	33
PM Peak Hour				
220 – Multifamily Housing Low-Rise	72	DU	0.56	40

As demonstrated, the proposed development is anticipated to generate 33 trips during the AM Peak Hour and 40 trips during the PM Peak Hour.

TIA APPLICABILITY

Ashland Code Requires a Traffic Impact Analysis when one of the following occurs:

- 1) Addition of 50 newly generated vehicle trips during the adjacent street peak hour

The expansion is anticipated to generate 40 vehicles trips in the PM peak hour and 33 vehicle trips in the AM peak hour. This criterion is not met for warranting a TIA.

- 2) Installation of any traffic control device and/or construction of any geometric improvements that will affect the progression or operation of traffic traveling on, entering, or exiting the highway

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From: Kelly Sandow PE Sandow Engineering
RE: Mid Town Lofts-Trip Generation and TIA applicability
Date: 8/2/18
Page 2

The applicant is not installing any traffic control devices or constructing any geometric improvements within the ROW. This criterion is not met for warranting a TIA.

3) *Addition of 20 newly generated heavy vehicle trips during the day.*

The proposal is for a studio apartment complex. There are a limited number of heavy vehicles that will access this site. The development is not expected to not generate more than 20 additional heavy vehicle trips during the day. This criterion is not met for warranting a TIA.

FINDINGS

As demonstrated, the proposed development of the Mid-Town Loft is not anticipated to generate more than 40 trips during any peak hour. The proposal does not meet any fo the criteria for warranting a Traffic Impact Analysis.

Please feel free to contact me if you have any questions or if you need any additional information.

Sincerely,



Kelly Sandow PE

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SANDOW
ENGINEERING

August 15, 2018

City of Ashland
Tree Commission
20 East Main Street
Ashland, OR 97520

Re: **Tree Removal Permit Request**
Mid-Town Urban Lofts
188 Garfield Street

Dear Tree Commission Members,

Efforts were made in the planning process of the Mid-Town Urban Lofts project to accommodate existing trees, particularly the large stature trees in the public right of way on Garfield and Iowa Street. All trees 6" diameter breast height (dbh) and larger are indicated on the Tree Protection & Removal Plan. Replacement trees will be provided as part of the standard development process in accordance with City of Ashland's Municipal Code. Removal of these trees will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks. Additionally, the removal of these trees will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. One or more trees will be planted in the new landscape as mitigation for each tree with a dbh of 6" or larger that is removed.

Tree #1 12" Redbud. Located within the proposed new driveway, this tree needs to be removed to allow for construction.

Tree #2 54" Black Locust (5 stems @ 10-12" ea). This tree is located on the adjacent property to the east and will be retained. A portion of the existing asphalt pavement on the property within the drip line of the tree will be removed and replaced with a new landscape planter.

Tree #3 30" Black Locust (3 stems @ 10" ea). This tree is located on the adjacent property to the east and will be retained. A portion of the existing asphalt pavement on the property within the drip line of the tree will be removed and replaced with a new landscape planter.

Tree #4 11" Norway Maple. Located within a proposed building footprint, this tree needs to be removed to allow for construction.

Tree #5 60" Leyland Cypress. This tree died recently and has already been removed.

Tree #6 Leyland Cypress. This tree was removed prior to this project.

Tree #7 40" Siberian Elm. Located within a proposed landscape area, this tree will be retained.

Tree #8 14" Mulberry. This tree is located within the proposed mid-block, continuous pedestrian walkway. Since more than half of this tree's root zone will be impacted by walkway construction, it is not likely to survive and is proposed to be removed.

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- Tree #9** 15" Mulberry. Located within a proposed landscape area, this tree will be retained.
- Tree #10** 12" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #11** 14" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #12** 12" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #13** 10" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #14** 12" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #15** 10" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #16** Maple. This tree was removed prior to this project.
- Tree #17** 12" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #18** 12" Sweetgum. Located within the proposed new driveway, this tree needs to be removed to allow for construction.
- Tree #19** 12" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #20** 8" Cherry. This tree, located close to a proposed building footprint, is in poor health and needs to be removed to allow for construction.
- Tree #21** 19" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #22** 17" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #23** 11" Sweetgum. Located within a proposed landscape area, this tree will be retained.
- Tree #24** 19" Spruce, double leader. This tree, located within a proposed building footprint, is in poor health and needs to be removed to allow for construction.
- Tree #25** 19" Spruce. This tree was removed prior to this project.
- Tree #26** 8" Redbud. Located within the proposed new public sidewalk, this tree needs to be removed to allow for construction.
- Tree #27** 10" Cherry. Located within the proposed new public sidewalk, this tree needs to be removed to allow for construction.
- Tree #28** 9" Redbud. Located within the proposed new public sidewalk, this tree needs to be removed to allow for construction.
- Tree #29** 7" Redbud. Located within the proposed new public sidewalk, this tree needs to be removed to allow for construction.
- Tree #30** 6" Crabapple. Located within a proposed paved area, this tree needs to be removed to allow for construction.

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*Tree Removal Permit Request
Mid-Town Urban Lofts
August 15, 2018*

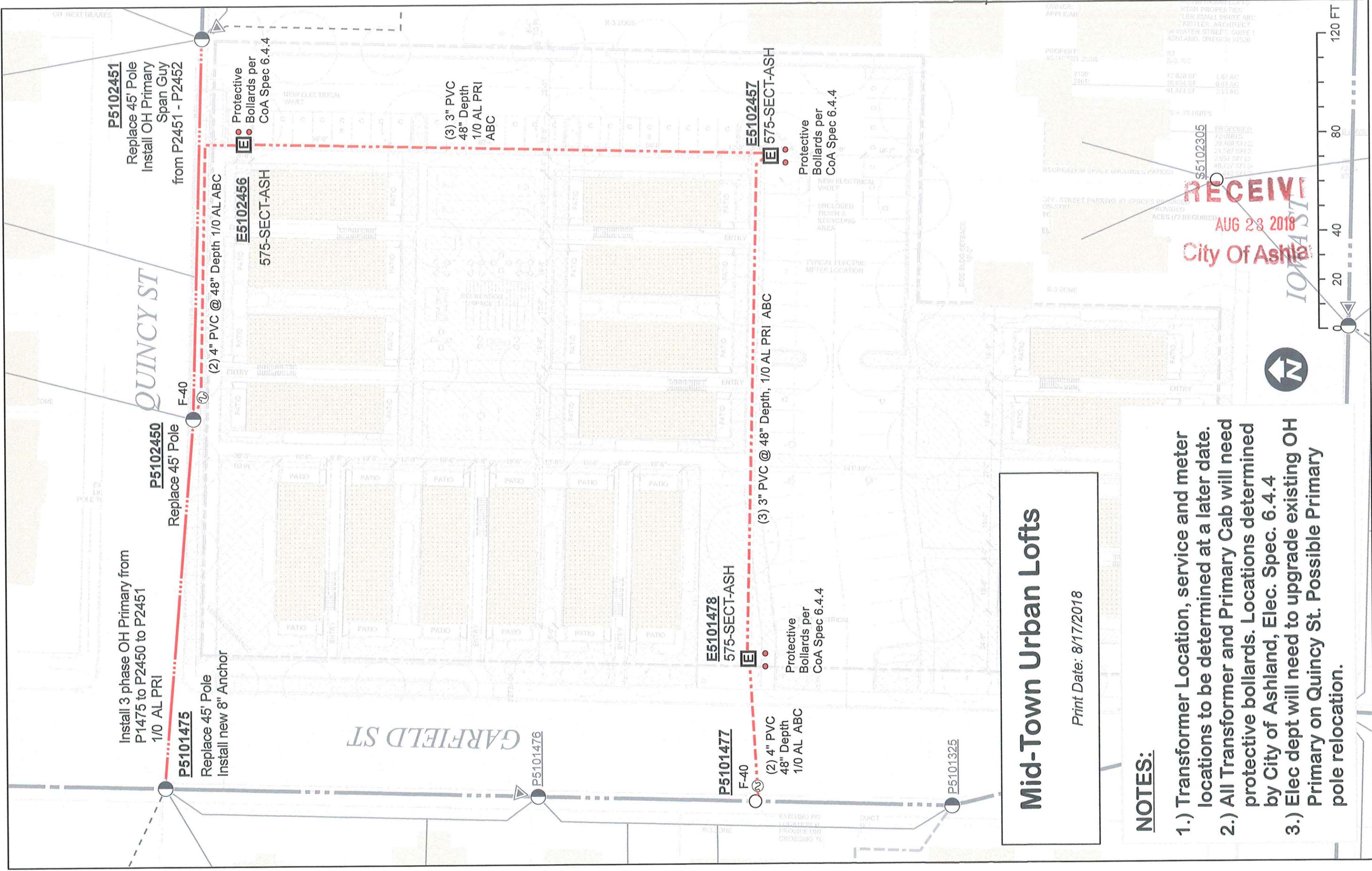
The Mid-Town Urban Lofts project will include new trees selected for hardiness, beauty, and longevity, and will be coordinated with the City of Ashland's landscape requirements.

Respectfully submitted,



Greg Covey
Covey Pardee Landscape Architects

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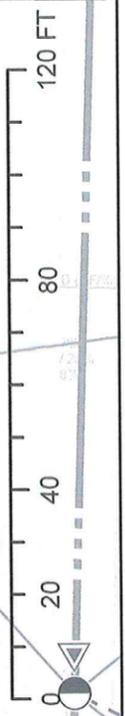
Mid-Town Urban Lofts

Print Date: 8/17/2018

NOTES:

- 1.) Transformer Location, service and meter locations to be determined at a later date.
- 2.) All Transformer and Primary Cab will need protective bollards. Locations determined by City of Ashland, Elec. Spec. 6.4.4
- 3.) Elec dept will need to upgrade existing OH Primary on Quincy St. Possible Primary pole relocation.

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OWNER:	RTAN PROPERTIES
APPLICANT:	CLER SMALL WHITE AND KISTLER ARCHITECT
PROJECT ADDRESS:	100 WATER STREET SUITE 1 ASHLAND, OREGON 97520
PROPOSED:	72 UNITS
	20,108 SF/2
	21,787 SF/2
	2,957 SF/2
	49,727 SF/2
	543 SF/2
	5 = 75 UNITS
	72,820 SF
	1.67 AC
	18,651 SF
	0.43 AC
	91,474 SF
	2.12 AC



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AUG 23 2018

City Of Ashland

kistler +
small
+ white
architects

MID-TOWN URBAN LOFTS

PLANNING
07.06.18

STREET VIEW

AP0.0

RELOCATE EXISTING POWER POLE.
GUIDE WIRE TO BE REMOVED AND RELOCATED ON NEXT NEAREST POLE TO THE WEST.

PROJECT INFORMATION
PROJECT NAME: MID-TOWN URBAN LOFTS
OWNER: SPARTAN PROPERTIES
APPLICANT: KISTLER SMALL WHITE ARCHITECTS
68 WATER STREET, SUITE 101
ASHLAND, OREGON 97520

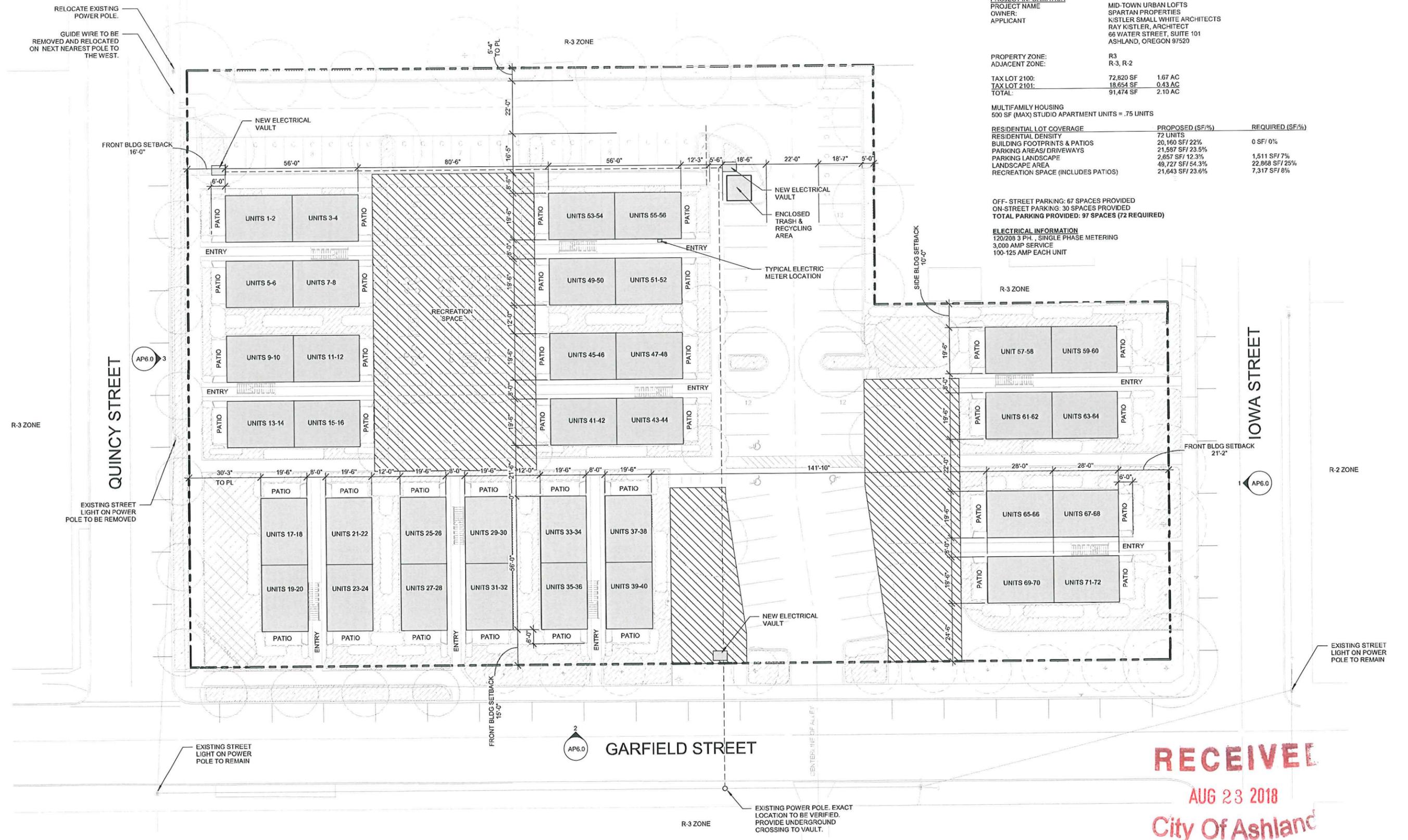
PROPERTY ZONE: R3
ADJACENT ZONE: R-3, R-2
TAX LOT 2100: 72,820 SF 1.67 AC
TAX LOT 2101: 18,654 SF 0.43 AC
TOTAL: 91,474 SF 2.10 AC

MULTIFAMILY HOUSING
500 SF (MAX) STUDIO APARTMENT UNITS = .75 UNITS

RESIDENTIAL LOT COVERAGE	PROPOSED (SF/%)	REQUIRED (SF/%)
RESIDENTIAL DENSITY	72 UNITS	0 SF/ 0%
BUILDING FOOTPRINTS & PATIOS	20,160 SF/ 22%	
PARKING AREAS/ DRIVEWAYS	21,587 SF/ 23.5%	
PARKING LANDSCAPE	2,657 SF/ 12.3%	1,511 SF/ 7%
LANDSCAPE AREA	49,727 SF/ 54.3%	22,868 SF/ 25%
RECREATION SPACE (INCLUDES PATIOS)	21,643 SF/ 23.6%	7,317 SF/ 8%

OFF-STREET PARKING: 67 SPACES PROVIDED
ON-STREET PARKING: 30 SPACES PROVIDED
TOTAL PARKING PROVIDED: 97 SPACES (72 REQUIRED)

ELECTRICAL INFORMATION
120/208 3 PH., SINGLE PHASE METERING
3,000 AMP SERVICE
100-125 AMP EACH UNIT



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SITE PLAN 1
1" = 20'-0"



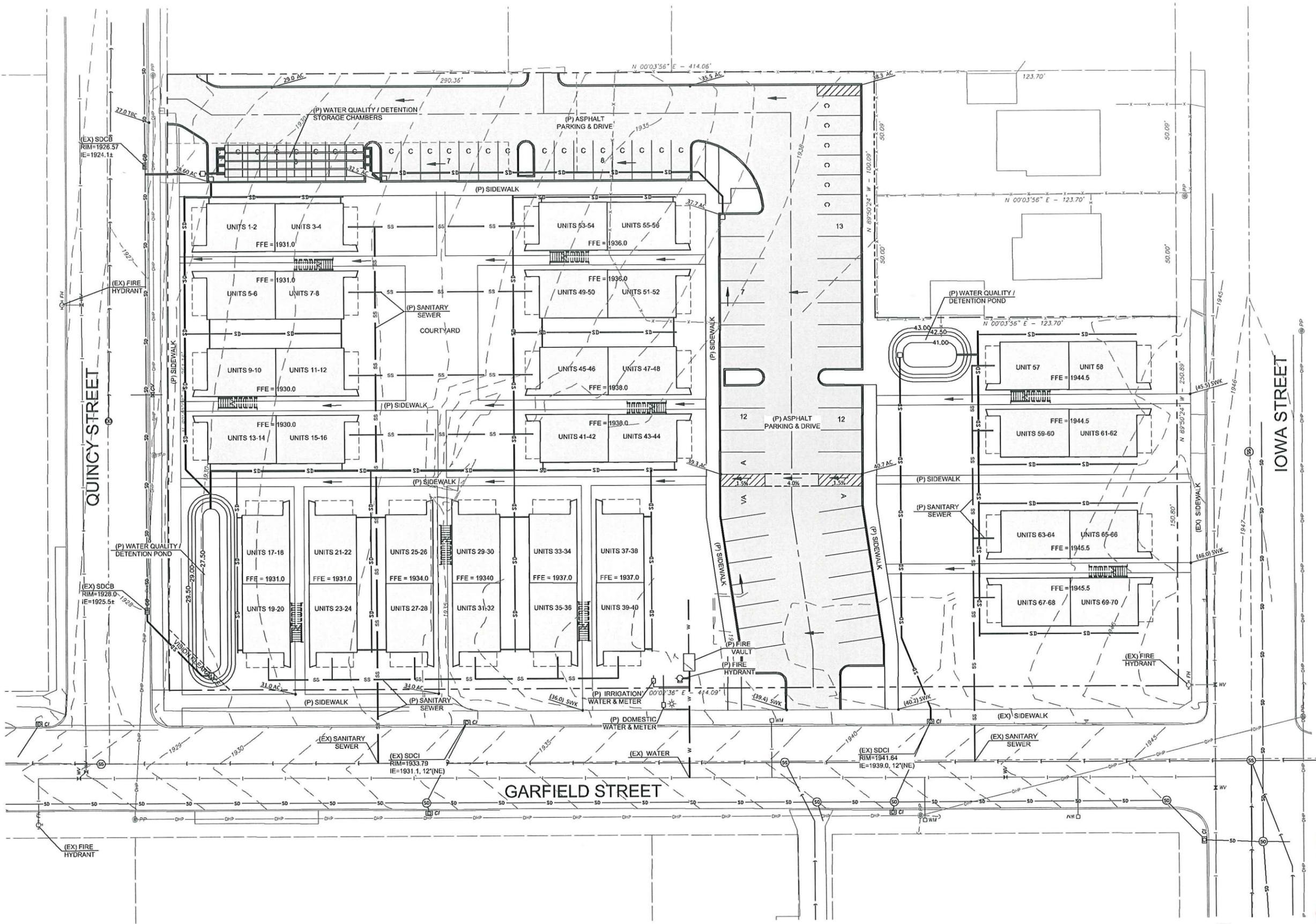
MID-TOWN URBAN LOFTS
 SPARTAN GROUP

CONCEPTUAL GRADING
 & DRAINAGE PLAN

REVISIONS
 MAIL JOB NO. 181150
 ISSUE DATE 08/14/18

SHEET

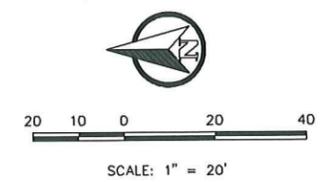
C1



LEGEND - GRADING, DRAINAGE & UTILITIES

- | | |
|------------------|----------------------------|
| AC ASPHALT | SD STORM DRAIN |
| CB CATCH BASIN | SS SANITARY SEWER |
| (EX) EXISTING | SWK SIDEWALK |
| (P) LINEAR FEET | FFE FINISH FLOOR ELEVATION |
| ← FLOW DIRECTION | IE INVERT ELEVATION |
| | W WATER |

NOTES:



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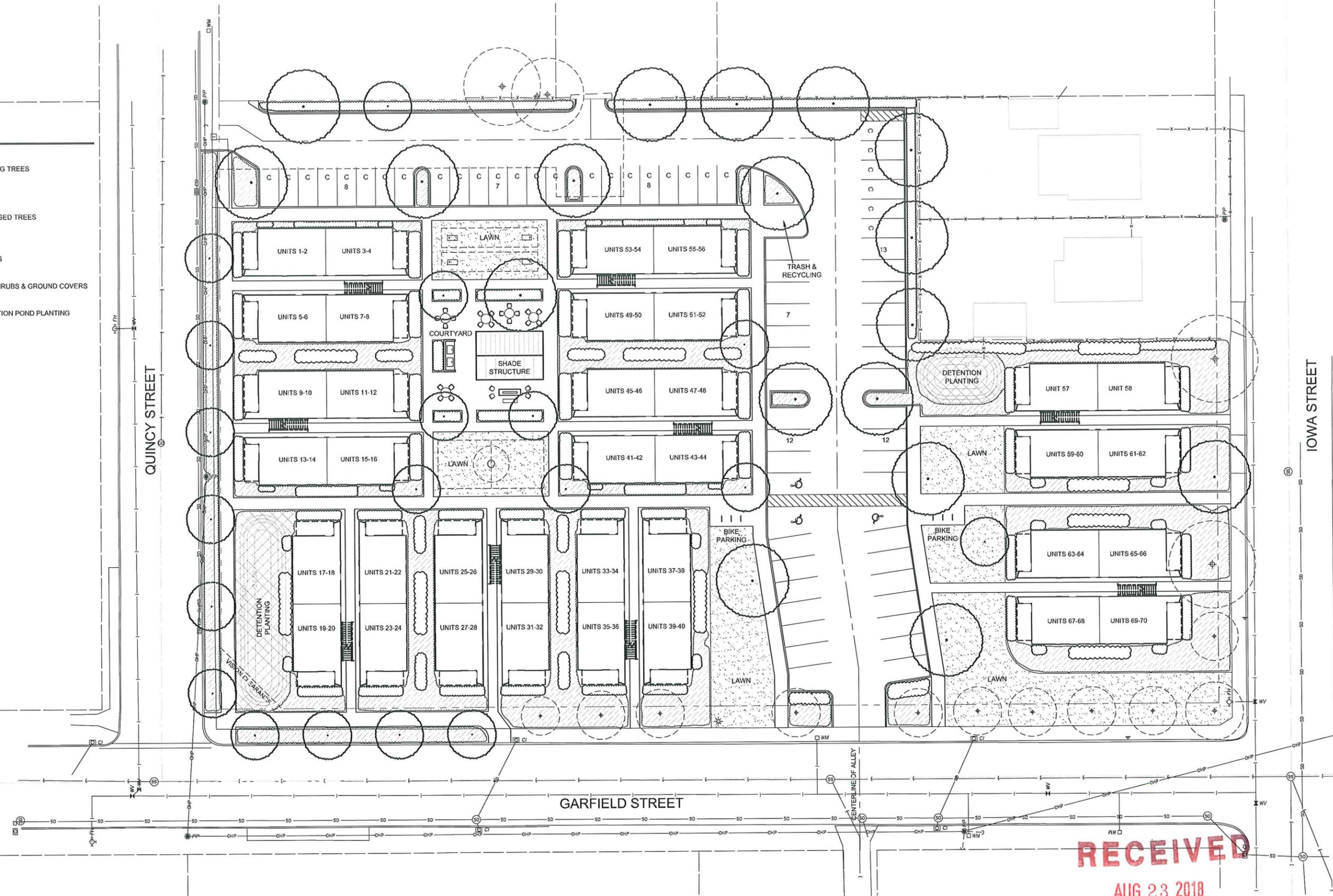
CALL BEFORE YOU DIG
 1-800-332-2344
 48 HOURS BEFORE BEGINNING EXCAVATION
 OREGON LAW REQUIRES YOU TO FOLLOW RULES
 ADOPTED BY THE OREGON UTILITY NOTIFICATION
 CENTER. THOSE RULES ARE SET FORTH IN OAR

current, and the ideas and designs incorporated herein, is the property of Marquess & Associates, Inc. and is not to be used, in whole or in part, for any other project without the written authorization of Marquess & Associates.

PRELIMINARY
THESE DRAWINGS SHALL NOT BE USED FOR:
CONSTRUCTION BIDDING
RECORDATION
CONVEYANCE
ISSUANCE OF A PERMIT
SITE REVIEW

LEGEND

-  EXISTING TREES
-  PROPOSED TREES
-  SHRUBS
-  LOW SHRUBS & GROUND COVERS
-  DETENTION POND PLANTING
-  LAWN



MID-TOWN URBAN LOFTS
SPARTAN GROUP
ASHLAND, OREGON

REVISIONS

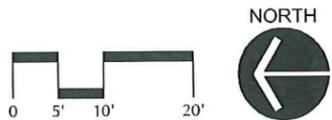
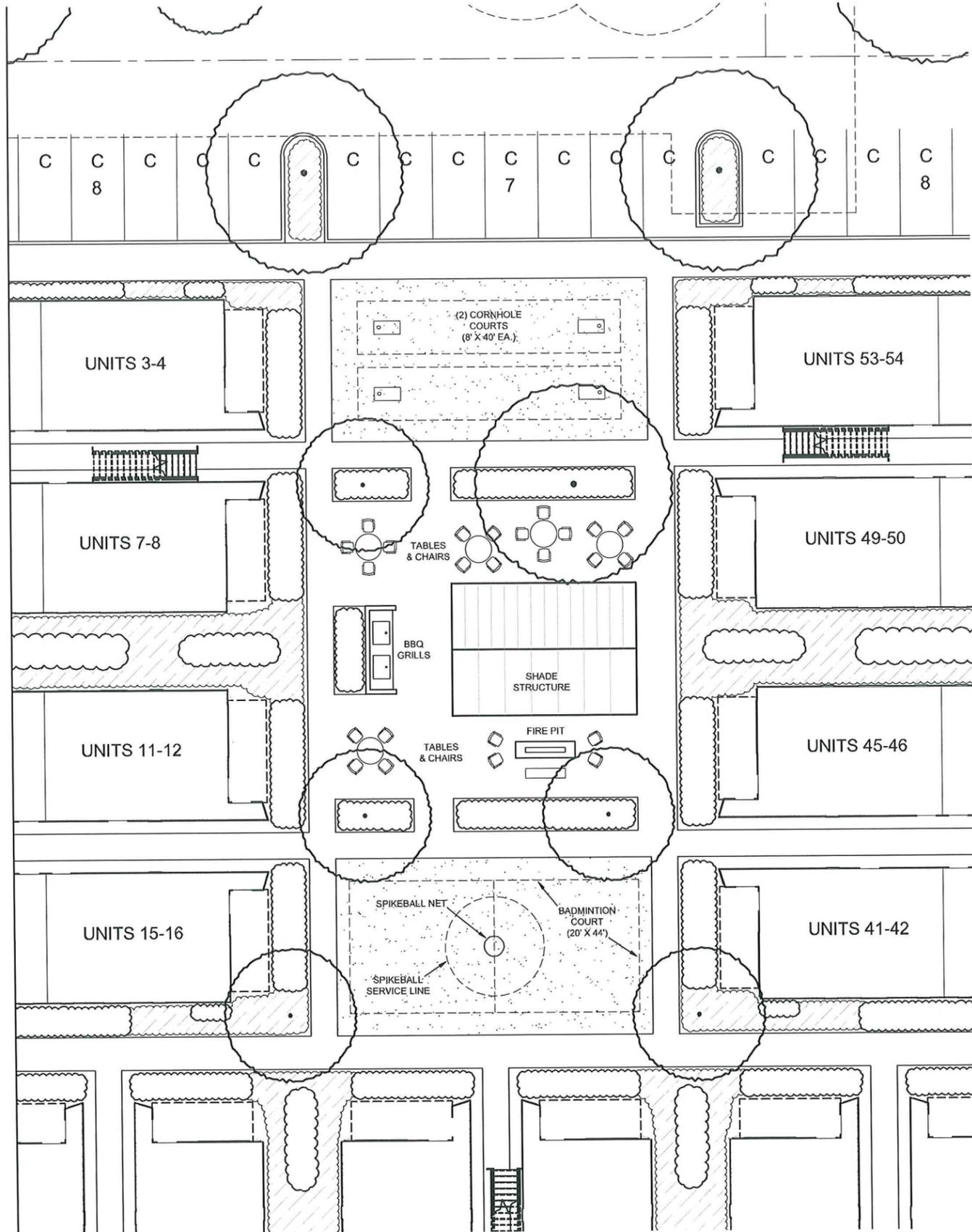
PRELIMINARY
LANDSCAPE
SITE PLAN

PROJECT NO.: -
ISSUE DATE: 07.06.2018
SHEET:

L0.1

PRELIMINARY LANDSCAPE SITE PLAN 1
1" = 20' @ 24x36"

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City of Ashland
NORTH



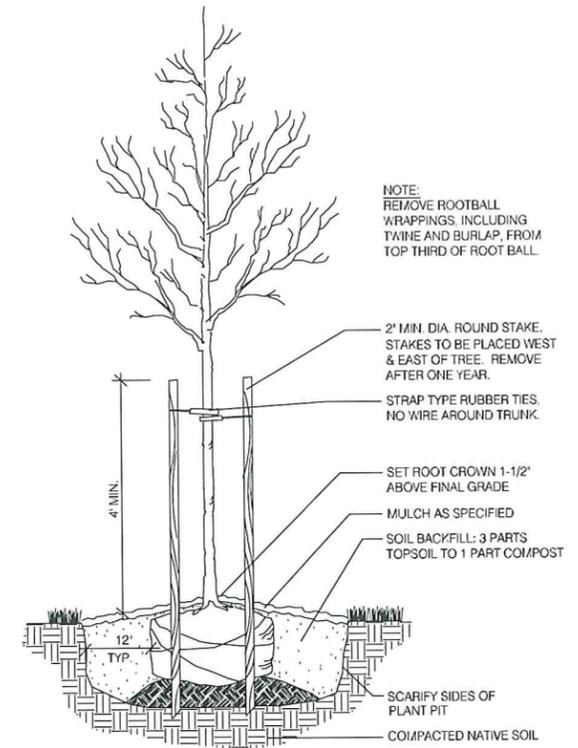
PRELIMINARY COURTYARD PLAN 1
1" = 10' @ 24x36"

PLANT LIST

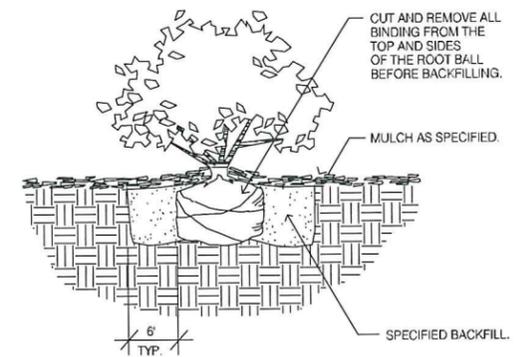
KEY	BOTANICAL NAME	COMMON NAME	SIZE / CONDITION
TREES			
ACGI	ACER GINNALA 'FLAME'	FLAME AMUR MAPLE	1.75" CAL, B&B
ACTR	ACER TRUN. X 'KEITHSFORM'	NORWEGIAN SUNSET MAPLE	1.75" CAL, B&B
COED	CORNUS 'EDDIE'S WHITE WONDER'	EDDIE'S WHT. WONDER DOGWOOD	1.75" CAL, B&B
FASY	FAGUS SYLVATICA 'ASPLENIFOLIA'	CUT LEAF EUROPEAN BEECH	1.75" CAL, B&B
GIBI	GINKGO BILOBA 'AUTUMN GOLD'	AUTUMN GOLD GINKGO	1.75" CAL, B&B
NYSY	NYSSA SYLVATICA	BLACK TUPELO	1.75" CAL, B&B
PAPE	PARROTIA PERSICA	PERSIAN IRONWOOD	1.75" CAL, B&B
PYJA	PYRUS CALLERYANA 'JACZAM'	JACK PEAR	1.75" CAL, B&B
QUFR	QUERCUS FRAINETTO 'SCHMIDT'	FOREST GREEN OAK	1.75" CAL, B&B
QURU	QUERCUS RUBRA	RED OAK	1.75" CAL, B&B
TICO	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN	1.75" CAL, B&B
ZESE	ZELKOVA SERRATA 'GREEN VASE'	GREEN VASE ZELKOVA	1.75" CAL, B&B
SHRUBS			
ARUN	ARBUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY TREE	5 GAL @ 5" O.C.
BEGR	BERBERIS T. 'CRIMSON PYGMY'	CRIMSON PYGMY BARBERRY	1 GAL @ 3" O.C.
BERO	BERBERIS T. 'ROSE GLOW'	ROSE GLOW BARBERRY	5 GAL @ 5" O.C.
BEWM	BERBERIS 'WM. PENN'	WM. PENN BARBERRY	3 GAL @ 4" O.C.
BUKO	BUXUS M. KOREANA 'WINTERGREEN'	WINTERGREEN BOXWOOD	1 GAL @ 3" O.C.
CHTE	CHOISYA TERNATA	MEXICAN ORANGE	5 GAL @ 5" O.C.
COHO	COTONEASTER H. 'PERPUSILLUS'	PROSTRATE ROCK COTONEASTER	1 GAL @ 5" O.C.
COKE	CORNUS S. 'KELSEY'	KELSEY'S RED TWIG DOGWOOD	5 GAL @ 5" O.C.
EUAL	EUONYMUS ALATUS 'COMPACTA'	COMPACT BURNING BUSH	3 GAL @ 4" O.C.
ILCO	ILEX CORNUTA 'CARISSA'	CARISSA HOLLY	2 GAL @ 4" O.C.
LOPI	LONICERA PILEATA	PRIVET HONEYSUCKLE	1 GAL @ 3" O.C.
MAAQ	MAHONIA AQUIFOLIUM	OREGON GRAPE	5 GAL @ 4" O.C.
MACO	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	1 GAL @ 3" O.C.
NADO	NANDINA DOMESTICA 'GULFSTREAM'	GULFSTREAM HEAVENLY BAMBOO	1 GAL @ 3" O.C.
OSHE	OSMANTHUS H. 'GULFTIDE'	GULFTIDE FALSE HOLLY	5 GAL @ 6" O.C.
OSRO	OSMANTHUS H. 'ROTUNDIFOLIUS'	ROUND LEAF FALSE HOLLY	1 GAL @ 4" O.C.
PIAB	PICEA ABIES 'NIDIFORMIS'	BIRD'S NEST SPRUCE	3 GAL @ 5" O.C.
POFR	POTENTILLA FR. 'GOLDFINGER'	GOLDFINGER CINQUEFOIL	1 GAL @ 4" O.C.
RHAR	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	1 GAL @ 5" O.C.
RIAU	RIBES AUREUM	GOLDEN CURRANT	1 GAL @ 5" O.C.
RISA	RIBES SANGUINEUM 'KING EDWARD'	KING EDWARD FLOWERING CURRANT	5 GAL @ 8" O.C.
RONU	ROSA NUTKANA	NOOTKA ROSE	1 GAL @ 5" O.C.
SAPU	SALIX PURPUREA	ALASKA BLUE WILLOW	5 GAL @ 8" O.C.
SARU	SARCOCCOA RUSCIFOLIA	SWEET BOX	1 GAL @ 4" O.C.
SPAN	SPIRAEA X B. 'ANTHONY WATERER'	ANTHONY WATERER SPIREA	1 GAL @ 4" O.C.
GROUND COVERS, GRASSES, RUSHES & SEDGES			
ARCTO.	UVA-URSI 'MASS.'	MASSACHUSETTS KINNIKINICK	1 GAL @ 36" O.C.
	RUBUS CALYCIINOIDES	CREeping RASPBERRY	1 GAL @ 36" O.C.
CAOB	CAREX OBNUPTA	SLOUGH SEDGE	1 GAL @ 3" O.C.
HESE	HELICTOTRICHON SEMPERVIRENS	BLUE OAT GRASS	1 GAL @ 3" O.C.
JUEF	JUNCUS EFFUSUS	SOFT RUSH	1 GAL @ 3" O.C.
PEHA	PENNISETUM A. 'HAMELN'	HAMELN FOUNTAIN GRASS	1 GAL @ 3" O.C.
PELI	PENNISETUM A. 'LITTLE BUNNY'	LITTLE BUNNY FOUNTAIN GRASS	1 GAL @ 3" O.C.
SAHE	SALVIA M. x G. 'HEATWAVE'	HEATWAVE SAGE	1 GAL @ 30" O.C.
TECH	TEUCRIUM CHAMAEDRYS	CREeping GERMANDER	1 GAL @ 30" O.C.

GENERAL NOTES

- VERIFY LOCATIONS OF ALL BELOW GRADE UTILITIES PRIOR TO BEGINNING WORK. OWNER IS RESPONSIBLE FOR COORDINATING ALL UTILITY LOCATES.
- TOPOGRAPHIC SURVEY OF EXISTING CONDITIONS PROVIDED BY POLARIS LAND SURVEYING, LLC.
- OWNER WILL PROVIDE AN AUTOMATIC IRRIGATION SYSTEM DESIGN FOR ALL NEW LANDSCAPE AREAS AT THE TIME OF BUILDING PERMIT SUBMITTAL.



2 TREE PLANTING
NOT TO SCALE



3 SHRUB PLANTING
NOT TO SCALE

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City Of Ashland

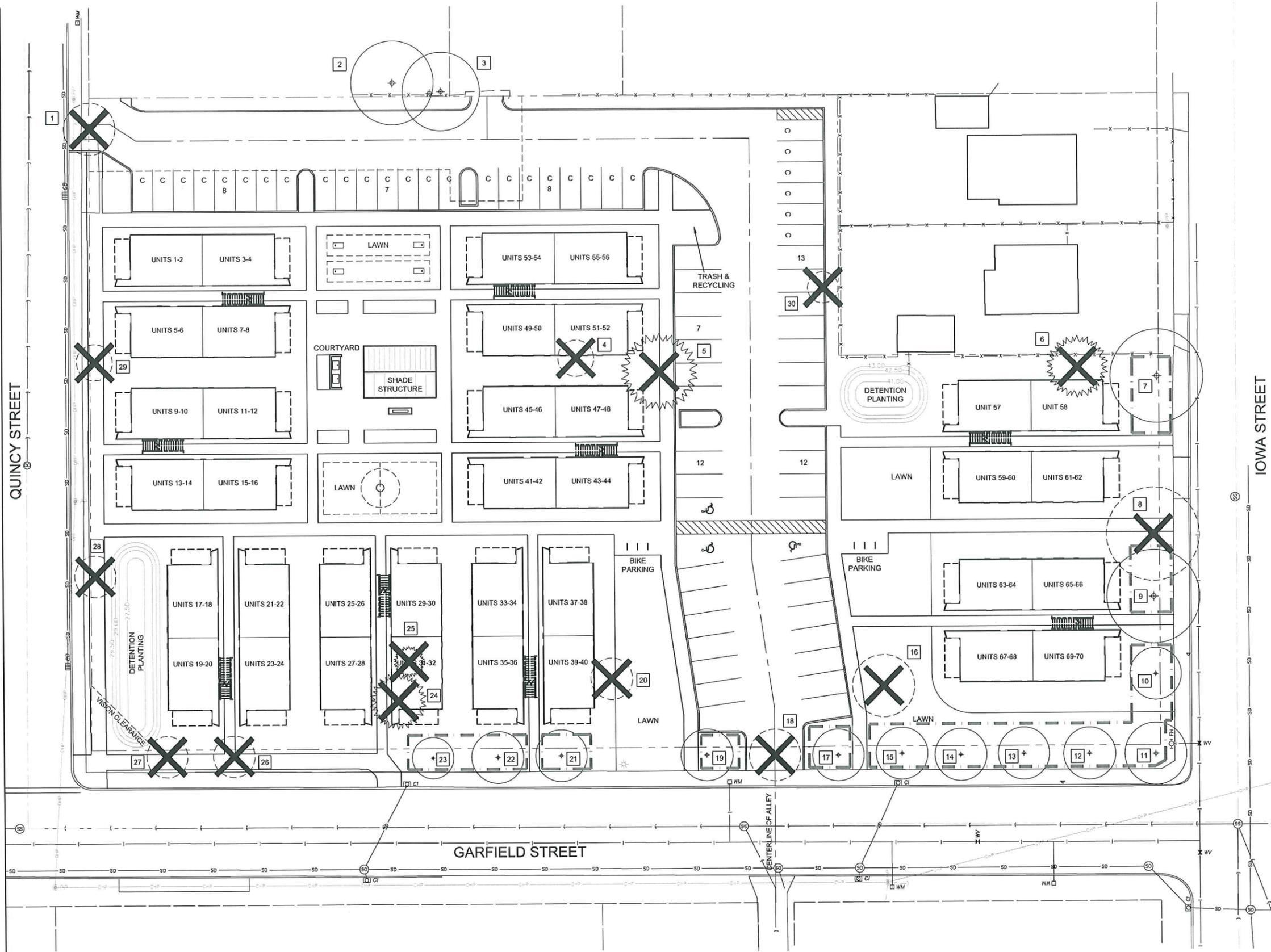
PRELIMINARY
THESE DRAWINGS SHALL NOT BE USED FOR: CONSTRUCTION, BIDDING, RECORDATION, CONVEYANCE, ISSUANCE OF A PERMIT.
SITE REVIEW

MID-TOWN URBAN LOFTS
SPARTAN GROUP
ASHLAND, OREGON

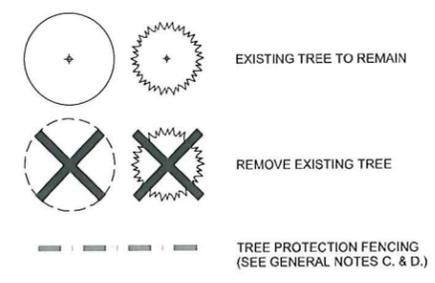
PLANT LIST & COURTYARD PLAN

PROJECT NO.: -
ISSUE DATE: 07.06.2018
SHEET:

L0.2



LEGEND



GENERAL NOTES

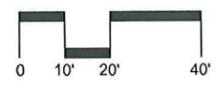
- A. Verify locations of all below-grade utilities prior to beginning work. Contractor is responsible for coordinating all utility locates.
- B. Topographic survey provided by Polaris Land Surveying, LLC.
- C. The information contained on this Tree Protection & Removal Plan supercedes the labels on the topographic survey for existing tree species and sizes.
- D. Contractor shall provide, install, and maintain tree protection fencing according to City of Ashland Municipal Code, Tree Preservation and Protection, Section 18.4.5.030 Part C, Tree Protection Measures Required.
- E. Final location of tree protection fencing shall be determined by Landscape Architect.
- F. See Specifications Section 01 56 39 - Temporary Tree and Plant Protection for additional requirements.

EXISTING TREES

- | | |
|----|--|
| 1 | 12" REDBUD |
| 2 | BLACK LOCUST, (5) STEMS @ 10-12" |
| 3 | BLACK LOCUST, (3) STEMS @ 10" |
| 4 | 11" NORWAY MAPLE |
| 5 | 60" LEYLAND CYPRESS (DEAD, HAS BEEN REMOVED) |
| 6 | LEYLAND CYPRESS, REMOVED PRIOR TO THIS PROJECT |
| 7 | 40" SIBERIAN ELM |
| 8 | 14" MULBERRY |
| 9 | 15" MULBERRY |
| 10 | 12" SWEETGUM |
| 11 | 14" SWEETGUM |
| 12 | 12" SWEETGUM |
| 13 | 10" SWEETGUM |
| 14 | 12" SWEETGUM |
| 15 | 10" SWEETGUM |
| 16 | MAPLE, REMOVED PRIOR TO THIS PROJECT |
| 17 | 12" SWEETGUM |
| 18 | 12" SWEETGUM |
| 19 | 12" SWEETGUM |
| 20 | 8" CHERRY |
| 21 | 19" SWEETGUM |
| 22 | 17" SWEETGUM |
| 23 | 11" SWEETGUM |
| 24 | 19" SPRUCE, DOUBLE-LEADER |
| 25 | 19" SPRUCE, REMOVED PRIOR TO THIS PROJECT |
| 26 | 8" REDBUD |
| 27 | 10" CHERRY |
| 28 | 9" REDBUD |
| 29 | 7" REDBUD |
| 30 | 6" CRABAPPLE |

TREE PROTECTION & REMOVAL PLAN

1" = 20' @ 22x34"



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AUG 23 2018
City Of Ashland

PRELIMINARY
THESE DRAWINGS SHALL NOT BE USED FOR: CONSTRUCTION BIDDING RECORDATION CONVEYANCE ISSUANCE OF A PERMIT
SITE REVIEW

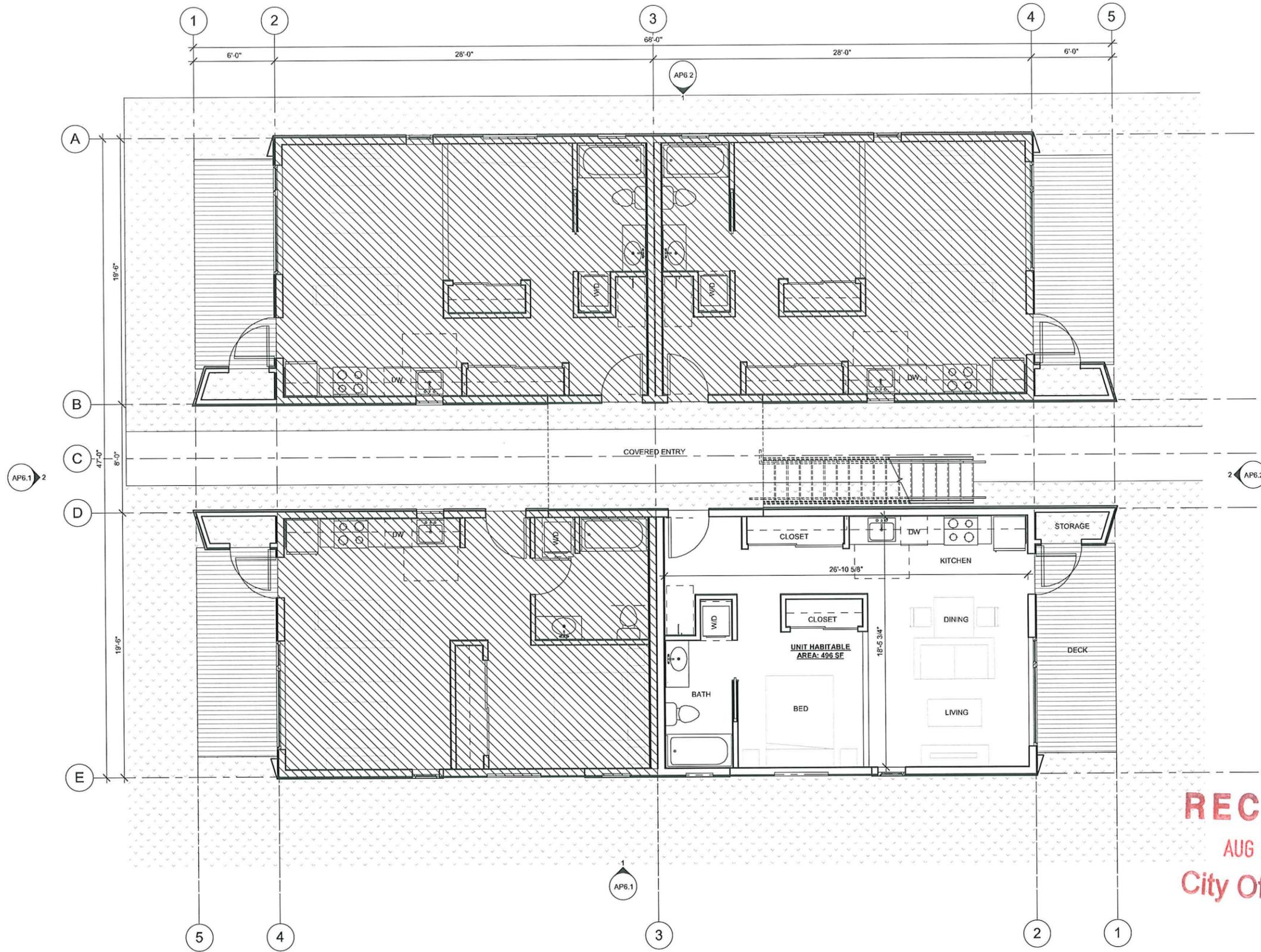
MID-TOWN URBAN LOFTS
SPARTAN GROUP
ASHLAND, OREGON

REVISIONS

TREE PROTECTION & REMOVAL PLAN

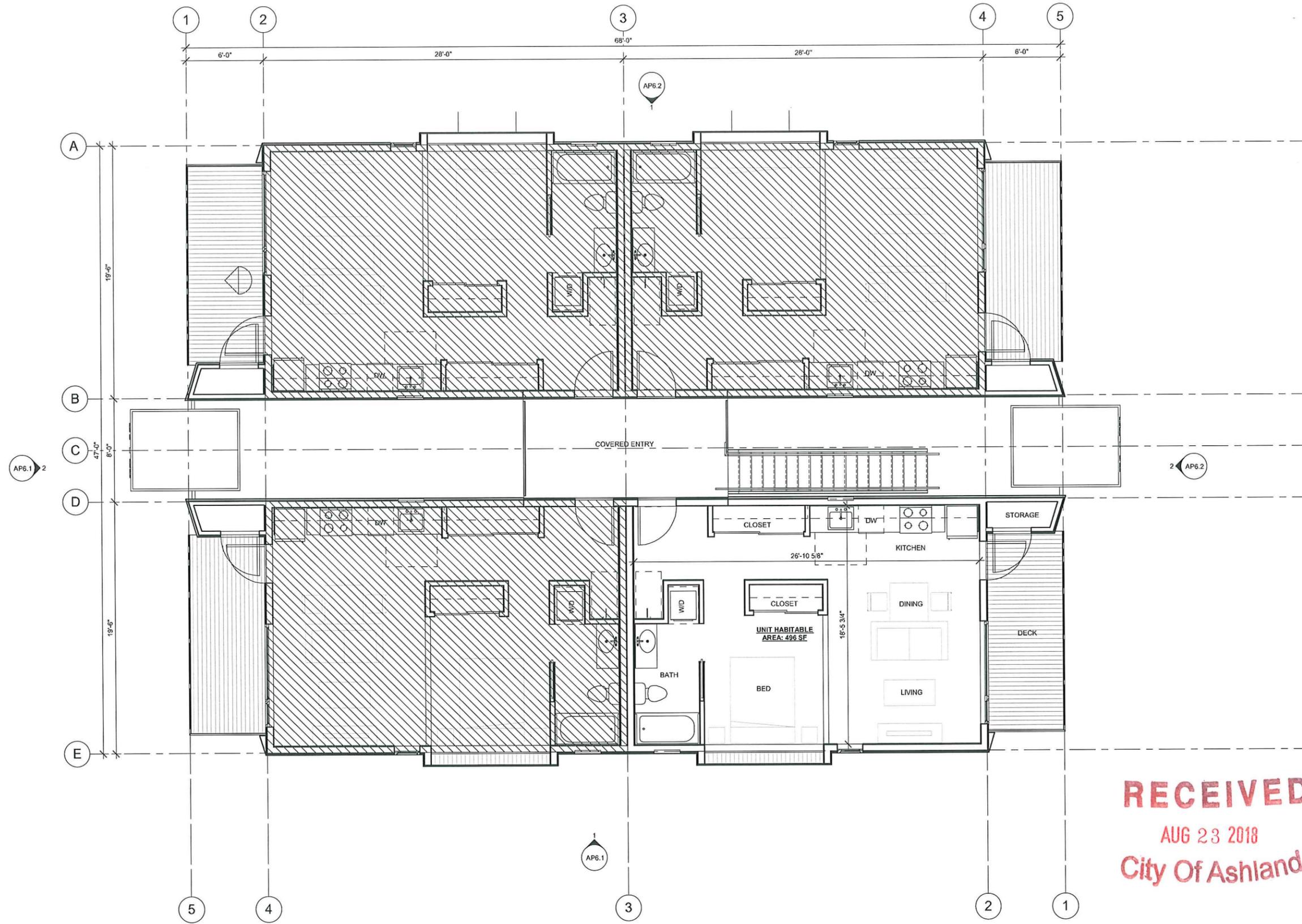
PROJECT NO.: -
ISSUE DATE: 08.15.2018
SHEET:

L0.3



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PLAN - LEVEL 1
 1/4" = 1'-0" 1



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PLAN - LEVEL 2
 1/4" = 1'-0" 1



STREET ELEVATION - IOWA ①
1" = 10'-0"



STREET ELEVATION - GARFIELD ②
1" = 10'-0"



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STREET ELEVATION - QUINCY ③
1" = 10'-0"



EXTERIOR ELEVATION - WEST 2
3/16" = 1'-0"



EXTERIOR ELEVATION - SOUTH 1
3/16" = 1'-0"

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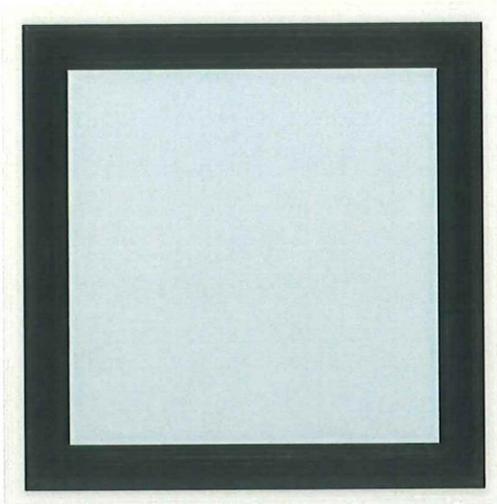


EXT ELEV - NORTH 1
3/16" = 1'-0"



EXT ELEV - EAST 2
3/16" = 1'-0"

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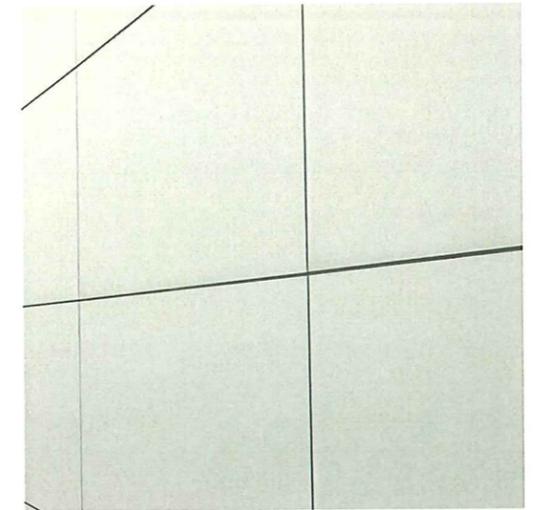
VINYL WINDOWS - DARK BRONZE



GUARDRAIL



SIDING - CORRUGATED METAL



SIDING - CEMENT PANEL



INTERIOR BIKE RACK



EXPOSED STEEL BEAMS



ROOF - STANDING SEAM METAL



SIDING - WOOD



ENTRY CANOPY



SIDING - WEATHERED WOOD

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**TYPE II
REVIEWS**

**PA-T2-2018-00004
1661 Ashland Street**



KISTLER SMALL + WHITE ARCHITECTS
 66 WATER STREET, SUITE 101 ASHLAND, OR 97520
 P.541.488.8200 | F.541.552.9512

TRANSMITTAL

TO: CITY OF ASHLAND BUILDING DIVISION

OF: COLUMBIA CARE – ROGUE RIDGE

FROM: KISTLER SMALL + WHITE ARCHITECTS

DATE: 08.24.2018

REGARDING: SITE REVIEW SUBMITTAL FOR COLUMBIA CARE ROGUE RIDGE

COMMENTS:

PLEASE FIND THE FOLLOWING DELIVERED ITEMS:

- 1 COPY FULL SIZE DRAWING SET (24X36)
- 3 COPIES HALF SIZE DRAWING SET (11X17)
- 2 COPIES EXTERIOR LIGHT FIXTURE CUT SHEETS
- 2 COPIES FINDINGS
- 1 APPLICATION FORM
- 1 CHECK FOR SITE REVIEW FEE AND LAND PARTITION FEE

ACCEPTED BY:

ACCEPTED ON:

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City of Ashland

BEFORE THE PLANNING COMMISSION FOR
THE CITY OF ASHLAND, OREGON

IN THE MATTER OF AN APPLICATION)	
FOR A MULTIFAMILY DEVELOPMENT)	
WITHIN THE C-1 ZONING DISTRICT ON)	FINDINGS OF FACT
1.02 ACRES LOCATED ALONG ASHLAND)	AND
STREET; DESCRIBED AS T.39S-R.1E-)	CONCLUSIONS
S.10DC, TAX LOT 9201; COLUMBIA CARE)	
SERVICES, INC., APPLICANT; KISTLER)	
SMALL & WHITE ARCHITECTS, AGENT)	

I. RECITALS:

OWNER: Rogue Credit Union
1370 Center Drive
Medford, OR 97504

APPLICANT: Columbia Care Services, Inc.
3587 Heathrow Way
Medford, OR 97504
(541) 858-8170

ARCHITECTS: Kistler Small & White Architects
66 Water Street, Ste. 101
Ashland, OR 97520
(541) 488-8200

CONSULTANTS: Richard Stevens & Associates, Inc.
PO Box 4368
Medford, OR 97501
(541) 773-2646

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PURPOSE:

The applicants are proposing to develop Rogue Ridge as a multifamily project within one structure on the subject property, demonstrating consistency with the approved shadow plan for Rogue Credit Union, Planning Action #2016-01894. Rogue Ridge is proposing to have 30 multifamily dwelling units that are proposed to have one unit as the manager's office/residence with the remaining 29 units as affordable housing units for low income families and persons.

Columbia Care is a non-profit, mental health agency in the business of changing people's lives. Columbia Care helps people become self-sufficient to the greatest extent possible, to experience wellbeing, and meet their full potential in their relationships and everyday life. Columbia Care specializes in designing, developing and operating a full continuum of mental health care in the community, whereby people can receive the most clinically appropriate treatment based on their individual needs and strengths. A part of their philosophy is the understanding that we must first meet people's most basic needs as human beings, as it is foundational to the rest of the treatment and support we provide to help them move forward in their recovery/healing process. The first basic goals are addressing their need for safe and stable housing.

Rogue Ridge is proposed to contain 30 dwellings units to be an affordable housing program that is consistent with Columbia Care's goals as an agency to provide safe and stable affordable housing with the necessary services and support that assist individuals to live successfully in the community.

II. APPLICABLE CRITERIA:

In order to approve a Site Design Review, the applicant must submit findings addressing Chapter 18.5 of the Ashland Land Use Ordinance (LUO). A land division "Partition" is also proposed with this project. Chapter 18.5.2 provides the standards and approval criteria for a Site Design Review and Chapter 18.5.3 provides the partition standards for approval.

Section 18.5.2.020(B)(1) is applicable when two or more dwelling units, including an accessory residential unit, are proposed on a lot in any zoning district, as prescribed in Section 18.5.2.030. Compliance with Section 18.2.3.130 is also applicable.

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Section 18.5.2.030(B) applies as the subject property is zoned C-1 and is proposing new construction for affordable multifamily dwelling units as a mixed use project, will be a Type II review.

Section 18.5.2.040 identifies the site plan requirements for submission. The Site Design Review Information prepared by Kistler Small & White Architects has addressed these requirements as outlined.

Section 18.5.2.050 identifies the approval criteria for a Site Design Review application.

Section 18.5.3 is applicable as the applicant, Columbia Care, is proposing a two lot partition with a flag pole to be used for shared access with adjacent properties.

Section 18.4.3 is applicable for the vehicle and bicycle parking requirements.

Section 18.4.7 provides the standards for signs within the C-1 district.

COMPLIANCE WITH CHAPTER 18.5.2:

Section 18.5.2.010 describes the purpose as:

“The purpose and intent of this chapter is to regulate the manner in which land in the City is used and developed, to reduce adverse effects on surrounding property owners and the general public, to create a business environment that is safe and comfortable, to further energy conservation efforts with the City, to enhance the environment for walking, cycling, and mass transit use, and to ensure that high quality development is maintained throughout the City.”

Section 18.5.2.050: Approval Criteria:

“An application for site Design Review shall be approved if the proposal meets the criteria in subsections A, B, C, and D below. The approval authority may, in approving the application, impose conditions of approval, consistent with the applicable criteria.”

A. Underlying Zone. *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”*

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Discussion:

The subject property is zoned C-1 with the City of Ashland. The original shadow plan submitted for Rogue Credit Union is the template used for allowing mixed uses on the subject property to demonstrate compliance.

Section 18.2.3.130(B) is also applicable for proposing a multifamily structure within the C-1 district. The special use standards for review are:

1. If there is one building on a site, ground floor residential uses shall occupy not more than 35 percent of the gross floor area of the ground floor. Where more than one building is located on a site, not more than 50 percent of the total lot area shall be designated for residential uses.

There is a single multifamily structure that contains two wings that are connected on the second and third floors for accessible access, proposed with the Rogue Ridge development. The approved 2016 shadow plan identified four separate structures that could be developed. The proposal before the City is that three separate structures can be constructed. Therefore, more than one building is being proposed consistent with the shadow plan, Rogue Credit Union, Rogue Ridge and the potential future development on Parcel 2.

Based on the approved shadow plan layout with the identified uses listed, it is calculated that 48.9% of the shadow plan is used for residential purposes. The shared access drive, flag drive, is divided out per the percentage of potential vehicle trips and uses, based on the Southern Oregon Transportation Engineering TIA Memo, dated 8/17/18 for the shadow plan. Only 200 ADT's of the calculated 915 ADT's, or 21.8% are used for residential purposes. See site areas by use attached.

2. Residential densities shall not exceed 15 dwelling units per acre in the E-1 zone, 30 dwelling units per acre in the C-1 zone, and 60 dwelling units per acre in the C-1-D zone. For the purpose of density calculations, units of less than 500 square feet of gross habitable floor area shall count as 0.75 of a unit.

The density of the original shadow plan does not exceed the 48 total units or the 33 total dwelling units that could be developed on "Lot 1" of the shadow plan within the C-1 district.

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Rogue Ridge is proposing 25 Studio units that are less than 500 sq.ft. of gross habitable floor area; therefore, these units total 18.75(19) dwelling units with 5 additional two and three bedroom units for a total of 24 dwelling units to meet the density standard for the approved shadow plan. The calculated 24 dwelling units will also meet the density standard for the proposed land division on Parcel 1 with the Tentative Plat from Polaris Land Surveying.

3. Residential uses shall be subject to the same setback, landscaping, and design standards as for permitted uses in the underlying zone.

The applicant's architect has designed the project to meet the setbacks, including solar setbacks for the abutting residential properties abutting to the north, and the landscaping areas to meet the design standards of the C-1 district. See solar setback and site plan attachments to demonstrate compliance. In addition, Kistler Small & White Architects have designed the eastern side yard setback to have 20-feet for the first two stories and a 10-foot step back design for the third floor to meet side yard setbacks to the residential zoning abutting to the east. The site plan also identifies the landscaped areas to meet the minimum 15% requirements for being within the C-1 district.

4. Off-street parking is not required for residential uses in the C-1-D zone.

Not applicable, the subject property is not zoned C-1-D.

5. Where the number of residential units exceeds ten, at least ten percent of the residential units shall be affordable for moderate-income persons...

The applicant, Columbia Care, is providing 100% of the dwelling units for affordable housing, other than the manager's office/residence with the development of Rogue Ridge. The rental charges will be consistent with the provisions in Subsection 18.2.5.050.B and the table found in 18.2.5.050.B

Finding:

The City of Ashland can find that the proposed residential density of the project does not exceed the density threshold based on the approved shadow plan for Rogue Credit Union and the proposed partition. The multifamily residential units are consistent with the C-1 zone provisions and all of the units are proposed to be available for low income housing, consistent with the provisions in Subsection 18.2.5.050.B.

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B. Overlay Zones. *“The proposal complies with applicable overlay zone requirements (part 18.3).”*

Discussion:

There are currently two overlays applicable with the subject property: the Detail Site Review Overlay and the Pedestrian Places Overlay. The Detail Site Review is discussed below and the Pedestrian Places Overlay regarding Building and Solar Setbacks along with Landscaping/Outdoor Plazas.

Upon review of the site plan, the setbacks have been established to meet the Solar Setback from the properties to the north that are zoned for residential purposes, see Solar Setback Exhibit to demonstrate compliance. The outdoor public space, outdoor plaza, is provided within the northern area of Parcel 2, meeting the standards of the Ordinance. The public plaza consists of approximately 26.5% of pervious area as outlined in green on the site plan. The public plaza will contain a sitting area, areas that provide both sunlight and shade, protection from wind, trees and an outdoor eating area. This outdoor area will be available to all persons within the original shadow plan for Rogue Credit Union. There are no changes from the approved shadow plan for the structural street frontage standard, which remains at 63%.

Finding:

The City of Ashland can find that there are two overlays present on the subject property, the Detail Site Review Overlay and the Pedestrian Places Overlay. The site plan and proposed development has been prepared with these overlays in mind, to demonstrate compliance.

C. Site Development and Design Standards. *“The proposal complies with the applicable site development and Design Standards of part 18.4, except as provided by subsection E, below.”*

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Discussion:

Section 18.4.2.030 is applicable for multifamily developments. Kistler Small & White, Architects have designed the site plan to meet the standards for parking layout for being in close proximity and visible from adjacent areas; the orientation of windows have been

thoughtfully located for viewing the parking and open space areas; the service and laundry areas are located within the structure to ensure security; the lighting proposed has considered the adjacent residential lands and are using 14-foot poles with the fixtures directed away from the residential uses to mitigate any significant impacts on the adjacent residential lands. In addition, each individual light will have an attached motion sensor for full illumination for a specified time and then reduce power to 30% illumination when no movements are present or detected.

The residential building on Parcel 1 and the future building on Parcel 2 are located and orientated towards Ashland Street, with no parking area proposed between the structures and the public street. There are no garages proposed with this development of Rogue Ridge.

The provisions of Section 18.4.2.040.B (Basic Site Review Standards) may also be applicable for the mixed uses of the project.

All proposed buildings are oriented toward the public street with the parking and maneuvering area located behind the proposed structures.

Multiple building facades are visible for a large percentage of the project frontage on Ashland Street. Approximately 63% of the buildings are fronting the street, with no changes to the shadow plan. The multifamily structure is also oriented towards the street with multiple facades present to provide an architectural element. All building entrances fronting Ashland Street have entrances within 20-feet of the public street and are oriented towards the street that will be accessed from the public sidewalk, as demonstrated on the site plan.

The streetscape and the landscaping will be in compliance with the Ordinance, as demonstrated on the Landscape plans submitted. The refuse/recycle disposal area is within a screened enclosure with gates, as identified on site plan, demonstrating compliance with the Ordinance.

The noise and lighting are regulated by the Ordinance and the applicant agrees to compliance with the standards.

The criterion in Section 18.4.2.040.C (Detailed Site Review Standards) is applicable for the project area, as identified with the approved shadow plan. The applicant is proposing a multifamily development on Parcel 1 and anticipated future commercial/mixed use structure on Parcel 2.

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The shadow plan demonstrated that with the future development of the site, a minimum of 50% Floor Area Ratio (FAR) could be achieved. The applicant's architect has designed and prepared the structures with a gross floor area that exceeds the 50% requirement with the shadow plan area as demonstrated on the site plan along with the square footage of uses that achieves the 50% FAR threshold.

The multifamily structure for Rogue Ridge will have frontage along Ashland Street that is greater than 100 feet in length. The front façade has numerous offsets to break up the continuous mass effect as seen on the front elevations submitted with the application. In addition, the applicant is proposing to use lateral and horizontal textures with differing styles to also break up the mass look, along with a variation of colors to provide distinctive changes with the front elevation appearance.

The site plan demonstrates that the building orientation is toward Ashland Street with the parking area located behind the proposed structures. The building on Parcel 2 will be within 20 feet of the public right of way and the structure on Parcel 1 is located as close as possible to the street, due to the site configuration as a flag lot. The design of the structures is in compliance with the standards in Subsection 18.4.2.040(C).

The parking requirements are found in Section 18.4.3. It is determined that 35 off-street parking space are needed for the multifamily development on Parcel 1. There are 25 studio units that are less than 500 sq.ft.; 3 two-bedroom units averaging 710 sq.ft.; and 2 three-bedroom units averaging 1300 sq.ft. The applicants are proposing to have a total of 30 parking spaces within Parcel 1. The parking area along the northern boundary already contains fencing ranging from 6-8 feet in height, therefore, no new fencing is being proposed. The applicant is proposing new landscaping between the fence and parking area to further mitigate any impacts from the parking and maneuvering of vehicles.

The applicant is requesting a reduction to the parking requirements found in Table 18.4.3.040 for Multifamily development. Due to Columbia Care's past experience with the anticipated occupants for these units, many of these occupants don't own a vehicle and rely on bicycles, transit transportation (RVTD) or walking to commute to work or shopping. Numerous multifamily projects in the Rogue Valley typically have less than 50% of their low income residents own or operate a vehicle. This is also supported by an Affordable Housing Parking Study, prepared by the City of San Diego, which includes Eugene, Oregon with reduced standards for affordable housing. It has been determined that approximately 47.5% of residents within affordable housing developments don't own a vehicle.

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The City of Eugene, Oregon also reflects this reduction by allowing a .67 parking space for each habitable room for studio and one-bedroom units. Using the proposed 25 studio units, this would calculate to 16.75 (17) parking spaces needed, a reduction of 8 spaces from Table 18.4.3.040. Therefore, a total of 27 parking spaces would be needed. Based on the parking study, if approximately 47% of the residents don't own a vehicle this number could be reduced even further.

The anticipated future use of Parcel 2 is commercial/mixed use. Therefore, the applicant is proposing to provide the joint use of parking with Parcel 2. The residential occupants typically use the parking area during evenings and weekends, where the commercial occupants will use the parking area during business hours, typically 8:00am to 6:00pm, Monday through Friday.

There are no specific uses on Parcel 2 anticipated at this time; however, it is anticipated that an additional 5-6 parking spaces may be warranted. With the documented vehicle-less residents and the anticipated day time only for the commercial use, the joint use of parking facilities can be satisfied for both Parcels 1 and 2. The applicant agrees to grant a lease or written instrument to ensure such joint use. In addition, the proposed joint use of parking facilities may have a 50% reduction in the number of off-street parking spaces on Parcel 2, as provided in 18.4.3.060.D.

Section 18.4.3.070 provides the standards for bicycle parking. There are two separate areas for covered bicycle parking, as identified on the site plan. It is required to have 34 sheltered bicycle parking spaces and the applicant agrees to provide.

Section 18.4.7 provides the standards for signs. The applicant is proposing signage for the project Rogue Ridge. The location of the sign is proposed on the overhead walkway and facing towards the south away from any residential zone. The sign will have 16-inch lettering that will face the C-1 zoning district and Ashland Street.

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Finding:

The City of Ashland can find that the site plan and elevation drawings have been designed to meet the site design and development standards, to be in compliance with Section 18.4. In addition, with the shared off-street parking facilities between Parcel 1 and Parcel 2 and the documented parking study, a reduction of 8 parking spaces on Parcel 1 is requested, and a 50% reduction may be allowed on Parcel 2, consistent with the day / night nature for the joint use of parking facilities for the proposed uses.

D. City Facilities. *“The proposal complies with the applicable standards in Section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.”*

Discussion:

The water facilities are adjacent to the property with an 8-inch line in Ashland Street. New water connections and meters will be needed with the future development. The sanitary sewer facilities are provided with two separate 6-inch lines flowing northerly to Parker Street. The storm drain facilities will connect to a 12-inch main line in Ashland Street. The applicant’s engineer will design these public facilities to ensure sufficient capacity is provided.

The electrical has recently been upgraded to meet anticipated demand. Upon several conversations with Mr. Tigerson with the Electrical Department, upgrades to the system, being the transformer and an additional service line, is warranted for the anticipated electrical demand. The applicant’s electrical engineer will develop a plan to meet the future electrical needs for the future development.

The traffic generation potential was reviewed with a Traffic Impact Analysis (TIA) prepared by Southern Oregon Transportation Engineering, with the original shadow plan for the project. Ms. Parducci, with Southern Oregon Transportation has prepared an amended analysis, see attached, that demonstrates that the proposed uses will have a reduced impact on traffic generation than what was originally approved.

Finding:

The City of Ashland can find that sufficient capacity exists with the public facilities to serve the future development of the site, or improvements can be made if warranted by the engineer.

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E. Exception to the Site Development and Design Standards.

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No exceptions are requested with this application. The proposed development, Floor Area Ratio and partition are consistent with the approved shadow plan and are consistent on Parcel 1 with the proposed partition.

CONCLUSIONS:

The City of Ashland concludes that Kistler Small & White Architects have designed the development of Rogue Ridge as an affordable housing project within the C-1 zone to meet the standards in Section 18.2.3.130 for the special use standards. The development of Rogue Ridge is also in compliance with Section 18.3 for the Pedestrian Places Overlay and the Detail Site Review Overlay. In addition, the proposed project is in compliance with Basic Site Review and Detailed Site Review Standards found in Section 18.4.

COMPLIANCE WITH CHAPTER 18.5.3:

Section 18.5.3.010, states:

“The purpose of this chapter is to provide rules, regulations and standards governing the approval of subdivisions, partitions and property line adjustments as follows.

- A. *Carry out the development pattern envisioned by the Comprehensive Plan.*
- B. *Encourage efficient use of land resources and public services, and to provide transportation options.*
- C. *Protect the natural environment and encourage sustainable building practices.*
- D. *Promote the public health, safety and general welfare through orderly and efficient urbanization.*
- E. *Coordinate land division requirements with other code provisions such as the Performance Standards Option.”*

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Section 18.5.3, Preliminary Partition Plat Criteria:

“The approval authority shall approve an application for preliminary partition plat approval only where all of the following criteria are met.”

A. *"The future use for urban purposes of the remainder of the tract will not be impeded.*

Discussion:

The proposed partition reflects the future development of Parcel 2 and the future building and use. The shared access way will also provide vehicle access to Parcel 2. The proposed partition will not impede any development on the remaining vacant area.

Finding:

The City of Ashland can find that the proposed partition will not impact or impede any future development on the vacant area, Parcel 2, consistent with Subsection (A).

B. *"The development of the remainder of any adjoining land or access thereto will not be impeded."*

Discussion:

The proposed partition will provide access to Parcel 2 and access to Tax Lot 9202 to the east through the shared access way. Rogue Credit Union, Tax Lot 8700 also uses this shared access way for maneuvering. Tax Lots 8700, 9202 and 9800 have legal easements for access through the subject property. This is planned and proposed to reduce the number of road approaches onto Ashland Street (Hwy. 66), an arterial street.

Finding:

The City of Ashland can find that the proposed partition will not prohibit or impede any access to adjoining lands, consistent with Subsection (B).

C. *"The partition plan conforms to applicable City-adopted neighborhood or district plans, if any, and any previous land use approvals for the subject area.*

Discussion:

The proposed partition and uses are consistent with the shadow plan approved by the Ashland City Council, Planning Action #2016-01894.

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D. *"The tract of land has not been partitioned for 12 months."*

Discussion:

The subject property has not been a part of a partition in the past 12 months. The subject site was reconfigured with an approved property line adjustment with Tax Lot 8700 to reflect the current configuration.

Finding:

The City of Ashland can find that the subject property has not been a part of a land division within the last 12 months.

E. *"Proposed lots conform to the requirements of the underlying zone, per part 18.2, any applicable overlay zone requirements, per part 18.3, and any applicable development standards, per part 18.4 (e.g., parking and access, tree preservation, solar access and orientation).*

Discussion:

The subject property is zoned C-1 and there are no minimum lot width, depth or area standards to meet. Parcel 1 does abut a residential zone; therefore, solar setback standards are applicable. The solar setback plan attached, demonstrates compliance with the solar setback standard.

Finding:

The City of Ashland can find that there are no minimum standards for new parcels within the C-1 district. The solar setback exhibit demonstrates that the proposed structure on Parcel 1 meets the standards of the Ordinance.

F. *"Accesses to individual lots conform to the standards in Section 18.4.3.080 Vehicle Area Design. See also, 18.5.3.060 Additional Preliminary Flag Lot Partition Plat Criteria."*

Discussion:

There is one main road approach to serve the subject site and adjacent parcels with access. Tax Lot 8700 does have a right out only movement to preserve the public arterial street function.

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The shared access way is planned to have 20-feet of driving surface with a Fire Apparatus work area totaling 26-feet, as requested by the Ashland Fire Department. The shared access provides vehicular circulation for the subject property and also the adjacent properties to accommodate the expected vehicles. Also provided adjacent to the shared access are pedestrian access sidewalks to the public street sidewalk. As requested by the Fire Marshall, a red stripe along the curb will be placed to designate no parking within the shared access drive.

Section 18.5.3.060 is addressed below, within subsection (K).

Finding:

The City of Ashland can find that the proposed shared access way is in conformance with the standards of the Ordinance by providing suitable traffic circulation for all adjacent properties. In addition, the 26-foot Fire Apparatus work area meets the Fire Marshall's request and need.

G. 'The proposed streets, utilities, and surface water drainage facilities conform to the street design standards and other requirements in part 18.4, and allow for transitions to existing and potential future development on adjacent lands. The preliminary plat shall identify all proposed public improvements and dedications.

Discussion:

There are no public streets proposed with this land division. The public utilities and storm drainage has been addressed with the information from Mr. Mark Dew with Dew Engineering. The engineering plan has incorporated the future development with the existing facilities and considering adjacent lands. The Preliminary Map prepared by Polaris Land Surveying has identified all necessary easement locations for the public utilities, including the electrical.

Finding:

The City of Ashland can find that the Preliminary Map prepared by Polaris Surveying has identified all public utility easements needed to serve the site and that Dew Engineering has designed the public facilities to meet the needs for the subject property and adjacent lands.

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H. *"Unpaved Streets."*

Discussion:

Not applicable, there are no unpaved streets proposed.

I. *"Where an alley exists adjacent to the partition, access may be required to be provided from the alley and prohibited from the street."*

Discussion:

Not applicable, there are no alleys adjacent to the subject property.

J. *"Required State and Federal permits, as applicable, have been obtained or can reasonably be obtained prior to development."*

Discussion:

There are no wetlands, or other natural hazards warranting a state or federal permit. This criterion is Not applicable.

K. *"A partition plat containing one or more flag lots shall additionally meet the criteria in Section 18.5.3.060."*

Discussion:

Section 18.5.3.060 is applicable with this partition request. Prior to the Property Line Adjustment with Tax Lot 8700 the subject property, Tax Lot 9201, was defined as a flag lot, The approved PLA reconfigured the site to a conforming property zoned C-1. The applicable criterion are:

A. *The criteria of section 18.5.3.050 are met.* Upon review discussed above it can be determined that this partition meets the standards of Section 18.5.3.050.

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B. For the purpose of meeting the minimum lot area requirement, the lot area, exclusive of the flag drive area, must meet the minimum square footage requirements of the zoning district. The C-1 zoning district does not contain any minimum lot area standards within the Ordinance.

C. Flag drives shall be in the same ownership as the flag lots served. Where two or more lots are served by the same flag drive, the flag drive shall be owned by one of the lots and an easement for access shall be granted to the other lot or lots. The flag drive is proposed to serve more than two lots adjacent to the subject property. The applicants are in agreement to create easements to ensure legal access to the adjacent properties.

D. Except as provided in subsection 18.5.3.060.H,below, ... Drives shared by adjacent properties shall have a width of 20 feet, with a 15 foot paved driving surface. The proposed shared access drive will have a 20-foot paved driving surface with a 28-foot easement for fire apparatus movements.

E. Curb cuts have been minimized, where possible, through the use of common driveways. The number of curb cuts and road approaches have already been reduced with the approval of the shadow plan and PLA with Rogue Credit Union. No new curb cuts are proposed with this partition.

F. Flag drive grades shall not exceed a maximum grade of 15 percent. The grade of the drive is approximately 4-5% which does not exceed the 15% threshold.

G. Flag drives shall be constructed to prevent surface drainage from flowing over sidewalks or other public ways. The applicant agrees to design and engineer the drive to prevent storm water from flowing over any sidewalks or public ways.

H. Flag lots adjacent to an alley shall meet all of the requirements of this section... Not applicable there are no alleys abutting the subject site.

I. Flag drives and fire work areas shall be deemed Fire Apparatus Access Roads under the Oregon Fire Code and subject to all requirements thereof. The applicant's Architect has already contact the Fire Department and designed the shared access road to meet the fire code standards.

J. *When required by the Oregon Fire Code, flag drives greater than 150 feet in length shall provide a turnaround...* The applicant's architect has designed the access to meet the Fire Marshall's request for movements and a turnaround, north of the proposed structures.

K. *Each flag lot has at least three parking spaces situated to eliminate the necessity for vehicles backing out.* The properties to be served with the flag drive have their separate parking areas designed to not have any backing out movements onto the shared access drive.

L. *There shall be no parking within ten feet of the centerline of the drive on either side of the flag drive entrance.* The proposed shared access drive does not provide any parking within the easement area, there will be no parking within the shared access drive.

M. *Flag drives serving structures greater than 24 feet in height, as defined in part 18.6, shall provide a fire work area...* The fire work area requirement shall be waived if the structure served by the drive has an approved automatic sprinkler system installed. The proposed structures will contain automatic sprinkler systems as designed by the applicant's architect.

N. *Both sides of the flag drive have been screened with a site-obscuring fence, wall or evergreen hedge to a height of form four to six feet, except in the front yard setback area...* The proposed shared access drive is to commercial uses that do not need buffering from traffic movements, noise and lights. Vision clearance needs to be maintained at the intersections with the parking lots and clear views for security purposes.

O. *The applicant has executed and filed with the Community Development Department an agreement between applicant and the City for paving and screening of the flag drive.* The applicant agrees to an agreement with the City for the timing for completion of the paving as required by the Public Works Director and the maintenance agreement for the paved shared access drive.

P. *Flag lots shall be required to provide a useable yard area that has a minimal dimension of 20 feet wide by 20 feet deep.* There are two yard areas proposed with this partition, one is located in the northwestern corner of Parcel 1 and the other is located along the northern boundary of Parcel 2. Both of these useable yard areas exceed the minimum dimensions required.

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Finding:

The City of Ashland can find that the proposed flag lot and flag drive meets or exceeds the standards of the Ordinance. The flag drive has been designed to meet Fire Code standards with the request from the Ashland Fire Department for providing a Fire apparatus work area with sufficient area for maneuvering for a turnaround. The intended uses for the flag drive are primarily for commercial uses with no parking proposed on the flag drive. This application is consistent with Section 18.5.3.060.

CONCLUSIONS:

The City of Ashland concludes that the proposed land division prepared by Polaris Surveying is in compliance with the partition standards for creating two parcels, one of which is a flag lot, which meets the standards in Section 18.5.3. of the Ordinance.

CONCLUSORY SUMMARY:

Based on the site plan, building elevations and information provided by Kistler Small & White Architects, the landscape plan prepared by Ken Cairn Landscaping, the electrical plan and lighting plan prepared by Marquess & Associates, the preliminary plat prepared by Polaris Surveying and these Findings of Fact, the City of Ashland can conclude that the applicant has addressed the applicable criteria for a multifamily development within the C-1 zoning district and it is demonstrated to be in compliance with the applicable criteria. This application is in compliance with the Basic Site Review and Detailed Site Review Standards including the Partition Standards of the Ashland Land Use Ordinance.

With this evidence and information provided the applicant respectfully request approval of Rogue Ridge as a multifamily development.

Respectfully Submitted,



Richard Stevens & Associates, Inc.
Clark Stevens

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Account Sequence	Map TL Sequence	Assessment Year 2017	Print Window	Close Window
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Assessment Info for Account 1-079288-5 Map 391E10DC Taxlot 9201
 Report For Assessment Purposes Only Created August 10, 2018

Account Info		Tax Year 2017 Info			Land Info	
Account	1-079288-5	Pay Taxes Online			Tax Code	5-01
Map Taxlot	391E10DC 9201	Tax Report			Acreage	1.02
Owner	ROGUE CREDIT UNION			Details	Zoning	
Situs Address		Tax Statement			Land Class	
ASHLAND ST ASHLAND	R			Details	UNK 1.02 Ac	
Mailing Address	ROGUE CREDIT UNION	Cor Tax Statement			Property Class	200
1370 CENTER DR				Details	Stat Class	000
MEDFORD OR, 97504		Tax History			Unit ID	200477-1
Appraiser				Details	Maintenance Area	2
		Tax Code 5-01			Neighborhood	000
		Tax Type	Due Date	Amount	Study Area	74
		Advalorem	11/15/17	\$3,650.81	Account Status	ACTIVE
		Tax Rate	15.9347		Tax Status	Assessable
		District Rates			Sub Type	NORMAL
		Tax Details				
		Tax Rates				

Sales Data (ORCATS)

Last Sale (consideration > 0)	Sale Date	Instrument Number	Sales History
\$ 1,200,000	Mar 24, 2016	2016-9013	Details

Value Summary Detail (For Assessment Year 2017)

Market Value Summary (For Assessment Year 2017)

Code Area	Type	Acreage	RMV	M5	MAV	AV
5-01	LAND	1.02	\$ 679,130	\$ 679,130	\$ 229,110	\$ 229,110
Value History Details		Total:	\$ 679,130	\$ 679,130	\$ 229,110	\$ 229,110

[Value Summary Details](#)

[Value History](#)

Photos and Scanned Documents

SCANNED ASSESSOR DOCUMENTS

(See new portal)

(See new portal)

[Portal](#)

ALL IN ONE REPORT?

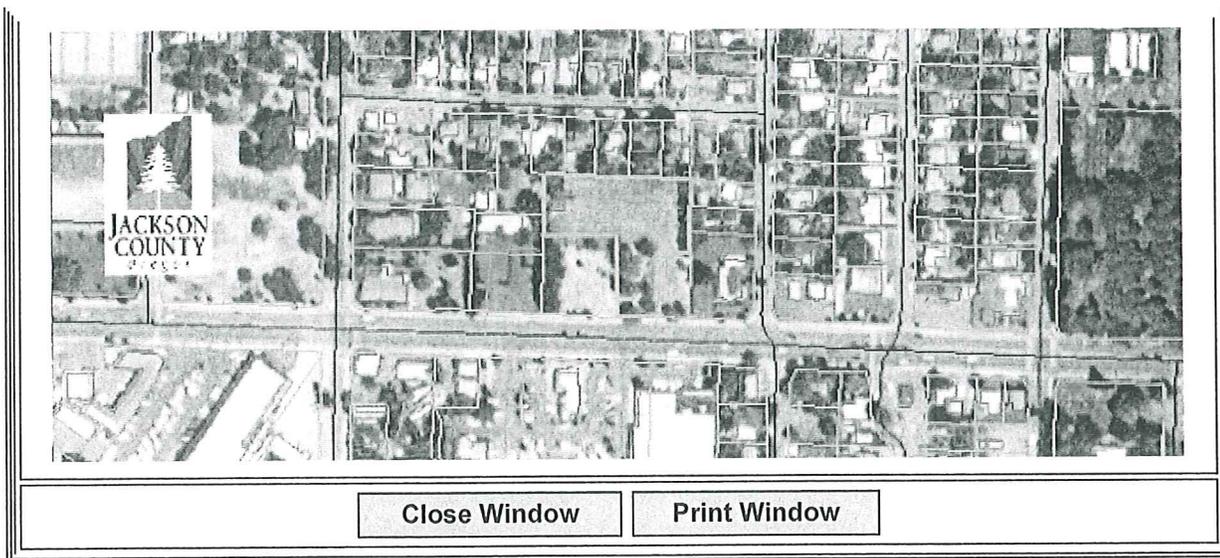
Exemptions / Special Assessments / Notations / Potential Liability

Notations

Description	Tax Amount	Year Added	Value Amount
RECALCULATION		2016	
BOPTA ORDER-REDUCTION 309.120		1991	

Location Map

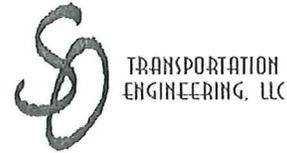
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City of Ashland



319 Eastwood Drive
 Medford, OR 97504
 Ph: 541.941.4148
 Kim.parducci@gmail.com

Memorandum

To: Cindy Dyer, Columbia Care
 Date: 08/17/2018
 Subject: Columbia Care Site Development Changes - Traffic Summary

Southern Oregon Transportation Engineering, LLC evaluated traffic generation impacts from proposed site changes on the subject property located along the north side of Ashland Street (OR 66) between Walker Avenue and Lit Way. The previous site plan included a 4,508 square foot (SF) credit union, 12,229 SF of general office, and 20 apartments. Proposed changes include reducing 8,005 SF of general office and adding 10 apartments within the site. Changes in traffic generations are provided in Tables 1 and 2 below:

Table 1 – Previous Trip Generations								
Land Use	Unit	Size	Daily Rate	Daily Trips	PM Rate	PM Peak Hour		
						Total	In	Out
912 – Drive-in Bank	1000 SF	4.508	148.15	668	24.3	110	55	55
Pass-by 47%						(52)	(26)	(26)
220 - Apartments	Units	20	6.65	133	0.62	12	8	4
710 – General Office	1000 SF	12.229	11.03	135	1.49	18	3	15
Total Trips				936		88	40	48

SF = square feet

Table 2 – Proposed Trip Generations								
Land Use	Unit	Size	Daily Rate	Daily Trips	PM Rate	PM Peak Hour		
						Total	In	Out
912 – Drive-in Bank	1000 SF	4.508	148.15	668	24.3	110	55	55
Pass-by 47%						(52)	(26)	(26)
220 - Apartments	Units	30	6.65 ¹	200	0.62 ¹	19	12	7
710 – General Office	1000 SF	4.224	11.03 ¹	47	1.49 ¹	6	1	5
Total Trips				915		83	42	41

SF = square feet

1. Rates for ITE land uses General Office and Apartments were kept consistent with previous editions for consistency. Newer rates in current editions show lower rates for both uses.

The net result of traffic generations from proposed site changes is a reduction of 21 average daily trips (ADT) and 5 p.m. peak hour trips to the transportation system. We hope this adequately clarifies the traffic result from proposed site changes. Please feel free to contact us with any further questions or concerns.

Southern Oregon Transportation Engineering, LLC

Kimberly Parducci, PE PTOE
 Firm Principal

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ColumbiaCare Rogue Ridge

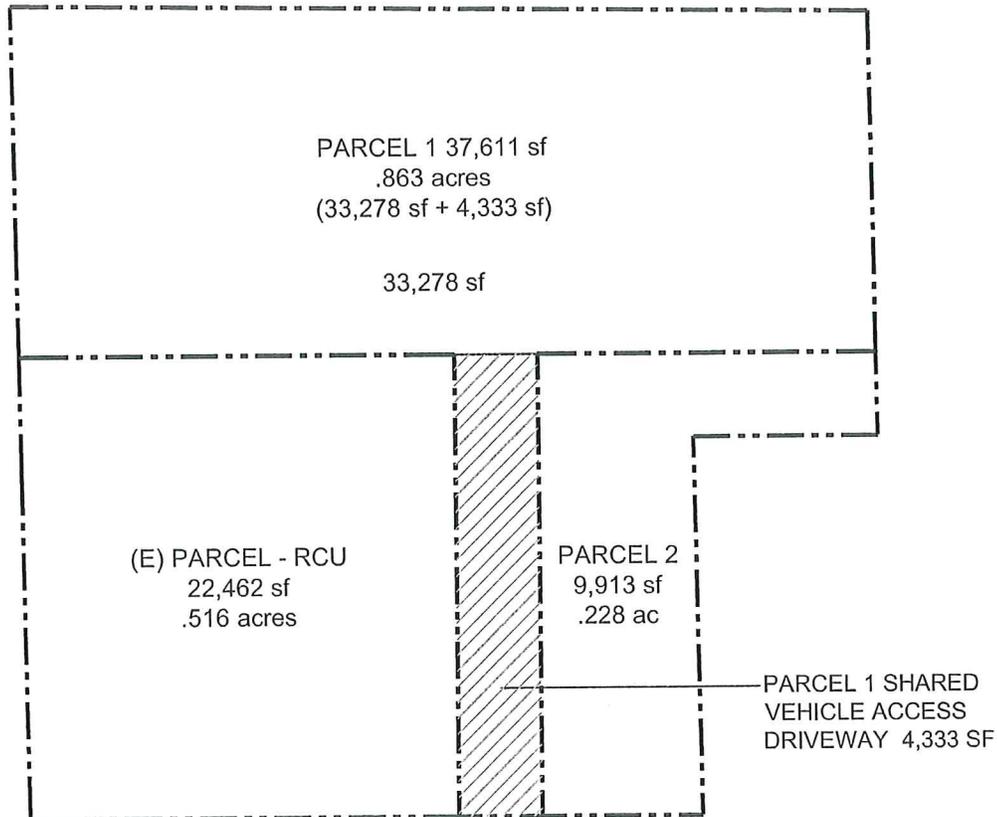
SITE AREAS BY USE

08-27-18

SHADOW PLAN AREA

69,987 sf

1.607 acres



SITE AREA APPORTIONMENT BY USE - SHADOW PLAN

<u>PARCEL/AREA</u>	<u>RESIDENTIAL USE</u>	<u>COMMERCIAL USE</u>	<u>TOTALS</u>
PARCEL 1	33,278 sf *		33,278 sf
PARCEL 1 DRIVE	947 sf *	3,386 sf **	4,333 sf
PARCEL 2		9,913 sf	9,913 sf
PARCEL - RCU		22,462 sf	22,462 sf
TOTAL	33,003 sf	32,651 sf	65,654 sf
GRAND TOTAL	35,183 sf	34,714 sf	69,987 sf
% OF TOTAL	48.9 %	51.5 %	100.0 %

* 200 DAILY VEHICLE TRIPS - 21.8% OF TOTAL TRIPS

** 715 DAILY VEHICLE TRIPS - 78.2% OF TOTAL TRIPS

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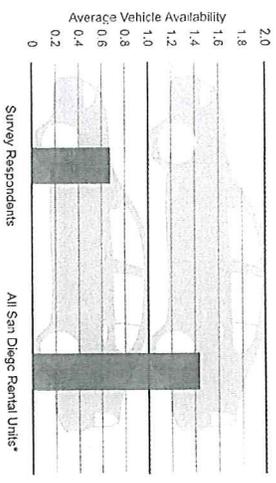
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Results From Affordable Housing Resident Survey

ORAGE HOUSEHOLD VEHICLE AVAILABILITY

On average, residents of affordable housing do not require as much parking as is typically required for rental housing in San Diego, which may justify the use of different parking requirements.

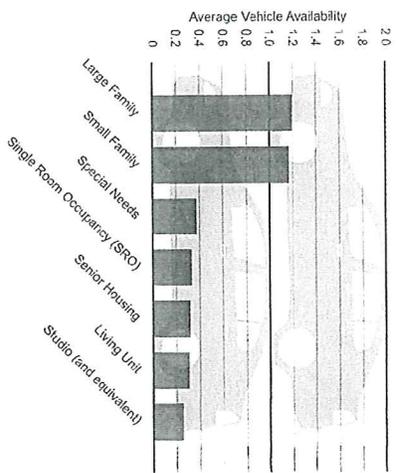
The results of the study show that the average level of household vehicle availability among survey respondents is almost half the average level for all rental housing units in San Diego.*



*Source: 2015-2019 U.S. Census Bureau's American Community Survey

AVERAGE VEHICLE AVAILABILITY BY HOUSING TYPE

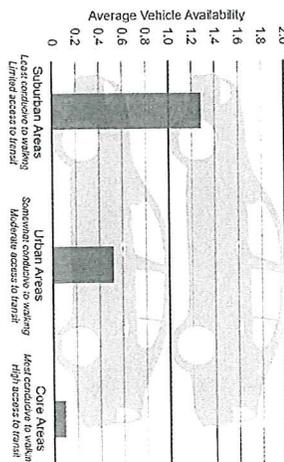
Large family and small family affordable housing have significantly higher average vehicle availability than all other housing types.



AVERAGE VEHICLE AVAILABILITY BY LAND USE AND TRANSPORTATION CONTEXT

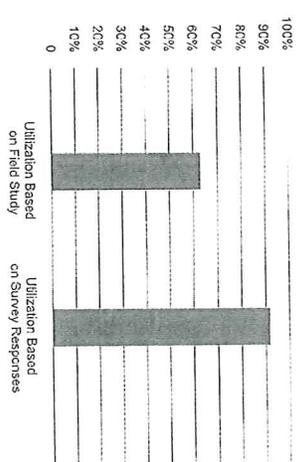
Neighborhood characteristics may influence vehicle ownership levels in affordable housing developments because people may not need cars if they can take transit or walk to destinations. The survey results showed that household vehicle availability is higher in areas that are less conducive to walking and have more limited access to transit.

As defined by a combined measure of the land use and transportation context, suburban areas have the highest mean vehicle availability and core areas have the lowest, with urban areas falling in the middle.



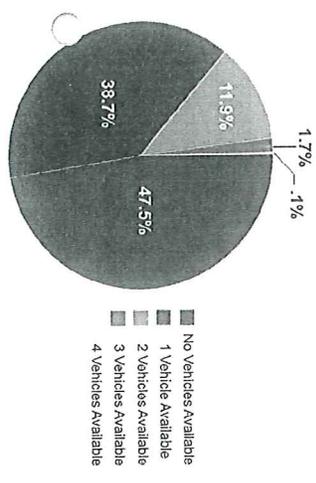
PARKING UTILIZATION

Overall, most of the affordable housing developments surveyed have unused parking. On-site parking utilization data indicated parking was less utilized than the household survey responses indicated. This is likely because data were collected at one point in time and the survey was based on the residents' aggregate experience. Overall, this indicates parking is oversupplied.



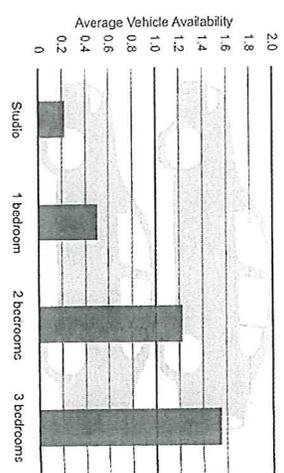
DISTRIBUTION OF RESIDENTS' HOUSEHOLD VEHICLE AVAILABILITY

Almost half the households surveyed had no vehicle and 38.7% had only one vehicle. Only 13.7% of households had more than one vehicle.



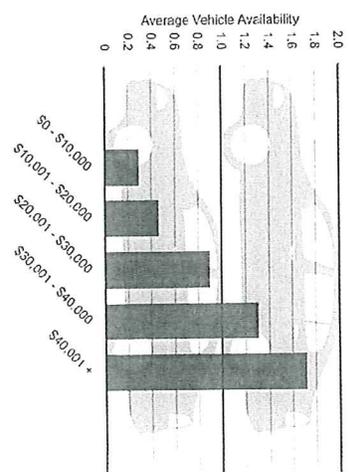
AVERAGE VEHICLE AVAILABILITY BY UNIT SIZE

Larger housing units, measured by number of bedrooms, are likely to have more residents, more drivers, and higher average vehicle availability.



AVERAGE VEHICLE AVAILABILITY BY HOUSEHOLD INCOME RANGE

Vehicle availability is higher in households with greater annual income.



OTHER RESULTS

- Average vehicle availability decreases in affordable housing developments with a higher percentage of residents over the age of 65. However, this is not considered individually significant because a senior housing development is likely to have a lower number of bedrooms AND more residents over 65 years of age.

POLICY CONSIDERATIONS

- The interrelationship of factors affecting parking demand at affordable housing is important when making decision (e.g., housing type, unit size, location, and walkability).
- Priorities should be given to distinct, measurable factors that are typically evaluated in the project development review process (e.g., unit size or location).

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AFFORDABLE HOUSING PARKING STUDY

City of San Diego

Fact Sheet #2: Understanding Parking Demands for Affordable Housing

INTRODUCTION

To understand parking conditions at existing affordable housing developments, the City of San Diego surveyed residents of existing affordable housing developments about the number of vehicles available to each household, vehicle use, travel patterns, number of persons per household, and the demographic characteristics of the residents of each household. In addition, a profile of each housing complex was developed based upon neighborhood characteristics (land use and transit) and characteristics of each housing complex. The on-site and off-site parking conditions were also identified and analyzed. About 2,750 surveys were distributed to 34 affordable housing developments, with a 37% return rate. Of those returned, 875 surveys from 21 sites were analyzed. The results of the analysis provide a foundation for evaluating potential modifications to parking requirements for future affordable housing developments.

KEY CONCERNS

To understand parking demand at affordable housing developments, the study sought to measure the number of cars, trucks, and motorcycles that are owned, leased, rented, or provided by employers for each housing unit. This measure is referred to as "household vehicle availability." The number of vehicles available to each household is important because it is roughly equal to the number of parking spaces that would be required. Additional parking needs for on-site staff and visitors were also analyzed as part of the study. Although household vehicle availability is an important measure of the needed number of parking spaces, other factors such as proximity to transit and neighborhood walkability were found to have an impact on parking demand and should be considered in making decisions about parking requirements. Environmental impacts and costs associated with providing the parking, the surrounding neighborhood, and policy goals are also important.

CITY OF SAN DIEGO BASE PARKING REQUIREMENTS

TYPE OF UNIT	BASE PARKING	TRANSIT AREA OR VERY LOW INCOME	PARKING IM ZONE
Single-Family Residences	2 per dwelling unit	na	na
Detached housing for senior citizens	1 per dwelling unit	na	na
Multi-Family Residences			
Studio up to 400 sf	1.25 per dwelling unit	1.0 per dwelling unit	1.5 per dwelling unit
1 bedroom / studio over 400 sf	1.5 per dwelling unit	1.25 per dwelling unit	1.75 per dwelling unit
2 bedrooms	2.0 per dwelling unit	1.75 per dwelling unit	2.25 per dwelling unit
3-4 bedrooms	2.25 per dwelling unit	2.0 per dwelling unit	2.5 per dwelling unit
5+ bedrooms	1.0 per tenant	0.75 per tenant	1.0 per tenant
Rooming houses	10 per two bedrooms or lodgers	10 per two bedrooms or lodgers	10 per bedrooms or tenants in shared impact
Boarder and lodger accommodations	1 per 3 beds or per permit	1 per 4 beds or per permit	1 per 3 beds or per permit
Residential care facility (6 or fewer persons)	1 per 3 beds or per permit	1 per 4 beds or per permit	1 per 3 beds or per permit
Transitional housing (6 or fewer persons)	2.5 per 1,000 sf	2.5 per 1,000 sf	2.5 per 1,000 sf
Residential accessory uses: retail sales	5 per 1,000 sf	5 per 1,000 sf	5 per 1,000 sf
Residential accessory uses: eating and drinking establishments	5 per 1,000 sf	5 per 1,000 sf	5 per 1,000 sf

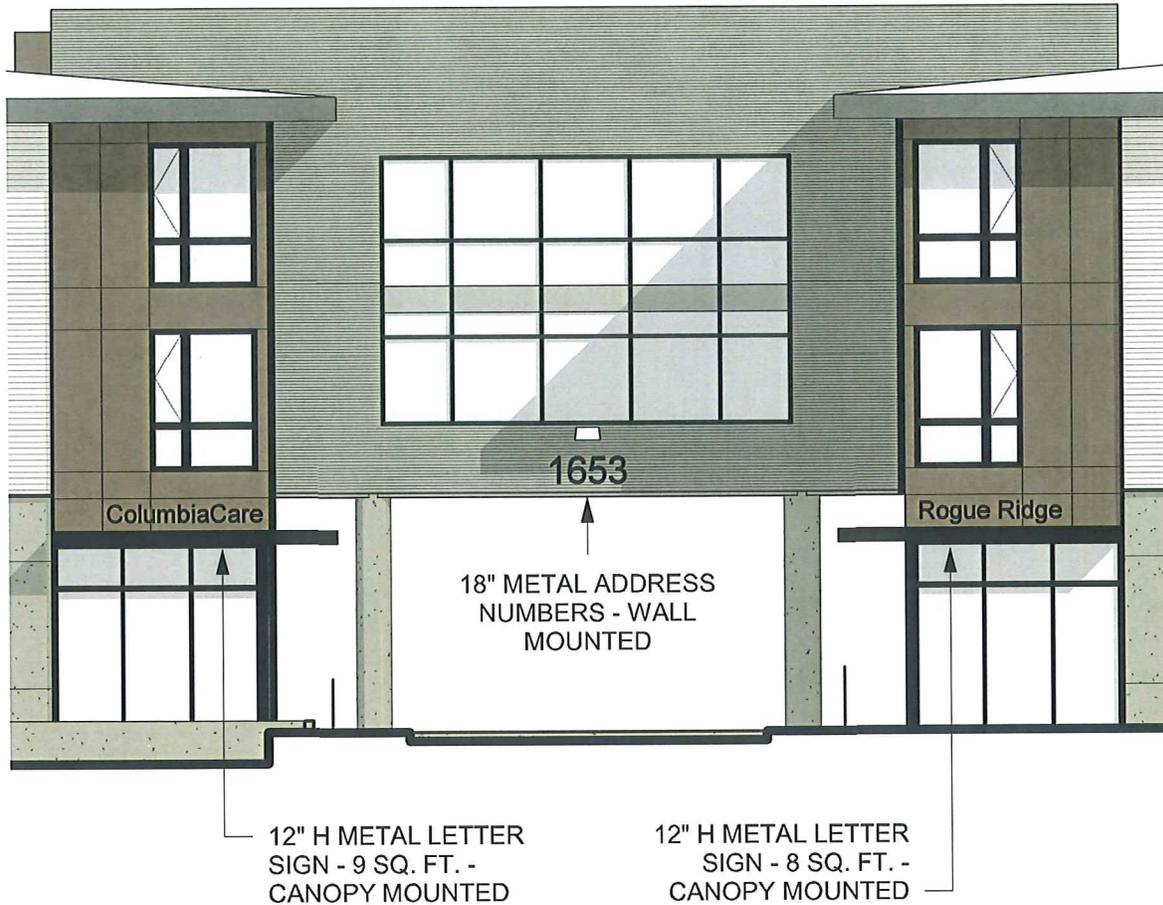
Source: San Diego Municipal Code Chapter 14, Article 2, Division 5

STRATEGIES FOR MEETING PARKING DEMANDS FOR AFFORDABLE HOUSING DEVELOPMENTS

STRATEGY	CITY	DETAILS
Reduced Parking Minimum for Affordable Housing Units	Los Angeles, CA	Up to 50% reduction in parking for affordable housing units
	San Leandro, CA	25% parking reduction for affordable housing units
	Santa Barbara, CA	1 space per dwelling unit for affordable housing parking maximum
	Pasadena, CA	25% parking reduction for affordable housing units
	Boulder, CO	Reduction in parking minimum for affordable housing based on site
Reduced Parking Minimum for Senior Housing	Denver, CO	25% parking reduction for affordable housing units
	Eugene, OR	0.67 spaces per affordable housing habitat a room or 3 spaces total for dwelling unit, whichever is greater based on total available units
	Berkeley, CA	75% parking reduction for senior or disabled living facility
	San Leandro, CA	50% parking reduction for senior or disabled living facility
Reduced Parking Minimum for Affordable Housing in Proximity to Transit	Los Angeles, CA	Reduced parking minimum for parking space per unit for a project located within 1,500 ft of transit and having less than 3 habitable rooms per unit
	Portland, OR	No parking minimums for sites within 500 ft of transit service that has less than 20-minute headways
	San Leandro, CA	Additional parking reductions for affordable housing and/or senior/disabled living dwelling units near transit
	Santa Clara, CA	25% parking reduction for affordable housing units for developments near transit stations containing mixed uses or participating in a TDM plan
	Seattle, WA	20% reduction in parking minimums if development is located within 80 ft of a transit station
Reduced Parking Minimum for Affordable Housing by Specific Location	Seattle, WA	Parking requirement reduced in urban areas
	Pasadena, CA	Alternative parking requirement for all developments that contain affordable housing units located in Parking Benefit Districts
Parking Maximum for Affordable Housing	Seattle, WA	Parking maximum of 1 parking space per 2 affordable single-family dwelling units

AFFORDABLE HOUSING PARKING STUDY

A-H = Affordable housing / * = 11 feet transit station / ** = within TDM plan



STREET FACING SIGNS:

STREET FRONTAGE: 28 FEET
 ALLOWABLE SIGN AREA: 28 SQ. FT.
 PROPOSED NO. OF SIGNS: 2
 AREA OF PROPOSED SIGNS: 17 SQ. FT.

STREET FACING SIGNAGE

3/32" = 1'-0"

1



COLUMBIA CARE - Rogue Ridge
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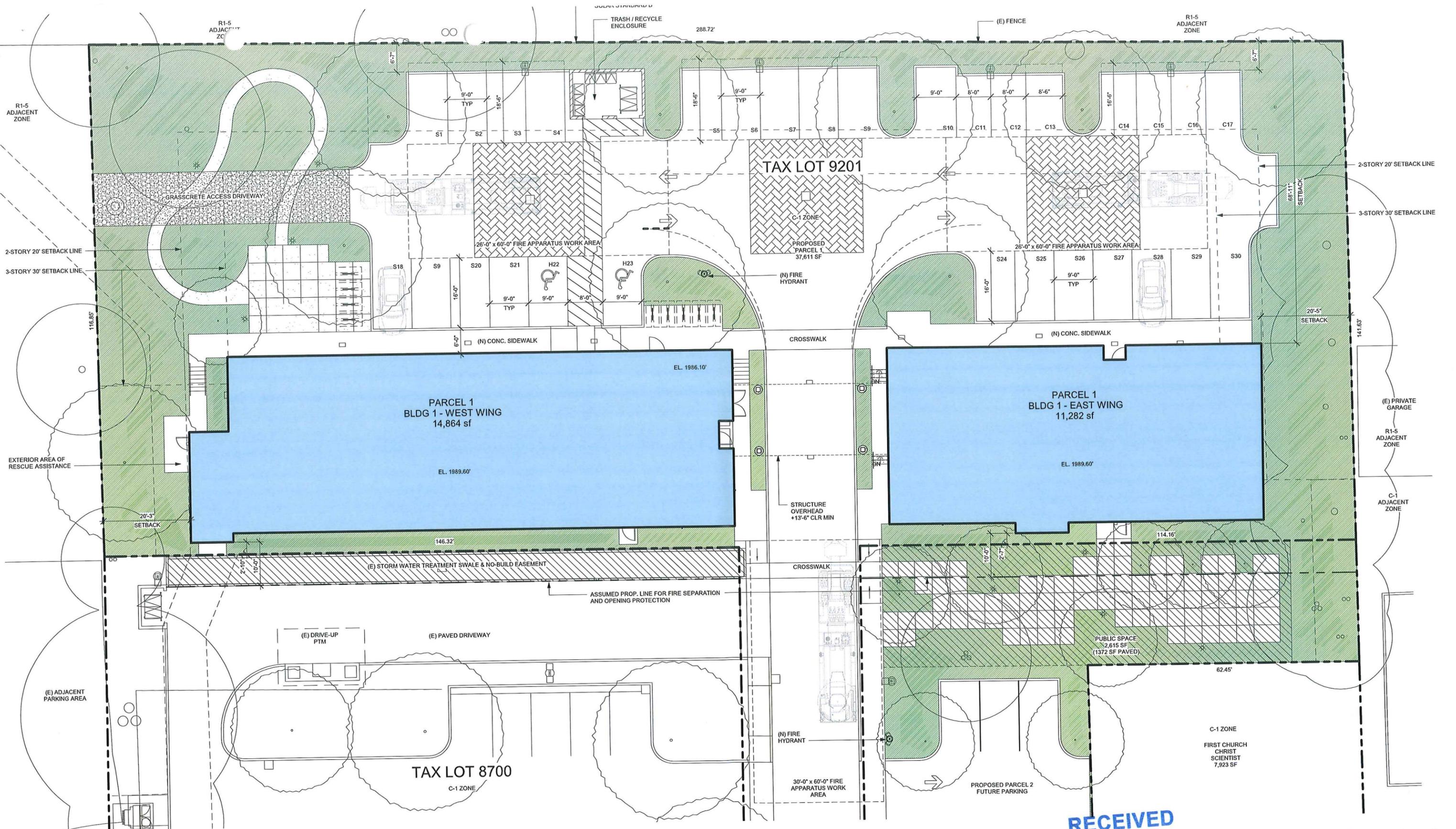
kistler +
small
+ white

COLUMBIA CARE - Rogue Ridge

ASHLAND ST. ASHLAND, OR 97520
SITE REVIEW SUBMITTAL
08.27.2018

COVER

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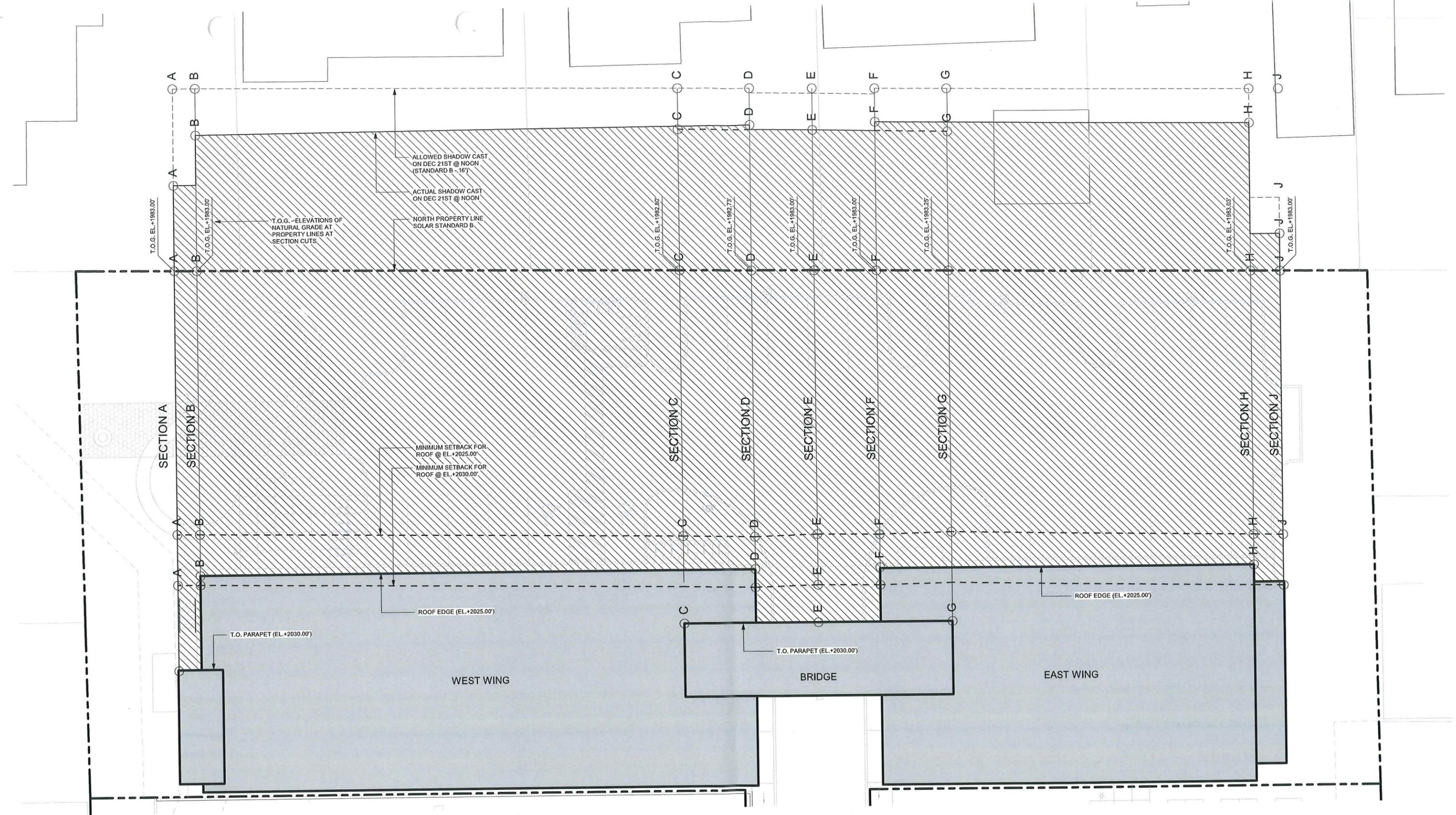
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SITE - ENLARGED PLAN 1
 1" = 10'-0"
 5'-0" 10'-0" 20'-0"

kistler +
 small
 + white
 architects

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 ASHLAND ST. ASHLAND, OR 97520
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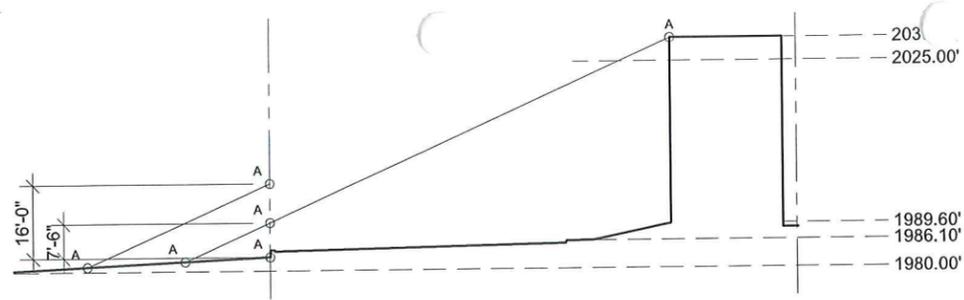
ENLARGED SITE PLAN A3



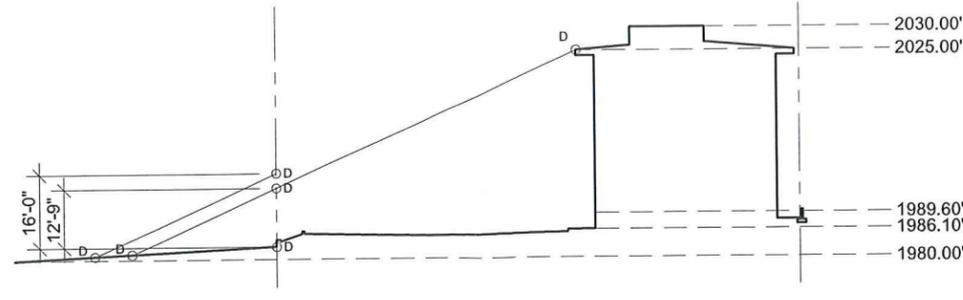
RECEIVED SITE SOLAR SHADOW PLAN 1

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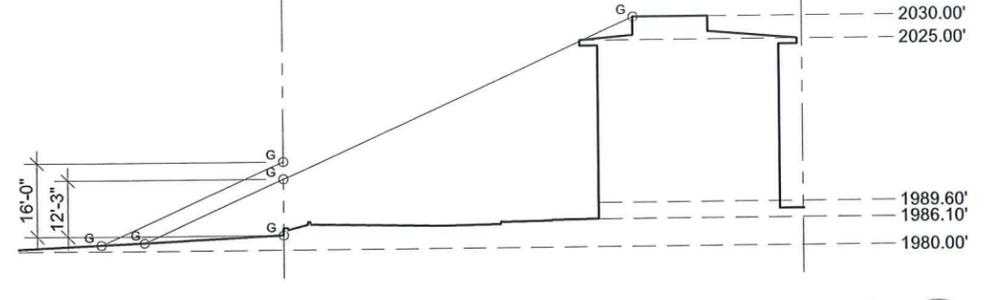




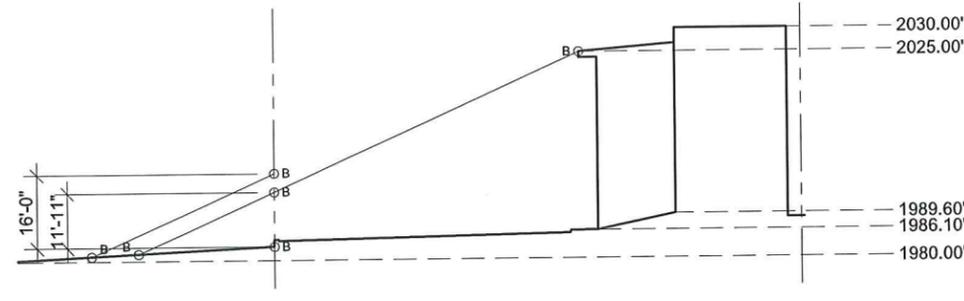
SOLAR SECTION A
1"=20' A



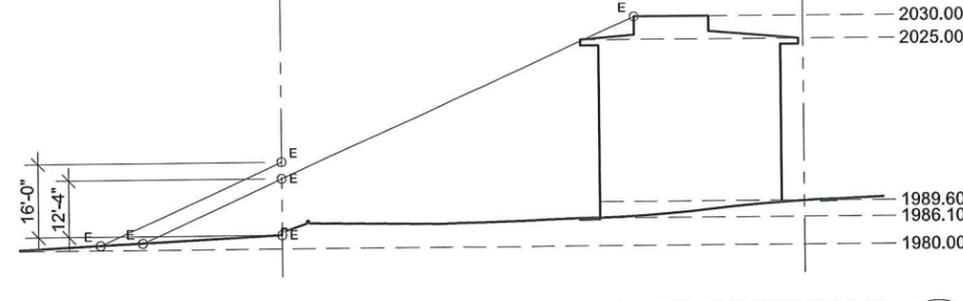
SOLAR SECTION D
1"=20' D



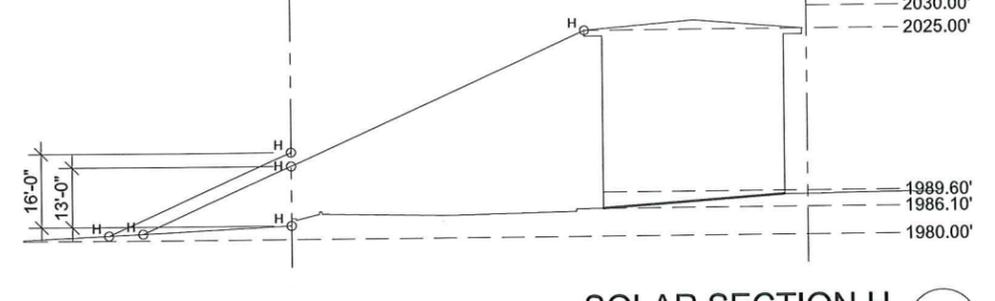
SOLAR SECTION G
1"=20' G



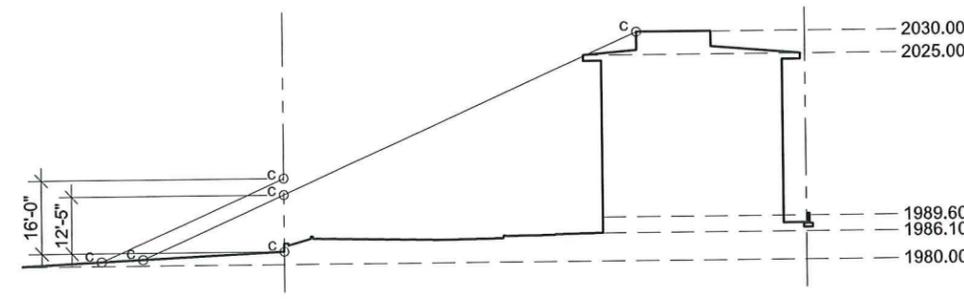
SOLAR SECTION B
1"=20' B



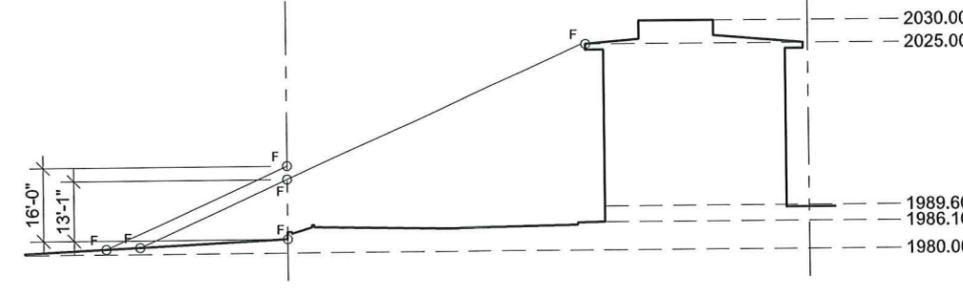
SOLAR SECTION E
1"=20' E



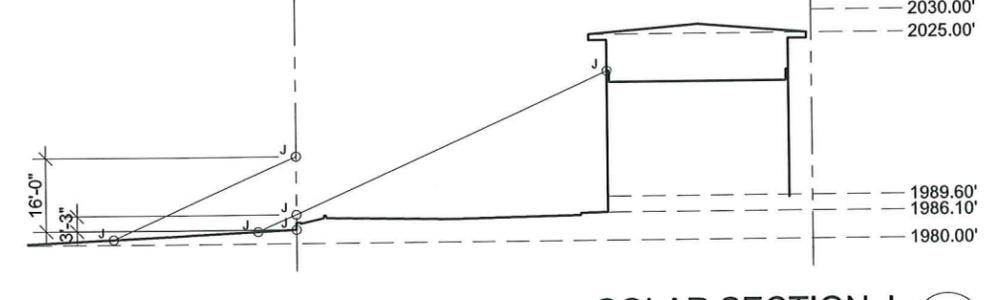
SOLAR SECTION H
1"=20' H



SOLAR SECTION C
1"=20' C



SOLAR SECTION F
1"=20' F



SOLAR SECTION J
1"=20' J



	A	B	C	D	E	F	G	H	J
POINT @ N. PROPERTY LINE									
SOLAR CONTOUR	2025.00 ft								
N. PROP. LINE POINT ELEVATION	1983.00 ft	1983.00 ft	1982.80 ft	1982.73 ft	1983.00 ft	1983.00 ft	1983.25 ft	1983.03 ft	1983.00 ft
H - HEIGHT ABOVE POINT	42.00 ft	42.00 ft	42.20 ft	42.27 ft	42.00 ft	42.00 ft	41.75 ft	41.97 ft	42.00 ft
H-16 ft	26.00 ft	26.00 ft	26.20 ft	26.27 ft	26.00 ft	26.00 ft	25.75 ft	25.97 ft	26.00 ft
SSB - SOLAR SETBACK (H-16/.445)	58.43 ft	58.43 ft	58.88 ft	59.03 ft	58.43 ft	58.43 ft	57.87 ft	58.36 ft	58.43 ft
16 ft SHADCW LINE PROJECTION									
ACTUAL SHADOW LINE PROJECTION									
POINT @ N. PROPERTY LINE									
SOLAR CONTOUR	2030.00 ft								
N. PROP. LINE POINT ELEVATION	1983.00 ft	1983.00 ft	1982.80 ft	1982.73 ft	1983.00 ft	1983.00 ft	1983.25 ft	1983.03 ft	1983.00 ft
H - HEIGHT ABOVE POINT	47.00 ft	47.00 ft	47.20 ft	47.27 ft	47.00 ft	47.00 ft	46.75 ft	46.97 ft	47.00 ft
H-16 ft	31.00 ft	31.00 ft	31.20 ft	31.27 ft	31.00 ft	31.00 ft	30.75 ft	30.97 ft	31.00 ft
SSB - SOLAR SETBACK	69.66 ft	69.66 ft	70.11 ft	70.27 ft	69.66 ft	69.66 ft	69.10 ft	69.60 ft	69.66 ft

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PLAN - LEVEL 1 1



AREA LEGEND

- ACCESSORY
- CIRCULATION
- DWELLING UNIT

UNIT COUNTS - LEVEL 1

2 BED	658 SF
2 BED: 1	
3 BED	1172 SF
3 BED: 1	
STUDIO	478 SF
STUDIO	461 SF
STUDIO	469 SF
STUDIO	469 SF
STUDIO	461 SF
STUDIO	467 SF
STUDIO	467 SF
STUDIO: 7	
Grand total:	9

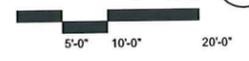
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PLAN - LEVEL 2 1



- AREA LEGEND**
- ACCESSORY
 - CIRCULATION
 - DWELLING UNIT

UNIT COUNTS - LEVEL 2

2 BED	737 SF
2 BED: 1	
3 BED	1260 SF
3 BED: 1	
STUDIO	482 SF
STUDIO	489 SF
STUDIO	490 SF
STUDIO	490 SF
STUDIO	462 SF
STUDIO	478 SF
STUDIO	489 SF
STUDIO: 8	467 SF
STUDIO: 8	
Grand total:	10

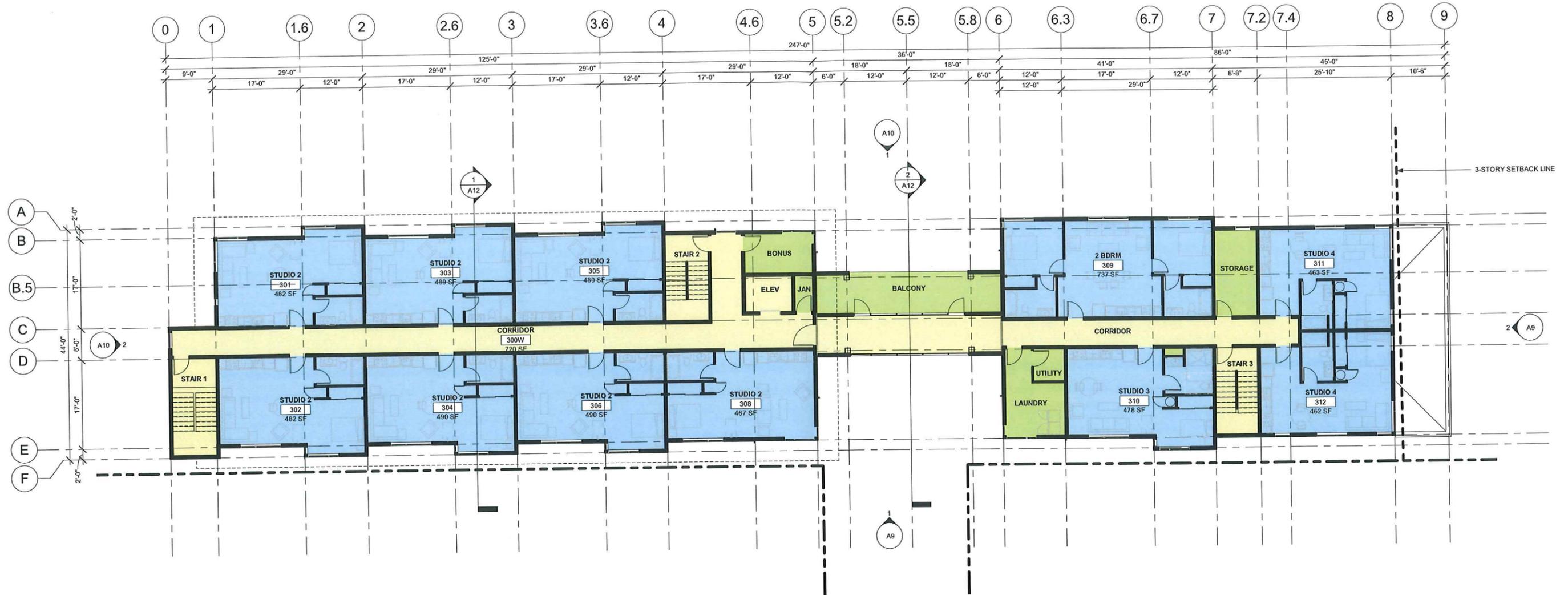


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PLAN - LEVEL 2

A7



PLAN - LEVEL 3 1



AREA LEGEND

- ACCESSORY
- CIRCULATION
- DWELLING UNIT

UNIT COUNTS - LEVEL 3

2 BED	737 SF
2 BED: 1	482 SF
STUDIO	482 SF
STUDIO	490 SF
STUDIO	463 SF
STUDIO	462 SF
STUDIO	478 SF
STUDIO	489 SF
STUDIO	489 SF
STUDIO	490 SF
STUDIO	467 SF
STUDIO	482 SF
STUDIO: 10	
Grand total:	11

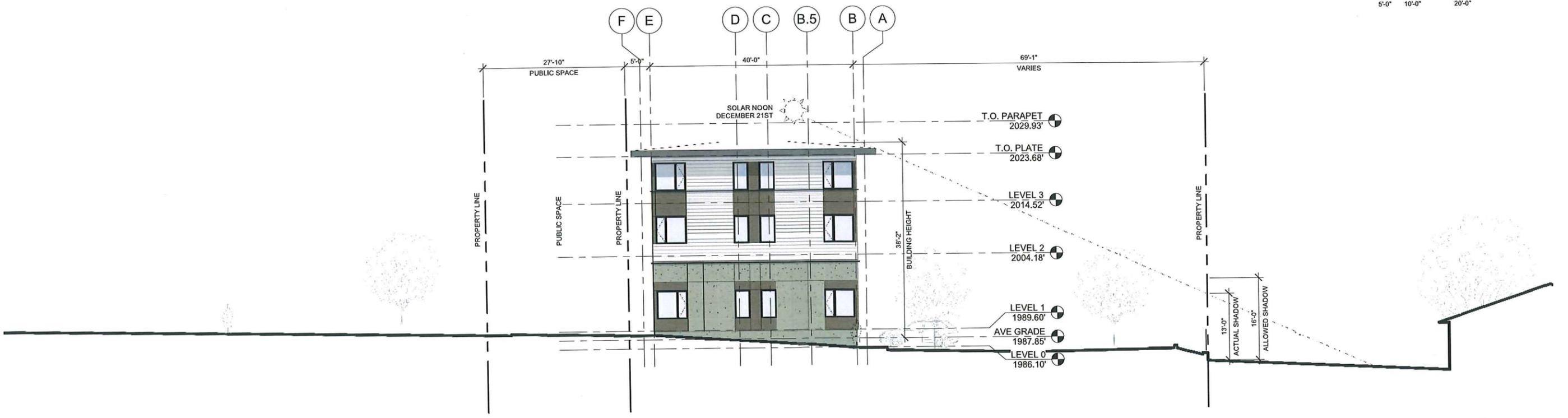
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EXT ELEV - SOUTH 1



EXT ELEV - EAST 2



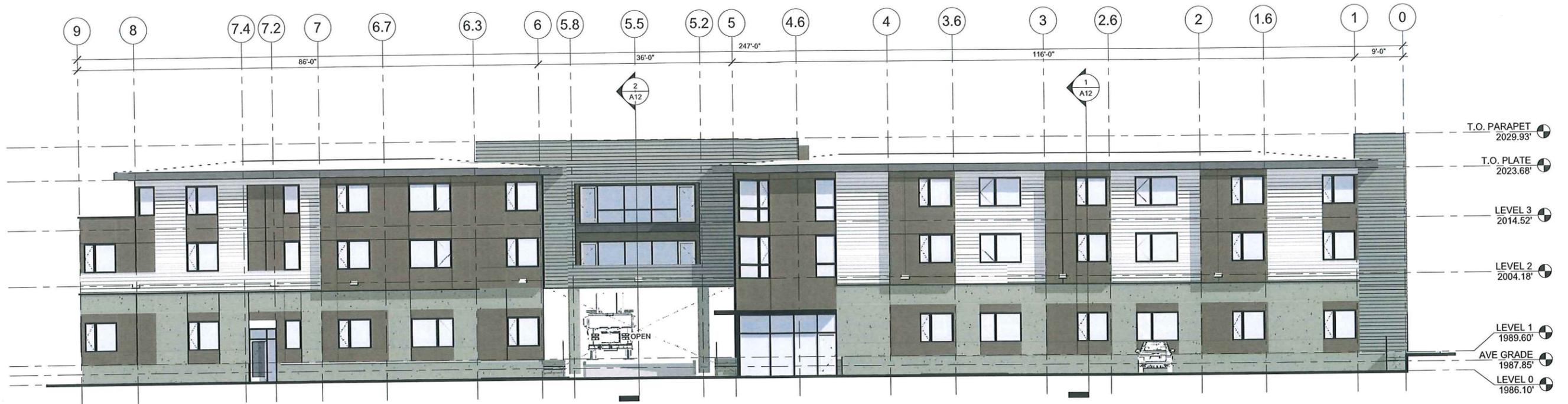
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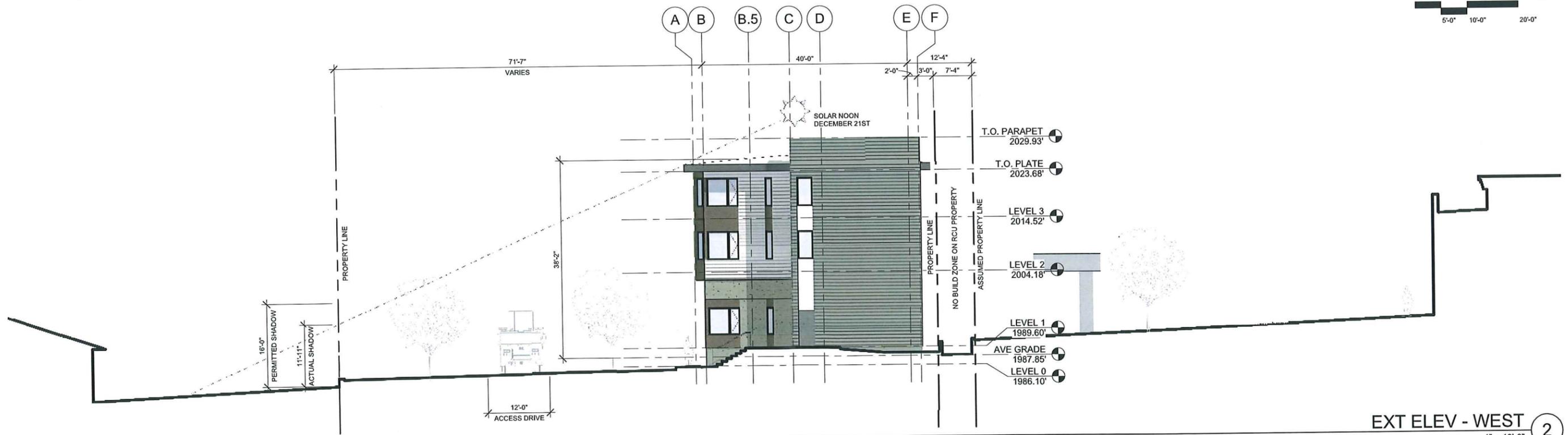
City of Ashland

EXTERIOR ELEVATIONS

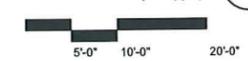
A9



EXT ELEV - NORTH 1
1" = 10'-0"



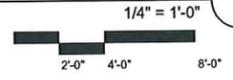
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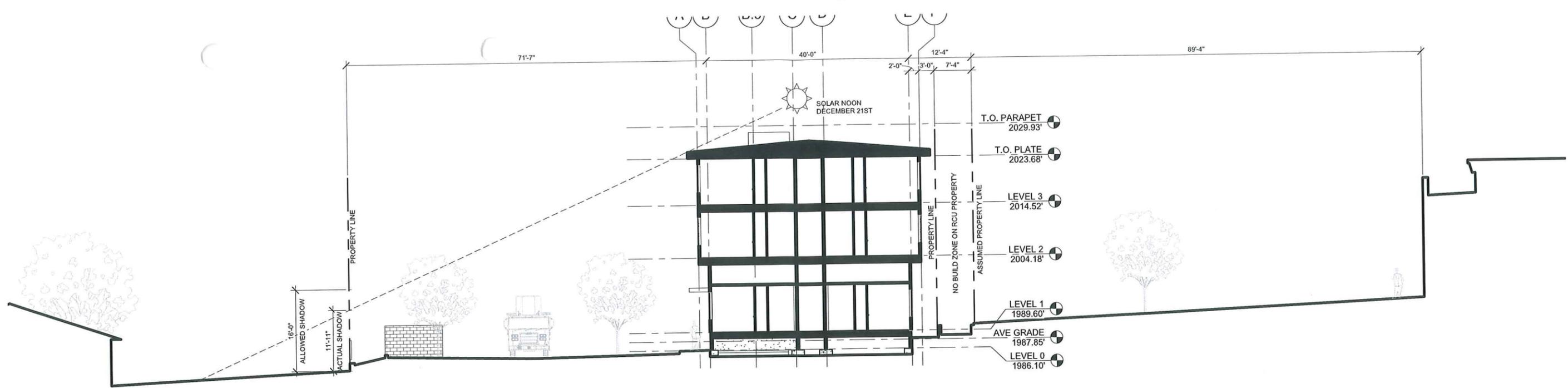
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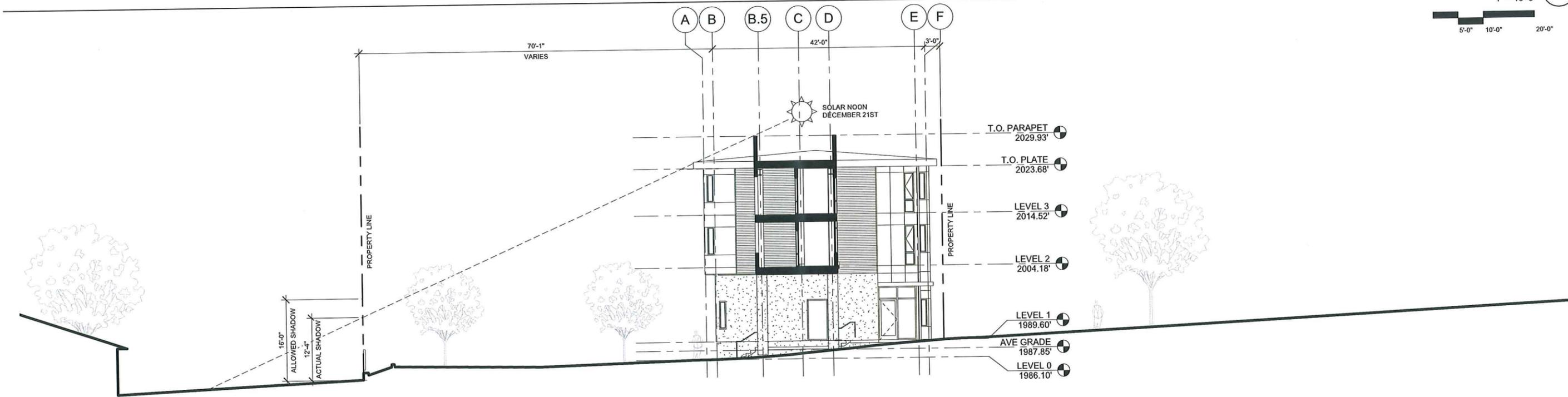
ENLARGED ELEVATION 1



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SITE SECTION - AT RCU 1



SITE SECTION - AT BRIDGE 2





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kistler +
small
+ white
architects

COLUMBIA CARE - Rogue Ridge

ASHLAND ST. ASHLAND, OR 97520
SITE REVIEW SUBMITTAL
08.27.2018

VIEW AT ENTRY

A13



PERSPECTIVE - AERIAL NW 3



PERSPECTIVE - BRIDGE FROM REAR 2



PERSPECTIVE - AERIAL NE 1



PERSPECTIVE - AERIAL SW 6



PERSPECTIVE - ENTRY CLOSEUP 5

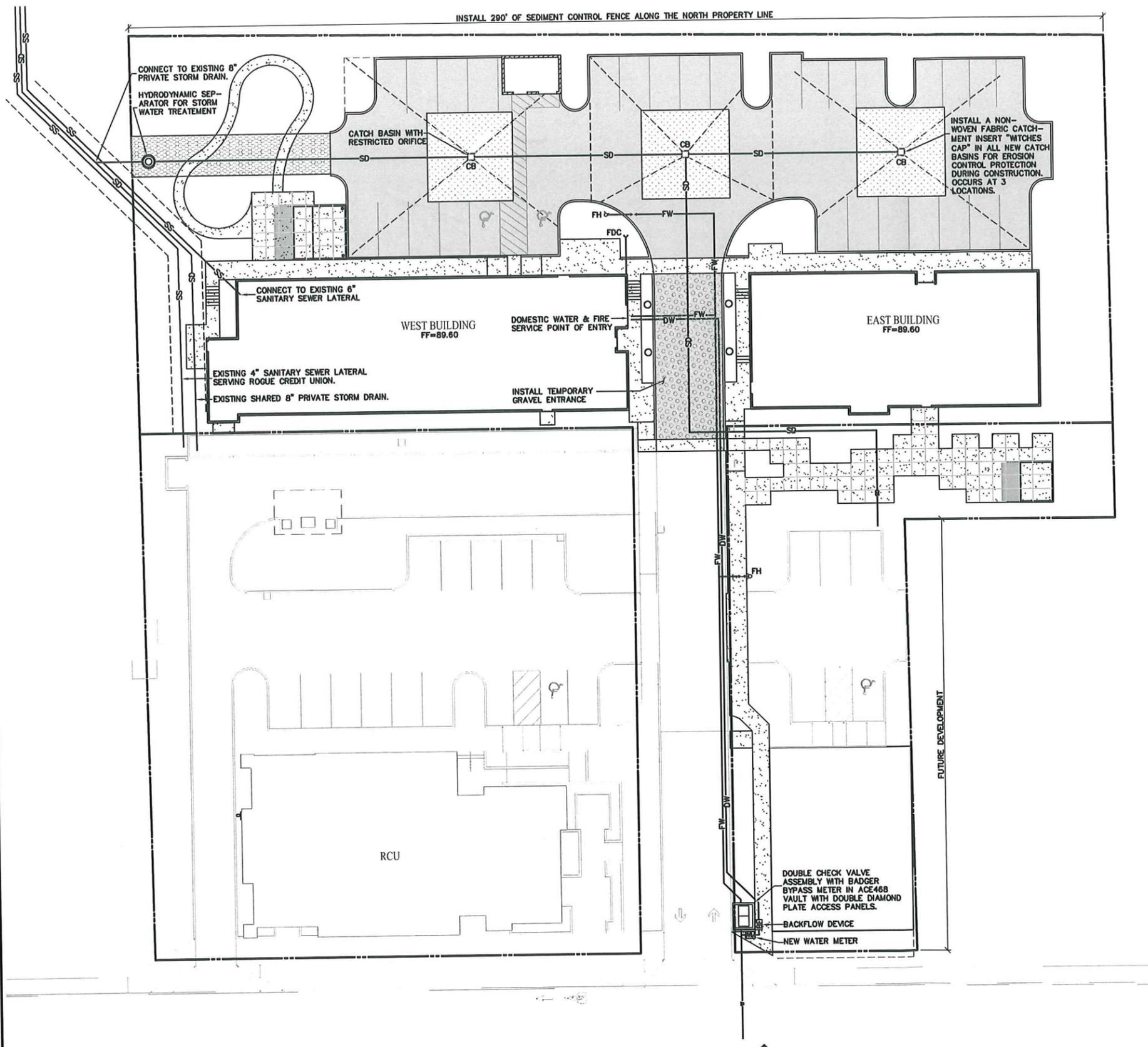


PERSPECTIVE - AERIAL SE 4

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LEGEND

---	PROPERTY LINE
[Pattern]	NEW AC PAVING
[Pattern]	NEW CONCRETE FLATWORK
[Pattern]	NEW POROUS PAVERS
[Pattern]	NEW GRASSCRETE DRIVE
---	NEW CURBS (DARK LINE)
---	EXISTING CURBS (LIGHT LINE)
[Pattern]	GRAVEL ENTRANCE
---	GRADE BREAKS
---	FINISH CONTOURS (1')
---	EXISTING CONTOURS (1')
---	SPOT ELEVATION
---	FINISH SLOPE

ABBREVIATIONS

AC	TOP OF ASPHALT
BF	BACKFLOW
CB	CATCH BASIN
DW	DOMESTIC WATER
FDC	FIRE DEPARTMENT CONNECTION
FF	FINISH FLOOR
FG	FINISH GRADE
FH	FIRE HYDRANT
FW	FIRE WATER
GR	GRATE ELEVATION
INV	INVERT
PL	PROPERTY LINE
SD	STORM DRAIN
SS	SANITARY SEWER
TC	TOP OF CURB
W	WATER

STORM WATER MANAGEMENT

0.55 acres of impervious surface from the proposed development and 0.10 acres from the future development will be detained in shallow surface ponds at the lower parking lot over the 3 porous paver pods then piped to the hydrodynamic separator in the Northwest corner for treatment in conformance with the requirements of the Rogue Valley Storm Water Design Manual. Treated water will then discharge into the shared, private 8" storm drain that outlets to Parker Avenue.

- EROSION CONTROL MEASURES**
1. INSTALL A TEMPORARY 20' x 50' GRAVEL ENTRANCE AT THE END OF THE ACCESS DRIVE AS SHOWN THAT ALL VEHICLES TRAVELING ON EXPOSED SOIL MUST USE.
 2. INSTALL NON-WOVEN FABRIC CATCHMENT INSERT "WITCHES CAP" IN ALL NEW CATCH BASINS.
 3. INSTALL A TEMPORARY SEDIMENT CONTROL FENCE ALONG THE NORTH PROPERTY LINE AS SHOWN.

1 CONCEPTUAL UTILITY & EROSION CONTROL PLAN
SCALE: 1/16"=1'-0"



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REVISIONS

DEW engineering inc.
A CIVIL & STRUCTURAL ENGINEERING FIRM
PHONE 541.772.1399
DEW-ENGINEERING.COM
415 BENNETT AVENUE
MEDFORD, OREGON 97504



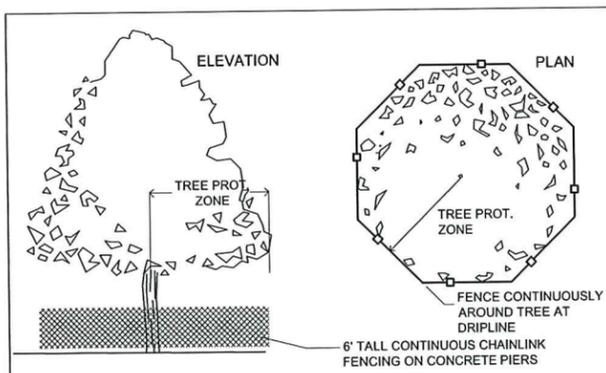
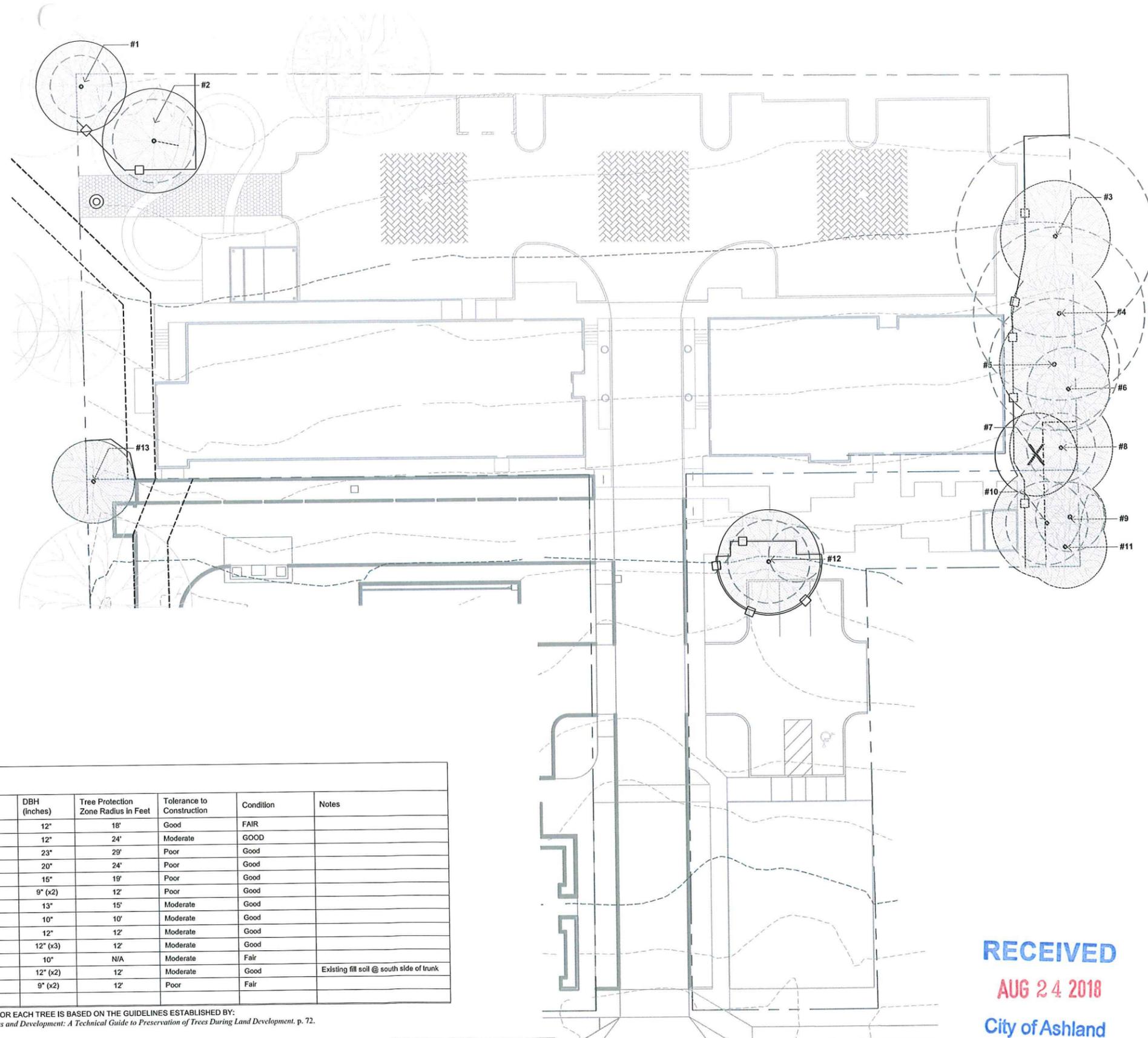
CONCEPTUAL UTILITY & EROSION CONTROL PLAN
(for Site Plan Review)
COLUMBIA CARE - ROGUE RIDGE
1655 ASHLAND STREET
ASHLAND, OREGON

DNW	ORW
MRD	MRD
DATE	8.24.18
PROJECT NO.	18-013
SHEET	C1

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TREE PROTECTION AND REMOVAL NOTES

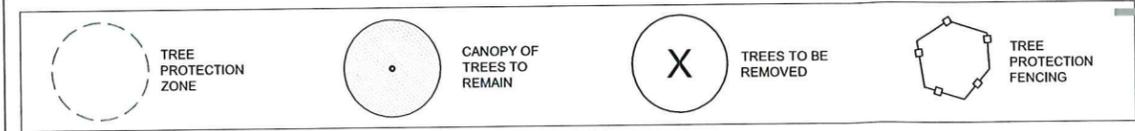
- PRIOR TO DELIVERING EXCAVATION EQUIPMENT OR COMMENCING ANY CONSTRUCTION ACTIVITIES ON THE SITE, THE GENERAL CONTRACTOR SHALL CONTACT THE LANDSCAPE ARCHITECT FOR A PRE-CONSTRUCTION MEETING WITH THE LANDSCAPE ARCHITECT AND EXCAVATION SUPERVISOR PRIOR TO COMMENCING ANY WORK ON THE SITE. THE LANDSCAPE ARCHITECT SHALL BE NOTIFIED BY THE CONTRACTOR 48 HRS. IN ADVANCE FOR ALL SITE VISITS REQUESTED. CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL FROM THE OWNER'S REPRESENTATIVE THAT CONSTRUCTION MAY BEGIN AFTER ALL OF THE DESCRIBED FENCING IS IN PLACE. FENCING SHALL REMAIN IN PLACE UNTIL THE PROJECT IS COMPLETED.
- FENCES MUST BE ERRECTED TO PROTECT TREES TO BE PRESERVED AS SHOWN IN DIAGRAM. FENCING SHALL BE 6' TALL TEMPORARY CHAIN LINK PANELS INSTALLED WITH METAL CONNECTIONS TO ALL PANELS AREA INTEGRATED, THESE FENCES SHALL BE INSTALLED SO THAT IT DOES NOT ALLOW PASSAGE OF PEDESTRIANS AND/ OR VEHICLES THROUGH IT. FENCES DEFINE A SPECIFIC PROTECTION ZONE FOR EACH TREE OR GROUP OF TREES. FENCES ARE TO REMAIN UNTIL ALL SITE WORK HAS BEEN COMPLETED. FENCES MAY NOT BE RELOCATED OR REMOVED WITHOUT THE PERMISSION OF THE LANDSCAPE ARCHITECT.
- CONSTRUCTION TRAILERS, TRAFFIC AND STORAGE AREAS MUST REMAIN OUTSIDE FENCED TREE PROTECTION ZONES AT ALL TIMES.
- ALL PROPOSED UNDERGROUND UTILITIES AND DRAIN OR IRRIGATION LINES SHALL BE ROUTED OUTSIDE THE TREE PROTECTION ZONE. IF LINES MUST TRANSVERSE THE PROTECTION AREA, THEY SHALL BE TUNNELED OR BORED UNDER THE TREE ROOTS. NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY IF ANY PROJECT PLANS CONFLICT WITH THIS REQUIREMENT.
- NO MATERIALS, EQUIPMENT, SPOIL, OR WASTE OR WASHOUT WATER MAY BE DEPOSITED, STORED, OR PARKED WITHIN THE TREE PROTECTION ZONE (FENCED AREA).
- NOTIFY THE LANDSCAPE ARCHITECT IF TREE PRUNING IS REQUIRED CONSTRUCTION CLEARANCE.
- ANY HERBICIDES PLACED UNDER PAVING MATERIALS MUST BE SAFE FOR USE AROUND TREES AND LABELED FOR THAT USE.
- IF INJURY SHOULD OCCUR TO ANY TREE DURING CONSTRUCTION, NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY. ALL DAMAGE CAUSED BY CONSTRUCTION TO EXISTING TREES SHALL BE COMPENSATED FOR BY THE OFFENDING PARTY, BEFORE THE PROJECT WILL BE CONSIDERED COMPLETE.
- WATERING SCHEDULE: WATERING PROTECTED TREES SHALL FOLLOW THESE STANDARDS, HOWEVER PERIODS OF EXTREME HEAT, WIND, RAINFALL OR DROUGHT MAY REQUIRE MORE OR LESS WATER THAN RECOMMENDED IN THESE NOTES.
 - MOST SPECIES: 1 TIME PER MONTH DURING IRRIGATION SEASON (USUALLY MARCH THROUGH SEPTEMBER)
 - QUERCUS/OAK: DEEP WATER IN MAY AND SEPTEMBER, DO NOT WATER DURING OTHER MONTHS. FOR OAKS ALREADY IN THE VICINITY OF IRRIGATED CONDITIONS, AUTOMATIC SPRINKLERS OR REGULAR WATERING SHALL NOT BE ALLOWED TO SPRAY ON OR WITHIN 3 FEET OF THE TRUNK. THE WATER SHALL NOT BE ALLOWED TO POOL OR DRAIN TOWARDS THE TRUNK.
 - WATERING METHOD: HAND WATERING SYSTEMS, RECOMMENDED FOR TREES THAT ARE PART OF A DEVELOPMENT PROJECT THAT MUST BE WATERED TO INSURE TREE SURVIVAL DURING THE COURSE OF CONSTRUCTION UNTIL AUTOMATIC IRRIGATION IS INSTALLED.
- EROSION CONTROL DEVICES SUCH AS SILT FENCING, DEBRIS BASINS, AND WATER DIVERSION STRUCTURES SHALL BE INSTALLED ON THE UPHILL SIDE OF THE TREE PROTECTION ZONE TO PREVENT SILTATION AND/ OR EROSION WITHIN THE TREE PROTECTION ZONE.
- BEFORE GRADING, PAD PREPARATION, OR EXCAVATION FOR THE FOUNDATIONS, FOOTINGS, WALLS, OR TRENCHING, ANY TREES WITHIN THE SPECIFIC CONSTRUCTION ZONE SHALL BE ROOT PRUNED 1 FOOT OUTSIDE THE TREE PROTECTION ZONE BY CUTTING ALL ROOTS CLEANLY AT A 90 DEGREE ANGLE TO A DEPTH OF 24 INCHES. ROOTS SHALL BE CUT BY MANUALLY DIGGING A TRENCH AND CUTTING EXPOSED ROOTS WITH A SAW, VIBRATING KNIFE, ROCK SAW, NARROW TRENCHER WITH SHARP BLADES, OR OTHER APPROVED ROOT-PRUNING EQUIPMENT.
- ANY ROOTS DAMAGED DURING GRADING OR CONSTRUCTION SHALL BE EXPOSED TO SOUND TISSUE AND CUT CLEANLY AT A 90 DEGREE ANGLE TO THE ROOT WITH A SAW. PLACE DAMP SOIL AROUND ALL CUT ROOTS TO A DEPTH EQUALING THE EXISTING FINISH GRADE WITHIN 4 HOURS OF CUTS BEING MADE.
- IF TEMPORARY HAUL OR ACCESS ROADS MUST PASS OVER THE ROOT AREA OF TREES TO BE RETAINED, A ROAD BED OF 6 INCHES OF MULCH OR GRAVEL SHALL BE CREATED TO PROTECT THE SOIL. THE ROAD BED MATERIAL SHALL BE REPLENISHED AS NECESSARY TO MAINTAIN A 6 INCH DEPTH.
- SPOIL FROM TRENCHES, BASEMENTS, OR OTHER EXCAVATIONS SHALL NOT BE PLACED WITHIN THE TREE PROTECTION ZONE, EITHER TEMPORARILY OR PERMANENTLY.
- NO BURN PILES OR DEBRIS PILES SHALL BE PLACED WITHIN THE TREE PROTECTION ZONE. NO ASHES, DEBRIS, OR GARBAGE MAY BE DUMPED OR BURIED WITHIN THE TREE PROTECTION ZONE.
- MAINTAIN FIRE-SAFE AREAS AROUND FENCED AREA. ALSO, NO HEAT SOURCES, FLAMES, IGNITION SOURCES, OR SMOKING IS ALLOWED NEAR MULCH OR TREES.
- DO NOT RAISE THE SOIL LEVEL WITHIN THE DRIP LINES TO ACHIEVE POSITIVE DRAINAGE, EXCEPT TO MATCH GRADES WITH SIDEWALKS AND CURBS, AND IN THOSE AREAS, FEATHER THE ADDED TOPSOIL BACK TO EXISTING GRADE AT APPROXIMATELY 3:1 SLOPE.
- REMOVE THE ROOT WAD FOR EACH TREE THAT IS INDICATED ON THE PLAN AS BEING REMOVED.
- EXCEPTIONS TO THE TREE PROTECTION SPECIFICATIONS MAY ONLY BE GRANTED IN EXTRAORDINARY CIRCUMSTANCES WITH WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO ANY WORK COMMENCING.
- AS A PROTECTIVE MEASURE TO COMPENSATE FOR CONSTRUCTION IMPACTS, TWO TO SIX WEEKS PRIOR TO CONSTRUCTION, ALL RETAINED TREES SHOWN ON THIS PLAN SHALL RECEIVE AN APPLICATION OF MYCOAPPLY ALL PURPOSE SOLUBLE PER MANUFACTURER'S INSTRUCTIONS. THIS MYCORRHIZAE PRODUCT IS A SPECIALLY FORMULATED NATURAL ROOT BIOSTIMULANT WHICH ENHANCES THE ABSORPTIVE SURFACE AREA OF THE TREES' ROOT SYSTEMS, THIS PROMOTES AND IMPROVES NUTRIENT AND WATER UPTAKE CAPABILITIES OF THE REMAINING ROOT STRUCTURE. DISTRIBUTE MYCOAPPLY EVENLY WITHIN THE ACTIVE ROOT ZONE OF RETAINED TREES. APPLY 30 GALS. OF SOLUTION PER TREE 6" DBH AND GREATER, A MINIMUM OF 4" BELOW SOIL SURFACE IN QUANTITIES OF 1/2 GALLON AT EACH POINT OF APPLICATION. LOCATE THE ACTIVE ROOT ZONES WITH LANDSCAPE ARCHITECT PRESENT. MYCOAPPLY IS AVAILABLE FROM MYCORRHIZAL APPLICATION, INC., PHONE (541) 476-3985.



- NOTE:**
- TREE PROTECTION FENCING SHALL BE INSTALLED PRIOR TO START OF CONSTRUCTION AND SHALL REMAIN IN PLACE THROUGH COMPLETION OF PROJECT.
 - ALL EXCAVATION WITHIN DRIFLINE OF TREES SHALL BE DONE BY HAND. IF ROOTS OVER 2" IN DIAMETER ARE ENCOUNTERED, CONTRACTOR SHALL CONSULT WITH LANDSCAPE ARCHITECT OR ARBORIST BEFORE PROCEEDING.
 - TREE ROOTS ENCOUNTERED DURING CONSTRUCTION, SHALL BE CUT CLEANLY AT A 90 DEGREE ANGLE AND PACKED WITH DAMP SOIL IMMEDIATELY.
 - DURING CONSTRUCTION ALL TREES TO REMAIN SHALL BE IRRIGATED ON A WEEKLY BASIS OR AS NECESSARY WITH LEAKY PIPE ENCIRCLING THE TREE FROM TRUNK OUT TO DRIP LINE.

#	Species	DBH (Inches)	Tree Protection Zone Radius in Feet	Tolerance to Construction	Condition	Notes
1	Ulmus pumila	12"	18'	Good	FAIR	
2	Quercus kelloggii	12"	24'	Moderate	GOOD	
3	Acer macrophyllum	23"	29'	Poor	Good	
4	Acer macrophyllum	20"	24'	Poor	Good	
5	Acer macrophyllum	15"	19'	Poor	Good	
6	Acer macrophyllum	9" (x2)	12'	Poor	Good	
7	Prunus dulcis	13"	15'	Moderate	Good	
8	Quercus kelloggii	10"	10'	Moderate	Good	
9	Quercus kelloggii	12"	12'	Moderate	Good	
10	Quercus kelloggii	12" (x3)	12'	Moderate	Good	
11	Prunus dulcis	10"	N/A	Moderate	Fair	
12	Quercus kelloggii	12" (x2)	12'	Moderate	Good	Existing fill soil @ south side of trunk
13	Acer macrophyllum	9" (x2)	12'	Poor	Fair	

THE TREE PROTECTION ZONE FOR EACH TREE IS BASED ON THE GUIDELINES ESTABLISHED BY: Matheny, N. & Clark, J. 1998. *Trees and Development: A Technical Guide to Preservation of Trees During Land Development*, p. 72.



THE TREE PROTECTION ZONE FOR EACH TREE IS BASED ON THE GUIDELINES ESTABLISHED BY: Matheny, N. & Clark, J. 1998. *Trees and Development: A Technical Guide to Preservation of Trees During Land Development*, p. 72.

Drawn By:
KK/JCL

COLUMBIA CARE - ROGUE RIDGE
1655 ASHLAND STREET
ASHLAND, OREGON

JOB NO.
REVISION DATE

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AUG 24 2018
City of Ashland

PRELIMINARY
TREE
PROTECTION
PLAN

ISSUE DATE:
August 24, 2018

L1

SCALE: 1" = 30'-0"
0 30' 60'

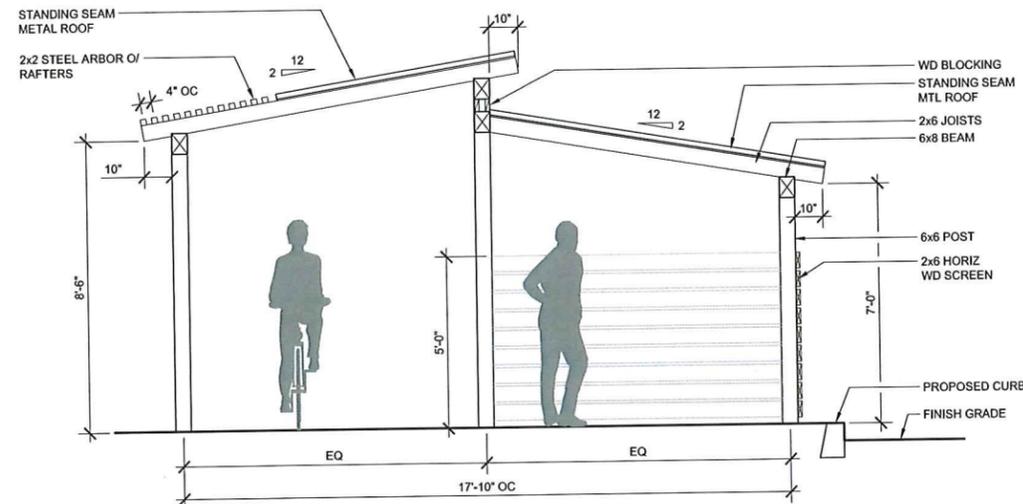
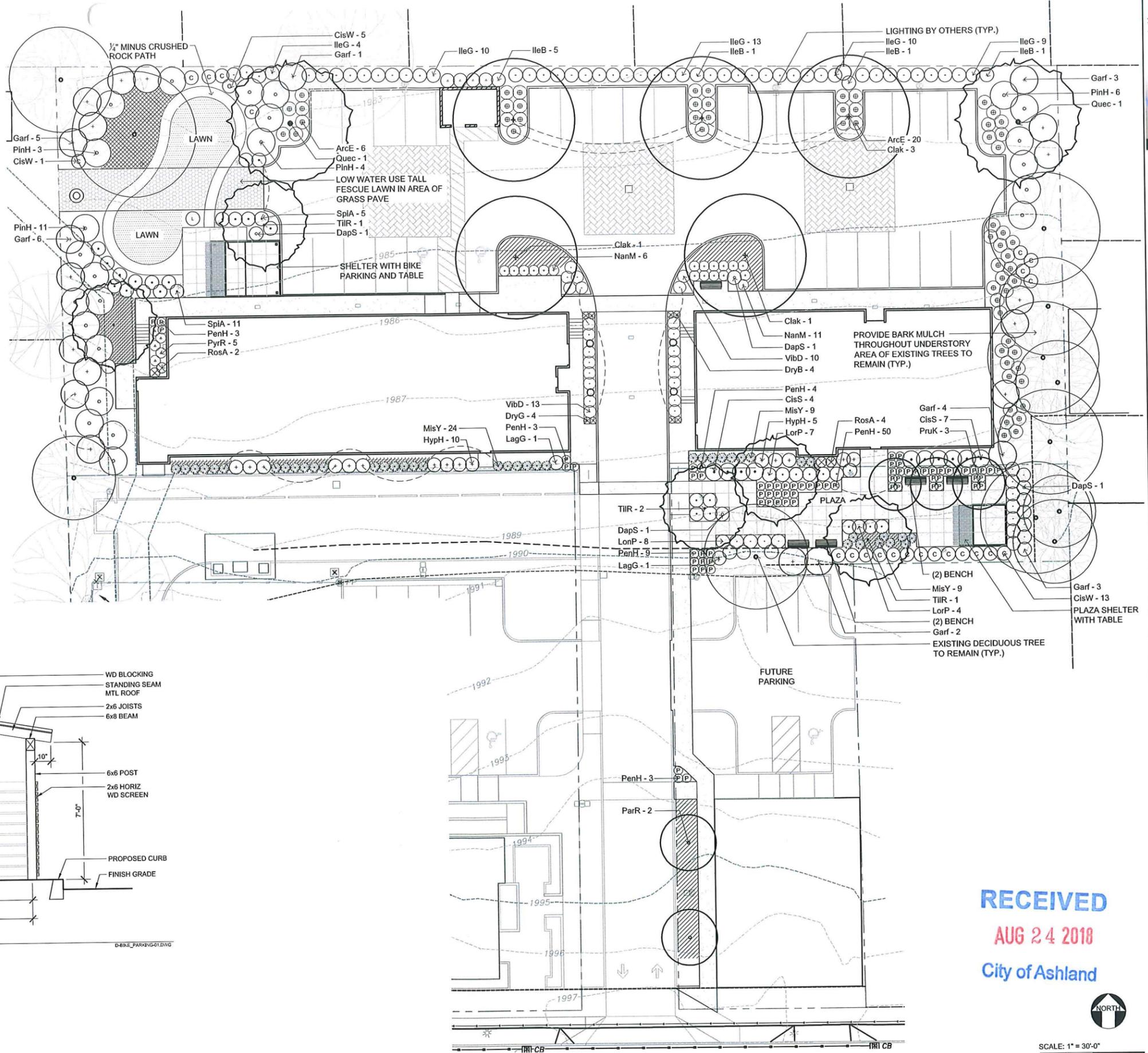
PRELIMINARY PLANT LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
TREES			
Clak	Cladastris kentukea	Kentucky Yellowwood	2" CAL.
ParR	Parrotia persica 'Inge's Ruby Vase'	Ruby Vase Parrotia	2" CAL.
PruK	Prunus serrulata 'Kwanzan'	Kwanzan Cherry	1-1/2" CAL.
Quec	Quercus coccinea	Scarlet Oak	2" CAL.
TIIR	Tilia americana 'Redmond'	Redmond Linden	2" CAL.
SHRUBS			
CisS	Cistus pulverulentus 'Sunset'	Sunset Rockrose	3 GAL.
CisW	Cistus laxus 'Snow White'	Snow White Rockrose	5 GAL.
DapS	Daphne transatlantica 'Summer Ice'	Summer Ice Daphne	5 GAL.
DryB	Dryopteris erythrosora 'Brilliance'	Brilliance Autumn Fern	1 GAL.
Garf	Garrya fremontii	Fremont Silkassel	3 GAL.
HypH	Hypericum x 'Hidcote'	Hidcote St. John's Wort	5 GAL.
IleB	Ilex x meserveae 'Blue Boy'	Blue Boy Holly	3 GAL.
IleG	Ilex x meserveae 'Blue Girl'	Blue Girl Holly	3 GAL.
LagG	Lagerstroemia x 'Gamad'	Cherry Dazzle Crape Myrtle	5 GAL.
LonP	Lonicera pileata	Privet Honeysuckle	5 GAL.
MisY	Miscanthus sinensis 'Yakushima'	Dwarf Maiden Grass	1 GAL.
NanM	Nandina domestica 'Monfar'	Slenna Sunrise Nandina	5 GAL.
PenH	Pennisetum alopecuroides 'Hameli'	Dwarf Fountain Grass	1 GAL.
PinH	Pinus sylvestris 'Hillside Creeper'	Hillside Creeper Pine	3 GAL.
PyrM	Pyracantha x 'Monell'	Red Elf Firethorn	5 GAL.
RosA	Rosmarinus officinalis 'Arp'	Arp Rosemary	1 GAL.
SpiA	Spirea japonica 'Alpina'	Daphne Spirea	5 GAL.
VibD	Viburnum davidii	David Viburnum	5 GAL.
GROUND COVER			
	Cotoneaster dammeri 'Coral Beauty'	Coral Beauty Lowfast	1 GAL.
	Rubus calycinoides	Creeping Bramble	36" O.C.
	Trachelospermum jasminoides	Star Jasmine	1 GAL.
			36" O.C.

NOTE: MATURE COMPOST SHALL BE ADDED TO THE TOPSOIL OF LANDSCAPING AREAS AT A RATE OF THREE CUBIC YARDS OF COMPOST PER 1,000 SQUARE FEET OF LANDSCAPING AREA TO BE PLANTED.

PRELIMINARY LANDSCAPE NOTES

1. ALL PROPOSED LANDSCAPE PLANTING AREAS SHALL RECEIVE CLEAN, SANDY LOAM TOPSOIL TO A MINIMUM DEPTH OF 12" OR AS NOTED ON THE PLAN. ADDITIONALLY, ALL PLANTING AREAS WILL BE PREPARED PER THE PROCEDURES AS OUTLINED IN THE ASHLAND MUNICIPAL CODE, CHAPTER 18.4.4.1.1.
2. ALL PLANTING AREAS SHALL RECEIVE 3" OF UNSETTLED ORGANIC MULCH.
3. THE PROPOSED LAWN AREA SHALL CONSIST OF A LOW WATER USE AND SHADE TOLERANT TALL FESCUE SOD.
4. ALL PLANTING AREAS SHALL BE SERVED BY A DESIGNATED 3/4" (MIN.) DOMESTIC WATER METER AND A CITY OF ASHLAND APPROVED BACKFLOW PREVENTION DEVICE.
5. THE IRRIGATION SYSTEM WILL PROVIDE A FULLY AUTOMATIC IRRIGATION CONTROLLER.
7. THE IRRIGATION SYSTEM WILL PROVIDE IRRIGATION TO 100% OF THE PLANTING AREA AND WILL FOLLOW THE GUIDELINES AS NOTED IN THE ASHLAND MUNICIPAL CODE, CHAPTER 18.4.4.1.2



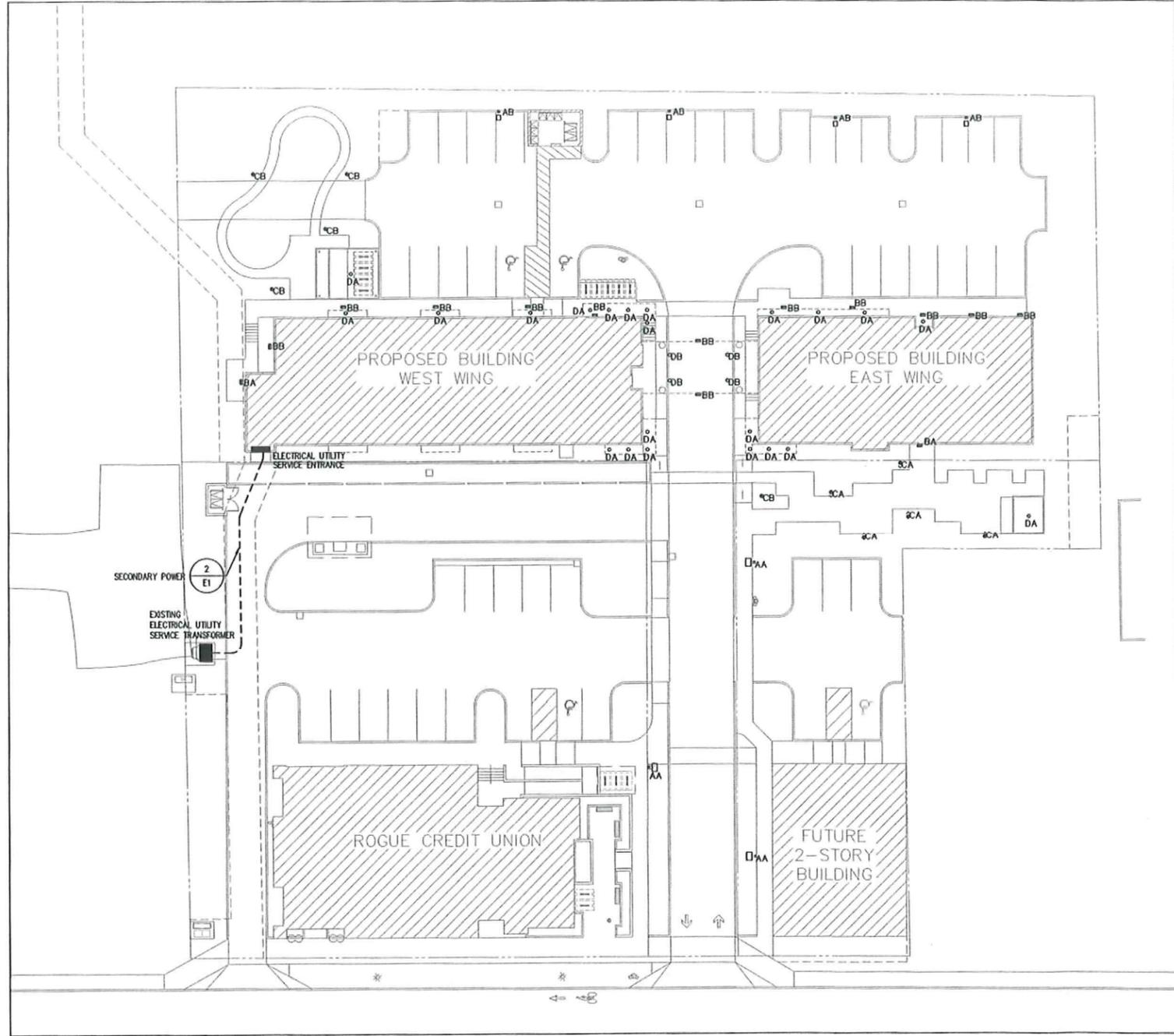
1 SECTION: BIKE PARKING SHELTER
Scale: 3/8" = 1'-0"

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SCALE: 1" = 30'-0"
0 30' 60'

ASHLAND STREET

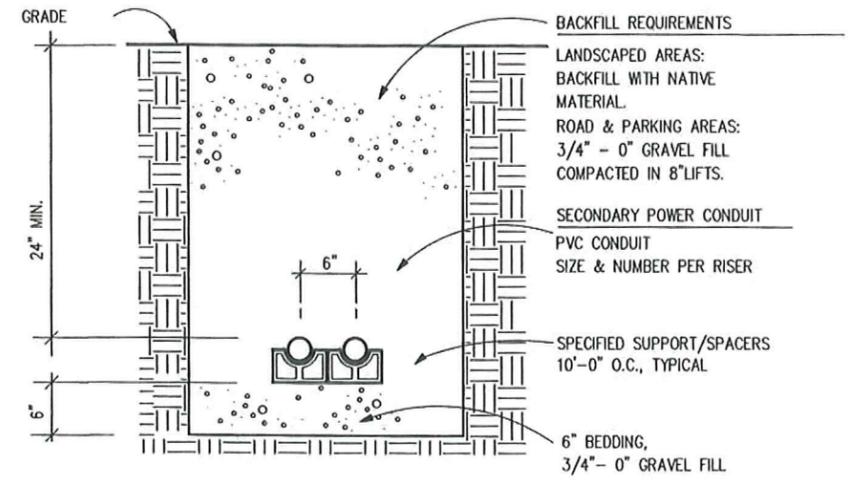


1 ELECTRICAL SITE PLAN
SCALE: 1" = 20'



LUMINAIRE SCHEDULE						
MARK	LUMINAIRE DESCRIPTION, MANUFACTURER & CATALOG #	MOUNTING	POSITION	#	LAMP QUALITIES	NOTES
AA	LED AREA LIGHT LITHONIA KAD LED 40C 700 40K R3 MVOLT SPD 09 PIRHFC2V D0XB0	POLE	1 @ 90°	40	10,244 LUMENS/4,000°K, 94W	1,2,4
AB	LED AREA LIGHT LITHONIA KAD LED 20C 530 40K R3 MVOLT SPD 09 PIRHFC2V HS D0XB0	POLE	1 @ 90°	20	4,123 LUMENS/4,000°K, 45W	2,3,4,5
BA	LED WALL-MOUNT EXTERIOR TERON TTM L12-5-LT350 120/277V BZ 30K PCL120	WALL	+7'-6" AFF	M	1195 LUMENS/3,000°K, 12.5W	6
BB	LED WALL-MOUNT EXTERIOR LITHONIA WST LED P1 30K VF MVOLT PIRH D0B8D	WALL	+18'-0" AFF	M	1500 LUMENS/3,000°K, 12W	7
CA	LED BOLLARD LITHONIA RB08 LED 12C 350 30K ASY MVOLT PE ELCH D0B8D	BASE	+42" AFG	12	541 LUMENS/3,000°K, 18W	6,8
CB	LED BOLLARD LITHONIA RB08 LED 16C 350 30K SYM MVOLT PE D0B8D	BASE	+42" AFG	12	858 LUMENS/3,000°K, 20W	6,8
DA	LED DOWNLIGHT LIGHTOLIER SSR 830K 7	CANOPY	SURFACE	M	650 LUMENS/3,000°K, 9.5W	
DB	LED DOWNLIGHT LIGHTOLIER S7R 830K 10	CANOPY	SURFACE	M	1000 LUMENS/3,000°K, 14.2W	

NOTES:
 1) PROVIDE 2" SQUARE STEEL POLE, DA BRONZE FINISH.
 2) TYPE III DISTRIBUTION
 3) HOUSE-SIDE SHIELD
 4) PROVIDE 90° LEVEL MOTION/AMBIENT SENSOR
 5) PROVIDE 1/2" SQUARE STEEL POLE, DA BRONZE FINISH.
 6) PROVIDE WITH INTEGRAL PHOTOCELL
 7) PROVIDE WITH 180° MOTION/AMBIENT SENSOR
 8) MOUNT ON CONCRETE BASE PER MANUFACTURER'S INSTRUCTIONS.
 9)



2 SECONDARY POWER UNDERGROUND CONDUIT
SCALE: NOT TO SCALE

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COLUMBIA CARE - ROGUE RIDGE
SITE REVIEW SUBMITTAL DRAFT
1651 ASHLAND ST.
ASHLAND
OREGON

ELECTRICAL SITE PLAN	
NO.	REVISIONS
1	ISSUE DATE: 08-24-2018
2	DRAWN BY: PTW
3	CHECKED BY: PTW

SHEET
E1
OF 3 SHEETS

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**ColumbiaCare Rogue Ridge
PLANNING SUMMARY**

	LOT 1 - Parcels 1 & 2 (Shadow Plan)			LOT 2-RCU		TOTALS
	BLDG 1 (Proposed)	BLDG 2 (deleted)	BLDG 3 (Future)	BLDG 1 (Existing)	BLDG 2 (Future)	
PARCEL AREAS						
EXISTING	47,524 sf	0 sf	0 sf	22,462 sf	22,462 sf	69,986 sf
PROPOSED	37,611 sf	9,913 sf	9,913 sf	22,462 sf	0 sf	69,986 sf
CHANGE	-9,913 sf	9,913 sf	9,913 sf	0 sf	0 sf	0 sf
PROJECT STREET FRONTAGE						
PROPERTY LINE FRONTAGE	28 ft	-	54 ft	137 ft	137 ft	219 ft
BUILDING FAÇADE FRONTAGE	0 ft	-	42 ft	96 ft	96 ft	138 ft
% FAÇADE / STREET				70.1%	70.1%	63.0%
PROPOSED BUILDING AREAS						
FIRST FLOOR	8,296 sf	-	2,268 sf	4,508 sf	4,508 sf	15,072 sf
SECOND FLOOR	9,135 sf	-	2,268 sf	0 sf	0 sf	11,403 sf
THIRD FLOOR	8,715 sf	-	-	0 sf	0 sf	8,715 sf
TOTAL FLOOR AREA	26,146 sf	-	4,536 sf	4,508 sf	4,508 sf	35,190 sf
GROUND FLOOR USES						
PERMITTED (P)	0 sf	-	2,268 sf	4,508 sf	4,508 sf	6,776 sf
SPECIAL PERMITTED (SP)	8,296 sf	-	-	-	-	8,296 sf
						15,072 sf
FLOOR AREA RATIO (F.A.R.)						
PROPOSED BUILDING AREAS	26,146 sf	-	4,536 sf	4,508 sf	4,508 sf	35,190 sf
PUBLIC SPACE TOWARD F.A.R.	2,615 sf	-	0 sf	828 sf	828 sf	3,443 sf
TOTAL AREA INCLUDED FOR F.A.R.	28,761 sf	-	4,536 sf	5,336 sf	5,336 sf	38,633 sf
% F.A.R. PROPOSED				70.1%	23.8%	55.2%
MINIMUM REQUIRED (.50)				23,762 sf	11,231 sf	34,993 sf
SURPLUS / (DEFICIT)				9,535 sf	(5895) sf	3,640 sf
PUBLIC SPACE / PLAZA						
AREA PROPOSED IN HARDSCAPE	1,372 sf	-	0 sf	548 sf	548 sf	1,920 sf
AREA PROPOSED IN LANDSCAPE	1,243 sf	-	0 sf	280 sf	280 sf	1,523 sf
TOT. PUBLIC SPACE PROPOSED	2,615 sf	-	0 sf	828 sf	828 sf	3,443 sf
AREA REQUIRED (10% BLDG. AREA)	2,615 sf	-	454 sf	451 sf	451 sf	3,065 sf
SURPLUS / (DEFICIT)	0 sf	-	(454) sf	377 sf	377 sf	377 sf
PARKING						
REQUIRED FIRST FLOOR	BLDG 1	BLDG 2	BLDG 3	TOTALS	BLDG 1	28.2
REQUIRED SECOND FLOOR	10.75 sp	-	4.54 sp	15.29 sp	12.88 sp	16.3
REQUIRED THIRD FLOOR	11.75 sp	-	4.54 sp	16.29 sp	-	11.8
TOTAL REQUIRED PARKING	11.75 sp	-	-	11.75 sp	-	56
TOT. PARKING PROVIDED / PROPOSED	34 sp	9 sp	43 sp	86 sp	13 sp	49.0
SURPLUS / (DEFICIT)	30 sp	5 sp	35 sp	70 sp	14 sp	(7)
	(4.3)	-	(4.1)	(8.3)	1.1	
BICYCLE PARKING						
MULTI-FAMILY RESIDENTIAL REQUIRED	33.5 sp	-	-	34 sp	3 sp	34 sp
NON-RESIDENTIAL REQUIRED	0.0 sp	-	2.0 sp	2 sp	3 sp	5 sp
TOTAL REQUIRED	34 sp	-	2.0 sp	36 sp	3 sp	38 sp
TOT. PROVIDED - SHELTERED	34 sp	2 sp	36 sp	70 sp	6 sp	42 sp
TOT. PROVIDED - NON-SHELTERED	10 sp	0 sp	10 sp	20 sp	0 sp	10 sp
DWELLING DENSITY (Residential)						
LOT ACREAGE	30 DU/ac	.23 ac	1.09 ac	1.52 ac	0.52 ac	1.61 ac
DWELLING UNITS PERMITTED	26	7 DU	33 DU	66 DU	15 DU	48 DU
DU'S PROPOSED	24 DU	0 DU	24 DU	48 DU	0 DU	24 DU
LANDSCAPE COVERAGE						
PROPOSED (PERVIOUS)	12,600 sf	3,911 sf	3,911 sf	19,422 sf	3,911 sf	16,511 sf
REQUIRED MINIMUM	7,129 sf	3,369 sf	3,369 sf	13,867 sf	3,369 sf	10,498 sf
SURPLUS / (DEFICIT)	5,471 sf	542 sf	542 sf	15,555 sf	542 sf	6,013 sf
% LANDSCAPE OF LOT AREA	26.5%	17.4%	17.4%	23.6%	17.4%	23.6%
BUILDING / HARDSCAPE COVERAGE						
TOTAL IMPERVIOUS	34,924 sf	18,551 sf	18,551 sf	71,926 sf	18,551 sf	53,475 sf
% IMPERVIOUS OF LOT AREA	73.5%	82.6%	82.6%	73.5%	82.6%	76.4%

RECEIVED

AUG 24 2018

City of Ashland

DESCRIPTION OF PROPERTY

Street Address Intersection of Scenic and Grandview near 195 Grandview

Assessor's Map No. 39 1E Tax Lot(s) _____

Zoning _____ Comp Plan Designation _____

PROPERTY OWNER

Name City of Ashland Phone 541488-5587 E-Mail _____

Address _____ City _____ Zip _____

Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

PROFESSIONAL PERFORMING THE TREE REMOVAL (e.g., tree service)

Name City of Ashland Street Div. Phone 541-488-5313 E-Mail avram.biondo@ashland.or.us

Address _____ City _____ Zip _____

ARBORIST, LANDSCAPE ARCHITECT, OTHER

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner. I hereby certify that the statements and information contained in this application are in all respects, true and correct. I further understand that if this request is subsequently contested, the burden will be on me to establish:

- 1) *that I produced sufficient factual evidence to support this request;*
- 2) *that the information contained in this application are adequate; and further*
- 3) *that all trees, structures, or improvements are properly located on the ground.*

Property Owner's Signature (required)

Date

STAFF DECISION:

Permit is hereby (circle one):	Approved	Approved with Conditions	Denied
Conditions of Approval _____			

Is the tree 18" d.b.h or greater? <input type="checkbox"/> NO <input type="checkbox"/> YES		Has the City Administrator has been notified: <input type="checkbox"/> NO <input type="checkbox"/> YES	
_____		_____	
Community Development Director/Planning Manager Signature		Date	

RECEIVED
SEP 12 2018
 City of Ashland

RECEIVED

SEP 12 2018

City of Ashland





205

ALMOND ST

185

165

159

Tree damaging curb

176

160

178

180

182

170

188

LOGAN DR

165

189

179

199

RECEIVED

SEP 12 2018

City of Ashland



Planning Division
 51 Winburn Way, Ashland OR 97520
 541-488-5305 Fax 541-488-6006

STREET TREE REMOVAL PERMIT

A tree that is located in any public street right-of-way or other public property may not be removed until a Street Tree Removal Permit has been submitted according to the Application Submission Requirements, below, and reviewed and approved by the City of Ashland.

An application for street tree removal must demonstrate that the tree is an emergency, hazard, or dead tree as outlined below in the Application Submission Requirements.

Application Submission Requirements. An application for a street tree removal permit shall include all of the following information.

1. **Application Form and Fee.** The application must include the information requested on the Street Tree Removal Permit form provided by the City of Ashland and the permit application fee. Only those property owners of a lot adjoining the street tree location or homeowners' associations responsible for street trees in their development or subdivision may apply to remove an adjoining street tree. If a tree is located in front of more than one property, each property owner or homeowners' association official must sign the Street Tree Removal Permit form.
2. **Site Plan.** A site plan of the property drawn to scale containing the following information. The scale of the site plan must be at least one inch equals 50 feet or larger.
 - a. North arrow and scale.
 - b. Property boundaries including dimensions of all lot lines and driveway locations.
 - c. Location and width of all public streets, planting strips, and sidewalks adjoining the site.
 - d. Size, species, and location of the tree(s) proposed to be removed.
3. **Written Statement.** A written statement explaining how the proposed street tree removal satisfies one of the following approval criteria. The Community Development director may require additional information to demonstrate that the proposed removal satisfies one of the following approval criteria including: 1) a written statement to be prepared by an arborist licensed by the State of Oregon Landscape Contractors Board of Construction Contractors Board and certified by the International Society of Arboriculture or American Society of Consulting Arborists; and 2) an International Society of Arboriculture (ISA) Basic Tree Risk Assessment Form to be completed by an arborist.

Street Tree Removal Approval Criteria

- a) **Emergency Tree Removal.** The tree presents an immediate danger of collapse and represents a clear and present hazard to persons or property. Immediate danger of collapse is defined as a tree that may already be leaning, with the surrounding soil heaving, and/or there is a significant likelihood that the tree will topple or otherwise fail and cause damage before a tree removal permit could be obtained through the non-emergency process.
- b) **Hazard Tree Removal.** The tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. A hazard tree is a tree that is physically damaged to the degree that it is clear the tree is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within a public right-of-way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated.
- c) **Dead Tree.** The tree is dead. A dead tree is lifeless. Such evidence of lifelessness may include unseasonable lack of foliage, brittle dry branches, or lack of any growth during the growing season.

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2. All street trees shall be an appropriate species selected from and planted according to the City of Ashland Recommended Street Tree List.
3. The minimum size for a replacement tree is eight feet in height or one inch in caliper measured at 12 inches above the root crown.
4. Applicants for a Street Tree Removal Permit may be required to replace the tree or trees being removed with a tree or trees of comparable value.
5. If a street tree is determined to be dead or dying, then the replacement need be no larger than the minimize size described above.

Type of Tree(s) Pine Tree RECEIVED

Approximate Diameter at breast height 30" Height 70' Canopy _____ SEP 12 2018

Location of Tree Intersection of Oarchard and Westwood streets City of Ashland

Reason for Request Dead Tree

Are there underground utility lines and/or overhead power lines present? Yes

If yes, please list which lines are present Guy lines

Is there sidewalk damage? N/A If yes, has a Public Works permit been issued? N/A

OVER >>

DESCRIPTION OF PROPERTY

Street Address Between 460 and 450 Orchard ST

Assessor's Map No. 39 1E _____ Tax Lot(s) _____

Zoning _____ Comp Plan Designation _____

PROPERTY OWNER

Name City of Ashland Phone 541-488-5587 E-Mail _____

Address _____ City _____ Zip _____

Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

PROFESSIONAL PERFORMING THE TREE REMOVAL (e.g., tree service)

Name City of Ashland Electric Dept. Phone 541-488-5357 E-Mail _____

Address _____ City _____ Zip _____

ARBORIST, LANDSCAPE ARCHITECT, OTHER

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner. I hereby certify that the statements and information contained in this application are in all respects, true and correct. I further understand that if this request is subsequently contested, the burden will be on me to establish:

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- 3) *that all trees, structures, or improvements are properly located on the ground.*

Property Owner's Signature (required) Date

STAFF DECISION:

Permit is hereby (circle one):		
Approved	Approved with Conditions	Denied
Conditions of Approval _____		

Is the tree 18" d.b.h or greater? <input type="checkbox"/> NO <input type="checkbox"/> YES		
Has the City Administrator has been notified: <input type="checkbox"/> NO <input type="checkbox"/> YES		
_____ Community Development Director/Planning Manager Signature		_____ Date

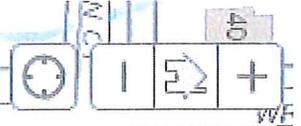
RECEIVED
SEP 12 2018
City of Ashland

RECEIVED

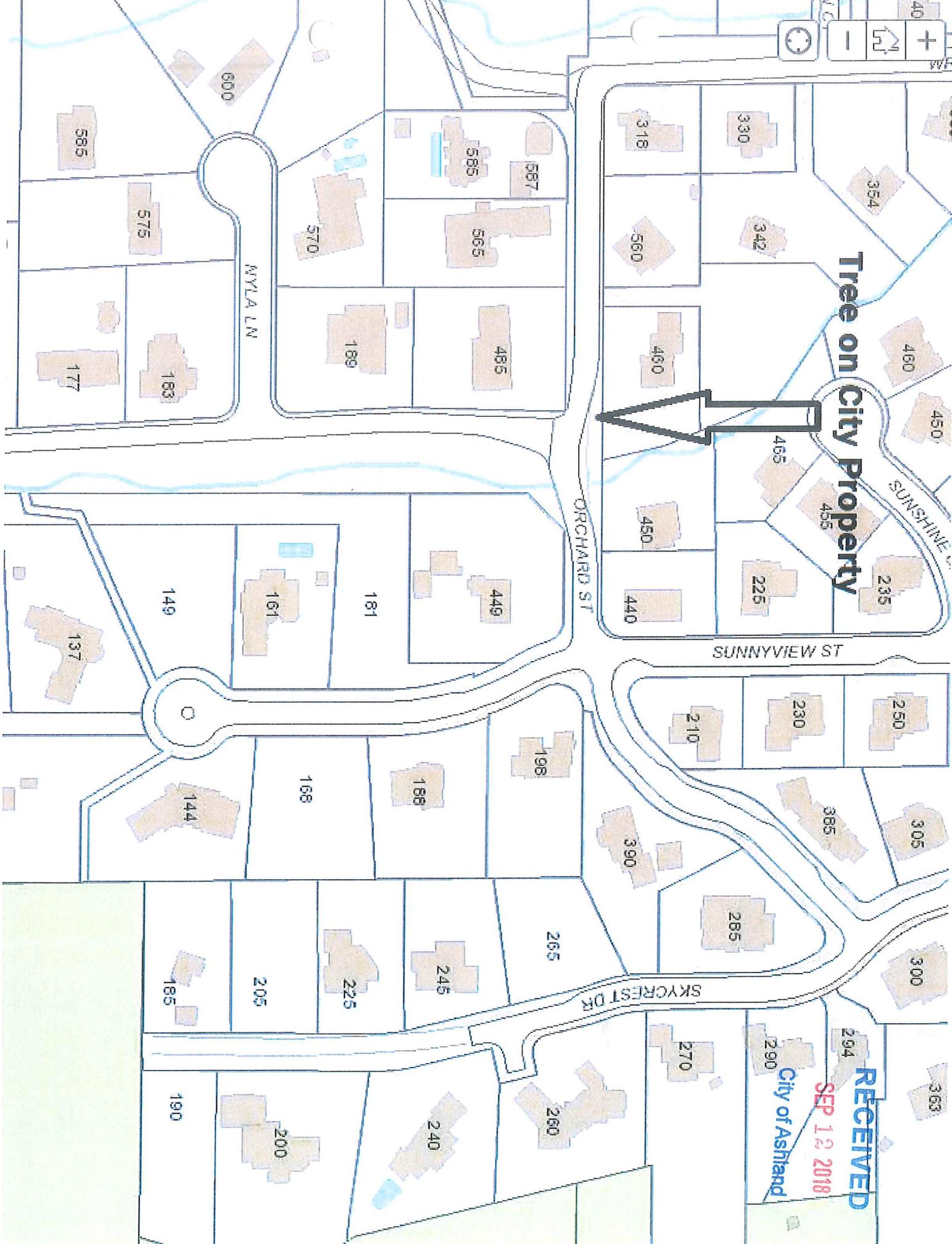
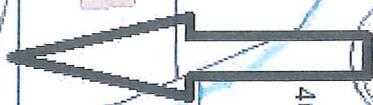
SEP 12 2018

City of Ashland





Tree on City Property



RECEIVED
SEP 12 2018
City of Ashland



Planning Division
 51 Winburn Way, Ashland OR 97520
 541-488-5305 Fax 541-488-6006

STREET TREE REMOVAL PERMIT

A tree that is located in any public street right-of-way or other public property may not be removed until a Street Tree Removal Permit has been submitted according to the Application Submission Requirements, below, and reviewed and approved by the City of Ashland.

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1. **Application Form and Fee.** The application must include the information requested on the Street Tree Removal Permit form provided by the City of Ashland and the permit application fee. Only those property owners of a lot adjoining the street tree location or homeowners' associations responsible for street trees in their development or subdivision may apply to remove an adjoining street tree. If a tree is located in front of more than one property, each property owner or homeowners' association official must sign the Street Tree Removal Permit form.
2. **Site Plan.** A site plan of the property drawn to scale containing the following information. The scale of the site plan must be at least one inch equals 50 feet or larger.
 - a. North arrow and scale.
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 - d. Size, species, and location of the tree(s) proposed to be removed.
3. **Written Statement.** A written statement explaining how the proposed street tree removal satisfies one of the following approval criteria. The Community Development director may require additional information to demonstrate that the proposed removal satisfies one of the following approval criteria including: 1) a written statement to be prepared by an arborist licensed by the State of Oregon Landscape Contractors Board of Construction Contractors Board and certified by the International Society of Arboriculture or American Society of Consulting Arborists; and 2) an International Society of Arboriculture (ISA) Basic Tree Risk Assessment Form to be completed by an arborist.

Street Tree Removal Approval Criteria

- a) **Emergency Tree Removal.** The tree presents an immediate danger of collapse and represents a clear and present hazard to persons or property. Immediate danger of collapse is defined as a tree that may already be leaning, with the surrounding soil heaving, and/or there is a significant likelihood that the tree will topple or otherwise fail and cause damage before a tree removal permit could be obtained through the non-emergency process.
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4. Applicants for a Street Tree Removal Permit may be required to replace the tree or trees being removed with a tree or trees of comparable value.
5. If a street tree is determined to be dead or dying, then the replacement need be no larger than the minimize size described above.

Type of Tree(s) Lodgepole Pine

Approximate Diameter at breast height 10" Height 20' Canopy 13'

Location of Tree ROW @ Northwest side of Strawberry Lane, adjacent 365 Strawberry Lane

Reason for Request Tree is on an eroding cut slope (from construction in 1980s) and exhibits exposed structural tree roots. Further cutting is recommended for foundation drainage improvements, therefore tree removal is recommended to prevent it from falling on the residence.

Are there underground utility lines and/or overhead power lines present? yes

If yes, please list which lines are present Storm sewer man hole is in close proximity; storm sewer for Strawberry Lane assumed below.

Is there sidewalk damage? no If yes, has a Public Works permit been issued? _____

OVER ►►

DESCRIPTION OF PROPERTY

Street Address 365 Strawberry Lane

Assessor's Map No. 39 1E 08AC Tax Lot(s) 602

Zoning RR-5 Comp Plan Designation low density residential

PROPERTY OWNER

Name William Potts Phone 541.482.4660 E-Mail wjpotts@msn.com

Address 365 Strawberry Lane City Ashland Zip 97520

Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

PROFESSIONAL PERFORMING THE TREE REMOVAL (e.g., tree service)

Name Regenesis Ecological Design Phone 541.488.7720 E-Mail info@regenesisdesign.com

Address 426 A Street, Ste 102 City Ashland Zip 97520

ARBORIST, LANDSCAPE ARCHITECT, OTHER

Title Ms Name Jane Alexanderr Phone 541.488.7720 E-Mail design@regenesisdesign.com

Address 426 A Street, Ste 102 City Ashland Zip 97520

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner. I hereby certify that the statements and information contained in this application are in all respects, true and correct. I further understand that if this request is subsequently contested, the burden will be on me to establish:

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- 3) *that all trees, structures, or improvements are properly located on the ground.*


William Potts (Jul 20, 2018)

Jul 20, 2018

Property Owner's Signature (required)

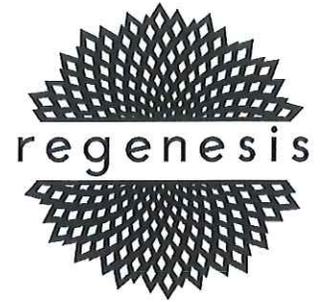
Date

STAFF DECISION:

Permit is hereby (circle one):	Approved	Approved with Conditions	Denied
Conditions of Approval _____			

Is the tree 18" d.b.h or greater? <input type="checkbox"/> NO <input type="checkbox"/> YES		Has the City Administrator has been notified: <input type="checkbox"/> NO <input type="checkbox"/> YES	
Community Development Director/Planning Manager Signature			Date

426 A Street Ste 102
Ashland, OR 97520
LCB #8720
p. 541.488.7720
f. 541.833.5018
info@regenesi sdesign.com



STREET TREE REMOVAL PERMIT – Written Statement

July 20, 2018

<u>Subject Property</u>	Address:	365 Strawberry Lane Ashland, OR 97520
	Map & Tax Lots:	39-1E-08AC; Tax Lot: 602
	Property Owner:	William J. Potts/ Sarah B. Esterling
	Agent:	Regenesi s Ecological Design C/O Jane Alexanderr 426 A Street, Ste 102 Ashland, OR 97520
	Zoning Designation:	RR-5
	Parcel Size:	.52 Acres

I. Street Tree Removal Narrative:

Request to remove a tree within the Public Right of Way of Strawberry Lane. The proposed tree removal is for a 10" DBH Lodgepole Pine at the Northwest side of Strawberry Lane, adjacent to 365 Strawberry Lane (Tax Lot 602, Zoning RR-5, Assessor's Map No. 08AC.

The tree removal will support a design which will properly support existing cut slopes on site that were not appropriately developed at the time of the home's construction in 1989. The attached 'Demolition and Tree Protection Plan' (L 01) references all tree protection and sediment control measures that will be taken before, during, and after construction of this project. The Lodgepole Pine is visible within sheet L 01 and is noted as 'to be removed' due to 'exposed tree roots within the cut slope zone of the proposed permanent erosion control wall system.'

Please refer to the plan set associated with the Physical & Environmental Review Permit Application, dated 6/29/2018 for additional site information + design parameters. Specifically, the 'Site Analysis' (A 03) details the constraints present to the area which the tree is located. The tree is shown within a Cut Slope exposing tree roots and bare soil above river rock wall. Utilities, drainage, and Contour lines present in the area surrounding the tree are also available to view within sheet A 03.

The tree is currently exposing structural tree roots and is in danger of falling on the established house if not removed. Therefore, the tree in question falls into the approval criteria for removal as a 'Hazard Tree Removal'. The tree presents a clear public safety hazard in that it is likely to fall and injure persons or property and the danger cannot be reasonably alleviated by treatment, relocation, or pruning.

The tree is currently on the edge on an eroding cut slope and will have to be removed for the establishment of the proposed cut slopes within the proposed design for this property. The removal of the tree will allow the establishment of a wall system designed by a licensed Geotechnical expert who has followed international building codes, with special precautions taken to minimize excavation and environmental site impacts.

The project manager, project engineer, geotechnical engineer, and landscape designer are available to review the requirements and answer questions during the processing of this application.

Thank you,

A handwritten signature in black ink, appearing to read 'Jane Alexanderr', written in a cursive style.

Jane Alexanderr | Lead Designer
Regenesi Ecological Design



Planning Division
 51 Winburn Way, Ashland OR 97520
 541-488-5305 Fax 541-488-6006

STREET TREE REMOVAL PERMIT

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5. If a street tree is determined to be dead or dying, then the replacement need be no larger than the minimize size described above.

RECEIVED
 SEP 17 2018
 City Of Ashland

Type of Tree(s) Juniper

Approximate Diameter at breast height 12" Height 20' Canopy 8'

Location of Tree Ally Way

Reason for Request Slaning into the ally wall. damage to foundation of garage

Are there underground utility lines and/or overhead power lines present? overhead & its slaning in them.

If yes, please list which lines are present electric

Is there sidewalk damage? no If yes, has a Public Works permit been issued? _____

OVER >>

DESCRIPTION OF PROPERTY

Street Address 166 N. Laurel

Assessor's Map No. 39 1E 04CC Tax Lot(s) 3400

Zoning R-2 Comp Plan Designation Multi-family Residential

PROPERTY OWNER

Name Vadim Agakharov Phone 541-951-6727 E-Mail _____

Address 262 B Street City Ashland Zip 97520

Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

PROFESSIONAL PERFORMING THE TREE REMOVAL (e.g., tree service)

Name: Quality Tree Service Phone (541) 631-8000 E-Mail _____

Address P.O. Box 3511 City Ashland Zip 97520

ARBORIST, LANDSCAPE ARCHITECT, OTHER

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner. I hereby certify that the statements and information contained in this application are in all respects, true and correct. I further understand that if this request is subsequently contested, the burden will be on me to establish:

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- 2) that the information contained in this application are adequate; and further
- 3) that all trees, structures, or improvements are properly located on the ground.

x Vadim
Property Owner's Signature (required)

9-18-18
Date

STAFF DECISION:

Permit is hereby (circle one): Approved Approved with Conditions Denied

Conditions of Approval _____

Is the tree 18" d.b.h or greater? NO YES Has the City Administrator has been notified: NO YES

Community Development Director/Planning Manager Signature Date

RECEIVED
SEP 17 2018
City Of Ashland

Quality Tree Service

PO BOX 3486
Central Point OR 97502
541-821-4594

September 11, 2018

RE: 166 N Laurel, Ashland OR 97520

It is my professional opinion as a certified Arborist that the tree in question poses an immediate hazard as it is causing damage to foundation and siding of garage. And it is also leaning on the power lines. It needs to be removed as soon as possible.

Thank you,

Jeff Ford (541)951-9953

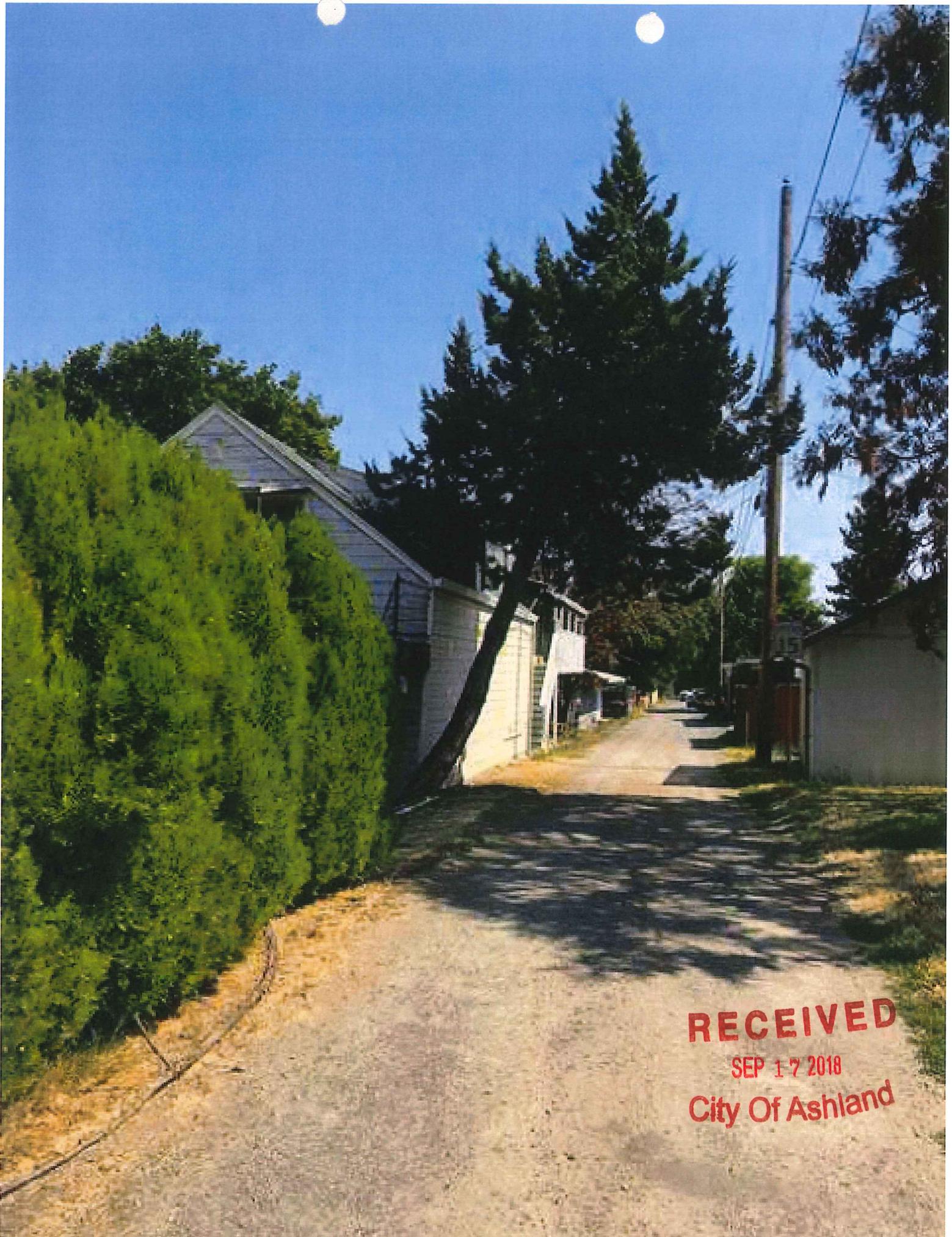
Quality Tree Service

ISA Certified Arborist PN-8187A

Quality Tree Service

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SEP 17 2018
City Of Ashland





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SEP 17 2018
City Of Ashland



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SEP 17 2018
City Of Ashland

