

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
SEPTEMBER 8, 2015**

**AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center Council Chambers, 1175 E. Main Street
- II. **ANNOUNCEMENTS**
- III. **AD-HOC COMMITTEE UPDATES**
- IV. **CONSENT AGENDA**  
A. **Approval of Minutes**  
1. August 11, 2015 Regular Meeting.
- V. **PUBLIC FORUM**
- VI. **UNFINISHED BUSINESS**  
A. Approval of Findings for PA-2015-00422, IPCO/City of Ashland, Tolman Creek Rd.
- VII. **TYPE II PUBLIC HEARINGS**  
A. **PLANNING ACTION: PA-2015-01370**  
**SUBJECT PROPERTY: 210-220 East Hersey St.**  
**OWNER: The Bernard Family Trust**  
**APPLICANT: Adroit Construction, as agent for the owners**  
**DESCRIPTION: A request for Site Design Review approval to allow the construction of a 24,621 square foot addition behind the existing 39,962 square foot Darex factory located at 210-220 East Hersey Street. (A second phase consisting of an 11,107 square foot stand-alone building along Clear Creek Drive will be reviewed separately at a later date.) Also included is a request for Tree Removal Permits to remove two trees six-inches or more in diameter at breast height: a six-inch Maple tree and a six-inch Pear tree. COMPREHENSIVE PLAN DESIGNATION: Employment; ZONING: E-1; ASSESSOR'S MAP: 39 1E 04CD; TAX LOTS: 2000.**
- B. **PLANNING ACTION: PA-2015-01496**  
**SUBJECT PROPERTY: 35 South Second Street**  
**OWNER/APPLICANT: MPM Investments**  
**AGENT: Kistler, Small & White, Architects**  
**DESCRIPTION: A request for Conditional Use Permit and Site Design Review approvals to allow 3,051 square feet of additions including a new kitchen, new bar, laundry room, two new second floor offices and an accessible lift, and the conversion of the existing kitchen into bussing and storage areas for the Winchester Inn located at 35 S. Second St. Also included are requests for Tree Removal Permits to remove two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house; and Exception to the Street Standards to retain the existing curbside sidewalks along the perimeter of the property. COMPREHENSIVE PLAN DESIGNATION: Commercial Downtown; ZONING: C-1-D; ASSESSOR'S MAP: 39 1E 09BD; TAX LOTS: 5600-5700.**
- VIII. **ADJOURNMENT**

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
MINUTES  
August 11, 2015

**CALL TO ORDER**

Chair Melanie Mindlin called the meeting to order at 7:00 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Troy J. Brown, Jr.  
Michael Dawkins  
Debbie Miller  
Melanie Mindlin  
Haywood Norton  
Roger Pearce

**Staff Present:**

Bill Molnar, Community Development Director  
Brandon Goldman, Senior Planner  
Derek Severson, Associate Planner  
April Lucas, Administrative Supervisor

**Absent Members:**

Lynn Thompson

**Council Liaison:**

Greg Lemhouse

**ANNOUNCEMENTS & AD HOC COMMITTEE UPDATES**

Community Development Director Bill Molnar noted the upcoming Planning Commission training opportunity on September 24, 2015 in Bend and asked interested members to contact staff. On August 30, 2015, the city commissioner appreciation event will be held at Oak Knoll Golf Course and members are asked to RSVP with City Recorder Barbara Christensen. Lastly, Mr. Molnar stated staff is anticipating a busy schedule over the next few months with four Type II actions coming before the commission.

Commissioner Dawkins commented on the Downtown Parking and Multi-Modal Circulation ad hoc committee. He stated the group is concentrating on parking and stated there are no major updates to report.

**CONSENT AGENDA**

**A. Approval of Minutes.**

1. July 14, 2015 Regular Meeting.
2. July 28, 2015 Special Meeting.

Commissioners Miller/Dawkins m/s to approve the July 14, 2015 Regular Meeting minutes. Voice Vote: all AYES.  
Motion passed 6-0.

Commissioners Norton/Pearce m/s to approve the July 28, 2015 Special Meeting minutes. Voice Vote: all AYES.  
Motion passed 4-0. *[Commissioners Brown and Miller abstained]*

**PUBLIC FORUM**

Joseph Kauth/482 Walker/Read aloud the City's proclamation of culture of peace and expressed concern about development.

**TYPE II PUBLIC HEARING**

**A. PLANNING ACTION: PA-2015-00422**

**SUBJECT PROPERTY:** 600-640-688-694-696 Tolman Creek Road, 2316 Hwy 66

**APPLICANT:** City of Ashland

**OWNERS:** Independent Printing Company, Inc., IPCO Development Corp.

AGENTS: CSA Planning, Ltd.

DESCRIPTION: A request for Site Design Review, Exception to Street Standards, Property Line Adjustment, Limited Use Permit/Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone, Physical & Environmental Constraints Review Permit for Floodplain Development, and Tree Removal Permit approvals to allow the construction of a new public street "Independent Way" between Washington Street and Tolman Creek Road and associated changes to the lane configuration and on-street parking on Tolman Creek Road to its intersection with Ashland Street. *(The proposal also includes the review of driveway locations and associated circulation to allow the coordinated initial grading and utility installation on the adjacent private property in conjunction with the new street installation, however the development of the adjacent private properties will be subject to future Site Design Review as individual buildings are proposed.)*

COMPREHENSIVE PLAN DESIGNATION: Employment; ZONING: E-1; ASSESSOR'S MAP: 39 1E 14BA; TAX LOTS: 500, 600, 601, 700, 800, 900 and 1000. *(Continued from July 14, 2015 Planning Commission Meeting)*

Commissioner Mindlin read aloud the public hearing procedures for land use hearings.

### Ex Parte Contact

No ex parte contact was reported.

### Staff Report

Associate Planner Derek Severson reviewed the issues raised last month at the commission's initial hearing:

- **Impacts to Tolman Creek Rd:** Mr. Severson stated the applicant's have submitted revised materials and this issue has been resolved. He explained the revised submittals include a 48 ft. parking bay on the new street to off-set some of the lost on-street parking on Tolman Creek Rd.
- **Independent Way Pedestrian Corridor:** The applicant's revised submittals address this element as well. The materials reflect the 13 ft. north side pedestrian corridor recommended by staff, incorporate an 8 ft. sidewalk and 5 ft. tree planting wells, and using structural soil to support the street tree canopy. The revised submittals also reflect a city standard sidewalk and parkrow installation on the south side where a crosswalk connection will be provided to the north side sidewalk. He noted the one exception that remains is the sidewalk section next to the existing IPCO building; however should this area redevelop in the future, there is adequate right of way to incorporate city standard sidewalks at that time.
- **Tree Removal and Rough Grading:** The revised grading plan and cross section drawings submitted by the applicant clarify the trees which were of greatest concern to the Planning Commission (#14, #15, #22) will be protected and preserved.

Mr. Severson briefly reviewed the revised conditions of approval, listed on pages 10-12 of the staff report. He added additional minor modifications the commission may wish to discuss and incorporate include: 1) Condition #4: Modify to read "*No hardscape parkrows shall be required in the area of the bridge crossing...*", 2) Condition #14: Modify to include a requirement for the landscape mitigation plan to include detail of the landscape materials to buffer the retaining wall as viewed from Washington St., and 3) Condition #17: Mr. Severson stated staff believes the current language which states "*Development of the site shall be subject to full review under the applicable standards at the time each building is proposed*" is sufficient, but if they feel it is necessary the commission could choose to modify this wording.

### Questions of Staff

Staff was asked when the management plan identified in Condition #12 is due and who reviews it. Mr. Severson responded it is required prior to the final approval of the creek corridor plan and it is reviewed by staff. He added the commission could modify the condition to make this clearer if they felt it was necessary.

### Applicant's Presentation

Mike Faught, City of Ashland Public Works Director and Brad Barber, Project Manager/City Surveyor addressed the commission. Mr. Faught stated this project is a public/private partnership and the city is working with the landowner to get this road installed. He commented on the revised materials and stated a driveway location has been moved to preserve trees #14 and #15, and two parking stalls have been removed to save the Ponderosa Pine. He stressed that they are not asking for site plan approval and stated the footprints shown represent the maximum size buildings allowed. He added the actual buildings may be smaller, however they wanted to give some certainty for the driveway locations. Mr. Faught stated

the revised street design brings them into full compliance and commented on the re-striping plan for Tolman Creek Rd. He also commented on the importance of the south side crosswalk and the hardscape bicycle and pedestrian corridor on the north side of the new street. Mr. Faught stated they listened to the commission's feedback from the last meeting, met with staff, and have made the requested changes to be consistent with staff's recommendations.

### Questions of the Applicant

Commissioner Mindlin stated the CSA memo, which is now part of the application, indicates the Planning Commission *"must conclude that the proposed building pads are located such that future buildings in these locations could feasibly be designed"* and wanted to make it clear that this would require site plan review, which the commission has not done.

**Mike Thornton, Thornton Engineering**, clarified they are not seeking approval for the building pads, only the rough grading plan. Mr. Thornton stated the reason for the grading request is due to the amount of earth moving that is needed to accommodate the road and the future buildings. He added given the fall of the site it makes sense to do this now and gives them a step forward in whatever site plan is ultimately approved.

Mr. Severson stated this is similar to the approval process for residential subdivisions; the building envelope locations are identified, but the applicant still has to go through a separate site review process and receive approval.

The applicant was asked if it is possible to preserve more trees on the site. Mr. Faught explained they were able to preserve three additional trees and more trees will be added as the site builds out, including street trees, water resource trees, and landscaping trees. Mr. Faught added the end result will be a net increase of trees on the site than what is there now.

### Applicant's Rebuttal

**Zach Brombacher/1370 Tolman Creek**/Stated he has been working with the city on this plan for the last four years and shared his desire to bring more businesses and jobs to Ashland. He pointed out that this is an employment zone and stated they have worked hard to develop a plan that provides for the new road and also takes into consideration the future of the site including parking, landscaping, etc. to make sure it can comply with city standards.

*Commissioner Mindlin closed the record and public hearing at 8:00 p.m.*

### Deliberations & Decision

**Commissioners Dawkins/Brown m/s to approve PA-2015-00422 with the modified conditions of approval outlined by staff.** **DISCUSSION:** Dawkins clarified this motion includes staff's modifications to conditions #4 and #14 and stated this connection is an essential link in the city's transportation system and he is pleased that staff and the property owner have been able to work this out. He added it is clear they are not approving new buildings, that will come later in the process, and stated the future plan for the area with the trees and landscaping will be far better than what is there now. Brown stated the clarifications provided by the applicant really helped and he is looking forward to seeing this completed. He stated tying the two streets together is absolutely necessary and he is 100% in favor of this project. Mindlin stated she is also supportive, but wants to make sure the findings reflect their understanding of what they are approving, and noted page two of CSA's memo include statements she does not agree with. Mindlin explained the following two statements are not accurate: *"In order to approve the grading and utility plan as proposed, the Planning Commission must conclude that the proposed building pads are located such that future buildings in these locations could be feasibly designed to comply with the Site Plan Review criteria that pertain to building locations"* and *"At its essence, approval of the building pads means that the Planning Commission finds that the proposed locations of the building pads are appropriate to site future buildings under the City's code"*.

**Commissioners Mindlin/Miller m/s to amend motion to include in the findings that the commission is not agreeing with the two identified statements contained in the CSA memo. Voice Vote: all AYES. Motion passed 6-0.**

**CONTINUED DISCUSSION on motion as amended:** Mindlin noted by approving the curb cuts, curbing, and retaining wall locations they are establishing pre-existing conditions that are not self imposed for when the applicant comes back in the future with a development application. **Roll Call Vote: Commissioners Brown, Dawkins, Miller, Norton, Pearce, and Mindlin, YES. Motion passed 6-0.**

Commissioner Miller stated she would not participate in the next hearing and left the meeting.

### TYPE III LEGISLATIVE PUBLIC HEARING

#### A. PLANNING ACTION: PL-2013-01858

APPLICANT: City of Ashland

LOCATION: Normal Neighborhood District Boundary

REQUEST: To amend the Comprehensive Plan, Comprehensive Plan Map, Transportation System Plan, and Ashland Land Use Ordinance to implement the Normal Neighborhood Plan. *(Continued from July 28, 2015 Planning Commission Meeting)*

### Staff Report

Senior Planner Brandon Goldman provided overview of last meeting and stated tonight's meeting focuses on the items identified for additional discussion. He explained based on the commission's previous discussion they may wish to include the following recommendations to the city council:

- To allow greater flexibility for cluster housing within the NN-1-5 single family zone, amend Table 18.3.4.040 Land Use Descriptions to list this as "Permitted" under NN-1-5, and amend the Normal Neighborhood plan framework housing type descriptions under "Pedestrian-Oriented Clustered Residential Units" to include NN-1-5 as a zone that permits this use.
- At the last meeting the commission discussed wording in the framework document that indicates the use of alleys and rear lanes reduces pavement and questioned if this wording should be stricken. Mr. Goldman stated one section has already been removed, and stated the commission could recommend eliminating *"The narrow street section of rear lanes reduces the extent of impervious surfaces in the Normal Neighborhood and supports wetland and stream health"* from page 28.
- To broaden the applicability for shared streets, the commission could recommend amending the Shared Street description in Ordinance #2 to read, *"Provides access to residential uses in an area in which right-of-way is constrained by natural features, topography, or historically significant structures. Shared streets may additionally be used in circumstances where a slower speed street, collectively shared by pedestrians, bicycles, and autos, is a functional and preferred design alternative. The design of the street should emphasize a slower speed environment and provide clear physical and visual indications the space is shared across modes."*
- To expand on the minor amendment process for alterations to the Open Space Network Map to reflect a DSL approved wetland delineation, the commission could recommend the following language be added to 18.3.4.060.A:
  - "5. Conformance with Open Space Network Plan*  
*New developments must provide open space consistent with the design concepts within the Greenway and Open Space chapter of the Normal Neighborhood Plan Framework and in conformance with the Normal Neighborhood Plan Open Space Network Map. The open space network will be designed to support the neighborhood's distinctive character and provide passive recreational opportunities where people can connect with nature, where water resources are protected, and where riparian corridors and wetlands are preserved and enhanced.*
    - a. The application demonstrates that equal or better protection for identified resources will be ensured through restoration, enhancement, and mitigation measures.*
    - b. The application demonstrates that connections between open spaces are created and maintained providing for an interlinked system of greenways.*
    - c. The application demonstrates that open spaces function to provide habitat for wildlife, promote environmental quality by absorbing, storing, and releasing stormwater, and protect future development from flood hazards,*
    - d. The application demonstrates that scenic views considered important to the community are protected, and community character and quality of life are preserved by buffering areas of development from one another."*

Mr. Goldman concluded his presentation and stated the commission's recommendation will be forwarded to the city council for consideration on September 1, 2015. Mr. Molnar noted the city's Public Works Director Mike Faught is present and can answer any questions the commission has regarding the transportation system or city water supply.

Mr. Faught addressed the commission. He explained when a development proposal comes forward for this area with the applicant wanting to connect to East Main St, the city will require a 250 ft. transition in either direction that will be a three-lane configuration on East Main St and will also require a bicycle and pedestrian connection for the full length. He added any additional development to the area will likely trigger the full length of East Main St to be improved. Regarding the railroad crossing, he explained any development on that end of the project area will trigger the need to change this from a private to a public crossing and the installation of a traffic signal at Normal Avenue and Ashland Street.

Mr. Faught was asked to comment on the testimony the commission received about the potential impact development will have on the city's water supply. Mr. Faught stated the current water master plan includes the assumption that the city will have a drought every few years and if the city continues to grow at 1% per year, the water supply is good through 2038 and up to 2060. He noted the city is getting started on a new master plan and they will run new assumptions to confirm the estimates are accurate, and stated they can make adjustments if necessary once the city starts receiving proposals for this area. He added while this is a large area to be developed with 450 potential units, it is not likely this will be built out very quickly.

Mr. Molnar clarified it is against the law to use water supply as a growth regulator. He explained if you want to limit growth due to lack of a public facility, the city would have to go through the process to enact a moratorium and are required to create a plan on how you are going to work your way out of it. He added each city needs to accommodate their fair share of the growth and Ashland has already made the finding through our master plans that we have the supply. He noted the city's growth projections used in the master plans have been conservative and actual growth is quite less. Mr. Faught stated the city can do a better job in updating their master plans if they know what it going to be in these areas in the future. He added if this plan is approved he will turn it over to his engineers for more analysis and it will assist them in future master planning.

#### Public Testimony

**Bryce Anderson/2092 Creek Dr**/Stated 1,000 ft. of the East Main St area to be improved fronts Ashland Middle School and stated 35%-45% of the total cost will be allocated to them. Mr. Anderson stated unless the developers pay for this section this plan does not adequately provide for the improvements. He added the East Main Street improvements should be completed before any development is approved.

**Randy Jones/815 Alder Creek Dr, Medford**/Stated he represents several property owners in the plan area and they would like to create workforce housing that is affordable to teachers, fireman, etc. He voiced his appreciation for the compromises that have been made and for the flexibility that has been built into the plan. Mr. Jones stated the working group worked very hard and encouraged the commission to look closely at the amendments brought forward by the working group and talk with those members before they make any modifications.

**Sabra Hoffman/345 Scenic Dr**/Stated the 2011 Buildable Lands Inventory states 1,800 buildable lots are available and stated this in addition to the 450 proposed units would need to be factored into the downturn of the city's water resources.

**David Hoffman/345 Scenic Dr**/Stated he does not believe the Buildable Lands Inventory has been considered as nothing in the record for this plan speaks to it. He questioned why they are proposing to add 450 new units when there are already 1,800 sites available within the city limits.

**Howard Miller/160 Normal**/Commented on urban agriculture and stated there is more and more interest in this. Mr. Miller stated there are very few areas in the valley that are better suited for urban agriculture than this area and once this land gets paved over it will be too late. He stated this plan is a mess of assumptions and complexities and stated without clarity he does not think this plan has a future.

#### Deliberations & Decision

Transportation: Commissioner Dawkins was asked to comment since he participated on the working group. Dawkins responded that he is comfortable with the plan with the four recommended amendments outlined by staff. He added the plan only establishes the framework and the final road locations and details will come into play when a development plan is

submitted. Commissioner Brown also expressed his support with the proposed master plan. He stated they are still on the same page that the East Main St. and railroad crossing improvements need to be done, and stated it may take 20-30 years before this whole development is completed. Commissioners Pearce and Norton also expressed their support. Commissioner Mindlin questioned if the criteria used to determine whether a proposal provides adequate transportation is sufficient. Comment was made that adequate transportation is different from adequate connectivity. Mr. Molnar agreed and stated adequate transportation is evaluated when a proposal comes forward based on the number of units, etc. Mr. Faught commented that the city's standpoint is clear that there needs to be a connection to Ashland St. as soon as possible and the framework document identifies the importance of this connection. Dawkins added the working group was clear that the railroad crossing is critical.

Housing: Support was voiced for the two housing amendments outlined by staff.

Open Space: Commissioner Mindlin suggested they recommend the major amendment process to alter the Open Space Network Map, but if the city council chooses to stay with the minor amendment process recommend the additional criteria language outlined by staff. Commissioners Brown, Dawkins, and Norton agreed. Commissioner Pearce stated he would be okay with the minor amendment process, but would support this as well. Commissioner Mindlin commented that it seems the working group did not fully vet this issue as it was only discussed at the last couple meetings.

**Commissioners Brown/Pearce m/s to recommend city council's approval of the three ordinances with the recommended changes outlined in the staff report addendum and discussed by the commission. DISCUSSION:** Brown voiced his support for the master plan and stated it lays out the appropriate parameters but still allows for some flexibility. Dawkins commented he agrees with a lot of the public testimony they have received, but that is not relevant to what this plan is about. He stated by doing this they at least get to establish some parameters, and without the plan it can get developed however the landowner chooses, including developing over wetland areas. **Roll Call Vote: Commissioners Pearce, Norton, Dawkins, Brown, and Mindlin, YES. Motion passed 5-0.**

#### **ADJOURNMENT**

Meeting adjourned at 9:35 p.m.

*Submitted by,  
April Lucas, Administrative Supervisor*

**BEFORE THE PLANNING COMMISSION**  
**September 8, 2015**

IN THE MATTER OF PLANNING ACTION #2015-00422, A REQUEST FOR )  
SITE DESIGN REVIEW; EXCEPTION TO STREET STANDARDS; PROPERTY )  
LINE ADJUSTMENT; LIMITED USE PERMIT AND WATER RESOURCE )  
PROTECTION ZONE REDUCTION FOR CONSTRUCTION IN THE WATER )  
RESOURCE PROTECTION ZONE; PHYSICAL & ENVIRONMENTAL )  
CONSTRAINTS REVIEW PERMIT FOR FLOODPLAIN DEVELOPMENT; AND )  
TREE REMOVAL PERMIT APPROVALS TO ALLOW THE CONSTRUCTION )  
OF A NEW PUBLIC STREET TO BE CALLED INDEPENDENT WAY BETWEEN )  
WASHINGTON STREET AND TOLMAN CREEK ROAD, AND ASSOCIATED )  
CHANGES TO THE LANE CONFIGURATION AND ON-STREET PARKING ON ) **FINDINGS,**  
TOLMAN CREEK ROAD TO ITS INTERSECTION WITH ASHLAND STREET. ) **CONCLUSIONS,**  
THE PROPOSAL ALSO INCLUDES THE REVIEW OF DRIVEWAY LOCATIONS ) **AND ORDERS**  
AND ASSOCIATED CIRCULATION TO ALLOW COORDINATED INITIAL )  
GRADING AND UTILITY INSTALLATION ON THE ADJACENT PRIVATE )  
PROPERTY IN CONJUNCTION WITH THE NEW STREET INSTALLATION, )  
HOWEVER THE DEVELOPMENT OF THE ADJACENT PROPERTIES WILL BE )  
SUBJECT TO FUTURE SITE DESIGN REVIEW AS INDIVIDUAL BUILDINGS )  
AND THEIR SITE ASSOCIATED IMPROVEMENTS ARE PROPOSED, AND IS )  
NEITHER PROPOSED NOR CONSIDERED HERE. )

**APPLICANT:** City of Ashland (CSA Planning, Ltd., agents)

-----  
**RECITALS:**

- 1) Tax lots 500, 600, 601, 700, 800, 900 and 1000 of Map 39 1E 14 BA are located at 600, 640, 688, 694 and 696 Tolman Creek Road, and 2316 Highway 66 and are zoned E-1, Employment..
- 2) The applicants are requesting Site Design Review, Exception to Street Standards, Property Line Adjustment, Limited Use Permit and Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone, Physical & Environmental Constraints Review Permit for Floodplain Development, and Tree Removal Permit approvals to allow the construction of a new public street to be called "Independent Way" between Washington Street and Tolman Creek Road, and associated changes to the lane configuration and on-street parking on Tolman Creek Road to its intersection with Ashland Street.

The proposal also includes the review of driveway locations and associated circulation to allow the coordinated initial grading and utility installation on the adjacent private property in conjunction with the new street installation, however the development of the adjacent private properties is neither proposed nor considered here and will be subject to future Site Design Review as individual buildings are proposed.

The proposal is outlined on plans on file at the Department of Community Development.

3) The criteria for Site Design Review approval are described AMC 18.5.2.050 as follows:

- A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
- B. **Overlay Zones:** *The proposal complies with applicable overlay zone requirements (part 18.3).*
- C. **Site Development and Design Standards:** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
- D. **City Facilities:** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*

4) The criteria for Exception to Street Standards are described AMC 18.4.6.020.B.1 as follows:

- a. *There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.*
- b. *The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.*
  - i. *For transit facilities and related improvements, access, wait time, and ride experience.*
  - ii. *For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.*
  - iii. *For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.*
- c. *The exception is the minimum necessary to alleviate the difficulty.*
- d. *The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.*

5) The criteria for Property Line Adjustments are described AMC 18.5.3.120.B as follows:

- 1. **Parcel Creation.** *No additional parcel or lot is created by the lot line adjustment.*
- 2. **Lot Standards.** *Except as allowed for nonconforming lots, pursuant to chapter 18.1.4, or as required by an overlay zone in part 18.3, all lots and parcels conform to the lot*

*standards of the applicable zoning district, including lot area, dimensions, setbacks, and coverage, per part 18.2. If a lot does not conform to the lots standards of the applicable zoning district, it shall not be made less conforming by the property line adjustment. As applicable, all lots and parcels shall identify a buildable area free of building restrictions for physical constraints (i.e., flood plain, greater than 35 percent slope, water resource protection zones).*

3. ***Access Standards.*** *All lots and parcels conform to the standards in section 18.4.3.080 Vehicle Area Design. Lots and parcels that do not conform to the access standards shall not be made less conforming by the property line adjustment.*

6) The criteria for a Limited Activities and Use Permit are described AMC 18.3.11.060.D as follows:

1. *All activities shall be located as far away from streams and wetlands as practicable, designed to minimize intrusion into the Water Resources Protection Zone and disturb as little of the surface area of the Water Resource Protection Zone as practicable.*
2. *The proposed activity shall be designed, located and constructed to minimize excavation, grading, area of impervious surfaces, loss of native vegetation, erosion, and other adverse impacts on Water Resources.*
3. *On stream beds or banks within the bank full stage, in wetlands, and on slopes of 25 percent or greater in a Water Resource Protection Zone, excavation, grading, installation of impervious surfaces, and removal of native vegetation shall be avoided except where no practicable alternative exists, or where necessary to construct public facilities or to ensure slope stability.*
4. *Water, storm drain, and sewer systems shall be designed, located and constructed to avoid exposure to floodwaters, and to avoid accidental discharges to streams and wetlands.*
5. *Stream channel repair and enhancement, riparian habitat restoration and enhancement, and wetland restoration and enhancement will be restored through the implementation of a mitigation plan prepared in accordance with the standards and requirements in section 18.3.11.110 Mitigation Requirements.*
6. *Long term conservation, management and maintenance of the Water Resource Protection Zone shall be ensured through preparation and recordation of a management plan as described in subsection 18.3.11.110.C, except a management plan is not required for residentially zoned lots occupied only by a single-family dwelling and accessory structures.*

7) The criteria for Water Resource Protection Zone Reductions are described AMC 18.3.11.070 as follows:

- A. *The proposed use or activity is designed to avoid intrusion into the Water Resource Protection Zone through the use of up to a 50 percent reduction of any dimensional standards (e.g., required front, side and rear yard setbacks; required distance between buildings) to permit development as far outside or upland of the Water Resource Protection Zone as possible. Such adjustment to any applicable dimensional standards shall be reviewed as part of the requested reduction, and shall not be subject to a separate Variance application under chapter 18.5.5 Variances. Reductions to dimensional standards may not be used to reduce required Solar Access setbacks without evidence of agreement by the effected property owner(s) to the north through a concurrent Solar Access Variance application as described in chapter 18.4.8 Solar Access.*
- B. *The alteration of the Water Resource Protection Zone is the minimum necessary to efficiently perform the proposed activity and/or use. The proposed development shall minimize disturbance to the Water Resource Protection Zone by utilizing the following design options to minimize or reduce impacts of development.*
  - 1. *Multi-story construction shall be considered.*
  - 2. *Parking spaces shall be minimized to no more than that required as a minimum for the use.*
  - 3. *Pavement shall be minimized, and all pavement used shall be installed and maintained in a porous solid surface paving material.*
  - 4. *Engineering solutions shall be used to minimize additional grading and/or fill.*
- C. *The application demonstrates that equal or better protection for identified resources will be ensured through restoration, enhancement, and mitigation measures. The structures, functions, and values of the Water Resource will be restored through the implementation of a restoration and enhancement strategy set forth in a mitigation plan prepared in accordance with the standards and requirements described in section 18.3.11.110 Mitigation Requirements.*
- D. *Long term conservation, management, and maintenance of the Water Resource Protection Zone shall be ensured through preparation and recordation of a management plan as described in subsection 18.3.11.110.C, except a management plan is not required for residentially zoned lots occupied only by a single-family dwelling and accessory structures.*

8) The criteria for a Physical & Environmental Constraints Review Permit are described AMC 18.3.10.050 as follows:

- A. *Through the application of the development standards of this chapter, the potential impacts to the property and nearby areas have been considered, and adverse impacts have been minimized.*
- B. *That the applicant has considered the potential hazards that the development may create and implemented measures to mitigate the potential hazards caused by the development.*
- C. *That the applicant has taken all reasonable steps to reduce the adverse impact on the environment. Irreversible actions shall be considered more seriously than reversible actions. The Staff Advisor or Planning Commission shall consider the existing development of the surrounding area, and the maximum development permitted by this ordinance.*

9) The criteria for a Tree Removal Permit are described in AMC 18.5.7.040.B as follows:

1. **Hazard Tree.** *A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
  - a. *The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.*
  - b. *The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*
2. **Tree That is Not a Hazard.** *A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.*
  1. *The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.*
  2. *Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*

3. *Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
4. *Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
5. *The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

10) The Planning Commission, following proper public notice, held a public hearing on July 14, 2015 at which time testimony was received and exhibits were presented. Following testimony, the Planning Commission noted that the submittal materials made it difficult to discern the specifics of the request, and asked that revised materials clarifying the proposal be brought to the next meeting. The hearing was continued until the Planning Commission's next regular meeting on August 11, 2015 at which time testimony was again received and exhibits were again presented. Subsequent to the closing of the hearing, the Planning Commission approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

## **SECTION 1. EXHIBITS**

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

## **SECTION 2. CONCLUSORY FINDINGS**

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the Staff Report, public hearing testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Site Design Review, Exception to Street Standards, Property Line Adjustment, Limited Use Permit and Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone, Physical & Environmental Constraints Review Permit for Floodplain Development, and Tree Removal Permit approvals meets all applicable criteria for Site Review approval described in Chapter 18.5.2.050, Exception to Street Standards as described in Chapter 18.4.6.020.B.1, Property Line Adjustment as described in Chapter 18.5.3.120.B, Limited Use Permit and Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone as described in Chapters 18.3.11.060.D and 18.3.11.070, Physical & Environmental Constraints Review Permit for Floodplain Development as described in Chapter 18.3.10.050, and Tree Removal Permits as described in Chapter 18.5.7.040.B.2.

2.3 The Planning Commission finds that the proposal is somewhat limited in terms of Site Design Review because it involves only the review of the street installation, driveway locations and associated circulation to allow public improvements associated with the new street and coordinated initial grading and utility installation on the adjacent private property.

The materials provided emphasize that it is logical to complete grading and the installation of the main underground utilities for the site in conjunction with the street construction, however the applicants recognize that the extent of the entitlements sought are limited to this rough grading and associated installation of utilities with site planning details including final building locations, building designs, parking, site circulation, plaza space, truck loading bays and similar elements to be evaluated when development is proposed. The Commission notes that despite this recognition, page two of the July 22, 2015 memorandum from the applicants' agents CSA Planning Ltd. contains the following statements: *"In order to approve the grading and utility plan as proposed, the Planning Commission must conclude that the proposed building pads are located such that future buildings in these locations could be feasibly designed to comply with the Site Plan Review criteria that pertain to building locations"* and *"At its essence, approval of the building pads means that the Planning Commission finds that the proposed locations of the building pads are appropriate to site future buildings under the City's code"*. The Commission explicitly rejects these statements, and finds it important to emphasize that no development of the adjacent private properties is being proposed by the applicants or considered by the Commission with this request, and when individual buildings and their associated site improvements are proposed, they will be subject to future land use review under all applicable regulations in effect at the time. The Commission further finds that by approving the curb cuts, curbing, and retaining wall locations, the Commission is recognizing the establishment of what will be pre-existing conditions that will not be considered self-imposed when the applicant brings forth a development proposal in the future.

The Commission further finds that consideration of underlying zoning requirements, overlay zones, and the Site Development and Design Standards are largely deferred until the adjacent properties develop, and that this Site Design Review focuses largely on determining that the improvements proposed comply with applicable standards, that public facilities (water, sewer, electricity, urban storm drainage,

and paved access) have adequate capacity to and throughout the property, and that adequate transportation can and will be provided.

The application materials provided note that there is an existing water main in Tolman Creek Road that will be extended through the subject property within the proposed Independent Way right-of-way in a new eight-inch water main. Similarly, there is an existing sanitary sewer main in Tolman Creek Road. An existing sanitary sewer lateral extends to serve the existing IPCO development on site. The application notes that a portion of this line will be reconstructed as part of the road project and that this line will serve any future development on the south side of Independent Way. Future development on the north side of Independent Way will be served by a new sanitary sewer lateral somewhere near the north property boundary that will connect to the existing sanitary sewer service in Tolman Creek Road.

The application further explains that storm drainage from the new Independent Way is designed to outflow into the Hamilton Creek drainage. The proposed street design includes two Filterra® bio-retention systems to treat stormwater before it enters the stream; these systems improve water quality by removing various undesirable particulates in the water for typical storm water events. The Filterra® systems are proposed at each of the two storm drain inlets on the south side of Independent Way. Future private development on the south side of Independent Way will direct storm drainage to the new storm drainage system in Independent Way. Future development on the north side of Independent Way will utilize a new storm drain installed at the north boundary of the project site and direct flows into Hamilton Creek. The application notes that future storm water treatment systems in this area are expected to be integrated into the site landscaping design and reviewed as part of future Site Design Review in conjunction with development.

The Commission further finds that in considering the proposal in light of the requirements for adequate transportation, a Traffic Impact Analysis has been provided with the application. Items of note with regard to the proposal include:

- No bike lanes are proposed on either side of the new street as vehicle trips are anticipated to be low enough that cyclists will be comfortable sharing the road, and the design is to include “sharrows”. The materials also suggest that the proposed pedestrian corridor is adequate to accommodate bicyclists.
- The application explains that driveway widths are proposed as somewhat wider than usual to accommodate semi-truck circulation to serve the existing IPCO facility and that this is necessitated because the new road is proposed only approximately 25 feet from the existing building.
- The application notes that the nature of the existing land uses on the south side of Independent Way as currently developed results in minimal pedestrian demand, which when combined with the anticipated truck traffic serving these uses, has lead the applicants to propose not to install standard sidewalks on the full south side of the proposed Independent Way. The application recognizes that in future years, the existing IPCO building on the south side of Independent Way

may redevelop and that the right-of-way width proposed is adequate to accommodate City standard pedestrian improvements with future redevelopment of the site.

- The application proposes to place street trees within grated wells in a five-foot parkrow on the north side of the new street, noting that this design option is typical where on-street parking is to be provided. These grated tree wells provide a flat area for people to step onto when exiting parked cars. The application emphasizes that the ultimate plan for the street is that in the event that the IPCO Printing business were to redevelop, on-street parking would be added on the north side of the street. As such, the street width is proposed at a 28-foot curb-to-curb width consistent with a cross-section to allow for parking on one side of the street. The application emphasizes that for the time being, full 14-foot lanes are needed to accommodate the large number of heavy vehicles and associated turning movements associated with the existing industrial use of the site.
- Based on the applicants' Traffic Impact Analysis, on-street parking on Tolman Creek Road from its intersection with Ashland Street (Highway 66) and the new Independent Way will be eliminated in conjunction with lane reconfiguration. The applicants' Traffic Engineer makes this recommendation to reduce northbound queuing along Tolman Creek Road, improve the signalized intersection operation of Tolman Creek Road and Ashland Street, and provide a left turn lane at Tolman Creek Road. The application emphasizes that parking between Ashland Street and the new Independent Way is seldom used, and the removal of this parking would be required with or without the proposed Independent Way construction to address operational deficiencies at the intersection due to background traffic growth. The application emphasizes that the functionality of the intersection, one of the busiest in Ashland, will be operationally improved because of the project and that the project will result in less circuitous access to Washington and Jefferson Street businesses by pedestrians and cyclists.

The Commission finds that the materials provided argue that most of the on-street parking which would to be removed is seldom used, and that the operation of the Ashland Street-Tolman Creek intersection is such that the on-street parking will need to be removed in this area regardless of the proposed project. The application further emphasizes that the construction of the proposed new Independent Way connection will improve intersection operations at this location. The applicants also indicate that the on-street parking that is being most utilized is around the Goodwill, and the applicants have raised the potential for installing a nine-foot deep parking bay 48 feet long on the south side of the new Independent Way near its intersection with Tolman Creek Road, across from Goodwill. The Commission finds that whatever measure of on-street parking can be provided on Independent Way to off-set the removal of on-street parking on Tolman Creek Road is beneficial, and a condition has accordingly been required below to require the applicants' proposed 48-foot parking bay on the south side of Independent Way near its intersection with Tolman Creek Road.

2.4 The Planning Commission finds that the proposed new Independent Way is identified in the recently adopted Transportation System Plan as a Commercial Neighborhood Collector Street, and will serve as a gateway to nearby areas likely to see significant local job and housing growth in the near future. The Jefferson/Washington employment area, much of which is outside current city limits but within the urban growth boundary, consists of approximately 45 acres, including the commercial/employment area along Ashland Street and Tolman Creek Road, the city's second largest employment center after the downtown. 45 acres developed to an approximate Floor Area Ratio of 0.35

and an employment density of 20 employees per acre equates to approximately 686,000 square feet of building floor area and 900 employees ultimately served in the vicinity.

The typical cross-section required for a city standard “Commercial Neighborhood Collector Street” includes two ten-foot motor vehicle travel lanes, eight-foot parking bays on one or both sides, a five- to seven-foot planting strip with street trees down both sides, and eight- to ten-foot sidewalks on both sides, all within a right-of-way width of between 55 and 71 feet. Bike lanes are only required where anticipated average daily trips exceed 3,000. The applicants initially proposed a 55-foot right-of-way width to consist of a 12-foot pedestrian corridor on the north side and two 14-foot travel lanes, with the potential to add on-street parking on the north side when the IPCO site redevelops. Exceptions to the Street Standards were requested to not install a city standard sidewalk on the south side of the street, to install six-foot curbside sidewalks on both sides of the street at the creek crossing, and to install a 12-foot pedestrian corridor on the north side of the street where a minimum of 13 feet is required. The Commission finds that Exceptions require a demonstration that the facilities and resultant connectivity will be equal or superior to those required under the standards; that the exceptions requested are the minimum necessary to alleviate the difficulty, and that the exceptions are consistent with the purpose and intent of the Street Standards.

The Commission finds that the city’s street standards are based on an assumption that all city streets are to have parkrows and sidewalks on both sides, while recognizing that in certain situations where the physical features of the land create severe constraints, or natural features should be preserved, Exceptions may be made which could result in meandering sidewalks, sidewalks on only one side of the street, or curbside sidewalk segments instead of setback walks. However, the Commission finds that the Street Standards emphasize that Exceptions should only be allowed when physical conditions exist that preclude development of a public street, or components of the street, and notes that such conditions may include, but are not limited to, topography, wetlands, mature trees, creeks, drainages, rock outcroppings, and limited right-of-way when improving streets through a local improvement district.

The Street Standards for a Neighborhood Commercial Collector Street call for a 13-foot pedestrian corridor while the application initially requested an Exception to provide only a 12-foot corridor on the north side. The Commission finds that insuring that the new street constructed provides for safe and functional use by users of all modes of transportation over the life of the street as the area develops fully is crucial, and given the street’s anticipated role as a Neighborhood Commercial Collector likely to serve the Washington Street and Croman employment areas, its anticipated level of vehicle trips with build-out in the area, and the truck circulation associated with the anticipated development of the applicants’ properties, it would be difficult to find that a substandard corridor is “equal or superior” or that the width reduction is necessary to alleviate any difficulty when the street is new and the right-of-way proposed is of a width intended to accommodate standard improvements. The Planning Commission finds that the revised submittals provided for the August 11<sup>th</sup> hearing reflect the city standard 13-foot width north side pedestrian corridor, not counting curbing, incorporating an eight-foot wide sidewalk and five-foot by five-foot tree planting wells, as recommended by planning staff,

The Commission further finds that where on-street parking is not planned, the 13-foot pedestrian corridor *may* consist of a continuous, seven-foot wide planting strip with a six-foot sidewalk rather than

a five-foot hardscape planting strip and eight-foot sidewalk. Planning staff had initially recommended that, given that on-street parking is not initially to be provided, a continuous seven-foot park row planting strip would better accommodate street tree growth to enhance the corridor and provide a greater buffer for pedestrians particularly given anticipated truck traffic. Staff noted that this parkrow configuration could be adapted to on-street parking installation with future redevelopment. The applicants however requested an eight-foot sidewalk and five-foot tree grate pedestrian corridor configuration, but proposed to utilize structural soil as a way to better accommodate street tree growth and thus address the concerns raised by planning staff.

Structural soil is a soil mix made of some combination of crushed stone, clay loam or other soil and a hydro-gel stabilizer that can be compacted for pavement installation while still accommodating root growth. For street trees, its use means that instead of having root growth confined to what amounts to a small planting area within a tree well it can instead continue beneath a much broader area of the sidewalk corridor providing for more robust tree growth, a fuller canopy and a longer tree life. The Commission finds that structural soil has previously been used on projects in Ashland along Lithia Way both at 150 Lithia Way and along the full frontage of the First Place development. The Commission finds that the use of structural soil in combination with the eight-foot sidewalk and five-foot grated tree planting well is an acceptable solution which addresses the concerns raised by staff while also addressing the applicants' concerns over water usage, maintenance, and providing a wider functional corridor to better accommodate pedestrians and bicycles.

The initial application also included the installation of an approximately 80-foot long section of eight-foot wide curbside sidewalk along the south side of the new street, along the frontage of Tax Lot #600 that would have required an Exception because no park row planting strip was proposed. Planning staff had noted that while an Exception was merited to defer the installation of standard sidewalks on the south side of the corridor to avoid conflicts with existing large truck traffic, they did not believe a substandard corridor was merited and instead recommended that sidewalk improvements on the south side either be deferred until Site Review for future buildings or constructed to current standards if installed now. The revised submittals provided at the August 11<sup>th</sup> hearing reflect a city standard sidewalk/parkrow installation in this section, and provide a location to connect a crosswalk to the north side sidewalk.

2.5 The Planning Commission finds that the property line adjustments proposed are intended to create a logical relationship between the new street, the existing development and future development areas. The applicants emphasize that right-of-way acquisition by itself does not partition properties, and the right-of-way acquisition by the city proposed here would result in portions of one parcel split across two sides of the new street without the proposed adjustments. As proposed, these adjustments would place property lines along the centerline of the proposed right-of-way acquisition area, allowing right-of-way to be acquired logically from each parcel and resulting in a logical lot configuration after the right-of-way acquisition by the city. There are no minimum lot sizes or dimensional requirements within the E-1 zone, and all resulting lots are physically configured to support either existing development of the property or its future development after the right-of-way acquisition occurs.

2.6 The Planning Commission finds that the application proposes a new public street crossing of the Hamilton Creek Water Resource Protection Zone, a relocated private driveway crossing of the Hamilton Creek Water Resource Protection Zone, and three reductions in the Water Resource Protection Zone.

The Commission finds that the proposed new street crosses Hamilton Creek in a location where the corridor transitions from an open creek bed to a piped reach flowing within a five-foot corrugated metal pipe and where the stream type transitions from a Local Stream with a 40-foot Water Resource Protection Zone to an Intermittent/Ephemeral Stream with a 30-foot Water Resource Protection Zone. The construction of new public access and utilities within a Water Resource Protection Zone is considered a Limited Use where deemed necessary to maintain a functional system and upon a finding that no other reasonable alternate location exists. The Transportation System Plan (TSP) is noted as a guiding document in this determination, and the Commission finds that the proposed new street crossing in this location is identified in the recently adopted TSP. Private access and utilities are also a limited use where necessary to provide access to an otherwise inaccessible or landlocked parcel where no other reasonable, alternate access exists as is the case with the applicants' proposed relocated crossing at the southeastern portion of the property.

The Commission finds that with the project, the applicants will mitigate the proposed protection zone disturbances by restoring a significant reach of Hamilton Creek resulting in 19,900 square feet of enhancements. This will include approximately 14,551 square feet of mitigation area immediately up and down stream of the proposed crossing. The Commission further finds that the proposed stream crossings will utilize open bottom designs, and goes on to explain that while there are currently barriers to fish passage downstream and immediately upstream, these designs will allow for fish passage in the future.

The Commission finds that the application discusses three reductions in the Water Resources Protection Zone. The first is at the proposed new Independent Way public street crossing of Hamilton Creek (applicants' Impact Area B) and involves a disturbance within the Water Resource Protection Zone of 9,573 square feet. The second area involves the relocation of an existing private driveway crossing (applicants' Impact Area E) at the southeast corner of the site. The application notes that there is already an existing private driveway crossing here, although it is relatively minimal due to the ephemeral nature of the stream. The proposal is to better align the crossing with the existing development on the site, and the application emphasizes that without the reduction to allow the crossing and minimal maneuvering, this corner of the site would be of limited practical utility. The Commission finds that public and private access as proposed here are permissible as Limited Uses, and do not need to be considered separately as protection zone reductions.

The Commission finds that the third reduction requested involves two small areas (applicants' Impact Areas C and D) totaling 124 square feet to allow straight curb lines for parking and maneuvering areas to serve the existing and future development. The Commission finds that these reductions are *de minimus* and necessary for the functioning of existing and future uses of the property and that the impacts have been minimized and will be mitigated.

2.7 The Planning Commission finds that because the application involves disturbance constituting development on Floodplain Corridor Lands, a Physical & Environmental Constraints Review Permit is also necessary. The application notes that Thornton Engineering has created hydraulic modeling for the full length of the stream corridor for all subject properties in conjunction with a Letter of Map Revision in order to reduce the extent of the property considered to be flood plain corridor lands by the Federal Emergency Management Agency (FEMA). Thornton Engineering has indicated that the proposed project can and will be designed in a manner that does not increase base flood elevations, and the applicants have recommended a condition of approval to require a new hydraulic analysis be provided based on the final design of the build-condition project that shows no rise in base flood elevations when compared to the existing conditions hydraulic modeling provided with the application.

2.8 The Planning Commission finds that the application as initially submitted included a Tree Inventory and Tree Removal/Protection Plan which identified a total of 31 trees greater than six-inches in diameter to be removed as part of the request. Of the trees to be removed, ten were proposed for removal to accommodate the construction of the proposed Independent Way and its crossing of Hamilton Creek and 21 were proposed for removal to allow development of the private portion of the project. The application emphasized that these proposed removals were necessary to create a new city street identified as a necessary connection in the city's Transportation System Plan and to prepare the adjacent private property for future development consistent with the E-1 zone. The application materials argued that the removal of these trees would not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks, and also asserted that the project would not have a significant negative impact on the tree densities, sizes, canopies or species diversity within 200 feet of the subject property. A mitigation plan was provided, with 37 trees proposed to be planted along Hamilton Creek to mitigate those proposed to be removed.

After a Planning Commissioner site visit on July 13<sup>th</sup> and the subsequent initial hearing on the project on July 14<sup>th</sup>, Commissioners found that Tree #22, a 30-inch diameter Ponderosa Pine behind the existing residence on the property, and Trees #14 and #15, 25- and 20-inch Oaks near the crossing proposed to access the building pad at the southeast corner of the site merited preservation through the modification of the initial proposal. The applicants subsequently provided a revised grading plan and cross-section drawings clarifying the rough grading proposed, and supporting narrative submittals which noted that the applicants would accept conditions requiring revised plans to reflect the preservation and protection of these three trees (#14, #15 and #22) which the Commission finds merit preservation and protection. .

2.9 The Planning Commission finds that Independent Way will be an important new gateway to those areas likely to see significant local job and housing growth in the future, and that this new street connection will provide for increased functionality of the street system. The Commission finds that first consideration with the proposal is in insuring that the street system ultimately develops not only to support a functionality for motor vehicle circulation, but also in providing for multiple transportation options to create a safe, optimal environment for all users as envisioned in the Street Standards and

Transportation System Plan. The Commission finds that the modifications made by the applicants since the July 14<sup>th</sup> meeting bring the proposal largely into line with the street standards with the exception of the frontage of the existing IPCO Printing building on the south side, and the Commission is supportive of these changes, including the proposed use of structural soil to better accommodate the development of a healthy and robust street tree canopy.

The Commission is also supportive of establishing driveway curb cuts, preliminary rough grading and extending utility connections to the adjacent private property in conjunction with the proposed new street construction with the recognition that further development of these sites will be subject to Site Design Review to consider both the building and site designs in light of standards in place when each building is proposed, and that specific building locations, sizes and broader associated site improvements have not yet been proposed and are not considered or approved with this application. The Commission finds that the establishment of a clear curb line delineating the boundary of the Hamilton Creek corridor, and a *de minimus* reduction in the Water Resource Protection Zone to square off this curb line for the functionality and efficiency of existing and future circulation is appropriate.

The Commission further finds that the creek crossing for the new public street, including the associated removal of ten trees, and the private creek crossing for a new private driveway access to the building pad at the southeast of the site, including the removal of Tree #13, is appropriate but the establishment of on-site parking and circulation on that portion of the property will be better considered with Site Review for development of that building pad. The Commission finds that the applicants have agreed to preserve and protect Trees #14 and #15 that were previously proposed to be removed to accommodate the new driveway here, and their preservation and protection have been incorporated into the conditions below.

### **SECTION 3. DECISION**

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review, Exception to Street Standards, Property Line Adjustment, Limited Use Permit and Water Resource Protection Zone Reduction for Construction in the Water Resource Protection Zone, Physical & Environmental Constraints Review Permit for Floodplain Development, and Tree Removal Permit approvals to allow the construction of a new public street is supported by evidence contained within the whole record.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #2015-00422. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2015-00422 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein, including that the final plans shall be revised to include the preservation and protection of Trees #14, #15 and #22.
2. That a final survey plat shall be submitted within 12 months and approved by the City of

- Ashland within 18 months of this approval. All easements for public and private utilities, fire apparatus access, and reciprocal utility, maintenance, and access shall be indicated on the final survey plat as required by the Ashland Engineering Division.
3. That final engineered street improvement, storm drainage and utility plans for the new Independent Way shall be submitted for the review and approval of the Ashland Engineering and Planning Divisions prior to signature of the final survey plat.
  4. That the final engineered construction drawings for the public sidewalks on Independent Way shall be submitted for review and approval of the Ashland Planning and Engineering Divisions prior to work in the street right-of-way and prior to installation of improvements in the pedestrian corridor. The sidewalk on the north side of Independent Way shall be a minimum of eight feet in width with a required five-foot hardscape parkrow planting strip with tree grates between the sidewalk and the curb. No hardscape parkrow shall be required in the area of the bridge crossing, but full improvements, including but not limited to the sidewalk, parkrows with irrigated street trees, and street lighting shall be required on the remainder of the north side. The final engineered construction drawings shall include details for the installation of structural soil, including tree planting cross-sections, soil mix details, and the extent of the structural soil installation.
  5. That street trees, spaced at one tree per 30 feet of street frontage, shall be installed on the north side of Independent Way. All street trees shall be chosen from the adopted Street Tree List and shall be installed in accordance with the specifications noted in AMC 18.4.4.030.E. All street trees shall be irrigated.
  6. The sidewalk on the south side shall be constructed to six-foot width with no parkrow planting strip within the bridge crossing as proposed by the applicants. Any additional pedestrian corridor improvements installed on the south side shall be to city street standards. The completion of any remaining sections of sidewalk on the south side of Independent Way shall be evaluated with future development applications.
  7. That the final engineered plans for the new Independent Way shall include the applicants' proposed 48-foot on-street parking bay on the south side of Independent Way near its intersection with Tolman Creek Road.
  8. That the applicants shall work with the Rogue Valley Transportation District (RVTD) and neighboring property owners to address relocation of the transit stop on the west side of Tolman Creek Road, if necessitated by the proposed on-street parking removal and lane reconfiguration.
  9. That, as proposed by the applicants, a new hydraulic analysis by the project engineers shall be provided based on the final design of the build-condition project that shows no rise in base flood elevations when compared to the existing conditions hydraulic modeling provided with the application.
  10. That a Tree Verification Permit shall be applied for and approved by the Ashland Planning Division prior to site work including excavation, staging or storage of materials. The Tree Verification Permit is to inspect the identification of the trees to be removed and the installation of tree protection fencing for trees to be retained. The tree protection shall be chain link fencing six feet tall and installed in accordance with the requirements of AMC 18.4.5.030.B. No construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
  11. That silt fencing or other protective measures shall be installed along the Water Resource

- Protection Zone boundary, inspected and approved by the Staff Advisor prior to issuance of excavation permits or any site work, staging or storage of materials on site.
12. That the applicants shall provide a management plan, and any necessary modification to existing conservation easements, providing for the long-term conservation, management and maintenance of the Water Resource Protection Zone as detailed in AMC 18.3.11.110.C prior to the approval of final civil engineering plans.
  13. That a final grading and erosion control plans shall be provided which include details addressing the “Vegetation Preservation and Construction Staging Requirements” found in AMC 18.3.110.A.
  14. That a final size- and species-specific mitigation plan including irrigation details and details of the landscape materials to be planted to buffer the visual impacts of the retaining wall as viewed from Washington Street shall be provided for the review and approval of the Staff Advisor. All mitigation plantings shall be installed according to the approved plan, inspected, and approved by the Staff Advisor, and the management plan and any necessary easement modifications recorded prior to final approval.
  15. That lots not actively being constructed or used for staging shall be treated with a low-water use wildflower seed mix and generally maintained in a weed-free condition.
  16. That no new paving or curbing other than that associated with the street installation, establishing new driveway curb cut locations, and delineating the Water Resources Protection Zone boundary shall be permitted until approved through future Site Design Review applications.
  17. That the applicants’ “Overall Master Site Plan (Sheet A-101)” has not been reviewed for compliance with applicable standards and is not approved here, as recognized by the applicants in the narrative submittals provided. Development of the site shall be subject to full review under the applicable standards at the time each building is proposed.
  18. That any demolition of existing buildings shall be subject to the applicable demolition requirements (*currently found in AMC 15.04.210-.216*) and may require that the applicants obtain Demolition/Relocation Review Permit approval prior to any demolition.

September 8, 2015

Date

---

Planning Commission Approval

**TYPE II  
PUBLIC HEARING**

---

**PA-2015-01370  
210-220 East Hersey St**



**PLANNING ACTION:** 2015-01370

**SUBJECT PROPERTY:** 210-220 East Hersey Street

**OWNER:** The Bernard Family Trust

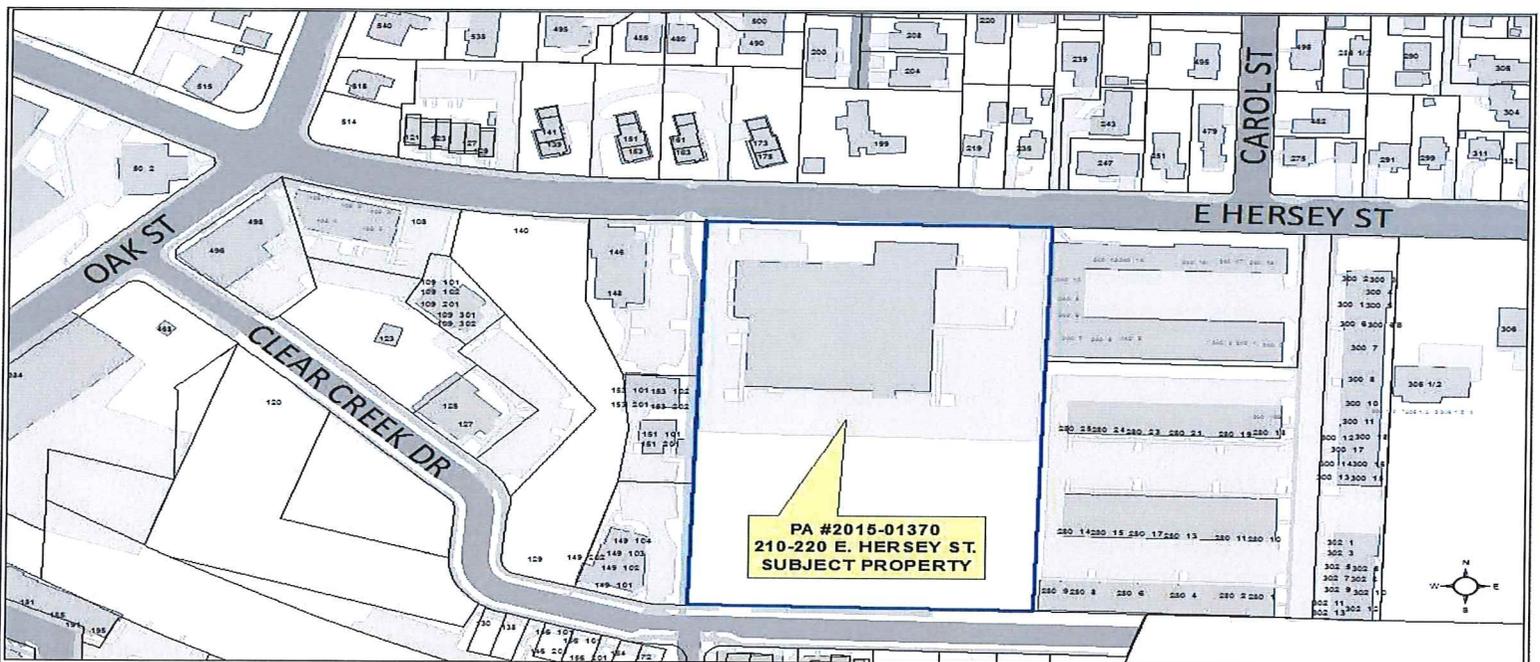
**APPLICANT:** Adroit Construction, as agent for the owners

**DESCRIPTION:** A request for Site Design Review approval to allow the construction of a 24,621 square foot addition behind the existing 39,962 square foot Darex factory located at 210-220 East Hersey Street. (A second phase consisting of an 11,107 square foot stand-alone building along Clear Creek Drive will be reviewed separately at a later date.) Also included is a request for Tree Removal Permits to remove two trees six-inches or more in diameter at breast height: a six-inch Maple tree and a six-inch Pear tree.

**COMPREHENSIVE PLAN DESIGNATION:** Employment; **ZONING:** E-1; **ASSESSOR'S MAP:** 39 1E 04CD; **TAX LOTS:** 2000

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **Thursday, September 3, 2015 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** *September 8, 2015 at 7:00 PM, Ashland Civic Center, 1175 East Main Street*



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### 18.5.2.050 Approval Criteria

The following criteria shall be used to approve or deny an application:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

## TREE REMOVAL PERMIT FROM THE UNIFIED LAND USE ORDINANCE

### 18.5.7.040.B Criteria for Issuance of Tree Removal Permit

#### B. Tree Removal Permit.

- 1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
- 2. **Tree That is Not a Hazard.** A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - 1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
  - 2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  - 3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  - 4. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  - 5. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

# ASHLAND PLANNING DIVISION STAFF REPORT

**September 8, 2015**

**PLANNING ACTION:** #2015-01370

**OWNERS:** The Bernard Family Trust

**APPLICANTS:** Adroit Construction, *as agent for the owners*

**LOCATION:** 210-220 East Hersey Street

**ZONE DESIGNATION:** E-1

**COMP. PLAN DESIGNATION:** Employment

**APPLICATION DEEMED COMPLETE:** August 31, 2015

**120-DAY TIME LIMIT:** December 29, 2015

**ORDINANCE REFERENCE** ( see <http://www.ashland.or.us/comdevdocs> to view land use code on-line):

18.2.6 Standards for Non-Residential Zones  
18.4.3 Parking, Access, and Circulation  
18.4.5 Tree Preservation & Protection  
18.4.6 Public Facilities  
18.4.7 Signs  
18.5.2 Site Design Review  
18.5.7 Tree Removal Permits

**REQUEST:** A request for Site Design Review approval to allow the construction of a 24,621 square foot addition behind the existing 39,962 square foot Darex factory located at 210-220 East Hersey Street. (A second phase consisting of an 11,107 square foot stand-alone building along Clear Creek Drive will be reviewed separately at a later date.) Also included is a request for Tree Removal Permits to remove two trees six-inches or more in diameter at breast height: a six-inch Maple tree and a six-inch Pear tree.

## **I. Relevant Facts**

### **A. Background - History of Application**

Planning Action #2001-00116 was a proposal for Site Review approval for a phased office and industrial development along the subject property's Clear Creek Drive frontage. This application was submitted, but ultimately appears to have been withdrawn as there is no indication that it was approved, and it was never built.

Planning Action #1995-00035, a request for Site Review approval for a four-phase

expansion of the Darex building, was administratively approved in April of 1995. This request included a 20,000 square foot production warehouse expansion as a first phase, an 8,000 square foot office expansion as a second phase, a 20,000 square foot production warehouse expansion as a third phase, and a final fourth phase that included an additional 20,000 square feet of production warehouse space. The later phases of this project were never completed.

Planning Action #1979-00017, a request for a Site Review permit to allow the construction of the original Darex building, was approved by the Planning Commission in February of 1979.

There are no other planning actions of record for this property.

## **B. Detailed Description of the Site and Proposal**

### **The Site**

The subject property is an approximately 4.82 acre rectangular parcel bounded on the north by East Hersey Street and on the south by Clear Creek Drive. The property currently contains a 39,962 square foot factory building. The property slopes down to the north at approximately two percent over its full length, but there are areas with slopes approaching 25 percent adjacent to Hersey Street and mid-lot behind the existing building. There are some trees and landscaping near the existing building, but the property lacks any significant natural features.

Paved access to the property is primarily from Hersey Street, which is considered an Avenue in Ashland's Transportation System Plan (TSP). Hersey Street is currently paved to a width of approximately 45 feet within the 60-foot right-of-way along the property's frontage. In addition to the motor vehicle travel and bike lanes, there are curbs and gutters but no sidewalks or parkrows along the south side. There is a steep, rock-covered embankment between the curb and the applicants' property and there is currently no pedestrian access from Hersey Street other than by using the driveway.

The property also fronts on Clear Creek Drive to the south, a Commercial Neighborhood Collector, which is currently paved to an approximate 30-foot width within a 60-foot right-of-way with curbs and gutters in place. A sidewalk and parkrow planting strip extend approximately 270 feet from the property's west boundary, leaving approximately 110 feet with no existing sidewalks. There are currently no street trees in place within the existing parkrow planting strip.

### **The Proposal**

#### **Site Design Review Permit Proposal**

The application involves a request for Site Design Review approval to allow the construction of a 24,621 square foot addition behind the existing 39,962 square foot Darex factory located at 210-220 East Hersey Street. This additional space would accommodate additional area for administration and assembly employees, and would be accompanied by the installation of 62 additional parking spaces for customer and employee parking, along with associated landscaping and a new stairway connection from the building's main entry to Hersey Street. The proposed addition would consist of

a single-story metal building with a metal roof, approximately 24 feet high, and matching the southern portion of the existing building in color and materials. The application suggests that this addition would ultimately accommodate an increase in employees from the current 129 to an anticipated 226.

Typically, a warehouse or industrial building would be considered under the parking ratios in place for “*industrial, manufacturing and production, warehousing and freight*” found in AMC 18.4.3.040 which require that one parking space be provided for each 1,000 square feet of building area or for each two employees, whichever is less, plus one for each company vehicle. The applicants here suggest that warehouse and industrial use categories do not properly reflect the number of people required for the more intense, hand-assembly process employed at Darex. They note that the typical workstations are similar in size, at about 100 square feet each, to the workstations typically found in an office environment, and the number of employees (currently 129) is also similar to that of a similar-sized office building. They therefore propose to use the office parking ratio of one parking space per 500 square feet of building area to better align with the unique hand assembly work performed at Darex.

The application also identifies a building pad along Clear Creek Drive which would accommodate a second phase of development, consisting of an 11,107 square foot stand-alone building which they hope to develop in approximately ten years. Approval of this building is not requested at this time, and the second phase would be considered under the standards in place at the time it is ultimately proposed. The applicants propose two alternatives for the treatment of the Clear Creek Drive frontage until the second phase of development ultimately occurs. Under their first alternative, they would improve this frontage with a neighborhood park, which would be under private ownership and management but open to the public, and would concurrently install the 22 parking spaces associated with development of their anticipated Phase II building. This park would feature a variety of ornamental trees in raised planters, a low-water use lawn area and a series of paths. If the park and associated parking area are not approved, the applicants propose an alternate design with a low-water use mowable field area that would include irrigation and the planting of ten shade trees.

### **Tree Removal Permit Proposal**

The application also involves the removal of three trees. Within the E-1 zoning district, any removal of trees six-inches in diameter at breast height (d.b.h.) or more requires a Tree Removal Permit. In this case two of the three trees to be removed meet this threshold: a six-inch d.b.h. Maple tree (#) and a six-inch d.b.h. Pear tree (#). The third tree to be removed is a 4 ½ inch d.b.h. Pear tree located; its removal does not require a permit.

## **II. Project Impact**

As explained more fully above, the application consists of Site Design Review and Tree Removal permit approval requests. Within the E-1 zoning district, new buildings or additions greater than 10,000 square feet are subject to a “Type II” Site Design Review application procedure which requires a decision by the Planning Commission through a public hearing. Because the application also includes the removal of two trees of six-inches or more in diameter at breast height, Tree Removal Permit approval is also

required.

## **A. Site Design Review Proposal**

### **Underlying Zone Requirements**

The first approval criterion for Site Design Review is that, “*The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*” The subject property’s underlying zone is E-1 (Employment) and within that zone, there is no minimum lot area, width, or depth; no minimum front, side or rear yard area except where abutting a residential zone to the side or rear; no maximum lot coverage; and no minimum residential density. The property does not abut residential zones to the side or rear, and is not located on an arterial street, and as such no setback requirements come into play. The maximum building height is limited to 40 feet, and the proposed 24 foot height complies with this limit.

### **Overlay Zone Requirements**

The second Site Design Review approval criterion is that, “*The proposal complies with applicable overlay zone requirements (part 18.3).*” Approximately the northern 320 feet of the property falls within the Residential Overlay zone, and no other overlay zones apply. The requirements of the Residential Overlay are only triggered when residential uses are proposed, and in this instance there is no residential component to the request.

### **Site Development and Design Standards**

The third approval criterion is that, “*The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*” Generally, the Site Development & Design Standards seek to improve each project’s appearance; to create a positive, human scale relationship between proposed buildings and the streetscape which encourages bicycle and pedestrian travel; to lessen the visual and climatic impacts of parking; and to screen adjacent uses from adverse impacts of development. To these ends, buildings are to have their primary orientation to the street rather than to parking areas, with visible, functional and attractive entrances oriented to the street, placed within 20 feet of the street, and accessed directly from the public sidewalk. Sidewalks and street trees are to be provided along subject properties’ frontages, and automobile parking and circulation areas are not to be placed between buildings and the street.

In responding to the design standards, the application notes that Phase I will use the existing driveway access from Hersey Street as well as the existing curb cut on Clear Creek Drive for vehicular access to the site. Parking is to be placed on each side of the addition, which is behind the face of the main building’s Hersey Street façade. The application emphasizes that with the placement of the Phase I addition behind the existing building, there will be no change in the orientation of the existing building to the street or its sense of entry, although a new stairway access is proposed to provide direct pedestrian access from the entry to Hersey Street, which has previously been lacking.

The Phase II building, which is not currently proposed or under review here, is noted as being oriented to Clear Creek Drive, with its wider side to the street and parking to one

side. This building and its associated site improvements will need to be reviewed under the applicable standards in the place at the time it is proposed.

With the placement of the proposed addition and its associated site improvements behind the existing building along Hersey Street, and development along the property's Clear Creek Drive frontage deferred until Phase II, the design standards with regard to the streetscape and building design have limited applicability to the request. For staff, the key issues with the proposal in terms of the Site Development and Design Standards in AMC 18.4 come down to verifying that adequate parking is to be provided and to ensuring that the parking proposes complies with the parking area design, parking lot landscaping and screening, and pedestrian access and circulation standards.

### **Required Parking**

Typically, a warehouse or industrial building would be considered under the parking ratios in place for "*industrial, manufacturing and production, warehousing and freight*" found in AMC 18.4.3.040 which require that one parking space be provided for each 1,000 square feet of building area or for each two employees, whichever is less, plus one for each company vehicle. This would require 65 parking spaces to serve the existing building and proposed addition, and because parking provided cannot exceed parking required by more than ten percent under city codes, not more than 71 parking spaces could be provided. The applicants here argue that the warehouse and industrial use categories do not properly reflect the number of people required for the more intense, hand-assembly process employed at Darex, and propose that parking be considered instead in terms of the parking ratio for office use, which is one parking space per 500 square feet of floor area. This would allow a minimum parking requirement of 129 parking spaces, and no more than 142 spaces could be provided. The applicants propose to provide 141 spaces to serve the existing use and proposed addition, but also propose to provide an additional 22 parking spaces to serve the future second phase and a quasi-public park area to be made available until that second phase occurs.

The applicants suggest that the current facility and site were designed according to the much lower warehouse parking ratio, which is out of sync with the actual business being conducted. They note that as the business has increased staffing over the years, undeveloped land at the rear of the property has become overflow parking for staff. They further note that while the ordinance tries to take advantage of on-street parking credits to reduce the need for off-street parking, the long, narrow, dual frontage nature of the site does not provide on-street parking in quantities sufficient to accommodate their staffing. The applicants also note that the limited off-street parking associated with recent Clear Creek Drive development has left on-street parking in very high demand, and this means that employee parking can spill across Hersey Street and adversely affect residential neighborhoods to the north.

The applicants explain that their business is based on assembly by hand of numerous individual components for their wide variety of products, and that they employ residents from all over the county (including Grants Pass, White City, Central Point, Medford, Phoenix and Talent as well as Ashland) with living wage jobs. They emphasize that the currently available transit service would require a commute of up to four hours for those living in the northern part of Medford and White City, and that commuting by bicycle is not viable for these employees either, particularly in inclement weather. They note that

over the years they have tried incentives to encourage car-pooling among staff, but that this has met with little success when employees often have obligations before and after work with daycare, medical appointments or volunteering commitments. They suggest that for many employees, individual automobiles are the only viable commuting option.

The Ashland Municipal Code provides that where automobile parking requirements for any use are not specifically listed, such requirements shall be determined based upon the most comparable use specified in this section, and other available data. Here the applicants note that their typical workstations are similar in size (at about 100 square feet each) to the workstations found in an office environment, and the number of employees is also similar to that of a similar-sized office building. They further emphasize that their business model is extremely labor intensive and relies on a long-term, skilled labor force which currently employs 129 people and which will grow to 139 people for the period of August through November at this site. They suggest that the first phase of expansion proposed here is intended to accommodate an increase in employees to 226. They propose to use the office parking ratio of one parking space per 500 square feet of building area to align with the unique hand assembly work performed at Darex, and note that if the property were to sell, it would most likely continue with similar uses involving clean technologies, intensive hand assembly or office use and the requested parking ratio would match the parking needs of these uses and facilitate the redevelopment of the site.

In considering the requested parking ratio, staff reviewed the Institute of Transportation Engineers (ITE) Parking Generation Manual, 3<sup>rd</sup> Edition which includes analysis of a number of study sites for each categorized use. Staff noted the following:

- **General Light Industrial (ITE Code 110)** – These are typically free standing facilities devoted to a single use other than manufacturing, with little or no office component to the use. Typical activities include printing, material testing and the assembly of data processing equipment. The average parking supply ratios were 1.1 parking spaces per 1,000 square feet of floor area (for five of nine sites reviewed) and 1.3 spaces per employee (for four of the sites), and these sites averaged an employment density of 1,200 square feet of gross floor area per employee.
- **Industrial Park (ITE Code 130)** – These are typically sites containing a number of industrial uses and related facilities involving a diverse mix of manufacturing, services and warehouses. Here the average parking supply ratios were 1.6 parking spaces per 1,000 square feet of floor area and 1.2 spaces per employee, and the average employee density was 900 square feet per employee.
- **Manufacturing (ITE Code 140)** - These are typically sites where raw materials or parts are converted to finished products. The average parking supply ratios were 1.3 parking spaces per 1,000 square feet of floor area and 1.3 spaces per employee, and the average employee density was 1, 000 square feet per employee.
- **Office (ITE Code 701)** – General office has several subcategories in the ITE analysis, including General Office (710), Corporate Headquarters (714), Single Tenant Office Building (715), Office Park (750) and Research and Development Center (760). Analysis across these subcategories had the average parking supply ratios were 4.0 parking

spaces per 1,000 square feet of floor area and 1.1 spaces per employee, and the average employee density was 303 square feet per employee.

Staff would concur with the applicants that on-street parking in the area is in high demand, and that determining the correct parking ratio to accommodate parking on site is important to minimize the impact of parking to the adjacent streetscape and to residential neighborhoods to the north. As proposed with 226 employees and 141 parking spaces, the applicants operation would have 2.19 parking spaces per 1,000 square feet, 0.62 parking spaces per employee, and an average employee density of approximately 285 square feet per employee. With the current 129 employees, these numbers would be 2.19 spaces per 1,000 square feet, 1.09 spaces per employee, and an average employee density of 500 square feet per employee. In staff's assessment, the square footage per employee at Darex clearly supports their argument that the business is more labor intensive than anticipated by the "*industrial, manufacturing and production, warehousing and freight*" parking ratio in the municipal code. In addition, staff would note that Darex currently has 26.76 employees per acre, and with the increase to 226 employees that would be accommodated by the addition this would increase to 46.88 employees per acre. The most recent Economic Opportunities Analysis for the city had Employment zones city wide averaging approximately 17 employees per acre. In staff's view, this further supports the argument that the Darex business model supports more employees than the average E-1 business in Ashland and merits consideration under the parking ratio for office as the use most comparable to that proposed.

In addition to the 141 spaces proposed, the applicants have proposed two alternatives for the treatment of the Clear Creek Drive frontage until the second phase of development ultimately occurs. Under their first and preferred alternative, they would improve this frontage with a neighborhood park, which would be under private ownership and management but open to the public, and would concurrently install the 22 parking spaces that they anticipate would be necessary with the development of the Phase II building. This park would feature a variety of ornamental trees in raised planters, a low-water use lawn area and a series of paths. If the park and associated parking are not approved, the applicants propose an alternate design with a low-water use field area that would include irrigation and the planting of ten shade trees.

The 141 spaces proposed is one space below the maximum allowed for the current proposal based on the requested office parking ratio. Approval of an additional 22 parking spaces would require the Planning Commission to either approve a Variance to the Maximum Number of Off-Street Automobile Parking Spaces in AMC 18.4.3.030.B (which has not been requested here) or to find that the additional parking was necessary to serve the park. The Parking Generation Manual has limited information relative to the demand for parks with their analysis limited to one site in Santa Barbara, California where 15 parking spaces per acre were provided and only a 5.1 space per acre peak demand was observed. Generally, in Ashland, neighborhood parks have been required to provide limited parking as they are planned to serve an area that is by intention within walking distance. Staff does not see a strong argument which would support the additional 22 spaces proposed to accommodate demand for a future building or in association with the quasi-public park area. However the national standards for parking generation for office uses detailed above reflect a substantial range of parking demand, and the commission could determine that the total number of parking spaces necessary

for the proposed addition includes these additional 22 spaces based on the employee density the applicants have described.

### **Parking Lot Design**

The application explains that the parking spaces to be provided are all full-size spaces, at nine feet by 18 feet with no compact spaces proposed, and that the back-up and circulation areas proposed will meet or exceed the required 22-foot dimension. The application further notes that the parking areas are separated by buildings, tree wells with parking lot trees, and split level parking areas with sidewalks, and that the parking lot design seeks to provide at least 50 percent shade from tree canopy over the parking area surface within five years of project completion in keeping with the requirements of AMC 18.4.3.080.B.5 which seek to minimize the adverse environmental and microclimatic impacts of surface parking. A drainage swale is located in the parking area to the west of the addition, within the planting strip between parking spaces. Drainage in the easterly parking area is to be accommodated with filtered treatment. All stormwater is to be detained on-site so as not to flow beyond the property lines. The parking area design requirements call for parking lots and other hard surface areas to be designed to capture and treat run-off with landscaped medians and swales, and the Commission may determine that the proposed detention system design in the eastern parking area needs to better incorporate a landscaped median or swale to capture and treat run-off.

The application suggests that continuous walkways are provided through the parking areas to connect to all existing and future buildings, and provide safe, direct and convenient connections from the building entries to the streets, sidewalks and proposed park areas. The walkways are also noted as being protected by planting strips, five feet in width, curbed except within crosswalks, with pedestrian lighting and marked in painted asphalt or concrete to differentiate them from the surrounding parking area. Parking areas are noted as generally grouped in areas of less than 50 spaces so pedestrians must traverse less than a 150 foot distance within the parking area, and well-distributed accessible parking. Pedestrian circulation is more clearly addressed in the western parking areas, however it appears that the number of potential entrances on the east side of the new building substantially reduce the distance a pedestrian would likely need to travel. Staff has recommended a condition to require that a pedestrian crossing through the parking area near the southeast corner of the existing building.

### **Public Facilities**

The fourth approval criterion for Site Design Review approval is that, *“The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.”*

The application materials provided note that the subject property is presently served by water, sewer, electricity, urban storm drainage, and paved access, and that existing facilities have adequate capacity to accommodate the proposed addition. They further note the following:

- **Water & Sewer** – The application materials note that with the proposed addition, four additional toilets, two lavatories and two drinking fountains will be added.

The applicants further explain that they have been in contact with the city's Water and Wastewater Division and both have confirmed that the existing 12-inch public water line in the Hersey Street right-of-way and eight-inch public sewer lines in the Hersey Street and Clear Creek Drive rights-of-way are sufficient to accommodate the additional fixtures proposed. The applicants indicate that they will continue to confer with the city utilities and Public Works and Engineering Department staff to verify capacity and identify any necessary upgrades as their designs are finalized.

A condition of approval has been recommended below to require that the applicants provide final utility plans for the review and approval of the Planning, Building, Public Works and Engineering Departments in conjunction with their building permit application.

- **Electricity** – The application materials note that the electrical contractor for the project has performed preliminary load calculations estimating the proposed additional demand associated with the first phase addition will be approximately 147,000 watts or 408 amps at 120/208 three phase. The applicants explain that the existing 2000 amp service size and 750 KVA 120/208 three-phase city transformer have sufficient capacity to serve the proposed addition.

A condition has been recommended below to require that the applicants provide a final electric service plan for the review and approval of the Planning, Building and Electric Departments in conjunction with their building permit application. The applicants have also been advised to contact the Electric and Conservation Departments as early in the process as possible if they are considering additional solar energy installations on the new addition to identify any financial incentives or technical assistance that may be available.

- **Urban Storm Drainage** – The application materials explain that all new improvements including the building, parking areas and sidewalks were designed with the Rogue Valley Stormwater Quality Design Manual to address both the quality and quantity of stormwater run-off and comply with city storm drainage requirements. The applicants note that their civil engineer has been in contact with the city's Engineering Department and confirmed that the applicants' stormwater detention strategy which proposes an eastern detention swale and additional asphalt detention areas as well will comply with city requirements. The application further explains that the stormwater will be treated for water quality and detained so that post-development flows do not exceed pre-development flows for the property in its undeveloped state and will thus have no adverse impact on downstream infrastructure.

A condition has been recommended below to require that a final stormwater drainage and erosion control plan be provided for the review and approval of the Planning, Building and Engineering Departments in conjunction with the building permit application.

- **Paved Access & Adequate Transportation** – Paved access to the property is primarily from Hersey Street, which is considered an Avenue in Ashland's Transportation System Plan (TSP). Hersey Street is currently paved to a width of approximately 45 feet within the 60-foot right-of-way along the property's frontage. In addition to the motor vehicle travel lanes, a bike lane is in place and there are curbs and gutters but no sidewalks or parkrows along the south side. There is a steep, rock-covered embankment between the curb and the applicants' property and there is currently no pedestrian access from Hersey Street other than by using the driveway.

The property also fronts on Clear Creek Drive to the south, a Commercial Neighborhood Collector, which is currently paved to an approximate 30-foot width within a 60-foot right-of-way with curbs and gutters in place. A sidewalk and parkrow planting strip extend approximately 270 feet from the property's west boundary, leaving approximately 110 feet with no existing sidewalks. There are currently no street trees in place within the existing parkrow planting strip.

The applicants have not proposed to install sidewalks along Hersey Street. In staff's view, this section of Hersey Street could be found to merit an Exception to Street Standards in that the narrow area between the curb and the applicants' property has slopes which vary from approximately 30 percent to more than 45 percent, and which would require substantial cuts and retaining walls, disturbance of existing established trees, and the potential to disrupt the established development on the lot which is only 12 to 24 feet behind the curb. The applicants have proposed to construct a stairway connection from the street which would provide direct pedestrian access from the on-street parking spaces along Hersey Street to the building entrance. Staff have included a condition of approval recommending that this stairway installation be a condition of approval, and a condition that the applicants be required to sign in favor of future Hersey Street improvements which could include a comprehensively planned sidewalk installation taking into account the slopes along the property's frontage and the necessary transition to less sloped sections along adjacent properties.

The applicants' proposal illustrates the completion of the remaining sidewalks on their Clear Creek Drive frontage, the planting of new street trees within the park row planting strip, and the replacement of two existing driveway aprons installed

with the original construction of Clear Creek Drive. A condition requiring this sidewalk installation is recommended below.

The application materials provided include a letter from James R. Hanks, P.E. of JRH Transportation Engineering. JRH analyzed the proposal and determined based on the thresholds established in city standards, no traffic impact analysis (TIA) is required. The letter explains that based on the anticipated 226 employees that could be accommodated on site with the addition, both the A.M. and P.M. peak hour trips are less than the 50 needed to trigger a TIA, that no traffic control device or geometric improvements are to be installed with the request, and anticipates no newly generated heavy vehicle trips which would require a TIA.

There is currently no transit service on either Hersey Street or Clear Creek Drive, but there is transit service on Lithia Way less than a half-mile walk from the subject property. The Transportation System Plan anticipates that long term modifications of the Rogue Valley Transportation District's Route 10, or a new express route, might ultimately provide transit service along Clear Creek Drive.

## **B. Tree Removal Permit**

The application includes a Tree Protection Plan (Sheet L1.1) identifying five existing trees within the vicinity of the proposed construction, two of which are six-inches in diameter at breast height or greater and thus regulated within the zone.

The application proposes the removal of three trees, two of which are six-inches in diameter or breast height or greater and thus require Tree Removal Permits. The two trees to be removed are: a six-inch d.b.h. Maple tree (#2) which is in the area of the demolition to accommodate the proposed site improvements and a six-inch d.b.h. Pear tree (#3) which is located in the path of the main irrigation line to serve the new landscaping for the site.

The third tree to be removed is a 4½-inch d.b.h. Pear tree (#1) located to the east of the existing building. Its removal does not require a permit. Tree protection fencing is identified for the two existing trees to remain, both Armstrong Maples, although their size means that they would not otherwise be regulated.

The materials provided note that the trees are in fair to good condition, relatively young and planted in constrained, paved areas. They are proposed to be removed because they are within proposed circulation areas necessary to accommodate pedestrian and vehicular traffic with the addition. The applicants emphasize that the 57 proposed 1½- to two-inch caliper trees and associated landscape plantings in their proposed landscape plan will provide better habitat, and more than make up for the lost canopy coverage and species diversity with the removals. They assert that the removals will allow for proper design of the parking and circulation areas according to applicable standards, and will have no effect on erosion, soil stability, flow of surface waters, protection of adjacent trees or existing windbreaks because the existing trees are within constrained, paved areas.

### III. Procedural - Required Burden of Proof

The criteria for Site Design Review approval are described in 18.5.2.050 as follows:

The following criteria shall be used to approve or deny an application:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

The criteria for an Exception to Street Standards are described in AMC 18.4.6.020.B.1 as follows:

- a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.
  - i. For transit facilities and related improvements, access, wait time, and ride experience.
  - ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.
  - iii. For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.
- c. The exception is the minimum necessary to alleviate the difficulty.
- d. The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.

**The criteria for a Tree Removal Permit to remove a “Tree That is Not a Hazard” are described in AMC 18.5.7.040.B.2 as follows:**

A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
4. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
5. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

#### **IV. Conclusions and Recommendations**

The application requests Site Design Review approval for a 24,621 square foot addition to the existing 39,962 square foot Darex building at 210-220 East Hersey Street and the associated removal of two trees, a Pear and a Maple, located within proposed circulation areas. The additional area is for administration and assembly employees and would ultimately accommodate an increase in employees from the current 129 to an anticipated 226. Given that the proposed addition and associated site improvements are behind the existing building along Hersey Street, and development along the property’s Clear Creek Drive frontage is to be deferred until a later Phase II, the design standards with regard to the streetscape and building design have limited applicability to the request. For staff, the key issue with the proposal comes down determining that the right amount of parking is to be provided.

The addition is to be accompanied by the installation of 62 additional parking spaces for customer and employee parking, along with associated landscaping and a new stairway connection from the building’s main entry to Hersey Street. A typical warehouse or industrial building would be considered under the “*industrial, manufacturing and production, warehousing and freight*” ratio for required parking, but the applicants here suggest that warehouse and industrial use categories do not accurately reflect the number of employees needed for the more intense, hand-assembly process at Darex. They argue

that the space per employee and number of employees is more akin to office use, and propose to use the office parking ratio of one parking space per 500 square feet of building area to better align with the unique hand assembly work performed at Darex.

The applicants suggest, and staff would concur, that on-street parking in the area is in high demand, and determining the correct parking ratio to accommodate parking on site is important in providing adequate parking on site to minimize the impact of development on the adjacent streetscape and residential neighborhoods to the north. In staff's assessment, the square footage per employee at Darex, which is has three to four times the employees in the same space as in more typical industrial and manufacturing uses, supports the argument that the business is more labor intensive than anticipated in the ITE's Parking Generation manual that underlies the parking ratios in the municipal code. In addition, Darex currently has roughly 27 employees per acre, and with the increase proposed here they seek to increase that to nearly 47 employees per acre. The most recent Economic Opportunities Analysis for the city had Employment zones city wide averaging approximately 17 employees per acre. In staff's view, the available information supports the argument that the Darex business model results in more employee density than the average E-1 business in Ashland and merits consideration under the parking ratio for office as the most comparable use in terms of required parking.

The application identifies a building pad along Clear Creek Drive which would accommodate a second phase of development to consist of an 11,107 square foot stand-alone building which they hope to develop within ten years. Approval of this building is not requested at this time, and the second phase would be considered under the standards in place at the time it is ultimately proposed. The applicants propose two alternatives for the interim treatment of the Clear Creek Drive frontage until the second phase of development occurs. Under their first alternative, they would improve this frontage with a neighborhood park, which would be under private ownership and management but open to the public, and would concurrently install the 22 parking spaces anticipated to be needed for the Phase II building. This park would feature a variety of ornamental trees in raised planters, a low-water use lawn area and a series of paths. If the park and associated parking area are not approved, the applicants propose to simply create a low-water use field area incorporating ten shade trees.

The 141 parking spaces proposed is one space below the maximum allowed for the Phase I proposal at the requested "General Office" parking ratio, and the approval of an additional 22 parking spaces would require the Planning Commission to either approve a Variance to the Maximum Number of Off-Street Automobile Parking Spaces in AMC 18.4.3.030.B, which has not been requested, or to find that the additional parking was necessary to serve the proposed addition and park. Neighborhood parks in Ashland have been required to provide limited parking as they are typically planned to serve an area that is by design within walking distance, and staff does not see a strong argument to support an additional 22 spaces proposed with the quasi-public park area proposed here.

\* \* \* \*

A family-owned business in Ashland since 1978, Darex moved to Ashland and began in its current location with 15 employees in a 5,000 square foot workspace and has grown with Ashland to receive national recognition and repeatedly be named one of the top workplaces in Oregon. In a previous application, it was noted that the applicants believe Darex is the type of business that is compatible with Ashland, a *“quiet, non-polluting company that asks very little of the town, but gives much back in good wages, taxes and community support.”* Staff believes that Ashland is fortunate to have Darex Corporation as a part of the community, and we are pleased to see their continued commitment to remain and grow in Ashland. Staff is generally very supportive of this request, and would recommend approval with the addition of the conditions detailed below:

- 1) That all proposals of the applicant shall be conditions of approval unless otherwise specifically modified herein.
- 2) That the plans submitted for the building permit shall be in conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this Site Design Review approval shall be submitted and approved prior to the issuance of a building permit.
- 3) That all recommendations of the Ashland Tree Commission from their September 3, 2015 meeting, where consistent with the applicable ordinances and standards and with final approval of the Staff Advisor, shall be conditions of approval unless otherwise modified herein.
- 4) That prior to the installation of any signage, a sign permit shall be obtained. All signage shall meet the requirements of the Sign Ordinance (AMC 18.4.7).
- 5) That the engineered construction drawings for the public sidewalk along Clear Creek Drive shall be submitted for review and approval of the Ashland Planning and Engineering Departments prior to work in the street right-of-way or approval of building permits. Sidewalk installation and driveway approach repair shall be permitted through the Engineering Division and completed according to city standards. Frontage improvements, including but not limited to the sidewalk, street trees, and street lighting, shall be completed across the entire frontage of the site. The sidewalk shall be constructed to City of Ashland Street Standards, and if necessary for alignment of frontage improvements, additional area for street improvements shall be dedicated as public street right of-way.
- 6) That the engineered construction drawings for the stairs to provide a connection from the existing building entrance to East Hersey Street shall be submitted for review and approval of the Ashland Planning, Building and Engineering Departments prior to work in the street right-of-way or approval of building permits. Stairway installation within the right-of-way shall be permitted through the Engineering Division.

- 7) That the parking requirement for the proposal shall be based on the office parking ratio of one parking space per 500 square feet of gross floor area, and parking provided on the subject property shall not exceed the 141 spaces proposed.
- 8) That building permit submittals shall include:
  - a) The identification of all easements, including but not limited to public and private utility easements and fire apparatus access easements.
  - b) The identification of exterior building materials and paint colors for the review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with the requirements of the Site Design and Use Standards, and the colors and materials selected shall be consistent with those approved with the application.
  - c) Specifications for all exterior lighting fixtures. Exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
  - d) Revised Landscape, Irrigation and Tree Protection Plans shall be provided for the review and approval of the Staff Advisor with the building permit submittals. This plan shall address: 1) the recommendations of the Tree Commission from their September 3, 2015; and 2) the required irrigation plans, including the requirements for programmable automatic timer controllers and a maintenance watering schedule with seasonal modifications, including the Clear Creek Drive park row planting strip. The applicants shall also obtain the required plumbing permits and inspections for installation of the required double-check valve(s) associated with the irrigation system.
  - e) That a revised stormwater drainage plan, including any necessary on-site detention measures, shall be provided for the review and approval of the Engineering, Building and Planning Departments with the building permit submittal. The drainage plan shall be designed to ensure that post-development peak stormwater flows are less than or equal pre-development levels as required by the Engineering Division.
  - f) That a final utility plan for the project shall be provided for the review and approval of the Engineering, Planning and Building Divisions. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins. Any necessary service upgrades shall be completed by the applicant at applicant's expense. Cabinets, vaults, meters and Fire Department connections shall be located in areas least visible from streets, sidewalks and pedestrian areas, while considering access needs.
  - g) The applicant shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary

services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric, Engineering, Building and Planning Departments prior to the issuance of excavation or building permits. Transformers, cabinets and vaults shall be located in areas least visible from streets, sidewalks and pedestrian areas, while considering the access needs of the Electric Department.

- h) Solar setback calculations demonstrating that all new construction complies with Solar Setback Standard B in the formula  $[(\text{Height} - 16) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$  and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and the height(s) from natural grade.
  - i) Revised plans identifying an additional pedestrian crossing through the eastern parking area near the southeast corner of the existing building and incorporating the landscaped medians in the eastern parking area as swales in the on-site detention system.
- 9) That prior to the issuance of the building or excavation permits or the commencement of site work or storage of materials:
- a) A Tree Verification Permit shall be obtained. Trees to be removed shall be marked, and tree protection measures installed according to the approved plan for any trees to be retained, inspected and approved by Staff Advisor. The Verification Permit is to inspect the identification of trees to be removed and the installation of tree protection fencing for the trees to be retained and protected on and adjacent to the site. Tree protection measures shall be in the form of chain link fencing six feet tall, installed and maintained in accordance with the requirements of AMC 18.4.5.030.C.
  - b) That the property owner shall sign in favor of Local Improvement District (LID) for the future street improvements, including but not limited to paving, curb gutter, storm drainage, sidewalks and undergrounding of utilities for East Hersey Street prior to the issuance of a building permit. Nothing in this condition is intended to prohibit an owner/developer, their successors or assigns from exercising their rights to freedom of speech and expression by orally objecting or participating in the LID hearing or to take advantage of any protection afforded any party by City ordinances and resolutions.
- 10) That prior to the final approval of the project and issuance of a certificate of occupancy:
- a) That all hardscaping including the Clear Creek Drive sidewalk and the stairway connection to East Hersey Street, landscaping and the irrigation system shall be installed according to the approved plan, inspected, and approved by the Staff Advisor.

- b) All utility service and equipment installations shall be completed according to Electric, Engineering, Planning, and Building Departments' specifications, inspected and approved by the Staff Advisor.
- c) The screening for the trash and recycling enclosure shall be installed in accordance with the approved plan, inspected and approved by the Staff Advisor. An opportunity to recycle site of equal or greater size than the solid waste receptacle shall be identified in the building permit submittals and shall be in place, inspected and approved by the Staff Advisor.
- d) The requirements of the Ashland Fire Department relative to fire apparatus access, including approach and easements; fire flow; fire alarm and sprinkler systems; fire department connection (FDC); fire hydrants; fire extinguishers; key box; approved addressing; approved gates and fences; waste and recycling container location; storage requirements and fire safety requirements during construction shall be satisfactorily addressed.
- e) Clear Creek Drive frontage improvements including but not limited to the installation of sidewalks, street trees with irrigation and street lighting shall be installed to City of Ashland standards under permit from the Public Works Department in accordance with the approved plan, inspected and approved by the Staff Advisor. Street trees shall be spaced at one per 30 feet of street frontage, shall be chosen from the adopted Street Tree List and shall be installed in accordance with the specifications noted in the Site Design and Use Standards. The street trees shall be irrigated.
- f) That the bicycle parking facilities shall be installed according to the approved plans, inspected, and approved by the Staff Advisor prior to the issuance of the certificate of occupancy. The building permit submittals shall verify the design and placement of bicycle parking according to applicable standards.
- g) That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent residential properties.

RECEIVED

AUG 31 2015

Ashland Planning Commission  
Ashland Planning Department  
51 Winburn Way  
Ashland, OR 97520

Aug. 27, 2015

Re: Planning Action 2015-01370

Dear Commissioners:

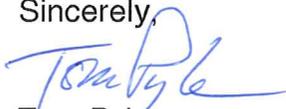
As a resident of a neighboring property, I received today notice of a planned addition to the Darex "factory" located at 210-220 East Hersey Street. I am not opposed to the project, but I do have a couple of minor concerns related to it -- concerns not serious enough to warrant taking up public hearing time.

I am worried that the addition will lead to increased heavy truck and other vehicle traffic using East Hersey Street for access to and from the Darex property. The street is already in poor condition in the vicinity of the driveway into Darex and increased traffic can only make it worse.

Also, I would hope that, as part of the expansion project, Darex consider installing a walking/bike path between East Hersey and Clear Creek Drive along the east side of its property. Generations of neighboring residents have used this part of the property as a short cut to Clear Creek Drive and it would be nice for Darex to respect this tradition instead of continuing to post "no trespassing" signs.

Thank you for your attention to these concerns.

Sincerely,



Tom Pyle  
251 East Hersey Street  
Ashland, OR 97520

Darex Expansion  
Planning Application Project Findings

August 17, 2015

City of Ashland  
Planning Division  
51 Winburn Way  
Ashland, OR 97520

**Project Identification**

Darex Expansion  
Site Address: 210-220 Hersey St, Ashland, OR 97520  
Map #: 39IE04CD  
Tax Lot: 2000  
Zoning: E 1 (Employment)  
ORW Architecture #: 1444

**Purpose**

These findings are intended to serve as the guiding document that will allow the City of Ashland to review and make its determinations regarding the planned expansion of the existing Darex facility. It will explain and demonstrate how the planned work will be acceptable within the Ashland Municipal Code.

**Project Overview**

The existing Darex facility located at 210-220 East Hersey Street is 39,962 SF with 79 existing parking spaces. Darex is planning two phases of future construction: Phase 1 is a 24,621 SF addition to their existing factory to accommodate additional space for administration and assembly employees, and 62 additional parking spaces for customer and employee parking; Phase 2 is an 11,107 SF stand-alone building at the south side of the property and 22 additional parking spaces. We request Site Design Review approval to construct the Phase 1 addition which is a one-story metal building and roof, approximately 24' high, and will match the southern portion of the existing building in color and materials. The project will provide additional area for the unique, hand assembly business model Darex is known for to accommodate an eventual increase in employees from the current 129 up to 226. The application includes a request for Tree Removal Permit to remove three trees, a 4.5-inch diameter Pear tree, a six-inch diameter Maple tree, and a six-inch diameter Pear tree. The application also proposes a private park be constructed in the location of the future Phase 2 building which will be open to the public from Clear Creek Drive, and 22 parking spaces for the future Phase 2 development. If the preferred proposal of the Phase 1 building addition, 84 (62+22) additional parking spaces and park is not allowed, an alternate site design has been developed which includes the same building addition, 69 additional parking spaces, and low-maintenance shrubs in place of the park. A site stair will also be added from Hersey Street to the main entry to further comply with City of Ashland standards.

**Municipal Codes**

The existing building will be renovated under the Ashland Land Use Ordinance (ALUO), Chapter 18 Land Use Standard development codes 18.2.2, 18.4.2, 18.4.3, and 18.5.2.



RECEIVED  
AUG 17 2015  
City Of Ashland

## **Zoning**

The project is in Zone E-1 with a Residential Overlay on the north half of the site that includes the existing Darex building fronting Hersey Street. The addition is south of and perpendicular to the existing building and complies with all applicable provisions of the zoning code including setbacks, lot coverage, building height and design, parking area requirements, and design criteria as demonstrated in this submittal.

## **Development Area**

The project expands the existing facility within the existing 4.85 acre site (556'x378').

## **Structural Site Coverage**

The project expands the existing building area by 24,621 SF. The total (existing + addition) proposed building footprint of 64,583 SF provides a site coverage of approximately 31 percent.

## **Impervious Surface Coverage**

The project has approximately 60,000 SF of new asphalt and concrete, plus minimal additional coverage for the bike rack and loading area curbs/bollards.

## **Parking**

The Phase 1 addition adds 63 parking spaces which includes 6 accessible spaces. 22 additional spaces are located adjacent to the park to serve the future Phase 2 building. Bicycle parking will be expanded and located adjacent to the existing building's main entrance. Parking will be accessed from three existing curb cut locations: two on Hersey St and one on the west side of Clear Creek Drive which reduces congestion at the main Hersey Street entry. A fourth existing curb cut on the east side of Clear Creek Drive will be improved with a drive apron only to finish the appearance between the existing curb cut and the proposed sidewalk.

A description of the parking space per person ratio is provided due to the nature of Darex's business and the quantity of employees. Refer to the Project Considerations narrative located at the end of this document.

Pedestrian circulation in parking areas comply with City standards. The eastern side of the proposed addition has nine entry locations, offering short travel distances from the parking area. The west parking areas have a network of compliant walkways connecting parking areas to building entries.

Accessible parking will be distributed such that all parking areas will be less than 50 spaces

## **Traffic Impact**

The proposed expansion of the manufacturing facility on 210 E. Hersey St. does not meet any of the threshold criteria and therefore the requirement for a Traffic Impact Analysis is not met. Refer to JRH Engineering document dated 07/09/2015 attached.

## **Landscaped Area**

The landscaped area of the proposed development is approximately 27% of the affected developed lot area, including the proposed park. This calculation excludes the north portion of the existing site which remains largely unchanged. Refer to the attached drawings which meet the requirements. Development of the future Phase 2 building will require that the landscaping be re-designed to meet Ashland Planning ordinance at that time.

### **Proposed Park (Preferred Site Design)**

The project proposes the construction of a private park, open to the public, in exchange for the early creation of parking to serve the future Phase 2 building.

Darex anticipates building a Phase 2 development in approximately 10 years. The proposal requests building the parking associated with that future building as part of Phase 1, and offers in exchange to build a privately held park, accessible to the public, that will serve the community until the Phase 2 building project is developed. The proposed park features:

- A variety of ornamental trees.
- Trees will grow and prosper in raised planters, but can be easily moved when the site is redeveloped. The materials are all reusable or recyclable.
- The park will be owned, maintained and insured by the applicant, while being fully accessible to the public.
- The park has a large "lawn" but uses a low water use "Lawn Alternative" seed mix that requires 50% less water and infrequent mowing.
- All the paths and hard surfaces are permeable crushed granite.

#### **Park Trees:**

The trees are planted in raised planters approximately 30" tall. The planters are built from a reusable, pinned segmental retaining wall system. Lining the retaining wall and the bottom of the planters (six inches below the exterior finish grade) is landscape fabric. This fabric, and the raised planters create a "grow bag" that contain the roots of the trees while giving them plenty of room to grow. When the time comes to develop the park area into a building, the walls can be taken down and the tree along with its intact soil volume can be moved with a fork lift. Basically, the trees are being placed in an extended nursery situation, and being grown as boxed trees to be replanted into the future building landscape. The retaining wall blocks will be reused to create terraces on the back slope of the lot for level tree wells.

#### **Park Ownership:**

The park will be built and cared for by the applicant. Liability and responsibility for the park will stay with the applicant. The employees at Darex will be free to use the park for their breaks and small gatherings. The public will be able to use this park in the same way that the Darex employees do, as a free-form neighborhood park. The park would include a public use easement that would be revoked when the building project moves forward. There will be winding paths and crushed granite patios along with a large green "Lawn Alternative".

### **Alternate Site Design**

If the park and associated parking are not allowed, an alternate site design (shown on the Alternate Site Plan drawing) develops the land adjacent to Clear Creek Drive with a low water use mowable field that includes irrigation and ten shade trees.

### **Public Facilities Overview**

City facilities have adequate capacity to serve the proposed addition for water, sewer, electricity, urban storm drainage, and paved access to the north and south edges of the site. Existing public transportation serves the site.

### **Electricity**

The electrical design/build contractor has performed preliminary load calculations estimating the proposed addition's demand will be 147,000 watts or 408 amps at 120/208 three-phase. The existing service size and City-owned transformer (2000 amp service at 750KVA 120/208 three-phase) have sufficient capacity to serve the proposed addition.

## **Gas**

Gas may be provided to the addition and will not result in public utility upgrades.

## **Water and Sewer**

The project proposes four additional toilets, two lavatories, and two drinking fountains. The design team contacted Steve Walker (Ashland Water Division) and Jason Robustelli (Ashland Wastewater Division) and both confirmed the existing public water line (12" main on Hersey Street) and sewer facilities are more than adequate to handle the additional proposed fixtures. As the design progresses the team will confer with the City (i.e. Steve Walker, Scott Fluery, Jason Robustelli, and others) to verify the existing system's capacity and any upgrades if needed.

## **Storm Water**

All new improvements (building, parking and sidewalks) were designed with the Rogue Valley Quality Design Manual to address both quality and quantity and comply with current storm drain requirements. The civil engineer contacted Pieter Smeenk (Ashland Public Works Division) and confirmed the detention strategy is compliant via an eastern detention swale and asphalt detention areas. Storm water will be treated for water quality and detained so that post-development flows do not exceed pre-development flows (undeveloped ground). Therefore the proposed development will not have any impact on the downstream infrastructure.

## **Waste**

All waste is controlled by Darex in accordance with OSHA and local regulation for waste disposal.

## **Landfill**

Waste generation will grow to be similar to the existing Darex building per square foot. The existing building accommodates one 25-yard dumpster for trash which is emptied seasonally, and one 2.5-yard dumpster for trash emptied weekly. Darex anticipates using the existing dumpsters and increasing the pick-up frequency as needed to accommodate additional waste for the addition.

## **Recyclables**

Recyclable generation will grow to be similar to the existing Darex building per square foot. The existing building accommodates one 25-yard dumpster for cardboard which is emptied weekly, and several comingled recyclable (paper/cans/glass) wheeled bins emptied weekly. Darex anticipates using the existing cardboard dumpster and increasing the pick-up frequency as needed to accommodate additional waste for the addition; Darex may acquire additional wheeled bins for comingled recyclables if needed.

## **Site Lighting**

Exterior lighting will be added to the building and parking areas per code requirements.

## **Air Pollution**

There is no expected addition to the local air pollution.

## **Hazards**

There are no hazardous substances introduced by the proposed scope of work.

## **18.5 APPLICATION REVIEW PROCEDURES AND APPROVAL CRITERIA**

Reader note: Code sections are shown in italics, followed by proposed findings and conclusions.

#### **18.5.2.010 Purpose**

*The purpose and intent of this chapter is to regulate the manner in which land in the City is used and developed, to reduce adverse effects on surrounding property owners and the general public, to create a business environment that is safe and comfortable, to further energy conservation efforts within the City, to enhance the environment for walking, cycling, and mass transit use, and to ensure that high quality development is maintained throughout the City.*

#### **18.5.2.020 Applicability**

*Site Design Review is required for the following types of project proposals:*

**A. Commercial, Industrial, Non-Residential, and Mixed Uses.** *Site Design Review applies to the following types of non-residential uses and project proposals, including proposals for commercial, industrial, and mixed-use projects, pursuant to section 18.5.2.030 Review Procedures.*

#### **18.5.2.030 Review Procedures**

**B. C-1, E-1, HC, and M-1 Zones.** *In the C-1, E-1, HC, and M-1 zones, but not within the Downtown Design Standards or Detail Site Review overlays, new structures or additions greater than 15,000 square feet in gross floor area, or greater than 50 percent of an existing building's gross floor area are subject to Type II review.*

**D. Detail Site Review Overlay.** *In the Detail Site Review overlay, new structures or additions greater than 10,000 square feet in gross floor area, or longer than 100 feet in length or width are subject to Type II review.*

**E. Bicycle Parking for Parking Lots and Structures.** *All public parking lots and structures shall provide two spaces per primary use, or one bicycle parking space for every five automobile parking spaces, of which 50 percent shall be sheltered.*

**G. Landscape and Irrigation Plan Amendments.** *Minor amendments to landscape and irrigation plans approved pursuant to chapter 18.4.4 to improve fire safety, public safety, water conservation, or energy efficiency may be processed as Ministerial or Type I actions.*

**Conclusion:** The application is subject to include B, D, E and G Submittal for Type II review

### **18.4.2 BUILDING PLACEMENT, ORIENTATION, AND DESIGN**

#### **18.4.2.040 Orientation and Scale**

**A.** *Buildings shall have their primary orientation toward the street and not a parking area. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or to one side.*

**Finding:** Phase I parking is on each side using existing access from Hersey Street and existing curb cut on Clear Creek Drive.

**Finding:** Phase 2 future development will be oriented toward Clear Creek Drive with parking to the west side accessed from Clear Creek Drive.

**Conclusion:** The proposed scope of work complies.

**B.** A building façade or multiple building facades shall occupy a large majority of a project’s street frontage as illustrated in Figure 18.4.2.040.B, and avoid site design that incorporates extensive gaps between building frontages created through a combination of driveway aprons, parking areas, or vehicle aisles. This can be addressed by, but not limited to, positioning the wider side of the building rather than the narrow side of the building toward the street. In the case of a corner lot, this standard applies to both street frontages. Spaces between buildings shall consist of landscaping and hard durable surface materials to highlight pedestrian areas.

**Finding:** There is no change to the existing building orientation.

**Finding:** The Phase 2 development will be oriented with its wider side toward the street and occupies a majority of the frontage.

**Conclusion:** The proposed scope of work complies. A clarification to the term “large” majority is requested. Refer to the Project Design Considerations narrative, item 2, at the end of this document.

**C.** Building entrances shall be oriented toward the street and shall be accessed from a public sidewalk. The entrance shall be designed to be clearly visible, functional, and shall be open to the public during all business hours. See Figure 18.4.2.040.B.1.

**Finding:** There no change to the existing building entrance. A new stair access will be added near the Hersey St. entrance.

**Finding:** The entrance design for the Phase 2 development will be submitted to planning at a future date for compliance. The existing public sidewalk will be extended to the east and west extents of the property along Clear Creek Drive.

**Conclusion:** The proposed scope of work complies.

**D.** Building entrances shall be located within 20 feet of the public right of way to which they are required to be oriented. Exceptions may be granted for topographic constraints, lot configuration, designs where a greater setback results in an improved access or for sites with multiple buildings, such as shopping centers, where other buildings meet this standard.

**Finding and Conclusion:** Not applicable.

**E.** Where a building is located on a corner lot, its entrance shall be oriented toward the higher order street or to the lot corner at the intersection of the streets. The building shall be located as close to the intersection corner as practicable.

**Finding and Conclusion:** Not applicable

**F.** Public sidewalks shall be provided adjacent to a public street along the street frontage.

**Finding** Additional public sidewalk has been provided on the south of the property to complete the existing public walk.

**Conclusion:** The proposed scope of work complies.

### **18.4.3 PARKING, ACCESS, AND CIRCULATION**

#### **18.4.3.010 Purpose**

Chapter 18.4.3 contains requirements for automobile and bicycle parking, and vehicular and pedestrian access, circulation, and connectivity. The purpose is to provide safe and effective

access and circulation for pedestrians, bicyclists, and vehicles. For transportation improvement requirements, refer to chapter 18.4.6 Public Facilities.

#### **18.4.3.020 Applicability**

- A.** The requirements of this chapter apply to parking, access, and circulation facilities in all zones, except those specifically exempted, whenever any building is erected or enlarged, parking, access or circulation is expanded or reconfigured, or the use is changed.
- B.** The City may require a study prepared by a qualified professional to determine offsets in parking demand, access, circulation, and other transportation impacts, pursuant to this section.
- C.** All required parking, access, and circulation facilities shall be constructed when a use is intensified by the addition of floor space, seating capacity, or change in use, or when an existing building or dwelling is altered or enlarged by the addition or creation of dwelling units or guest rooms.
- D.** Exceptions and Variances. Requests to depart from the requirements of this chapter are subject to chapter 18.5.5 Variances, except that deviations from the standards in subsections 18.4.3.080.B.4 and 5 and section 18.4.3.090 Pedestrian Access and Circulation are subject to 18.5.2.050.E Exception to the Site Development and Design Standards that reuse of the building stock within the Historic District overlay is an exceptions I circumstance and an unusual hardship for the purposes of granting a variance.
- E.** Variance to Parking Standard for Commercial Buildings in the Historic District. In order to preserve existing structures within the Historic District overlay while permitting the redevelopment of property to its highest commercial use, the Staff Advisor, through a Type I procedure and pursuant to section 18.5.1.050, may grant a Variance to the parking standards of section 18.4.3.040 by up to 50 percent for commercial uses within the Historic District overlay. The intent of this provision is to provide as much off-street parking as practical while preserving existing structures and allowing them to develop to their full commercial potential. The City, through this ordinance provision, finds that reuse of the building stock within the Historic District overlay is an exceptional circumstance and an unusual hardship for the purposes of granting a variance.

#### **18.4.3.030 General Automobile Parking Requirements and Exceptions**

- A. Minimum Number of Off-Street Automobile Parking Spaces.** Off-street parking shall be provided pursuant to one of the following three methods and shall include required Disabled Person Parking.
  - 1. Standard Ratios for Automobile Parking. The standards in Table 18.4.3.040.
  - 2. Unspecified Use. Where automobile parking requirements for any use are not specifically listed in Table 18.4.3.040, such requirements shall be determined by the Staff Advisor based upon the most comparable use specified in this section, and other available data.
  - 3. Parking Demand Analysis. The approval authority through a discretionary review may approve a parking standard that is different than the standards under subsection 1 and 2, above, as follows.
    - a. The applicant submits a parking demand analysis with supporting data prepared by a professional engineer, planner, architect, landscape architect, or other qualified professional;
    - b. The parking analysis, at a minimum, shall assess the average parking demand and available supply for existing and proposed uses on the subject site; opportunities for shared parking with other uses in the vicinity; existing public parking in the vicinity; transportation options

existing or planned near the site, such as frequent bus service, carpools, or private shuttles; and other relevant factors. The parking demand analysis option may be used in conjunction with, or independent of, the options provided under section 18.4.3.060 Parking Management Strategies.

c. The review procedure shall be the same as for the main project application.

**B. Maximum Number of Off-Street Automobile Parking Spaces.** *The number of spaces provided by any particular use in ground surface lots shall not exceed the number of spaces required by this chapter by more than ten percent. Spaces provided on-street, or within the building footprint of structures, such as in rooftop parking, or under-structure parking, or in multi-level parking above or below surface lots, shall not apply towards the maximum number of allowable spaces.*

**C. Downtown Zone.** *All uses within the C-1-D zone, except for hotel, motel, and hostel uses, are exempt from the off-street parking requirements of this section.*

**D. North Mountain Plan District.** *Within the Neighborhood Central zone of the North Mountain (NM) Neighborhood Plan district, all uses are exempt from the off-street parking requirements of this section, except that residential uses are required to provide a minimum of one parking space per residential unit.*

**Conclusion:** The scope of work complies with items A and B. Items C and D are not applicable.

#### **18.4.3.040 Parking**

As noted in Table 18.4.3.040, minimum parking per land use for Commercial General Office is 1 space per 500 SF floor area. The proposed quantity of parking spaces includes both Phase 1 and Phase 2 spaces.

**Finding:** The total proposed building area for Phase 1 is 64,483 SF (existing 39,962 SF + Phase 1 24,521 SF). The proposed parking for the addition is 141, which falls between the minimum ( $64,483 \text{ SF} / 500 \text{ SF} = 129 \text{ spaces}$ ) and maximum ( $129 \times 1.1 = 142 \text{ spaces}$ ).

**Finding:** The future Phase 2 building area is approximately 11,107 SF; the proposed parking for Phase 2 is 22 spaces, which complies with the minimum ( $11,107 \text{ SF} / 500 \text{ SF} = 22 \text{ spaces}$ ).

**Finding:** The total proposed parking for this project is 163, which is the sum of Phase 1 and 2 parking ( $141 + 22 \text{ spaces}$ ).

**Conclusion:** The proposed scope of work complies. Due to the unique nature of Darex's business activities, the building's use most closely aligns with Commercial General Office; refer to the Project Design Considerations narrative, item 1, at the end of this document.

#### **8.4.3.050 Accessible Parking Spaces**

*Accessible parking shall be provided consistent with the requirements of the building code, including but not limited to the minimum number of spaces for automobiles, van-accessible spaces, location of spaces relative to building entrances, accessible routes between parking areas and building entrances, identification signs, lighting, and other design and construction requirements. Accessible parking shall be included and identified on the planning application submittals.*

**Finding:** Accessible parking is included per building code and proposed preliminary locations are identified on the proposed site plan.

**Conclusion:** The proposed scope of work complies.

#### **18.4.3.070 Bicycle Parking**

**E. Bicycle Parking for Parking Lots and Structures.** All public parking lots and structures shall provide two spaces per primary use, or one bicycle parking space for every five automobile parking spaces, of which 50 percent shall be sheltered.

**Finding:** Minimum bicycle parking spaces is 33 (163 vehicle parking spaces / 5 = 33). The proposed work expands the existing parking adjacent to the main entrance to accommodate 34 bicycle spaces, with at least 50% being covered.

**Conclusion:** The proposed scope of work complies.

#### **18.4.3.080 Vehicle Area Design**

**A. Parking Location** (Only item A.2. is applicable)

2. Except as allowed in the subsection below, automobile parking shall not be located in a required front and side yard setback area abutting a public street, except alleys.

**Finding:** No parking is designated within the yard setbacks.

**Conclusion:** The proposed scope of work complies.

**B. Parking Area Design.** Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in 18.4.3.080.B. See also, accessible parking space requirements in section 18.4.3.050 and parking lot and screening standards in subsection 18.4.4.030.F.

1. Parking spaces shall be a minimum of 9 feet by 18 feet.
2. Up to 50 percent of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 feet by 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."
3. Parking spaces shall have a back-up maneuvering space not less than 22 feet, except where parking is angled, and which does not necessitate moving of other vehicles.
4. Parking lots with 50 or more parking spaces, and parking lots where pedestrians must traverse more than 150 feet of parking area, as measured as an average width or depth, shall be divided into separate areas by one or more of the following means: a building or group of buildings; plazas landscape areas with walkways at least five feet in width; streets; or driveways with street-like features as illustrated in Figure 18.4.3.080.B.4 Street-like features, for the purpose of this section, means a raised sidewalk of at least five feet in width, with six-inch curb, accessible curb ramps, street trees in planters or tree wells and pedestrian-oriented lighting (i.e., not exceeding 14 feet typical height).
5. Parking areas shall be designed to minimize the adverse environmental and microclimatic impacts of surface parking through design and material selection as illustrated in Figure 18.4.3.080.B.5. Parking areas of more than seven parking spaces shall meet the following standards (options include 50% shade from tree canopy over parking surface within 5 years of occupancy).

**Findings:** Parking spaces are 9 feet by 18 feet.

**Findings:** Compact spaces are not used in design.

**Findings:** Maneuverability meets or exceeds 22'.

- Findings:** The parking areas are separated by buildings, tree wells with street trees and split level lots with sidewalks.
- Findings:** As designed the parking lots have over 50% shade within the first five years of occupancy.
- Conclusion:** The proposed scope of work complies.

**C. Vehicular Access and Circulation.** (Only items C.1, C.2, and C.3 are applicable)

*The intent of this subsection is to manage access to land uses and on-site circulation and maintain transportation system safety and operations. For transportation improvement requirements, refer to chapter 18.4.6 Public Facilities.*

1. *Applicability. This section applies to all public streets within the City and to all properties that abut these streets. The standards apply when developments are subject to a planning action (e.g., Site Design Review, Conditional Use Permit, Land Partition, Performance Standards Subdivision).*
2. *Site Circulation. New development shall be required to provide a circulation system that accommodates expected traffic on the site. All on-site circulation systems shall incorporate street-like features as described in 18.4.3.080.B.4. Pedestrian connections on the site, including connections through large sites, and connections between sites and adjacent sidewalks must conform to the provisions of section 18.4.3.090.*
3. *Intersection and Driveway Separation. The distance from a street intersection to a driveway, or from a driveway to another driveway shall meet the minimum spacing requirements for the street's classification in the Ashland Transportation System Plan (TSP) as illustrated in Figures 18.4.3.080.C.3.a and Figure 18.4.3.080.C.3.b.*
  - a. *In no case shall driveways be closer than 24 feet as measured from the bottom of the existing or proposed apron wings of the driveway approach.*
  - b. *Partitions and subdivisions of property located in an R-2, R-3, C-1, E-1, CM, or M-1 zone shall meet the controlled access standards set forth below. If applicable, cross access easements shall be required so that access to all properties created by the land division can be made from one or more points.*
  - c. *Street and driveway access points in an R-2, R-3, C-1, E-1, CM, or M-1 zone shall be limited to the following.*
    - i. *Distance between driveways: on boulevard streets: 100 feet. On collector streets: 75 feet. On neighborhood streets: 24 feet for 2 units or fewer per lot, 50 feet for three or more units per lot.*
    - ii. *Distance from intersections: on boulevard streets: 100 feet. On collector streets: 50 feet. On neighborhood streets: 35 feet.*
  - d. *Access Requirements for Multi-family Developments. Not Applicable.*

**D. Driveways and Turn-Around Design.** (Only items D.3-4, and D.6-9 are applicable)

3. *Parking areas of more than seven parking spaces shall be served by a driveway 20 feet in width and constructed to: facilitate the flow of traffic on or off the site, with due regard to pedestrian and vehicle safety; be clearly and permanently marked and defined; and provide adequate aisles or turn-around areas so that all vehicles may enter the street in a forward manner.*
4. *The width of driveways and curb cuts in the parkrow and sidewalk area shall be minimized.*
6. *Vertical Clearances. Driveways, aisles, turn-around areas and ramps shall have a minimum vertical clearance of 13.5 feet for their entire length and width. Parking structures are exempt from this requirement.*
7. *Vision Clearance. No obstructions may be placed in the vision clearance area except as set forth in section 18.2.4.040.*

8. Grades for new driveways in all zones shall not exceed 20 percent for any portion of the driveway. If required by the City, the developer or owner shall provide certification of driveway grade by a licensed land surveyor.
9. All driveways shall be installed pursuant to City standards prior to issuance of a certificate of occupancy for new construction.

- Finding:** Existing Curb cuts on Hersey Street and Clear Creek Drive will not be changed or relocated. Drive aisles and/or driveway aprons will be added to existing Clear Creek Drive curb cuts.
- Finding:** Curb cuts are existing and the width of drive aisles are minimized.
- Finding:** Vertical clearances are compliant.
- Finding:** Vision clearance are compliant.
- Finding:** Driveway grades are compliant.
- Finding:** Driveways will be connected to existing curb cuts and installed per City standards.
- Conclusion:** The proposed scope of work complies. See Project Design Considerations narrative, item 3, at the end of this document.

**E. Parking and Access Construction** (Only items E.1-4 are applicable)

1. Paving. All required parking areas, aisles, turn-arounds, and driveways shall be paved with concrete, asphaltic, porous solid surface, or comparable surfacing, constructed to standards on file in the office of the City Engineer.
2. Drainage. All required parking areas, aisles, and turn-arounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.
3. Driveway Approaches. Approaches shall be paved with concrete surfacing constructed to standards on file in the office of the City Engineer.
4. Marking. Parking lots of more than seven spaces shall have all spaces permanently and clearly marked.

- Finding:** Paving is asphalt.
- Finding:** A drainage swale is located to the west of the addition, in the planting strip between parking areas. All drainage will remain on-site and will not flow beyond property line.
- Finding:** All driveway aprons will be concrete and comply with City standards.
- Finding:** All parking area markings will comply.
- Conclusion:** The proposed scope of work complies.

**18.4.3.090 Pedestrian Access and circulation**

**A. Purpose.** The purpose of section 18.4.3.090 is to provide for safe, direct, and convenient pedestrian access and circulation.

**B. Standards.** Development subject to this chapter, except single-family dwellings on individual lots and associated accessory structures, shall conform to the following standards for pedestrian access and circulation.

1. Continuous Walkway System. Extend the walkway system throughout the development site and connect to all future phases of development, and to existing or planned off-site adjacent sidewalks, trails, public parks, and open space areas to the greatest extent practicable. The developer may also be required to connect or stub walkway(s) to adjacent streets and to private City of Ashland 4-64 Land Use Ordinance 18.4.3 – Parking, Access, and Circulation property for this purpose.

2. *Safe, Direct, and Convenient.* Provide safe, reasonably direct, and convenient walkway connections between primary building entrances and all adjacent streets. For the purposes of this section, the following definitions apply.
  - a. "Reasonably direct" means a route that does not deviate unnecessarily from a straight line or a route that does not involve a significant amount of out-of-direction travel for likely users.
  - b. "Safe and convenient" means reasonably free from hazards and provides a reasonably direct means of walking between destinations.
  - c. "Primary entrance" for a non-residential building means the main public entrance to the building. In the case where no public entrance exists, street connections shall be provided to the main employee entrance.
  - d. "Primary entrance" for a residential building is the front door (i.e., facing the street). For multifamily buildings and mixed-use buildings where not all dwelling units have an individual exterior entrance, the "primary entrance" may be a lobby, courtyard, or breezeway serving as a common entrance for more than one dwelling.
3. *Connections within Development.* Walkways within developments shall provide connections meeting all of the following requirements as illustrated in Figures 18.4.3.090.B.3.a and 18.4.3.090.B.3.b
  - a. Connect all building entrances to one another to the extent practicable.
  - b. Connect on-site parking areas, recreational facilities, and common areas, and connect offsite adjacent uses to the site to the extent practicable. Topographic or existing development constraints may be cause for not making certain walkway connections.
  - c. Install a protected raised walkway through parking areas of 50 or more spaces, and where pedestrians must traverse more than 150 feet of parking area, as measured as an average width or depth.
4. *Walkway Design and Construction.* Walkways shall conform to all of the following standards in as illustrated in Figure 18.4.3.090.B.3.a and 18.4.3.090.B.3.b. For transportation improvement requirements, refer to chapter 18.4.6 Public Facilities.
  - a. *Vehicle/Walkway Separation.* Except for crosswalks, where a walkway abuts a driveway or street, it shall be raised six inches and curbed along the edge of the driveway. Alternatively, the approval authority may approve a walkway abutting a driveway at the same grade as the driveway if the walkway is distinguished from vehicle-maneuvering areas. Examples of alternative treatments are mountable curbs, surface treatments such as stamped concrete or reflector bumps, and using a row of decorative metal or concrete bollards to separate a walkway from a driveway.
  - b. *Crosswalks.* Where walkways cross a parking area or driveway, clearly mark crosswalks with contrasting paving materials (e.g., light-color concrete inlay between asphalt), which may be part of a raised/hump crossing area. Painted or thermo-plastic striping and similar types of non-permanent applications may be approved for crosswalks not exceeding 24 feet in length.
  - c. *Walkway Surface and Width.* Walkway surfaces shall be concrete, asphalt, brick/masonry pavers, or other durable surface, and at least five feet wide. Multi-use paths (i.e., for bicycles and pedestrians) shall be concrete or asphalt, and at least ten feet wide, in accordance with the section 18.4.6.040 Street Design Standards.
  - d. *Accessible routes.* Walkways shall comply with applicable Americans with Disabilities Act (ADA) and State of Oregon requirements. The ends of all raised walkways, where the walkway intersects a driveway or street, shall provide ramps that are ADA accessible, and walkways shall provide direct routes to primary building entrances.
  - e. *Lighting.* Lighting shall comply with section 18.4.4.050.

- Finding:** Continuous walkways are provided throughout the parking areas and connect to all existing and future buildings.
- Finding:** Walkway connections are safe, direct, and convenient from building entries to streets, sidewalks, and proposed park.
- Finding:** Walkway connections are safe, direct, and convenient between building entries and the Phase 1 addition is connected to the existing building internally.
- Finding:** Walkways are provided in planting strips (i.e. protected) to connect parking areas to building entries.
- Finding:** Accessible parking will be distributed such that all parking areas will be less than 50 spaces.
- Finding:** Pedestrians traverse less than 150 feet of parking area to access building or protected walkways.
- Finding:** All walkways (except crosswalks) are separated from the drive path with a 6" curb.
- Finding:** All walkways within the parking areas are painted asphalt or concrete, which clearly differentiate it from the surrounding parking area asphalt.
- Finding:** All walkways and sidewalks are 5' wide.
- Finding:** All walkways comply with ADA and State of Oregon accessibility requirements.
- Finding:** All walkway lighting shall comply with section 18.4.4.050.
- Conclusion:** The proposed scope of work complies.

#### **18.4.4 LANDSCAPING, LIGHTING, AND SCREENING**

##### **18.4.4.030 Landscaping and Screening**

**A. General Landscape Standard.** All portions of a lot not otherwise developed with buildings, accessory structures, vehicle maneuvering areas, parking, or other approved hardscapes shall be landscaped pursuant to this chapter.

**B. Minimum Landscape Area and Coverage.** All lots shall conform to the minimum landscape area standards of the applicable zoning district (per Table 18.2.6.030 for non-residential zones, the minimum landscaped area is 15% for zone E-1). Except as otherwise provided by this chapter, areas proposed to be covered with plant materials shall have plant coverage of not less than 50 percent coverage within one year and 90 percent coverage within five years of planting.

**Finding:** Site is in E-1 zone; per Table 18.2.6.030 it requires minimum 15% landscaping. The proposed site plan, excluding the park, is approximately 16% landscaping.

**Finding:** Landscape areas will attain at least 50% coverage in the first year and 90% coverage within five years of planting.

**Conclusion:** The proposed scope of work complies.

##### **C. Landscape Design and Plant Selection.** (Item C.4 is not applicable)

The landscape design and selection of plants shall be based on all of the following standards.

1. **Tree and Shrub Retention.** Existing healthy trees and shrubs shall be retained, pursuant to chapter 18.4.5. Consistent with chapter 18.4.5 Tree Preservation and Protection, credit may be granted toward the landscape area requirements where a project proposal includes preserving healthy vegetation that contribute(s) to the landscape design.
2. **Plant Selection.**
  - a. Use a variety of deciduous and evergreen trees, shrubs, and ground covers.
  - b. Use plants that are appropriate to the local climate, exposure, and water availability. The presence of utilities and drainage conditions shall also be considered.
  - c. **Storm Water Facilities.** Use water-tolerant species where storm water retention/detention or water quality treatment facilities are proposed.
  - d. **Crime Prevention and Defensible Space.** Landscape plans shall provide for crime prevention and defensible space, for example, by using low hedges and similar plants

allowing natural surveillance of public and semi-public areas, and by using impenetrable hedges in areas where physical access is discouraged.

- e. *Street Trees.* Street trees shall conform to the street tree list approved by the Ashland Tree Commission. See the Ashland Recommended Street Tree Guide.
- 3. *Water Conserving Landscaping.* Commercial, industrial, non-residential, and mixed-use developments that are subject to chapter 18.5.2 Site Design Review, shall use plants that are low water use and meet the requirements of 18.4.4.030.I Water Conserving Landscaping.
- 5. *Screening*
  - a. Evergreen shrubs shall be used where a sight-obscuring landscape screen is required.
  - b. Where a hedge is used as a screen, evergreen shrubs shall be planted so that not less than 50 percent of the desired screening is achieved within two years and 100 percent is achieved within four years. Living groundcover in the screen strip shall be planted such that 100 percent coverage is achieved within two years.
- 6. *Plant Sizes*
  - a. Trees shall be not less than two-inch caliper for street trees, and 1.5-inch caliper for other trees at the time of planting.
  - b. Shrubs shall be planted from not less than one gallon containers, and where required for screening shall meet the requirements of 18.4.4.030.C.5 Screening.

**Finding:** Per the proposed site plan and tree removal and protection drawings, three trees are proposed to be demolished and three are proposed to remain and be protected per City standards. The parking area shade strategy proposes many new trees which more than mitigates the three trees to be demolished.

**Finding:** Plant selections are predominantly deciduous with a mix of evergreen and deciduous shrubs.

**Finding:** All proposed plants are adapted to this region and are positioned based on exposure, water needs, site soils and drainage.

**Finding:** The Stormwater bio-swales are proposed to be planted with appropriate shrubs and trees.

**Finding:** Defensible design is integrated into the design.

**Finding:** The city of Ashland street tree list was used to generate the proposed street trees.

**Finding:** Plant selections meet the City's water conservation landscaping standards.

**Finding:** The East and West project boundaries are designed as site-obscuring landscape screens containing evergreen shrubs and deciduous trees.

**Finding:** The site-obscuring landscape screens (hedges) will achieve 50 percent of the desired screening within two years and 100 percent within four years. Living groundcover in the screen strip shall be planted such that 100 percent coverage is achieved within two years.

**Finding:** All street trees are specified as two inch caliper; all other trees are specified as a minimum 1.5 inch caliper.

**Finding:** All shrubs are at least one gallon at planting and 5 gallon for screening purposes.

**Conclusion:** The proposed scope of work complies.

**D. Tree Preservation, Protection, and Removal.** See chapter 18.4.5 for Tree Protection and Preservation and chapter 18.5.7 for Tree Removal Permit requirements.

**Finding:** A tree removal and protection plan is included in this submittal.

**Conclusion:** The proposed scope of work complies.

**E. Street Trees.** (Item E.4 not applicable)

*The purpose of street trees is to form a deciduous canopy over the street. The same effect is also desired in parking lots and internal circulation streets; rows of street trees should be*

included in these areas where feasible. All development fronting on public or private streets shall be required to plant street trees in accordance with the following standards and chosen from the recommended list of street trees.

1. *Location of Street Trees.* Street trees shall be located in the designated planting strip or street tree wells between the curb and sidewalk, or behind the sidewalk in cases where a planting strip or tree wells are or will not be in place. Street trees shall include irrigation, root barriers, and generally conform to the standards established by the Community Development Department.
2. *Spacing and Placement of Street Trees.* All street tree spacing may be made subject to special site conditions that may, for reasons such as safety, affect the decision. Any such proposed special condition shall be subject to the Staff Advisor's review and approval. The placement, spacing, and pruning of street trees shall meet all of the following requirements.
  - a. Street trees shall be placed at the rate of one tree for every 30 feet of street frontage. Trees shall be evenly spaced, with variations to the spacing permitted for specific site limitations, such as driveway approaches.
  - b. Street trees shall not be planted closer than 25 feet from the curb line of intersections of streets or alleys, and not closer than ten feet from private driveways (measured at the back edge of the sidewalk), fire hydrants, or utility poles.
  - d. Street trees shall not be planted closer than 20 feet to light standards. Except for public safety, no new light standard location shall be positioned closer than ten feet to any existing street tree, and preferably such locations will be at least 20 feet distant.
  - e. Street trees shall not be planted closer than 2.5 feet from the face of the curb. Street trees shall not be planted within two feet of any permanent hard surface paving or walkway. Sidewalk cuts in concrete for trees, or tree wells, shall be at least 25 square feet; however, larger cuts are encouraged because they allow additional air and water into the root system and add to the health of the tree. Tree wells shall be covered by tree grates in accordance with City specifications.
  - f. Street trees planted under or near power lines shall be selected so as to not conflict with power lines at maturity.
  - g. Existing trees may be used as street trees if there will be no damage from the development which will kill or weaken the tree. Sidewalks of variable width and elevation, where approved pursuant to section 18.4.6.040 Street Design Standards, may be utilized to save existing street trees, subject to approval by the Staff Advisor.
3. *Pruning.* Street trees, as they grow, shall be pruned to provide at least eight feet of clearance above sidewalks and 12 feet above street roadway surfaces.

- Finding:** Trees along the Clear Creek Drive frontage are placed one tree per thirty feet.
- Finding:** Street trees are not near street or alley intersections, and not closer than 10' from proposed driveways.
- Finding:** Per the tree planting detail on our proposed planting plan, all street trees include irrigation, root barriers, and other approved systems for the health and wellbeing of new trees to promote their growth.
- Finding:** Trees and light standard locations are compliant.
- Finding:** All trees have at least 3' of space between the trunk and hardscape.
- Finding:** Power lines are underground at this site and will not conflict with trees.
- Finding:** Existing trees will not be used as street trees.
- Finding:** Street trees will be pruned to comply with City standards.
- Conclusion:** The proposed scope of work complies.

**F. Parking Lot Landscaping and Screening.** (Items 2.c is not applicable)

Parking lot landscaping, including areas of vehicle maneuvering, parking, and loading, shall meet the following requirements.

1. Landscaping.

- a. Parking lot landscaping shall consist of a minimum of seven percent of the total parking area plus a ratio of one tree for each seven parking spaces to create a canopy effect.
- b. The tree species shall be an appropriate large canopied shade tree and shall be selected from the street tree list approved by the Ashland Tree Commission to avoid root damage to pavement and utilities, and damage from droppings to parked cars and pedestrians. See the Ashland Recommended Street Tree Guide.
- c. The tree shall be planted in a landscaped area such that the tree bole is at least two feet from any curb or paved area.
- d. The landscaped area shall be distributed throughout the parking area and parking perimeter at the required ratio.
- e. That portion of a required landscaped yard, buffer strip, or screening strip abutting parking stalls may be counted toward required parking lot landscaping but only for those stalls abutting landscaping as long as the tree species, living plant material coverage, and placement distribution criteria are also met. Front or exterior yard landscaping may not be substituted for the interior landscaping required for interior parking stalls.

2. Screening.

- a. Screening Abutting Property Lines. A five foot landscaped strip shall screen parking abutting a property line. Where a buffer between zones is required, the screening shall be incorporated into the required buffer strip, and will not be an additional requirement.
- b. Screening Adjacent to Residential Building. Where a parking area is adjacent to a residential building it shall be setback at least eight feet from the building, and shall provide a continuous hedge screen.

**Finding:** The total area of parking and circulation is 54,690 square feet. The total landscape area is 14,539. Landscape cover within the combined parking and circulation areas is 26.5%.

**Finding:** Proposed parking lot tree varieties include the list below and follow the Ashland recommended street tree guide except where parenthetically noted:

Betula nigra 'Duraheat' (in the bioswale)

Carpinus betulus 'Fastigiata'

Malus ioensis 'Klehms Improved Bechtel'

Platanus x a. 'Bloodgood' (used in larger planting areas)

Quercus rubra

Zelkova serrata 'Green Vase'

**Finding:** All trees are planted a minimum of three feet from any curb or paved area.

**Finding:** The landscaping is distributed throughout the parking area and at the perimeter.

**Finding:** Landscaping meets all City ratios and standards.

**Finding:** The parking lots and circulation are buffered by a five foot wide hedge on both the east and west property lines.

**Conclusion:** The proposed scope of work complies.

**G. Other Screening Requirements.** (Item G.2 is not applicable)

Screening is required for refuse and recycle containers, outdoor storage areas, loading and service corridors, mechanical equipment, and the City may require screening other situations, pursuant with the requirements of this ordinance.

1. Recycle and Refuse Container Screen. Recycle and refuse containers or disposal areas shall be screened by placement of a solid wood fence or masonry wall five to eight feet in height

to limit the view from adjacent properties or public rights-of-way. All recycle and refuse materials shall be contained within the screened area.

3. **Loading Facilities and Service Corridors.** Commercial and industrial loading facilities and service corridors shall be screened when adjacent to residential zones. Siting and design of such service areas shall reduce the adverse effects of noise, odor, and visual clutter upon adjacent residential uses.
4. **Mechanical Equipment.** Mechanical equipment shall be screened by placement of features at least equal in height to the equipment to limit view from public rights-of-way, except alleys, and adjacent residentially zoned property. Mechanical equipment meeting the requirements of this section satisfy the screening requirements in 18.5.2.020.C.3.
  - a. **Roof-mounted Equipment.** Screening for roof-mounted equipment shall be constructed of materials used in the building's exterior construction and include features such as a parapet, wall, or other sight-blocking features. Roof-mounted solar collection devices are exempt from this requirement pursuant to subsection 18.5.2.020.C.3.
  - b. **Other Mechanical Equipment.** Screening for other mechanical equipment (e.g., installed at ground level) include features such as a solid wood fence, masonry wall, or hedge screen.

**Finding:** The recycle and Refuse area will be screened with a five foot tall masonry wall and slatted fence.

**Finding:** The site is screened so that these facilities are screened as part of the parking lot screening requirement.

**Finding:** Roof-mounted mechanical equipment is approximately 80' from Clear Creek Drive and is not planned to be screened similar to the existing building. Street Trees and park plantings are expected to provide sufficient screening from Clear Creek Drive.

**Conclusion:** The proposed scope of work complies.

**H. Irrigation.** Irrigation systems shall be installed to ensure landscape success. If a landscape area is proposed without irrigation, a landscape professional shall certify the area can be maintained and survive without artificial irrigation. Irrigation plans are reviewed through a Ministerial process at the time of building permit submittals.

**Finding:** The project proposal includes a professionally designed irrigation system that will support the proposed plant material and comply with water saving irrigation technology.

**Conclusion:** The proposed scope of work complies.

**I. Water Conserving Landscaping.** The following standards are intended to conserve water while encouraging attractive landscaping. Further, requirements are aimed at reducing water demand when water is most scarce, during the dry late summer months when water reserves are low.

1. **Landscaping Design Standards.**

- a. **Landscaping Coverage.** Water conserving designs shall have plant coverage of not less than 90 percent with five years of planting, but are not required to meet the standard of 50 percent coverage within one year.
- b. **Plant Selection.** At least 90 percent of plants in the non-turf areas shall be listed as drought tolerant in the Sunset Western Garden book, City's Water-Wise Landscaping website, or be similarly well-suited for this climate of region as determined by the Staff Advisor. Up to ten percent of the plants may be of a non-drought tolerant variety or

species as long as they are grouped together and are located in a separate irrigation zone.

- c. Screening. Plant screening hedges to attain 50 percent coverage after two years.
- d. Mulch. Add a minimum of two inches of mulch in non-turf areas to the soil surface after planting. Neither large nuggets nor fine bark may be used for mulch. Non-porous material shall not be placed under the mulch.
- e. Turf and Water Areas. Limit combined turf or water areas (i.e., pools, ponds, and fountains) to 20 percent of the landscaped areas. Turf limitations do not apply to public parks, private common open space, required outdoor recreation areas, golf courses, cemeteries, and school recreation areas.
- f. Fountains. Design all fountains to recycle their water.
- g. Turf Location. Turf is restricted to slopes less than ten percent grade.
- h. Berms and Raised Beds.
- i. Soil Quality. When new vegetation is planted, soils shall be amended for plant health and water absorption. Add mature compost at a rate of three cubic yards of compost per 1,000 square feet of area to be landscaped, and work soil and amendment(s) to a depth of four to six inches. This requirement may be waived for one or more of the following circumstances.
  - a. The area to be landscaped is fenced off to fully protect native soil from disturbance and compaction during construction.
  - b. Soil tests document an organic content of a least three percent based on a representative core sample taken at a rate of one test per 20,000 square feet, based on a minimum of three core sample per test. Samples shall be taken at least 40 feet apart to a depth of six inches following attainment of rough grade.
- c. The area to be landscaped will be used to capture and treat storm water runoff, and is subject to separate design standards.

**Finding:** The landscape as designed will achieve greater than 90% coverage in five years.

**Finding:** All plants on this plan are well-suited to this climate and are considered water-wise and well adapted. All plants used for this site are low water use except for the swale plantings which are on their own irrigation zone.

**Finding:** The screens plantings on the East and West sides of the site are planted to screen at greater than 50% within 2 years.

**Finding:** A minimum two of inches of City-approved mulch will be specified for all planting area excluding the low water use turf area.

**Finding:** We are proposing the use of a Drought Tolerant Lawn Alternative for the proposed park which is documented to use 50% less water than a typical sod lawn. The Alternate Site Design is a mowable, seeded erosion control field.

**Finding:** The design excludes fountains, berms, and raised beds.

**Finding:** The proposed Lawn Alternative are is relatively flat. Turf is not used on slopes of 10% or more.

**Finding:** The existing soils on this site will be amended to meet this soil quality criteria and new topsoil will be brought in to fill all landscape area to an additional 12" of depth.

**Conclusion:** The proposed scope of work complies.

2. *Irrigation System Design Standards. Irrigation plans are reviewed through a Ministerial process at the time of building permit submittals, and are subject to the following standards.*

- a. *Design sprinkler head spacing for head-to-head coverage.*
- b. *Design irrigation system to minimize runoff and overspray to non-irrigated areas.*
- c. *Match precipitation rates for all irrigation heads for each circuit.*

- d. Separate irrigation zones based on water needs of plantings and type of sprinklers being used (i.e., rotating, fixed spray, or drip). Plants with similar watering needs shall be in the same irrigation zone unless irrigated by drip irrigation having emitters sized for individual plant water needs.
- f. Use sprinkler heads with a precipitation rate of .85 inches per hour or less on slopes exceeding 15 percent to minimize run-off, or when slope exceeds ten percent within ten feet of hardscape.
- g. Serviceable check valves (or pressure compensating emitters for drip systems) are required where an elevation difference greater than 20 feet exists on any circuit.
- h. Drip irrigation systems are required for trees unless within lawn areas.
- i. Equip all irrigation zones with pressure regulator valves (PRV) to meet the manufacturer's recommended operating pressure for the components of each zone; except in those instances where a PRV is in place. PRV's shall be located at the meter or solenoid valve.
- j. Automatic Sprinkler Controls.
  - i. Equip all irrigation systems with a controller capable of dual or multiple programming. Controllers shall have a multiple start time capability, station run times in minutes to hours, and water days by interval, day of the week, and even/odd day
  - ii. Use controllers with a percent adjust (water budget) feature, or the capability of accepting an external rain or soil moisture sensor.

- Finding:** The irrigation system, as designed, has head-to-head coverage.
- Finding:** The irrigation system is designed to minimize runoff and overspray.
- Finding:** All zones contain heads with matched precipitation rates.
- Finding:** All zones are plant requirement specific.
- Finding:** The irrigation system has been designed to meet precipitation rate requirements.
- Finding:** No zone has an elevation difference more than 20'.
- Finding:** The irrigation system incorporates bubblers for all trees on a separate zone.
- Finding:** The irrigation system incorporates PRV's where needed to meet City standards.
- Finding:** The irrigation system controllers meet City standards.
- Conclusion:** The proposed scope of work complies.

**J. Maintenance.** All landscaping shall be maintained in good condition, or otherwise replaced by the property owner; dead plants must be replaced within 180 days of discovery. Replacement planting consistent with an approved plan does not require separate City approval.

- Finding:** Landscaping will be maintained to comply with City standards.
- Conclusion:** The proposed scope of work complies.

**18.4.4.040 Recycling and Refuse Disposal Areas** (A.1 is not applicable)

- A. Recycling.** All residential, commercial, and manufacturing developments that are subject to chapter 18.5.2 Site Design Review shall provide an opportunity-to-recycle site for use of the project occupants.
  - 2. Commercial. Commercial developments having a refuse receptacle shall provide a site of equal or greater size adjacent to or with access comparable to the refuse receptacle to accommodate materials collected by the local sanitary service franchisee under its on-route collection program for purposes of recycling.
- B. Service Areas.** Recycling and refuse disposal areas shall be located to provide truck access and shall not be placed within any required front yard or required landscape area.
- C. Screening.** Recycle and refuse disposal area screening shall be provided pursuant to section 18.4.4.030.G.1.

**Finding:** Darex currently provides a large recycle site for cardboard, mixed recyclables, and glass. The proposed addition will make use of the existing recycle site and enlarge as needed.

**Finding:** Recycling site is in a hardscaped area adjacent to the existing building with truck access.

**Finding:** Recycle area is existing and is screened from view by the building and parking lot landscaping.

**Conclusion:** The proposed scope of work complies.

#### **18.4.4.050 Outdoor Lighting**

**A. Purpose.** *This section contains regulations requiring adequate levels of outdoor lighting while minimizing light spillover onto adjacent properties*

**B. Applicability.** *All outdoor lighting is subject to the requirements of this section. Where a proposed development is subject to Type I, Type II, or Type III review, the approval authority may require specific lighting levels or limit lighting as a condition of approval to protect the public health, safety, and welfare.*

**C. Standards.** *As a guideline, lighting levels shall be no greater than necessary to provide for pedestrian safety, property/business identification, and crime prevention. All outdoor lighting, except streetlights, shall comply with the following standards.*

- 1. Arrange and install artificial lighting so there is no direct illumination onto adjacent residential properties.*
- 2. Provide light poles no greater than 14 feet in height for pedestrian facilities. (Pedestal- or bollard-style lighting is an alternative method for illuminating walkways located inside a development but not located in a public street right-of-way.)*
- 3. Where a light standard is placed over a sidewalk or walkway, maintain a minimum vertical clearance of eight feet.*
- 4. Install light fixtures where they will not obstruct public ways, driveways, or walkways. Where a light standard must be placed within a walkway, maintain an unobstructed pedestrian through zone per Americans with Disabilities Act (ADA) compliance.*
- 5. Except as permitted for signs, direct outdoor light fixtures downward and have full shielding to minimize excessive light spillover onto adjacent properties.*
- 6. For streetlight requirements, see subsection 18.4.6.040.D.18.*

**D. Maintenance.** *Outdoor lighting shall be maintained in good condition, or otherwise replaced by the property owner.*

**Finding:** Outdoor lighting will be designed to comply with City standards relative to minimizing spillover onto adjacent properties, and light pole height and placement.

**Finding:** While the site is across the street from a residential zone, the site is not directly adjacent to a residential property.

**Finding:** Outdoor lighting will be maintained to comply with City standards.

**Conclusion:** The proposed scope of work complies.

#### **18.4.4.060 Fences and Walls** (Not applicable)

The proposed work does not incorporate fences or walls. Landscape screens meet City standards.

## 18.4.5 TREE PRESERVATION AND PROTECTION

### **18.4.5.030 Tree Protection**

- A. Tree Protection Plan.** A tree protection plan shall be approved by the Staff Advisor concurrent with applications for Type I, Type II, and Type III planning actions. If tree removal is proposed, a Tree Removal Permit pursuant to chapter 18.5.7 may be required.
- B. Tree Protection Plan Submission Requirements.** In order to obtain approval of a tree protection plan; an applicant shall submit a plan to the City, which clearly depicts all trees to be preserved and/or removed on the site.
- C. Tree Protection Measures Required.** Measures are noted on tree protection plan.
- D. Inspection.** The applicant shall not proceed with any construction activity, except installation of erosion control measures, until the City has inspected and approved the installation of the required tree protection measures and a building and/or grading permit has been issued by the City.

- Finding:** See tree protection plan for proposed removal, and protection measures.
- Finding:** Construction will not proceed until City has approved the required tree protection measures.
- Conclusion:** The proposed scope of work complies.

## 18.5.7 TREE REMOVAL PERMITS

### **18.5.7.040 Approval Criteria** (Only Item B.2 is applicable)

#### **B. Tree Removal Permit.**

- 2. For a Tree That is Not a Hazard. A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.
  - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

- Finding:** This proposal removes two 6 inch dbh trees and one 4.5 inch dbh tree. In all cases the trees to be removed interfere with the proposed circulation improvements for both pedestrian and vehicular traffic. Tree #1 (4.5" dbh) is an ornamental Pear, tree # 2 (6" dbh) is an Armstrong Maple and tree # 3 (6" dbh) is an ornamental Pear. All trees are in fair to good condition, relatively young, and planted in constrained paved areas. The proposed trees and plantings will provide better habitat for the proposed trees as well as more than make up for canopy coverage and species diversity.
- Finding:** The removal of the three trees allows for proper design of parking and circulation consistent with the Land Use Ordinance and Site Development and Design standards.
- Finding:** Removal of the relatively isolated trees (in constrained paved areas) will have no effect on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
- Finding:** Removal of these trees will not have any effect on the surrounding tree densities (within 200' of the property). The trees are small and insignificant in their relationship to the greater environment and to a larger goal of species diversity, and the trees are very common in the urban and sub-urban landscape. The surrounding developed area has many large trees that are of similar species as they are typically used for parking lots and commercial developments.
- Finding:** This is not a residential situation; criteria B.2.d does not apply.
- Finding:** The proposal includes 57 new trees that provide sufficient mitigation.
- Conclusion:** The proposed scope of work complies.

**18.5.7.050 Mitigation Required** (Only item A is applicable)

**A. Replanting On-Site.** *The applicant shall plant either a minimum 1.5-inch caliper healthy and well-branched deciduous tree or a five to six-foot tall evergreen tree for each tree removed. The replanted tree shall be of a species that will eventually equal or exceed the removed tree in size if appropriate for the new location. Larger trees may be required where the mitigation is intended, in part, to replace a visual screen between land uses. Suitable species means the tree's growth habits and environmental requirements are conducive to the site, given existing topography, soils, other vegetation, exposure to wind and sun, nearby structures, overhead wires, etc. The tree shall be planted and maintained per the specifications of the Recommended Street Tree Guide.*

- Finding:** Because this proposal includes planting 57 new trees, significantly more than a typical landscape plan, this provides tree coverage beyond the required mitigation.
- Finding:** Proposed trees are 1.5" and 2" caliper healthy trees which will eventually equal or exceed the removed trees in size.
- Finding:** Proposed species were selected from the Recommended Street Tree Guide.
- Conclusion:** The proposed scope of work complies.

## PROJECT DESIGN CONSIDERATIONS

We are writing to address several planning issues associated with a proposed development by Darex LLC. As you know, Darex is an established company and major employer in Ashland, the epitome of responsible local business that is so vital to Ashland's economic stability. Both short- and long-term growth in Darex's business requires additional space for assembly of their products, as well as additional area for support functions, including storage and offices. The owners chose to relocate their business to Ashland because of its unique character, and their overwhelming desire is to remain in Ashland.

Darex's business is based on the assembly by hand of numerous individual components that are sourced from multiple suppliers. Additionally, Darex sells a wide variety of different products, each of which is assembled by hand at this facility, before being shipped to retailers and consumers around the world. This business model provides living wage jobs to residents from all over Jackson County, including Grants Pass, White City, Central Point, Medford, Phoenix, Talent and Ashland. Part of Darex's ongoing success is their ability to get a large amount of business done in a small space, meaning the staff on site is at a much higher density than typically found. It is important to note that this does not come at the expense of employee happiness, in fact the opposite, Darex has been named one of Oregon's best employers multiple times, spanning decades.

As shown in the attached supporting documentation provided as Exhibit A, the hand assembling process at Darex is extremely labor intensive and relies on a long-term, skilled labor force. Currently, Darex has 129 currently employees which will grow to 139 for the period of August through November, all at this site. The proposed phase 1 expansion of this facility will allow them to increase this number to 226.

The ability to expand Darex's current facility, and remain in Ashland, is contingent upon finding viable solutions to several different planning issues that are outlined below in detail. Some issues are related to Darex's unique, employee-intensive business model. Others issues are beyond their control, the result of development standards enacted after Darex started operations on this site in 1979, 36 years ago. When Darex first purchased the property it was Zoned M1 Industrial, and Hersey Street was a dirt road, which met the criteria for Darex future plans.

### **Design Consideration 1: Parking Ratio**

The type of business conducted by Darex does not fall into any of the established categories of business uses listed in the parking tables. This is not a *warehouse* facility, as products are only stored here for a short time before being shipped out around the world. Similarly, this is not an *industrial* facility, which is based on the use of heavy machinery and mechanized fabrication processes to limit the number of people required to do the work. That is to say, the 'warehouse' and 'industrial' use categories do not properly reflect the number of people required by Darex to assemble products by hand.

Rather than an "assembly line", the hand assembly process at Darex relies on numerous individual workstations, similar to a workbench. These staffed workstations are similar in size (about 100 square feet net) to workstations found in a traditional office environment. Furthermore, the actual head count at the facility (129 currently) is very similar to what it would be if this building was used as an office building. Therefore, we propose to use a parking ratio of 500 square feet per person, the same as the parking ratio for an Office use, to align with the unique hand assembly work performed at Darex. We offer the following additional justification for supporting this design:

- The property is adjacent to the Clear Creek development, which severely limits the amount of off-street parking. It would benefit the city for Darex to keep its staff parking on site, so that future developments on Clear Creek Drive can use the on-street parking.
- The current facility is on a site that was designed according to a much lower “warehouse” type parking ratio, which is out of sync with the actual business type. As Darex has increased its staff over the years, the undeveloped land near Clear Creek has been used for overflow staff parking.
- Although the ordinance code tries to take advantage of on-street parking to help alleviate the requirements for large parking areas, the long, narrow, dual frontage nature of this site does not provide on-street parking in sufficient quantities to provide adequate parking for Darex staff. And this on-street parking is already in extremely high demand by neighboring businesses, which requires employees to park in more remote residential neighborhoods.
- The unique nature of Darex’s business model requires long term skilled workers, most of whom do not live in Ashland. The available bus service would require a commute of up to four hours from Medford and White City. This also makes commuting by bicycle unreasonable, particularly in inclement weather. This leaves independent travel by car as the only viable option.
- Over the years, Darex has provided incentives to its staff to encourage car-pooling, especially when fuel prices were high. Unfortunately, this effort met with no success. They found that their employees have obligations before or after work (e.g. daycare, medical appointment, volunteer commitments), which requires the use of a car.
- If Darex were to sell the property, the most likely uses would be a similar hand assembly operation or a ‘clean tech’ assembly / office use. The requested parking ratio would match the parking required for these future uses, facilitating redevelopment of this site.

### **Design Consideration 2: “Majority” of the street frontage**

This site has some unique constraints, which were not self-imposed by Darex. Rather, they result from the creation of Clear Creek Drive and gave this property a dual frontage. Although the current Darex facility fronts Hersey Street, the owner and design team have developed a site plan that allows for the creation of a future building that will front on Clear Creek Drive. This building will occupy the southernmost portion of the site, the area not occupied by the Darex expansion. Since this building also requires its own parking, that limits the building size and thus its frontage. The current city standard requires a “large majority” of street frontage be dedicated to building façade. We request that the future building occupy a “simple majority” (51% or more) along Clear Creek Drive. We offer the following additional justification for granting this exception:

- The pad lot that is reserved for a future building needs to be a reasonable depth, in order to have any useful purpose. It also needs to be immediately adjacent to the Darex expansion, in case it is used by Darex and needs a physical connection to their building. This means that the parking must be located on the west side, rather than behind it.
- The ordinance calls for site development to include building frontage on a “large majority” of the streetscape. It is not possible to meet this standard, for the reasons outlined above. In previous situations like this one (e.g. Les Schwab), staff has agreed to a simple “majority” of frontage.
- The narrow configuration of this site, coupled with the size and nature of the addition to the existing building, makes it impossible to increase the frontage even further.

### **Design Consideration 3: Clear Creek Drive Curb Cuts and Drive Aprons**

As discussed during our recent Pre-Application conference, this site has some unique constraints, which were not self-imposed by Darex. Rather, they result from the creation of Clear Creek Drive. When Darex bought this property, the site had a single frontage on Hersey Street. However,

RECEIVED

JUL 15 2015

City Of Ashland



July 9, 2015

Dave Ross  
Project Manager  
Adroit Construction Co., Inc  
PO Box 609  
Ashland, Oregon



RENEWS 6/30/17

Subject: Traffic Impact Analysis for Darex  
Expansion, 210 E. Hersey St.

Dear Dave:

This letter confirms that under the City of Ashland threshold standards, a Traffic Impact Analysis (TIA) is not required.

A new manufacturing assembly structure of slightly less than 25,000 square-feet will be added to an existing 40,000 square-foot manufacturing assembly structure. The structure will allow the addition of 97 more employees beyond the current 129.

The City of Ashland has three criteria for determining the need for a TIA: Trip Generation, Mitigation, and Heavy Vehicle Trip Generation. Each is discussed below.

**Trip Generation Threshold:** A TIA is required if there are 50 newly generated trips (inbound and outbound) during the adjacent street peak hour.

The Institute of Transportation Engineers (ITE) *Trip Generation Manual 9<sup>th</sup> Edition* is the industry standard for determining the number of trips generated by a development. The proposed development falls under ITE land use number 140, Manufacturing. The number of trips for a manufacturing use is based on the number of employees.

The equation to calculate the number of trips was derived from over 50 individual studies. It follows a logarithmic distribution curve. The number of trips, per employee, goes down as the number of employees goes up. To understand why,

RECEIVED

JUL 15 2015

City Of Ashland

Traffic Impact Analysis – 210 E. Hersey St  
 July 9, 2015

think of a very small facility that only has two employees. If both of them drive to work, they would generate two peak-hour trips, both inbound, and if there is a single delivery made, there would be an additional trip inbound when it arrived and an outbound trip when it left for a total for the site of 4 trips, two per employee.

If the site had 100 employees, there would be more chances to car pool, more of a chance that some of the commuting would be outside the peak hour and a higher probability that some employees would bike or walk. The delivery that doubled the number of trips in the two person shop would be a minor component of the larger facility.

Because the rate goes down as the number of employees increases, the calculation for the incremental number of trips for a site addition requires the subtraction of the number of trips prior to the expansion from the number of trips after the expansion. The table below shows the calculation for the original site with 129 employees and then site after it has expanded by 97 employees to 226 total employees.

Darex Expansion Trip Generation

Land Use Type	Land Use Code	Variable	PM Peak Hour of Adjacent Street						AM Peak Hour of Adjacent Street					
			Peak Hour		Inbound		Outbound		Peak Hour		Inbound		Outbound	
			Rate	Trips	Split	Trips	Split	Trips	Rate	Trips	Split	Trips	Split	Trips
Proposed Land Use		Employees												
Manufacturing Existing	140	129	Curve 1	67	73%	49	27%	18	Curve 2	68	80%	54	20%	14
Manufacturing Future	140	226	Curve 1	107	73%	78	27%	29	Curve 2	107	80%	86	20%	21
Increase		97		41		30		11		40		32		8

T= Number of Trips in time period

X = Number of Employees

Curve 1  $\ln(T) = 0.85 * \ln(X) + 0.07$

Curve 2  $\ln(T) = 0.78 * \ln(X) + 0.48$

The calculated 41 PM peak-hour trips and 40 AM peak-hour trips are less than the threshold 50 trips and therefore a TIA is not needed under the Trip Generation Threshold.

**Mitigation Threshold:** This threshold is met if the project includes the installation of any traffic control device or any geometric improvements that will affect the progression or operation of traffic entering or exiting the driveway.

Traffic Impact Analysis – 210 E. Hersey St  
July 9, 2015

The project does not include the installation of any traffic control device or geometric improvements and therefore a TIA is not needed under the Mitigation Threshold.

**Heavy Vehicle Trip Generation Threshold:** This threshold is met if the project generates 20 newly generated heavy vehicle trips (inbound and outbound) during the day.

The site currently generates 25 trucks per week. This is a daily trip rate average of 10 heavy vehicle trips (inbound and outbound) per day. No additional trips are anticipated as part of this expansion. This falls below the threshold for heavy vehicle trip generation and therefore a TIA is not needed under the Heavy Vehicle Trip Generation Criteria.

**Summary:** The proposed expansion of the manufacturing facility on 210 E. Hersey St. does not meet any of the threshold criteria and therefore the requirement for a Traffic Impact Analysis is not met.

Please let me know if you have any questions.

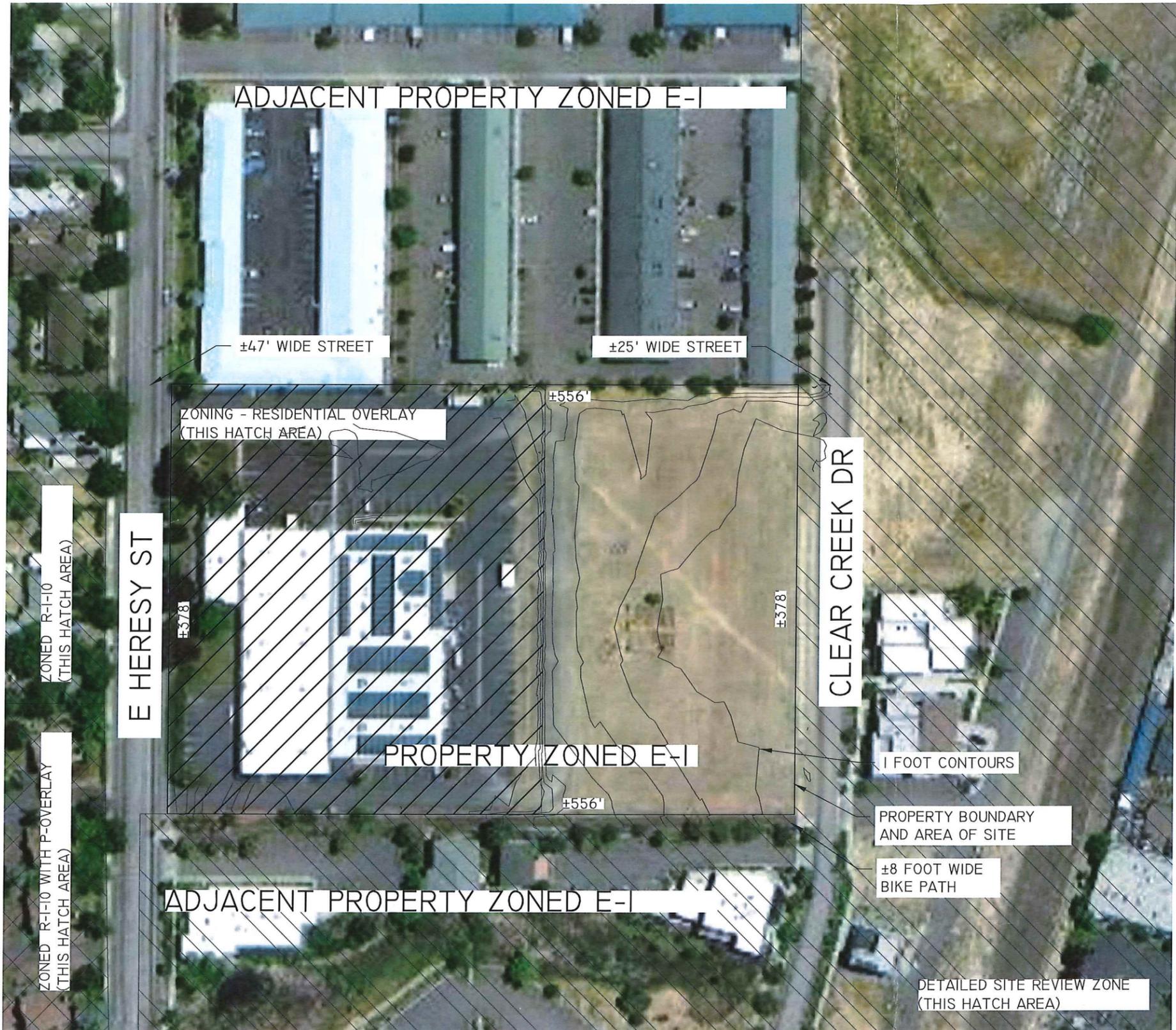
Very truly yours,

James R. Hanks, PE

RECEIVED

JUL 15 2015

City Of Ashland



prepared by:  
 OgdenRoemerWilkerson Architecture, AIA  
 2950 East Barnett Road  
 Medford, Oregon 97504  
 contact: David Wilkerson  
 david@orwarch.com  
 tel: 541-779-5237 x20

deed names and address:  
 Bernard, David A; Trustee and Bernard, Marjorie A, Trustee FBO; Bernard Family Trust  
 210 E Hersey St Ashland, Oregon 97520

statistics:  
 address: 210 E HERSEY ST ASHLAND  
 map #: 391E04CD  
 tax lot#: 2000  
 zoning: E1  
 overlay: PARTIAL RESIDENTIAL OVERLAY  
 site review: BASIC ZONE

setbacks  
 front yard: 25 ft  
 side yard street: None required  
 side yard: None required  
 rear yard: None required

**PROPOSED PROJECT NARRATIVE:**

Darex is intending to add to their existing structure towards the rear of the Darex property.  
 Structure 1 will extend and expand the current facilities. Proposed materials are: Metal siding and metal standing seam roof.  
 Phase 2 will be a future development to be determined by the owner.

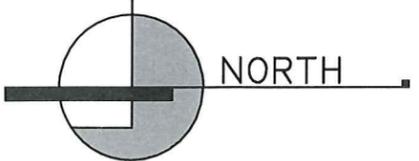


RECEIVED  
 AUG 17 2015  
 City Of Ashland

SITE

**VICINITY MAP**

NO SCALE



COLLABORATIVE APPROACH - HONEST DESIGN

SITE ANALYSIS MAP  
**darex expansion**  
 210 hersey street  
 7 July 2015  
 © 2013 OgdenRoemerWilkerson Architecture

**PROPERTY ZONING DIAGRAM**

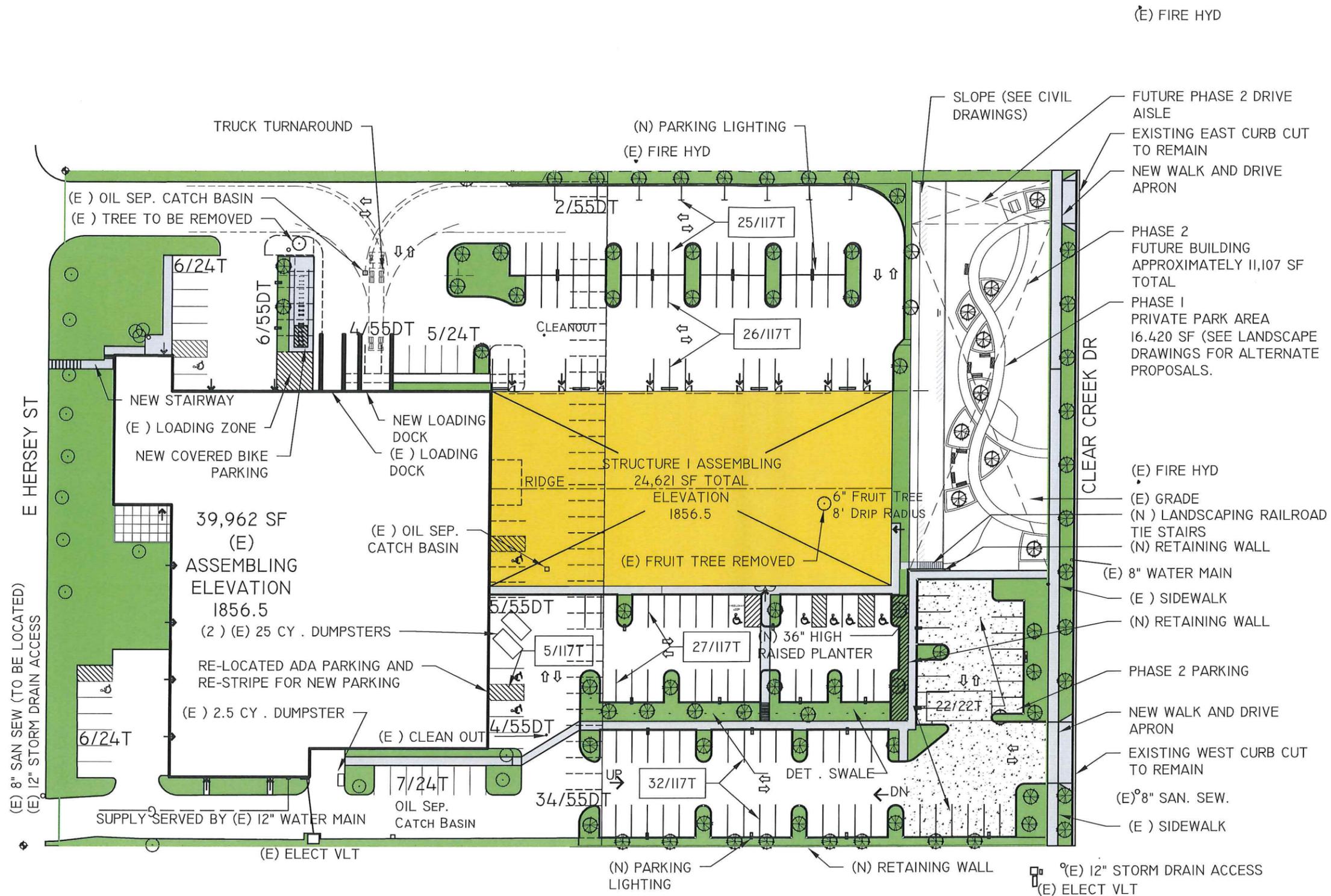
SCALE: 1"=100'0"

- NOTES:  
 1. PROPERTY ZONED IN BASIC SITE REVIEW.  
 2. MAP IS ORIENTED TO ALIGN WITH ATTACHED SITE PLAN.



PROPOSED SITE PLAN  
**darex expansion**  
 210 hersey street

7 July 2015  
 © 2013 OgdenKosmerWilkerson Architecture



(E) FIRE HYD

- PROPERTY LINE
- RETAINING WALL
- ☼ DROUGHT RESISTANT PLANTS
- ⊙ NEW LANDSCAPE TREE
- NEW CONC WALK
- NEW SOFTSCAPE
- NEW WATER RETENTION
- ▨ PAINTED STRIPING
- ▨ PARKING FOR FUTURE PHASE 2
- ➔ DOOR/ENTRANCE

0/0T	REMAINING PARKING IN ROW/ PARKING TOTAL
0/0T	NEW PARKING IN ROW/ PARKING TOTAL
0/0DT	PARKING IN ROW/ DEMO PARKING TOTAL

PHASE 1 parking (ord 18.92.030)	
existing parking	79
demolished parking	-55
remaining	24

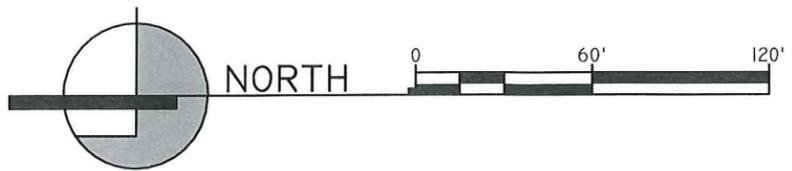
new proposed parking	
total proposed parking	117

[accessible spaces: 5 req./ 1 Wheelchair accessible]	
	6

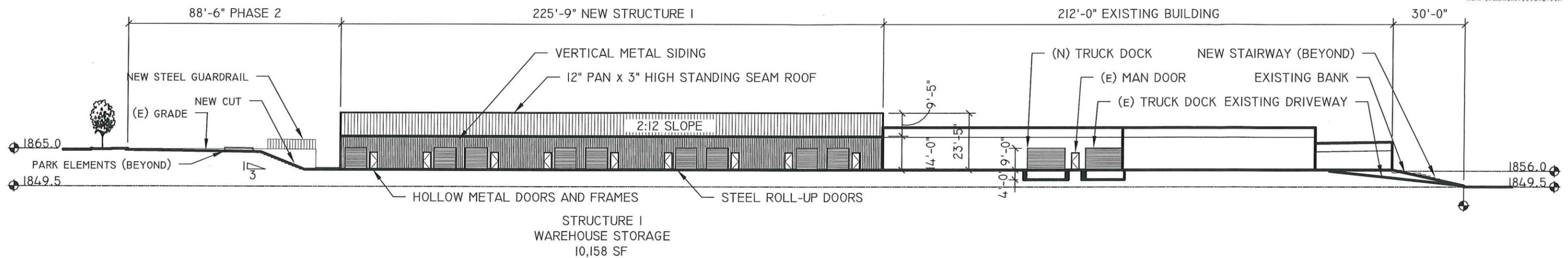
PHASE 2 parking (ord 18.92.030)	
new proposed parking	22

[accessible spaces: 1 req./ 1 Wheelchair accessible]	
	1

Phase 1 Parking range is 129-142 max  
 Phase 2 Parking range is 22-24 max

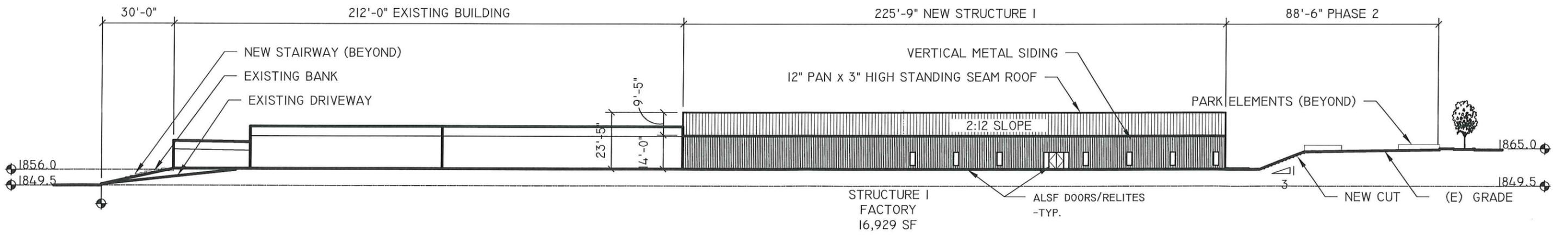


COLLABORATIVE APPROACH - HONEST DESIGN



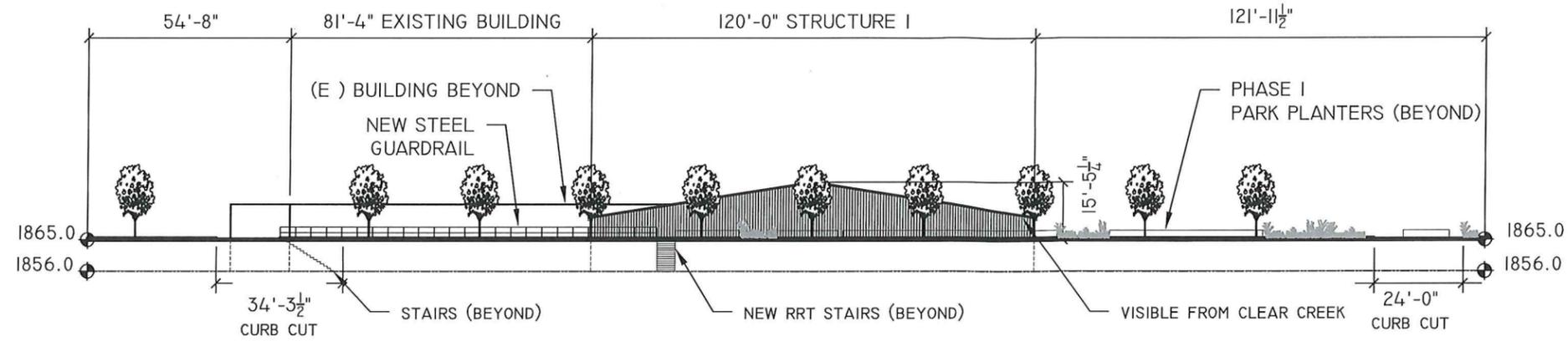
**1 EAST ELEVATION**

SCALE: 1"=40'-0"

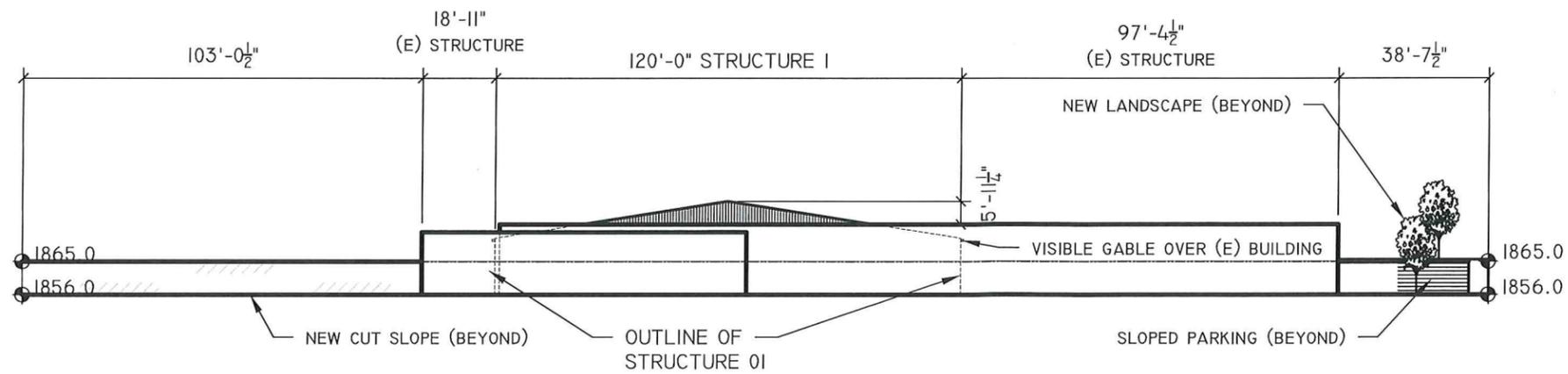


**2 WEST ELEVATION**

SCALE: 1"=40'-0"

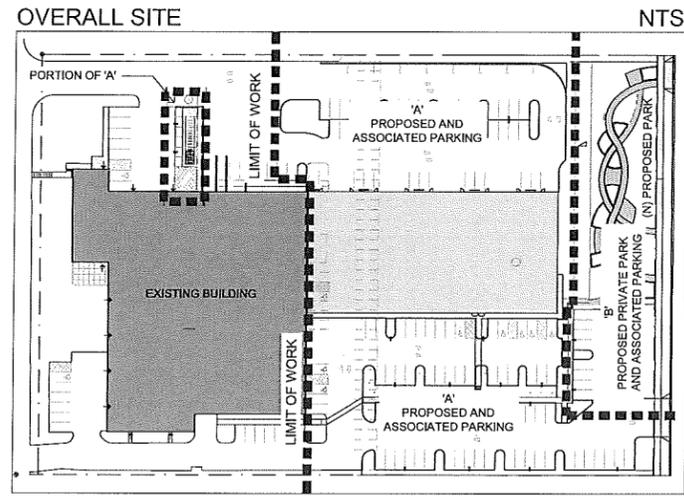
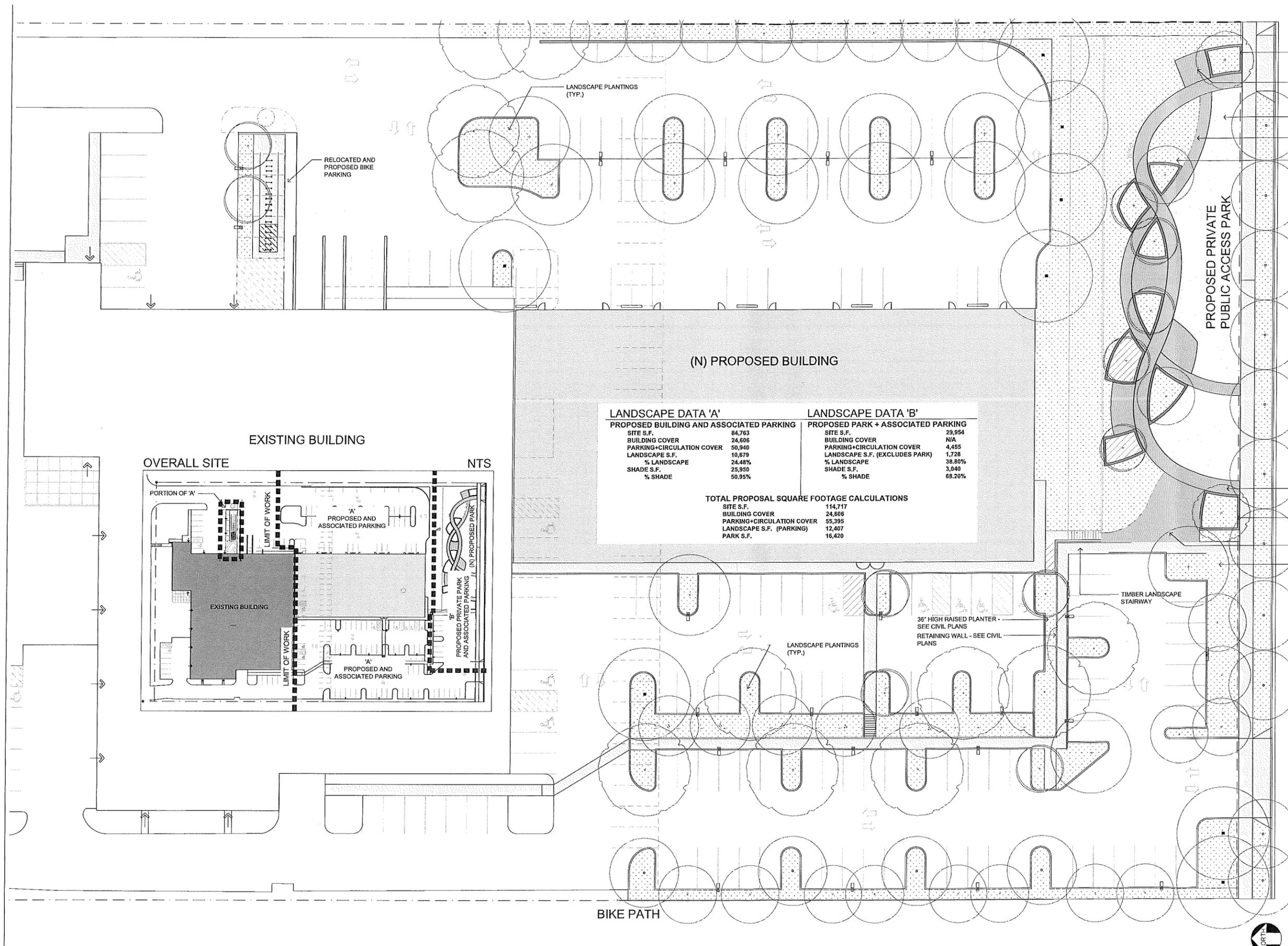


**1** SOUTH ELEVATION  
 SCALE: 1"=40'-0"



**2** NORTH ELEVATION  
 SCALE: 1"=40'-0"

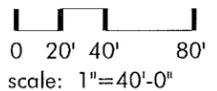




LANDSCAPE DATA 'A'		LANDSCAPE DATA 'B'	
<b>PROPOSED BUILDING AND ASSOCIATED PARKING</b>		<b>PROPOSED PARK + ASSOCIATED PARKING</b>	
SITE S.F.	84,763	SITE S.F.	29,954
BUILDING COVER	24,606	BUILDING COVER	N/A
PARKING+CIRCULATION COVER	50,940	PARKING+CIRCULATION COVER	4,455
LANDSCAPE S.F.	10,679	LANDSCAPE S.F. (EXCLUDES PARK)	1,728
% LANDSCAPE	24.48%	% LANDSCAPE	38.80%
SHADE S.F.	25,950	SHADE S.F.	3,040
% SHADE	50.95%	% SHADE	68.20%
<b>TOTAL PROPOSAL SQUARE FOOTAGE CALCULATIONS</b>			
SITE S.F.	114,717		
BUILDING COVER	24,606		
PARKING+CIRCULATION COVER	55,395		
LANDSCAPE S.F. (PARKING)	12,407		
PARK S.F.	16,420		

- 30" HIGH VERSALOK WALL RAISED PLANTER (TYP.)
- CRUSHED GRANITE PAVING (TYP.)
- LOW WATER USE LAWN
- CRUSHED GRANITE PATH (TYP.)
- LANDSCAPE PLANTINGS (TYP.)

- CRUSHED GRANITE PATH (TYP.)
- 30" HIGH VERSALOK WALL RAISED PLANTER (TYP.)
- CRUSHED GRANITE PAVING (TYP.)
- LANDSCAPE PLANTINGS (TYP.)



**KenCairn**  
Landscape Architecture



545 A ST., STE 3, ASHLAND, OR 97520  
541.545.1455



Drawn By:  
STAFF

SCALE 1" = 40'-0"

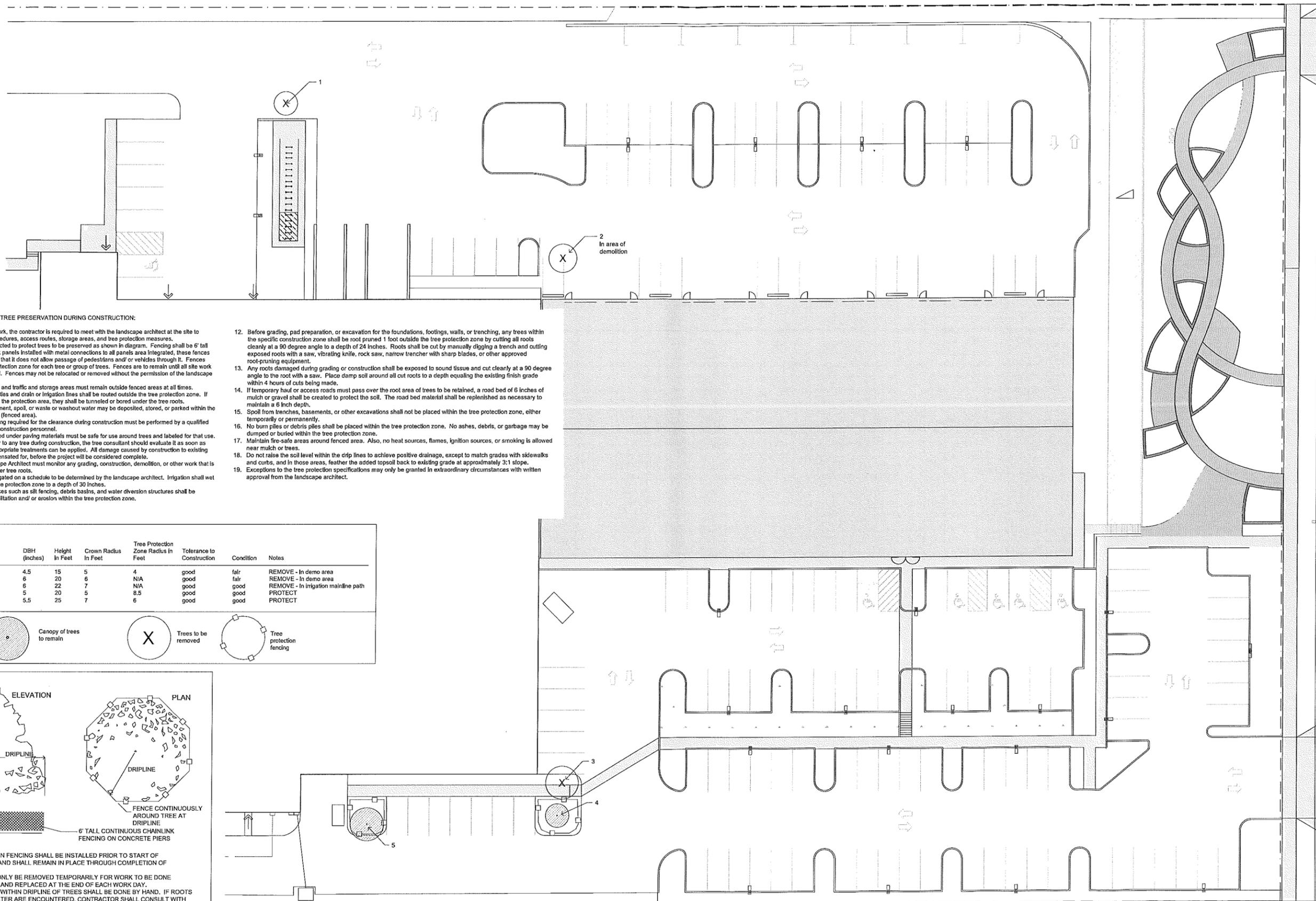
**DAREX EXPANSION**  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

**LANDSCAPE  
SITE  
PLAN**

ISSUE DATE:  
JULY 6, 2015

**L 1.0**

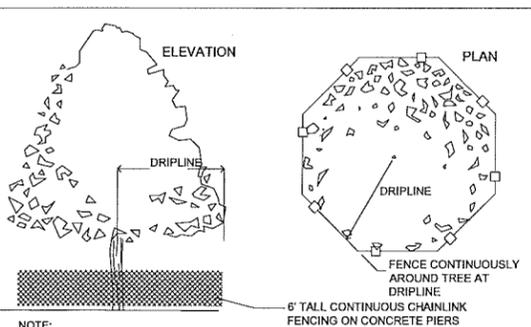
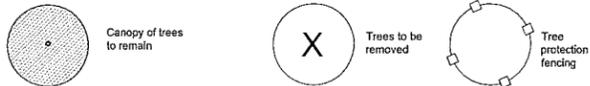


**SPECIFICATIONS FOR TREE PRESERVATION DURING CONSTRUCTION:**

- Before beginning work, the contractor is required to meet with the landscape architect at the site to review all work procedures, access routes, storage areas, and tree protection measures.
- Fences must be erected to protect trees to be preserved as shown in diagram. Fencing shall be 6' tall temporary chain link panels installed with metal connections to all panels area integrated. These fences shall be installed so that it does not allow passage of pedestrians and/or vehicles through it. Fences define a specific protection zone for each tree or group of trees. Fences are to remain until all site work has been completed. Fences may not be relocated or removed without the permission of the landscape architect.
- Construction trailers and traffic and storage areas must remain outside fenced areas at all times.
- All underground utilities and drain or irrigation lines shall be routed outside the tree protection zone. If lines must traverse the protection area, they shall be tunneled or bored under the tree roots.
- No materials, equipment, spoil, or waste or washout water may be deposited, stored, or parked within the tree protection zone (fenced area).
- Additional tree pruning required for the clearance during construction must be performed by a qualified arborist and not by construction personnel.
- Any herbicides placed under paving materials must be safe for use around trees and labeled for that use.
- If injury should occur to any tree during construction, the tree consultant should evaluate it as soon as possible so that appropriate treatments can be applied. All damage caused by construction to existing trees shall be compensated for, before the project will be considered complete.
- The project Landscape Architect must monitor any grading, construction, demolition, or other work that is expected to encounter tree roots.
- All trees shall be irrigated on a schedule to be determined by the landscape architect. Irrigation shall wet the soil within the tree protection zone to a depth of 30 inches.
- Erosion control devices such as silt fencing, debris basins, and water diversion structures shall be installed to prevent siltation and/or erosion within the tree protection zone.
- Before grading, pad preparation, or excavation for the foundations, footings, walls, or trenching, any trees within the specific construction zone shall be root pruned 1 foot outside the tree protection zone by cutting all roots cleanly at a 90 degree angle to a depth of 24 inches. Roots shall be cut by manually digging a trench and cutting exposed roots with a saw, vibrating knife, rock saw, narrow trencher with sharp blades, or other approved root-pruning equipment.
- Any roots damaged during grading or construction shall be exposed to sound tissue and cut cleanly at a 90 degree angle to the root with a saw. Place damp soil around all cut roots to a depth equaling the existing finish grade within 4 hours of cuts being made.
- If temporary haul or access roads must pass over the root area of trees to be retained, a road bed of 6 inches of mulch or gravel shall be created to protect the soil. The road bed material shall be replenished as necessary to maintain a 6 inch depth.
- Spill from benches, basements, or other excavations shall not be placed within the tree protection zone, either temporarily or permanently.
- No burn piles or debris piles shall be placed within the tree protection zone. No ashes, debris, or garbage may be dumped or buried within the tree protection zone.
- Maintain fire-safe areas around fenced area. Also, no heat sources, flames, ignition sources, or smoking is allowed near mulch or trees.
- Do not raise the soil level within the drip lines to achieve positive drainage, except to match grades with sidewalks and curbs, and in those areas, feather the added topsoil back to existing grade at approximately 3:1 slope.
- Exceptions to the tree protection specifications may only be granted in extraordinary circumstances with written approval from the landscape architect.

**TREE LEGEND**

#	Species	DBH (inches)	Height In Feet	Crown Radius In Feet	Tree Protection Zone Radius In Feet	Tolerance to Construction	Condition	Notes
1	Pyrus sp.	4.5	15	5	4	good	fair	REMOVE - In demo area
2	Acer r. 'Armstrong'	6	20	6	N/A	good	fair	REMOVE - In demo area
3	Pyrus sp.	6	22	7	N/A	good	good	REMOVE - In irrigation mainline path
4	Acer r. 'Armstrong'	5	20	5	8.5	good	good	PROTECT
5	Acer r. 'Armstrong'	5.5	25	7	6	good	good	PROTECT



- NOTE:**
- TREE PROTECTION FENCING SHALL BE INSTALLED PRIOR TO START OF CONSTRUCTION AND SHALL REMAIN IN PLACE THROUGH COMPLETION OF PROJECT.
  - FENCING SHALL ONLY BE REMOVED TEMPORARILY FOR WORK TO BE DONE WITHIN DRIPLINE AND REPLACED AT THE END OF EACH WORK DAY.
  - ALL EXCAVATION WITHIN DRIPLINE OF TREES SHALL BE DONE BY HAND. IF ROOTS OVER 2" IN DIAMETER ARE ENCOUNTERED, CONTRACTOR SHALL CONSULT WITH LANDSCAPE ARCHITECT OR ARBORIST BEFORE PROCEEDING.
  - TREE ROOTS ENCOUNTERED DURING CONSTRUCTION, SHALL BE CUT CLEANLY AT A 90 DEGREE ANGLE AND PACKED WITH DAMP SOIL IMMEDIATELY.
  - DURING CONSTRUCTION ALL TREES TO REMAIN SHALL BE IRRIGATED ON A WEEKLY BASIS OR AS NECESSARY WITH LEAKY PIPE ENCIRCLING THE TREE FROM TRUNK OUT TO DRIP LINE.

1 TREE PROTECTION



www.KenCairnLandscape.com  
545 A ST, STE 3, ASHLAND, OR 97520  
541.545.1465



Drawn By:  
STAFF

SCALE 1" = 40'-0"

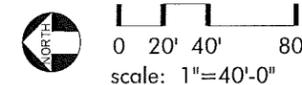
**DAREX EXPANSION**  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

**TREE PROTECTION PLAN**

ISSUE DATE:  
JULY 6, 2015

**L 1.1**





www.KenCairnLandscape.com  
545 A ST, STE 3, ASHLAND, OR 97520  
541.545.1455



Drawn By:  
STAFF

SCALE 1" = 40'-0"

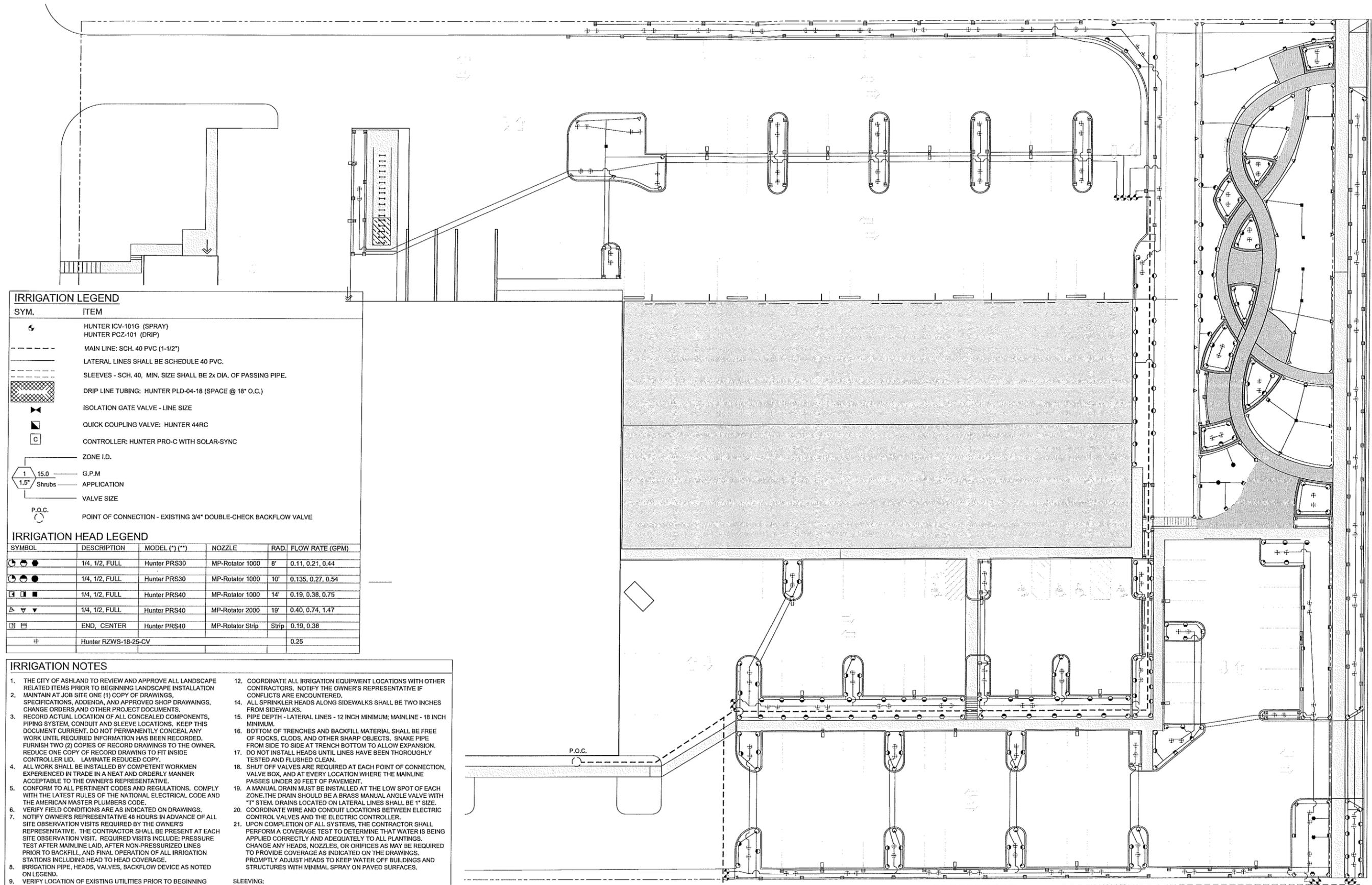
**DAREX EXPANSION**  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

**LANDSCAPE  
IRRIGATION  
PLAN**

ISSUE DATE:  
JULY 6, 2015

**L 2.0**



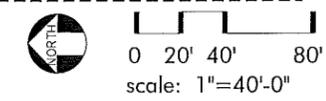
**IRRIGATION LEGEND**

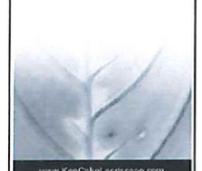
SYM.	ITEM
	HUNTER ICV-101G (SPRAY) HUNTER PCZ-101 (DRIP)
	MAIN LINE: SCH. 40 PVC (1-1/2")
	LATERAL LINES SHALL BE SCHEDULE 40 PVC.
	SLEEVES - SCH. 40, MIN. SIZE SHALL BE 2x DIA. OF PASSING PIPE.
	DRIP LINE TUBING: HUNTER PLD-04-18 (SPACE @ 18" O.C.)
	ISOLATION GATE VALVE - LINE SIZE
	QUICK COUPLING VALVE: HUNTER 44RC
	CONTROLLER: HUNTER PRO-C WITH SOLAR-SYNC
	ZONE I.D.
	G.P.M. APPLICATION 15.0 Shrubs
	VALVE SIZE
	POINT OF CONNECTION - EXISTING 3/4" DOUBLE-CHECK BACKFLOW VALVE

**IRRIGATION HEAD LEGEND**

SYMBOL	DESCRIPTION	MODEL ("")	NOZZLE	RAD.	FLOW RATE (GPM)
	1/4, 1/2, FULL	Hunter PRS30	MP-Rotator 1000	8"	0.11, 0.21, 0.44
	1/4, 1/2, FULL	Hunter PRS30	MP-Rotator 1000	10"	0.135, 0.27, 0.54
	1/4, 1/2, FULL	Hunter PRS40	MP-Rotator 1000	14"	0.19, 0.38, 0.75
	1/4, 1/2, FULL	Hunter PRS40	MP-Rotator 2000	19"	0.40, 0.74, 1.47
	END, CENTER	Hunter PRS40	MP-Rotator Strip	Strip	0.19, 0.38
		Hunter RZWS-18-25-CV			0.25

- IRRIGATION NOTES**
- THE CITY OF ASHLAND TO REVIEW AND APPROVE ALL LANDSCAPE RELATED ITEMS PRIOR TO BEGINNING LANDSCAPE INSTALLATION
  - MAINTAIN AT JOB SITE ONE (1) COPY OF DRAWINGS, SPECIFICATIONS, ADDENDA, AND APPROVED SHOP DRAWINGS, CHANGE ORDERS AND OTHER PROJECT DOCUMENTS.
  - RECORD ACTUAL LOCATION OF ALL CONCEALED COMPONENTS, PIPING SYSTEM, CONDUIT AND SLEEVE LOCATIONS. KEEP THIS DOCUMENT CURRENT. DO NOT PERMANENTLY CONCEAL ANY WORK UNTIL REQUIRED INFORMATION HAS BEEN RECORDED. FURNISH TWO (2) COPIES OF RECORD DRAWINGS TO THE OWNER. REDUCE ONE COPY OF RECORD DRAWING TO FIT INSIDE CONTROLLER LID. LAMINATE REDUCED COPY.
  - ALL WORK SHALL BE INSTALLED BY COMPETENT WORKMEN EXPERIENCED IN TRADE IN A NEAT AND ORDERLY MANNER ACCEPTABLE TO THE OWNER'S REPRESENTATIVE.
  - CONFORM TO ALL PERTINENT CODES AND REGULATIONS. COMPLY WITH THE LATEST RULES OF THE NATIONAL ELECTRICAL CODE AND THE AMERICAN MASTER PLUMBERS CODE.
  - VERIFY FIELD CONDITIONS ARE AS INDICATED ON DRAWINGS.
  - NOTIFY OWNER'S REPRESENTATIVE 48 HOURS IN ADVANCE OF ALL SITE OBSERVATION VISITS REQUIRED BY THE OWNER'S REPRESENTATIVE. THE CONTRACTOR SHALL BE PRESENT AT EACH SITE OBSERVATION VISIT. REQUIRED VISITS INCLUDE: PRESSURE TEST AFTER MAINLINE LAID, AFTER NON-PRESSURIZED LINES PRIOR TO BACKFILL, AND FINAL OPERATION OF ALL IRRIGATION STATIONS INCLUDING HEAD TO HEAD COVERAGE.
  - IRRIGATION PIPE, HEADS, VALVES, BACKFLOW DEVICE AS NOTED ON LEGEND.
  - VERIFY LOCATION OF EXISTING UTILITIES PRIOR TO BEGINNING WORK.
  - PIPING LAYOUT IS DIAGRAMMATIC ONLY. ROUTE PIPING IN PLANTERS AND AVOID UTILITIES AND STRUCTURES. LAYOUT SHALL FOLLOW AS CLOSELY AS PRACTICAL THE SCHEMATIC DESIGN ON THE DRAWINGS. MAKE NO SUBSTANTIAL CHANGES WITHOUT PRIOR APPROVAL FROM THE OWNER'S REPRESENTATIVE.
  - ALL LATERAL PIPE SIZES ARE INDICATED ON THE PLAN
  - COORDINATE ALL IRRIGATION EQUIPMENT LOCATIONS WITH OTHER CONTRACTORS. NOTIFY THE OWNER'S REPRESENTATIVE IF CONFLICTS ARE ENCOUNTERED.
  - ALL SPRINKLER HEADS ALONG SIDEWALKS SHALL BE TWO INCHES FROM SIDEWALKS.
  - PIPE DEPTH - LATERAL LINES - 12 INCH MINIMUM; MAINLINE - 18 INCH MINIMUM.
  - BOTTOM OF TRENCHES AND BACKFILL MATERIAL SHALL BE FREE OF ROCKS, CLODS, AND OTHER SHARP OBJECTS. SNAKE PIPE FROM SIDE TO SIDE AT TRENCH BOTTOM TO ALLOW EXPANSION.
  - DO NOT INSTALL HEADS UNTIL LINES HAVE BEEN THOROUGHLY TESTED AND FLUSHED CLEAN.
  - SHUT OFF VALVES ARE REQUIRED AT EACH POINT OF CONNECTION, VALVE BOX, AND AT EVERY LOCATION WHERE THE MAINLINE PASSES UNDER 20 FEET OF PAVEMENT.
  - A MANUAL DRAIN MUST BE INSTALLED AT THE LOW SPOT OF EACH ZONE. THE DRAIN SHOULD BE A BRASS MANUAL ANGLE VALVE WITH 1" STEM. DRAINS LOCATED ON LATERAL LINES SHALL BE 1" SIZE.
  - COORDINATE WIRE AND CONDUIT LOCATIONS BETWEEN ELECTRIC CONTROL VALVES AND THE ELECTRIC CONTROLLER.
  - UPON COMPLETION OF ALL SYSTEMS, THE CONTRACTOR SHALL PERFORM A COVERAGE TEST TO DETERMINE THAT WATER IS BEING APPLIED CORRECTLY AND ADEQUATELY TO ALL PLANTINGS. CHANGE ANY HEADS, NOZZLES, OR ORIFICES AS MAY BE REQUIRED TO PROVIDE COVERAGE AS INDICATED ON THE DRAWINGS. PROMPTLY ADJUST HEADS TO KEEP WATER OFF BUILDINGS AND STRUCTURES WITH MINIMAL SPRAY ON PAVED SURFACES.
- SLEEVING:**
- CONTRACTOR SHALL VERIFY SLEEVING LOCATIONS AND COORDINATE WITH THE OWNER'S REPRESENTATIVE. SLEEVES ARE TO BE PROVIDED BY GENERAL CONTRACTOR.
  - COORDINATE THE INSTALLATION OF ELECTRICAL SERVICE AND CONDUIT TO THE LOCATION OF THE PROPOSED IRRIGATION CONTROLLER.





www.KenCairnLandscape.com  
545 A ST, STE 3, ASHLAND, OR 97520  
541.545.1465



Drawn By:  
**STAFF**

SCALE 1" = 40'-0"

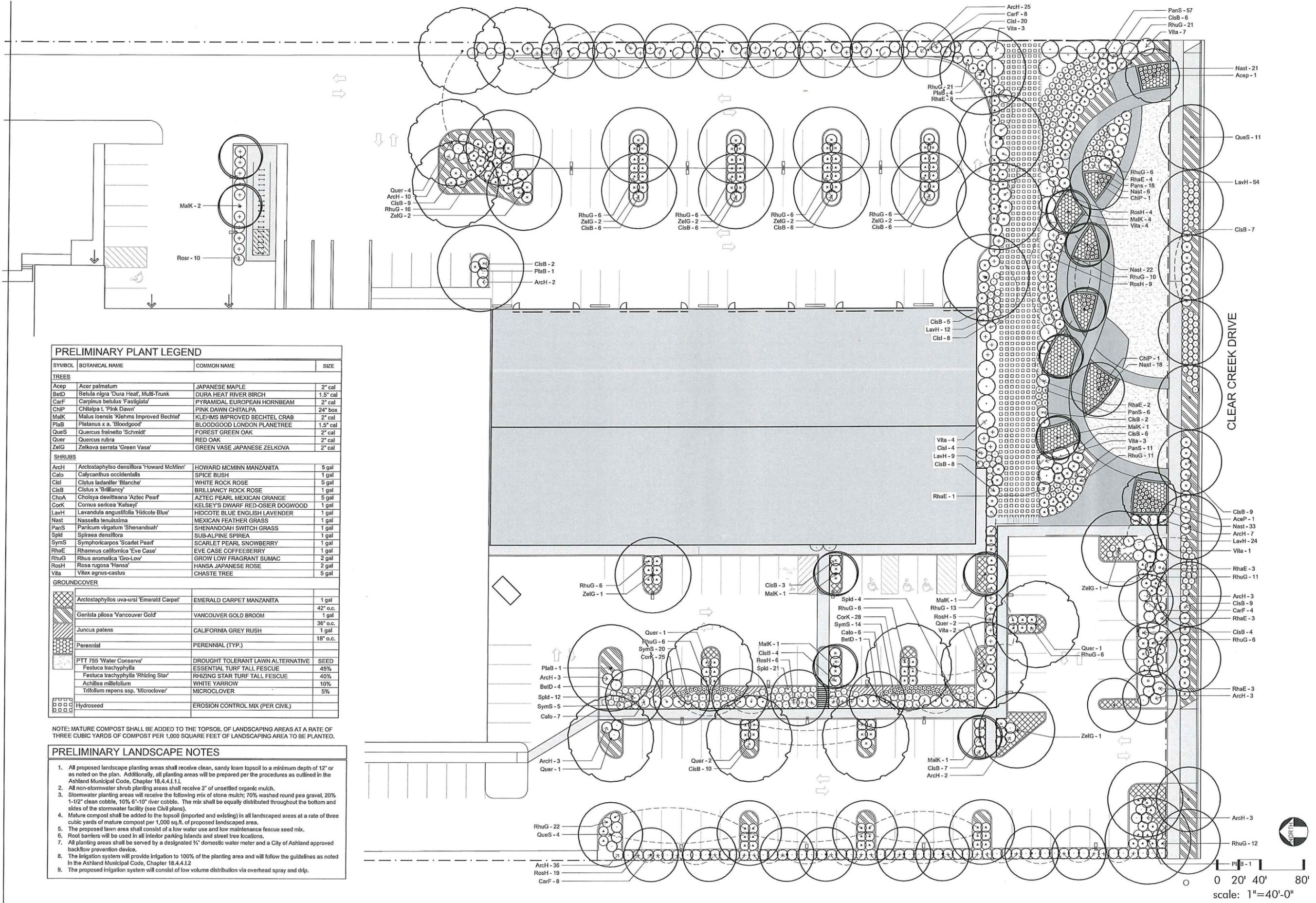
**DAREX EXPANSION**  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

**LANDSCAPE PLANTING PLAN**

ISSUE DATE:  
JULY 6, 2015

**L 3.0**



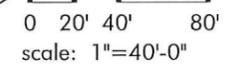
**PRELIMINARY PLANT LEGEND**

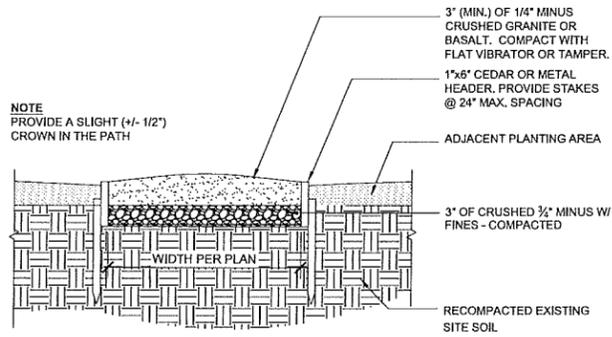
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
<b>TREES</b>			
Acep	Acer palmatum	JAPANESE MAPLE	2" cal
BetD	Betula nigra 'Dura Heat', Multi-Trunk	DURA HEAT RIVER BIRCH	1.5" cal
CarF	Carpinus betulus 'Fastigiata'	PYRAMIDAL EUROPEAN HORNBEAM	2" cal
ChiP	Chitalpa L 'Pink Dawn'	PINK DAWN CHITALPA	24" box
MalK	Malus ioensis 'Klehms Improved Bechtel'	KLEHMS IMPROVED BECHTEL CRAB	2" cal
PlaB	Platanus x a. 'Bloodgood'	BLOODGOOD LONDON PLANETREE	1.5" cal
QueS	Quercus frainetto 'Schmidt'	FOREST GREEN OAK	2" cal
Quer	Quercus rubra	RED OAK	2" cal
ZelG	Zelkova serrata 'Green Vase'	GREEN VASE JAPANESE ZELKOVA	2" cal
<b>SHRUBS</b>			
Arch	Arctostaphylos densiflora 'Howard McMinn'	HOWARD MCMINN MANZANITA	5 gal
Calo	Calycanthus occidentalis	SPICE BUSH	1 gal
Cisl	Cistus ladanifer 'Blanche'	WHITE ROCK ROSE	5 gal
CisB	Cistus x 'Brillianty'	BRILLIANTY ROCK ROSE	1 gal
ChoA	Cholsya dewitteana 'Aztec Pearl'	AZTEC PEARL MEXICAN ORANGE	5 gal
Cork	Cornus sericea 'Kelsey'	KELSEY'S DWARF RED-OSIER DOGWOOD	1 gal
LavH	Lavandula angustifolia 'Hidcole Blue'	HIDCOTE BLUE ENGLISH LAVENDER	1 gal
Nast	Nassella tenuissima	MEXICAN FEATHER GRASS	1 gal
PanS	Panicum virgatum 'Shenandoah'	SHENANDOAH SWITCH GRASS	1 gal
Spld	Spiraea densiflora	SUB-ALPINE SPIREA	1 gal
SymS	Symphoricarpos 'Scarlet Pearl'	SCARLET PEARL SNOWBERRY	1 gal
RhaE	Rhamnus californica 'Eve Case'	EVE CASE COFFEEBERRY	1 gal
RhuG	Rhus aromatica 'Gro-Low'	GROW LOW FRAGRANT SUMAC	2 gal
RosH	Rosa rugosa 'Hansa'	HANSA JAPANESE ROSE	2 gal
Vita	Vitex agnus-castus	CHASTE TREE	5 gal
<b>GROUND COVER</b>			
[Symbol]	Arctostaphylos uva-ursi 'Emerald Carpet'	EMERALD CARPET MANZANITA	1 gal
[Symbol]	Genista pilosa 'Vancouver Gold'	VANCOUVER GOLD BROOM	42" o.c.
[Symbol]	Juncus patens	CALIFORNIA GREY RUSH	1 gal
[Symbol]	Perennial	PERENNIAL (TYP.)	18" o.c.
[Symbol]	PTT 755 'Water Conserve'	DROUGHT TOLERANT LAWN ALTERNATIVE	SEED
[Symbol]	Festuca trachyphylla	ESSENTIAL TURF TALL FESCUE	45%
[Symbol]	Festuca trachyphylla 'Rhizing Star'	RHIZING STAR TURF TALL FESCUE	40%
[Symbol]	Achillea millefolium	WHITE YARROW	10%
[Symbol]	Trifolium repens ssp. 'Microclover'	MICROCLOVER	5%
[Symbol]	Hydeseed	EROSION CONTROL MIX (PER CIVIL)	

NOTE: MATURE COMPOST SHALL BE ADDED TO THE TOPSOIL OF LANDSCAPING AREAS AT A RATE OF THREE CUBIC YARDS OF COMPOST PER 1,000 SQUARE FEET OF LANDSCAPING AREA TO BE PLANTED.

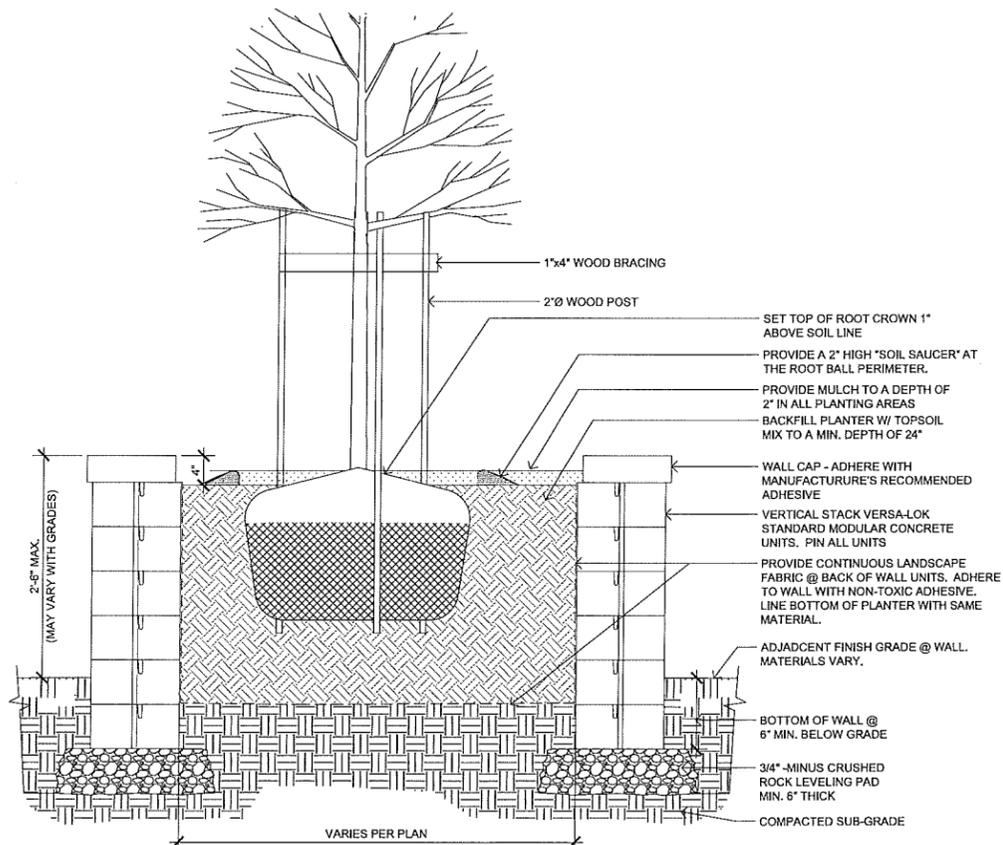
**PRELIMINARY LANDSCAPE NOTES**

- All proposed landscape planting areas shall receive clean, sandy loam topsoil to a minimum depth of 12" or as noted on the plan. Additionally, all planting areas will be prepared per the procedures as outlined in the Ashland Municipal Code, Chapter 18.4.4.1.1.
- All non-stormwater shrub planting areas shall receive 2" of unsegregated organic mulch.
- Stormwater planting areas will receive the following mix of stone mulch: 70% washed round pea gravel, 20% 1-1/2" clean cobble, 10% 6"-10" river cobble. The mix shall be equally distributed throughout the bottom and sides of the stormwater facility (see Civil plans).
- Mature compost shall be added to the topsoil (imported and existing) in all landscaped areas at a rate of three cubic yards of mature compost per 1,000 sq.ft. of proposed landscaped area.
- The proposed lawn area shall consist of a low water use and low maintenance fescue seed mix.
- Root barriers will be used in all interior parking islands and street tree locations.
- All planting areas shall be served by a designated 1/2" domestic water meter and a City of Ashland approved backflow prevention device.
- The irrigation system will provide irrigation to 100% of the planting area and will follow the guidelines as noted in the Ashland Municipal Code, Chapter 18.4.4.1.2.
- The proposed irrigation system will consist of low volume distribution via overhead spray and drip.





1 SECTION - CRUSHED GRANITE PATH  
Scale: N.T.S.



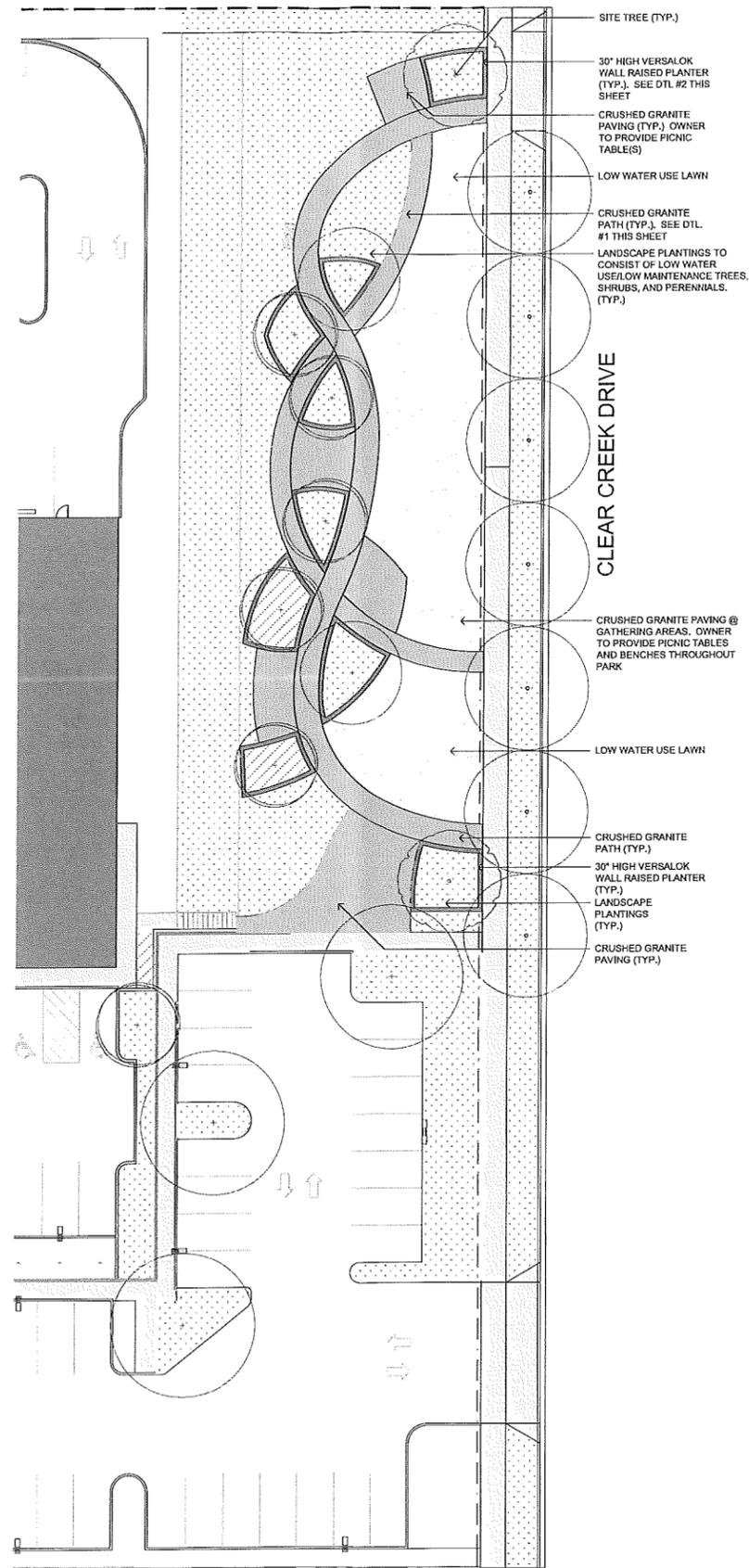
2 SECTION - RAISED VERSA-LOK PLANTER  
Scale: 1" = 1'-0"



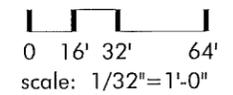
VERTICAL STACK VERSA-LOK PLANTER



LOW WATER USE LAWN & CRUSHED GRANITE PATH



3 PLAN - MINI-PARK (PRIVATE W/ PUBLIC ACCESS)  
Scale: 1/32" = 1'-0"



Drawn By:  
STAFF

SCALE 1" = 40'-0"

DAREX EXPANSION  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

LANDSCAPE  
EXHIBIT

ISSUE DATE:  
JULY 6, 2015

L 4.0



545 A ST, STE 3, ASHLAND, OR 97520  
541.545.1465



Drawn By:  
STAFF

SCALE 1" = 40'-0"

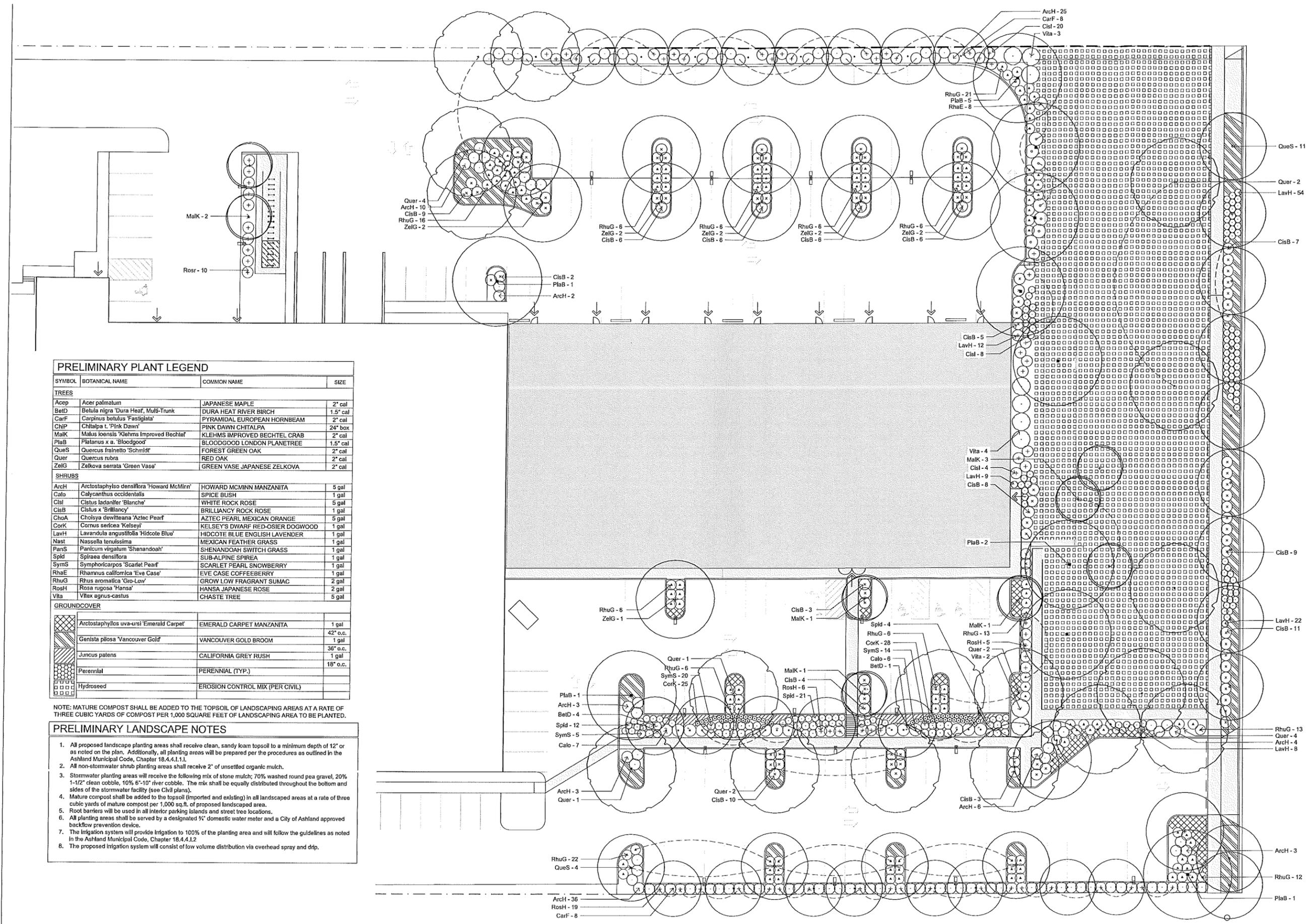
**DAREX EXPANSION**  
210 E. Hersey  
Ashland, Oregon, 97520

REVISION DATE

**ALTERNATE  
LANDSCAPE  
PLANTING  
PLAN**

ISSUE DATE:  
JULY 6, 2015

**L 5.0**



**PRELIMINARY PLANT LEGEND**

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
<b>TREES</b>			
Acep	<i>Acer palmatum</i>	JAPANESE MAPLE	2' cal
BetD	<i>Betula nigra</i> 'Dura Heat', Multi-Trunk	DURA HEAT RIVER BIRCH	1.5" cal
CarF	<i>Carpinus betulus</i> 'Fastigiata'	PYRAMIDAL EUROPEAN HORNBEAM	2" cal
ChiP	<i>Chitalpa l.</i> 'Pink Dawn'	PINK DAWN CHITALPA	24" box
Malk	<i>Malus ioensis</i> 'Klehms Improved Bechtel'	KLEHMS IMPROVED BECHTEL CRAB	2" cal
Plab	<i>Platanus x a.</i> 'Bloodgood'	BLOODGOOD LONDON PLANETREE	1.5" cal
QueS	<i>Quercus trainetto</i> 'Schmidt'	FOREST GREEN OAK	2" cal
Quer	<i>Quercus rubra</i>	RED OAK	2" cal
ZelG	<i>Zelkova serrata</i> 'Green Vase'	GREEN VASE JAPANESE ZELKOVA	2" cal
<b>SHRUBS</b>			
Arch	<i>Arctostaphylos densiflora</i> 'Howard McMinn'	HOWARD MCMINN MANZANITA	5 gal
Calo	<i>Calycanthus occidentalis</i>	SPICE BUSH	1 gal
CisI	<i>Cistus ladanifer</i> 'Blanche'	WHITE ROCK ROSE	5 gal
CisB	<i>Cistus x</i> 'Brillancy'	BRILLANCY ROCK ROSE	1 gal
ChoA	<i>Cholysa dewitteana</i> 'Aztec Pearl'	AZTEC PEARL MEXICAN ORANGE	5 gal
CorK	<i>Cornus sericea</i> 'Kelsey'	KELSEY'S DWARF RED-OSIER DOGWOOD	1 gal
LavH	<i>Lavandula angustifolia</i> 'Hidcole Blue'	HIDCOTE BLUE ENGLISH LAVENDER	1 gal
Nast	<i>Nassella tenuissima</i>	MEXICAN FEATHER GRASS	1 gal
PanS	<i>Panicum virgatum</i> 'Shenandoah'	SHENANDOAH SWITCH GRASS	1 gal
SpId	<i>Spiraea densiflora</i>	SUB-ALPINE SPIREA	1 gal
SymS	<i>Symphoricarpos</i> 'Scarlet Pearl'	SCARLET PEARL SNOWBERRY	1 gal
RhaE	<i>Rhamnus californica</i> 'Eve Case'	EVE CASE COFFEEBERRY	1 gal
RhuG	<i>Rhus aromatica</i> 'Gro-Low'	GROW LOW FRAGRANT SUMAC	2 gal
RosH	<i>Rosa rugosa</i> 'Hansa'	HANSA JAPANESE ROSE	2 gal
Vita	<i>Vitex agnus-castus</i>	CHASTE TREE	5 gal
<b>GROUND COVER</b>			
	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	EMERALD CARPET MANZANITA	1 gal
	<i>Genista pilosa</i> 'Vancouver Gold'	VANCOUVER GOLD BROOM	42" o.c.
	<i>Genista pilosa</i>	VANCOUVER GOLD BROOM	1 gal
	<i>Juncus patens</i>	CALIFORNIA GREY RUSH	36" o.c.
		CALIFORNIA GREY RUSH	1 gal
	Perennial	PERENNIAL (TYP.)	18" o.c.
	Hydroseed	EROSION CONTROL MIX (PER CIVIL)	

NOTE: MATURE COMPOST SHALL BE ADDED TO THE TOPSOIL OF LANDSCAPING AREAS AT A RATE OF THREE CUBIC YARDS OF COMPOST PER 1,000 SQUARE FEET OF LANDSCAPING AREA TO BE PLANTED.

**PRELIMINARY LANDSCAPE NOTES**

- All proposed landscape planting areas shall receive clean, sandy loam topsoil to a minimum depth of 12" or as noted on the plan. Additionally, all planting areas will be prepared per the procedures as outlined in the Ashland Municipal Code, Chapter 18.4.4.1.1.
- All non-stormwater shrub planting areas shall receive 2" of unsettled organic mulch.
- Stormwater planting areas will receive the following mix of stone mulch; 70% washed round pea gravel, 20% 1-1/2" clean cobble, 10% 6"-10" river cobble. The mix shall be equally distributed throughout the bottom and sides of the stormwater facility (see Civil plans).
- Mature compost shall be added to the topsoil (imported and existing) in all landscaped areas at a rate of three cubic yards of mature compost per 1,000 sq.ft. of proposed landscaped area.
- Root barriers will be used in all interior parking islands and street tree locations.
- All planting areas shall be served by a designated 3/4" domestic water meter and a City of Ashland approved backflow prevention device.
- The irrigation system will provide irrigation to 100% of the planting area and will follow the guidelines as noted in the Ashland Municipal Code, Chapter 18.4.4.1.2
- The proposed irrigation system will consist of low volume distribution via overhead spray and drip.



## Land Use: 110 General Light Industrial

### Land Use Description

Light industrial facilities are free-standing facilities devoted to a single use. The facilities have an emphasis on activities other than manufacturing and typically have minimal office space. Typical light industrial activities include printing, material testing and assembly of data processing equipment. Industrial park (Land Use 130) and manufacturing (Land Use 140) are related uses.

### Database Description

The database consisted of a mix of suburban and urban sites. Parking demand rates at the suburban sites were similar to those at urban sites and therefore the data were combined and analyzed together.

- Average parking supply ratios: 1.1 spaces per 1,000 sq. ft. GFA (five study sites) and 1.3 spaces per employee (four study sites).
- Average site employment density: 1,200 sq. ft. GFA per employee (five study sites).

The number of employees for this land use was the total number of employees working on all shifts. Facilities with employees that work on shifts may peak at different hours. It is unclear from the data collected for this land use whether the parking demand counts occurred during, prior to, or after shift changes at the study sites.

Four sites that were submitted were substantially smaller than the other sites contained in the database. The sites ranged in size between 1,200 and 5,100 sq. ft. GFA and employed between one and nine persons. The peak period demand observed at these four sites was 1.13 parked vehicles per employee. The parking demand information from these sites was excluded from the data plots and analysis.

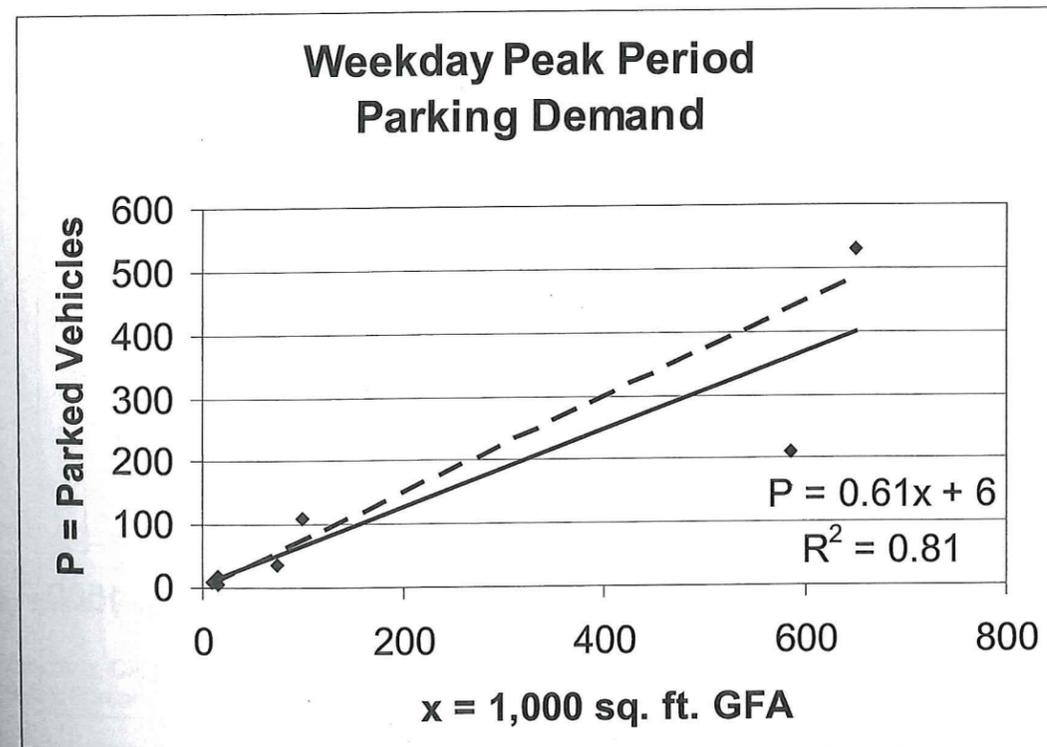
### Study Sites/Years

Anaheim, CA (1984); Dallas, TX (1985); Oklahoma City, OK (1987); Glenview, IL (1990); Anaheim, CA (1991); Seattle, WA (1999); Norristown, PA (2001)

## Land Use: 110 General Light Industrial

### Average Peak Period Parking Demand vs: 1,000 sq. ft. GFA On a: Weekday

Statistic	Peak Period Demand
Peak Period	7:00–9:00 a.m.; 11:00 a.m.–12:00 p.m.; 1:00–3:00 p.m.
Number of Study Sites	7
Average Size of Study Sites	210,000 sq. ft. GFA
Average Peak Period Parking Demand	0.75 vehicles per 1,000 sq. ft. GFA
Standard Deviation	0.33
Coefficient of Variation	44%
Range	0.36–1.19 vehicles per 1,000 sq. ft. GFA
85th Percentile	1.13 vehicles per 1,000 sq. ft. GFA
33rd Percentile	0.49 vehicles per 1,000 sq. ft. GFA



◆ Actual Data Points

— Fitted Curve

- - - Average Rate

**RECEIVED**

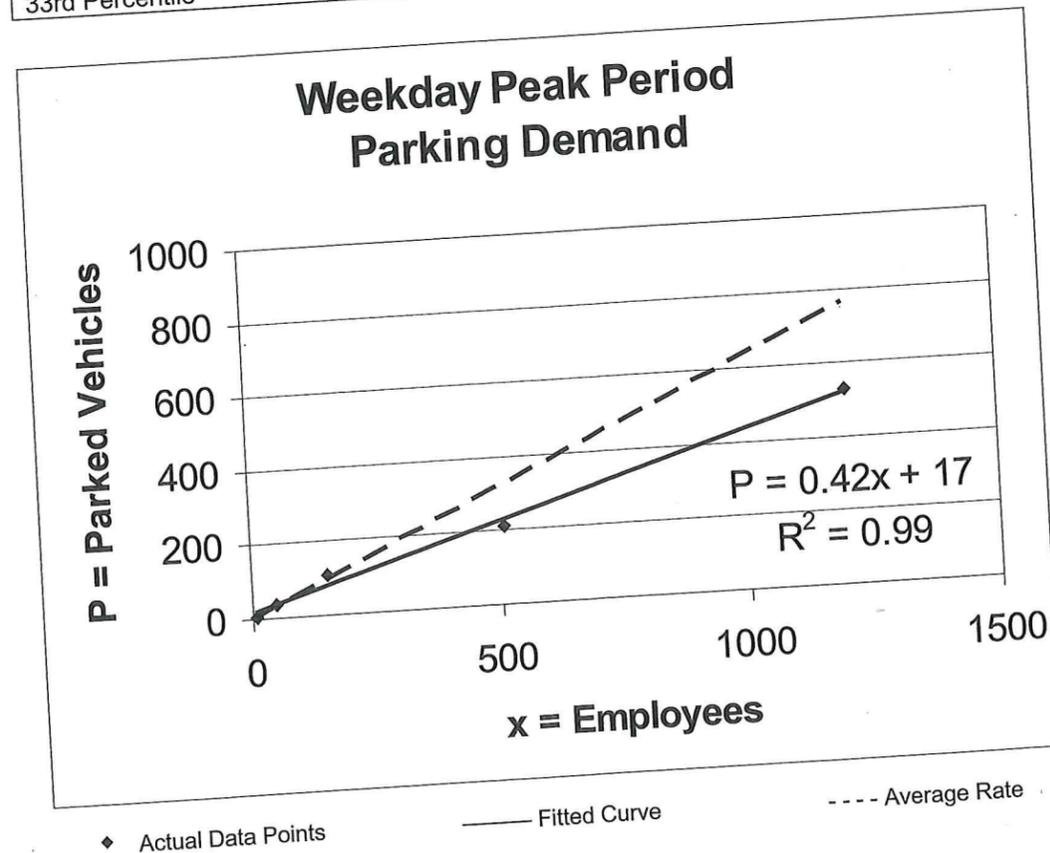
JUL 15 2015

**City Of Ashland**

## Land Use: 110 General Light Industrial

### Average Peak Period Parking Demand vs: Employees On a: Weekday

Statistic	Peak Period Demand
	Peak Period
Number of Study Sites	5
Average Size of Study Sites	380 employees
Average Peak Period Parking Demand	0.64 vehicles per employee
Standard Deviation	0.21
Coefficient of Variation	32%
Range	0.42–0.88 vehicles per employee
85th Percentile	0.81 vehicles per employee
33rd Percentile	0.53 vehicles per employee



## Land Use: 130 Industrial Park

### Land Use Description

Industrial parks contain a number of industrial or related facilities. Industrial parks are characterized by a mix of manufacturing, service and warehouse facilities, with a wide variation in the proportion of each type of use from one location to another. Many industrial parks contain highly diversified facilities—some with a large number of small businesses and others with one or two dominant industries. General light industrial (Land Use 110) and manufacturing (Land Use 140) are related uses.

### Database Description

- Average parking supply ratios: 1.6 spaces per 1,000 sq. ft. GFA (11 study sites) and 1.2 spaces per employee (eight study sites).
- Average site employment density: 900 sq. ft. GFA per employee (eight study sites).

The following table presents a time-of-day distribution of parking demand for five study sites.

Based on Vehicles per 1,000 sq. ft. GFA	Weekday Data	
Hour Beginning	Percent of Peak Period	Number of Data Points*
12:00–4:00 a.m.	—	0
5:00 a.m.	—	0
6:00 a.m.	—	0
7:00 a.m.	55	2
8:00 a.m.	82	5
9:00 a.m.	88	5
10:00 a.m.	89	5
11:00 a.m.	90	5
12:00 p.m.	92	4
1:00 p.m.	97	2
2:00 p.m.	100	2
3:00 p.m.	95	2
4:00 p.m.	77	2
5:00 p.m.	62	2
6:00 p.m.	—	0
7:00 p.m.	—	0
8:00 p.m.	—	0
9:00 p.m.	—	0
10:00 p.m.	—	0
11:00 p.m.	—	0

\* Subset of database

### Study Sites/Years

San Francisco, CA (1985); Berkeley, CA (1990); Anaheim, CA (1991); Renton, WA (1991); Clackamas, OR (1995); Portland, OR (1995); Tempe, AZ (1995); Wilsonville, OR (1995)

RECEIVED

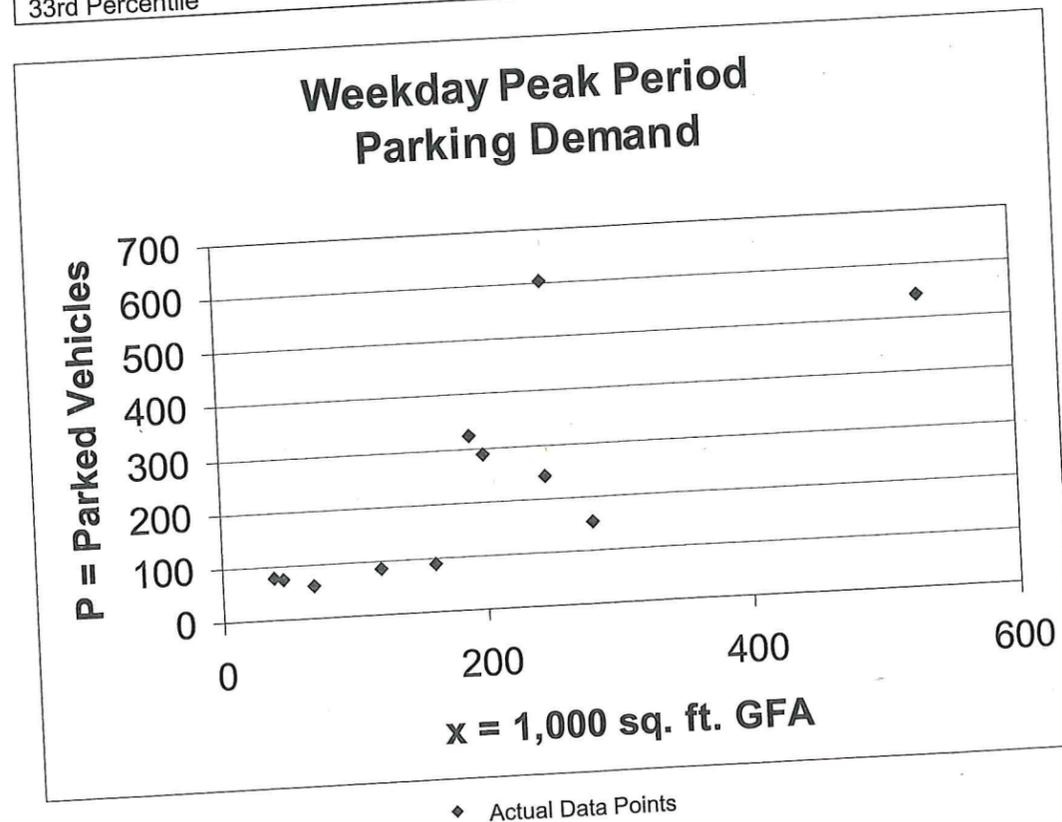
JUL 15 2015

City Of Ashland

## Land Use: 130 Industrial Park

### Average Peak Period Parking Demand vs: 1,000 sq. ft. GFA On a Weekday

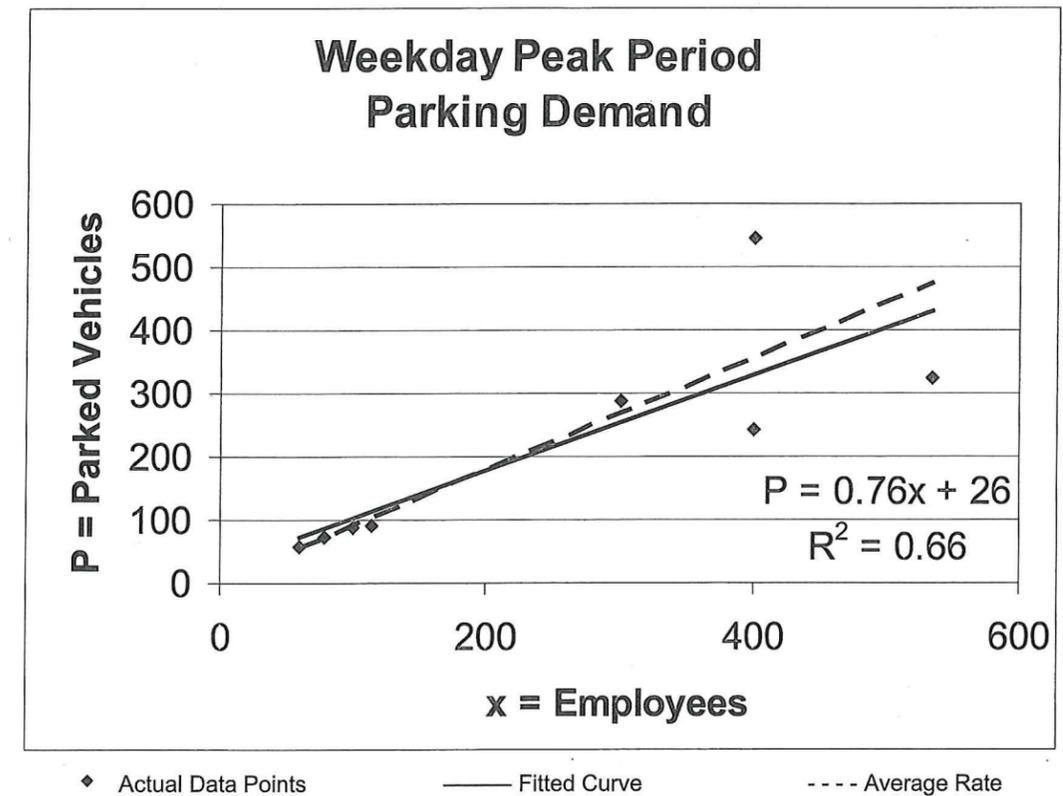
Statistic	Peak Period Demand
Peak Period	7:00 a.m.–12:00 p.m.; 1:00–4:00 p.m.
Number of Study Sites	11
Average Size of Study Sites	194,000 sq. ft. GFA
Average Peak Period Parking Demand	1.27 vehicles per 1,000 sq. ft. GFA
Standard Deviation	0.62
Coefficient of Variation	49%
Range	0.55–2.44 vehicles per 1,000 sq. ft. GFA
85th Percentile	1.85 vehicles per 1,000 sq. ft. GFA
33rd Percentile	0.90 vehicles per 1,000 sq. ft. GFA



## Land Use: 130 Industrial Park

### Average Peak Period Parking Demand vs: Employees On a Weekday

Statistic	Peak Period Demand
Peak Period	7:00 a.m.–12:00 p.m.; 1:00–4:00 p.m.
Number of Study Sites	8
Average Size of Study Sites	250 employees
Average Peak Period Parking Demand	0.89 vehicles per employee
Standard Deviation	0.24
Coefficient of Variation	27%
Range	0.60–1.36 vehicles per employee
85th Percentile	0.98 vehicles per employee
33rd Percentile	0.83 vehicles per employee



RECEIVED

JUL 15 2015

City Of Ashland

## Land Use: 140 Manufacturing

### Land Use Description

Manufacturing facilities are areas where the primary activity is the conversion of raw materials or parts into finished products. Size and type of activity may vary substantially from one facility to another. In addition to the actual production of goods, manufacturing facilities generally also have office, warehouse, research and associated functions. General light industrial (Land Use 110) and industrial park (Land Use 130) are related uses.

### Database Description

- Average parking supply ratios: 1.3 spaces per 1,000 sq. ft. GFA (three study sites) and 1.3 spaces per employee (three study sites).
- Average site employment density: 1,000 sq. ft. GFA per employee (three study sites).

Parking demand counts were only submitted between 10:00 a.m. and 11:00 a.m. and between 2:00 p.m. and 3:00 p.m.

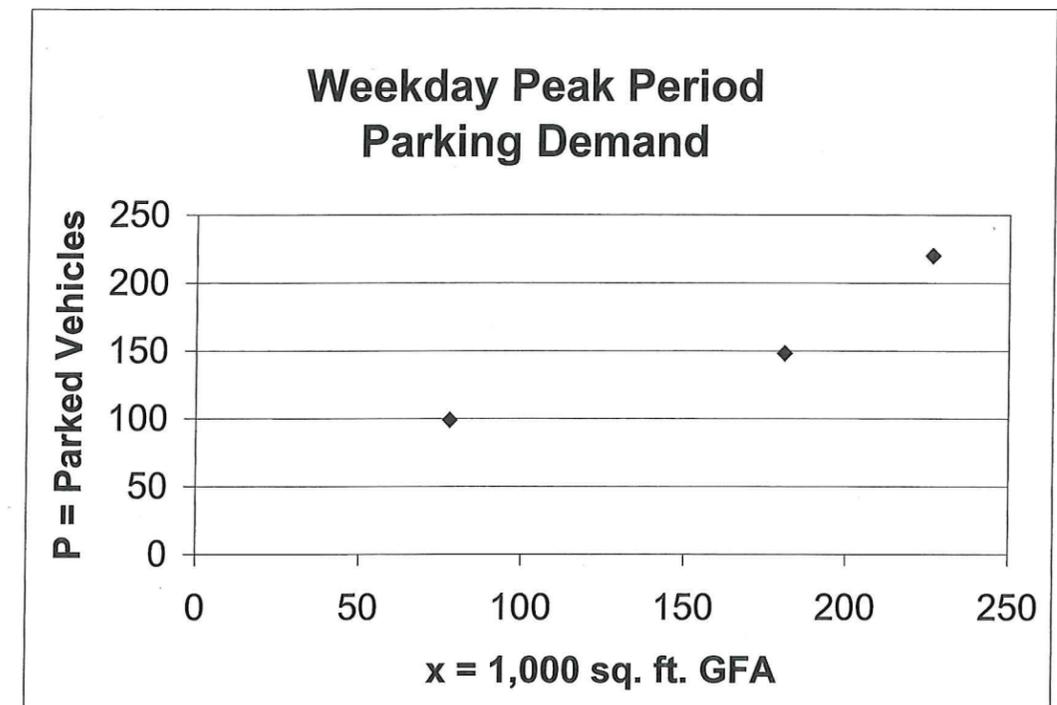
### Study Sites/Years

Bellevue, WA (1991); Kent, WA (1991)

## Land Use: 140 Manufacturing

### Average Peak Period Parking Demand vs: 1,000 sq. ft. GFA On a Weekday

Statistic	Peak Period Demand
Peak Period	2:00–3:00 p.m.
Number of Study Sites	3
Average Size of Study Sites	165,000 sq. ft. GFA
Average Peak Period Parking Demand	1.02 vehicles per 1,000 sq. ft. GFA
Standard Deviation	0.23
Coefficient of Variation	23%
Range	0.82–1.27 vehicles per 1,000 sq. ft. GFA
85th Percentile	1.18 vehicles per 1,000 sq. ft. GFA
33rd Percentile	0.92 vehicles per 1,000 sq. ft. GFA



◆ Actual Data Points

RECEIVED

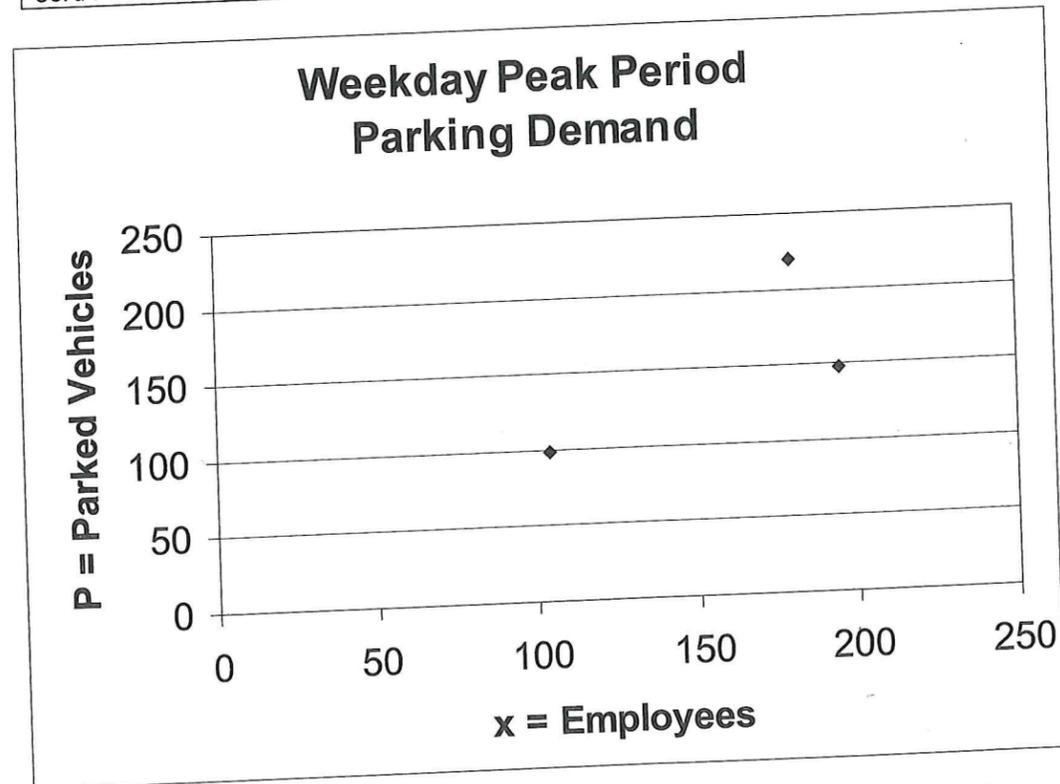
JUL 15 2015

City Of Ashland

## Land Use: 140 Manufacturing

### Average Peak Period Parking Demand vs: Employees On a: Weekday

Statistic	Peak Period Demand
Peak Period	2:00–3:00 p.m.
Number of Study Sites	3
Average Size of Study Sites	160 employees
Average Peak Period Parking Demand	0.97 vehicles per employee
Standard Deviation	0.24
Coefficient of Variation	24%
Range	0.75–1.22 vehicles per employee
85th Percentile	1.14 vehicles per employee
33rd Percentile	0.88 vehicles per employee



◆ Actual Data Points

## Land Use: 150 Warehousing

### Land Use Description

Warehouses are primarily devoted to the storage of materials, but they may also include office and maintenance areas.

### Database Description

- Average parking supply ratios: 0.5 spaces per 1,000 sq. ft. GFA and 1.0 space per employee.
- Average site employment density: 4,100 sq. ft. GFA per employee.

The study sites consisted of a grocery store distribution warehouse, dot.com warehouses, paper supplier warehouses and transfer and storage companies.

The following table presents a time-of-day distribution of parking demand for the 10 study sites.

Based on Vehicles per 1,000 sq. ft. GFA	Weekday Data	
Hour Beginning	Percent of Peak Period	Number of Data Points*
12:00–4:00 a.m.	–	0
5:00 a.m.	–	0
6:00 a.m.	–	0
7:00 a.m.	–	0
8:00 a.m.	71	10
9:00 a.m.	92	10
10:00 a.m.	100	10
11:00 a.m.	99	10
12:00 p.m.	88	10
1:00 p.m.	–	0
2:00 p.m.	–	0
3:00 p.m.	–	0
4:00 p.m.	–	0
5:00 p.m.	–	0
6:00 p.m.	–	0
7:00 p.m.	–	0
8:00 p.m.	–	0
9:00 p.m.	–	0
10:00 p.m.	–	0
11:00 p.m.	–	0

\* Subset of database

For eight of the study sites, data were also collected for trucks parked at the site. The average truck parking demand ratio was 0.11 trucks per 1,000 sq. ft. GFA with a range between 0.04 and 0.25 trucks per 1,000 sq. ft. GFA.

### Study Sites/Years

Syracuse, NY (1988); Bellevue, WA (1991); Seattle, WA (1991); Clackamas, OR (1995); Gresham, OR (1995); Milwaukie, OR (1995); Portland, OR (1995); Wilsonville, OR (1995)

RECEIVED

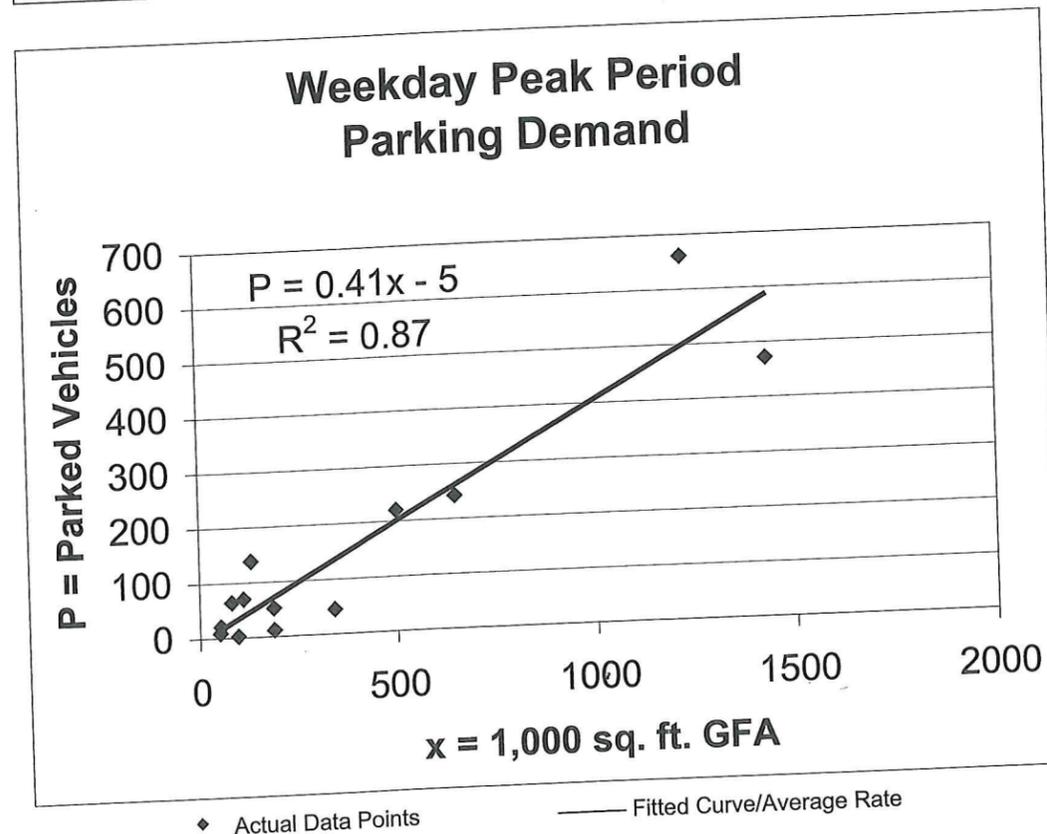
JUL 15 2015

City Of Ashland

## Land Use: 150 Warehousing

Average Peak Period Parking Demand vs: 1,000 sq. ft. GFA  
On a: Weekday

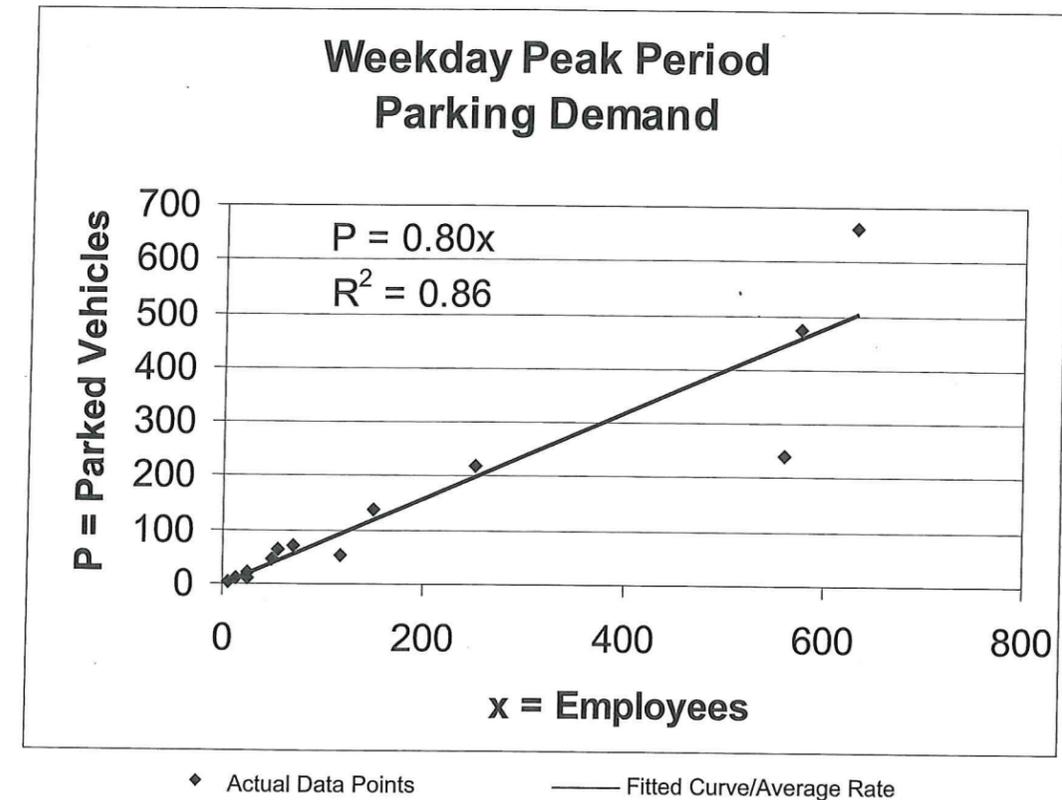
Statistic	Peak Period Demand
Peak Period	9:00 a.m.–12:00 p.m.; 2:00–3:00 p.m.
Number of Study Sites	13
Average Size of Study Sites	390,000 sq. ft. GFA
Average Peak Period Parking Demand	0.41 vehicles per 1,000 sq. ft. GFA
Standard Deviation	0.30
Coefficient of Variation	73%
Range	0.03–1.06 vehicles per 1,000 sq. ft. GFA
85th Percentile	0.67 vehicles per 1,000 sq. ft. GFA
33rd Percentile	0.28 vehicles per 1,000 sq. ft. GFA



## Land Use: 150 Warehousing

Average Peak Period Parking Demand vs: Employees  
On a: Weekday

Statistic	Peak Period Demand
Peak Period	9:00 a.m.–12:00 p.m.; 2:00–3:00 p.m.
Number of Study Sites	13
Average Size of Study Sites	190 employees
Average Peak Period Parking Demand	0.78 vehicles per employee
Standard Deviation	0.26
Coefficient of Variation	33%
Range	0.36–1.16 vehicles per employee
85th Percentile	1.01 vehicles per employee
33rd Percentile	0.81 vehicles per employee



RECEIVED  
JUL 15 2015  
City Of Ashland

**TYPE II  
PUBLIC HEARING**

---

**PA-2015-01496  
35 South Second St**



**PLANNING ACTION:** 2015-01496

**SUBJECT PROPERTY:** 35 South Second Street

**OWNER:** MPM Investments

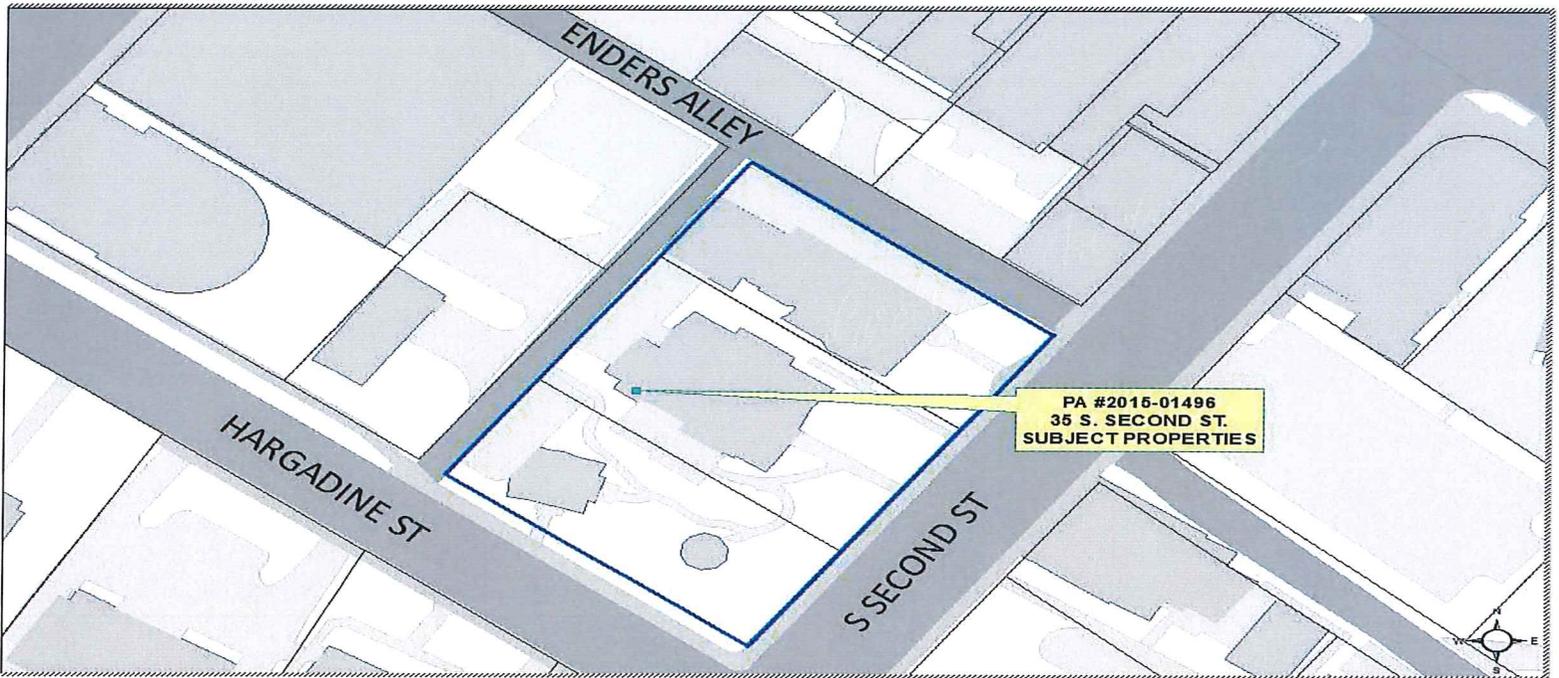
**APPLICANT:** Kistler, Small & White, Architects

**DESCRIPTION:** A request for Conditional Use Permit and Site Design Review approvals to allow 3,051 square feet of additions including a new kitchen, new bar, laundry room, two new second floor offices and an accessible lift, and the conversion of the existing kitchen into bussing and storage areas for the Winchester Inn located at 35 S. Second St. Also included are requests for Tree Removal Permits to remove two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house; and Exception to the Street Standards to retain the existing curbside sidewalks along the perimeter of the property. **COMPREHENSIVE PLAN DESIGNATION:** Commercial Downtown; **ZONING:** C-1-D; **ASSESSOR'S MAP:** 39 1E 09BD; **TAX LOTS:** 5600-5700.

**NOTE:** The Ashland Historic Commission will also review this Planning Action on Wednesday, September 2, 2015 at 6:00 PM in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**NOTE:** The Ashland Tree Commission will also review this Planning Action on Thursday, September 3, 2015 at 6:00 PM in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** *September 8, 2015 at 7:00 PM, Ashland Civic Center, 1175 East Main Street*



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102-.35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### **18.5.2.050 Approval Criteria**

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. *Exception to the Site Development and Design Standards.* The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

## CONDITIONAL USE PERMITS

### **18.5.4.050.A. Approval Criteria**

A Conditional Use Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

- 1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
- 2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.
- 3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.
  - a. Similarity in scale, bulk, and coverage.
  - b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  - c. Architectural compatibility with the impact area.
  - d. Air quality, including the generation of dust, odors, or other environmental pollutants.
  - e. Generation of noise, light, and glare.
  - f. The development of adjacent properties as envisioned in the Comprehensive Plan.
  - g. Other factors found to be relevant by the approval authority for review of the proposed use.
- 4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.
- 5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.
  - a. WR and RR. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - b. R-1. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - d. C-1. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
  - e. C-1-D. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 1.00 gross floor to area ratio, complying with all ordinance requirements.
  - f. E-1. The general office uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.

- g. M-1. The general light industrial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
- h. CM-C1. The general light industrial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
- i. CM-OE and CM-MU. The general office uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area, complying with all ordinance requirements.
- k. CM-NC. The retail commercial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area ratio, complying with all ordinance requirements.
- l. HC, NM, and SOU. The permitted uses listed in chapters 18.3.3 Health Care Services, 18.3.5 North Mountain Neighborhood, and 18.3.6 Southern Oregon University District, respectively, complying with all ordinance requirements.

### TREE REMOVAL PERMIT FROM THE UNIFIED LAND USE ORDINANCE

#### 18.5.7.040.B Criteria for Issuance of Tree Removal Permit

#### **B. Tree Removal Permit.**

1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. **Tree That is Not a Hazard.** A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
  2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  4. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  5. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

### EXCEPTION TO STREET STANDARDS

18.4.6.020.B.1. Exception to the Street Design Standards. The approval authority may approve exceptions to the standards section in 18.4.6.040 Street Design Standards if all of the following circumstances are found to exist.

- a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.
  - i. For transit facilities and related improvements, access, wait time, and ride experience.
  - ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.
  - iii. For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.
- c. The exception is the minimum necessary to alleviate the difficulty.
- d. The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.

# ASHLAND PLANNING DIVISION STAFF REPORT

**September 8, 2015**

**PLANNING ACTION:** #2015-01496

**OWNERS:** MPM Investments

**APPLICANTS:** Kistler, Small & White, Architects

**LOCATION:** 31-35 South Second Street

**ZONE DESIGNATION:** C-1-D

**COMP. PLAN DESIGNATION:** Commercial Downtown

**APPLICATION DEEMED COMPLETE:** August 31, 2015

**120-DAY TIME LIMIT:** December 29, 2015

**ORDINANCE REFERENCE** ( see <http://www.ashland.or.us/comdevdocs> to view land use code on-line):

18.2.6 Standards for Non-Residential Zones  
18.4.3 Parking, Access, and Circulation  
18.4.5 Tree Preservation & Protection  
18.4.6 Public Facilities  
18.4.7 Signs  
18.5.2 Site Design Review  
18.5.4 Conditional Use Permit  
18.4.6.020 Exception to Street Standards  
18.5.7 Tree Removal Permits

**REQUEST:** A request for Conditional Use Permit and Site Design Review approvals to allow 3,051 square feet of additions including a new kitchen, new bar, laundry room, two new second floor offices and an accessible lift, and the conversion of the existing kitchen into bussing and storage areas for the Winchester Inn located at 35 S. Second St. Also included are requests for Tree Removal Permits to remove two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house; and Exception to the Street Standards to retain the existing curbside sidewalks along the perimeter of the property.

## **I. Relevant Facts**

### **A. Background - History of Application**

Planning Action #2013-00003, a request for Site Review and Conditional Use Permit approval to allow the conversion of the existing 960 square foot Enders Annex building located at 207 Enders Alley, to a single motel unit associated with the Winchester Inn.

Planning Action #2003-00005, a request for Conditional Use and Site Review Permits for a 625 square foot glass conservatory on the south side of the building and a 425 square foot guest unit on the second floor of the Winchester Inn at 35 South Second Street was approved administratively in February of 2003.

Planning Action #1994-00044, a request for Site Review and Conditional Use Permit approval to expand the existing hotel, the Enders House at 31 South Second Street from four to nine units with the construction of a third story.

Planning Action #1993-00024, a request for Site Review and Conditional Use Permit to allow a two-unit motel expansion in a separate cottage at 35 South Second Street was approved by the Planning Commission in February of 1993.

Planning Action #1991-00003, a request for a two-unit motel expansion in a separate cottage at 35 South Second Street was approved by the Planning Commission in January of 1991.

Planning Action #1988-00008, a request to construct a two-unit carriage house addition at 31 South Second Street, was approved by the Planning Commission in January of 1988.

Planning Action #1986-00095, a request for Site Review and Conditional Use Permit approval to allow a five-unit hotel at 31 South Second Street. Three units were to be accommodated in the existing, two-story Enders House building and a two-story carriage house building was to be built off the alley to accommodate two-units. The Site Review of the carriage house building was deferred to a later date.

Planning Action #1982-00089, a request for Conditional Use and Site Review Permit approval to convert the Winchester House at 35 South Second Street, into a seven-unit Traveler's Accommodation and Restaurant, was approved by the Planning Commission in January of 1983.

There are no other planning actions of record for these properties.

### **B. Detailed Description of the Site and Proposal**

#### **The Site**

The subject property is comprised of three tax lots, two of which are involved in the current request. The properties are located north of the intersection of South Second

Street and Hargadine Street, and are rectangular. The existing slope of the combined properties runs from southwest to northeast and is around 15 percent. The application notes that 19 parking spaces are located off the alleys at the side and rear of the parcels.

Tax Lot #5500 is the northernmost of the three properties. It contains the Heritage House, formerly called the Enders House, and a detached Carriage House to the rear, and is not proposed for any disturbance or alteration here. This property is addressed as 31 South Second Street.

Tax Lot #5600, the middle parcel is addressed as 35 South Second Street and identified as the Winchester Inn Main House in the applicants' submittal materials. The main house was originally constructed on East Main Street in 1886, but was moved to its current location in 1910 after a major fire. The building was originally constructed as a residence, but was later converted to a sanitarium and since 1923 has been used as a boarding house. The building is listed on the National Register of Historic Places and its historic designation is the Roper and Julia Fordyce House.

Tax Lot #5700, the southernmost of the three parcels, contains the Larkspur Cottage, and does not currently have a separate address assigned in city records.

Paved access to the property is from Second Street, which is considered a Neighborhood Street in Ashland's Transportation System Plan (TSP). Second Street is currently paved along the property's frontage, with curbs, gutters and curbside sidewalks in place. The property also fronts on Hargadine Street to the south, a Neighborhood Street as well. Hargadine is paved with curbs, gutters and curbside sidewalks in place along the property frontage. There are paved alley's along the properties' north and west frontages, with Enders Alley running between First and Second Streets to the north of the Heritage House, and an un-named alley running along the west side of the properties between Enders Alley and Hargadine Street.

## **The Proposal**

### **Site Design Review & Conditional Use Permit Proposal**

The application includes requests for Conditional Use Permit and Site Design Review approvals to allow 3,051 square feet of additions including a new kitchen, new bar, laundry room, two new second floor offices and an accessible lift, and the conversion of the existing kitchen into bussing and storage areas. The proposal includes two additions:

The south addition contains the new 360 square foot bar and is designed as a Victorian conservatory. It is to be located to the south side of the existing main building adjacent to the existing dining area, and will be physically attached to the main building with stairs and a new accessible lift connecting the main levels. The bar will include an outdoor patio area to its east, with interior seating for roughly 20 and bar seating for five in addition to a new serving area for preparing drinks.

The west addition, on the west side of the main facility, will include a new full-service 869 square foot kitchen with walk-in cooler, range hood, grille, range, dishwashing equipment and storage on the first floor. The existing kitchen area will be remodeled into a new bussing station and storage area. Below the new kitchen will be a new 193 square

foot laundry room, and the existing laundry area will be remodeled to create an accessible route to the new accessible lift. Above the kitchen there will be two new offices. The applicants currently rent office space off-site, and this will enable their offices to move to the property. (*The application notes that it is hoped that at some point in the future these offices could be converted into additional guest suites, and also recognizes that such a conversion would need to comply with applicable codes in place at the time of application.*)

### **Exception to Street Standards Proposal**

An Exception to the Street Standards is requested to retain the existing approximately six-foot wide curbside sidewalk configuration along the Second Street and Hargadine Street frontages of the property, where current standards would require a five-foot hardscape parkrow, with grated tree planting wells between the curb and sidewalk.

### **Tree Removal Permit Proposal**

The application also involves the removal of two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house. Within the C-1-D zoning district, any removal of trees six-inches in diameter at breast height (d.b.h.) or more requires a Tree Removal Permit.

## **II. Project Impact**

As explained more fully above, the application consists of Site Design Review, Conditional Use Permit, Exception to Street Standards and Tree Removal Permit approval requests. Within the Downtown Design Standards Overlay zone, new buildings or additions greater than 2,500 square feet are subject to a “Type II” Site Design Review application procedure which requires a decision by the Planning Commission through a public hearing.

### **A. Site Design Review Proposal**

#### **Underlying Zone Requirements**

The first approval criterion for Site Design Review is that, “*The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*” The subject property’s underlying zone is C-1-D (Commercial Downtown) and within that zone, there is no minimum lot area, width, or depth; no minimum front, side or rear yard area except where abutting a residential zone to the side or rear in which case a ten-foot per story setback is required; no maximum lot coverage; and no minimum residential density.

The subject properties here abut the R-2 residential zone to the south, across Hargadine Street, and the proposed additions are more than 40 feet from the Hargadine Street property line. The site is not located on an arterial street, and as such no setback requirements come into play. The maximum building height is limited to 40 feet, and with the proposed additions here, the building height still averages 34½ feet and so complies with this limit.

### **Overlay Zone Requirements**

The second Site Design Review approval criterion is that, “*The proposal complies with applicable overlay zone requirements (part 18.3).*” The subject property is located within three overlay zones: the Detail Site Review Overlay, the Downtown Design Standards Overlay and the Historic District Overlay. The applicable standards for these overlay zones are incorporated into the Site Development and Design Standards in part 18.4, and are addressed below.

### **Site Development and Design Standards**

The third approval criterion is that, “*The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*” Generally, these Site Development & Design Standards seek to improve each project’s appearance; to create a positive, human scale relationship between proposed buildings and the streetscape which encourages bicycle and pedestrian travel; to lessen the visual and climatic impacts of parking; and to screen adjacent uses from adverse impacts of development. To these ends, buildings are to have their primary orientation to the street rather than to parking areas, with visible, functional and attractive entrances oriented to the street, placed within 20 feet of the street, and accessed directly from the public sidewalk. Sidewalks and street trees are to be provided along subject properties’ frontages, and automobile parking and circulation areas are not to be placed between buildings and the street.

### **Basic Site Review**

The application explains that the existing building is oriented to the streets and that the established facade, setback, relationship to the streetscape and general orientation will not be altered with the requested additions. Similarly, existing parking, vehicular circulation and street trees are not proposed to be altered. Building materials proposed are to be similar to the existing Inn, with wood siding, trim and wood shingle roofing proposed. The west addition will be painted to match the existing Inn. The proposed bar addition will be the exception in terms of matching materials, as it will have a metal roof, exposed concrete base and wood windows with aluminum cladding. The bar building use glass to create a conservatory feel in the bar addition.

### **Detail Site Review Overlay**

Within the Detail Site Review Overlay, properties are to have a minimum 0.50 floor area ratio (FAR). In this instance, the proposed building area is 9,715 square feet on the two parcels involved which have a total area of approximately 13,939 square feet. This equates to a 0.69 FAR without consideration of the site’s pedestrian areas and thus complies with the standard.

The existing building does not have frontages over 100 feet in length, nor is it within 30 feet of the street, however it nonetheless uses windows, doorways and the proposed conservatory addition to contribute to an engaging streetscape, with changes in massing, material and surface finishes to emphasizes entries from the street.

The existing building is setback more than five feet from the street; the current setback is approximately 35 feet and this is maintained with the proposed addition in keeping with the Historic District Design Standards which seek additions which are visually

unobtrusive from the right-of-way and which do not obscure or detract from the character defining features of the historic building. Placement of an addition between the existing building and the street would obscure the character of the existing building and the site, and would run counter to the Historic District Development Standards for additions.

### **Historic District Overlay**

The materials provided explain that the location of the existing building may be considered a transitional zone between the downtown commercial area and the adjacent residential zone just south of the site. The application notes that the building height is to remain unchanged with the additions.

The application points out that the bar addition, which is located on the east face of the existing building, is set well back from the front of the building facing Second Street and will be set back from the original house bay window, allowing the existing entry to retain its prominence. In addition, it will provide a single-story step up to the existing two-story building. The most impacted view will be from Hargadine Street, which is at a higher elevation, and the application suggests that the differing elevations and the presence of the Larkspur Cottage between the bar addition and Hargadine Street will reduce the perceived scale and visual impact of the addition.

The applicants suggest that the massing of the buildings will continue to be appropriate to the existing Victorian architecture, and they will be smaller than other commercial buildings in the immediate neighborhood. They emphasize that the existing setbacks are not to be altered, that the roof form and roofing of the additions will be consistent with those of the existing building, and that the building form, primary entry and façade rhythms will remain unchanged. The application explains that the applicants believe it is important that the addition be as cohesive with the existing as possible, and they note that architectural interest will be added with the bar designed as a conservatory which will complement the existing Victorian architecture but also be contemporary rather than a trite mimic of the historic period. Windows proposed are to be compatible in shape, size and proportion of the existing, with the exception of the bar where the windows will be specific to the proposed conservatory feature.

A condition has been recommended below to require that the roofing of the west addition not utilize wood shingles, which are specifically called out as to be avoided in the Historic District Development Standards, and that the final roofing material treatment be approved by the Historic Commission's Review Board with review of the building permits.

### **Downtown Design Standards Overlay**

The application notes that while in the Downtown, the existing building is a stand-alone structure with a residential style dating to its original use, and is located in a transitional zone near the edge of the overlay. As such, the current building is not in keeping with many of the Downtown Design Standards seeking a continuous, commercial storefront streetscape. The additions proposed do not seek to alter this character, and instead seek to be compatible with the existing contributing historic resource. In staff's view, this can be found to be in keeping with the transitional character of the site and the underlying purpose of the Downtown Design Standards which note that the standards seek to guide development in the context with their historic surroundings.

In staff's assessment, the proposed additions can be found to be in keeping with the applicable site development and design standards and overlay zone requirements. The additions proposed have been thoughtfully designed and placed to respect the historic character of the existing building and to not detract from its relationship to the streetscape.

### **Public Facilities**

The fourth approval criterion for Site Design Review approval is that, "The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property." The subject property is presently served by water, sewer, electricity, urban storm drainage, and paved access via the adjacent rights-of-way.

The Electric Department has indicated that a service upgrade would be desirable with the addition. The application materials provided note that an electrical service plan will be developed in consultation with the city's Electric Department to ensure both that adequate facilities are provided and that any aesthetic impacts are minimized. All electrical services are proposed to be provided from Second Street where the current service exists.

The application also notes that two fire hydrants are within 150 feet of the property, one on the property at the corner of Second and Hargadine and the other on the adjacent tax lot at the corner of Second Street and Enders Alley. The applicants indicate that these hydrants have adequate pressure available to serve the building, and note that at the time of the building permit submittal all fire code requirements shall be addressed including provisions for a Fire Department connection along the front of the building.

Conditions of approval have been recommended below to require that the applicants provide final electrical service, utility, stormwater drainage and erosion control plans for the review and approval of the Planning, Building, Electric and Public Works/Engineering Departments in conjunction with their building permit application.

Paved access to the property is from Second Street, which is considered a Neighborhood Street in Ashland's Transportation System Plan (TSP). Second Street is currently paved along the property's frontage, with curbs, gutters and curbside sidewalks in place. The property also fronts on Hargadine Street to the south, a Neighborhood Street as well. Hargadine is paved with curbs, gutters and curbside sidewalks in place along the property frontage. There are paved alley's along the properties' north and west frontages, with Enders Alley running between First and Second Streets to the north of the Heritage House, and an un-named alley running along the west side of the properties between Enders Alley and Hargadine Street.

## **B. Conditional Use Permit**

The applicants explain that the subject property already operates under a Conditional Use Permit within the C-1-D zone as a Hotel/Motel, and been in operation since 1983.

The Electric Department has indicated that a service upgrade may be desirable to service the addition. The application materials provided note that an electrical service plan will be developed in consultation with the city's Electric Department to ensure both that adequate facilities are provided and that any aesthetic impacts are minimized. All electrical services are proposed to be provided from Second Street where the current service exists.

The application also notes that two fire hydrants are within 150 feet of the property, one on the property at the corner of Second and Hargadine and the other on the adjacent tax lot at the corner of Second Street and Enders Alley. The applicants indicate that these hydrants have adequate pressure available to serve the building, and note that at the time of the building permit submittal all fire code requirements shall be addressed including provisions for a Fire Department connection along the front of the building.

The applicants emphasize that the proposal has been designed to limit the impact of the increase in bulk and is well within an appropriate scale to the building and site. They note that the existing building is an historic contributing resource within the local historic district and that the additions have been designed to be architecturally compatible.

The additions proposed are intended to enhance the facility, the guest experience and the working environment with minimal impact on the neighborhood, and don't involve any increase in the number of guest units. The application explains that most of the visual impact of the changes will be oriented to the rear of the site along the alley, and that bordering this alley are the backs of the Oregon Shakespeare Festival's new rehearsal center and the Oregon Cabaret Theater building. The application suggests that there will be little or no impediment to neighboring views.

The application suggests that the proposal will result in no discernible increases in environmental impacts including those related to air quality, dust, odors or other pollutants. The addition, with the exception of the bar, will not result in any increase in noise, light or glare. The applicants emphasize that while the bar has a primarily glass façade, its placement on the site relative to the existing buildings and streetscape, the existing landscaping, and the natural topography of the site will prevent any resultant light or glare from being distracting. The applicants further assert that the addition will result in an overall reduction in noise by reducing the available outside seating and shifting seats indoors.

The application further emphasizes that at this time, no additional guest suites are being added so there should be no additional impact in terms of parking or traffic. The improvements will allow additional seating capacity in the restaurant and bar, but they are noted as serving primarily guests of the Inn or others who arrive on foot from elsewhere in the downtown.

The application concludes that the conservatory style bar will be a unique and exciting addition to the Inn and to Ashland's downtown.

### C. Exception to Street Standards

The applicants suggest that it would not be desirable to remove the existing, established significant trees in order to create a parkrow to plant trees as this would be counter-productive and they suggest that maintaining the current sidewalk condition would be more in keeping with the purpose and intent of the street standards and would not diminish the safety or effectiveness of the existing facilities. In staff's view, the site's topography could also be found to pose a demonstrable difficulty in widening the sidewalks to meet current standards.

### D. Tree Removal Permit

The application proposes the removal of two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house. Within the C-1-D zoning district, any removal of trees six-inches in diameter at breast height (d.b.h.) or more requires a Tree Removal Permit. The application notes that there are many mature trees on the site, and that the removal of the two trees proposed will not change the overall tree canopy coverage of the site nor will it have significant negative impacts on erosion, soil stability, flow of surface waters, adjacent trees or existing windbreaks.

Tree protection details have been noted on the Tree Removal and Protection Plan (L2) provided, however the proposed new bar addition extends into the tree protection zone fencing shown and there is no arborist report speaking to the ability of the trees to be retained to tolerate the proposed disturbances in or near their root zones. Conditions have accordingly been recommended below to require an arborist report assessing the conditions of the trees and their abilities to accommodate the proposed construction; a revised Tree Protection Plan detailing revised tree protection necessary during construction; and that a Tree Verification inspection to verify identification of the trees to be removed and installation of fencing for trees to be protected occur prior to any tree removal or site disturbance.

The Tree Commission has not yet reviewed the application as this report is being prepared. A condition of approval has been recommended below to require that the recommendations of the Tree Commission be made conditions of approval, where consistent with applicable standards and with final approval of the Staff Advisor.

## III. Procedural - Required Burden of Proof

The criteria for Site Design Review approval are described in 18.5.2.050 as follows:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm

drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.

E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

**The criteria for Conditional Use Permit approval are described in 18.5.4.050.A as follows:**

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.
3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.
  - a. Similarity in scale, bulk, and coverage.
  - b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  - c. Architectural compatibility with the impact area.
  - d. Air quality, including the generation of dust, odors, or other environmental pollutants.
  - e. Generation of noise, light, and glare.
  - f. The development of adjacent properties as envisioned in the Comprehensive Plan.
  - g. Other factors found to be relevant by the approval authority for review of the proposed use.
4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.
5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.
  - a. WR and RR. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - b. R-1. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - d. C-1. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
  - e. **C-1-D.** *The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses,*

*developed at an intensity of 1.00 gross floor to area ratio, complying with all ordinance requirements.*

- f. E-1. The general office uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
- g. M-1. The general light industrial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, complying with all ordinance requirements.
- h. CM-C1. The general light industrial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
- i. CM-OE and CM-MU. The general office uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area, complying with all ordinance requirements.
- k. CM-NC. The retail commercial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area ratio, complying with all ordinance requirements.
- l. HC, NM, and SOU. The permitted uses listed in chapters 18.3.3 Health Care Services, 18.3.5 North Mountain Neighborhood, and 18.3.6 Southern Oregon University District, respectively, complying with all ordinance requirements.

**The criteria for an Exception to Street Standards are described in AMC 18.4.6.020.B.1 as follows:**

- a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.
  - i. For transit facilities and related improvements, access, wait time, and ride experience.
  - ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.
  - iii. For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.
- c. The exception is the minimum necessary to alleviate the difficulty.
- d. The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040.A.

**The criteria for a Tree Removal Permit to remove a “Tree That is Not a Hazard” are described in AMC 18.5.7.040.B.2 as follows:**

A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

- 1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
- 2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
- 3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall

grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.

4. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
5. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

#### **IV. Conclusions and Recommendations**

The application requests Conditional Use Permit and Site Design Review approvals to allow 3,051 square feet of additions including a new kitchen, new bar, a new laundry room, two new second floor offices and an accessible lift, and the conversion of the existing kitchen into bussing and storage areas. Also included are requests for Tree Removal Permits to remove two trees: a six-inch diameter Plum tree located within the footprint of the proposed new bar, and an eight-inch diameter Birch tree within the footprint of the addition at the rear of the main house; and an Exception to the Street Standards to retain the existing curbside sidewalks along the perimeter of the property.

In staff's assessment, the proposal can be found to be in keeping with the applicable criteria and standards for approval. The existing building is of a residential style in keeping with its original, historic use, and has been restored and maintained by the applicants and in continuous hotel/motel use since the mid 1980's. It is well-suited to its location at the transition between the downtown core and the residential neighborhood just across Hargadine Street, and the additions proposed have been thoughtfully designed and placed to respect the historic character of the existing building and to not detract from its relationship to the streetscape or surrounding neighborhood. Staff is supportive of the request, and recommends approval with the conditions detailed below:

- 1) That all proposals of the applicant shall be conditions of approval unless otherwise specifically modified herein.
- 2) That the plans submitted for the building permit shall be in conformance with those approved as part of this application. If the plans submitted for the building permit are not in conformance with those approved as part of this application, an application to modify this Site Design Review and Conditional Use Permit approval shall be submitted and approved prior to the issuance of a building permit.
- 3) That all recommendations of the Ashland Historic Commission from their September 2, 2015 meeting, where consistent with the applicable ordinances and standards and with final approval of the Staff Advisor, shall be conditions of approval unless otherwise modified herein.

- 4) That all recommendations of the Ashland Tree Commission from their September 3, 2015 meeting, where consistent with the applicable ordinances and standards and with final approval of the Staff Advisor, shall be conditions of approval unless otherwise modified herein.
- 5) That prior to the installation of any signage, a sign permit shall be obtained. All signage shall meet the requirements of the Sign Ordinance (AMC 18.4.7).
- 6) That prior to the issuance of a building permit for the bar addition, the applicants shall provide evidence of lot consolidation or otherwise address the building code prohibition on construction over a property line.
- 7) That the roofing of the west addition shall not utilize wood shingles, which are to be avoided in the Historic District Development Standards. The final roofing material treatment shall be detailed in the building permit submittals for the review and approval of the Staff Advisor and Historic Commission's Review Board.
- 8) That building permit submittals shall include:
  - a) The identification of all easements, including but not limited to public or private utility easements.
  - b) The identification of exterior building materials and paint colors for the review and approval of the Staff Advisor and Historic Commission Review Board. Very bright or neon paint colors shall not be used in accordance with the requirements of the Detailed Site Review Standards, and the colors and materials selected shall be consistent with those approved with the application.
  - c) Specifications for all exterior lighting fixtures. Exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
  - d) Revised Landscape, Irrigation and Tree Protection Plans shall be provided for the review and approval of the Staff Advisor with the building permit submittals. These revised plans shall address: 1) The recommendations of the Tree Commission from their September 3, 2015 meeting where consistent with applicable criteria and standards, and with final approval by the Staff Advisor; 2) An arborist's report assessing the conditions of the trees within the area of disturbance and their abilities to accommodate the proposed construction, and a Tree Protection Plan detailing revised tree protection zones and any additional measures or recommendations necessary during construction; and 3) required size and species specific replacement planting details and associated irrigation plan modifications, including the requirements for programmable automatic timer controllers and a maintenance watering schedule with seasonal modifications.

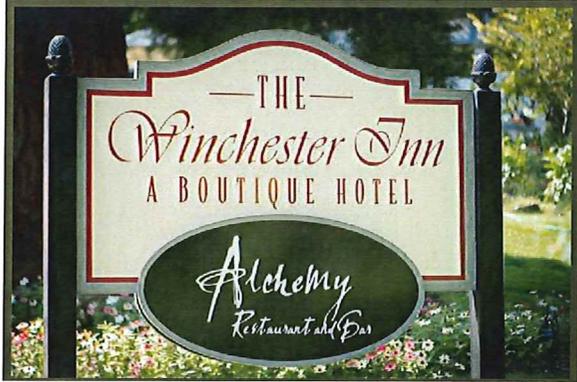
- e) A stormwater drainage plan, including any necessary on-site detention measures for the review and approval of the Engineering, Building and Planning Departments with the building permit submittal.
  - f) A final utility plan for the project for the review and approval of the Engineering, Planning and Building Divisions. The utility plan shall include the location of any necessary connections to public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins. Cabinets, vaults and Fire Department Connections shall be located in areas least visible from streets, sidewalks and pedestrian areas, while considering access needs. Any necessary service upgrades shall be completed by the applicant at applicant's expense.
  - g) An electric design and distribution plan including load calculations and locations of all primary and secondary services including any transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric, Engineering, Building and Planning Departments prior to the issuance of demolition, excavation or building permits. Transformers, cabinets and vaults shall be located in areas least visible from streets, sidewalks and pedestrian areas, while considering the access needs of the Electric Department.
- 6) That prior to the issuance of the building, the commencement of site work or storage of materials:
- a) A Tree Verification Permit shall be obtained, and tree protection measures installed according to the approved plan, inspected and approved by Staff Advisor. The Verification Permit is to inspect the identification of trees to be removed and the installation of tree protection fencing for the trees to be retained and protected on and adjacent to the site. Tree protection measures shall be in the form of chain link fencing six feet tall, installed and maintained in accordance with the requirements of AMC 18.4.5.030.C.
- 7) That prior to the final approval of the project and issuance of a certificate of occupancy:
- a) That all hardscaping, landscaping and the irrigation system shall be installed according to the approved plan, inspected, and approved by the Staff Advisor.
  - b) All utility service and equipment installations shall be completed according to Electric, Engineering, Planning, and Building Departments' specifications, inspected and approved by the Staff Advisor.
  - c) The requirements of the Ashland Fire Department, including approved addressing, fire hydrant clearance and provisions for a "Knox Box" key box shall be satisfactorily addressed. Fire Department requirements shall

be included in the building permit documents. (**Note:** *If this project takes place during fire season restrictions and is on lands within 1/8 of a mile of Oregon Department of Forestry-protected lands, the applicants are subject to ODF fire restrictions and will need to check construction restrictions at [www.swofire.com](http://www.swofire.com) or call (541) 664-3328).*

- e) That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent residential properties.

kistler + small + white  
architects

August 5, 2015



Site Review

## THE WINCHESTER INN REMODEL & ADDITION

RECEIVED

AUG 05 2015

City Of Ashland

kistler +  
small  
+ white  
architects

table of contents

• PROJECT INFORMATION	3
• PROJECT DESCRIPTION	5
• FINDING OF FACT	7
• DOWNTOWN HISTORIC DISTRICT	7
• DOWNTOWN ASHLAND	11
• CONDITIONAL USE PERMIT	13
• EXCEPTION TO STREET STANDARDS	16
• TREE PRESERVATION AND PROTECTION	18
• OTHER ISSUES PER PRE-APPLICATION	20

PROJECT INFORMATION

**PLANNING ACTION:**

The proposed project includes a new bar on the first floor, two (2) new second floor offices, a new kitchen, a new laundry room and the remodel of the existing kitchen into the bussing station and storage areas. The site will require a Type II Site Design Review because the combined new square footage exceeds 2,500 square feet. The applicant will be requesting a Conditional Use Permit for Hotel use within the C-1-D district, and an Exception to Frontage Improvements

**ADDRESS & LEGAL DESCRIPTION:**

**OWNER:**

MPM Investments  
35 Second Street  
Ashland, OR 97520

**ARCHITECTS:**

Kistler Small & White  
66 Water Street  
Ashland, OR 97520  
541.488.8200

**LAND USE PLANNING:**

Kistler, Small & White  
66 Water Street  
Ashland, OR 97520  
541.4883.8200

**PROJECT LANDSCAPE:**

Kistler Small & White  
66 Water Street  
Ashland, OR 97520  
541.488.8200

**COMPREHENSIVE PLAN DESIGNATION:** Commercial-Downtown

**ZONING DESIGNATION:** C-1-D

**ADDRESS:** Winchester Inn, 35 South Second Street

**MAP AND TAX LOT:** 39 1e 09BD, Tax Lots 5600, 5700

**ACREAGE:** .32 Acres

**CURRENT OCCUPANCY TYPE:** R-1, B

# kistler + small + white

architects

**PROPOSED OCCUPANCY TYPE:** R-1, A-2, B, S-2

**BUILDING CONSTRUCTION TYPE:** Type VB

**FIRE SPRINKLER SYSTEM:** Yes

**EXISTING PARKING:** Nineteen (19) on site. The additions do not require more parking to be added, existing parking count to remain unchanged.

## MAIN FACILITY SQUARE FOOTAGE:

<b>Floor:</b>	<b>Existing:</b>	<b>Proposed:</b>	<b>Total:</b>
Basement	2,450 s.f.	360 s.f.	2,810 s.f.
First Floor	2,252 s.f.	1,601 s.f.	3,853 s.f.
Second Floor	1,962 s.f.	1,090 s.f.	3,052 s.f.
<b>TOTAL</b>	<b>6,664 s.f.</b>	<b>3,051 s.f.</b>	<b>9,715 s.f.</b>

## APPLICABLE ORDINANCES

C-1-D, Commercial Downtown

Site Design & Use Standards, Chapter 18.4.2

Basic Site Review, Section 18.4.2.040

Historic District Design Standards, 18.4.2.050

Downtown Design Standards, 18.4.2.060

Conditional-Use for Hotel/Restaurant/Bar use in the C-1-D district

Street Standard Exception, Chapter 18.4.6.040

Tree Preservation and Protection, Chapter 18.5.7

## ADJACENT ZONING/USE

NORTH: C-1-D

EAST: C-1-D

SOUTH: R2

WEST: C-1-D

SUBJECT SITE: C-1-D

## PROJECT DESCRIPTION

**INTRODUCTION:** The Winchester Inn was open in July of 1983 by Laurie and Michael Gibbs, and ranks among Ashland's top Bed and Breakfasts, with nationally acclaimed restaurant and award winning gardens. The applicant, MPM Investments, is requesting approval for a Site Review to build a new bar, a new kitchen, two (2) offices, a laundry room, provided a new accessible lift, new accessible entries and remodel the existing kitchen into a bussing station and storage areas. These improvements will greatly improve the guest experience and improve overall conditions and safety for the Inn's staff.

The site consists of two adjoining tax lots that will be consolidated into one to accommodate the new addition.

A written narrative and finding of fact as well as a Site Plan and Building Elevations are enclosed. This information is provided in the application materials and addresses the submittal requirements of Chapter 18.3.9

**ADDITIONS:** There are two proposed additions, the bar addition to the south and the kitchen addition to the west. The "west" addition would include a new kitchen, laundry room and offices and provide cover for the existing parking at ground level. This addition will extend to the westerly property line with a two (2) foot setback. The second addition, to the south, is a bar adjoining *Alchemy*. The proposed total additional square footage is 3,051 s.f.. The additions are the style of the existing Victorian and designed to look as cohesive as possible.

### SOUTH ADDITION:

**NEW BAR:** The proposed +/-360 s.f. bar is designed as a Victorian Conservatory. It is to be located to the south side of the existing main building adjacent to the existing dining area. The new addition will be physically attached to the main building with stairs and an accessible lift connecting the two levels. The bar will include an outdoor patio area located to the east of the new bar, interior seating for roughly twenty (20), a bar seating roughly five (5) and a serving area for preparing drinks.

### WEST ADDITION:

**KITCHEN:** The new kitchen will occupy the first floor of the new addition proposed for the west side of the main facility. The new kitchen will replace the outdated, inefficient and undersized existing kitchen. It will be a full service kitchen including, but not limited, to a new walk-in cooler, range hood, grille, range, dishwashing equipment and storage. The kitchen will vastly improve the service provided to customers and safety of

# kistler + small + white

architects

those working in the kitchen. The proposed new square footage for the kitchen is +/- 869 s.f.

**NEW LAUNDRY ROOM:** The proposed new laundry room will replace the existing one located in the basement. The new laundry room will be located at the current grade elevation and below the new kitchen. The proposed new square footage is +/- 193 s.f.

**NEW OFFICES:** The two (2) new offices will be part of the new addition above the new kitchen. It is hoped, at some future time, these offices may be converted into suites. The applicant understands that all the additional conditions of the conversion would have to comply with code at that time. In the mean time, the applicant have off-site offices that can be moved on-site.

**REMODEL:** The remodeled area consists of converting the existing kitchen into a new bus station and storage. Remodel of existing laundry room will create a new accessible route to the basement utilizing the accessible lift.

## FINDING OF FACT

The following information has been provided by the applicants to help the Planning Staff, Planning Commission and neighbors better understand the proposed project. In addition, the required *findings of fact* have been provided to ensure the proposed project meets the Site Design & Use Standards as outlined in the Ashland Municipal Code (AMC), 18.4.2, Site Design & Use Standards

*For clarity reasons, the following documentation has been formatted in "outline" form with the City's approval criteria noted in **BOLD** font and the applicant's response in regular font. Also, there are a number of responses that are repeated in order to ensure that the findings of fact are complete. Where appropriate numbering follows the sited AMC.*

## CHAPTER 18.4.2.040 -.060, DOWNTOWN HISTORIC DISTRICT.

### **18.4.2.040.B NON-RESIDENTIAL DEVELOPMENT/BASIC SITE REVIEW STANDARDS:**

#### **1. Orientation and Scale:**

- a. The existing orientation faces the street and will not be altered. The existing parking and vehicular circulation will not be altered.
- b. The existing facade arrangement will not be altered
- c. The existing building will not be altered and remains oriented towards 2nd Street.
- d. The existing setback at the front is already established and will not be altered.
- e. The building entrance faces 2nd Street, the primary street
- f. It is the intent of the Applicant to use the existing sidewalk.
- g. N/A

- 2. Streetscape:** The site has significant mature trees which are in compliance with the streetscape standard of one tree per 30 ft. A Parkrow frontage improvement would be undesirable because of the significant trees that exist on the site. The applicant is requesting an exception, see page 16.

#### **3. Buffering and Screening:**

- a. Existing mature landscaping buffers the site on 2nd Street and Hargadine Street. The other two sides of the site are boarded by alleys.

- b. Parking areas will remain unchanged and are buffered by existing landscaping where boarding the residential zoned area to the South.

**4. Building Material:**

- a. Building materials will be similar to the existing Inn including, wood siding, wood trim and wood shingle roofing. The exception to the matching materials will be in the bar addition which will have a metal roof, exposed concrete base and wood windows with aluminum cladding. The overall building will not have glass as a majority of the skin, but glass will be used to create a conservatory feel in the bar addition.
- b. The west addition will be painted to match the existing historic Inn.

**18.4.2.050.B HISTORIC DISTRICT/HISTORIC DISTRICT DESIGN STANDARDS:**

1. **Transitional Areas:** The location of the existing building may be considered occupying a transitional zone between the downtown commercial area and the adjacent residential area to the south of the site.
2. **Height:** The existing height of the building is unchanged.
3. **Scale:** The bar addition is located on the East face of the existing building. This addition is set well back from the front of the building facing 2nd street, the existing main entrance will remain prominent, also the new addition will be set back from the original house bay window. In addition, it will provide a single story "step up" to the existing two story building. The most impacted view is from Hargadine Street, which is at a higher elevation, reducing the perceived scale. Larkspur cottage is between the new construction and Hargadine, also reducing the impact of the new addition.
4. **Massing:** The massing of the building will continue to be appropriate to the existing Victorian architecture, and smaller than the other commercial buildings in the immediate neighborhood.
5. **Setback:** The setback of the existing building will not be altered.
6. **Roof:** The roof of the additions will be consistent with the existing building.
7. **Rhythms of Openings:** The rhythms of the primary facade will remain unchanged.

# kistler + small + white

architects

this addition a more modern feel and designation its use as different.

- f. **Imitative materials:** N/A
- g. **Replacement windows:** Windows in the new additions shall be compatible in proportion, shape and size, with the exception of the bar addition, where the windows will be specific to the "conservatory" feature.
- h. **Reconstructed Roofs:** The roofs on the west addition will match the pitch and form of the historic building. The roof on the bar addition will be at a different height and pitch.
- i. **Asphalt or composition shingle roofs:** For cohesiveness, the roof will be wood shingle as with the historic building. The bar addition will have a metal roof appropriate to a conservatory or natatorium.
- j. **New porches:** N/A
- k. **New detached buildings:** N/A
- l. **Standards for Rehabilitation:** N/A

**18.4.2.060.C DOWNTOWN ASHLAND/DOWNTOWN DESIGN STANDARDS:**

1. **Height:** The height of the additions will be slightly dissimilar to the original building to maintain the traditional staggered streetscape.
2. **Setback:**
  - a. The original setback will not be changed.
  - b. Primary entry will not be changed.
  - c. N/A
3. **Width:**
  - a. N/A, the historic building is not in compliance with this design standard.
  - b. N/A
4. **Openings:** N/A; While in the Downtown area the Winchester Inn is a stand-alone structure and has a residential style as opposed to a commercial building.
5. **Horizontal Rhythms:** N/A; The historic building is a stand-alone structure and has a residential style as opposed to a commercial building.
6. **Vertical Rhythms:** N/A; The historic building is a commercial building structure and has a residential style as opposed to a store front.
7. **Roof Forms:** N/A; The historic building is a stand-alone structure and has a residential style as opposed to a commercial building.
8. **Materials:**
  - a. The exterior of the building additions will be painted wood with a concrete base.
  - b. The building will have Victorian features for visual interest.
9. **Awnings:**
  - a. N/A
  - b. N/A
  - c. N/A
10. **Non-street or alley elevations:**
  - a. The exterior of the building will be painted wood with a concrete base.
  - b. Visual integrity of the original building will be maintained.
  - c. N/A

- d. The parking will remain unchanged.
- e. The site has existing brick and stamped concrete pathways incorporated into the mature landscaping.
- f. N/A

**11. Exceptions to Standards:** Many of the Downtown Design Standards do not apply because the Winchester Inn is not a "Store Front" commercial building.

**18.5.4.050.A Conditional Use Permit for Hotel uses within the C-1-D district.**

The Winchester Inn has been operating as a hotel since 1983. The additions requested would enhance the facility, guest experience and working environment with minimal impact on the neighborhood. Most of the visual impact of changes are oriented toward the rear of the site on an alley. The buildings bordering the alley are the back of the OSF and Oregon Cabaret buildings, and therefore will have little or no impediment to the neighboring views. See photos below.



Adjacent Buildings Across the Alley to the West

At this point no additional guest suites are being added so there should be no additional impact on parking or traffic. The improvements will allow additional seating capacity in the restaurant and bar but they will still serve primarily guest of the Inn or others that arrive by walking. Also there will be handicapped access to the restaurant and bar that is not currently available.

The applicant and architect believe the conservatory style bar will be a unique and exciting addition to the Inn and to Ashland's downtown.

**18.104.050 Approval Criteria**

**A conditional use permit shall be granted if the approval authority finds that the proposed use conforms, or can be made to conform through the imposition of conditions, with the following approval criteria.**

- 1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.** The proposed Conditional-Use is a permissible use in with C-1-D. Hotel/Motel, as already permitted.
- 2. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.** An Electric Utility Plan will be developed in consultation with the City's Electric Department, Dave Tygerson, to ensure not only capacities can be accommodated, but to also minimize aesthetic impact to the proposed building. All electrical services will be provided from Second Street where the service currently exist. All electrical work will be completed under the direction of the Ashland Electric and Building Departments. Further, all improvements within the adjacent rights-of-way, including construction detouring, will be completed under the direction of the Ashland Engineering Department

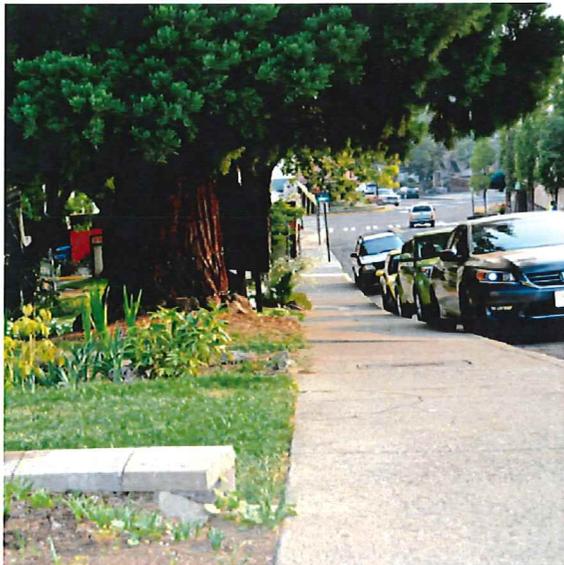
Applicants have addressed or will address at the time of the building permit all code issues relating to the Ashland Fire Department, including an FDC valve along the front of the building. Two fire hydrants are within 150' of the property boundary (on the property, at the corner of 2nd and Hargadine, and on adjacent tax lot at the corner of 2nd Street and Endor's Alley) with adequate pressure to service the building. All work will be completed under the direction of the Ashland Building and/or Fire Departments.

- 3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone: The site is already being used in this capacity.**
  - a. Similarity in scale, bulk, and coverage.** The proposal has been designed to limit the impact of the increase of bulk and are well within the site and location scale.

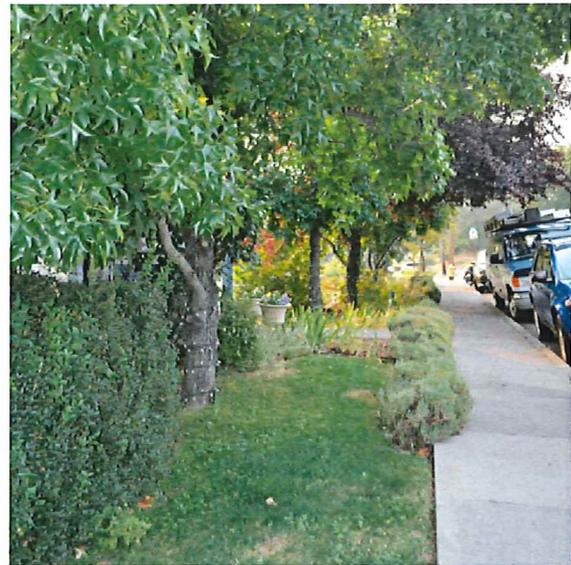


**18.4.6.040 Exception to Street Standards**

**Frontage Improvements:** The applicants request an exception to the required frontage improvement of a parkrow. In this case it would not be desirable to remove existing significant trees in order to create the parkrow.



2nd Street



Hargadine

**1. Exception to the Street Design Standards.**

- a. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site of proposed use of the site.** The requirement to upgrade to a parkrow would be undesirable because of the significant trees that would be removed making this requirement counter-productive. See photos above.
- b. The exception will result in equal or superior transportation facilities and connectivity considering the following factors where applicable.**
  - i. For transit facilities and related improvements, access, wait time, and ride experience.**
  - ii. For bicycle facilities, feeling of safety, quality of experience (i.e., comfort level of bicycling along the roadway), and frequency of conflicts with vehicle cross traffic.**

- iii. **For pedestrian facilities, feeling of safety, quality of experience (i.e., comfort level of walking along roadway), and ability to safety and efficiency crossing roadway.**
  - i, ii, ii There will be no diminishing effect to transit, safety for bicycles or pedestrians.
  
- c. **The exception is the minimum necessary to alleviate the difficulty.** The exception would leave in place the current situation.
  
- d. **The exception is consistent with the Purpose and Intent of the Street Standards in subsection 18.4.6.040 A.** At this site, maintaining the current landscaping is more aligned with the *purpose and intent of the street standards*, than requiring a parkrow.

## TREE PRESERVATION AND PROTECTION

### 18.4.5 TREE PRESERVATION AND PROTECTION

The applicants will request a tree removal permit for two (2) trees that are within the proposed footprint of the new addition. There are many mature trees on the site and the removal of these effected trees will not change the overall feel of tree coverage on the site.



Overview of Site

Tree Protection Plan: A plan to identify and protect plan will be submitted. The plan will included a survey of the Trees on site and measures that will be taken for their protection during construction.

#### 18.5.7.040.B Tree Removal Permit

##### 2. Tree That in Not a Hazard:

- a. **The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.**

- b. Removal of the tree will not have a significant negative impact on erosion, soil stability, floor of surface waters, protection of adjacent trees, or existing windbreaks.** The removal of the trees will not have a significant negative impact on erosion, soil stability, surface waters, adjacent trees or existing windbreaks.
- c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.** The site has many mature trees and removal of the subject trees will not significant negatively impact the overall tree canopy. See photo above.
- d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider City of Ashland 5-59 Land Use Ordinance 18.5.7 – Tree Removal Permits alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.** N/A
- e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.**

# kistler + small + white

architects

## ADDITIONAL ISSUES FROM PLANNING STAFF'S COMMENTS:

**Property Line Issue:** The tax lots will be consolidated in order to resolve the problem of the addition crossing over a property line.

**Landscaping:** The site has mature park-like landscaping. The proposed plan will have minimal impact on the existing gardens. The landscaping plan post-construction will be to replant any areas that have been disturbed.



Award Winning Gardens

Prepared and Respectfully Submitted by:

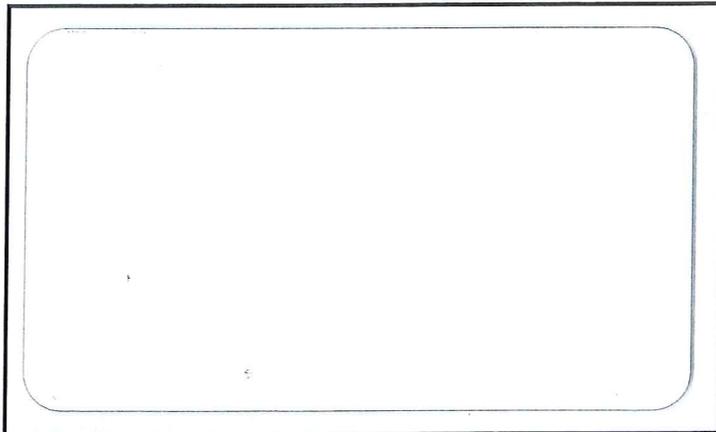
Leslie Gore, Planning Consultant  
Kistler Small + White, Architect

Date

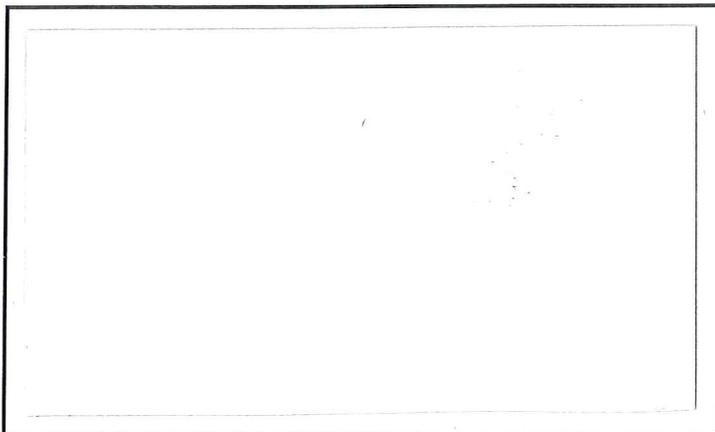
8/5/2015



METAL ROOFING: ZINCALUME



CLAD WINDOW FRAMES & WOOD TRIM: VANILLA CREAM



WOOD SIDING: MILLER PAINT - ENTRY GATE

RECEIVED  
 AUG 14 2015  
 City of Ashland

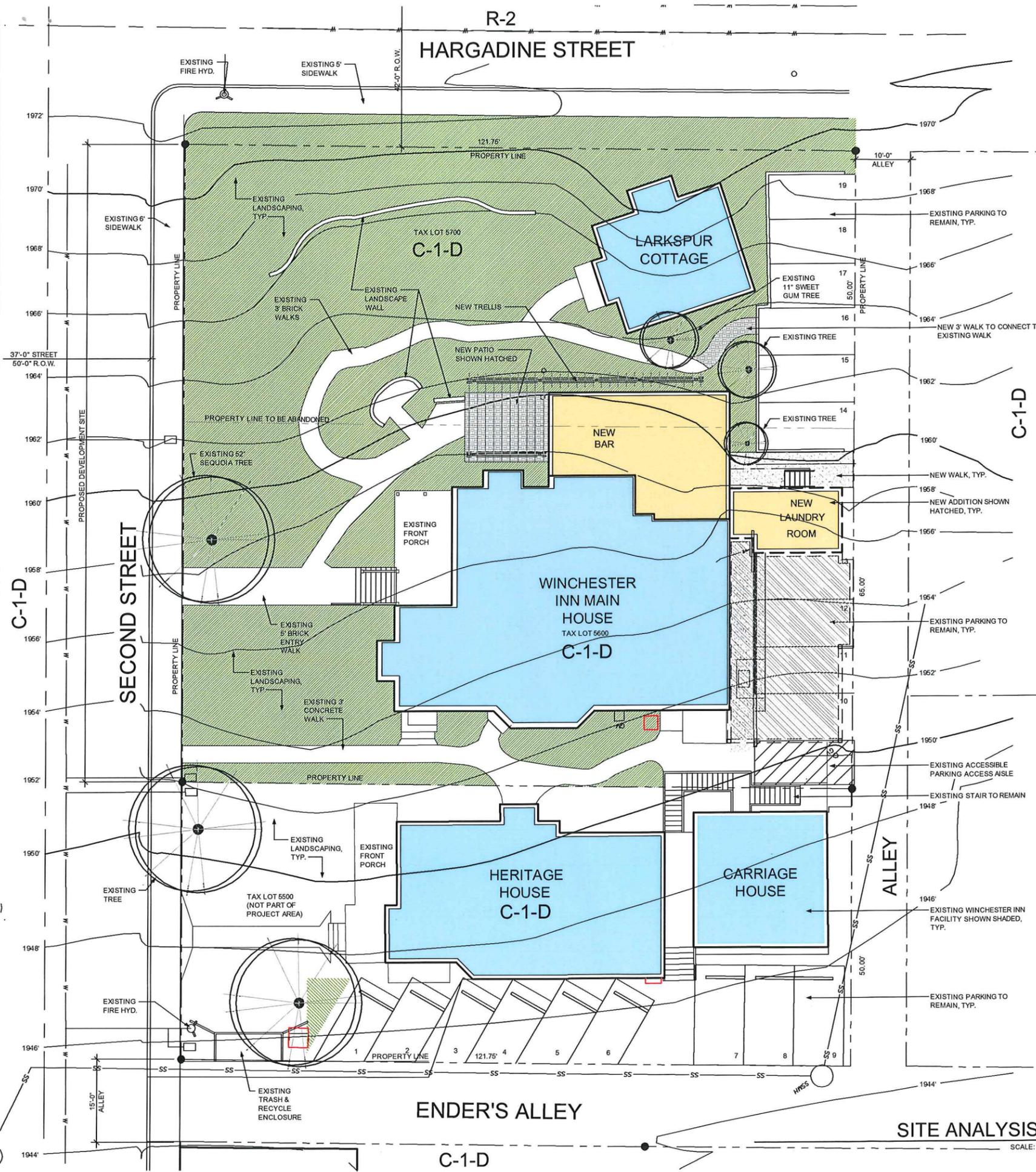
kistler+  
 small  
 +white  
 architects  
 (541) 488 8200  
 66 Water Street  
 Ashland, OR  
 97520

WINCHESTER INN -  
 REMODEL AND ADDITION

ASHLAND, OR

EXTERIOR FINISH MATERIALS	F1
SCALE: NO SCALE	
08.14.15	
15-016	

© 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.

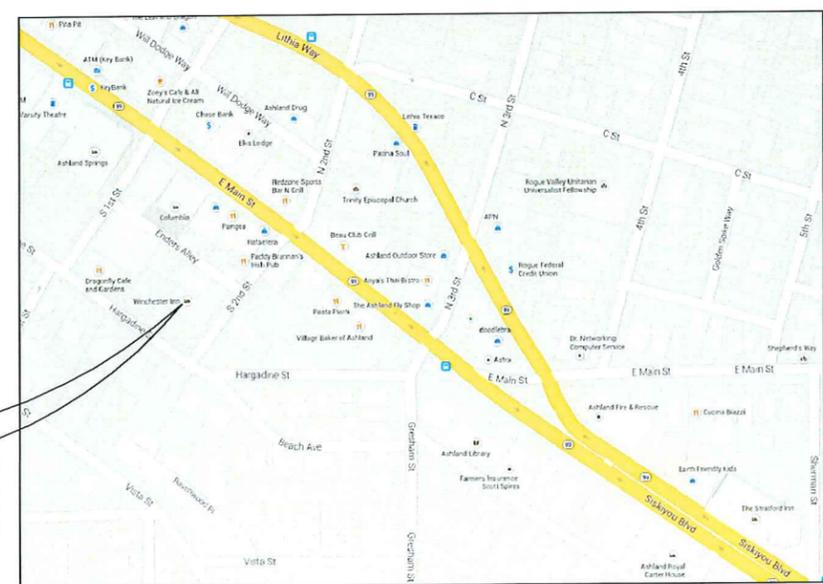
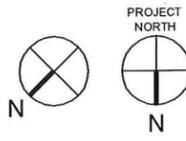


- LEGEND:**
- LANDSCAPE AREA (PERVIOUS)
  - EXISTING BUILDING
  - NEW FIRST FLOOR ADDITION ABOVE EXISTING PARKING
  - NEW GROUND FLOOR ADDITION
  - NEW PAVING
  - EXISTING FIRE HYDRANT - (2) PLACES

PROJECT AREA = 13965 SQ.FT.  
= 0.3206 ACRES

PROJECT AREA CONSISTS OF TAX LOTS 5600 AND 5700 TO BE COMBINED INTO ONE TAX LOT AS PART OF THIS PROJECT

**PROJECT LOCATION**



**SITE ANALYSIS MAP**  
SCALE: 1" = 20' 11x17'

1

**VICINITY MAP**

2

COPY

**kistler+small+white**  
architects.

66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200

**WINCHESTER INN  
REMODEL AND ADDITION**

35 SOUTH SECOND STREET.

ASHLAND, OREGON

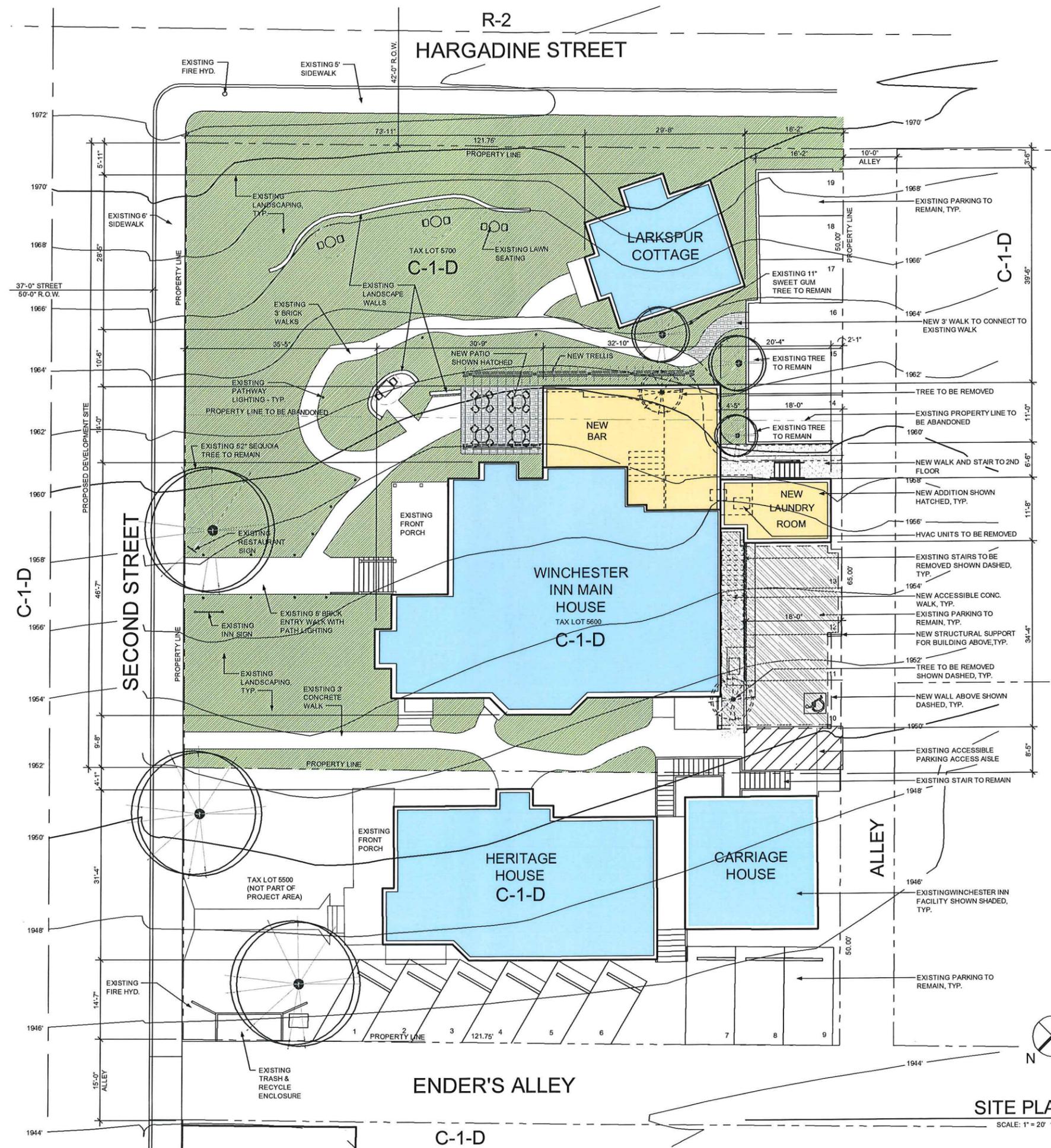
REVISIONS

**SITE ANALYSIS MAP**

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

1

© 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.



**LEGEND:**

- LANDSCAPE AREA (PERVIOUS)
- EXISTING BUILDING
- NEW FIRST FLOOR ADDITION ABOVE EXISTING PARKING
- NEW GROUND FLOOR ADDITION
- NEW PAVING
- EXISTING FIRE HYDRANT - (2) PLACES

PROJECT AREA = 13965 SQ.FT.  
= 0.3206 ACRES

PROJECT AREA CONSISTS OF TAX LOTS 5600 AND 5700 TO BE COMBINED INTO ONE TAX LOT AS PART OF THIS PROJECT

PROJECT NORTH  
N

N

**SITE PLAN** 1

SCALE: 1" = 20' 11x17

**kistler+small+white**  
architects

66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200

ASHLAND, OREGON

35 SOUTH SECOND STREET.

**WINCHESTER INN  
REMODEL AND ADDITION**

REVISIONS

**SITE PLAN**

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

**2**



NORTH ELEVATION

SCALE: 3/16" = 1'-0"



EAST ELEVATION - SECOND ST.

SCALE: 3/32" = 1'-0"

RECEIVED  
AUG 23 2015  
City Of Ashland

C: 2015 Kistler, Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler, Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler, Small + White Architects



SOUTH ELEVATION - HARGADINE ST.

SCALE: 3/16" = 1'-0"



WEST ELEVATION - ALLEY

SCALE: 3/32" = 1'-0"

ASHLAND, OREGON

WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET.

REVISIONS

RECEIVED  
AUG 23 2015  
City Of Ashland

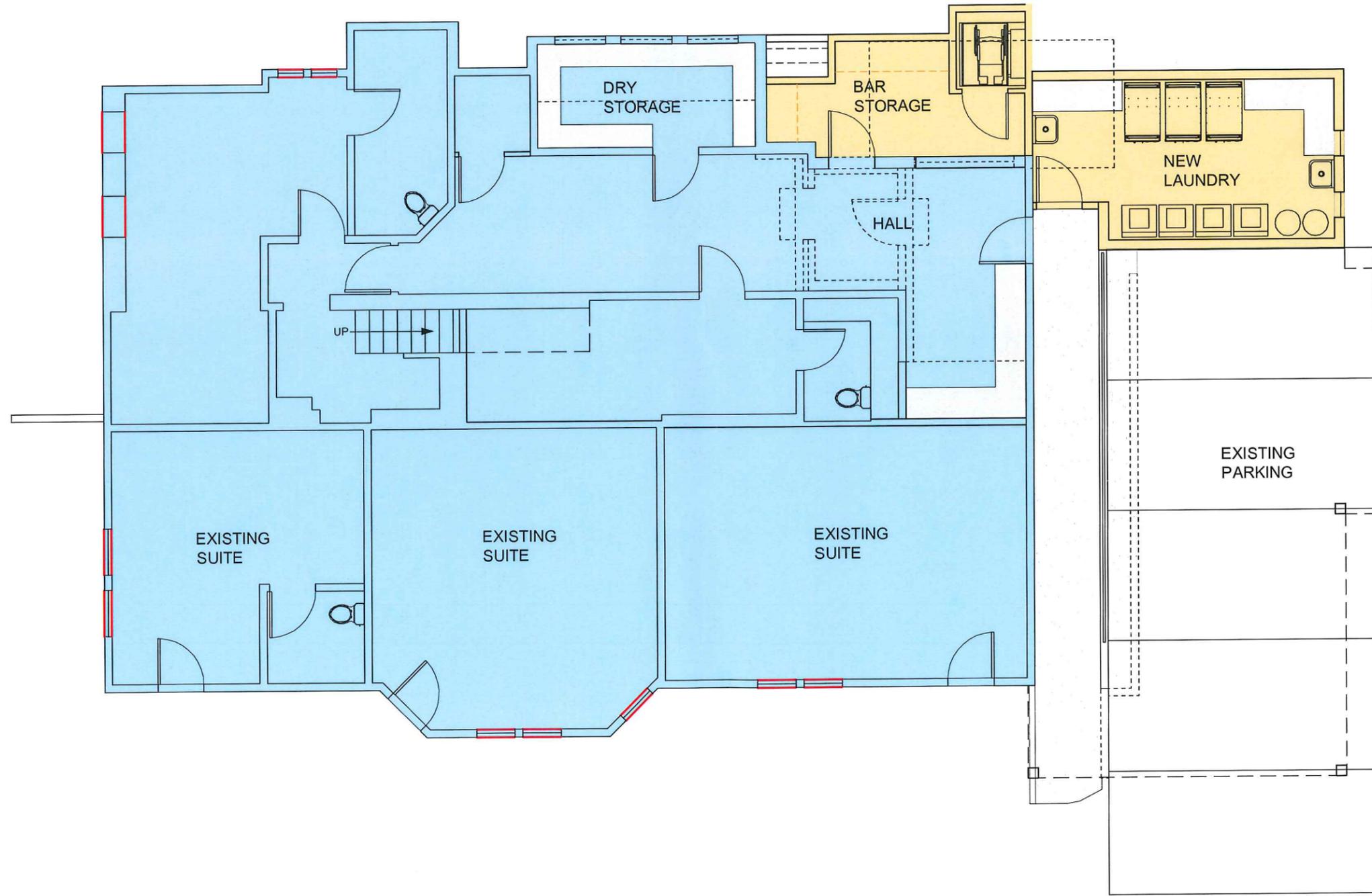
EXTERIOR  
ELEVATIONS

PROJECT: 15-016

ISSUE DATE: 08-07-15

SHEET:

© 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.



**BASEMENT FLOOR PLAN**  
1/4" = 1'-0"

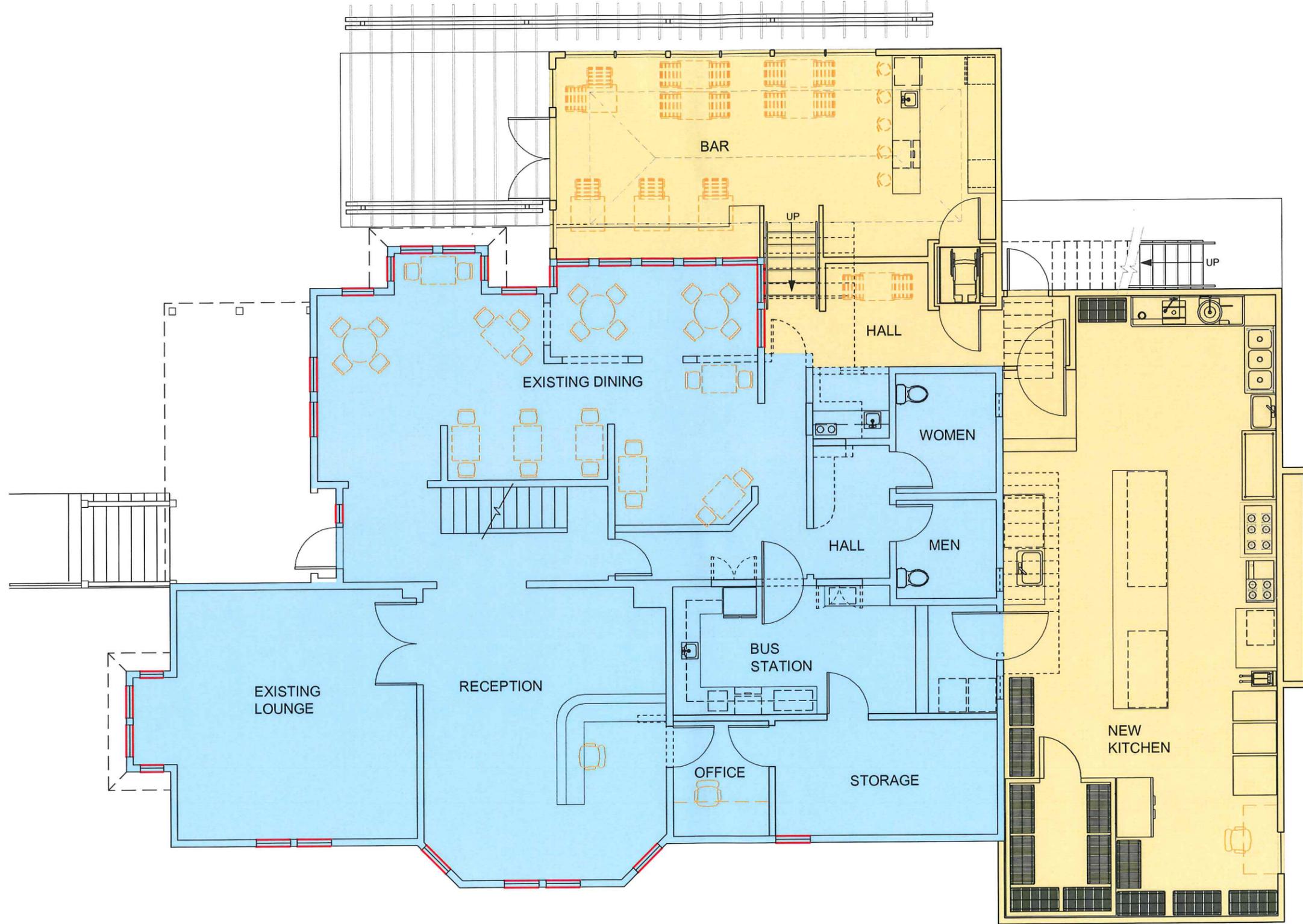


1

REVISIONS

FIRST FLOOR  
PLAN

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:



FIRST FLOOR PLAN

1/4" = 1'-0"

1

WINCHESTER INN  
REMODEL AND ADDITION

35 SOUTH SECOND STREET.

ASHLAND, OREGON

REVISIONS

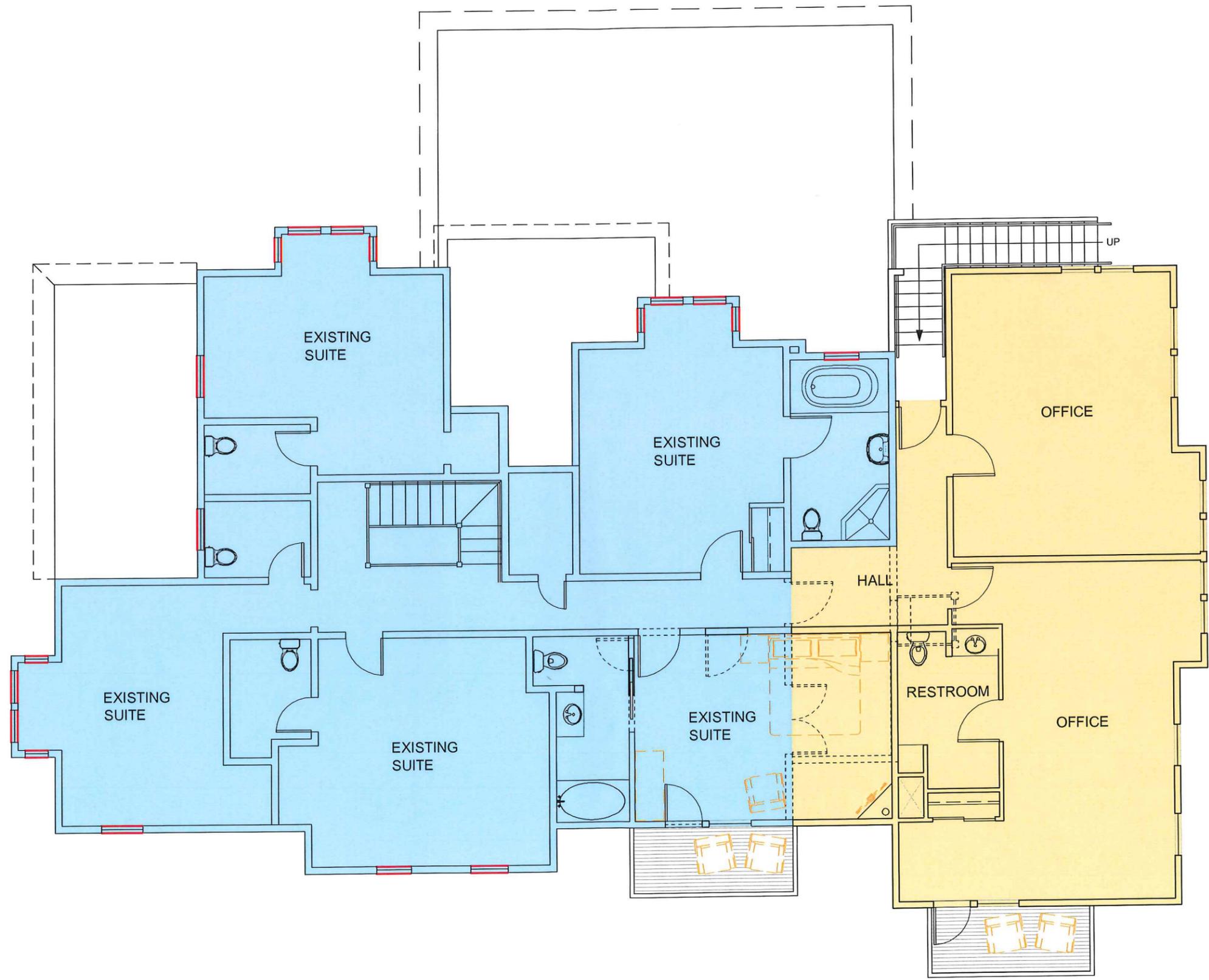
SECOND FLOOR  
PLAN

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

© 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.



66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200



SECOND FLOOR PLAN

1/4" = 1'-0"

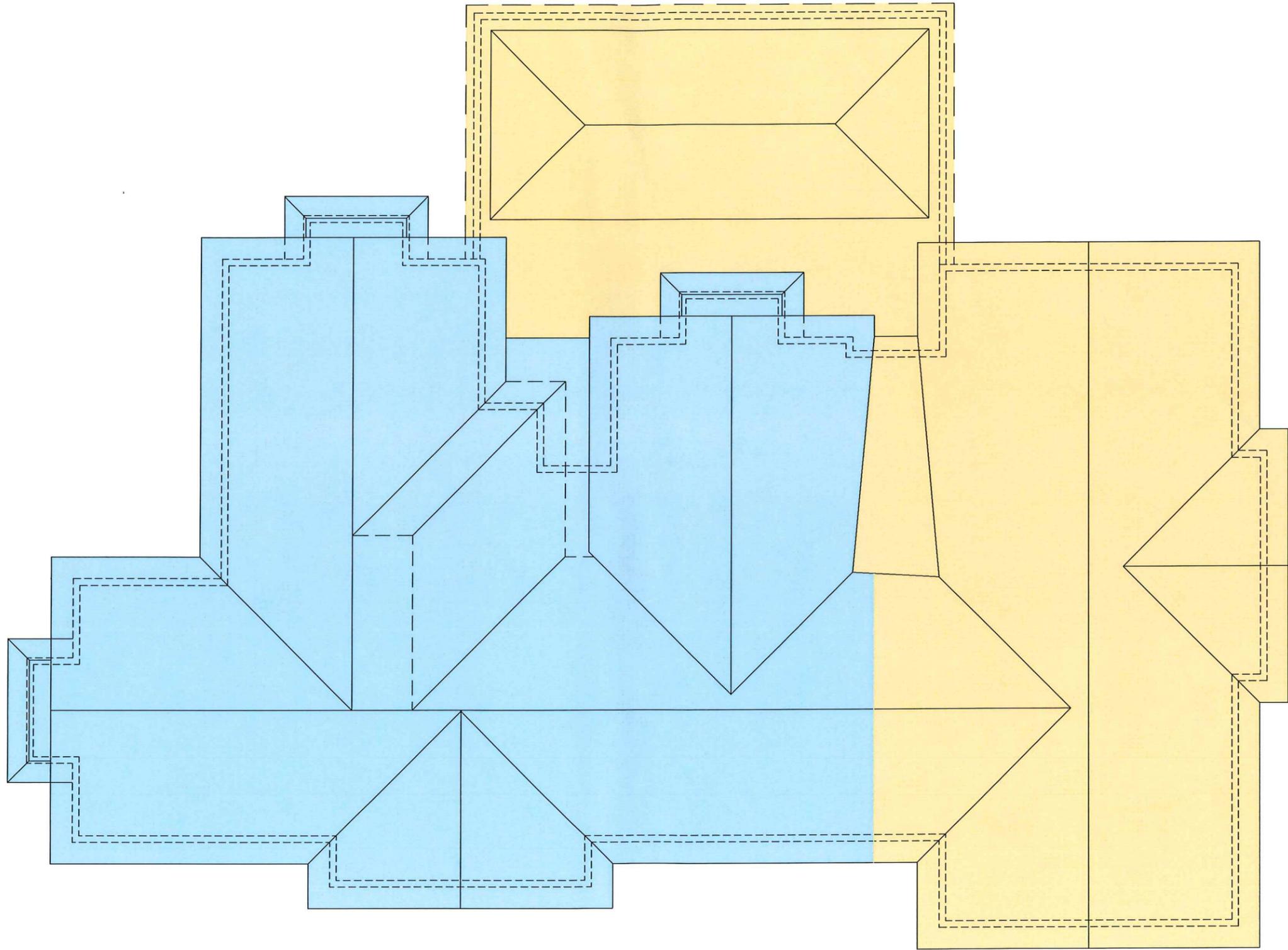
1

WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET.  
ASHLAND, OREGON

REVISIONS

THIRD FLOOR  
PLAN

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:



ROOF PLAN

1/4" = 1'-0"



1

kistler+  
small  
+white  
architects

66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200

WINCHESTER INN  
REMODEL AND ADDITION

35 SOUTH SECOND STREET.

ASHLAND, OREGON

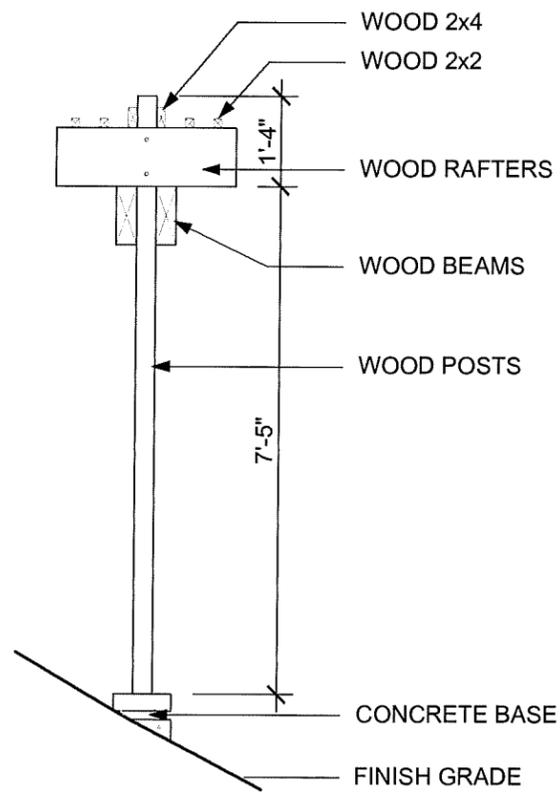
REVISIONS

ROOF PLAN

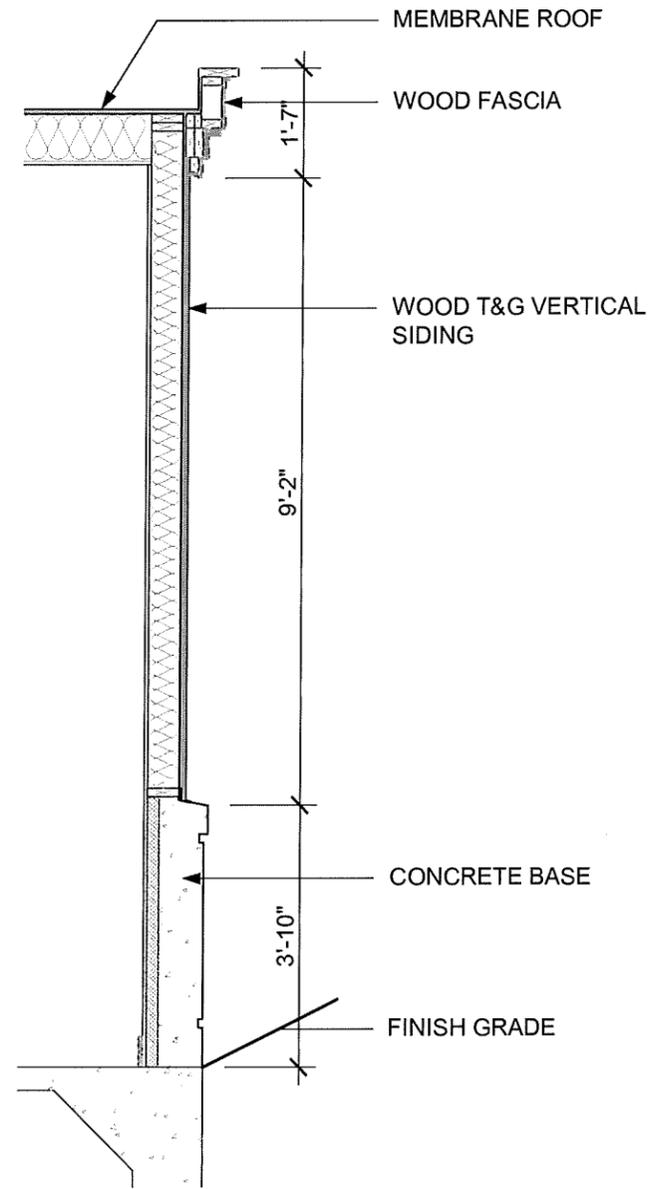
PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

8

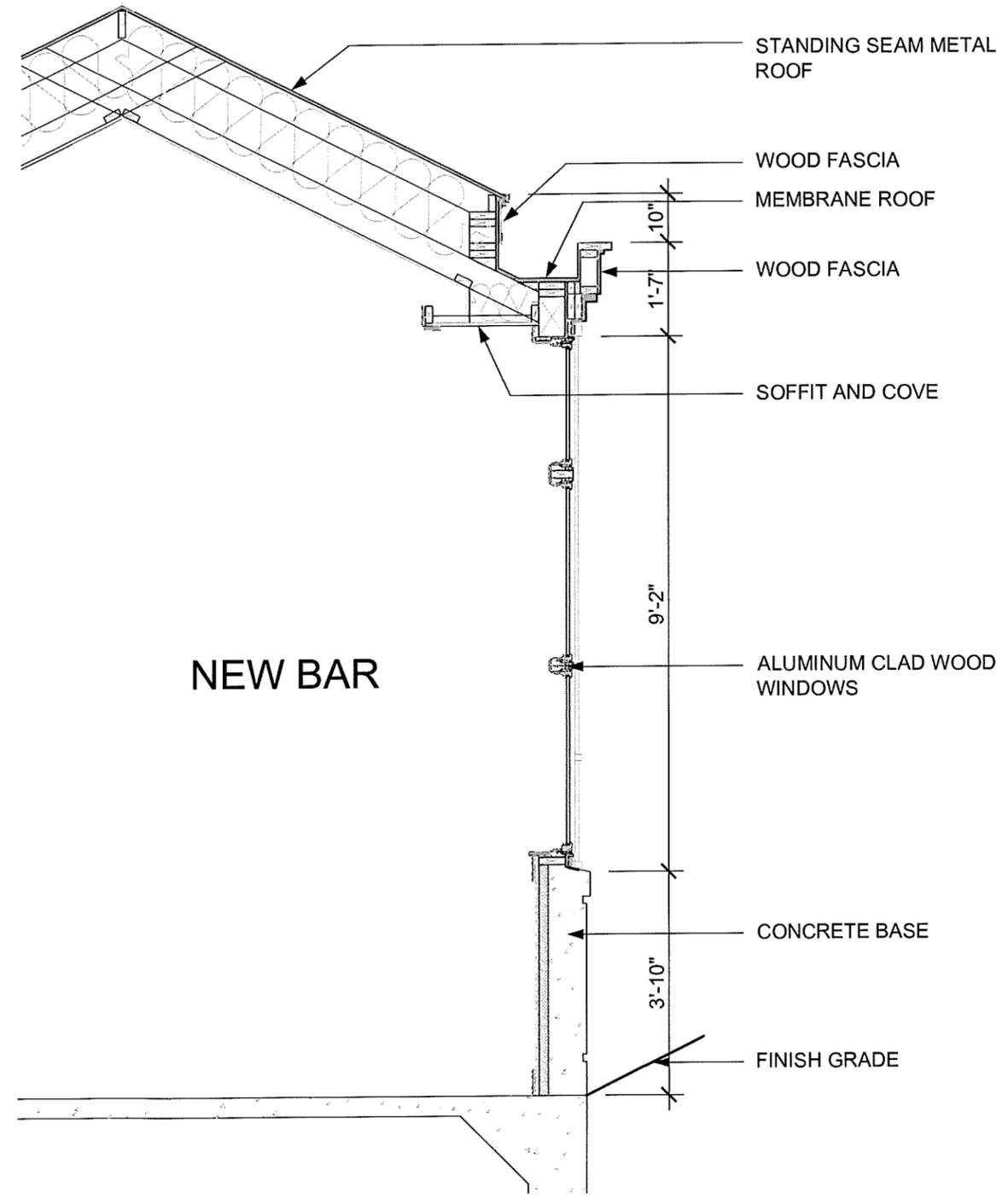
© 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.



**TRELLIS SECTION** ②  
 3/4" = 1'-0" 24 x 36  
 3/8" = 1'-0" 11 x 17



**WALL SECTION** ②  
 3/4" = 1'-0" 24 x 36  
 3/8" = 1'-0" 11 x 17



**NEW BAR**

**WALL SECTION** ①  
 3/4" = 1'-0" 24 x 36  
 3/8" = 1'-0" 11 x 17

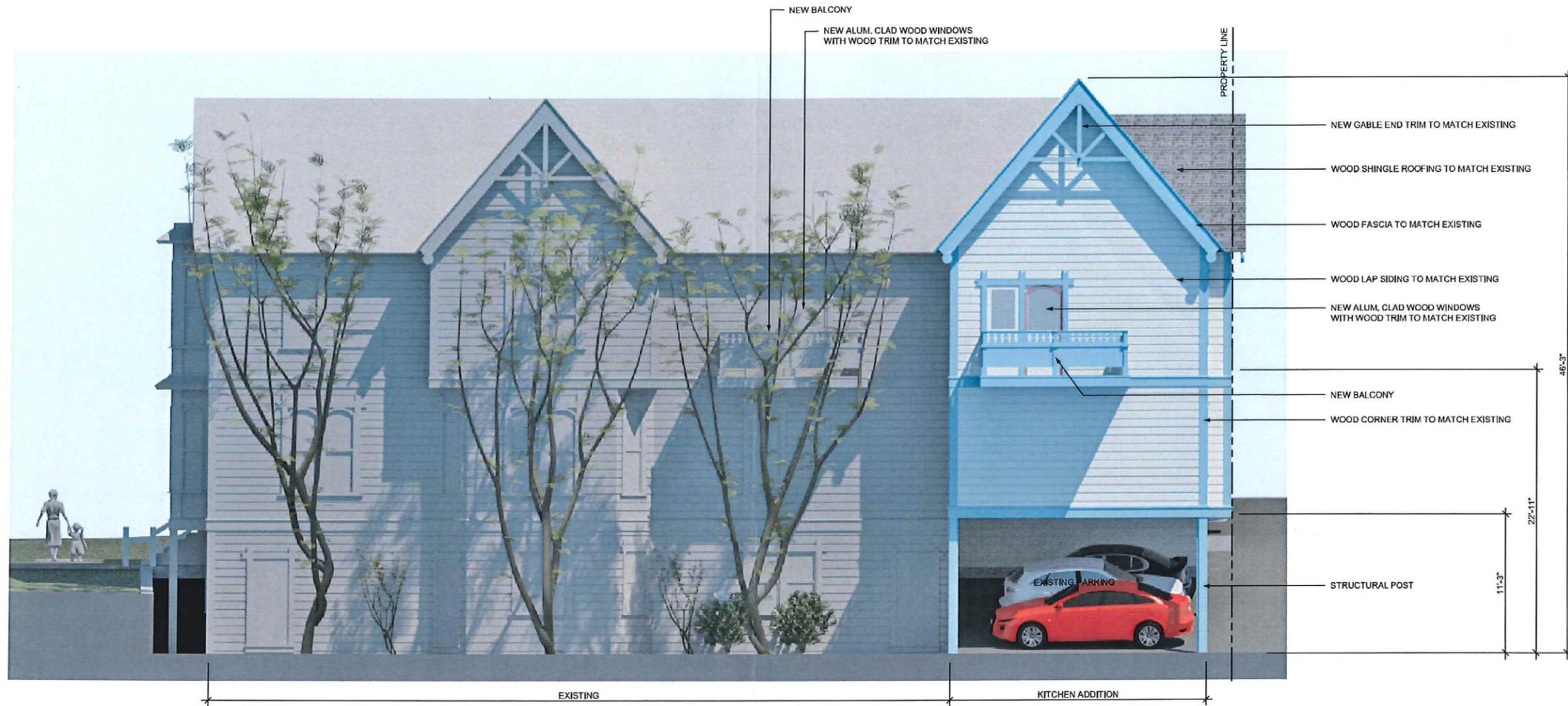
WINCHESTER INN  
 REMODEL AND ADDITION  
 35 SOUTH SECOND STREET. ASHLAND, OREGON

REVISIONS	

WALL SECTIONS  
 PROJECT: 15-016  
 ISSUE DATE: 08-07-15  
 SHEET:

COPY

© 2015 Kistler Small + White Architects, LLC This document, and the fees and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects



NORTH ELEVATION



EAST ELEVATION - SECOND ST.

RECEIVED  
AUG 14 2015  
City of Ashland

kistler+  
small  
+white

66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200

ASHLAND, OREGON

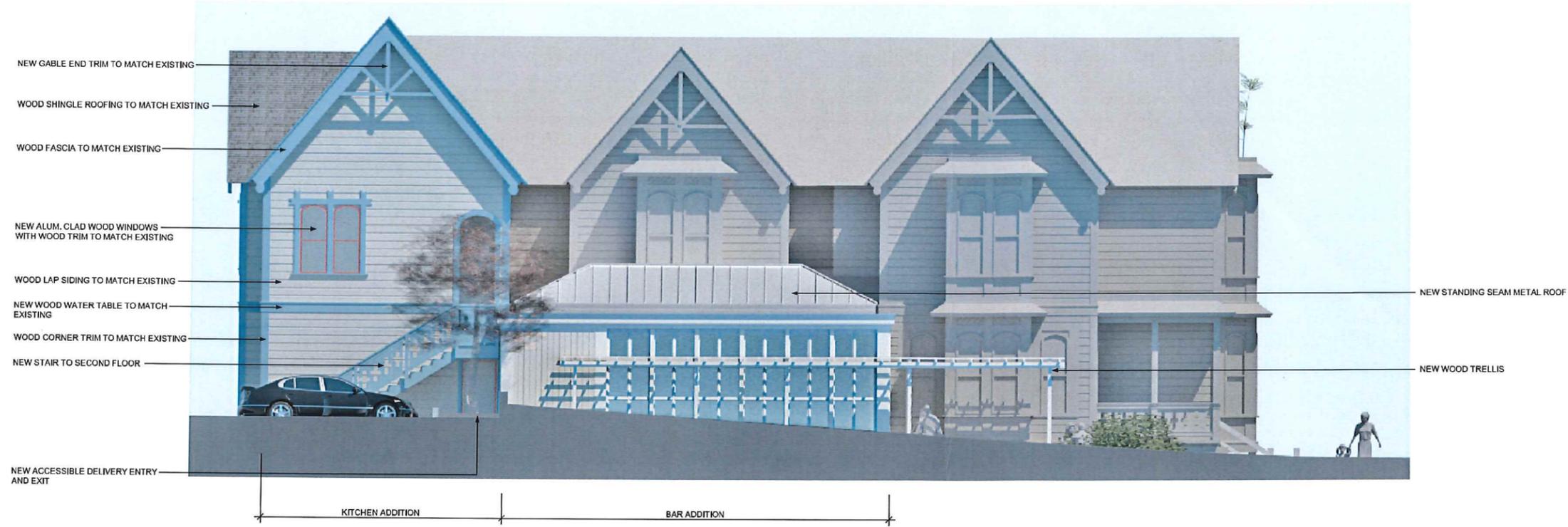
WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET.

REVISIONS

EXTERIOR  
ELEVATIONS

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

© 2015 Kistler Small + White Architects, LLC This document and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects



SOUTH ELEVATION - HARGADINE ST.



WEST ELEVATION - ALLEY

RECEIVED  
AUG 14 2015  
City of Ashland

kistler+  
small  
+white

88 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200

ASHLAND, OREGON

WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET.

REVISIONS

EXTERIOR  
ELEVATIONS

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

RECEIVED

AUG 14 2015

City of Ashland

kistler+  
small  
+white

68 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200



BAR ADDITION CLOSE PERSPECTIVE



NORTH-WEST PERSPECTIVE



PERSPECTIVE FROM INTERSECTION



SOUTH WEST PERSPECTIVE

ASHLAND, OREGON

WINCHESTER INN  
REMODEL AND ADDITION

35 SOUTH SECOND STREET.

REVISIONS

EXTERIOR  
ELEVATIONS

PROJECT: 15-016

ISSUE DATE: 08-07-15

SHEET:

RECEIVED  
AUG 14 2015  
City of Ashland

kistler+  
small  
+white  
architects  
68 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200



WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET. ASHLAND, OREGON

REVISIONS

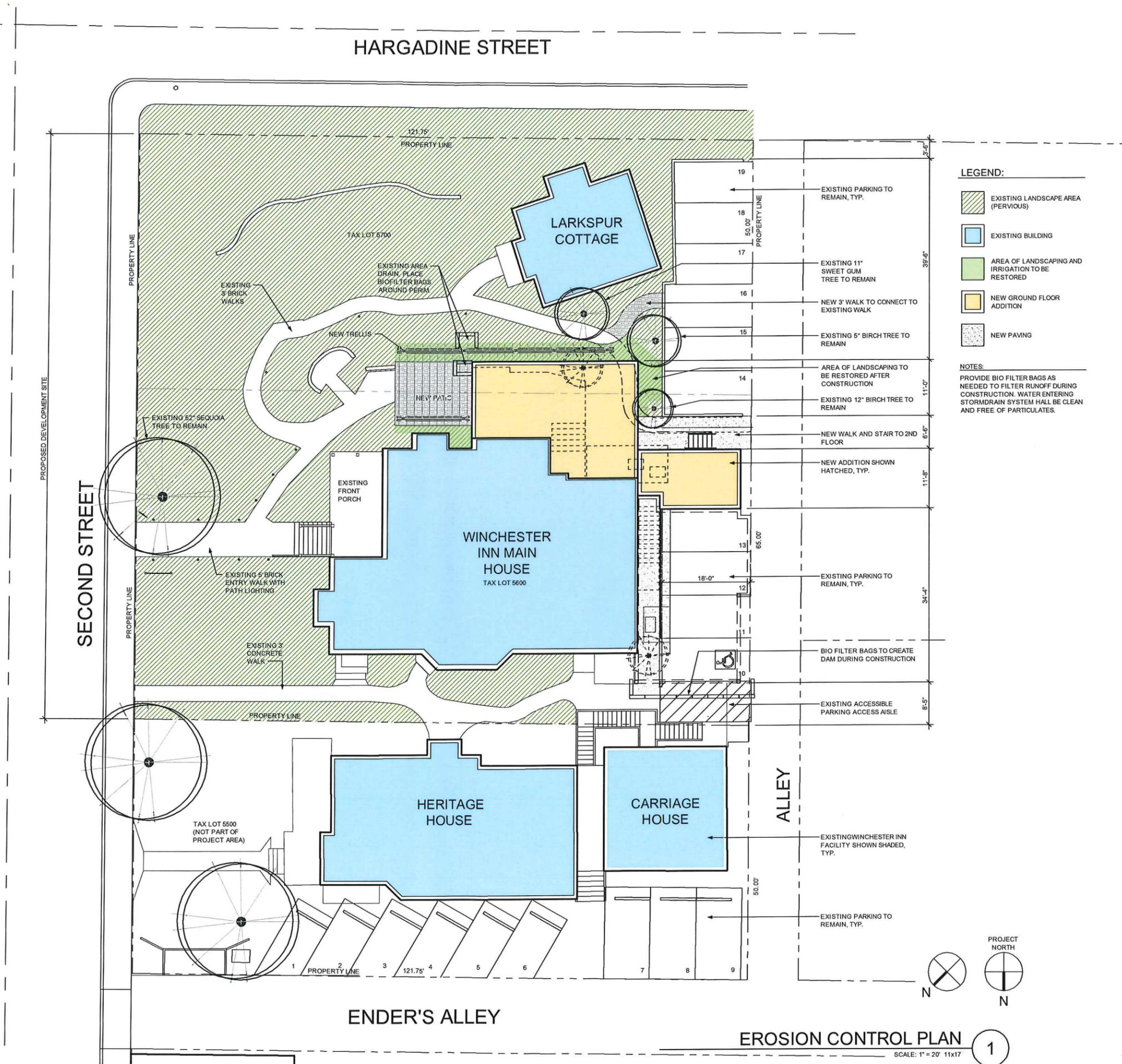
EXTERIOR  
ELEVATIONS

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

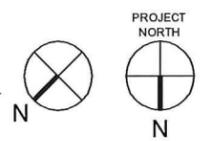
BAR PATIO PERSPECTIVE

C: 2015 Kistler Small + White Architects, LLC This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of Kistler Small + White Architects and is not to be used, in whole or in part, for any other project without the written authorization of Kistler Small + White Architects.

**kistler + small + white**  
architects  
66 WATER STREET  
SUITE 101  
ASHLAND, OR  
97520  
TEL.: 541.488.8200



- LEGEND:**
- EXISTING LANDSCAPE AREA (PERVIOUS)
  - EXISTING BUILDING
  - AREA OF LANDSCAPING AND IRRIGATION TO BE RESTORED
  - NEW GROUND FLOOR ADDITION
  - NEW PAVING
- NOTES:**
- PROVIDE BIO FILTER BAGS AS NEEDED TO FILTER RUNOFF DURING CONSTRUCTION. WATER ENTERING STORMDRAIN SYSTEM SHALL BE CLEAN AND FREE OF PARTICULATES.



**EROSION CONTROL PLAN**  
SCALE: 1" = 20' 11x17

1

WINCHESTER INN  
REMODEL AND ADDITION  
35 SOUTH SECOND STREET.  
ASHLAND, OREGON

REVISIONS

EROSION CONTROL PLAN

PROJECT: 15-016  
ISSUE DATE: 08-07-15  
SHEET:

**C1**



