

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
APRIL 14, 2009  
AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center Council Chambers, 1175 E. Main Street
  
- II. **ANNOUNCEMENTS**
  
- III. **CONSENT AGENDA**
  - A. **Approval of Minutes**
    - 1. March 10, 2009 Planning Commission Meeting
    - 2. March 31, 2009 Planning Commission Study Session
  
- IV. **PUBLIC FORUM**
  
- V. **TYPE II PUBLIC HEARINGS**
  - A. **PLANNING ACTION: 2009-00314**  
**SUBJECT PROPERTY: 500 Strawberry Lane**  
**APPLICANT: Robert & Laura McLellan**  
**DESCRIPTION:** A request for Final Plan approval for a six-lot, five-unit Performance Standards Subdivision for the property located at 500 Strawberry Lane. The application also includes a request to modify the phasing and conditions of approval of Planning Action #2008-00182 which granted Outline Plan approval, a Physical and Environmental Constraints Review Permit, Tree Removal Permits, and an Exception to Street Standards.  
**COMPREHENSIVE PLAN DESIGNATION:** Rural Residential; **ZONING:** RR-.5-P;  
**ASSESSOR'S MAP #:** 39 1E 08AC; **TAX LOT #:** 201
  
- VI. **OTHER BUSINESS**
  - A. **Update on Timeline for Croman Mill Site Redevelopment Plan Phase II: Implementation Package**
  
- VII. **ADJOURNMENT**

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
MINUTES  
MARCH 10, 2009

**CALL TO ORDER**

Chair Michael Dawkins called the meeting to order at 7:00 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Planning Commissioners Present:**

Michael Dawkins  
Larry Blake  
Tom Dimitre  
Dave Dotterer  
Pam Marsh  
Deborah Miller  
Melanie Mindlin  
Mike Morris

**Staff Present:**

Bill Molnar, Community Development Director  
Maria Harris, Planning Manager  
April Lucas, Administrative Assistant

**Absent Members:**

None

**Council Liaison:**

Eric Navickas

**ANNOUNCEMENTS**

Community Development Director Bill Molnar announced the March Study Session has been moved to March 31 in order to accommodate individuals who will be out of town for spring break.

**CONSENT AGENDA**

**A. Approval of Minutes**

1. February 10, 2009 Planning Commission Meeting
2. February 24, 2009 Planning Commission Meeting

**B. Letter of Support for City of Ashland Transportation and Growth Management Grant**

Commissioners Marsh/Mindlin m/s to approve the February 10, 2009 Planning Commission minutes. Voice Vote: all AYES. Motion passed.

Commissioners Dotterer/Marsh m/s to approve the February 24, 2009 Planning Commission minutes. Voice Vote: all AYES. Motion passed.

Planning Manager Maria Harris provided a brief overview of the Transportation and Growth Management (TGM) Grant and explained several of the City's appointed and elected bodies are being asked to sign letters of support to accompany the grant application. Commissioner Marsh expressed disappointment with the letter and stated she would have preferred for the language to be stronger and clearer.

**PUBLIC FORUM**

No one came forward to speak.

**DISCUSSION ITEMS**

**A. Exit 14 and 19 Aesthetics Advisory Committee Update**

Commissioner Dawkins stated he is a member of the Exit 14 and 19 Aesthetics Advisory Committee (AAC) and provided an update on the proposed bridge designs. Dawkins passed around the initial design concepts and stated most of the Committee

members preferred a clean look to the bridges. He noted the design for Exit 14 will include a bicycle lane and sidewalks across the bridge; he also noted the proposed railing and banners. Dawkins shared his concern with the amount of funding the State would provide for this project and suggested the City may have to pay a portion of the landscaping materials and installation.

Commissioner Dotterrer stated it was his understanding that the State viewed Exit 14 as an entry into the Oregon and they were willing to spend the money to make it look nice. Dawkins indicated the State has not provided a budget, so the AAC is unsure about the amount of funding available for this project. Mr. Molnar stated after the AAC comes up with a few options there will likely be an assessment of the State's budget. Then the Committee will be able to make a final decision based on the total costs and funding available.

Mr. Molnar commented briefly on the concept of flying banners on the bridge and clarified the City has a permit process for placing banners on public right of way and this would not conflict with the City's Sign Code.

Commissioner Miller voiced her support for an attractive bridge design. She stated the preliminary designs are lovely and hopes the State will spend the extra money to make the bridges look nice.

## **B. Planning Commission Input to Council Goal Setting – Discussion of Priorities**

### Transportation Planning

Commissioner Marsh provided an overview of the Planning Commission's role in the Transportation System Plan (TSP) update and incorporating a planning vision and active participation. She noted the Commission's goal of updating the Ashland Comprehensive Plan and stated you cannot do land use planning without transportation planning, and vice versa. Marsh commented on the recent formation of the Transportation Commission and noted she has had conversations with Mayor Stromberg, Michael Dawkins, and Bill Molnar about how the Planning Commission can specifically be apart of the TSP update process. She recommended there be an official delegate from the Planning Commission to the Transportation Commission and stated they should define an arrangement and obtain the City Council's blessing.

Commissioner Dawkins voiced his support for a Planning Commission liaison. Dotterrer suggested the two commissions consider exchanging liaisons and they would participate as an active member on issues related to the TSP update. Mr. Molnar noted he had spoken with Public Works Director Mike Faught and Mayor Stromberg about how to have more interaction between the two commissions and they have discussed bringing the two commissions together every four months or so. Commissioner Blake recommended staff provide the Transportation Commission's agendas so they are aware of when they are meeting and what is being discussed.

Marsh restated her recommendation for the Planning Commission to formalize a role in transportation planning in the form of joint Study Sessions and possibly a dedicated liaison. Dotterrer noted the motion regarding the recommended goals from the previous meeting and felt this adequately captured the intent.

Mr. Molnar clarified the Council goal setting is scheduled to take place on April 11 and staff would prepare a statement on behalf of the Commission and bring it back for their review at the March 31<sup>st</sup> Study Session. Comment was made questioning if the Planning Commission Chair could attend the Council goal setting and present the Commission's recommendations in person. Council Liaison Navickas voiced his support for this idea and encouraged Commissioner Dawkins to speak with the Mayor about this possibility.

### Sustainability

Commissioner Mindlin provided an overview of the sustainability goal. She commented on the completion of the sustainability inventory by the Sustainability Work Group and stated when people were asked "What does sustainability mean to you?" some of the main answers were: protecting the environment, wise use of resources, food security, local economic resiliency, and communications. Mindlin stated the second phase of this project is to determine what other communities are doing to promote sustainability, and stated a starting point might include researching sustainability plans, how citizen groups are interfacing with local governments, and local economic development. She stated her hope is that the City Council will support

the Planning Commission's ongoing research into this subject. The Commission voiced their support for continuing this research.

#### Croman Mill Site Redevelopment Plan

Commissioner Morris commented on the Croman Mill Site Redevelopment Plan. He stated this is the last big chunk of available land and voiced his support for adopting a master plan before it gets divided up and developed into smaller parcels. He noted that he does have some questions and concerns with the Draft Plan, but overall supports moving this forward.

Clarification was requested on what the City Council has directed the Planning Commission to do, and whether this Plan has already been adopted. Mr. Molnar explained the City Council did not adopt the Plan, but rather directed staff to initiate the planning for creating the Croman Master Plan. He stated the direction from the Council is for the Planning Commission to start hashing out the details and clarified the Commission would be able to incorporate some flexibility. Comment was made questioning if the Commission would be able to make substantial changes to the Plan. Mr. Molnar indicated they would need to consult with the Council on any major changes. Council Liaison Navickas stated the basic overlay was endorsed by the Council and they have forwarded it onto the Planning Commission to work out the details.

Several commissioners shared their preferences on how this land should be developed and some voiced frustration that the Planning Commission was not given the opportunity to review and issue a recommendation on the Draft Plan before it went to Council. Planning Manager Maria Harris explained the City Council has directed staff to begin the process to implement the Draft Plan, which includes creating new ordinances and zoning overlays. She stated the Council did not adopt the Plan, but rather directed staff to work with the Commission on moving this forward. She noted the lengthy public involvement process that went into creating the Draft Plan and stated this public involvement will continue. Ms. Harris commented on the upcoming outreach that will take place as the Commission moves forward, and stated while some may disagree with elements in the Plan, it is important to respect the work that the community has put in thus far. She added it does not mean the Plan cannot be massaged, but it also should not be ignored.

Council Liaison Navickas commented on ways the Plan could be expanded on or changed and provided some examples.

Commissioner Marsh commented that it is clear the Croman Plan should be on their list of goals. She noted the dissenting opinions and stated they will have plenty to talk about as this moves forward. Commissioner Dimitre requested staff provided an analysis of how this Plan fits in with the expressed goals, which included workforce housing and sustainability.

#### ADJOURNMENT

Meeting adjourned at 8:45 p.m.

*Respectfully submitted,  
April Lucas, Administrative Assistant*



**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
STUDY SESSION  
MINUTES  
MARCH 31, 2009

**CALL TO ORDER**

Chair Michael Dawkins called the meeting to order at 7:00 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Planning Commissioners Present:**

Michael Dawkins  
Tom Dimitre  
David Dotterer  
Pam Marsh  
Melanie Mindlin  
Mike Morris

**Staff Present:**

Bill Molnar, Community Development Director  
Maria Harris, Planning Manager  
April Lucas, Administrative Assistant

**Absent Members:**

Debbie Miller  
Larry Blake (*Recused himself due to his affiliation with SOU*)

**Council Liaison:**

Eric Navickas

**ANNOUNCEMENTS**

Community Development Director Bill Molnar noted the Recommended Council Goals Memo that staff had prepared and stated if needed they could discuss this after the Master Plan Presentation.

**SOUTHERN OREGON UNIVERSITY MASTER PLAN UPDATE**

Mr. Molnar provided a brief history of the Southern Oregon University Master Plan. He explained the first plan was done in the 1950s and updated in the 1960s to establish the campus boundaries. In the late 1970s and early 1980s the City became more actively involved in the Plans after the adoption of the Oregon Statewide Planning Program which created a coordination requirement. In 1979 the City approved a Cooperation Agreement with the University, adopted the 1980-1990 Campus Plan, and added Chapter 18.64 to the Municipal Code. The Master Plan has continued to be updated every 10 years and the Cooperation Agreement signed in 1979 is still in place. Mr. Molnar explained a few months ago the University began the process to create the 2010-2020 Master Plan and tonight Eric Ridenour with SERA Architects is here to present some of the options the University has been discussing.

Mr. Eric Ridenour introduced himself to the Commission and explained SERA Architects was hired by the University to assist with their Master Plan update. He stated the University has conducted site visits, spoke with key stakeholder groups, have held general public forums and are at a point where they are honing in on a preferred alternative. He stated the next steps in this process will involve preparing the Draft Master Plan, coming back to the Planning Commission for a public hearing, and continuing on to the City Council for adoption of the Plan.

Mr. Ridenour delivered a presentation to the Commission which addressed the following objectives and options for the 2010-2020 SOU Master Plan:

- **Renovate and make modest expansions to the existing academic buildings.**  
The Theatre Arts building would be the first priority, followed by the Sciences Complex and McNeal Pavilion.
- **Improve the quantity and quality of residential facilities.**  
Option 1 proposes new residential housing to the north of campus, but retains the existing residence halls with minor renovations. Option 2 proposes more or less the same strategy as Option 1, but would remove dining services from the Cascade Complex and relocate it to a separate facility. Option 3 proposes to begin moving residential life to the

north side of campus and would preserve the location of the Cascade Complex for future academic needs. Mr. Ridenour explained the preferred option is consistent with Option 3 but would retain the newer Madrone apartment complex and the Suzanne Holes building. It would move the bulk of the residential housing to the area between Ashland Street and McNeal Pavilion, and if this mixed use housing is successful, future residences could be added in that same area east of Wightman. Mr. Ridenour added if funding became available, the University would also like to pursue additional faculty housing.

- **Enhance the pedestrian core.**

Mr. Ridenour stated the idea is to “bookend” the two ends of the pedestrian core of the campus and to further enhance the pedestrian experience.

- **Enhance the athletic facilities.**

Ideas include adding field turf to the stadium field, developing a soccer field north of Iowa Street, and renovating the existing recreation building north of Iowa Street into a new women's athletics facility. The tennis courts could also be relocated to this area should housing be added on the east side of Wightman. Additional ideas include improving the circulation into McNeal Pavilion and creating a more grand entry to the stadium from Wightman.

- **Improve the campus gateways and transportation circulation.**

Ideas include improving the way you approach the campus along Siskiyou Blvd and changing University Way to a two-way street (right in, right out only). It was noted pedestrian safety is an ongoing issue, especially as more housing becomes available north of campus. Various options have been discussed, including a pedestrian ramp, pedestrian underpass, center lane underpass, and traffic circles. However because all these suggestions have significant constraints, the most likely option is to improve the crosswalk configurations and increase pedestrian visibility by using pavement and color more aggressively. Mr. Ridenour added at some point there may be enough pedestrian traffic during peak times to have an all pedestrian phase to the traffic signal.

Mr. Ridenour's presentation also indicated the University would incorporate sustainability by: 1) increasing efficiency of the residence halls and academic buildings, 2) utilizing renewable energy development and water conservation measures, and 3) integrating sustainability teaching into the curriculum. He also commented on transportation demand management and encouraged the Commission's input on this subject. Mr. Ridenour noted the City's parking space requirement and commented on possible amendments to this requirement for the University.

Mr. Ridenour concluded his presentation by proving the timeline for the 2010-2020 Master Plan adoption process and asked if the Commission had any questions. He clarified approximately 25% of students currently live on campus and the goal is to increase this figure to 35%-50%. Several comments were made voicing support for the preferred plan, including the creation of a sense of entry along Siskiyou Blvd and development of mixed use housing along Ashland Street. Additional suggestions were made to include transportation planning in this process and looking into possible amendments to the University's parking requirements.

Mr. Molnar noted the timeline reviewed by Mr. Ridenour and stated staff would bring forward updates as this process moves forward.

## **OTHER BUSINESS**

### **1. Recommended Council Goals Memo**

No changes were requested to the Memo. Mr. Molnar indicated staff would submit the Memo as presented for inclusion in the Council's Goal Setting packet.

## **ADJOURNMENT**

Meeting adjourned at 8:10 p.m.

*Respectfully submitted,  
April Lucas, Administrative Assistant*



**PLANNING ACTION:** #2009-00314

**SUBJECT PROPERTY:** 500 Strawberry Lane

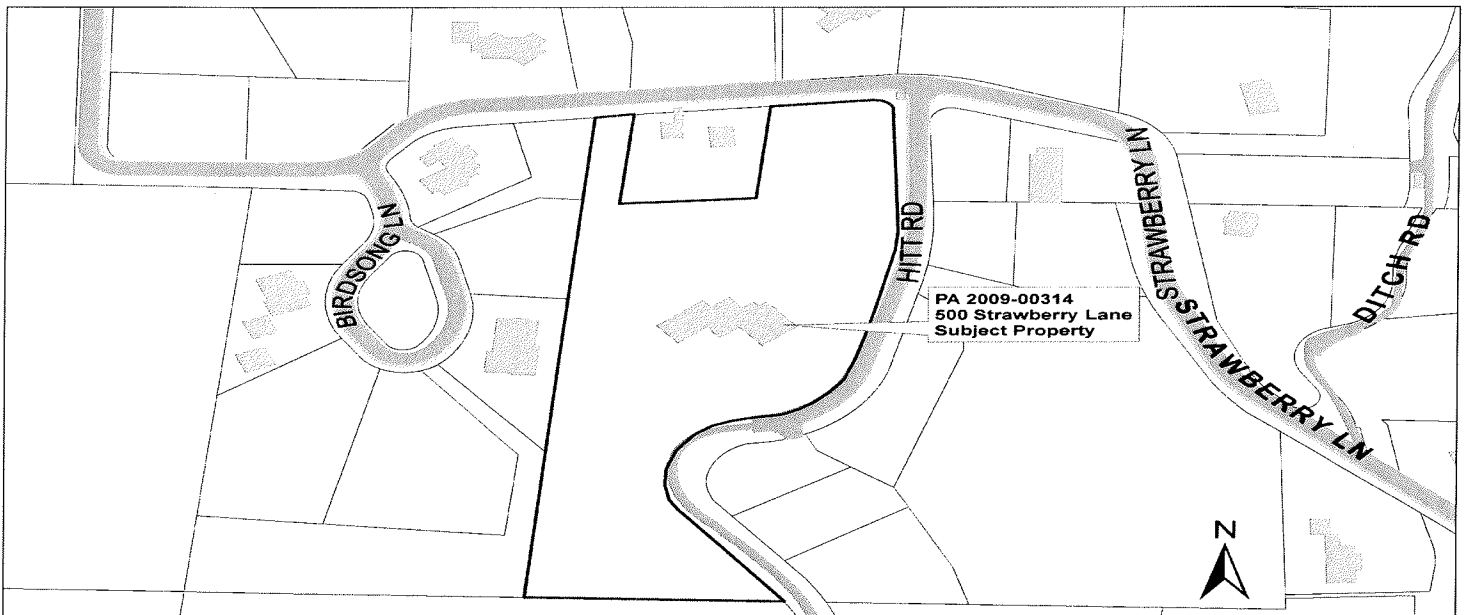
**OWNER/APPLICANT:** Robert & Lara McLellan

**DESCRIPTION:** A request for Final Plan approval for a six-lot, five-unit Performance Standards Subdivision for the property located at 500 Strawberry Lane. The application also includes a request to modify the phasing and conditions of approval for Planning Action #2008-00182 as they relate to the Outline Plan approval and Exception to Street Standards.

**COMPREHENSIVE PLAN DESIGNATION:** Rural Residential; **ZONING:** RR-5-P; **ASSESSOR'S MAP #:** 39 1E 08AC; **TAX LOT #:** 201.

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **April 9, 2009 at 6:00 p.m.** in the Community Development and Engineering Services building (Siskiyou Room) located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** April 14, 7:00 PM, Ashland Civic Center



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102-.35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Department, 541-488-5305.

## **FINAL PLAN APPROVAL**

### **18.88.030.B.5 Criteria for Final Approval**

Final plan approval shall be granted upon finding of substantial conformance with the outline plan. Nothing in this provision shall limit reduction in the number of dwelling units or increased open space provided that, if this is done for one phase, the number of dwelling units shall not be transferred to another phase, nor the open space reduced below that permitted in the outline plan. This substantial conformance provision is intended solely to facilitate the minor modifications from one planning step to another. Substantial conformance shall exist when comparison of the outline plan with the final plan shows that:

- a. The number of dwelling units vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.
- b. The yard depths and distances between main buildings vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Title.
- c. The open spaces vary no more than ten (10%) percent of that provided on the outline plan.
- d. The building size does not exceed the building size shown on the outline plan by more than ten (10%) percent.
- e. The building elevations and exterior materials are in conformance with the purpose and intent of this Title and the approved outline plan.
- f. That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.
- g. The development complies with the Street Standards.

(ORD 2836, 1999)

## **OUTLINE PLAN APPROVAL**

### **18.88.030.A.4 Criteria for Approval**

The Planning Commission shall approve the outline plan when it finds the following criteria have been met:

- a. That the development meets all applicable ordinance requirements of the City of Ashland.
- b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.
- c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.
- d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.
- e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.
- f. That the proposed density meets the base and bonus density standards established under this Chapter.
- g. The development complies with the Street Standards.

(ORD 2836, 1999)

## **EXCEPTION TO STREET STANDARDS**

### **18.88.050 F – Exception to Street Standards**

An exception to the Street Standards is not subject to the Variance requirements of section 18.100 and may be granted with respect to the Street Standards in 18.88.050 if all of the following circumstances are found to exist:

- A. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- B. The variance will result in equal or superior transportation facilities and connectivity;
- C. The variance is the minimum necessary to alleviate the difficulty; and
- D. The variance is consistent with the stated Purpose and Intent of the Performance Standards Options Chapter.

(ORD 2951, 2008; ORD 2836, 1999)

# ASHLAND PLANNING DEPARTMENT STAFF REPORT

April 14, 2009

**PLANNING ACTION:** 2009-00314

**APPLICANT:** McLellan, Robert & Laura

**LOCATION:** 500 Strawberry Lane  
39 1E 08 AC Tax Lot #201

**ZONE DESIGNATION:** RR-.5-P

**COMPREHENSIVE PLAN DESIGNATION:** Rural Residential

**APPLICATION DEEMED COMPLETE:** April 6, 2009

**120-DAY TIME LIMIT:** August 4, 2009

**ORDINANCE REFERENCE:** 18.16 R-R Rural Residential District  
18.61 Tree Preservation and Protection  
18.62 Physical & Environmental Constraints  
18.88 Performance Standards Options

**REQUEST:** Planning Action #2009-00314 is a request for Final Plan approval for a six-lot, five-unit Performance Standards Subdivision for the property located at 500 Strawberry Lane. The application also includes a request to modify the phasing and conditions of approval for Planning Action #2008-00182 as they relate to the Outline Plan approval and associated Exception to Street Standards.

## **I. Relevant Facts**

### **A. Background - History of Application**

Planning Action #2008-00182, a request for Outline Plan Approval for a six-lot, five-unit Performance Standards subdivision, was approved by the Planning Commission in April of 2008. The application included requests for a Physical & Environmental Constraints Review Permit for the Development of Hillside Lands, a Tree Removal Permit to remove 13 trees six-inches in diameter at breast height (d.b.h.) or larger, and an Exception to Street Standards.

The Planning Commission's decision to approve the project was subsequently appealed to the City Council by neighbors Lou and Catherine Dimino. Following a public hearing on the appeal in November 2008, the City Council upheld the Planning Commission decision.

The Diminos subsequently filed an appeal of the Council decision with the Land Use Board of Appeals (LUBA) however the appellants and the applicants were able to reach a Settlement Agreement in January of 2009, and the Diminos dismissed their LUBA appeal.

There are no other planning actions of record for this site since its creation by Land Partition in 2004.

## **B. Detailed Description of the Site and Proposal**

### **Site**

The subject property is located at 500 Strawberry Lane, on the southwest corner of the intersection of Strawberry Lane and Hitt Road. The project site consists of a single irregularly-shaped tax lot covering approximately 4.62 acres, with approximately 170 feet of frontage along Strawberry Lane and 935 feet of frontage on Hitt Road.

The property has an average slope of approximately 18 percent down to the north, toward Strawberry Lane, but includes a range of slopes from zero to 40 percent. The steeper portions of the site are along the roadside at the north and east of the site and at the south end, with the heavily-wooded area on the southern portion of the property having slopes in excess of 35 percent. The northern two-thirds of the site has slopes which are generally less than 25 percent. An existing house, constructed by the applicants in 2002, sits near the center of the property, and is presently accessed via a private driveway from Strawberry Lane.

The primary natural features of the site are the existing trees, which include a mix of scrub oak, pine, manzanita and madrone, and the sloped areas on the southern third of the site, which include lands considered to have "severe constraints" to development under the Physical and Environmental Constraints Chapter AMC 18.62 because they have slopes in excess of 35 percent.

The subject parcel and surrounding properties to the north, east and west are located in the R-R-.5-P Rural Residential zoning district, which is intended to stabilize and protect the rural residential characteristics of areas which because of their topography, level of services, or other natural or development factors are better suited to larger lots. The area to the south is zoned W-R Woodland Residential, a zone applied to ensure that the forest, environmental erosion control, and scenic values of the area are protected. The entire subject property is also located within the Wildfire Lands overlay.

The existing street improvements on Hitt Road include pavement, curbs, and gutters, with curbside sidewalks in place only on the west side of the road. There is an existing gate and fire apparatus turn-around at the end of the improvements, and access is limited primarily to city vehicles going to the city water tank located on the parcel south of the subject property. The two properties immediately south of the subject property are within the city limits in the WR Woodland Residential zoning district. One of these properties is city-owned and contains the city water tank, and the other is privately owned. Future development of either property is largely constrained by slope issues, as both are made up almost entirely of hillside lands with severe constraints due to the presence of slopes in excess of 35 percent.

## **Final Plan Proposal**

The applicants are requesting Final Plan Approval to allow a six-lot subdivision under the Performance Standards Options Chapter AMC 18.88. Outline Plan approval was granted in April of 2008, and included a Physical & Environmental Constraints Review Permit for the Development of Hillside Lands, a Tree Removal Permit to remove 13 trees six-inches in diameter at breast height (d.b.h.) or larger, and an Exception to Street Standards to allow the applicants to end street improvements at the driveway of Lot 5, their last proposed buildable lot, rather than extending them to the southern boundary of the development.

Of the proposed six lots, one would contain the existing residence, four would accommodate future homes, and one at the most steeply-sloped south end of the site would be reserved as common open space. Lot 1 is proposed at ½-acre and is to be located at the northeast corner of the site. It is to take access via a private driveway from Strawberry Lane. Lot 2, also proposed at ½-acre, is to be located south of Lot 1, and is proposed to take access from a private driveway off of Hitt Road. Lot 3, comprising approximately 0.68 acres is to be located at the northwestern portion of the subject property and will take access from the existing driveway off of Strawberry Lane. Lot 4 is proposed at 1.42 acres, and is to contain the existing residence; the existing driveway connection to Strawberry Lane is to be terminated, and a new driveway off of Hitt Road is proposed. Lot 5 will be located to the south of Lot 4, and is proposed at one-acre with access from a new driveway off of Hitt Road. The existing gate at the south end of Hitt Road is to be relocated approximately 15 feet to the south to accommodate this new driveway. Lot 6 is proposed at 0.54 acres and is to be preserved as common open space for the benefit of subdivision residents and to protect the more steeply sloped areas of the subject property from the impacts of development.

Because the Outline Plan application included a report on the suitability of the site for development from a geo-technical expert based on a slightly different lot configuration, a condition of approval was included to require that a letter from the geo-technical expert be provided to confirm that the development as proposed was consistent with the recommendations. A letter from Amrhein Associates, Inc. has been provided, and it indicates that the updated plans are in general accordance with the recommendations and that no changes or modifications to the plans are necessary to address the original recommendations.

The application identifies building envelopes for each of the proposed lots on lands with slopes of less than 35 percent, as required by ordinance, and also provides conceptual indications of the proposed future building footprints within the proposed envelopes. Existing and proposed public facilities are illustrated in the provided civil drawings, which identify existing transportation facilities and water, sanitary sewer, stormwater, and electrical services within the Strawberry Lane and Hitt Road rights-of-way, as well as existing and proposed fire hydrants, and identify proposed service extensions/connections to the proposed individual lots, along with private stormwater detention facilities proposed for installation in or near the driveways of the individual lots as they are developed.

## **Proposed Modifications to Phasing & Conditions of Approval**

As part of the current application, the applicants are proposing to complete the six-lot subdivision in two phases. The first phase would immediately create Lots 1, 2 and the open

space Lot 6, with the existing house on the remaining lot. The second phase, which is proposed to occur within 18 months, would involve splitting the lot containing the existing home into three lots – Lot 4 would contain the existing home, and Lots 3 and 5. Street trees and sidewalks which were required to be installed along the Strawberry Lane frontage of Lot 3 are proposed to be completed with this second phase.

In addition to this proposed phasing, the applicants also propose to modify conditions of approval 5j and 7c which required that in addition to providing a third off-street parking space on each of the lots the applicants would also identify four on-street parking spaces to be provided in bays on Hitt Road near the relocated gate and between the driveways of Lots 2 and 4. The applicants propose to modify these conditions by providing all four on-street parking spaces in bays near the driveways of Lots 2 and 4, indicating that placement of parking near the relocated gate would result in tree removal, slope disturbance, and the need for retaining walls while leading to difficulties in vehicular circulation due to site and street grades near the gate. The applicants also propose to install all four on-street parking bays with the second phase of the development.

Modifications to condition of approval 5j are also proposed as the applicants are requesting not to install a third off-street parking space on Lot 1. This modification is proposed at the request of neighbors Lou and Catherine Dimino, who have asked that the third off-street parking space not be required for Lot 1 as it would result in additional lot coverage that would be detrimental to their views. The applicants have also entered into a private agreement with the Diminos to limit the type of lot coverage to be allowed on Lot 1.

## **II. Project Impact**

The project requires a subdivision approval since it involves the creation of residential lots, and application has been made under the Performance Standards Options Chapter (AMC 18.88) requesting Final Plan approval. In accordance with the Procedures Chapter (AMC 18.108), applications for Final approval may be approved administratively under a “Type I” procedure, however in this instance because the application proposes changes which affect some conditions of approval imposed upon the Outline Plan by the Planning Commission and reaffirmed upon appeal to the City Council, staff scheduled the matter for a public hearing. The modifications proposed with the Final Plan submittal involve construction of the project in two phases which was not discussed with the Outline Plan approval. The proposed phasing affects the timing of the installation of required improvements on Strawberry Lane and Hitt Road, and some additional changes are proposed in response to the Settlement Agreement with the neighboring Diminos including a request to not provide a required third parking space on Lot 1.

### **A. Final Plan for Performance Standards Options Subdivision**

In reviewing the application in terms of the Final Plan approval criteria, the proposal appears to be essentially the same as the approved Outline Plan. The number of dwelling units; lot configurations, dimensions and sizes; and open space areas have not changed by more than ten percent. No density bonus was requested with the Outline Plan, and as such there are no additional density bonus-related standards to be addressed with the Final Plan.



## **B. Proposed Modifications to Phasing & Conditions of Previous Approval**

In terms of the proposed phasing, staff has no specific concern with the creation of the subdivision in two phases or with the proposed placement of all of the parking along Hitt Road near the driveways of Lots 2 and 4, however the proposed modifications relating to the timing of the installation of required improvements raises some concerns. Specifically, the applicants have proposed to delay the installation of all four on-street parking spaces required by the Planning Commission along Hitt Road, as well as the completion of sidewalks and street tree planting adjacent to the existing home's driveway on Strawberry Lane, until the second phase of the development. In staff's view, delaying these improvements cannot be found to be consistent with the applicable criteria or conditions of approval for the Outline Plan which required adequate transportation facilities be provided and street standards met with the installation of the Strawberry Lane improvements, or with the approved Exception to Street Standards, which required installation of the Hitt Road parking spaces in order to *"ensure adequate fire access in the wildfire interface and to off-set on-street parking that would otherwise have been provided if Hitt Road were extended to the boundary of the proposed subdivision"* as would have been required without the requested Exception to Street Standards.

### **Hitt Road Parking**

While the application characterizes the four parking spaces required along Hitt Road largely as hiker parking, these spaces were required by the Planning Commission during the Outline Plan hearings in response to the combination of the proposed developments impacts to Hitt Road and the requested Exception to Street Standards. During the hearing, it was determined that the variable 20-22 foot width of Hitt Road was insufficient to accommodate on-street parking while maintaining the minimum 20-foot clear travel lane called for in Hillside Lands under Ashland's Street Standards Handbook. Commissioners noted that on-street parking was currently allowed, and was at times was in high demand due to hiker parking, and concluded that in order to provide a clear travel lane for fire apparatus access and wildfire evacuation with the increased resident and visitor traffic associated with the proposed development, on-street parking would likely need to be restricted in the near future. The findings from that Planning Commission decision indicate that the four on-street parking spaces required to be located in bays on Hitt Road were necessary *"to ensure adequate fire access in the wildfire interface and to off-set on-street parking that would otherwise have been provided if Hitt Road were extended to the boundary of the proposed subdivision"* as would have been required without the approval of the requested Exception to Street Standards.

These spaces were required to be installed by the Planning Commission to respond to increased resident and visitor traffic associated with the development and to ensure adequate fire apparatus access in keeping with the Exception to Street Standards criterion that equal or superior transportation facilities be provided, and in staff's view the proposed phasing would better address these issues by phasing the parking installation in a manner consistent with the proposed phasing of the development, i.e. providing two of the four parking spaces with the first phase of development as the first phase does create a new lot and driveway, and associated resident and visitor traffic impacts, on Hitt Road. As such, a condition of approval has been recommended below to require that one two-space bay along the Hitt Road frontage of Lot 2 be installed prior to signature of the survey plat for the first phase of the

proposed development, with the remaining two-space parking bay to be installed on the frontage of Lot 4 with the second phase of the development.

### **Strawberry Lane Improvements**

The applicants propose the installation of sidewalks and street trees along the Strawberry Lane frontage of Lot 3, adjacent to the driveway which now serves the existing home, with the second phase of development. While this improvement is only a roughly 45-foot section of sidewalk and associated street trees, in staff's view it is nonetheless necessary in addressing the Outline Plan approval criteria for providing adequate transportation and needs to be installed concurrently with the first phase where two distinct lots are being created along the Strawberry Lane frontage. The conditions of approval suggested below reflect this staff-recommended modification to the applicants' proposed phasing plan.

### **Third Parking Space on Lot 1**

By definition, single dwelling or parcel accesses greater than 50 feet in length are considered to be flag drives, and are subject to all of the development requirements thereof (AMC 18.08.195). During the public hearing for the Outline Plan, the applicants proposed to provide a third space for each of their proposed lots, all of which are served by driveways of at least 50 feet in length, thus addressing neighbors' and commissioners' concerns with visitor parking and while also addressing the development requirements that lots served by flag drives be provided with three off-street parking spaces situated in such a manner as to eliminate the necessity for backing up. The conditions of approval included requirements both that the driveways satisfy the dimensional and paving requirements for flag drives and that the required third parking spaces be provided for each lot.

With the current application, the applicants are requesting to modify this condition (5j) with regard to Lot 1. Specifically, the application notes that this modification is proposed at the request of neighbors Lou and Catherine Dimino who live across the street from Lot 1 and who have asked that the applicants seek a modification of this condition as it relates to the third parking space for Lot 1. The reasoning given for the proposed modification is that the third parking space and its associated paving would create unnecessary detriment to the Diminos' views. The application also notes the applicants have reached a private agreement with the Diminos to limit the lot coverage on Lot 1. Of the 26 percent lot coverage allocated to this lot in the Outline Plan approval, six percent is to be limited to pervious concrete pavers, pervious concrete, gravel, or similar pervious surface treatments which, while still considered to be lot coverage under city regulations, are more acceptable forms of coverage in the Diminos' view.

There was considerable concern expressed over parking impacts from residents and visitors to the development during the hearing, and the Planning Commission ultimately felt that parking was of sufficient concern to merit not only reiterating the required third space for flag drives on all of the proposed lots, but also requiring that the applicants provide additional on-street parking in bays along Hitt Road. In staff's view, visitors to Lot #1 are likely to be discouraged from utilizing the proposed parking bays along Hitt Road by the combination of the distance between the bays and the driveway for Lot 1, and the steep roadside slopes along this lot's Hitt Road frontage. As such, staff believe that Lot 1 has an equal if not greater need for the required third visitor parking space than the other lots where

additional on-street parking is to be provided in bays, and as such can see no compelling reason to lessen the parking requirement from the approved Outline Plan.

### **III. Procedural - Required Burden of Proof**

**The criteria for Outline Plan approval are described in 18.88.030.A.4 as follows:**

- a. That the development meets all applicable ordinance requirements of the City of Ashland.
- b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.
- c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.
- d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.
- e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.
- f. That the proposed density meets the base and bonus density standards established under this Chapter.
- g. The development complies with the Street Standards.

**The criteria for Final Plan approval are described in 18.88.030.B.5 as follows:**

- a. The number of dwelling units vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.
- b. The yard depths and distances between main buildings vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Title.
- c. The open spaces vary no more than ten (10%) percent of that provided on the outline plan.
- d. The building size does not exceed the building size shown on the outline plan by more than ten (10%) percent.
- e. The building elevations and exterior materials are in conformance with the purpose and intent of this Title and the approved outline plan.
- f. That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.
- g. The development complies with the Street Standards.

**The criteria for an Exception to Street Standards are described in 18.88.050.F as follows:**

- A. There is demonstrable difficulty in meeting the specific requirements of this chapter due to a unique or unusual aspect of the site or proposed use of the site.
- B. The variance will result in equal or superior transportation facilities and connectivity;
- C. The variance is the minimum necessary to alleviate the difficulty; and
- D. The variance is consistent with the stated Purpose and Intent of the Performance Standards Options Chapter.

#### **IV. Conclusions and Recommendations**

Planning staff were supportive of the application for a six-lot, five-unit Performance Standards Subdivision; Physical and Environmental Constraints Review Permit; Tree Removal Permit; and Exception to Street Standards at the Outline Plan level, and we remain generally supportive of the application here at the Final Plan. However, in reviewing the applicants' proposed modifications to the Outline Plan approval, we do not believe that the phasing of improvements proposed, which would delay the installation of the on-street parking until the second phase, adequately addresses either the approval criteria or concerns raised during the previous hearings, and as such we have recommended that Final Plan approval be subject to a number of conditions below which would call for a somewhat alternate phasing by requiring the installation of two of the four previously required on-street parking spaces on Hitt Road and completion of the Strawberry Lane sidewalks with the first phase, and the remaining two on-street parking spaces on Hitt Road with the second phase. In addition, staff do not believe that the proposal to eliminate the required third parking space on Lot 1 is merited and a condition is recommended below that three parking spaces continue to be required for each of the proposed new lots.

- 1) That all proposals of the applicant are conditions of approval unless otherwise modified herein, including the modifications to the building envelopes which were proposed by the applicants at the Outline Plan public hearing before the Planning Commission and which are reflected on Sheet L-1 received with the application submittal on March 12, 2009.
- 2) All conditions of the geotechnical report prepared by Amrhein Associates, Inc. and dated October 12, 2007, including but not limited to the inspection schedule, shall be conditions of approval unless otherwise modified herein.
- 3) That all measures installed for the purposes of long-term erosion control, including but not limited to vegetative cover, rock walls, retaining walls and landscaping shall be maintained in perpetuity on all areas in accordance with 18.62.089.B.7.
- 4) That the installation of sidewalks, curbing and driveway approaches shall be completed according to city standards under permit from the Public Works/Engineering Department and any necessary inspections approved.
- 5) That prior to the issuance of an excavation permit:
  - a) Final civil engineering for the utility plan including but not limited to the water, sewer, storm drainage, electric and transportation facilities shall be submitted for the review and approval of the Planning, Building, and

Engineering Departments. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, fire hydrants, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins, and locations of all primary and secondary electric services including line locations, transformers (to scale), cabinets, meters and all other necessary equipment. Transformers and cabinets shall be located in areas least visible from streets, while considering the access needs of the Electric Department. Any required private or public utility easements shall be delineated on the utility plan.

- b) A preconstruction conference to review the requirements of the Physical Constraints Review Permit shall be held prior to site work, storage of materials, or the issuance of an excavation permit. The conference shall include the Ashland Planning Department, Ashland Building Department, the project engineer, project geotechnical experts, landscape professional, arborist, and contractor. The applicants or applicants' representative shall contact the Ashland Planning Department to schedule the preconstruction conference.
  - c) That a Verification Permit in accordance with 18.61.042.B shall be applied for and approved by the Ashland Planning Division prior to site work, storage of materials and/or the issuance of an excavation or building permit. The Verification Permit is to inspect the trees to be removed and the installation of tree protection fencing. The tree protection for the trees to be preserved shall be installed according to the approved Tree Protection Plan prior to site work or storage of materials. Tree protection fencing shall be chain link fencing a minimum of six feet tall and installed in accordance with 18.61.200.B.
  - d) That the temporary erosion control measures (i.e. fabric sediment fencing, straw bales, crushed rock pads, straw erosion control matting or plastic sheeting) shall be installed and maintained according to the approved plan prior to any site work, storage of materials, or issuance of an excavation permit. These measures shall be inspected and approved by the Staff Advisor prior to site work, storage of materials, or the issuance of an excavation permit.
  - e) The applicants shall provide a performance bond, letter of credit or other financial guarantee in an amount equal to 120 percent of the value of the erosion control measures necessary to stabilize the site.
- 6) That prior to the signature of the final survey plat for Phase One:
- a) All easements for sewer, water, drainage, electric, streets or public pedestrian access shall be indicated on the final survey plat as required by the City of Ashland.
  - b) Street trees, located one per 30 feet of street frontage, shall be installed along the Strawberry Lane street frontage as part of the subdivision infrastructure improvements. Street trees shall be chosen from the Recommended Street Tree List and shall be installed in accordance with the specifications noted in

- the Recommended Street Tree List. The street trees shall be irrigated.
- c) Phase One subdivision infrastructure improvements, including but not limited to utilities; driveway approaches and any associated erosion control measures; a two-space parking bay to be located north of the driveway of Lot 2; and sidewalks and street trees on Strawberry Lane shall be installed according to approved plans prior to the signature of the final survey plat for Phase One. On-street parking space bays shall be installed so as to maintain a 20-foot wide clear travel lane as required in the Street Standards Handbook.
  - d) Electric services shall be installed underground to serve Lots 1, 2 and 4. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.
  - e) That the sanitary sewer laterals and water services including connection with meters at the street shall be installed for Lots 1, 2 and 4.
  - f) That Amrhein Associates, Inc. shall inspect the site according to the inspection schedule of the engineering geology report dated October 12, 2007 provided with the application. Prior to signature of the final survey plat, Amrhein Associates, Inc. shall provide a final report indicating that the approved grading, drainage and erosion control measures were installed as per the approved plans, and that all scheduled inspections were conducted by the project geotechnical expert periodically throughout the project.
  - g) The landscaping and irrigation for re-vegetation of cut/fill slopes and erosion control shall be installed in accordance with the approved plan prior to signature of the final survey plat. Vegetation shall be installed in such a manner as to be substantially established within one year of installation.
  - h) The applicants shall sign an agreement to participate in the future cost of street improvements for Hitt Road, including but not limited to sidewalks, curbs, gutters, paving, and storm drains, to be recorded on the deeds of the newly created lots concurrently with the final plat.
  - i) The recommendations from the April 9, 2009 meeting of the Ashland Tree Commission, where consistent with applicable standards, shall be incorporated into revised Landscaping, Irrigation, and Tree Protection and Removal Plans for the review and approval of the Staff Advisor.
  - j) A final copy of the CC&R's and the applicants' proposed Deed Restrictions shall be provided for the review and approval of the Staff Advisor and Legal Department. The CC&R's shall describe responsibility for the maintenance of all commonly-owned open space including but not limited to the implementation and maintenance of the approved fire prevention and control plan, and perpetual maintenance of required long term erosion control measures. The CC&R's shall note that any deviation from the approved Tree Removal and Protection Plan must receive written approval from the City of Ashland Planning Department. The "Building Height Plan" exhibit to be attached to the CC&R's shall be revised to reflect the building envelope modifications proposed by the applicants during the Outline Plan hearings as

illustrated on Sheet L-1 submitted with the application on March 12, 2009, and shall also correct the sight line elevation depicted for Lot 5, which is currently shown as being 17 feet below the finished floor elevation of the floor elevation of the first floor. The CC&R's and Deed Restrictions shall be recorded concurrently with the final plat.

- 7) That prior to the signature of the final survey plat for Phase Two:
- a) All easements for sewer, water, drainage, electric, streets or public pedestrian access shall be indicated on the final survey plat as required by the City of Ashland.
  - b) Phase Two subdivision infrastructure improvements, including but not limited to utilities; driveway approaches and any associated erosion control measures; a two-space parking bay to be located north of the driveway of Lot 4; any necessary street or sidewalk improvements on Hitt Road between the end of the existing improvements and the driveway for Lot 5 shall be installed according to approved plans prior to the signature of the final survey plat for Phase Two. On-street parking space bays shall be installed so as to maintain a 20-foot wide clear travel lane as required in the Street Standards Handbook.
  - c) Electric services shall be installed underground to serve Lots 3 and 5. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.
  - d) That the sanitary sewer laterals and water services including connection with meters at the street shall be installed for Lots 3 and 5.
  - e) That the applicants shall complete the relocation of the gate at the end of the improvements on Hitt Road to the southern extent of the street improvements. The relocation of the gate will be coordinated with the City of Ashland Water Department.
  - f) The applicants shall sign an agreement to participate in the future cost of street improvements for Hitt Road, including but not limited to sidewalks, curbs, gutters, paving, and storm drains, to be recorded on the deeds of the newly created lots concurrently with the final plat.
  - g) That Amrhein Associates, Inc. shall inspect the site according to the inspection schedule of the engineering geology report dated October 12, 2007 provided with the application. Prior to signature of the final survey plat, Amrhein Associates, Inc. shall provide a final report indicating that the approved grading, drainage and erosion control measures were installed as per the approved plans, and that all scheduled inspections were conducted by the project geotechnical expert periodically throughout the project.
  - h) The landscaping and irrigation for re-vegetation of cut/fill slopes and erosion control shall be installed in accordance with the approved plan prior to signature of the final survey plat. Vegetation shall be installed in such a

manner as to be substantially established within one year of installation.

8) That prior to the issuance of a building permit:

- a) Individual lot coverage calculations including all impervious surfaces shall be submitted with each building permit to demonstrate compliance with the lot coverage allocated to each lot. Lots 1 and 2 are each allocated 26 percent coverage, Lot 3 is allocated 28 percent coverage, and Lots 4 and 5 are allocated each 20 percent coverage. Lot 6 is open space and no coverage has been allocated. Building footprints, walkways, driveways including the flag drive for Lot 3, parking areas, and any impervious surfaces shall be counted for the purpose of lot coverage calculations.
- b) The setback requirements of 18.88.070 shall be met and identified on the building permit submittals including but not limited to the required width between buildings as described in 18.88.070.D.
- c) Building permit submittals shall clearly demonstrate compliance with the applicants' proposed "Elevation Height Limits" by providing cross-sections or elevation drawings with building heights and elevations above sea level clearly labeled.
- d) That a Physical and Environmental Constraints Permit for Hillside Development shall be applied for and approved in accordance with 18.62.040 for the development of Lot 5 prior to submission or issuance of a building permit.
- e) Storm drainage plans shall be provided with the building permit submittal for each lot, and shall be consistent with the private stormwater detention system described in the approved civil improvements plans. The private lot stormwater detentions systems were proposed in the Outline Plan both to address stormwater drainage and to minimize impacts to the Oak Trees on site, which are highly susceptible to moisture-related fungus. Any modifications to the individual lots' stormwater detention systems will be required to demonstrate that they adequately address the civil engineering, geotechnical and arboricultural design assumptions.
- f) That all proposed lots shall be subject to Solar Access Standard A with the exception of Lot 5, which has a negative north slope in excess of 15 percent which renders it subject to Solar Access Standard B. Solar setback calculations shall be submitted with each building permit to demonstrate compliance with the applicable standards, and shall include identification of the required solar setbacks with supporting formula calculations and elevation or cross-section drawings clearly labeling the height of the solar producing point(s) from the identified natural grade.

9) That prior to the issuance of a certificate of occupancy:

- a) That the requirements of the Fire Department, including that approved addressing shall be installed prior to combustible construction; that a fire prevention and control plan shall be implemented and maintained; and that fire apparatus access, fire sprinklers as proposed by the applicants, and a fire



- hydrant shall be installed, shall be addressed.
- b) All exterior lighting shall be directed on the property and shall not illuminate adjacent properties.
  - c) For Lot #3, the applicants shall provide mitigation for the removal of Tree #31 through on-site replanting, off site replanting, or payment in lieu of planting as provided for in AMC 18.61.084.
  - d) Driveways greater than 50 feet in length, which are considered by definition to be flag drives and thus subject to all development requirements for flag drives, shall be constructed according to flag drive requirements that a 12-foot paved width and 15-foot clear width be maintained, and that parking spaces be configured so that vehicles can turn and exit to the street in a forward manner.
  - e) Three off-street parking spaces shall be provided on each of the individual lots as proposed by the applicants in the Outline Plan approval.

**PROJECT DESCRIPTION AND FINDINGS OF FACT  
FOR A PROPOSED 6-LOT OUTLINE PLAN MODIFICATION  
AND A FINAL PLAN SUBDIVISION  
FOR THE PROPERTY AT 500 STRAWBERRY LANE**



**SUBMITTED TO**

**CITY OF ASHLAND PLANNING DEPARTMENT  
ASHLAND, OREGON**

**SUBMITTED BY**

**URBAN DEVELOPMENT SERVICES, LLC**

**March 12<sup>th</sup>, 2008**

City of Ashland  
Community Development

MAR 12 2009

RECEIVED

**ADDRESS & LEGAL DESCRIPTION:** 500 Strawberry Lane; 391E 08AC 201

**PROJECT INFORMATION:**

**APPLICANTS:**

Robert & Laura McLellan  
500 Strawberry Lane  
Ashland, OR 97520  
Tel: 482-7040

**LAND USE PLANNING:**

Urban Development Services, LLC  
485 W. Nevada Street  
Ashland, OR 97520  
Tel: 482-3334

**DRAFTING**

Computerized Architecture Drafting  
170 Ashland Loop Road  
Ashland, OR 97520  
Tel: 488-5899

**ARBORIST:**

Upper Limb-it Tree Service  
P.O. Box 881  
Ashland, Oregon 97520  
Tel: 482-3667

**SURVEYOR:**

Polaris Land Survey  
P.O. Box 459  
Ashland, Oregon 97520  
Tel: 482-5009

**LANDSCAPE ARCHITECT:**

Laurie Sager  
700 Mistletoe Road, Suite 201  
Ashland, OR 97520  
Tel: 941-7659

**CIVIL ENGINEERING:**

Construction Engineering Consultants  
P.O. Box 1724  
Medford, Oregon 97501  
Tel: 779-5268

**COMPREHENSIVE PLAN DESIGNATION:**

Low Density Residential

**ZONING DESIGNATION:**

RR-.5-P

**LOT STATISTICS:**

Project Area: 4.65 acres

Area Allocation: Private: 4.15 acres  
Common Open Space: .50 acres

**APPLICABLE ORDINANCES:**

Rural Residential, Chapter 18.16  
Performance Standards Option, Chapter 18.88

City of Ashland  
Community Development

MAR 12 2009

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**ADJACENT ZONING/USE:**

West: RR-.5-P, Rural Residential  
 East: RR-.5-P, Rural Residential  
 South: W-R; Woodland Residential  
 North: RR-.5-P, Rural Residential  
 Subject Site: RR-.5-P, Rural Residential

MAR 12 2009

City of Ashland  
 Community Development

**PLANNING ACTION:** The application is for a modification to the recently approved Outline Plan (PA-2008-00182) and Final Plan approval for a six-lot Performance Standards Option Subdivision for the property located at 500 Strawberry Lane. The modification request pertains to the subdivision's *phasing* and *conditions of approval*.

**PROPERTY DESCRIPTION:** The property is located at 500 Strawberry Lane, Assessor's Map #391E 08AC; Tax Lot 201, on the southwest corner on Strawberry Lane and Hitt Road (below water tank). The parcel is irregular shaped with approximately 170' of frontage along Strawberry Lane and 935' along Hitt Road. The Strawberry Lane frontage is broken-up by a .51 acre parcel owned by Karen Darling (490 Strawberry Lane) which once was part of the subject property and partitioned in 1994 (P-16-1994). A 42' property width (flag pole) and 12' flag driveway leading to the property owner's house is west of 490 Strawberry Lane and 128' of property width is on the east side. Curbside sidewalks exist along the two street frontages, but for approximately 20' along Strawberry Lane and 360' along Hitt Road (far south end - at the top).

The property has a range of slopes from 0 to 40% with the steeper slopes on the south side. Along Hitt Road and to a lesser degree along Strawberry Lane, severe cut slopes exist primarily due to the City's widening and the recent improvements of both public streets. The site has a numerous Oak Trees, but most are less than 6" d.b.h. and most are located south of the property owner's house. The trees greater than 6" d.b.h. are identified on the attached Site Plan and are generally grouped together.

Other than to the south, the property has a number of recently approved subdivisions and partitions with housing construction in process. The subject property owners have owned the property since 2000 and constructed the existing house in 2002. The property owners are active community members and in particular, Robert McLellan was the principal founder in the Ashland Woodlands Trail Association and has helped in the coordination, construction and maintenance of many pedestrian trails in Ashland – one of which runs along the south end of the property connecting Hitt Road with Birdsong Lane.

The existing house is a single level home constructed by Dale and Dean Shostrum in 2002 and is considered by many as being one of the nicest architecturally constructed homes in Ashland. In addition, the home was specifically designed to blend into its natural surroundings by being single level with a shallow roof line and a concave footprint limiting the building's mass from any one direction. In addition, architectural elements such as the use of natural building materials and colors to mitigate the home's exposure were used. Overall, the home is probably one of the nicest and most discrete hillside homes in Ashland and an

example of superb hillside development. Because of these qualities, the property owners have designed the proposed subdivision with these same principals in mind.

**BACKGROUND:** In 1994, the property was partitioned (P-16-1994) into two parcels with one being .508 acres (490 Strawberry Lane) and the subject parcel being 4.622 acres. The existing house at 490 Strawberry Lane, owned by the Darling family, was constructed in 1938 and one of the only houses in this area for many years and was the parent parcel of the subject property.

In April of 2008, the applicants and property owners of the 4.622 acre parcel (500 Strawberry Lane) received approval from the Ashland Planning Commission for an Outline Plan, Tree Removal Permit, Street Exceptions and Physical & Environmental Constraints Permit. The Council, on appeal, affirmed the Planning Commission's decision on November 4<sup>th</sup>, 2008.

**PROJECT PROPOSAL:** The application is for a modification to the recently approved Outline Plan (PA-2008-00182) and Final Plan approval for a six-lot Performance Standards Option Subdivision for the property located at 500 Strawberry Lane. The modification request pertains to the subdivision's phasing and conditions of approval requiring four on-street public parking spaces, required a portion of the spaces to be located near the gate, and required each lot to have an extra parking space per lot (Condition #5j and #7c).

### **I. Outline Plan Modification:**

Phasing: Due to market conditions, the applicants are requesting the 6-lot subdivision be completed in two phases with Lots #1, #2, and #6 (open space lot) being created in Phase I and the remaining lots being created in Phase II. The phasing proposal allows the applicants to install the necessary infrastructure and prepare lots #1 and #2 for eventual construction. Lot #6 (open space lot) will also be created with Phase I. Phase II will include the existing house (Lot #4) as well the lots below and above (Lots #3 and 5). Minor infrastructure improvements to the existing house will be completed with Phase I, but the actual separation and final improvements of the Phase II lots is expected to occur within the next 18 months. NOTE: The development's improvements and their timing is based upon their phase and further described below:

Modification of Conditions of Approval: The applicants are also proposing a modification request to two conditions of approval, specifically condition 5.j. and 7.c., imposed as part of PA-2008-00182 as it relates to parking. Condition 5.j. reads as follows – followed by the requested amendment:

*5.j.) That in addition to the third off-street parking space proposed to be provided on each of Lots 1-5 by the applicants, the Final Plat submittal shall identify four on-street parking spaces to be provided on Hitt Road in bays near the relocated gate and between the driveways of Lots 2 and 4.*

*5.j.) That in addition to the third off-street parking space proposed to be provided on each of Lots 1-5 by the applicants, the Final Plat submittal shall identify four on-street parking*

*spaces to be provided on Hitt Road in bays near ~~the relocated gate and between the~~ driveways of Lots 2 and 4.*

*Explanation:* The applicants' have multiple reasons for this request. First, the wording was originally proposed by the applicants during the Outline Plan when certain Planning Commissioners and neighbors desired more parking for the hiking community, but specific engineering was yet to be completed and the applicants didn't fully understand the amount of earth disturbance, impact on vegetation, engineering effort and cost of the improvement. Second, since the Outline Plan approval, the applicants, project Civil Engineer, Landscape Architect, Planner and Contractor have had an opportunity to weigh-in on the condition and feel that installing head in parking near the gate (directly opposite the driveway serving Tax Lots #506, 507 and 508) would require removal of additional trees, additional earth removal and additional retaining walls. Additionally, the site and street grades in this location (two different grade angles) are difficult at best which would have made it very difficult entering and exiting vehicles. As such, the applicants propose to put all of the on-street parking space in bays along Hitt Road – adjacent to Lots #2 and #4.

Finally, the second modification to condition 5j is being proposed at the request of the neighbors – Catherine and Lou Dimino (appellants of the Outline Plan) who live directly north of Lot #1 (Tax Lot 110) have consistently asked the applicants to amend this condition's wording as it relates to Lot #1's "extra" parking space with the primary reasoning that one additional parking space and its required hard surface treatment would create unnecessary detriment to their views (extra hard surface). The applicants, in an attempt to mitigate the neighbors concerns, have no objection to the request.

Condition 7.c. reads as follows – followed by the requested amendment:

*c) Subdivision infrastructure improvements, including but not limited to utilities; driveways, driveway approaches and associated erosion control measures; the extension of curbs, gutters, paving and sidewalk improvements on Hitt Road between the end of the existing improvements and the relocated gate location; and sidewalks and street trees on Strawberry Lane shall be installed according to approved plans prior to the signature of the final survey plat. The Hitt Road improvements beyond the driveway of Lot 5 shall be sufficient to accommodate the placement of parking bays to accommodate two parking spaces and associated vehicular circulation, and shall be shown in the Final Plan submittal.*

*c) Subdivision infrastructure improvements, including but not limited to utilities; driveways, driveway approaches and associated erosion control measures; the extension of curbs, gutters, paving and sidewalk improvements on Hitt Road between the end of the existing improvements and the relocated gate location; and sidewalks and street trees on Strawberry Lane shall be installed according to approved plans prior to the signature of the final survey plat. ~~The Hitt Road improvements beyond the driveway of Lot 5 shall be sufficient to accommodate the placement of parking bays to accommodate two parking spaces and associated vehicular circulation, and shall be shown in the Final Plan submittal.~~*

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*Explanation:* As noted above, after reviewing the conditions with the project Engineer and Landscape Architect, Condition 5.j. is not appropriate as the area proposed for the parking has natural constraints that would be unnecessarily impacted. Condition 7.c. is essentially a related correction.

Overall the applicants contend placing the hiker parking spaces along Hitt Road, near lots #2 and #4, provide the vehicle occupants with easier entering and existing. The applicants also contend the proposed parking site is more consistent with the Planning Commission's original desire to have extra parking spaces for hikers of the Hitt Road area considering there currently is no on-street parking along Hitt Road. (NOTE: On-street parking is "not" a requirement in this zone and was not installed or planned for during the street's recent construction due to the site's sensitive topographical nature. Regardless, even though the applicants agreed to have an "extra" on-site parking space for each lot, the Planning Commission felt the additional on-street parking spaces were also necessary).

#### IV. FINDINGS OF FACT:

The following information has been provided by the applicants to help the Planning Staff, Planning Commission and neighbors better understand the proposed project. In addition, the required *findings of fact* have been provided to ensure the proposed project meets the Outline and Final Plan criteria as outlined in the Ashland Municipal Code (AMC), Section 18.88. Finally, the applicants have responded to the applicable Conditions of Approval as they relate to the Final Plan stage of the project as well as the phasing elements (i.e. utilities, easements, etc.).

*For clarity reasons, the following documentation has been formatted in "outline" form with the City's approval criteria noted in **BOLD** font and the applicant's response in regular font. Also, there are a number of responses that are repeated in order to ensure that the findings of fact are complete.*

**18.88.030 A. Outline Plan Criteria:** *NOTE: The Ashland Municipal Code does not address "criteria" for phasing or amendments to conditions. However, because the application is technically modifying the Outline Plan's approval, the applicant's are responding to the Outline Plan criteria so as to be thorough. Regardless, other than the modification for phasing and the modification of Condition 5.j., the Final Plan application is consistent with the Outline Plan. The Outline Plan criteria and response to each is as follows:*

**a. That the development meets all applicable ordinance requirements of the City of Ashland.**

All applicable City ordinances have been met with the submitted application. The phasing plan is an acceptable development option that is often included in land use development applications.

**b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire**

**protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.**

All public utilities are available to service the subject proposal and are located within the adjacent Strawberry Lane or Hitt Road rights-of-way. Multiple meetings have been held with the Ashland Public Works, Engineering, Fire, Sewer and Electrical Departments in order to verify and coordinate service abilities and connection points. All of the departments stated there is capacity to service the four new homes.

In addition, the project's Civil Engineer, in consultation with the project's Landscape Architect and Arborist, has designed all new private service connections to extend at right angles into and through each new lot's proposed driveways in an attempt to minimize site disturbances. Nevertheless, disturbances in areas greater than 25% slope, a geotechnical engineer will provide technical advice in an attempt to mitigate erosion possibilities.

**c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.**

The site plan and civil engineering (utilities, walls, driveways, etc.) have also been designed to recognize the site's natural features such as the various large trees, various groups of trees, neighbor's trees, extreme slopes, existing earth cuts, etc. The vast majority of the site's natural elements has been incorporated into the site planning and is either outside of the building envelopes, outside the path of the new driveways or within the common open space area of Lot #6. In addition, the project's Civil Engineer and Landscape Architect have attempted to place the driveways and private utilities in such a way that little to no impact on the site's trees or existing exposed cuts (Hitt Road).

Lastly, the common open space area of Lot #6 will remain in private ownership of the subdivision's property owners. Since the common open space area is intended to remain natural, the applicants do not foresee any significant issues or changes than what exists today other than the necessary fuels reduction. Attached are draft Conditions, Covenants and Restrictions which outline the property owners responsibilities. A final version will be submitted with the final plat.

**d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.**

The proposed development will not prevent adjacent land from being developed as shown on the Comprehensive Plan. The City's Transportation Plan, Zoning or Comprehensive Plan maps do not identify the area south of the property to be urbanized. The properties to the east, west and north are either under construction with houses, have houses recently constructed or have house plans pending. Essentially, from the applicant's perspective, this property appears to be the last or one of the last remaining parcels in the Strawberry Lane neighborhood to be developed – most of which has occurred in the last five to seven years.



**e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.**

The proposed subdivision will have Conditions, Covenants, and Restrictions (CC&R's) that provides the management structure to oversee the necessary maintenance of the project's common open space. The applicants do not foresee the need to create a Home Owner's Association due to the limited amount of expense associated with the open space's bi-annual fuels reduction, but has provided a draft copy of CC&R's with the application with a final version being submitted with the Final Plat.

**f. That the proposed density meets the base and bonus density standards established under this Chapter.**

The property is 4.62 acres in size, zoned RR-.5-P with a base density of 1.20 units per acre for an allowed density of 5.54 dwelling units. Under the Performance Standards Options Subdivision process, the density bonus options are available allowing additional units, but again, the property owners have elected to not increase the density beyond the proposed five (1 existing and 4 new).

**g. The development complies with the Street Standards.**

No new streets are proposed with this application as all of the proposed lots extend from existing rights-of-way. Both Strawberry Lane and Hitt Road were recently upgraded with new improvements consisting of sidewalks, curbs and gutters. The applicants will be installing two-two car parking bays along Hitt Road per a condition of approval.

#### **18.88.030 B. Final Plan Procedure & Criteria**

##### **Procedural and submittal requirements for Final Plan:**

**1. Procedure for approval. Type I procedure, as defined in this Title, shall be used for approval of final plans, unless an outline plan has been filed, in which case Type II procedure shall be used, and the criteria for approval of an outline plan shall also be applied.**

The proposal is for a six-lot Final Plan Subdivision and an Outline Plan modification request. The criteria addressing both entitlements are noted below.

**2. The final plan may be filed in phases as approved on the outline plan.**

This proposal includes a request for phasing with Lots #1, #2, and # 6 (common area) to be divided first and Lots #3, #4 and #5 to be divided in a second phase. Once fully subdivided, the property boundaries, driveways, and common area will be completed as originally presented.

**3. If the final plan or the first phase of the outline plan is not approved within eighteen (18) months from the date of the approval of the outline plan, then the approval of the plan is terminated and void and of no effect whatsoever. Extensions may be granted as a Type I procedure.**

The proposed recording of Phase II is intended to occur within 18 months of Phase I's completion.

**4. Contents. The final plan shall contain a scale map or maps and a written document showing the following for the development:**

**a. A topographic map showing contour intervals of five (5) feet.**

See attached plans.

**b. Location of all thoroughfares and walks, their widths and nature of their improvements, and whether they are to be public or private.**

See attached plans.

**c. Road cross sections and profiles, clearly indicating the locations of final cuts and fills, and road grades.**

See attached plans.

**d. The location, layout, and servicing of all off-street parking areas.**

See attached plans.

**e. The property boundary lines.**

See attached plans.

**f. The individual lot lines of each parcel that are to be created for separate ownership.**

See attached plans.

**g. The location of easements for water line, fire hydrants, sewer and storm sewer lines, and the location of the electric, gas, and telephone lines, telephone cable and lighting plans.**

See attached plans.

**h. Landscaping and tree planting plans with the location of the existing trees and shrubs which are to be retained, and the method by which they are to be preserved.**

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See attached plans.

**i. Common open areas and spaces, and the particular uses intended for them.**

See attached plans. The common open space area is intended to be left in a natural state.

**j. Areas proposed to be conveyed, dedicated, reserved or used for parks, scenic ways, playgrounds, schools or public buildings.**

Not applicable.

**k. A plan showing the following for each existing or proposed building or structure for all sites except single-family, detached housing which meets the parent zone setbacks.**

Not applicable as the subdivision is for single family houses.

**l. Elevation drawings of all typical proposed structures except single-family, detached residences which meet parent zone setback requirements. The drawings shall be accurate and to scale, including all attached exterior hardware for heating and cooling.**

Not applicable as the subdivision is for single family homes. Regardless, the Site Plan identifies building envelopes which are equal to or greater than the City's standards.

**m. Manner of financing.**

Financing is a conventional.

**n. Development time schedule.**

Phases are expected to be on 18 month increments and depend on market demand.

**p. Final plans for location of water, sewer, drainage, electric and cable T.V. facilities and plans for street improvements and grading or earth-moving improvements.**

See attached plans.

**q. The location of all trees over six (6) inches diameter at breast height, which are to be removed by the developer. Such trees are to be tagged with flagging at the time of Final Plan approval.**

See attached plans. All trees will be tagged with tree protection fencing around their perimeter.

**Final Plan Criteria:**

**Final plan approval shall be granted upon finding of substantial conformance with the outline plan. Nothing in this provision shall limit reduction in the number of dwelling units or increased open space provided that, if this is done for one phase, the number of dwelling units shall not be transferred to another phase, nor the open space reduced below that permitted in the outline plan. This substantial conformance provision is intended solely to facilitate the minor modifications from one planning step to another. Substantial conformance shall exist when comparison of the outline plan with the final plan shows that:**

**a. The number of dwelling units vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall the number of units exceed those permitted in the outline plan.**

The number of units and lots remain the same as approved with the Outline Plan.

**b. The yard depths and distances between main buildings vary no more than ten (10%) percent of those shown on the approved outline plan, but in no case shall these distances be reduced below the minimum established within this Title.**

The yard depths and distances between main buildings (See Building Envelopes on plans) remain the same as approved with the Outline Plan.

**c. The open spaces vary no more than ten (10%) percent of that provided on the outline plan.**

The open space has not changed and remains the same as approved with the Outline Plan.

**d. The building size does not exceed the building size shown on the outline plan by more than ten (10%) percent.**

The buildings shown on the Outline Plan were conceptual and are not applicable to this standard. However, the building heights, as identified on the Outline Plan, are applicable and remain as originally submitted.

**e. The building elevations and exterior materials are in conformance with the purpose and intent of this Title and the approved outline plan.**

The building elevations shown on the Outline Plan were conceptual and are not applicable to this standard.

**f. That the additional standards which resulted in the awarding of bonus points in the outline plan approval have been included in the final plan with substantial detail to ensure that the performance level committed to in the outline plan will be achieved.**

No density bonus points were requested with the Outline Plan.

*Don't know*

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**g. The development complies with the Street Standards.**

The development complies with the adopted Street Standards.

**OUTLINE PLAN CONDITIONS OF APPROVAL:**

**1) That all proposals of the applicant are conditions of approval unless otherwise modified herein.**

All of the proposals of the applicant, unless specifically modified with this request, are conditions of approval and will be met at the appropriate times.

**2) All conditions of the geotechnical report prepared by Amrhein Associates, Inc. and dated October 12, 2007, including but not limited to the inspection schedule, shall be conditions of approval unless otherwise modified herein.**

All of the conditions of the Amrhein Associates, Inc. report have been incorporated into the projects various development plans.

**3) That all proposed lots shall be subject to Solar Access Standard A unless 1) materials are provided with the Final Plan submittal demonstrating that an individual lot has a negative north slope in excess of 15 percent which would render it subject to Solar Access Standard B; or 2) a Solar Access Variance is applied for and approved for the individual lots concurrently with Final Plan approval. Solar setback calculations shall be submitted with each building permit to demonstrate compliance with the applicable standards, and shall include identification of the required solar setbacks with supporting formula calculations and elevation or cross-section drawings clearly labeling the height of the solar producing point(s) from the identified natural grade.**

All of the lots, other than Lot #5, are on slopes less than 15%. Lot #5 has north facing slopes of 25% and is thus has a Solar Access Standard B. At the time house plans are submitted, the property owner will be responsible for submitting solar access calculations.

**4) That all measures installed for the purposes of long-term erosion control, including but not limited to vegetative cover, rock walls, retaining walls and landscaping shall be maintained in perpetuity on all areas in accordance with 18.62.089.B.7.**

All erosion control measures as described will remain in perpetuity as required under 18.62.089. B.7. Such measures are described in the project's Conditions, Covenants and Restrictions.

**5) That prior to Final Plan approval:**

**a) Engineering for the utility plan including but not limited to the water, sewer, storm drainage and electric facilities shall be submitted. The utility plan shall include the location of connections to all public facilities in and adjacent to the development,**

**including the locations of water lines and meter sizes, fire hydrants, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins, and locations of all primary and secondary electric services including line locations, transformers (to scale), cabinets, meters and all other necessary equipment. Transformers and cabinets shall be located in areas least visible from streets, while considering the access needs of the Electric Department. Any required private or public utility easements shall be delineated on the utility plan.**

See attached Civil Engineering plans.

**b) An Electric Distribution Plan shall be coordinated with the Ashland Electric Department, and shall be included in the utility plan with the Final Plan submittal.**

See attached Civil Engineering plans. Such plans have been coordinated with the Ashland Electric Department.

**c) A drainage plan including necessary final engineering for the private lot stormwater detention systems and any off-site storm drain system improvements shall be provided.**

See attached Civil Engineering plans.

**d) The engineering for sidewalk improvements to complete sidewalk installation along the subject property's full Strawberry Lane frontage shall be provided with the Final Plan submittal.**

See attached Civil Engineering plans.

**e) The recommendations from the March 6, 2008 meeting of the Ashland Tree Commission, where consistent with applicable standards, shall be incorporated into the Final Plan submittal's Landscaping, Irrigation, and Tree Protection and Removal Plans.**

See attached Landscape Architect plans.

**f) A draft copy of the CC&R's and the applicants' proposed Deed Restrictions shall be provided. The CC&R's shall describe responsibility for the maintenance of all commonly-owned open space including but not limited to the implementation and maintenance of the approved fire prevention and control plan, and perpetual maintenance of required long term erosion control measures. The CC&R's shall note that any deviation from the approved Tree Removal and Protection Plan must receive written approval from the City of Ashland Planning Department. The CC&R's and Deed Restrictions shall be recorded concurrently with the final plat.**

See attached plans. A final copy shall be submitted at the time of Final Plat as requested.

**g) The overall lot coverage for the subdivision as a whole shall be limited to no more than 20 percent. At the time of final plan submittal, the applicants shall provide a**

**breakdown, by square footage, of the allowed lot coverage allocated to each lot and demonstrating that the overall subdivision's lot coverage does not exceed the 20 percent allowed in the RR-.5 zoning district.**

The overall lot coverage for the entire subdivision will not exceed 20%. In Performance Standards Developments, where building envelopes and driveways are constrained by shape, size and/or length in order for natural features to be retained or mitigated, the coverage issue for individual lots becomes critical. This is compounded by the applicant's efforts to provide a more horizontal footprint rather than a vertical one where lot coverage can actually force homes to be taller and have more mass – thus becoming more visible. Because the applicants propose to retain the most visible portion of the property as common open space, the applicant's are proposing the common open space's lot coverage allowance be allocated to the remaining parcels as follows (NOTE: At time of final survey, these amounts may change slightly based on the lot's actual surveyed dimensions):

Lot #1: 26% (.50 acre size) = 5,662 sq. ft. lot coverage (footprint, driveway, sidewalks, etc.)\*  
Lot #2: 26% (.50 acre size) = 5,662 sq. ft. lot coverage  
Lot #3: 28% (.50 acre size) = 6,098 sq. ft. lot coverage  
Lot #4: 20% (1.40 acre size) = 12,196 sq. ft. lot coverage  
Lot #5: 20% (1 acre size) = 8,712 sq. ft. lot coverage  
Lot #6: 0% (common open space) (.50 acre size)

Maximum Total Average Lot Coverage: 20%

*\*NOTE: The applicants have entered into a "private agreement" with the neighbors to the north (Tax Lot #110, Lou & Catherine Dimino) to limit Lot #1's Coverage to 20%, with an additional 6% allowed for pervious surface improvements such as pervious concrete pavers, pervious concrete, decking, gravel, etc.*

**h) That written verification from the project geotechnical expert shall be provided with the Final Plan submittal indicating that the revised six-lot subdivision configuration and associated improvements are consistent with the original report.**

See attached letter from Amrhein & Associates.

**i) That a landscape and irrigation plan addressing the re-vegetation of cut and fill slopes required in the geotechnical report shall be provided with the Final Plan submittal.**

See attached Landscape Architect plans.

**j) That in addition to the third off-street parking space proposed to be provided on each of Lots 1-5 by the applicants, the Final Plat submittal shall identify four on-street parking spaces to be provided on Hitt Road in bays near the relocated gate and between the driveways of Lots 2 and 4.**

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As noted, the applicants are requesting two modifications to this condition. First, the applicants are proposing to not be required to have an "extra" parking space on Lot #1, based upon a neighbors' request. As such, the zoning requirement of two on-site parking spaces per lot will apply to lot #1, unless a future property owner of Lot #1 decides a third space is desired. Secondly, the additional public/hiker parking along Hitt Road is proposed to be amended to be adjacent to Lots #2 and #4 instead of one bay being near the gate. Again, once this condition was evaluated by the project's Civil Engineer, Landscape Architect and Contractor, it became apparent the amount of earth disturbance, impact on vegetation, engineering effort, cost of the improvement and that the end result would create limited functionality for people existing their vehicles, the applicants decided the four spaces should be adjacent to Lots #2 and #4.

**6) That prior to the issuance of an excavation permit:**

**a) A preconstruction conference to review the requirements of the Physical Constraints Review Permit shall be held prior to site work, storage of materials, or the issuance of an excavation permit. The conference shall include the Ashland Planning Department, Ashland Building Department, the project engineer, project geotechnical experts, landscape professional, arborist, and contractor. The applicants or applicants' representative shall contact the Ashland Planning Department to schedule the preconstruction conference.**

The applicants and various team members are aware of this condition and will comply as noted.

**b) That a Verification Permit in accordance with 18.61.042.B shall be applied for and approved by the Ashland Planning Division prior to site work, storage of materials and/or the issuance of an excavation or building permit. The Verification Permit is to inspect the trees to be removed and the installation of tree protection fencing. The tree protection for the trees to be preserved shall be installed according to the approved Tree Protection Plan prior to site work or storage of materials. Tree protection fencing shall be chain link fencing a minimum of six feet tall and installed in accordance with 18.61.200.B.**

The applicants and various team members are aware of this condition and will comply as noted.

**c) That the temporary erosion control measures (i.e. fabric sediment fencing, straw bales, crushed rock pads, straw erosion control matting or plastic sheeting) shall be installed and maintained according to the approved plan prior to any site work, storage of materials, or issuance of an excavation permit. These measures shall be inspected and approved by the Staff Advisor prior to site work, storage of materials, or the issuance of an excavation permit.**

The applicants and various team members are aware of this condition and will comply as noted.

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**d) The applicants shall provide a performance bond, letter of credit or other financial guarantee in an amount equal to 120 percent of the value of the erosion control measures necessary to stabilize the site.**

The applicants and various team members are aware of this condition and will comply as noted.

**7) That prior to the signature of the final survey plat:**

**a) All easements for sewer, water, drainage, electric, streets or public pedestrian access shall be indicated on the final survey plat as required by the City of Ashland.**

As noted, the applicants are proposing to phase the subdivision in two phases, but will survey and identify all necessary easements or dedications for sewer, water, drainage, electric, streets or pedestrian access initially, but not record the actual boundaries of the Phase II lots until the Phase II improvements are completed.

**b) Street trees, located one per 30 feet of street frontage, shall be installed along the Strawberry Lane street frontage as part of the subdivision infrastructure improvements. Street trees shall be chosen from the Recommended Street Tree List and shall be installed in accordance with the specifications noted in the Recommended Street Tree List. The street trees shall be irrigated.**

Street trees along Strawberry Lane for Lot #1 will be installed as noted at the time Phase I is completed. Street trees in front of the flag pole of Lot #3, including the sidewalks, will be installed with the Phase II improvements.

**c) Subdivision infrastructure improvements, including but not limited to utilities; driveways, driveway approaches and associated erosion control measures; the extension of curbs, gutters, paving and sidewalk improvements on Hitt Road between the end of the existing improvements and the relocated gate location; and sidewalks and street trees on Strawberry Lane shall be installed according to approved plans prior to the signature of the final survey plat. The Hitt Road improvements beyond the driveway of Lot 5 shall be sufficient to accommodate the placement of parking bays to accommodate two parking spaces and associated vehicular circulation, and shall be shown in the Final Plan submittal.**

As noted, the applicants are also proposing a modification to this condition as a technical follow-up to Condition 5.j. which is being modified to not have any parking near the gate due to this area's physical constraints. In addition, with the phasing plan, the applicants are requesting various improvements be allocated to each phase with the public/hiker parking bays being completed with the Phase II improvements.

**d) That the installation of driveway approaches shall be completed according to city standards under permit from the Public Works/Engineering Department and any necessary inspections approved.**

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The applicants and contractor are aware of this condition and will comply.

**e) The existing sidewalk on Hitt Road shall be extended to the northerly edge of the Lot 5 driveway's approach.**

See attached Civil Engineering plans.

**f) Electric services shall be installed underground to serve Lots 1-5. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.**

The applicants and contractor are aware of this condition and will comply.

**g) That the sanitary sewer laterals and water services including connection with meters at the street shall be installed for Lots 1-5.**

See attached Civil Engineering plans.

**h) That Amrhein Associates, Inc. shall inspect the site according to the inspection schedule of the engineering geology report dated October 12, 2007 provided with the application. Prior to signature of the final survey plat, Amrhein Associates, Inc. shall provide a final report indicating that the approved grading, drainage and erosion control measures were installed as per the approved plans, and that all scheduled inspections were conducted by the project geotechnical expert periodically throughout the project.**

The applicants and the project's Geotechnical Engineer (Amrhein associates) are aware of this condition and will comply as noted.

**i) The landscaping and irrigation for re-vegetation of cut/fill slopes and erosion control shall be installed in accordance with the approved plan prior to signature of the final survey plat. Vegetation shall be installed in such a manner as to be substantially established within one year of installation.**

The applicants and the project's Landscape Architect and Contractor are aware of this condition and will comply as noted. All re-vegetation and erosion control measures noted will be completed prior to that phase's final plat.

**j) The applicants shall sign an agreement to participate in the future cost of street improvements for Hitt Road, including but not limited to sidewalks, curbs, gutters, paving, and storm drains.**

The applicants are aware of this condition and will comply at time of final plat.

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**k) That the applicants shall complete the relocation of the gate at the end of the improvements on Hitt Road to the southern extent of the street improvements. The relocation of the gate will be coordinated with the City of Ashland Water Department.**

The applicants are aware of this condition and will comply as noted. The gates relocation will be completed with the Phase II improvements.

**8. That prior to the issuance of a building permit:**

**a) Individual lot coverage calculations including all impervious surfaces shall be submitted with each building permit to demonstrate compliance with the lot coverage allocated to each lot. Building footprints, walkways, driveways including the flag drive for Lot 3, parking areas, and any impervious surfaces shall be counted for the purpose of lot coverage calculations.**

To be completed at time of building permit by the future property owner(s).

**b) The setback requirements of 18.88.070 shall be met and identified on the building permit submittals including but not limited to the required width between buildings as described in 18.88.070.D.**

To be completed at time of building permit by the future property owner(s).

**c) Building permit submittals shall clearly demonstrate compliance with the applicants' proposed "Elevation Height Limits" by providing cross sections or elevation drawings with building heights and elevations above sea level clearly labeled.**

To be completed at time of building permit by the future property owner(s).

**d) That a Physical and Environmental Constraints Permit for Hillside Development shall be applied for and approved in accordance with 18.62.040 for the development of Lot 5 prior to submission or issuance of a building permit.**

To be completed at time of building permit by Lot 5's future property owner(s).

**9. That prior to the issuance of a certificate of occupancy:**

**a) That the requirements of the Fire Department, including that approved addressing shall be installed prior to combustible construction; that a fire prevention and control plan shall be implemented and maintained; and that fire apparatus access, fire sprinklers as proposed by the applicants, and a fire hydrant shall be installed, shall be addressed.**

To be completed at time of an Occupancy Permit by the future property owner(s).

**b) All exterior lighting shall be directed on the property and shall not illuminate adjacent properties.**

To be completed at time of an Occupancy Permit by the future property owner(s).

**c) For Lot #3, the applicants shall provide mitigation for the removal of Tree #31 through on-site replanting, off site replanting, or payment in lieu of planting as provided for in AMC 18.61.084.**

To be completed at time of an Occupancy Permit by the future property owner(s) of Lot #3.

**d) Driveways greater than 50 feet in length, which are considered by definition to be flag drives and thus subject to the flag drive standards, shall be constructed according to flag drive requirements that a 12-foot paved width and 15-foot clear width be maintained, and that parking spaces be configured**

To be completed at time of an Occupancy Permit by the future property owner(s) of lot #3.

### **PHASING CLARIFICATIONS:**

The following is intended to describe and clarify the timing of the project's various private and public improvements based upon the two proposed phases. Again, Phase I is planned to begin immediately and will include Lots #1, #2, #4 and #6 (open space lot). Phase II will include the division of the remaining property (existing house lot) into three lots (Lot #3, #4 and #5). The applicants are proposing the phasing for mostly logical timing reasons, but some of the improvements associated with Phase II are related to expense and current housing market conditions. In particular, the requirement for public/hiker parking along Hitt Road was a condition of approval and not a development standard and thus its expense never anticipated or budgeted. Also, the applicants attempted to mitigate any on-street parking issues by "proposing" extra on-site parking (two spaces currently required). In doing so, the applicant believed they were addressing the development's impacts and that any required on-street public/hiker parking spaces were not their responsibility.

Regardless, the applicants are more than willing to install the on-street public/hiker parking spaces, but would like to defer this improvement until Phase II in order to generate the necessary funds to complete the work. In the applicant's opinion and considering the circumstances noted, they believe this is a fair proposal. All other phasing improvements are described as follows:

**Phase I improvements:** *Again, Lot #4 (existing house lot) will not be divided until Phase II and will retain the area of Lots #3, #4 and #5 until Phase II is recorded.*

#### **Lot #1 and #2**

- All utilities and easements for Lot #1 & #2.
- Street Trees along Lot #1's Strawberry Lane frontage.
- Lot #1 and #2's driveway cut and apron.
- All associated retaining, erosion control, geotechnical measures related to improvements.

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- House, driveway, storm drainage, tree protection measures, etc. will be completed by future lot owners.

Lot #6 (open space lot)

- No improvements proposed.

Remaining property (includes the area of Lots #3, #4 (existing house) and #5.

- All easements associated with “both” phases of the subdivision.
- All existing utilities to be relocated within planned easements, where necessary.
- All associated retaining, erosion control, geotechnical measures related to utility improvements.

**Phase II improvements (remaining property):**

Lot #3, #4 and #5

- All necessary remaining easements.
- Lot #3, #4 and #5 driveway cuts and aprons.
- All associated retaining, erosion control, geotechnical measures related to improvements.
- House, driveway aprons, storm drainage, tree protection measures, etc. will be completed by future lot owners.
- All improvements associated with existing house such as the new driveway off of Hitt Road, associated retaining walls and tree protection measures. Such improvements will also include the street trees and sidewalk improvements along the frontage of Lot #3’s driveway entrance along Strawberry Lane.
- All improvements to Hitt Road, including two two-space parking bays adjacent to Lots #3 and #4, gate relocation, sidewalk extension to Lot #5’s driveway.

**Conclusion:** Overall, the applicants and project consultants believe the submitted Outline modifications and Final Plan documents will prove to be an example for hillside development planning in Ashland. In addition to the significant amount of time working with the adjacent neighbor to the north, a significant amount of effort was spent on minimizing typical hillside mass and disturbance of natural features. Again, the fact that one additional lot could have been created in the area now proposed to be common open space – an area visible to most residents in Ashland, is a reflection of the applicant’s efforts to minimize the developments impact on the neighborhood and community.

2009

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2009

March 12, 2008  
Project No. U264-01.01

Mr. Derek Severson, Project Planner  
City of Ashland  
51 Winburn Way  
Ashland, Oregon 97520



Re: Geotechnical Review  
Strawberry Lane Subdivision  
Ashland, Oregon

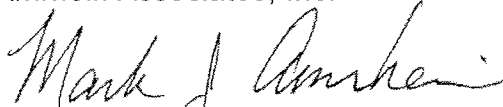
Dear Mr. Severson:

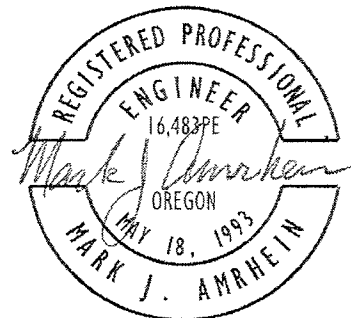
Amrhein Associates, Inc. (AAI) has reviewed the Civil Improvement Plans, prepared by CEC, inc. and dated March 2009, for the Strawberry Lane Subdivision. We understand some modifications have been made to Plans since our Geotechnical Engineering Report was prepared in October 2007.

Based upon our review, the updated Plans have been prepared in general accordance with our recommendations presented in our Geotechnical Engineering Report. No changes or modifications in our original recommendations or the Plans are necessary.

AAI appreciates the opportunity to review the Civil Improvement Plans. Please give me a call if you have any questions.

Sincerely,  
Amrhein Associates, Inc.

  
Mark J. Amrhein, P.E.  
President / Senior Engineer



RENEWAL DATE: 12/31/09

cc: Mark Knox, Urban Development Services

U264-01.01

MAR 12 2009

City of Ashland  
Community Development

**\*\*“DRAFT”\*\***  
**(final CC&R’s to be completed by an Attorney)**

**DECLARATION OF CONDITIONS, COVENANTS AND RESTRICTIONS  
FOR  
“Strawberry Fields”  
A CLASS II PLANNED COMMUNITY**

THIS DECLARATION is hereby made and executed in Jackson County, Oregon, this \_\_\_\_ day of \_\_\_\_\_, \_\_\_\_ by the undersigned, Robert and Laura McLellan (hereinafter collectively, the “declarant”).

**RECITALS**

Declarant is the owner of all of the real property known as “Strawberry Fields” a Planned Unit Development in the City of Ashland, Oregon and also appears on the map and plat thereof recorded in the Volume, \_\_\_\_ at Page \_\_\_\_ of the Plat Records of Jackson County Oregon, and described on the attached Exhibit “A” (hereinafter the “Property”). Declarant desires to create a Class II Planned Community on the Property, to be known as Strawberry Fields, comprised of five residential lots and one common open space area. This Declaration shall constitute covenants to run with all of the land and shall be binding upon all persons claiming under them and that these conditions and restrictions shall be for the benefit of and the limitations on the undersigned, its successors and assigns, and all future owners of said real property or any portion thereof, and the same are hereby made a part of all conveyances of real property within the said subdivision as described below.

These Conditions, Covenants, and Restrictions are for the purpose of maintaining and protecting the value, desirability and attractiveness of the common open space area and the lots in Strawberry Fields and for providing a meaningful and reasonable provision for operations, maintenance and upkeep of the common open space area and to insure the health, safety, and welfare of the owners within Strawberry Fields without a Homeowners Association, but consistent with the applicable provisions of the Ashland Land Development Ordinance and as described as follows:

**Section 1: Land Use, Building Type and Use, and Location:**

The use of the lots in this development shall be restricted to residential purposes, except as hereinafter set out. No asbestos or tar paper siding shall be used on the exterior of any building in the project. Architectural composition shingle, shake, or tile roof finishes may be used on any dwelling or accessory building, and accessory buildings shall conform generally to the same finish as the dwelling to which it is appurtenant. All building heights shall be regulated as described on the project’s Building Height Plan. All buildings shall be located upon the lots, within the described building envelopes, in conformity with the provisions of the development’s Site Plan and all Ashland Building and Planning Department’s regulations and codes in effect upon the date of recording these covenants.

**Section 2: Utilities and Easements:**

Easements for the installation and maintenance of utilities and drainage outside the dwelling unit are reserved as shown in the recorded Final Plat. Separate utility services shall be supplied to each individual lot line. It is the responsibility of each lot owner to bring utility service from their respective lot line to their respective dwelling unit. Service equipment and meters may be grouped in one or more locations as recommended and approved by the Ashland Public Works Department. At the time of construction and installation of public utility lines, the party Strawberry Fields CC&R’s

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installing same shall bear the entire cost of the construction and installation of the public utility lines.

**Section 3: Signs:**

No sign of any kind shall be displayed in public view on any lot or building on said property, except real estate (ie. "For Sale") signs of not more than 4 square feet.

**Section 4: Sanitation:**

No part of said property shall be used or maintained as a dumping ground for rubbish, trash, garbage, or any other waste. No garbage, trash, or other waste shall be kept or maintained on any part of said property except in a sanitary container, which shall be kept in a clean and sanitary condition, and shall be stored so as not to be visible from the street except for garbage pickup day.

**Section 5: Nuisance:**

No noxious or offensive conditions shall be permitted upon any part of said property, nor shall anything be done thereon which may be or become an annoyance or nuisance to the neighborhood. With the exception of dogs, cats or other domestic pets, no animals, livestock or poultry of any kind shall be raised, bred or kept on any lot. Domestic pets shall not be kept or maintained for any commercial purpose or in unreasonable numbers.

The term "Nuisance" as used in this section, includes but is not limited to (1) loud and offensive noise, (2) barking dogs, (3) storage of junked, wrecked or inoperative vehicles, and (4) littering, waste materials, offensive odors or other conditions deemed a nuisance as defined by State law. Nuisance enforcement will be via complaint and citation through the Ashland Police Department.

**Section 6: Vehicle Storage:**

Parking of boats, trailers, motorcycles, truck/campers, recreational vehicles, vehicles under repair, and like equipment shall not be allowed on the grounds of any lot, nor on any private or public ways adjacent thereto (with the exception of temporary parking for the purposes of loading and unloading) excepting only within the confines of an enclosed garage. Automobile washing, waxing, and cleaning etc. is exempt from this requirement.

**Section 7: Poles, antennas and overhead wires:**

No poles, overhead wires or antennas shall be erected or used for any purpose (This provision is not to apply to power poles or lines already existing at the time of this declaration, or the rights of the power utility to maintain or reconstruct same, or for temporary purposes during construction). Twenty-Four (24") inch or smaller satellite dishes are permissible and may be installed on the roof of exterior walls near the roof eaves. Satellite dishes shall be installed to minimize view impacts from adjacent residents.

**Section 8: Common Open Space Area:**

Declarant shall convey to each lot owner in Strawberry Fields an equal undivided percentage ownership interest in the common open space area as shown on the Plat. The common open space area shall be jointly maintained by all of the owners as a common element of Strawberry Fields for the purpose of retaining the common open space area in its natural state, except for fire prevention management and maintenance purposes.

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**Section 9: Temporary Residence:**

No camper, recreational vehicle, tent, trailer or other vehicle or structure shall be used at any time for a temporary residence on this property or any lot or adjacent public right or way.

**Section 10: Architectural Review:**

The plans, specifications, and plot plan for the first residential dwelling and garage to be constructed upon each lot shall be approved before construction may commence by the Declarant. Approval thereof shall be obtained by the application of the lot owner to the Declarant, primarily as to conformity and harmony of external design with the uniform design standards adopted by the Declarant for Strawberry Fields as amended from time to time. The Declarant shall, within 5 business days from the application, shall approve, approve with conditions, or disapprove such plans or specifications. Such approval shall not be unreasonably withheld.

**Section 11: Fence and Hedges:**

All fencing and hedges shall comply with the City of Ashland's zoning requirements, except maximum height of any sight-obscuring fence shall be six feet. Fences shall be constructed only of Declarant approved materials and shall not detract from the appearance of the Strawberry Fields, or detract from the appearance of dwellings located on adjacent lots.

**Section 12: Right of Enjoyment and Enforcement:**

These covenants, conditions, and restrictions, shall operate for the benefit of and may be enforced by Declarant and also by the owner or owners of any portion of said property. Enforcement shall be by legal proceedings against any person or persons violating or attempting to violate any of the covenants, restrictions, conditions either to restrain violation or to recover damages for their violation, or both. The prevailing party or parties in any such enforcement proceeding including any arbitration shall be entitled to recover from the losing party or parties reasonable attorney's fees including attorney's fees in the arbitration or in any trial or appeal. Failure by any owner to enforce any covenant or restriction herein contained shall in no event be deemed a waiver of the right to do so. In the event of any dispute related to this Declaration, the parties to the dispute shall use nonbinding mediation and binding arbitration to resolve the same. Arbitration shall be in accordance with the rules of the Arbitration Services of Portland, Inc., or a comparable private arbitration service selected by the party requesting binding arbitration. No delay or omission on the part of Declarant, or the owners of other Lots in exercising any right, power or remedy herein provided in the event of any breach of the covenants, conditions or restrictions herein contained shall be construed as a waiver thereof or acquiescence therein; and no right of action shall accrue nor shall any action be brought or maintained by any one whatsoever against Declarant and no right of action except specific performance shall accrue on account of their failure to bring any action on account of any breach of these covenants, conditions or restrictions, or for imposing restrictions herein which may be unenforceable.

**Section 13: Severability:**

Invalidation of any one of these covenants, conditions or restrictions by judgment or court order shall in no way affect any other provisions, which shall remain in full force and effect.

**Section 14: Impervious Surfaces:**

Each lot shall be limited to the amount of lot coverage, as defined in the Ashland Land Use Ordinance, Section 18.08.160, as described below:

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Lot #1: 20%. An additional 6% of Lot #1 may contain pervious surface coverage including, but not limited to: pervious concrete, pavers, gravel, or decking, for the construction of a driveway, sidewalks, paths, patios and decks.

Lot #2: 26%

Lot #3: 28%

Lot #4: 20%

Lot #5: 20%

**Section 15: Landscaping (Lot #1):**

Within 1 year of the date that the City of Ashland issues a Certificate of Occupancy for any improvement constructed upon Lot #1, the owners of Lot #1 shall install landscaping to mitigate the appearance of the driveway and the mass of the building in the area bounded by the driveway, building and Strawberry Lane. The landscaping shall be maintained or replaced by the owners and any successors of Lot #1. In addition to the City-required Strawberry Lane street trees, the landscaping of this bounded area shall include a minimum of 2 trees and 21 bushes and/or dwarf/small trees. When planted the trees shall be a minimum of 1 ¾ inch to 2 inch caliper trees of a species that grows to a minimum of 20 feet and a maximum of 30 feet when mature. When planted the bushes and/or dwarf/small trees shall be a minimum of 5 gallon plants of a species that grows to a minimum height of 2 to 4 feet when mature. At the Lot #1 owner's discretion, these landscaping requirements may be periodically replaced with new plantings.

**Section 16: Erosion Control Measures:**

Each property owner shall be responsible for maintaining in perpetuity erosion control measures as outlined on the Erosion Control Plan, in accordance with Ashland Municipal Code, Section 18.62.089.B.7, and within the Amrhein Associates, Inc. report dated October 12, 2007, including the inspection schedule during site construction.

**Section 17: Fire Prevention and Control Plan:**

In addition to contributing an equal share for fuels reduction maintenance for the common open space area, each property owner shall be responsible maintaining their property in accordance with the Fire Prevention and Control Plan. Prior to any combustible construction or material stored on a lot, the measures identified on the Fire Prevention and Control Plan for that specific lot shall be implemented.

**Section 18: Tree Removal and Protection Plan:**

Unless otherwise identified within the building envelopes identified with the Tree Removal and Protection Plan or as permitted under the Fire Prevention and Control Plan, no trees greater than 6" in diameter at breast height shall be removed without approval from the City of Ashland's Planning Department.

**Section 19: Miscellaneous:**

The conditions, covenants and restrictions of this declaration shall run with and bind the land henceforth, and shall inure to the benefit of and be enforceable by the owner of a lot subject to this declaration, their respective legal representatives, heir, successors and assigns.

Strawberry Fields CC&R's

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City of Ashland  
Field ☐ Office ☐ Court ☐

Any of the conditions, covenants or restrictions of this declaration may be amended by an instrument signed by sixty (60%) of the property owners within the development and the consent of the Declarant so long as the Declarant owns any lots in Strawberry Fields. Such amendments shall be recorded in the appropriate Deed Records of Jackson County, Oregon, to be effective. Easements herein granted and reserved shall not be amended except by instrument signed and acknowledged by all of the Owners of the property concerned.

The provisions of this Declaration shall run with and bind the land, and shall inure to the benefit of and be enforceable by Declarant, the Association, and the Owners of all Lots subject to this Declaration, their respective legal representatives, heirs, successors and assigns, for a term of fifty (50) years from the date of the recording of this Declaration, after which time said provisions will be automatically extended for successive periods of ten (10) years each unless an instrument signed by 51% of the owners, agreeing to terminate or modify said provisions.

In order to protect and preserve the appearance and value of the Real Property, each Owner is required to immediately commence, and diligently pursue without delay, the repair or rebuilding of his Residence or other Structure after any loss to it.

If the Declarant conveys its title to all or part of the lots to a third party and designates in such conveyance that such party shall be the successor Declarant then such successor Declarant shall have all duties, rights, powers and reservations of the Declarant contained in this Declaration upon the acceptance and recording of such conveyance.

THE UNDERSIGNED, AS DECLARANT, SUBSCRIBED TO AND DATED THESE CONDITIONS, COVENANTS AND RESTRICTIONS in Medford, Jackson County, Oregon,

this \_\_\_\_ day of \_\_\_\_\_ 200\_\_ :

\_\_\_\_\_  
Robert McLellan

\_\_\_\_\_  
Laura McLellan

STATE OF OREGON )

) ss.

County of Jackson )

Personally appeared Robert and Laura McLellan, as Declarant, and acknowledge the Conditions, Covenants and restrictions to be a voluntary act and deed:

BEFORE ME:

\_\_\_\_\_  
Notary Public for Oregon

My Commission Expires \_\_\_\_\_

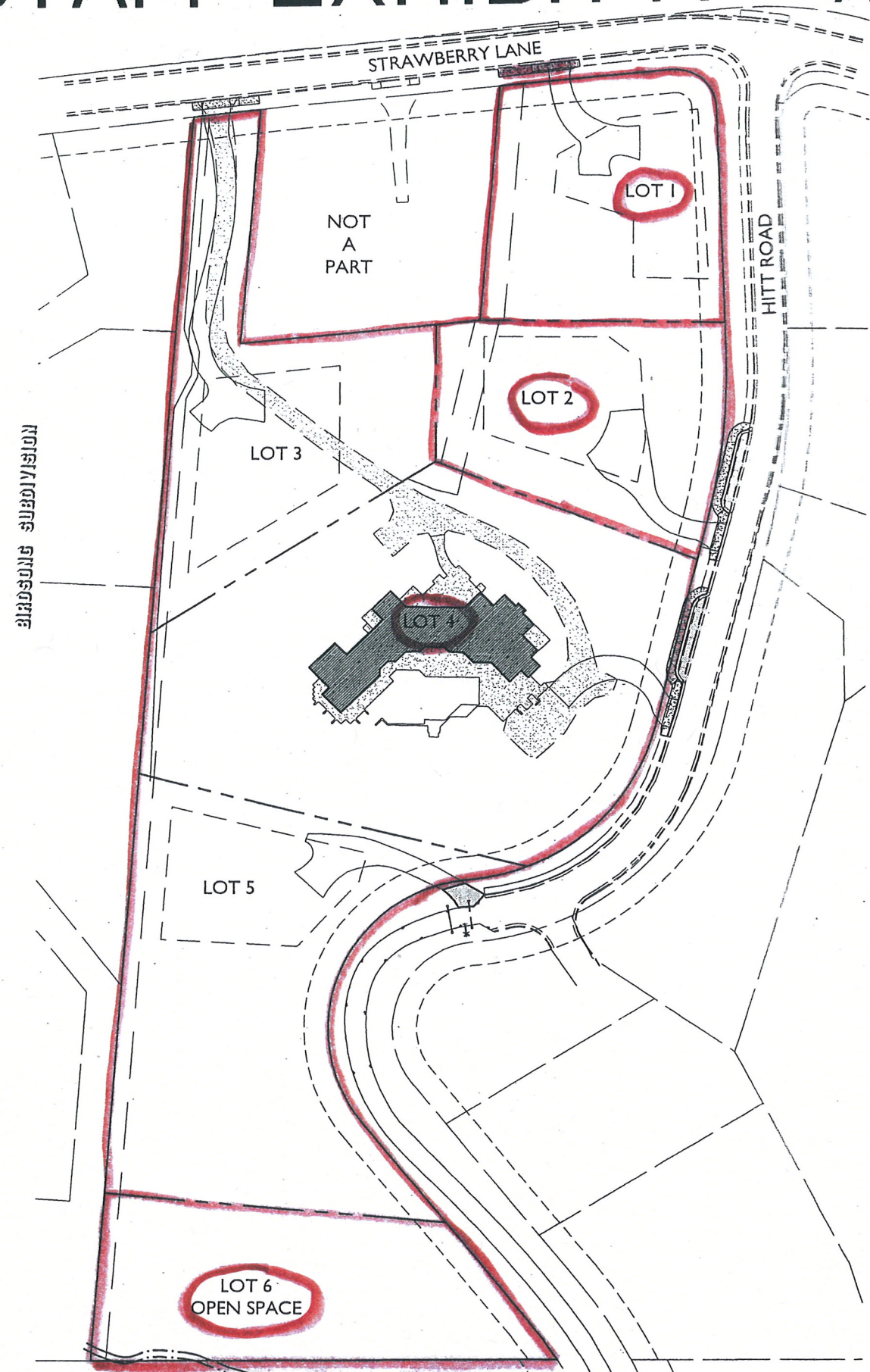
Strawberry Fields CC&R's

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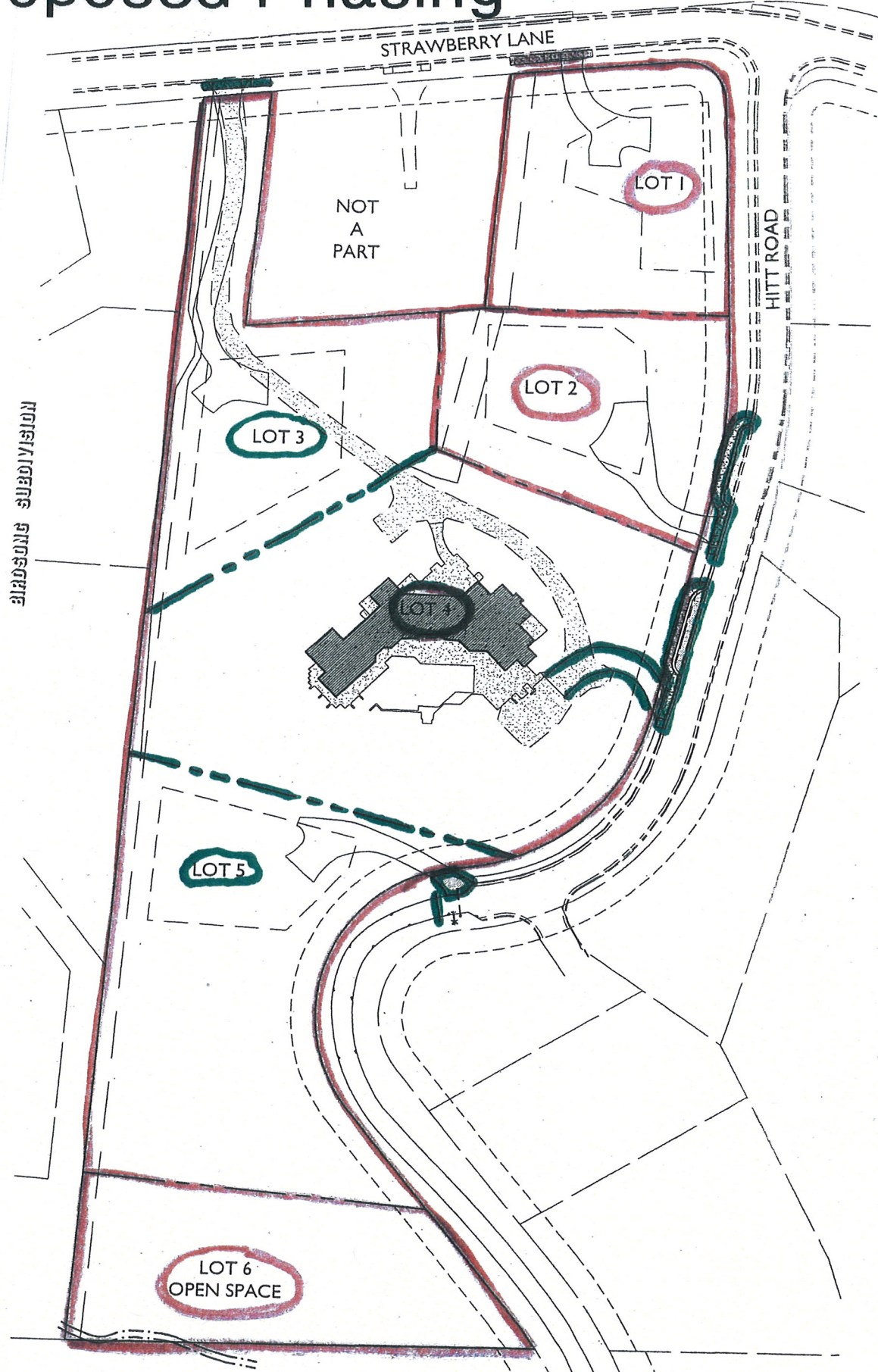


# STAFF EXHIBIT A

# Applicant's Proposed Phasing



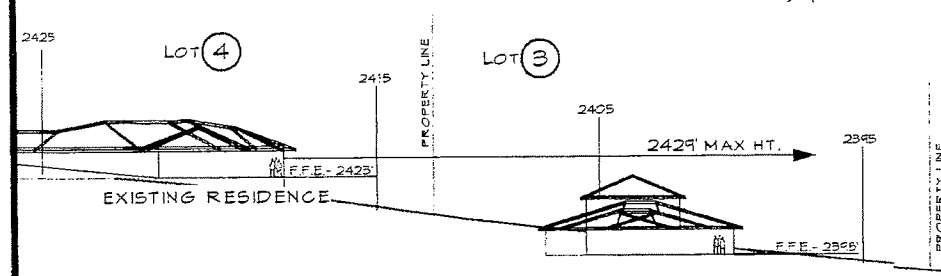
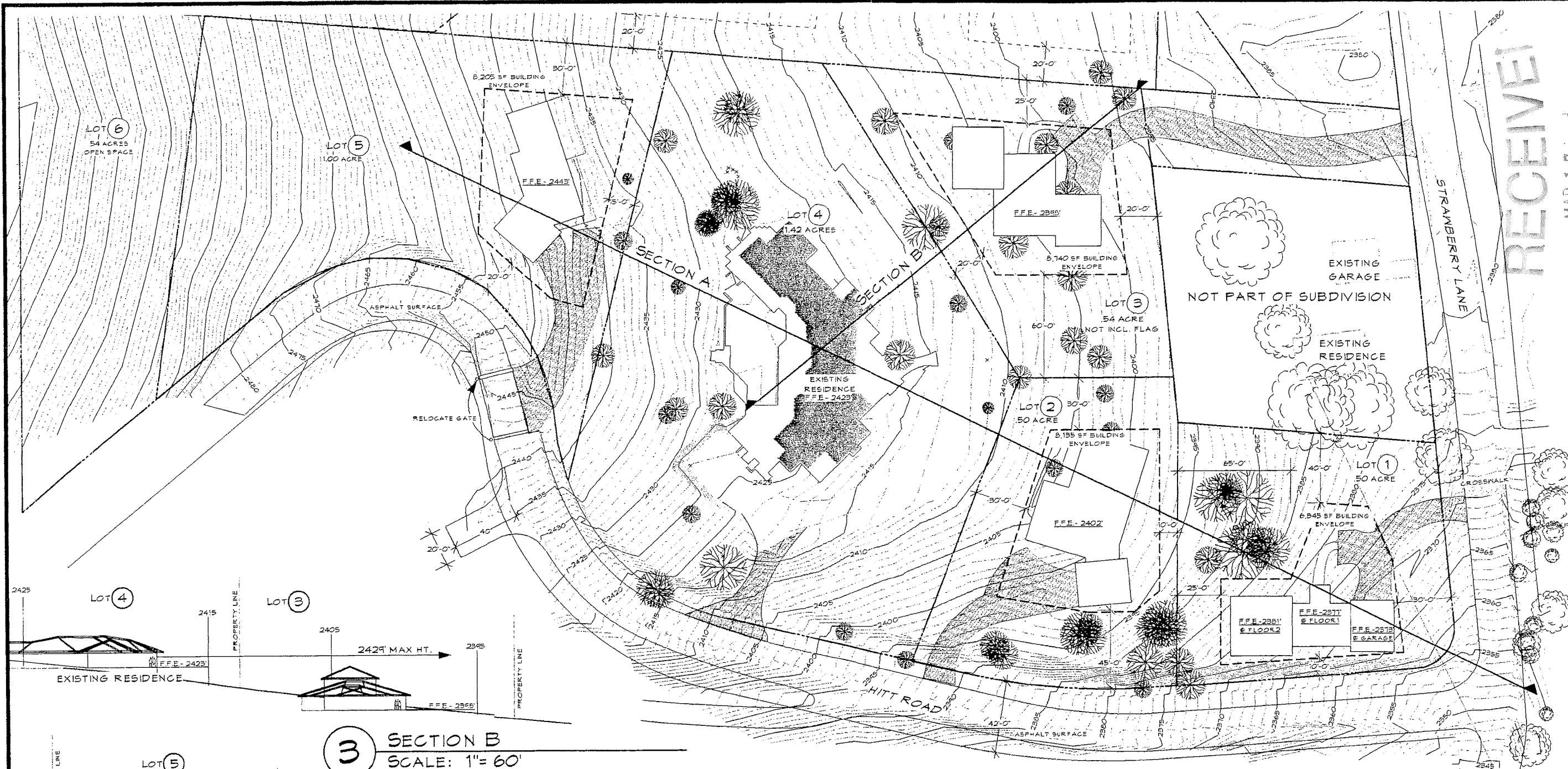
Phase I



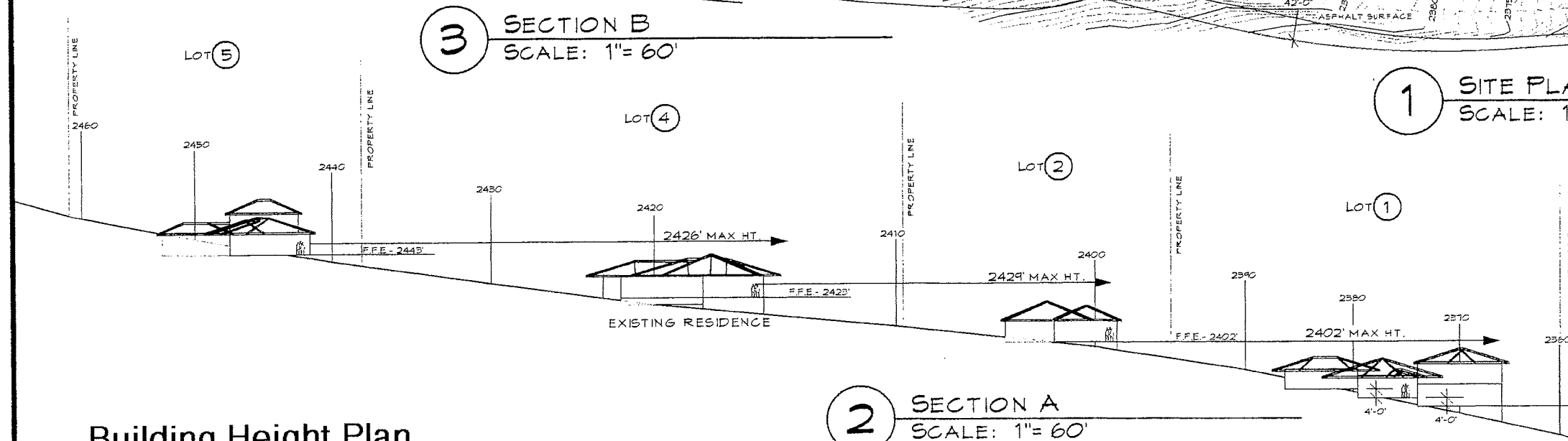
Phase II

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**3** SECTION B  
SCALE: 1" = 60'



**2** SECTION A  
SCALE: 1" = 60'

**1** SITE PLAN-CONCEPTUAL  
SCALE: 1" = 60'

**NOTES:**

ALL HOMES EXCEPT EXISTING ARE CONCEPTUAL  
CEILINGS ARE SHOWN AT 9'-0" HT.  
ROOFS ARE SHOWN AT 5:12 PITCH  
SIGHT LINES SHOWN ARE 6'-0" FROM FINISHED FLOOR, U.N.O.

THE IDEAS INCLUDED HEREIN ARE BASED UPON EXISTING CONDITIONS PROVIDED BY OTHERS, AND WHILE EVERY ATTEMPT HAS BEEN MADE AT ACCURACY, ALL PLANS ARE CONCEPTUAL AND NOT GUARANTEED.

Building Height Plan

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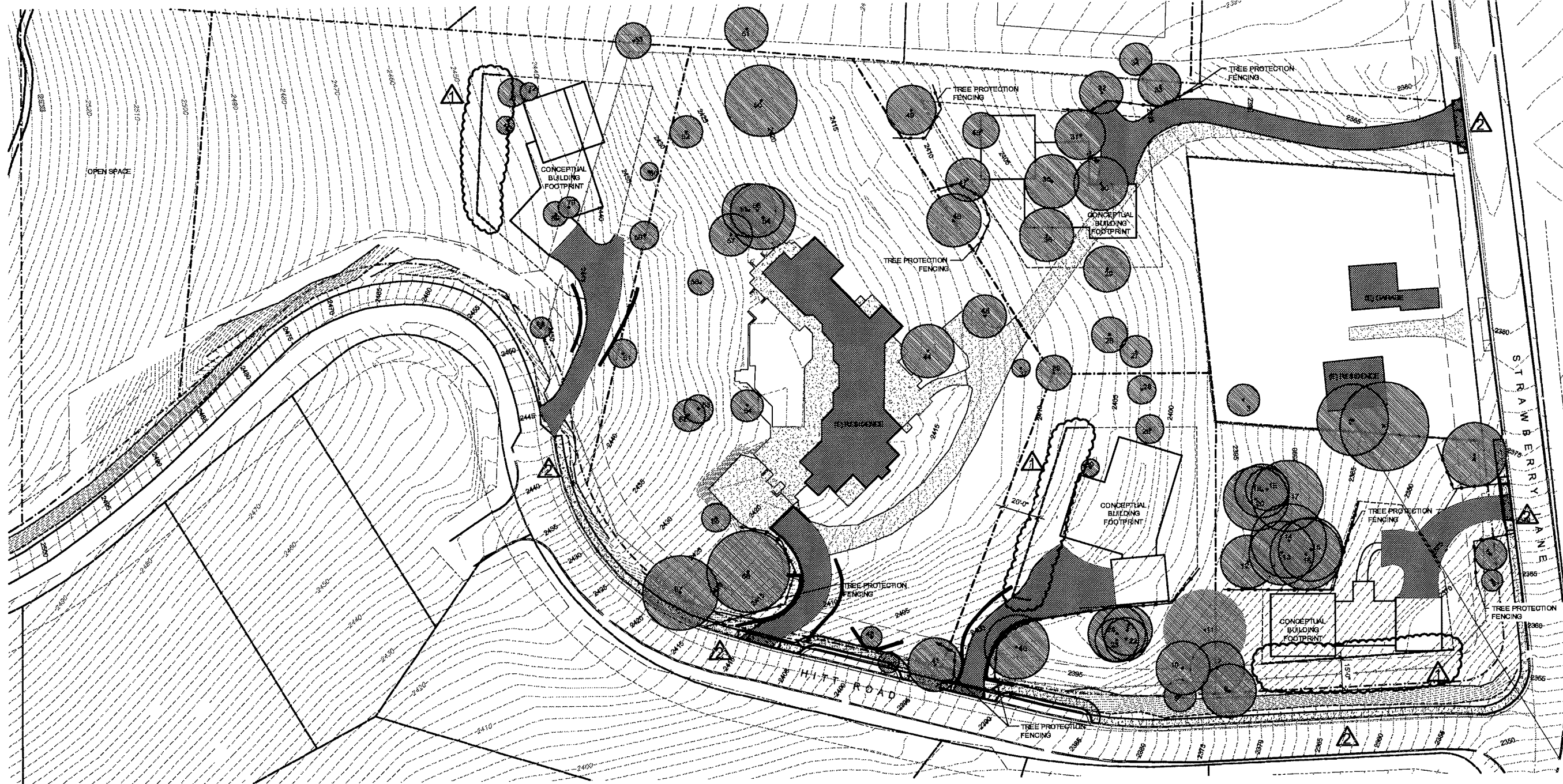
MAR 17 2008  
City of Ashland  
Field Office

BLUE DESIGN  
541-890-2159  
blue@blue-design.com

DATE ISSUED: 2/7/08  
JOB NO.: 237  
DRAWN BY: JB  
CHECKED BY:

MCLELLAN PROPERTY  
PROPOSED SUBDIVISION  
500 STRAWBERRY LN  
ASHLAND, OR 97520

SHEET:  
A1 OF 1



**TREE PROTECTION DETAILS**

**TREE PROTECTION NOTES**

A. Landscape adjacent to the project area shall be protected from damage. No storage of equipment or materials shall occur within drip lines of trees to be preserved which are those identified on this plan.

B. Trees that are shown to remain shall be protected with fencing as shown in Detail. Fencing shall be 6' tall temporary chain link panels installed with metal connections so that all panels are integrated, these fences shall be installed so that they do not allow passage of pedestrians and/or vehicles through it.

C. Exceptions to the tree protection specifications may only be granted with written approval from owner's representative.

D. Work within dripline of trees to remain may require disturbance of tree protection fences. Contractor shall obtain authorization from owner's representative prior to moving fence. Contractor shall remove the fence temporarily to complete work, and replace at the end of each work day. No storage of equipment or materials shall occur within dripline of trees. After the proposed work within dripline is completed, fencing shall be reinstalled. Note: Where protection fencing overlaps proposed construction, the following measures shall be followed:

- 1) Hand dig to required depth of final work.
- 2) Roots under 2" in diameter may be hand cut at a 90° angle.
- 3) Where roots greater than 2" in diameter are encountered, contractor shall notify Landscape Architect or arborist for direction.
- 4) Do not raise the soil level within the drip lines of existing trees.
- 5) Trees to be preserved shall be deep watered throughout construction period as necessary - see detail.
- 6) Inspection Schedule:

- 1) Fencing locations and installation technique shall be approved by owner's representative before demolition or rough grading begins.
- 2) Routine inspections of fencing and site conditions will occur randomly during construction. Work shall cease if fencing is damaged or moved without prior approval from owner's representative.
- 3) Upon completion of project to determine condition of trees post construction.

Tree #	Species	DBH in inches	Height in feet	Crown Radius in feet	Condition	Species tolerance to construction	Tree Protection Zone radius in feet	Notes
1	Quercus laevis	12	25	15	Good	High	20	
2	Quercus laevis	10	20	12	Good	High	18	
3	Quercus laevis	8	18	10	Good	High	16	
4	Quercus laevis	6	15	8	Good	High	14	
5	Quercus laevis	4	12	6	Good	High	12	
6	Quercus laevis	3	10	5	Good	High	10	
7	Quercus laevis	2	8	4	Good	High	8	
8	Quercus laevis	1	6	3	Good	High	6	
9	Quercus laevis	1	6	3	Good	High	6	
10	Quercus laevis	1	6	3	Good	High	6	
11	Quercus laevis	1	6	3	Good	High	6	
12	Quercus laevis	1	6	3	Good	High	6	
13	Quercus laevis	1	6	3	Good	High	6	
14	Quercus laevis	1	6	3	Good	High	6	
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98	Quercus laevis	1	6	3	Good	High	6	
99	Quercus laevis	1	6	3	Good	High	6	
100	Quercus laevis	1	6	3	Good	High	6	

**Laurie Sager and Associates Landscape Architects Inc**  
 700 Mistletoe Road, Suite 201  
 Ashland, Oregon 97520

**REGISTERED**  
 STATE OF OREGON  
 REG. # 527

Revision Date:  
 Based on 3/11/08  
 Planning Commission Comm  
 March 10, 2009  
 Based on 11/4/08  
 City Council Comments

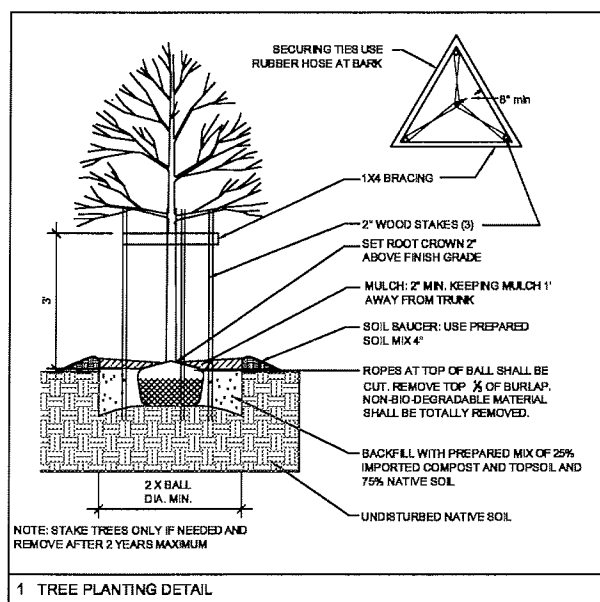
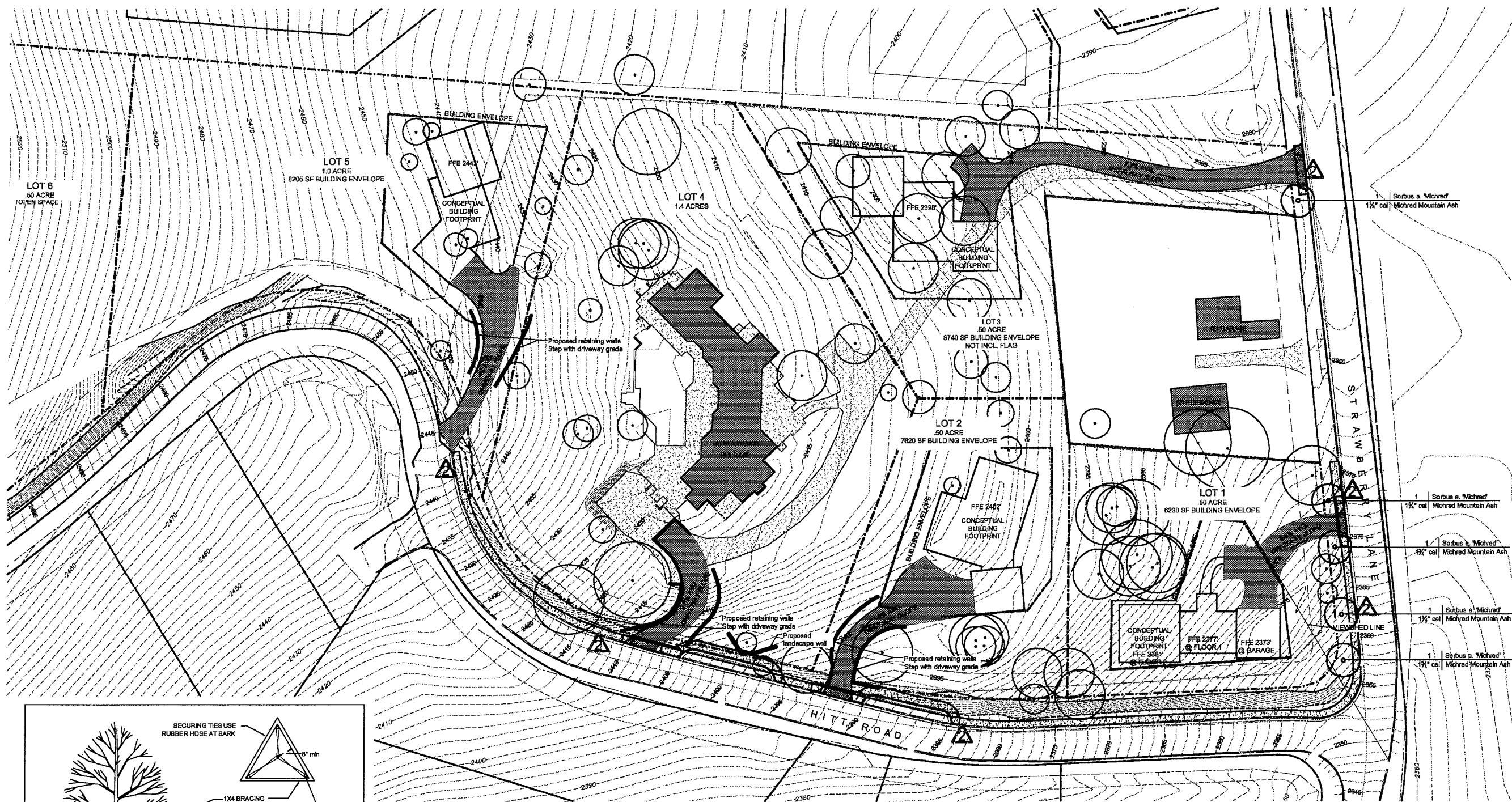
Drawn By:  
 KAG  
 Scale: NTS

**McLellan Property**  
 PROPOSED SUBDIVISION  
 500 STRAWBERRY LANE  
 ASHLAND, OREGON

February 6, 2008

**TREE REMOVAL AND PROTECTION PLAN**

**L-1**



**NOTES:**  
 1. See grading plan for information about proposed driveways and wells.  
 2. Automated irrigation shall be provided for all proposed street trees.  
 3. Revision #2 includes: proposed sidewalk along Hit Road and at driveways on Strawberry Lane, street trees along Strawberry Lane seen on L-2, grade changes and landscape well to accommodate on-street parking.



Revision Date:  
 March 12, 2009  
 Based on 11/4/08  
 City Council Comments  
 Drawn By:  
 KAG  
 Scale: NTS

**McLELLAN PROPERTY**  
 PROPOSED SUBDIVISION  
 500 STRAWBERRY LANE  
 ASHLAND, OREGON

February 6, 2008

**FINAL PLAN**





#### Fire Prevention and Control Notes

1. Provide 'fuel free' zone within 5' of structures including decks.
2. Cut all grasses and weeds to 6" before June 15th each year.
3. Thin trees and shrubs to prevent interlocking canopies. Maintain isolated islands of vegetation. Distance between branch tips of trees shall be 10' minimum within 45' of any structure.
4. Within 100' of any structure, thin 'ladder fuel' under trees. Understory shrubs, dead wood, and lower tree branches to 6' shall be thinned to reduce interlocking stands of fast burning plant species.
5. Provide 13'-6" minimum clearance above road surface for fire vehicles to pass under.
6. All existing trees indicated on plans shall remain unless shown within a proposed building envelope and approved by the City.

#### NOTE

Revision #2 includes: proposed sidewalk along Hitt Road and at driveways on Strawberry Lane, street trees along Strawberry Lane seen on L-2, grade changes and landscape wall to accommodate on-street parking.

#### Erosion Control Notes

1. For erosion control measures during construction follow recommended erosion control provisions in Amrhein & Associates, Inc. Geotechnical Engineering Report dated 10-12-07. For erosion control hydroseeding requirements along Hitt Road follow provisions outlined on sheet D-1 by Construction Engineering Consultants. For post-grading erosion control seeding follow provisions outlined on sheet L-3 by Landscape Architect.
2. After excavation and/or vegetation removal, contractor shall immediately revegetate all areas disturbed by construction.
3. Erosion control seed mix shall be applied to areas with over 15% slope, in a mix of wood fiber mulch, fertilizer, and tackifier per manufacturers specifications.
4. Seeding shall occur between March 1 and May 1 or November 1 and December 1, no supplemental irrigation shall be required.
5. Erosion control mix available from S & S Seed - [www.sseeds.com](http://www.sseeds.com)

Species	Pounds per acre
Bromus hordeaceus	24
Trifolium hirtum, Hykon	10
Vulpia myuros var. hirsuta	6



**40' Fuel Break Zone** - Remove all dead and dying vegetation and thin fast burning species, creating isolated islands of vegetation, so that there are no interlocking canopies. Limb all trees to 6' above grade except in islands of vegetation.



**100' Fuel Break Zone** - Remove all dead and dying vegetation, clear sufficient vegetation to reduce fuel loads throughout 100' zone, creating vegetation islands as appropriate.



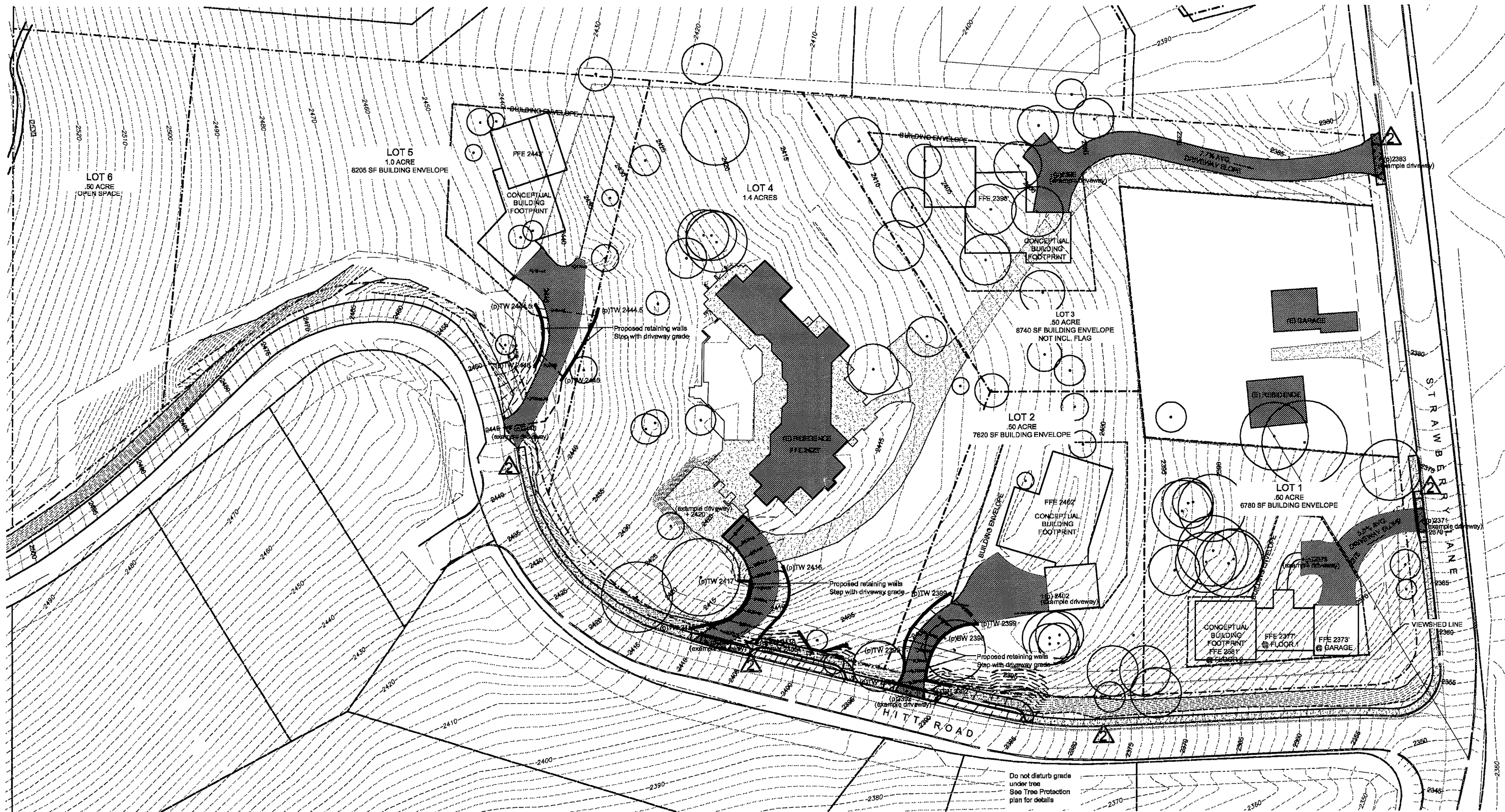
**Currently Managed Zone** - Vegetation in designated areas has been removed in accordance to the Standards for Wildfire Lands. Ongoing annual maintenance per plan, shall be required to continue compliance with the Fire Prevention and Control Plan.



**FIRE PREVENTION AND CONTROL PLAN /  
EROSION CONTROL PLAN**







# NOTES:

1. All trees shall be protected during the construction of the driveway approach and retaining walls. See Sheet L-1 for additional tree protection information.
2. Regrading under existing trees is not permitted unless supervised and approved by Certified Arborist.
3. All proposed retaining walls shall be a maximum of 4' in height.
4. All slopes disturbed by the driveway and retaining wall construction shall be revegetated. See Sheet L-3 for additional erosion control information.
5. Additional information shall be provided by the Landscape Architect, prior to layout and construction of driveway and retaining walls.
6. For erosion control measures during construction follow recommended erosion control provisions in Amrhein & Associates, Inc. Geotechnical Engineering Report dated 10-12-07. For erosion control hydroseeding requirements along Hitt Road follow provisions outlined on sheet D-1 by Construction Engineering Consultants. For post-grading erosion control seeding follow provisions outlined on sheet L-3 by Landscape Architect.
7. Revision #2 includes: proposed sidewalk along Hitt Road and at driveways on Strawberry Lane, street trees along Strawberry Lane seen on L-2, grade changes and landscape wall to accommodate on-street parking.



GRADING PLAN

LAURIE SAGER



McELIHAN PROPERTY

Revised  
March  
Based on  
City Code

Scale

McELIHAN PROPERTY

Feb

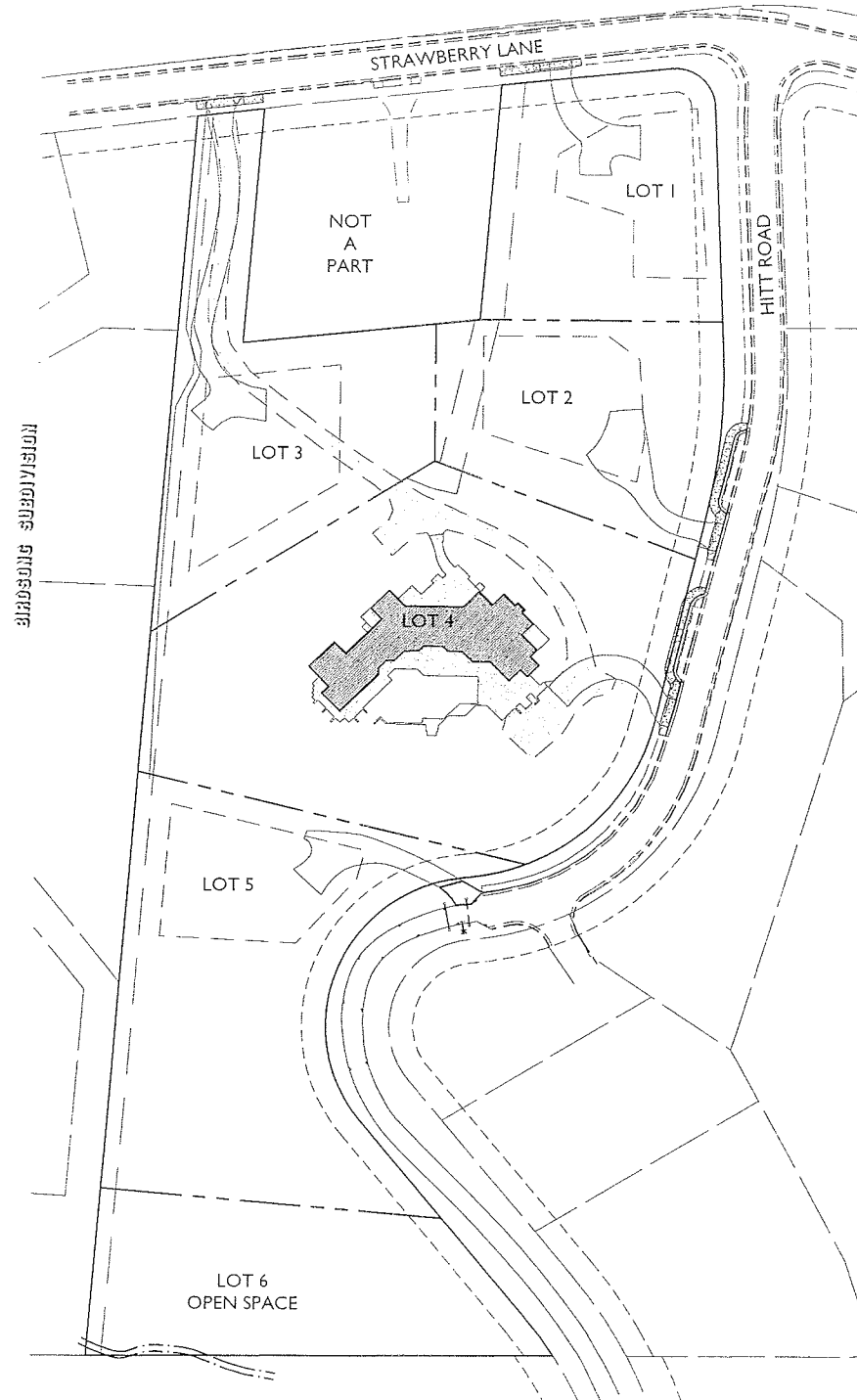
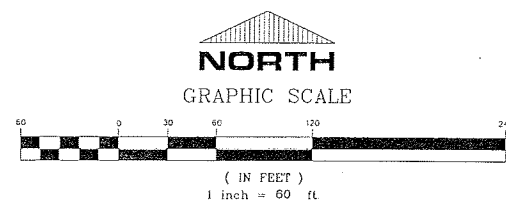
VICINITY MAP

## ABBREVIATIONS

B/W = BACK OF WALK  
C.I. = CURB INLET  
CR = CURB RETURN  
FG = FINISH GRADE  
GB = GRADE BREAK  
G.I. = GUTTER INLET - RD364  
G.C. = POINT OF CURVE  
PRC = POINT OF REVERSE CURVE  
PT = POINT OF TANGENT  
PV = PAVEMENT  
R/W = RIGHT-OF-WAY  
SD = STORM DRAIN  
SDMH = STORM DRAIN MANHOLE  
S.I. = STREET INTERSECTION  
SS = SANITARY SEWER  
SS C.O. = SANITARY SEWER CLEANOUT  
SSMH = SANITARY SEWER MANHOLE  
TC = TOP OF CURB  
VC = VERTICAL CURVE  
W = WATER

THE CONTRACTOR IS RESPONSIBLE TO MEET THE ALL THE REQUIREMENTS OF THE GEOTECHNICAL REPORT PREPARED BY THE PROJECT GEOTECHNICAL ENGINEER. A PRE-CONSTRUCTION MEETING WITH THE CONTRACTOR, C.E.C., AND THE GEOTECHNICAL ENGINEER IS REQUIRED PRIOR TO BEGINNING WORK

THE CONTRACTOR IS RESPONSIBLE TO OBTAIN A COPY OF THE TREE PROTECTION PLAN PREPARED BY THE PROJECT LANDSCAPE ARCHITECT. ALL TREE PROTECTION MEASURES SHALL BE IN PLACE PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. A PRE-CONSTRUCTION MEETING WITH THE LANDSCAPE ARCHITECT IS REQUIRED PRIOR TO CONSTRUCTION.



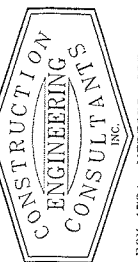
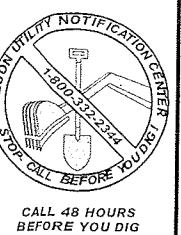
## SITE INDEX MAP

# CIVIL IMPROVEMENT PLANS FOR STRAWBERRY LANE SUBDIVISION

LOCATED IN  
NORTHEAST QUATER OF SECTION 8, T. 39 S, R. 1 E, W.M.  
CITY OF ASHLAND  
JACKSON COUNTY, OREGON

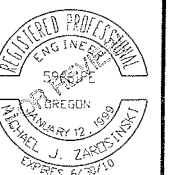
## GENERAL NOTES

- ALL WORK SHALL CONFORM TO THE CITY OF ASHLAND ENGINEERING DEPARTMENT SPECIFICATIONS FOR CONSTRUCTION, 2008' ODDT / APWA OREGON CHAPTER AND AS AMENDED BY THE CITY OF ASHLAND ENGINEERING DEPARTMENT.
2. STORM DRAIN PIPE SHALL BE AS INDICATED ON THE PROJECT PLANS. ONLY WHITE OR BLACK PIPE IS PERMITTED PER CITY STANDARDS. PRIVATE STORM DRAIN PIPE (4", 6" & 8") SHALL BE PVC CONFORMING TO ASTM D-3034. EACH SERVICE TERMINATION POINT SHALL BE MARKED WITH A PAINTED 2" x 4" AND STORM DRAIN DETECTOR TAPE SHALL BE USED.
3. SANITARY SEWER PIPE SHALL BE PVC (ASTM D-3034). SANITARY SEWER LATERALS SHALL BE CONNECTED TO THE MAIN WITH A ONE PIECE PREFABRICATED SEWER WYE AND SHALL EXTEND 2' BEYOND THE P.U.E. EACH SERVICE TERMINATION POINT SHALL BE MARKED WITH A PAINTED GREEN 2" x 4". SANITARY SEWER DETECTOR TAPE SHALL BE USED. ONLY GREEN PIPE ALLOWED PER CITY STANDARDS.
4. CLASS "B" BACKFILL SHALL BE USED IN ALL TRENCHES WITHIN THE RIGHT OF WAY AND SHALL BE COMPACTED TO 95% MAXIMUM DENSITY (PER CITY OF ASHLAND).
5. GRANULAR MATERIALS SHALL BE OBTAINED FROM A CITY APPROVED SOURCE. CONTRACTOR SHALL NOTIFY THE CITY ENGINEER OF THE MATERIAL SOURCE PRIOR TO ANY GRANULAR MATERIAL PLACEMENT. CONTRACTOR SHALL NOT CHANGE MATERIAL SOURCE WITHOUT PRIOR APPROVAL FROM THE CITY ENGINEER.
6. ALL STORM DRAIN CURB INLETS SHALL BE CITY OF ASHLAND TYPE 4A WITH A 4' CURB OPENING UNLESS OTHERWISE SPECIFIED ON THE PLANS.
7. ALL STORM DRAIN PIPES SHALL HAVE WATER-TIGHT JOINTS. H.D.P.E. PIPE JOINTS SHALL CONFORM TO ASTM D-3212 (TESTABLE JOINTS). TEES SHALL BE MANUFACTURED OR 'INSERT-A-TEE'.
8. ALL WATER PIPE SHALL BE PER CURRENT CITY OF ASHLAND STANDARDS, BE FULLY RESTRAINED AND VISIBLY RECOGNIZABLE AS SUCH (EBAA MEGALUG OR APPROVED EQUAL).
  - a. PIPE: AWWA 21.51 DUCTILE IRON, CLASS 50, FULLY RESTRAINED JOINT UNLESS NOTED OTHERWISE.
  - b. FITTINGS: AWWA C116 OR C153 WITH "EBAA MEGALUGS" OR APPROVED EQUAL. RESTRAIN ALL JOINTS UNLESS OTHERWISE NOTED ON PLANS.
  - c. THRUST BLOCKS: AT TAPPING TEES ONLY.
  - d. THE CITY SHALL INSTALL ALL WATER SERVICES INCLUDING THE TAP, SERVICE LINE & METER. THE CONTRACTOR SHALL INSTALL THE METER BOX, LID & ACCESSORIES.
  - e. THE OWNER SHALL ENTER INTO AN AGREEMENT WITH THE CITY TO STERILIZE ALL WATER SERVICES. COSTS FOR THIS WORK WILL BE BILLED BY THE CITY TO THE OWNER ON AN ACTUAL COST BASIS FOR LABOR, MATERIALS, EQUIPMENT RENTAL AND OVERHEAD.
  - f. CONTRACTOR SHALL FURNISH ENGINEER OF RECORD AND CITY WITH DATA ON ALL VALVES AND PIPE APPURTENANCES WITH THE AS-BUILTS.
9. SEPARATION OF SANITARY SEWER AND WATER MAINS SHALL BE IN ACCORDANCE WITH OREGON STATE HEALTH DIVISION RULES AND/OR AS MODIFIED BY THE CITY OF ASHLAND.
10. ALL GIVEN PIPE LENGTHS AND SLOPES ARE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
11. ALL WORK ON GRAVITY STORM DRAIN AND SANITARY SEWER SYSTEMS SHALL BEGIN AT THE LOWEST POINT AND PROCEED UPGRADE TO THE HIGHEST POINT. WORK SHALL NOT BEGIN ANYWHERE OTHER THAN THE LOWEST POINT. DO NOT LEAVE OUT SECTIONS OF THE SYSTEM AND RESTART AT A HIGHER POINT.
12. THE LOCATION AND ELEVATION OF ALL EXISTING UTILITIES AND STRUCTURES ARE APPROXIMATE. THERE MAY EXIST UTILITIES THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO CONSTRUCTION.
13. CONTRACTORS AND SUBCONTRACTORS SHALL BE PREQUALIFIED WITH THE CITY OF ASHLAND ENGINEERING DEPARTMENT PRIOR TO STARTING WORK. CONTRACTOR SHALL NOTIFY OREGON UTILITY NOTIFICATION CENTER (1-800-332-2344) AND THE CITY OF ASHLAND ENGINEERING (488-5347) 48 HOURS PRIOR TO STARTING WORK.
14. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH THE VARIOUS UTILITY COMPANIES REGARDING INSTALLATION AND/OR RELOCATION REQUIREMENTS FOR THEIR FACILITIES.
15. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS PRIOR TO CONSTRUCTION.
16. STABILIZATION FABRIC SHALL BE WOVEN AND CONFORM TO 1991 OSHD PLACEMENT SECTION 02320 AND CONFORM TO 1991 OSHD SECTION 350.
17. POWER FACILITIES SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY. SEE POWER PLANS PREPARED BY CITY OF ASHLAND ELECTRIC DEPARTMENT FOR DETAILED PLANS & SPECIFICATIONS.
18. THE BASIS OF VERTICAL CONTROL IS CITY OF ASHLAND G.P.S. CONTROL NETWORK STATION 525, A BRASS CAP IN A MONUMENT WELL LOCATED AT THE CENTERLINE INTERSECTION OF WIMER STREET AND WRIGHTS CREEK ROAD. BENCHMARK ELEVATION = 2206.49', BASED ON THE NATIONAL GEODETIC VERTICAL DATUM OF 1929, ADJUSTED IN 1956 (NGVD 29/56).



P.O. BOX 1724 • MEDFORD, OREGON 97501  
PH. (541) 779-5268 • FAX (541) 779-3139

CHECKED BY MRK. PIRH	DATE 3/09
AGENCY REVIEW	DATE
AGENCY REVIEW	DATE
APPROVED	DATE
APPROVED	DATE













## CONTACT INFORMATION

<b>ENGINEER OF RECORD:</b> PATRICK W HAYRD, P.E. C.E.C., INC. 132 W. MAIN ST., SUITE 201 MEDFORD, OR 97501 PH: (541) 779-5268 FX: (541) 779-3139	<b>CITY/ASHLAND PUBLIC WORKS:</b> KARL JOHNSON ENGINEERING DIVISION 51 WINGBURY WAY ASHLAND, OR 97520 PH: (541) 488-5347 FX: (541) 489-6006	<b>CABLE COMPANY:</b> CHARTER COMMUNICATIONS BRAD DILL 926 S GRAPE STREET MEDFORD, OR 97501 PH: (541) 282-8672 FX: (541) 772-4625
<b>SURVEYOR OF RECORD:</b> SHAWN KAMPMANN POLARIS LAND SURVEYING P.O. BOX 459 ASHLAND, OR 97520 PH: (541) 482-5009 FX: (541) 488-0797	<b>ELECTRIC/AFN:</b> DAVE TYGSON CITY OF ASHLAND 90 NORTH MOUNTAIN ASHLAND, OR 97520 PH: (541) 488-5357 FX: (541) 552-2436	<b>GAS COMPANY:</b> DENNIS OTT AUSTIA UTILITIES 580 BUSINESS PARK DR MEDFORD, OR 97504 PH: (541) 858-4740 FX: (541) 858-4790
<b>DEVELOPER/OWNER:</b> ROBERT & LAURA MCLELLAN 500 STRAWBERRY LANE ASHLAND, OR 97520 PH: (541) 821-4071	<b>GEOTECHNICAL ENGINEER:</b> MARK AMRHEIN AMRHEIN ASSOCIATES 706 JEFFERSON AVENUE ASHLAND, OR 97520 PH: (541) 482-6680	<b>TELEPHONE COMPANY:</b> JIM MARTIN OWEST 132 W. 4th STREET MEDFORD, OR 97501 PH: (541) 776-8268 FX: (541) 776-8026

## LEGEND

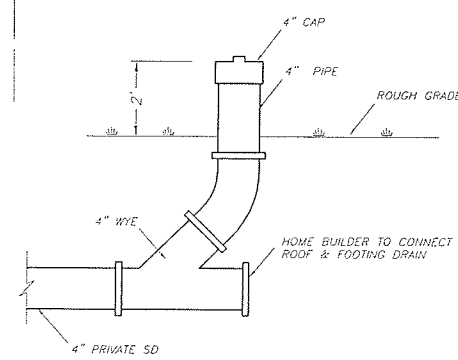
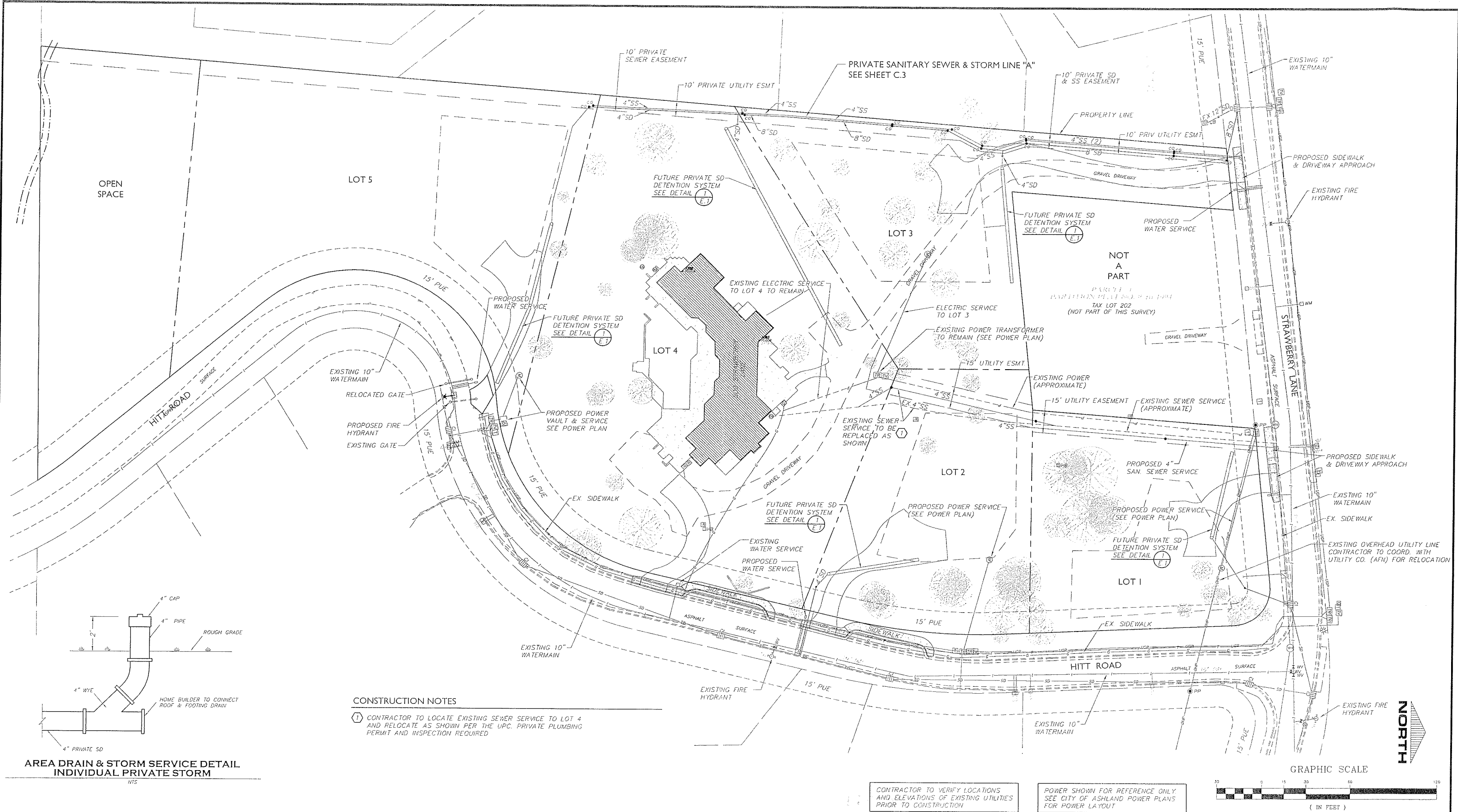
LINES		EXISTING		PROPOSED	
PROPOSED PAVMENT	EX SD	EXISTING STORM			
PROPOSED SIDEWALK	EX SS	EXISTING SEWER			
XX" SD	EX W	EXISTING WATER			
XX" SS	EX G	EXISTING GAS			
XX" W	EX P	EXISTING POWER			
P/W	EX T	EXISTING PHONE			
PROPERTY LINE	EX TV	EXISTING TV			
CENTER LINE		EXISTING PAVMENT			
PUE		EXISTING C & G			
PROPOSED C & G		EXISTING SIDEWALK			
PROPOSED DITCH		EXISTING DITCH			
	X	EXISTING FENCE			

## SYMBOLS

MANHOLE		AREA DRAIN
CLEAN-OUT		PIPE PLUG
SERVICE WYE		CATCH/LYNCH BASIN
WATER SERVICE		UTILITY POLE
FIRE HYDRANT		STREET LIGHT
WATER VALVE		TELEPHONE PEDESTAL
FITTING & TEE		POWER TRANSFORMER
CUT-IN SLEEVE		POWER SWITCHGEAR
AIR VALVE		GAS VALVE
CURB INLET		

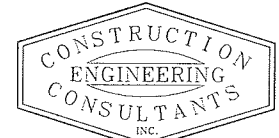
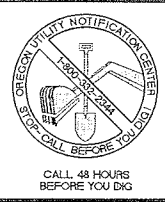
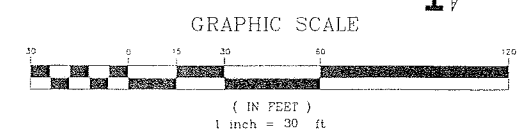
## SHEET INDEX

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**CONSTRUCTION NOTES**  
① CONTRACTOR TO LOCATE EXISTING SEWER SERVICE TO LOT 4 AND RELOCATE AS SHOWN PER THE UPC. PRIVATE PLUMBING PERMIT AND INSPECTION REQUIRED

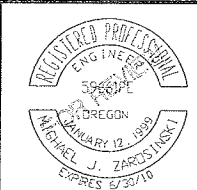
CONTRACTOR TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION  
POWER SHOWN FOR REFERENCE ONLY. SEE CITY OF ASHLAND POWER PLANS FOR POWER LAYOUT



P.O. BOX 1724 • MEDFORD, OREGON 97501  
PH. (541) 779-5268 • FAX (541) 779-3139

DRAWN BY: DLG	DATE: 12/07
CHECKED BY: MWK, PWH	DATE: 12/07
AGENCY ENGINEER:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:

NO	REVISION	DATE	BY



**CITY OF ASHLAND**  
STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS  
UTILITY PLAN

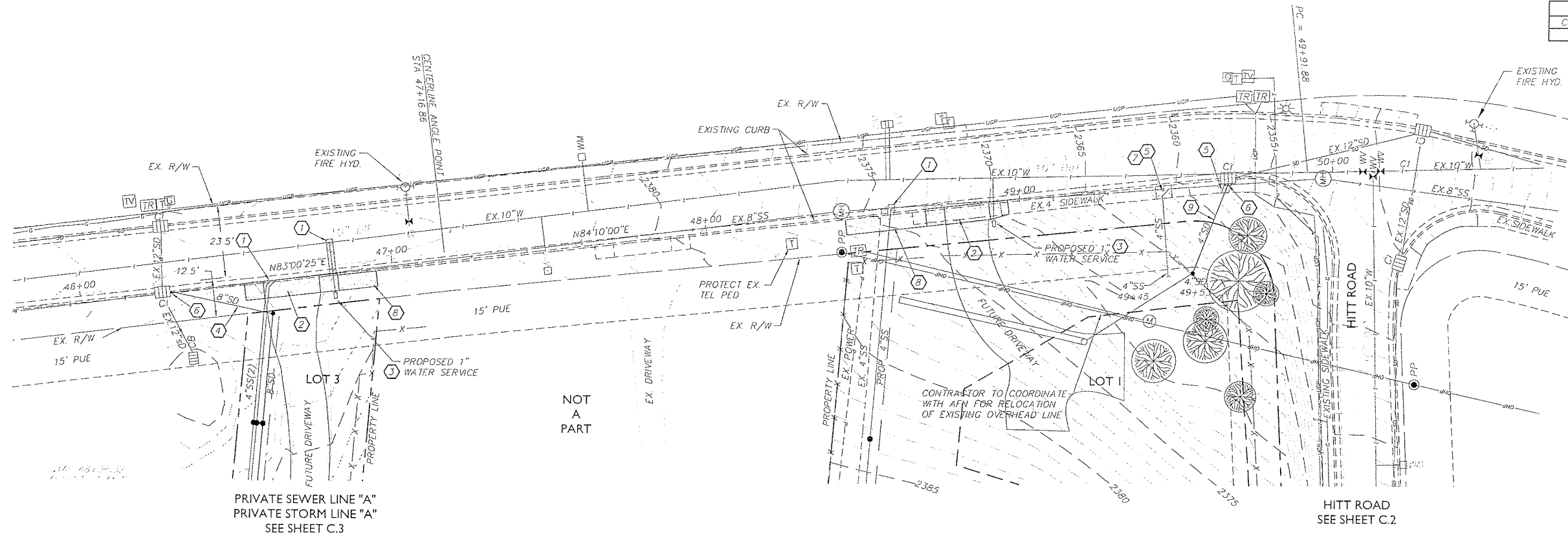
PROJECT NO.  
  
DRAWING NO.  
**B.1**

REV. CITY ENGINEER: \_\_\_\_\_ DATE: \_\_\_\_\_

POWER SHOWN FOR REFERENCE ONLY.  
SEE CITY OF ASHLAND POWER PLANS  
FOR POWER LAYOUT.

CURVE TABLE					
CURVE	LENGTH	RADIUS	DELTA	TANGENT	CHORD
C1	87.12	223.50	22°20'00"	44.12	86.57

WATER METER TABLE			
LOTS	STATION	OFFSET	DESCRIPTION
1	48+91.3	6.35' RT	1" SINGLE
3	46+81.1	8.82' RT	1" SINGLE



## STRAWBERRY LANE

### CONSTRUCTION NOTES

- 1 SAWCUT, REMOVE & REPLACE EXIST. PAVEMENT AS REQ'D FOR UTIL. CONST. PER CITY STD. DTL. CD302. SLURRY TRENCH BACKFILL.
- 2 CONSTRUCT 18' DRIVEWAY APPROACH PER COA STD. DTL. CD740
- 3 INSTALL 1" WATER METER/SERVICE PER COA STD.
- 4 INSTALL 8" PRIVATE COMMON STORM SERVICE PER COA STD. & UPC
- 5 SAWCUT, REMOVE & REPLACE EXIST. SIDEWALK AS REQ'D FOR UTIL. CONST. PER CITY STANDARDS
- 6 CORE DRILL EXISTING INLET AND INSTALL 8" STORM LINE PER CITY STANDARDS
- 7 CONNECT TO EXISTING 8" SANITARY SEWER MAIN W/ 4" SERVICE PER CITY STANDARDS
- 8 CONSTRUCT 5' SIDEWALK PER COA STD.
- 9 INSTALL 4" PRIVATE STORM SERVICE PER COA STD. & UPC

RECEIVED

MAR 10 2009

City of Ashland  
Engineering Department

CONTRACTOR TO VERIFY LOCATION  
& ELEVATION OF EXIST. UTILITIES  
PRIOR TO CONSTRUCTION

NORTH

GRAPHIC SCALE



( IN FEET )  
1 inch = 20 ft



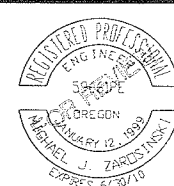
CALL 48 HOURS  
BEFORE YOU DIG



P.O. BOX 1724 • MEDFORD, OREGON 97501  
PH. (541) 779-5268 • FAX (541) 779-3139

DRAWN BY: MWK	DATE: 3/09
CHECKED BY: MWK, PWH	DATE: 3/09
AGENCY ENGINEER:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:

NO.	REVISION	DATE	BY



CITY OF ASHLAND  
STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS  
STRAWBERRY LANE

PROJECT NO.

DRAWING NO.

C.1

REV. CITY ENGINEER:

DATE



POWER SHOWN FOR REFERENCE ONLY  
SEE CITY OF ASHLAND POWER PLANS  
FOR POWER LAYOUT.

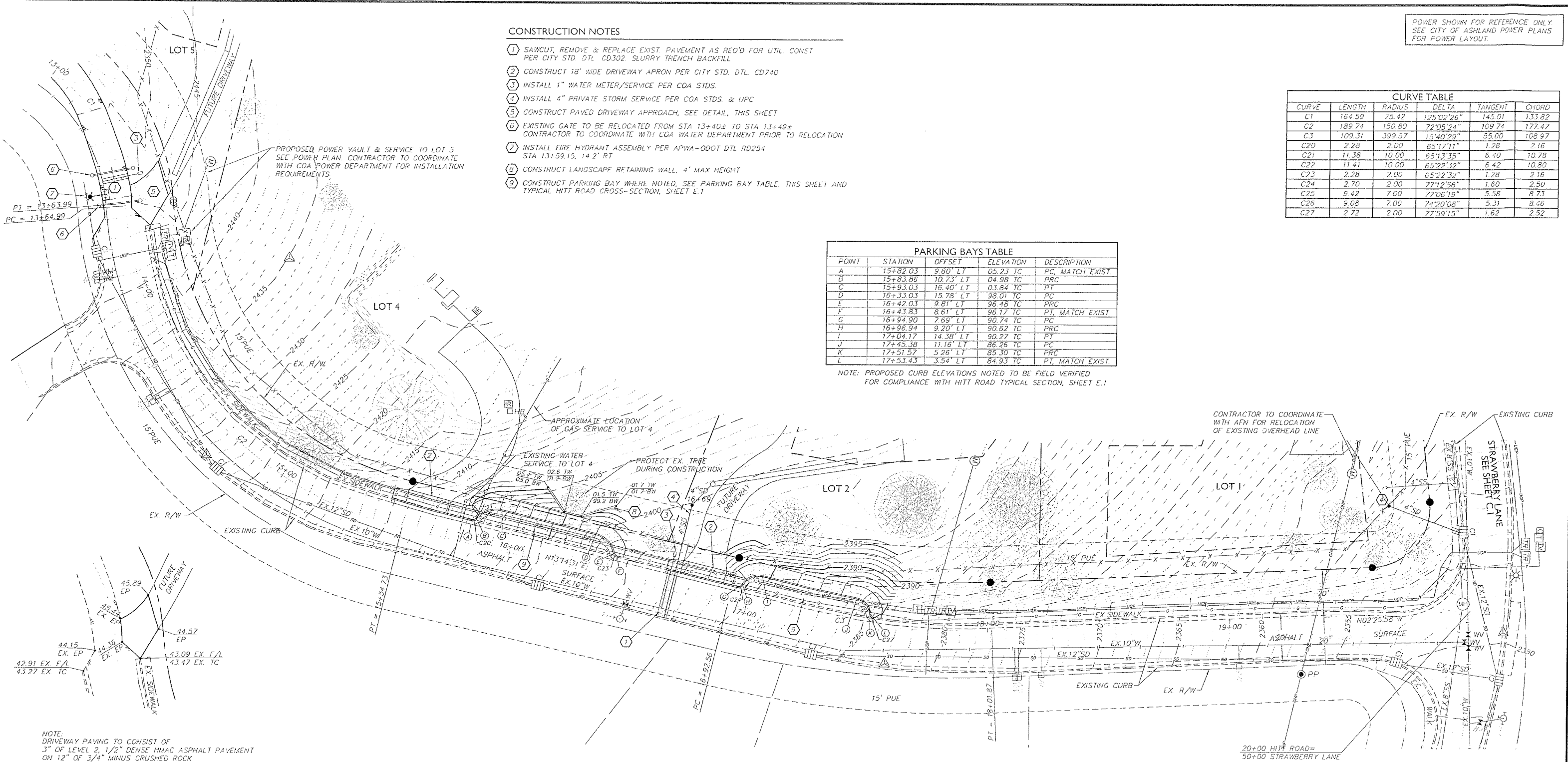
### CONSTRUCTION NOTES

- 1 SAWCUT, REMOVE & REPLACE EXIST. PAVEMENT AS REQ'D FOR UTIL. CONST. PER CITY STD. DTL CD302. SLURRY TRENCH BACKFILL.
- 2 CONSTRUCT 18' WIDE DRIVEWAY APRON PER CITY STD. DTL CD740.
- 3 INSTALL 1" WATER METER/SERVICE PER COA STDS.
- 4 INSTALL 4" PRIVATE STORM SERVICE PER COA STDS. & UPC.
- 5 CONSTRUCT PAVED DRIVEWAY APPROACH, SEE DETAIL, THIS SHEET.
- 6 EXISTING GATE TO BE RELOCATED FROM STA 13+40± TO STA 13+49±. CONTRACTOR TO COORDINATE WITH COA WATER DEPARTMENT PRIOR TO RELOCATION.
- 7 INSTALL FIRE HYDRANT ASSEMBLY PER APWA-ODOT DTL RD254 STA 13+59.15, 14.2' RT.
- 8 CONSTRUCT LANDSCAPE RETAINING WALL, 4' MAX HEIGHT.
- 9 CONSTRUCT PARKING BAY WHERE NOTED, SEE PARKING BAY TABLE, THIS SHEET AND TYPICAL HITT ROAD CROSS-SECTION, SHEET E.1.

CURVE TABLE					
CURVE	LENGTH	RADIUS	DELTA	TANGENT	CHORD
C1	164.59	75.42	125°02'26"	145.01	133.82
C2	189.74	150.80	72°05'24"	109.74	177.47
C3	109.31	399.57	15°40'29"	55.00	108.97
C20	2.28	2.00	65°17'11"	1.28	2.16
C21	11.38	10.00	65°13'35"	6.40	10.78
C22	11.41	10.00	65°22'32"	6.42	10.80
C23	2.28	2.00	65°22'32"	1.28	2.16
C24	2.70	2.00	77°12'56"	1.60	2.50
C25	9.42	7.00	77°06'19"	5.58	8.73
C26	9.08	7.00	74°20'08"	5.31	8.46
C27	2.72	2.00	77°59'15"	1.62	2.52

PARKING BAYS TABLE				
POINT	STATION	OFFSET	ELEVATION	DESCRIPTION
A	15+82.03	9.60' LT	05.23 TC	PC, MATCH EXIST.
B	15+83.86	10.73' LT	04.98 TC	PRC
C	15+93.03	16.40' LT	03.84 TC	PT
D	16+33.03	15.78' LT	98.01 TC	PC
E	16+42.03	9.81' LT	96.48 TC	PRC
F	16+43.83	8.61' LT	96.17 TC	PT, MATCH EXIST.
G	16+94.90	7.69' LT	90.74 TC	PC
H	16+96.94	9.20' LT	90.62 TC	PRC
I	17+04.17	14.38' LT	90.27 TC	PT
J	17+45.38	11.16' LT	86.26 TC	PC
K	17+51.57	5.26' LT	85.30 TC	PRC
L	17+53.43	3.54' LT	84.93 TC	PT, MATCH EXIST.

NOTE: PROPOSED CURB ELEVATIONS NOTED TO BE FIELD VERIFIED FOR COMPLIANCE WITH HITT ROAD TYPICAL SECTION, SHEET E.1.



NOTE:  
DRIVEWAY PAVING TO CONSIST OF  
3" OF LEVEL 2, 1/2" DENSE HMA/AC ASPHALT PAVEMENT  
ON 12" OF 3/4" MINUS CRUSHED ROCK  
ON WOVEN STABILIZATION FABRIC

### LOT 5 DRIVEWAY APPROACH

SCALE: 1"=20'

WATER METER TABLE			
LOTS	STATION	OFFSET	DESCRIPTION
2	16+66.71	14.4' LT	1" SINGLE
5	13+44.77	7' LT	1" SINGLE

## HITT ROAD

MAR 12 2009

City of Ashland  
Community Development

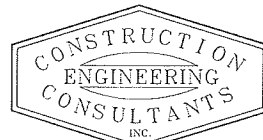
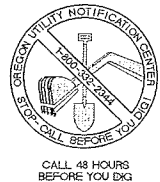
CONTRACTOR TO VERIFY LOCATION  
& ELEVATION OF EXIST. UTILITIES  
PRIOR TO CONSTRUCTION.



### GRAPHIC SCALE



( IN FEET )  
1 inch = 20 ft



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DRAWN BY: MWK	DATE: 3/09
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AGENCY ENGINEER:	DATE:
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NO.	REVISION	DATE	BY



CITY OF ASHLAND  
STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS  
HITT ROAD

PROJECT NO.

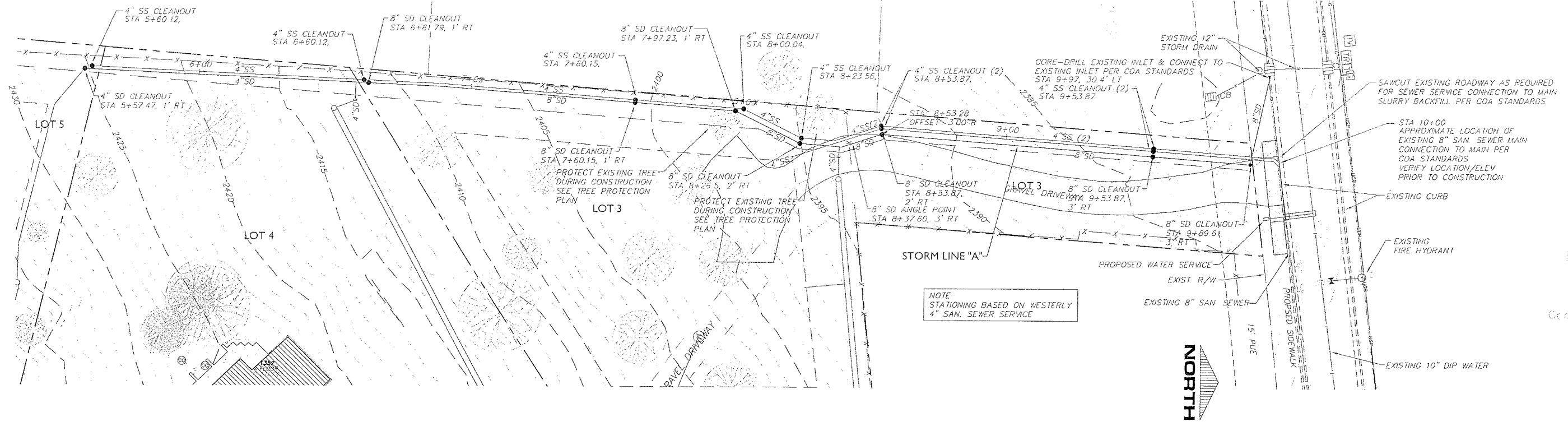
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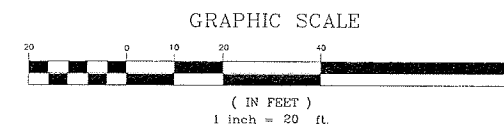
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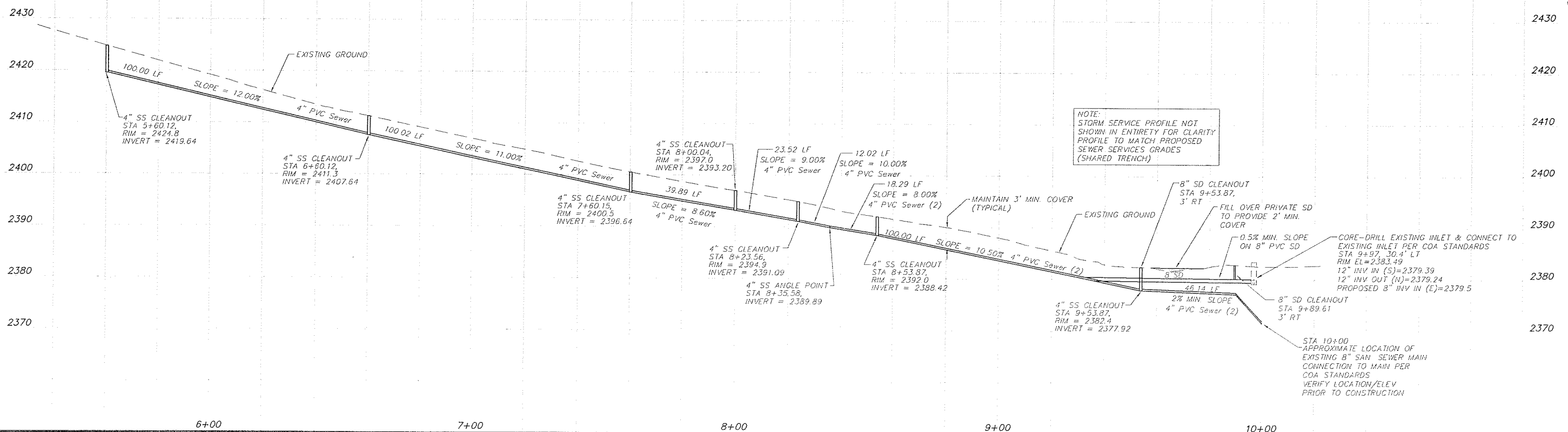
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# SANITARY SEWER LINE "A" & STORM LINE "A" (PRIVATE)



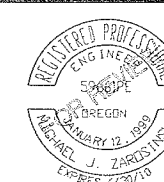
CONTRACTOR TO VERIFY LOCATION  
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AGENCY ENGINEER:	DATE:
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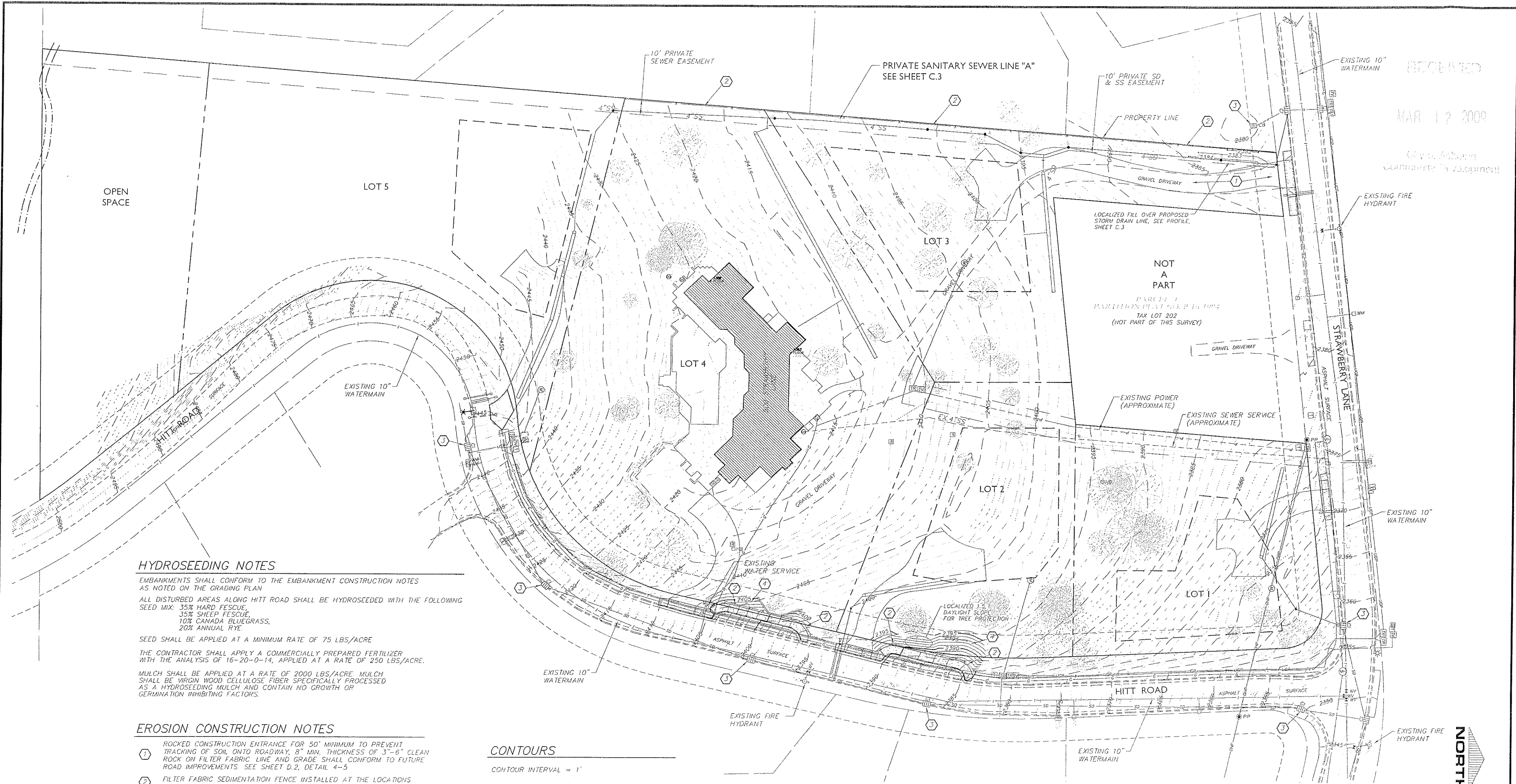


CITY OF ASHLAND	
STRAWBERRY LANE SUBDIVISION CIVIL IMPROVEMENTS	PROJECT NO. -
PRIVATE SANITARY SEWER LINE "A" PRIVATE STORM DRAIN LINE "A"	DRAWING NO. C.3
REV. CITY ENGINEER.	DATE

RECEIVED

MAR 12 2009

CITY OF ASHLAND  
COMMUNITY DEVELOPMENT



**HYDROSEEDING NOTES**

EMBANKMENTS SHALL CONFORM TO THE EMBANKMENT CONSTRUCTION NOTES AS NOTED ON THE GRADING PLAN

ALL DISTURBED AREAS ALONG HITT ROAD SHALL BE HYDROSEEDING WITH THE FOLLOWING SEED MIX: 35% HARD FESCUE, 35% SHEEP FESCUE, 10% CANADA BLUEGRASS, 20% ANNUAL RYE.

SEED SHALL BE APPLIED AT A MINIMUM RATE OF 75 LBS/ACRE

THE CONTRACTOR SHALL APPLY A COMMERCIALLY PREPARED FERTILIZER WITH THE ANALYSIS OF 16-20-0-14, APPLIED AT A RATE OF 250 LBS/ACRE.

MULCH SHALL BE APPLIED AT A RATE OF 2000 LBS/ACRE. MULCH SHALL BE VIRGIN WOOD CELLULOSE FIBER SPECIFICALLY PROCESSED AS A HYDROSEEDING MULCH AND CONTAIN NO GROWTH OR GERMINATION INHIBITING FACTORS.

**EROSION CONSTRUCTION NOTES**

- 1 ROCKED CONSTRUCTION ENTRANCE FOR 50' MINIMUM TO PREVENT TRACKING OF SOIL ONTO ROADWAY, 8" MIN. THICKNESS OF 3"-6" CLEAN ROCK ON FILTER FABRIC LINE AND GRADE SHALL CONFORM TO FUTURE ROAD IMPROVEMENTS. SEE SHEET D.2, DETAIL 4-5
- 2 FILTER FABRIC SEDIMENTATION FENCE INSTALLED AT THE LOCATIONS SHOWN. SEE SHEET D.2, DETAIL 4-16
- 3 PROPOSED STORM DRAIN INLETS SHALL BE PROTECTED BY FILTER FABRIC INLET BARRIERS. SEE SHEET D.2, DETAIL 4-24
- 4 CUT SLOPES OVER 3:1 SHALL BE COVERED WITH A PERMANENT EROSION CONTROL MATTING, INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS

**CONTOURS**

CONTOUR INTERVAL = 1'

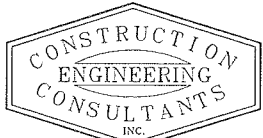
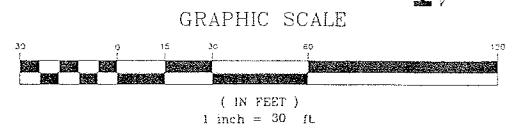
INDEX INTERVAL = 5'

EXISTING GROUND \_\_\_\_\_

FINISHED GROUND \_\_\_\_\_ ELEV \_\_\_\_\_

THE CONTRACTOR IS RESPONSIBLE TO MEET THE ALL THE REQUIREMENTS OF THE GEOTECHNICAL REPORT PREPARED BY THE PROJECT GEOTECHNICAL ENGINEER. A PRE-CONSTRUCTION MEETING WITH THE CONTRACTOR, C.E.C., AND THE GEOTECHNICAL ENGINEER IS REQUIRED PRIOR TO BEGINNING WORK.

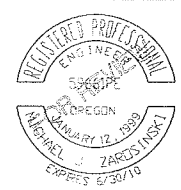
POWER SHOWN FOR REFERENCE ONLY. SEE CITY OF ASHLAND POWER PLANS FOR POWER LAYOUT.



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**CITY OF ASHLAND**

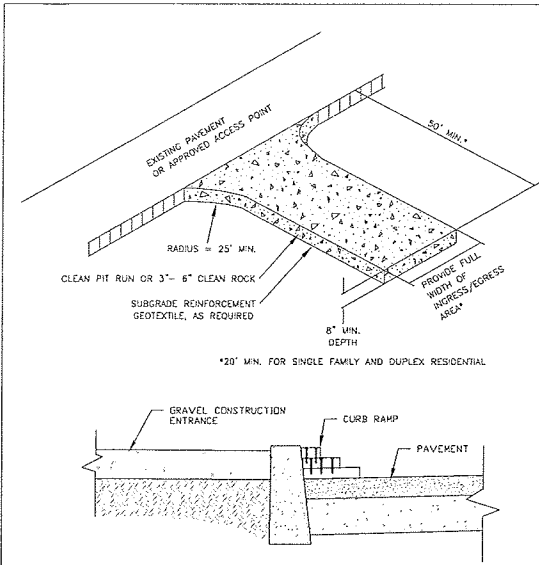
STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS

GRADING PLAN  
EROSION & SEDIMENT CONTROL PLAN

REV. CITY ENGINEER: \_\_\_\_\_ DATE \_\_\_\_\_

PROJECT NO. \_\_\_\_\_

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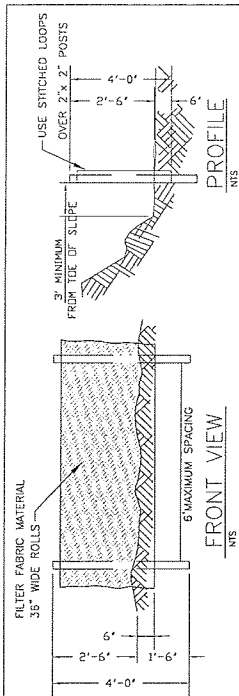


NOTES:  
DIMENSIONS  
SINGLE FAMILY  
20' LONG BY 20' WIDE  
8\"/>

CONSTRUCTION ENTRANCE

Detail Drawing 4-5

Page 4-42



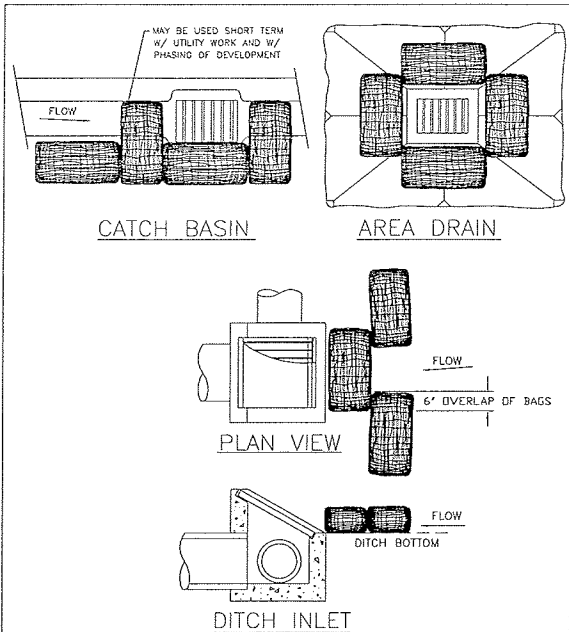
NOTES:

1. BURY BOTTOM OF FILTER FABRIC 4\"/>

SEDIMENT FENCE

Detail Drawing 4-16

Page 4-81



- NOTES:  
1. ADDITIONAL MEASURES MUST BE CONSIDERED DEPENDING ON SOIL TYPES.  
2. BIOFILTER BAGS SHOULD BE STAKED WHERE APPLICABLE USING (2) 1"x2" WOODEN STAKES OR APPROVED EQUAL PER BAG.

INLET PROTECTION  
TYPE 4

Detail Drawing 4-24

Page 4-109

## EROSION & SEDIMENT CONTROL STANDARD NOTES

1. HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS. (SCHEDULE A.5.B.I.(3))

2. THE ESCP MUST BE KEPT ONSITE AND ALL EROSION AND SEDIMENT CONTROL MEASURES SHOWN ON THE PLAN MUST BE INSTALLED IN SUCH A MANNER TO ENSURE THAT SEDIMENT OR SEDIMENT LADEN WATER THAT ENTERS OR IS LIKELY TO ENTER SURFACE WATERS OR CONVEYANCE SYSTEMS LEADING TO SURFACE WATER, ROADWAY, OR OTHER PROPERTIES DOES NOT OCCUR. (SCHEDULE A.3.A.) AND (SCHEDULE B.3.B.)

3. THE IMPLEMENTATION OF THE ESCP AND CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THE EROSION AND SEDIMENT CONTROL MEASURES IS THE RESPONSIBILITY OF THE PERMIT REGISTRANT UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED BY THE LOCAL DEVELOPMENT AGENCY AND VEGETATION/LANDSCAPING IS ESTABLISHED. THE PERMIT REGISTRANT SHALL BE RESPONSIBLE FOR MAINTENANCE AFTER THE LOTS ARE APPROVED, UNTIL THE LOTS ARE SOLD AND THE 1200-C PERMIT IS TERMINATED. (SCHEDULE A.4.A.) AND (SCHEDULE D.3.)

4. THE PERMIT REGISTRANT MUST BE RESPONSIBLE FOR PROPER INSTALLATION AND MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES, IN ACCORDANCE WITH LOCAL, STATE, OR FEDERAL REGULATIONS. (SCHEDULE A.5.A.) AND (SCHEDULE A. 6.A.)

5. EROSION AND SEDIMENT CONTROL MEASURES INCLUDING PERIMETER SEDIMENT CONTROL MUST BE IN PLACE BEFORE VEGETATION IS DISTURBED AND MUST REMAIN IN PLACE AND BE MAINTAINED, REPAIRED, AND PROMPTLY IMPLEMENTED FOLLOWING PROCEDURES ESTABLISHED FOR THE DURATION OF CONSTRUCTION, INCLUDING PROTECTION FOR ACTIVE STORM DRAIN INLETS AND CATCH BASINS AND APPROPRIATE NON-STORMWATER POLLUTION CONTROLS. (SCHEDULE A.5.B.II.(2)), (SCHEDULE A.5.B.II.(7)), (SCHEDULE A.7.D.I.(2)) & (SCHEDULE A.7.F.)

6. BEGIN LAND CLEARING, EXCAVATION, TRENCHING, CUTTING OR GRADING AND EARTHWORK-SURFACE ROUGHING AFTER INSTALLING APPLICABLE SEDIMENT, EROSION PREVENTION AND RUNOFF CONTROL MEASURES NOT IN THE DIRECT PATH OF WORK. (SCHEDULE A.5.B.II.(5)(A)), (SCHEDULE A.7.C.I.(1)) AND (SCHEDULE A.7.C.II.(1))

7. APPLY TEMPORARY AND/OR PERMANENT SOIL STABILIZATION MEASURES IMMEDIATELY ON ALL DISTURBED AREAS AS GRADING PROGRESSES AND FOR ALL ROADWAYS INCLUDING GRAVEL ROADWAYS. (SCHEDULE A.5.B.II.(5)(B), (SCHEDULE A.5.B.II.(5)(C) & SCHEDULE A.5.B.II.(6))

8. WET WEATHER BMPs: CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND ON SLOPES GREATER THAN FIVE (5) PERCENT FROM OCTOBER 1 THROUGH MAY 31 EACH YEAR. (SCHEDULE A.7.A.I.)

9. WET WEATHER BMPs: TEMPORARY STABILIZATION OF THE SITE MUST BE INSTALLED AT THE END OF THE SHIFT BEFORE A HOLIDAY OR WEEKEND OR AT THE END OF EACH WORKDAY IF RAINFALL IS FORECAST IN THE NEXT 24 HOURS AND EACH WEEKEND AND HOLIDAY. (SCHEDULE A.7.A.II.)

10. IDENTIFY, MARK, AND PROTECT (BY FENCING OFF OR OTHER MEANS) CRITICAL RIPARIAN AREAS AND VEGETATION INCLUDING IMPORTANT TREES AND ASSOCIATED ROOTING ZONES AND VEGETATION AREAS TO BE PRESERVED. IDENTIFY VEGETATIVE BUFFER ZONES BETWEEN THE SITE AND SENSITIVE AREAS (E.G., WETLANDS), AND OTHER AREAS TO BE PRESERVED, ESPECIALLY IN PERIMETER AREAS. PRESERVE EXISTING VEGETATION AND RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION. (SCHEDULE A.5.B.I.(1) & (2)) AND (SCHEDULE A.7.C.III.(1))

11. PROVIDE PERMANENT EROSION PREVENTION MEASURES ON ALL EXPOSED AREAS TO PREVENT FROM BECOMING A SOURCE OF EROSION AND REMOVE ALL TEMPORARY CONTROL MEASURES, UNLESS LOCAL ORDINANCES REQUIRE OTHERWISE, AS AREAS ARE STABILIZED. (SCHEDULE A.5.B.II.(8)) AND (SCHEDULE A.7.C.II.(2))

12. ALL TEMPORARY SEDIMENT CONTROLS MUST REMAIN IN PLACE UNTIL PERMANENT VEGETATION OR OTHER PERMANENT COVERING OF EXPOSED SOIL IS ESTABLISHED. IDENTIFY THE TYPE OF VEGETATIVE SEED MIX USED. (SCHEDULE A.7.C.III.(3)) & (SCHEDULE A.7.C.III.(4))

13. SEDIMENT CONTROLS MUST BE INSTALLED AND MAINTAINED ALONG THE SITE PERIMETER ON ALL DOWN GRADIENT SIDES OF THE CONSTRUCTION SITE AND AT ALL ACTIVE AND OPERATIONAL INTERNAL STORM DRAIN INLETS AT ALL TIMES DURING CONSTRUCTION. (SCHEDULE A.7.D.I.(1) - (2))

14. PRIOR TO ANY LAND DISTURBING ACTIVITIES EACH SITE MUST HAVE GRAVELED, PAVED, OR CONSTRUCTED ENTRANCES, EXITS AND PARKING AREAS WITH EXIT TIRE WASH TO REDUCE THE TRACKING OF SEDIMENT ONTO PUBLIC OR PRIVATE ROADS. (SCHEDULE A.7.D.III.(1))

15. WHEN TRUCKING SATURATED SOILS FROM THE SITE, EITHER WATER-TIGHT TRUCKS MUST BE USED OR LOADS MUST BE DRAINED ON-SITE UNTIL DRIPPING HAS BEEN REDUCED TO MINIMIZE SPILLAGE ON ROADS. (SCHEDULE A.7.D.III.(3))

16. TEMPORARY STABILIZATION OR COVERING OF SOIL STOCKPILES AND PROTECTION OF STOCKPILE LOCATED AWAY FROM CONSTRUCTION ACTIVITY MUST OCCUR AT THE END OF EACH WORKDAY OR OTHER BMPs, SUCH AS DIVERSION OF UNCONTAMINATED FLOWS AND INSTALLATION OF SEDIMENT FENCES AROUND STOCKPILES, MUST BE IMPLEMENTED TO PREVENT TURBID DISCHARGES TO SURFACE WATERS. (SCHEDULE A.7.E.I.(1)) & (SCHEDULE A.7.E.II.(1) - (3))

16. TEMPORARY STABILIZATION OR COVERING OF SOIL STOCKPILES AND PROTECTION OF STOCKPILE LOCATED AWAY FROM CONSTRUCTION ACTIVITY MUST OCCUR AT THE END OF EACH WORKDAY OR OTHER BMPs, SUCH AS DIVERSION OF UNCONTAMINATED FLOWS AND INSTALLATION OF SEDIMENT FENCES AROUND STOCKPILES, MUST BE IMPLEMENTED TO PREVENT TURBID DISCHARGES TO SURFACE WATERS. (SCHEDULE A.7.E.I.(1)) & (SCHEDULE A.7.E.II.(1) - (3))

17. BMPs THAT WILL BE USED TO PREVENT OR MINIMIZE STORMWATER FROM BEING EXPOSED TO POLLUTANTS FROM SPILLS, NO DISCHARGE OF CONCRETE TRUCK WASH WATER, VEHICLE AND EQUIPMENT CLEANING, VEHICLE AND EQUIPMENT FUELING, MAINTENANCE, AND STORAGE, OTHER CLEANING AND MAINTENANCE ACTIVITIES, AND WASTE HANDLING ACTIVITIES. THESE POLLUTANTS INCLUDE FUEL, HYDRAULIC FLUID, AND OTHER OILS FROM VEHICLES AND MACHINERY, AS WELL AS DEBRIS, LEFTOVER PAINTS, SOLVENTS, AND GLUES FROM CONSTRUCTION OPERATIONS. (SCHEDULE A.7.E.I.(2))

18. ANY USE OF TOXIC OR OTHER HAZARDOUS MATERIALS MUST INCLUDE PROPER STORAGE, APPLICATION, AND DISPOSAL. (SCHEDULE A.7.E.III.(2))

19. SOLID WASTE AND HAZARDOUS MATERIALS MANAGEMENT. FOLLOW PROJECT WRITTEN SPILL PREVENTION AND RESPONSE PROCEDURES, EMPLOYEE TRAINING ON SPILL PREVENTION AND PROPER DISPOSAL PROCEDURES, REGULAR MAINTENANCE SCHEDULE FOR VEHICLES AND MACHINERY, AND MATERIAL DELIVERY AND STORAGE CONTROLS, TRAINING AND SIGNAGE, MATERIAL USE, COVERED STORAGE AREAS FOR WASTE AND SUPPLIES. (SCHEDULE A.7.E.III.(3))

20. THE PERMITTEE MUST PROPERLY MANAGE HAZARDOUS WASTES, USED OILS, CONTAMINATED SOILS, CONCRETE WASTE, SANITARY WASTE, LIQUID WASTE, OR OTHER TOXIC SUBSTANCES DISCOVERED OR GENERATED DURING CONSTRUCTION AND MEET ALL STATE AND FEDERAL REGULATIONS AND APPROVALS. (SCHEDULE A.7.E.III.(4))

21. THE ESCP MEASURES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE MEASURES MUST BE UPGRADED AS NEEDED TO COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL EROSION AND SEDIMENT CONTROL REGULATIONS. CHANGES TO THE ESCP MUST ALSO BE SUBMITTED IN THE FORM OF AN ACTION PLAN TO DEQ OR ITS AGENT FOR APPROVAL. (SCHEDULE A.7.F.)

22. SIGNIFICANT AMOUNTS OF SEDIMENT, WHICH LEAVES THE SITE, MUST BE CLEANED UP WITHIN 24 HOURS AND PLACED BACK ON THE SITE AND STABILIZED OR PROPERLY DISPOSED. THE CAUSE OF THE SEDIMENT RELEASE MUST BE FOUND AND PREVENTED FROM CAUSING A RECURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DIVISION OF STATE LANDS REQUIRED TIME FRAME. (SCHEDULE A.7.F.I.(1))

23. VACUUMING OR DRY SWEEPING MUST BE USED TO CLEAN-UP RELEASED SEDIMENT AND MUST NOT BE INTENTIONALLY WASHED INTO STORM SEWERS, DRAINAGE WAYS, OR WATER BODIES. (SCHEDULE A.7.F.I.(2))

24. THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS TO MINIMIZE NUTRIENT RELEASES TO SURFACE WATERS. TIME RELEASE FERTILIZERS SHOULD BE USED WITH CARE WITHIN ANY WATER WAY RIPARIAN ZONE. (SCHEDULE A.7.F.I.(3))

25. SEDIMENT MUST BE REMOVED FROM BEHIND A SEDIMENT FENCE WHEN IT HAS REACHED A HEIGHT OF 1/3 THE HEIGHT OF THE FENCE ABOVEGROUND AND BEFORE FENCE REMOVAL. (SCHEDULE A.7.F.II.(1))

26. SEDIMENT MUST BE REMOVED FROM BEHIND BIO BAGS AND OTHER BARRIERS IT HAS REACHED A HEIGHT OF TWO (2) INCHES AND BEFORE BMP REMOVAL. (SCHEDULE A.7.F.II.(2))

27. REMOVAL OF TRAPPED SEDIMENT IN A SEDIMENT BASIN OR SEDIMENT TRAP OR CATCH BASINS MUST OCCUR WHEN THE SEDIMENT RETENTION CAPACITY HAS BEEN REDUCED BY FIFTY (50)% AND AT COMPLETION OF PROJECT. (SCHEDULE A.7.F.II.(3) & (4))

28. DEQ MUST APPROVE OF ANY TREATMENT SYSTEM AND OPERATIONAL PLAN THAT MAY BE NECESSARY TO TREAT CONTAMINATED CONSTRUCTION DEWATERING OR SEDIMENT AND TURBIDITY IN STORMWATER RUNOFF. (SCHEDULE A.7.F.III.)

29. SHOULD ALL CONSTRUCTION ACTIVITIES CEASE FOR THIRTY DAYS OR MORE, THE ENTIRE SITE MUST BE TEMPORARILY STABILIZED USING VEGETATION OR A HEAVY MULCH LAYER, TEMPORARY SEEDING, OR OTHER METHOD. (SCHEDULE A.8.A.)

30. SHOULD CONSTRUCTION ACTIVITIES CEASE FOR FIFTEEN (15) DAYS OR MORE ON ANY SIGNIFICANT PORTION OF A CONSTRUCTION SITE TEMPORARY STABILIZATION IS REQUIRED FOR THAT PORTION OF THE SITE WITH STRAW, COMPOST, OR OTHER TACKIFIED COVERING THAT PREVENT SOIL OR WIND EROSION UNTIL WORK RESUMES ON THAT PORTION OF THE SITE. (SCHEDULE A.8.B.)

31. DAILY INSPECTIONS WHEN RAINFALL AND RUNOFF OCCURS OF THE BMPs AND DISCHARGE OUTFALLS MUST BE THE PROJECT ESCP INSPECTOR. THESE INSPECTIONS AND OBSERVATIONS MUST BE RECORDED IN A LOG THAT IS AVAILABLE ON SITE. (SCHEDULE A.6.B.I.) & (SCHEDULE B.1.B.(1))

32. BMPs MUST BE INSPECTED BEFORE, DURING, AND AFTER SIGNIFICANT STORM EVENTS. (SCHEDULE A.7.F.)

33. ALL ESCP CONTROLS AND PRACTICES MUST BE INSPECTED VISUALLY ONCE TO ENSURE THAT BMPs ARE IN WORKING ORDER PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY AND MUST BE INSPECTED VISUALLY ONCE EVERY TWO (2) WEEKS DURING INACTIVE PERIODS GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS. (SCHEDULE B.1.B.(2)-(3))

34. IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION DURING PERIODS WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER. (SCHEDULE B.1.B.(4))

## EMBANKMENT CONSTRUCTION NOTES

1. PRIOR TO THE CONSTRUCTION OF EMBANKMENTS, THE CONTRACTOR SHALL EXCAVATE UNSUITABLE FOUNDATION MATERIAL. BASEMENTS, TRENCHES, AND HOLES ENCOUNTERED WITHIN EMBANKMENT LIMITS SHALL BE FILLED WITH APPROVED MATERIAL. PRIOR TO BACKFILLING, THE CONTRACTOR SHALL BREAK CONCRETE FLOORS OF BASEMENTS AS DIRECTED. THE CONTRACTOR SHALL BREAK UP AND ROUGHEN THE GROUND SURFACE BEFORE EMBANKMENT MATERIAL IS PLACED. THE NATURAL GROUND UNDERLYING EMBANKMENTS SHALL BE COMPACTED TO THE DENSITY SPECIFIED FOR THE EMBANKMENT MATERIAL TO BE PLACED, AND TO THE DEPTH OF THE GRUBBING OR A MINIMUM OF 6 INCHES.

2. EMBANKMENT CONSTRUCTION SHALL INCLUDE PREPARATION OF THE AREAS UPON WHICH EMBANKMENTS ARE PLACED, THE PLACEMENT AND COMPACTION OF APPROVED EMBANKMENT MATERIAL AND FILLING OF HOLES, PITS, AND OTHER DEPRESSIONS WITHIN THE SUBDIVISION.

3. THE CONTRACTOR SHALL PLACE EMBANKMENTS AND FILLS IN HORIZONTAL LAYERS OF 8 INCHES MAXIMUM DEPTH AND COMPACT EACH LAYER TO THE DENSITY SPECIFIED.

4. EMBANKMENTS SHALL NOT BE CONSTRUCTED WHEN THE EMBANKMENT MATERIAL OR THE FOUNDATION ON WHICH THE EMBANKMENT WOULD BE PLACED IS FROZEN.

5. IMMEDIATELY PRIOR TO COMPLETION OF THE EARTHWORK, THE CONTRACTOR SHALL CLEAN THE ENTIRE WORK AREA OF DEBRIS AND FOREIGN MATTER.

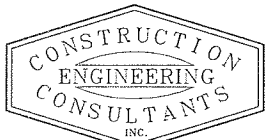
6. THE MAXIMUM DENSITY OF COMPACTED MATERIALS WILL BE DETERMINED BY AASHTO T 99.

7. THE CONTRACTOR SHALL COMPACT ALL EMBANKMENTS, FILLS, AND BACKFILLS TO A MINIMUM INPLACE DENSITY OF 95 PERCENT.

8. THE CONTRACTOR SHALL WATER THE MATERIALS TO PROVIDE OPTIMUM MOISTURE FOR COMPACTION OF EMBANKMENTS AND BACKFILLS. EMBANKMENT OR BACKFILL MATERIALS SHALL NOT BE PLACED IN FINAL POSITION UNTIL MOISTURE IN EXCESS OF OPTIMUM MOISTURE HAS BEEN REMOVED.

9. IF THE SPECIFIED COMPACTION IS NOT OBTAINED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER. THE CONTRACTOR MAY BE REQUIRED TO USE A MODIFIED COMPACTION PROCEDURE OR APPLY ADDITIONAL COMPACTION EFFORT. IF APPROVED MATERIALS MEETING THE SPECIFICATIONS CANNOT BE COMPACTED TO THE REQUIRED DENSITY REGARDLESS OF COMPACTION EFFORT OR METHOD, THE ENGINEER MAY REDUCE THE REQUIRED DENSITY OR DIRECT THAT ALTERNATE MATERIALS BE USED. IN NO CASE SHALL EARTHWORK OPERATIONS PROCEED UNTIL THE CONTRACTOR IS ABLE TO COMPACT THE MATERIAL TO THE SATISFACTION OF THE ENGINEER.

THE CONTRACTOR IS RESPONSIBLE TO MEET THE ALL THE REQUIREMENTS OF THE GEOTECHNICAL REPORT PREPARED BY THE PROJECT GEOTECHNICAL ENGINEER. A PRE-CONSTRUCTION MEETING WITH THE CONTRACTOR, C.E.C., AND THE GEOTECHNICAL ENGINEER IS REQUIRED PRIOR TO BEGINNING WORK.



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1	RECORD DRAWING	2/09	MWK



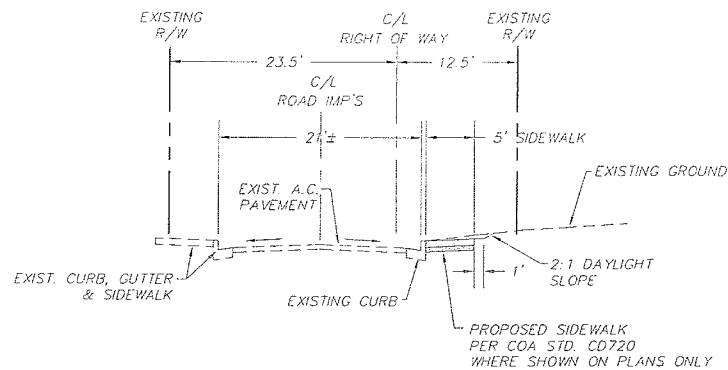
CITY OF ASHLAND	
ALEPH SPRINGS SUBDIVISION CIVIL IMPROVEMENTS	
GRADING, EROSION & SEDIMENT CONTROL DETAILS & NOTES	
REV. CITY ENGINEER:	DATE

PROJECT NO.

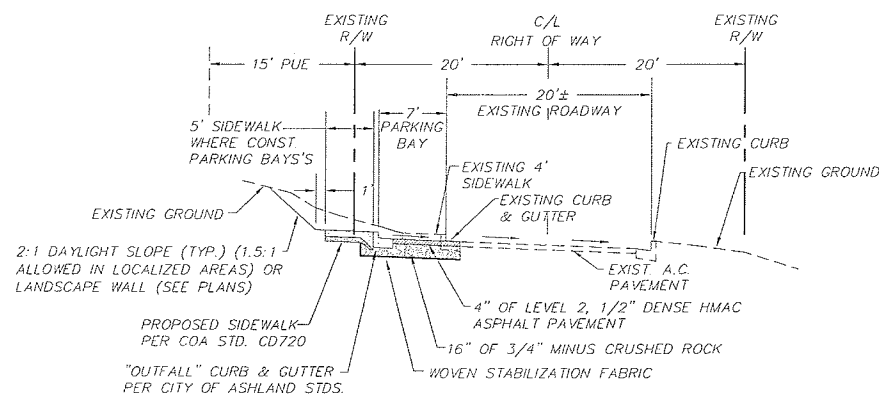
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D.2





TYPICAL SECTION  
STRAWBERRY LANE



TYPICAL SECTION  
HITT ROAD

NOTE:  
ROAD WIDENING & NEW SIDEWALKS ONLY WHERE PARKING  
TO BE PROVIDED. SEE PLANS FOR LOCATIONS.

**Nyloplast** Tomorrow's Storm Drainage Structures Today

## Weir Structure

The Weir Structure is a Nyloplast catch basin with a panel or plate device secured inside the structure that is designed to divert inflowing water to a preferred outlet, or to regulate the outflow of water from the drainage system.

**How the Weir Structure Works:**

- Storm water flows into the Nyloplast catch basin structure.
- The water flow is diverted to a desired outlet from the catch basin, typically to a water quality device, in order to effectively capture pollutants during the "first flush" of a storm event.
- The Weir Structure may also serve to retard or regulate the flow of water exiting the drainage system. The retardation is determined by height of the weir and/or the size of the orifice hole in the weir plate.

**Weir Structure Benefits:**

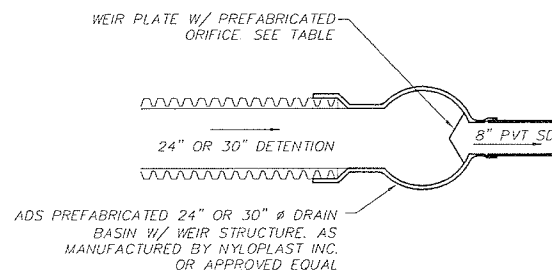
- Simple and effective method to direct the inflow into a storm water management system or water quality device.
- Simple and effective method to regulate outflow from a storm water management system.
- Enhances ability of a water quality device to capture pollutants from stormwater.
- Allows for flexibility to re-direct water flow during a high-flow event.
- Proven technology used for many years in the impoundment market.
- Nyloplast can customize Weir Structures for the direction of the design engineer to provide a variety of weir functions for site-specific needs, including "key way slot" and "notch" weir designs, and high flow or low flow orifice hole designs to further regulate the flow of stormwater.

**Ordering Information:**

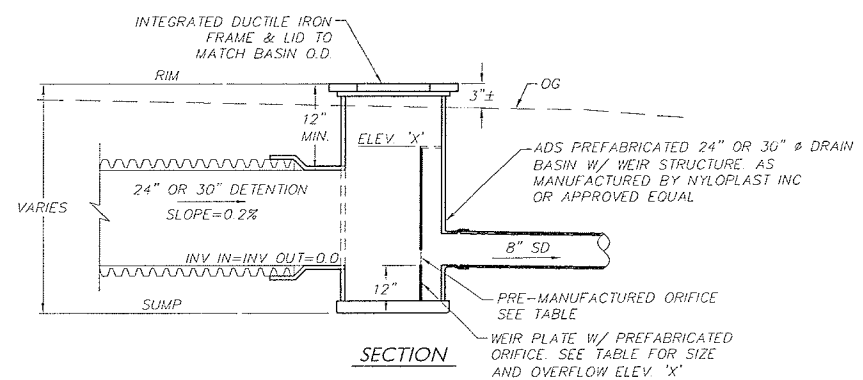
- Weir Structures available for 24" and 30" diameter drain basins.
- For use with 4" through 30" outlets.
- Minimum pumps are required in Weir Structures.
- Flow measure is taken upstream to regulate weir device in the drain basin.
- Orifice hole diameter / placement based on direction from project engineer.

**Nyloplast**  
3130 VERNONA AVE  
BURLINGAME, CA 94001  
PH: (714) 932-2443  
FAX: (714) 932-2452  
www.nyloplast.com

NOTE:  
THE PRIVATE STORM DRAIN DETENTION SYSTEM SHOWN IS FOR FUTURE  
IMPROVEMENTS OF INDIVIDUAL BUILDING LOTS AND IS REQUIRED WITH  
THE BUILDING PERMIT OF EACH LOT. THE DESIGN SHOWN IS ONLY ONE  
OPTION OF MANY STORM DRAIN DETENTION SYSTEMS THAT COULD BE  
IMPLEMENTED WITH EACH LOT'S DEVELOPMENT.



PLAN



SECTION

LOT #	LOT AREA	STORAGE REQ'D	LENGTH OF PIPE		ORIFICE SIZE		ORIFICE INV. ELEV.	OVERFLOW ELEV. 'X'	
			24" Ø	30" Ø	24" Ø	30" Ø		24" Ø	30" Ø
1	0.5 AC	186 CF	60 LF	47 LF	2 1/2"	2 3/8"	INV=0.0	X=+2.12	X=+2.59
2	0.5 AC	186 CF	60 LF	47 LF	2 1/2"	2 3/8"	INV=0.0	X=+2.12	X=+2.59
3	0.66 AC	246 CF	78.5 LF	62 LF	2 3/8"	2 3/4"	INV=0.0	X=+2.16	X=+2.62
4	1.41 AC	526 CF	167 LF	132 LF	4 1/2"	4"	INV=0.0	X=+2.33	X=+2.76
5	0.97 AC	362 CF	115 LF	91 LF	3 1/2"	3 3/8"	INV=0.0	X=+2.23	X=+2.68

PRIVATE STORM DRAIN DETENTION  
NOT TO SCALE

1  
E.2

**NYLOPLAST DRAIN BASIN WITH WEIR**

① INTEGRATED DUCTILE IRON FRAME & SOLID COVER TO MATCH BASIN O.D.  
② ADAPTER ANGLE VARIABLE 0°-30° ACCORDING TO PLANS  
③ WATERTIGHT JOINT (CORRUGATED HDPE SHOWN) VARIOUS TYPES OF INLET & OUTLET ADAPTERS AVAILABLE: 4" - 30" FOR CORRUGATED HDPE (ADS W-12, ADS SINGLE WALL, HANCOCK DUAL WALL), SDR 35, SCH 40 DWV, CORRUGATED & RIBBED PVC  
④ 24" - 30" DRAIN BASIN BODY  
⑤ 18" - 30" FRAME & SOLID COVER  
⑥ INLET PIPE  
⑦ OUTLET WEIR  
⑧ HIGH-FLOW/SECONDARY OUTLET  
⑨ FIRST-FLUSH OUTLET  
THE BACKFILL MATERIAL SHALL BE CRUSHED STONE OR OTHER GRANULAR MATERIAL MEETING THE REQUIREMENTS OF CLASS II MATERIAL AS DEFINED IN ASTM D321. BEDDING & BACKFILL FOR SURFACE DRAINAGE INLETS SHALL BE PLACED & COMPACTED UNIFORMLY IN ACCORDANCE WITH ASTM D321.  
SIDE SECTION & TOP VIEW ENLARGED TO SHOW DETAIL

**WEIR AVAILABLE FOR ALL 24" - 30" STRUCTURE OPTIONS (CUSTOM BASIN, ROAD & HIGHWAY, & CURB INLET)**  
**DRAIN BASIN TO BE CUSTOM MANUFACTURED ACCORDING TO PLAN DETAILS. RISERS ARE REQUIRED FOR BASINS OVER 8' DUE TO SHIPPING RESTRICTIONS. SEE DRAWING NO. 7001-110-005.**  
**HEIGHT OF WEIR TO BE DETERMINED BY ENGINEER.**  
**WEIR MANUFACTURED TO MINIMIZE LOSS OF OUTLET PIPE OPEN AREA.**  
**ADAPTERS CAN BE MOUNTED ON ANY ANGLE 0° TO 30° TO DETERMINE MINIMUM ANGLE BETWEEN ADAPTERS. SEE DRAWING NO. 7001-110-010, 7001-110-013, & 7001-110-014.**  
**DRAINAGE CONNECTION INTO JOINT TIGHTNESS SHALL CONFORM TO ASTM D321-2 FOR CORRUGATED HDPE (ADS & HANCOCK DUAL WALL) & SDR 35 PVC.**  
**FRAMES, GRATES, HOODS, & BASE PLATES SHALL BE DUCTILE IRON PER ASTM A536 GRADE 10-30-06.**

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DRAWN BY: EBC  
DATE: 5-13-04  
APPROVED BY: CJA  
DATE: 5-13-04  
DWG SIZE: A  
SCALE: 1/4" = 1'-0"  
SHEET: 1 OF 1  
DWG NO.: 7004-110-058  
REV: C

**Nyloplast**  
3130 VERNONA AVE  
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P.O. BOX 1724 • MEDFORD, OREGON 97501  
PH: (541) 779-5268 • FAX (541) 779-3139

DRAWN BY: DLG	DATE: 12/07
CHECKED BY: MWH, PWH	DATE: 12/07
AGENCY ENGINEER:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:

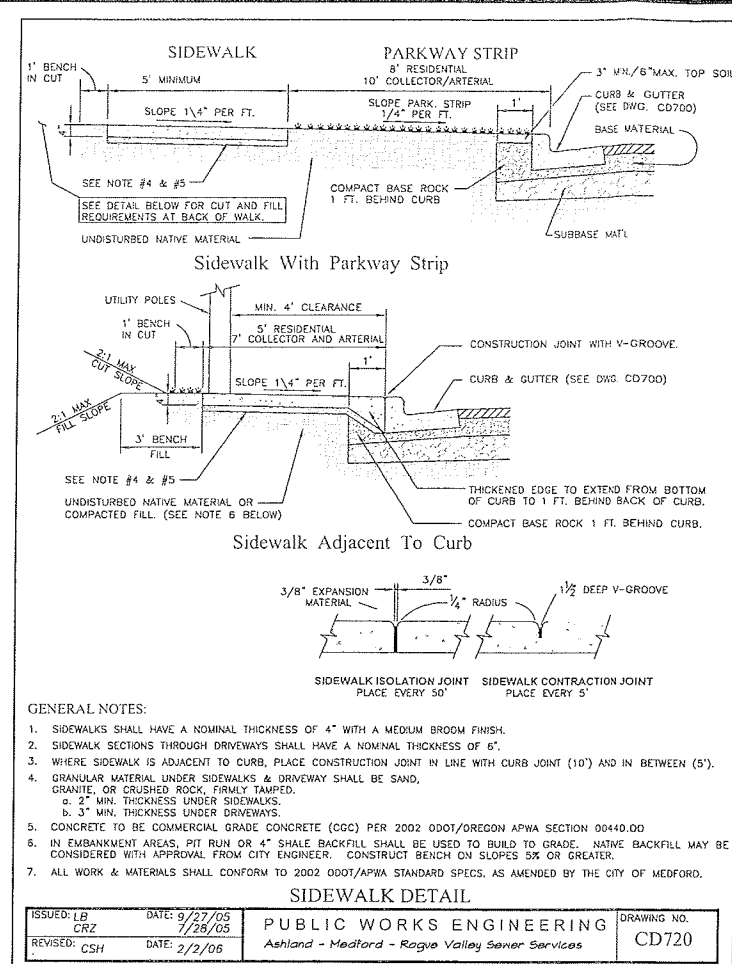
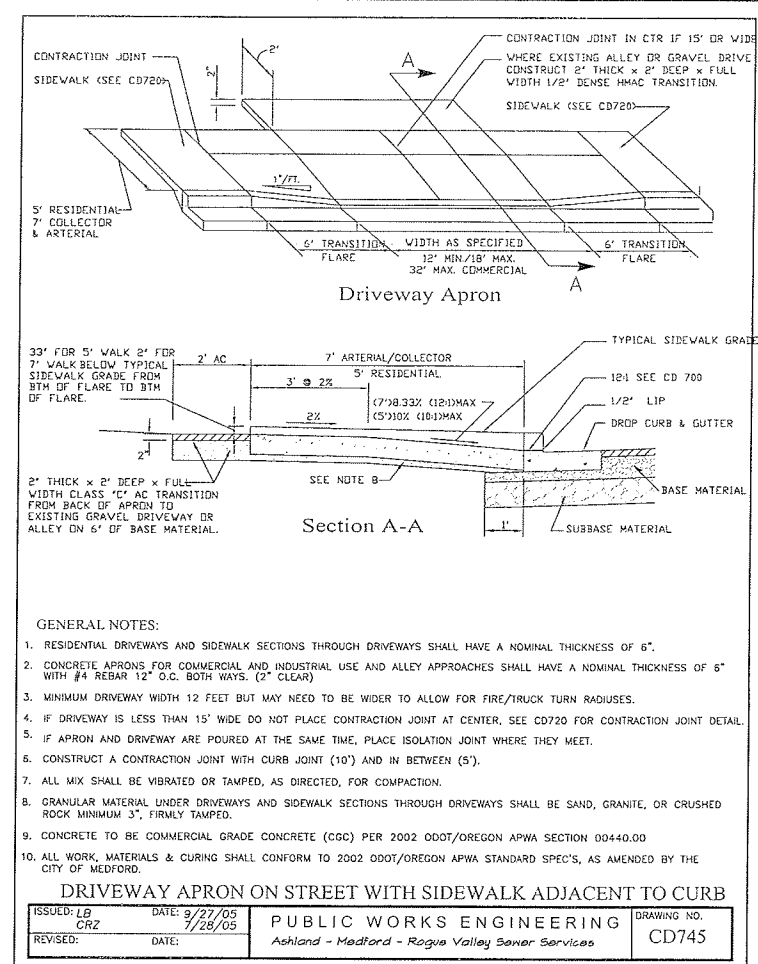
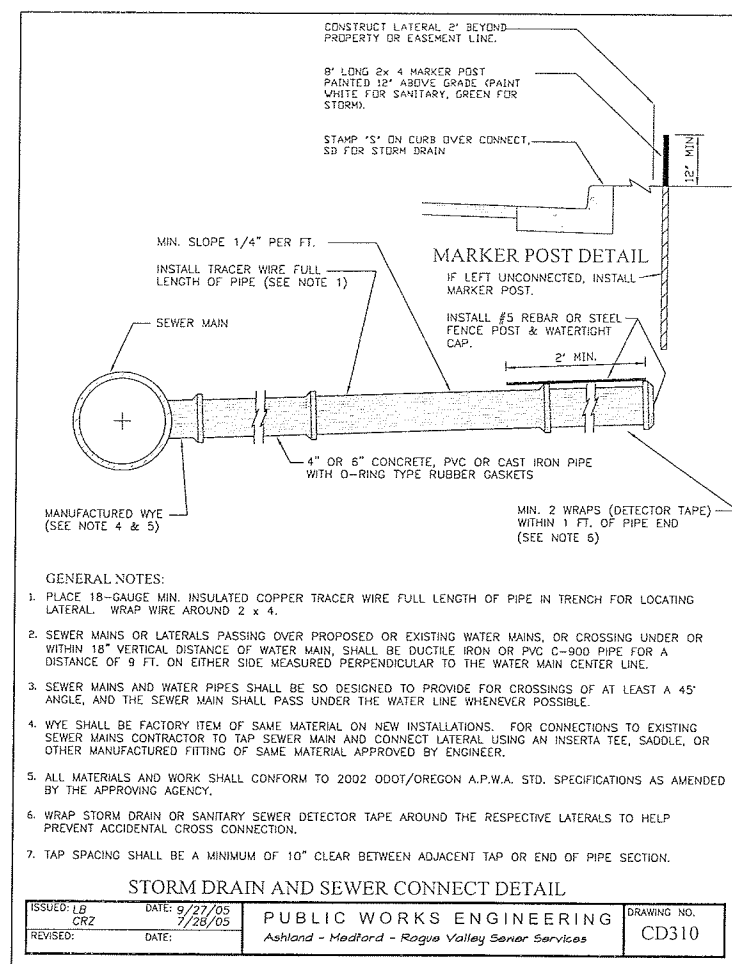
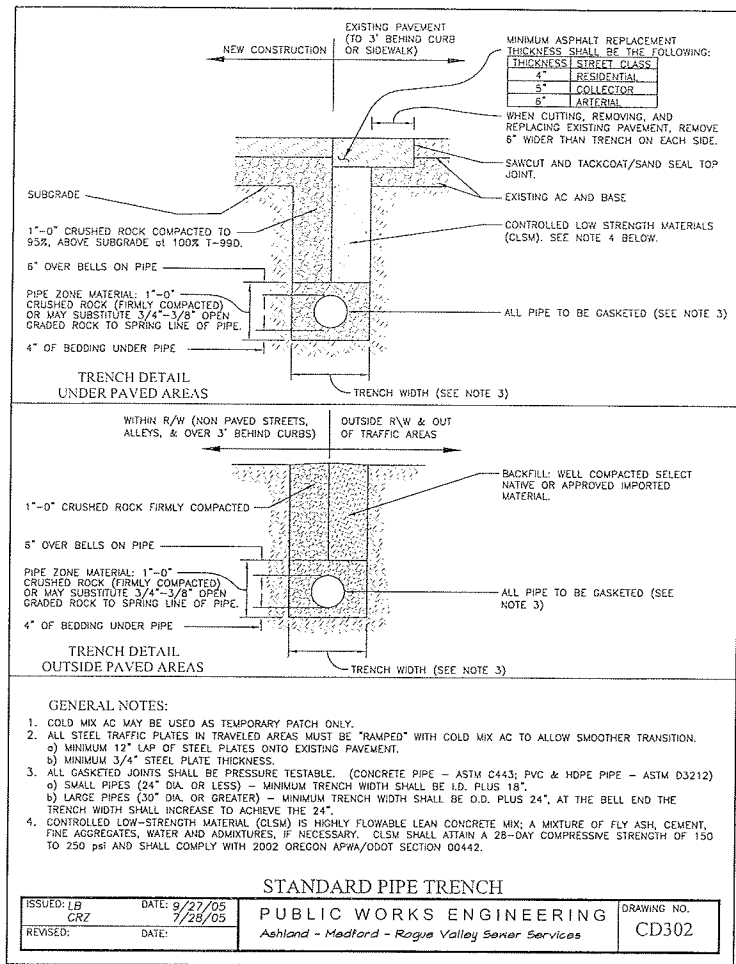
NO.	REVISION	DATE	BY



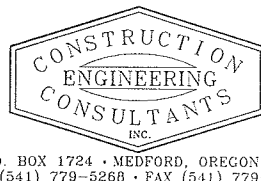
**CITY OF ASHLAND**  
STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS  
STREET CROSS-SECTIONS  
DETAILS

REV CITY ENGINEER DATE

PROJECT NO.  
DRAWING NO.  
E.1



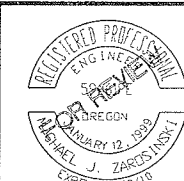
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CHECKED BY: MWK, PWH	DATE: 12/07
AGENCY ENGINEER:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:
AGENCY REVIEW BY:	DATE:

NO.	REVISION	DATE	BY



**CITY OF ASHLAND**

STRAWBERRY LANE SUBDIVISION  
CIVIL IMPROVEMENTS

DETAILS

REV. CITY ENGINEER: DATE

PROJECT NO.  
DRAWING NO. E.2

