

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please rise and, after you have been recognized by the Chair, give your name and complete address for the record. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
November 13, 2007  
AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center, 1175 E. Main Street
- II. **ANNOUNCEMENTS**
- III. **APPROVE AGENDA**
- IV. **CONSENT AGENDA:**
1. **APPROVAL OF MINUTES:**  
October 4, 2007 – Special Planning Commission Meeting  
October 9, 2007 – Regular Planning Commission Meeting  
October 23, 2007 – Planning Commission Study Session
- V. **PUBLIC FORUM**
- VI. **TYPE II PUBLIC HEARINGS**
- A. **PLANNING ACTION:** PA-2007-01756  
**SUBJECT PROPERTY:** 705 Helman St  
**OWNER/APPLICANT:** OgdenRoemerWilkerson Architecture AIA  
**DESCRIPTION:** Request for Site Review approval to construct an approximately 6,400 square foot gym addition and a 5,010 square foot library addition at Helman School, with related interior modifications and sitework. **COMPREHENSIVE PLAN DESIGNATION:** Single Family Residential  
**ZONING:** R-1-5; **ASSESSOR'S MAP #:** 39 1E 04BC; **TAX LOTS:** 200,600,700
- B. **PLANNING ACTION:** PA-2007-01398  
**SUBJECT PROPERTY:** 167, 185 and 203 N Mountain Avenue  
**OWNER/APPLICANT:** Havurah Friends Investment Group LLC  
**DESCRIPTION:** request for Outline Plan Approval to allow a 12-lot, 15-unit subdivision for the properties located at 167, 185 and 203 North Mountain Avenue. Also included are requests for: the modification of a previously approved Site Review and Conditional Use Permit (#2001-0039) for the Havurah Jewish Synagogue; Site Review approval to construct a two-story, six-unit residential building; a Tree Removal Permit for the removal of one nine-inch diameter pine tree from Tax Lot #1701; and a boundary line adjustment with Tax Lots 1600, 1701, 1800 and 1700. (This request supersedes the previous Outline Plan approval for a 14-lot, 13-unit subdivision granted under Planning Action #2006-01091.) **COMPREHENSIVE PLAN DESIGNATION:** Single Family Residential  
**ZONING:** R-1-5-P; **ASSESSOR'S MAP #**39 1E 09 AA **TAXLOTS:** 1500,1600,1701,1800,1700

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

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**VIII. TYPE III PUBLIC HEARINGS**

None

**IX. OTHER**

Review Planning Commission Goals

**X. ADJOURNMENT**

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**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
SPECIAL MEETING  
October 4, 2007  
MINUTES

I. CALL TO ORDER

The meeting was called to order at 7:00 p.m. by Chair John Stromberg at the Ashland Civic Center, 1175 E. Main Street, Ashland, OR

Commissioners Present:

John Stromberg, Chair  
Michael Dawkins  
Tom Dimitre  
John Fields  
Pam Marsh  
Melanie Mindlin  
Mike Morris

Absent Members:

Olena Black  
Dave Dotterrer

Council Liaison:

Cate Hartzell, Council Liaison, absent due to quasi-judicial agenda items.

Staff Present:

David Stalheim, Community Development Director  
Bill Molnar, Planner  
Adam Hanks, Permit Manager  
Diana Shiplet, Executive Secretary

II. ANNOUNCEMENTS

Mr. Stromberg stated Mike Morris will be late, Olena Black and Dave Dotterrer have previous commitments and so have excused absences. Mr. Stromberg stated that Planet Citizen, an organization listed in the most recent Planning magazine, has on-line training programs for Planning Commissioners. There is some money in the budget for PC training. He would encourage commissioner who is interested in participating in the training to talk to Susan Yates in Community Development to get registered.

III. APPROVE AGENDA

Marsh/Dawkins m/s to approve the agenda. Voice Vote: Approved.

IV. TYPE III PLANNING ACTIONS

- A.      **PLANNING ACTION:**      PA2007-01283  
          **APPLICANT:**              City of Ashland  
          **DESCRIPTION:**          Proposed amendments to the Ashland Land Use Ordinance implementing portions of the recommendations in the Land Use Ordinance Review prepared by Siegel Planning Services. In addition, other recommendations of the City Planning Director concerning land use decision-making procedures will be considered.

Mr. Stromberg stated that this is the kind of project where you can easily get caught up in the specific issues but in the big picture we have a very large collection of detailed changes in the land use ordinance. He stated that due to the size of this project often some of the details are not captured in the summary. Staff is much more familiar with all the details than the commission they are looking at the practical use, efficiency, and customer service. The Planning Commission is more focused on the balance between community interests and private interests. With that in mind, he, John Fields, Mike Morris, David Stalheim, Bill Molnar, and Adam Hanks met for about two and a half hours going over all the non-policy, non-procedural portions of the proposed changes. During that meeting he realized how challenging and how many questions arise from these changes. He stated that to the extent that it is possible to dig into the details tonight will mean the Planning Commission is doing their part more effectively. He reminded the commission that they have until October 12<sup>th</sup> to make any suggestions or alterations to the ordinance so that staff can make changes in time for the October 23<sup>rd</sup> Planning Commission meeting. On October 23<sup>rd</sup>, it is hoped that the Planning Commission will bring this process to completion. Also, on the 23<sup>rd</sup> he would like to bring back the revised Planning Commission Goals and talk about the strategy for getting everything approved by the City Council.

David Stalheim, Community Development Director, stated this process started with the Zucker report and the Seigel report in February and April of 2006. Following that, John Stromberg, John Fields, and Mike Morris met between June 2006 and February 2007 and went through the Seigel report. Mr. Stalheim prepared recommendations based on the reports and the meetings in February of 2007. Planning Commission did a study session in April of 2007, a retreat in June of 2007, another study session and the end of June 2007, and the first draft of the ordinance was released in July of 2007. Planning Commission agreed to start the public hearing process at the end of July 2007 and the second draft, based on public comments, staff review and Planning Commission input, was released in August of 2007. That second draft is the version we are working from tonight. Mr. Stalheim reminded the Commission that they did have a public hearing on this topic which started on September 11<sup>th</sup> and was continued to tonight. He stated that the Commission need to see if there was any public input tonight and then close the public hearing.

Mr. Stalheim reminded the Commission that at tonight's meeting they need accomplish 4 things tonight; 1) address any new public input, 2) to discuss the procedure changes in the ordinance, 3) discuss any policy issues identified in the ordinance and 4) determine what the next steps might be in the process.

Mr. Stalheim stated he would like to highlight some of the issues he thinks the Planning Commission should focus on during their discussions tonight. The first is the procedures issue. He stated that the real issue is whether or not they should be packaged together for passing on to the Council for approval or if they should be separated. The proposed changes in the site design chapter require changes in the procedures chapter, so they can't really be separated. If the Planning Commission determines they would like the procedures separated out, they much also separate out the site design chapter.

Mr. Stalheim stated he went through the ordinance and attempted to identify other sections that would be affected. The first is that in every zoning district there is a reference regarding wireless communication facilities that refers to a specific section in the revised site design chapter. It is the same standard as before but they moved things around and so there is a reference to that section and that would get lost if they didn't update the site design chapter. The second is a small reference to procedures in the tree preservation chapter which needs to be updated in the site design chapter.

Mr. Stalheim stated that by not doing the site design chapter there are some things in the site design chapter that are standards improvements in the chapter and staff feels that these improvements will be valuable and last for a long period of time without needed additional updates.

#### **Mike Morris arrived at 7:13 pm.**

Mr. Stalheim stated that he met with planning staff last week to make sure that they are really comfortable with the procedures changes. Staff stated that they believe winter is the best time to implement any procedure changes, due to lower planning permit activity levels. One staff member also recommended a longer timeframe for implementing the procedure changes.

Mr. Stromberg stated he also recommends the winter as a good time to implement this, not only with us but also with the City Council as they have fewer things on their agenda during that time as well. He stated that the important thing is that this is very valuable and there is a lot of potential for success with these changes.

Mr. Stromberg stated that the group who had met including himself, John Fields, Mike Morris, Bill Molnar, and Adam Hanks would like to become an official subcommittee of the commission in order to continue working on this project and complete looking at all the changes. Mr. Molnar stated that because there already is a land-use subcommittee there is no need for a motion to approve this subcommittee.

Mr. Stalheim stated that one of the other procedure issues which needs to be discussed is the appeal fees. The ordinance does not prescribe fees, but states that if they are set, need to be paid and notes the ORS requirement to reimburse fees if the appeal is successful. So the question to the Planning Commission is do you wish to recommend to Council appeal fees? The second issue is whether or not the appeals to Council will continue to be de novo or will become on the record appeals.

Mr. Stalheim stated that the first policy issues which needs to be discussed includes that the Commission can review the options – including no action. The other “top six” options that need to be discussed include:

#### **Residential Ground Floor in C-1 and E-1 zonings**

The options regarding this were outlined in the September 11, 2007 staff report. The first option was No Action. This does not address the problems of interpretation regarding lot area. He reminded the Commission that it currently states that

65% of the ground floor if there is one building, but if there are multiple buildings it calls for 50% of the lot area. The problem is that they don't really know what is the lot area when measuring buildings and square footage. Option two is what is in the current draft of the ordinance and that is to allow one small unit or approximately 500 square feet on the ground floor. Option three is the original draft which prohibited residential units on ground floor.

#### Additional Dwelling units in R-2 and R-3 zonings

The options for this were also outlined in the September 11, 2007 staff report. The first option is to take no action. The result of that would be that you could have more density in the R-1 zone than in the multi-family zones. The second option is in the August 28<sup>th</sup> draft which would allow units of up to 1,000 square feet. Option three is what was in the original draft of the ordinance which would allow units of up to 500 square feet. And option four has not been drafted but there were some discussions about "splitting the difference" and allowing units of up to 750 square feet.

#### Hotel/Motel definition

The options for this were, again, outlined in the September 11, 2007 staff report. The first option is to take no action. Option two has a new definition that would require a lobby and on-site staff. The intention of this is to avoid timeshare residential units which were really not hotels or motels. Option three is a new definition without any of the limits on the lobby or on-site staff.

#### Temporary Storage

This relates to units like the PODS, which we are starting to see around the city. Again, the options for this were outlined in the September 11, 2007 staff report. The first option is to take no action so there would be no regulation of these within the city. The second option, which is in the current draft, places limits on these facilities. Option three would allow for some longer time periods for active building projects. The fourth option is to set standards that consider size of the sign on the storage building in order to make a distinction between these temporary storage units, because some might have signs that are not that obvious and others have quite a bit of advertising on the sides of them.

#### Vision Clearance

Option one is no action. This option does not address the problems with the current standards. However, the details of a replacement standard have not been resolved with Engineering. Option two would be for the commission to direct staff to continue to work on this issue.

#### Fire Turnaround on Flag Partitions

Option one is no action. This does not address the Council directive to make the Land Use Ordinance consistent with the Fire Code. The Commission can table this for a later discussion. The second option is to adopt the update as drafted in the ordinance.

Mr. Stalheim reiterated that the idea is to come out of this discussion with a clear set of directions on procedure and policy issues. Also, it is hoped that the group will review details and make recommendations for the full Planning Commission action at the October 23, 2007 meeting. Also, to perhaps identify which of these issues need to be tabled for discussion at a later time and should not be included in this round of amendments.

#### PUBLIC HEARING

No members of the public were present.

**Dimitre/Morris m/s to close the public hearing. Voice Vote: all ayes. Motion passes.**

#### Fire Turnaround

Mr. Stalheim asked Fire Division Chief, Margueritte Hickman, to discuss with the Commission the Fire Department's concerns regarding the fire turnaround on flag lots issue. Ms. Hickman stated the new 2007 Oregon Fire Code, which was adopted by the City Council, does not match with our current Land use code. In order to get those two aligned they need to amend the Land Use Code. Currently the Ashland City Code states that you don't need a turn around on a dead-end street until 250 feet. However, the Oregon Fire Code requires a turn around after 150 feet. The Council requested that the two codes match.

Ms. Hickman stated that the Fire Department did some research and found that in the last 7 months of building permits there were 5 projects that required a turn around and if the 150 foot requirement were in place, an additional 2 turnarounds would have been required. She stated that it is her understanding that, historically, the land use code originally adopted the 250 foot requirement based on the Fire Code.

Ms. Hickman stated that the Council requested that Fire staff change the fire code and to bring to them a different amendment so that Ashland did not amend the fire code in a less restrictive fashion. The reason this is less restrictive is that we are allowing people to have a longer driveway or a longer dead-end before they have a turn around. Rather than requiring it at a shorter length which would be more restrictive. The numbers are bigger but it is less restrictive. The Council wanted this change to be made to the Ashland Fire Code. She stated that the concern they brought to the Council was that this change would be in conflict with the State Fire Code. After bringing this to the Council, the Council decided that they would like the Land Use Code updated to reflect the State requirements.

Ms. Mindlin questioned if there were other alternatives in place of the turn around requirement, such as requiring fire sprinklers instead of a turnaround. Ms. Hickman stated that currently the Fire Code does not give such an alternative as an option. That is something they could consider, if necessary. Typically, the requirement for sprinklers is to mitigate a longer response time and the purpose of the update really has more to do with being able to turn the equipment around and not have to back the equipment out. Backing equipment has been proven to be more risky than moving forward.

Mr. Stromberg asked if there were any comments from builders regarding this requirement. A comment was made that on a single flag lot, the amount of paving for a turn around is huge. Mr. Stalheim read a previously submitted comment from Mark Knoxs which stated, "...that staff should re-evaluate the standard primarily due to the amount of asphalt associated with the turn around standard as illustrated in the multitude of in-fill parcels this is going to effect. He would hope 250 feet would still remain as long as fire sprinklers were added which are probably far more important than the turn around. This seems to be a better compromise and one he is sure Fire could agree upon. For many years the Fire Department touted sprinklers as the primary safety measure, minimizing fire hazard and the spread of fire." Mr. Stromberg stated fire prevention isn't really the issue here, it is the difficulty of backing out the extra 100 feet and he assumes that could mean slower response time if they have to back out prior to heading to the next call. Ms. Hickman agreed that it certainly could decrease the response time.

Mr. Stromberg questioned if there are any insurance issues with the City not adhering to the State Fire Code. Ms. Hickman stated it was researched and determined that City Land Use Code could supersede State Code, as it relates to access.

Mr. Molnar also pointed out that the 250 foot requirement is also identified in the performance standards under street standards. We would want to make that consistent with the other parts of the code. He stated that one of the issues over the years has been that often private driveways or flag-lot drives are in areas of slope. This becomes an issue of design in terms of how do you construct these taking into consideration treed areas, cuts, and fill. The issue the Commission needs to look at is if there is some flexibility when determining location of the turnaround. For example, if the turn around could be moved 15 feet in either direction this may preserve trees, etc. In discussions with Fire over the years, they have been flexible and it doesn't seem like this new requirement will be even more restrictive. Ms. Hickman clarified that this does not mean there has to be a turn around every 150 feet. It is one turn around required for any driveway over 150 feet. At the same time, they have interpreted the code to mean that if a driveway is, for example, over 750 feet and a turn around is not possible at the end of that 750 feet Fire will accept a turn around within 150 feet of the end of the driveway. As Mr. Molnar stated, the Fire Department can be flexible as to where the actual turn around is located.

Ms. Mindlin stated that she has issues with this, because, having just installed one, it is a lot of lot coverage, especially when you add in the 5 foot clearance for under 18" plants requirement.

Mr. Stromberg asked if this code is subject to administrative variance. Mr. Molnar stated that anything in the ordinance, unless it has been specified as not, is subject to a variance. Any variances issued, however, are done so while working closely with the Fire Department. Ms. Hickman stated that she would need to re-read that section of the code, however, there has been discussion in the code that if it is impractical due to physical land features, existing buildings, etc... the Fire code official can make some adjustments.

Mr. Morris stated that he prefers the flexibility of working with the Fire Department to determine what is acceptable to them. He does not like seeing rigid numbers in the code. He also questioned why Council would go to the Fire Department and require them to get the land use ordinance changed without directing the Planning Commission to change it.

Ms. Marsh asked how much square footage is required to install a turn around. Ms. Hickman stated it was approximately 1500 square feet. Ms. Marsh questioned if the added turn around will significantly effect the lot coverage requirements. Mr. Hanks stated, yes, with smaller rural residential lots those issues do come quicker, however there are many options such as moving the garage closer to the street to limit the driveway length. Ms. Marsh stated that it might be interesting to take a look at the two which would have fallen under this rule and see how this rule would have effected what was developed there.

Mr. Stalheim stated that another option, based on Ms. Marsh's comments, is that the Commission could look at exemptions to lot coverage for those smaller lots which require a turn around. Mr. Dimitre asked, since we are currently requiring turnarounds for driveways over 250 feet are we currently having issues regarding lot coverage? Mr. Stalheim stated that recently there have been some variance requests regarding lot coverage in flag lots. Mostly this is due to people not taking into consideration the shared drive when planning the lot coverage. Mr. Molnar stated the most effected areas like lower Clay Street or Tolman Creek, where it is an R-5 zoning, 5000 square foot lot size, and there are some very long, skinny lots with very small total lot size.

Mr. Stromberg stated that he doesn't have a clear sense of what direction the commission would like to head in, and asked if anyone wanted to make a motion on this topic.

Mr. Dawkins asked for clarification as to what option one (no action) would mean. Does it mean that essentially staff would continue to work with the Fire department to determine when turnarounds were necessary? Mr. Stalheim stated it means that the 250 foot requirement would still be in place and the city code would not match the state code. He stated that we could write an option three which changed it to the 150 foot requirement but grant exemptions for lot coverage and could also write as part of the code that 150 feet is the requirement but that with the Fire Marshall's approval a variance on that requirement could be given.

Ms. Mindlin stated that she doesn't think we should make our exemption related to lot coverage. She sees the point in shortening it if there is flexibility and a variance. If we leave it at 250 feet, there is no way to require anyone to put one in at any shorter distance. If we go with the shorter distance and have some flexibility depending upon each individual situation she much prefers that option.

Mr. Fields stated that by changing the requirement where it becomes an issue is steepness. If it is a wide open area it is different than a heavily treed area. If it is one house versus 22 houses it can be an issue. He wanted clarification on where a driveway technically ends. Is it where a garage begins? Does it start at the back of sidewalk? If so, his driveway would be more than 150 feet, and he can't imagine a fire engine ever needing to go down his driveway and if they do, he can't imagine them having a hard time backing out. He suggested we table this issue, as there are too many unknown issues.

Mr. Dimitre asked if Mr. Fields was interested in recommending any parts of this issue tonight, or if he would prefer that staff take it and re-work the whole thing. Mr. Fields stated that 250 feet is a long distance if it is multiple units, if it is on a steep hillside. There are conditions in which 250 feet is too far and other times where it isn't worth the cost to do a turn around on a shorter distance. He thinks we need to take a look at the real safety issue. We need to take this issue out of this current process or we will never get finished with any other items.

Ms. Hickman stated that, in response to Mr. Fields question as to where does a driveway end it ends at the point where the Fire Department can reach within 150 feet of the furthest point of the house.

Mr. Stromberg suggested we sent it back to staff to bring back to the Commission as part of the October 23<sup>rd</sup> meeting with something that goes for 150 feet, per Council request, but gives staff and Fire Department the Flexibility to lengthen that per the special conditions of any given situation.

**Fields/Dimitre m/s that staff follow up and present the Commission with exceptions and conditions for ways to not restrict the requirement to 150 feet but to make something that is a little more balanced. Voice Vote: all ayes. Motion passes.**

#### Procedures

Mr. Stalheim stated that the two issues the Commission needs to discuss are the appeal fee and the appeal to Council.

Mr. Stromberg stated he also has several other issues he would like to discuss, but he will wait until after other have discussed these two appeal topics. He asked if any other members of the commission have any other procedural issues they would like to have discussed.

Ms. Mindlin stated she did have a question from page 6 regarding notice requirements. She asked, regarding the staff permits versus Type I permits if anything anyone applies for anything if it meant they would have to wait a month before getting their permit? Mr. Stalheim stated, no there is a tier of permits and some of them are ministerial actions, such as fence permits, home occupations, sign permits, etc... those have no noticing requirements and therefore have no delay in processing.

Mr. Stromberg asked that the commission take a look at the staff report and refresh their memory as to what the major changes are that they are going through tonight. The first is the Expedited Land Divisions. Mr. Stalheim stated that the State requires a "referee" for handling appeals who is not a city employee or official and so in the updated land use ordinance it states that the City Administrator has the authority to hire a "referee" if that issue ever came up. The other choice as with many jurisdictions in Oregon we could adopt no procedures, but if someone asked for that type of application that procedure could be available. So the Commission could adopt no procedure but refer to the ORS. Mr. Fields requested clarification on the maximum densities – how is this different than a minor land partition and how you could do incrementally every 12 months to create more? Mr. Stalheim stated that this is a minor land partition and you couldn't create more because you have to put in that density. The intention of the statute was that you couldn't come in and divide up a parcel and do it expedited and then come back in 12 months.

Mr. Stromberg stated that the next change is to staff permit and type I planning actions. He stated that the main thing with this modification is a consolidation of these two types of permits into one style – a Type I permit. He asked if the change to Type I permits is being made because it simplifies the noticing requirements. Mr. Hanks stated, yes, the change helps staff because it keeps all permits tracking on the same timeline. Otherwise staff has to notify individual actions which each have different expiration and approval deadlines but all the Type I, II, and III actions have the same deadline regarding notifications.

Mr. Fields asked if there were any Type II permits being turned into Type I permits? Mr. Stalheim stated, yes, there are and that this will be discussed further later in the meeting. He stated that in order to increase efficiencies it is important that the Type I permit final action be by the staff.

Mr. Stromberg asked if there is a downside for citizens or developers in doing this? Mr. Stalheim stated that the downside would be that with staff permits people can apply at any time. They did write into the procedures the ability for staff to adopt application deadlines so there is the ability for staff to say certain types of applications could be accepted more often than the larger applications. That way staff can still proceed in a timely fashion.

Mr. Stromberg stated the next issue he would like to talk about is staff decisions final and reconsideration and appeal procedures. He read the summaries from the staff report. He then asked if anyone believed that the City Council would actually agree to have the Planning Commission be the final hearing authority? Mr. Stalheim stated he believes there should be only one appeal process in the city and they need to decide where that appeal body is. Ms. Marsh stated that as the Commission goes through this process it is their job to put forward the best recommendation possible – they can't look to try and put together a packet just so Council will approve it. Mr. Stalheim reminded the Commission that appeals consume a considerable amount of staff time. He estimates the last few appeals have consumed roughly 20 hours of staff time each.

Ms. Marsh asked if we have any idea of how many times in the last five years a Type I permit has been pulled by the hearings board for review by the full Planning Commission? Mr. Stalheim stated that there have been two in the last nine months. Mr. Molnar stated that prior to those two there have been fairly few. Ms. Marsh stated that the point is that these are fairly routine kinds of actions. Mr. Dimitre stated the other question is did any of those that came to the Planning Commission get over turned. Mr. Morris asked for confirmation on whether or not staff has taken Type I permits and turned them into Type II. Mr. Stalheim stated that has been done on a lot of occasions over the years. Mostly this is because some of the Type I permits are on the edge of public interest and concern and so staff has bumped those up to a Type II. Both the current and proposed ordinances allow staff to change Type I permits to Type II in order to have a public hearing. It also allows the applicant to request this change. Mr. Stromberg asked if the ordinance allows any Planning Commissioners to request the change. Mr. Stalheim stated the Commissioners need to be cautious with that sort of action because they will run into conflict of interest and ex-parte contact before the action came before the Commission for review. Mr. Stromberg questioned if the hearings board sending items to the Planning Commission created ex-parte or conflict of interest problems. Mr. Stalheim stated that as there is no contact because it is not a public hearing and so it doesn't create an ex-parte contact.

Mr. Dimitre asked for clarification as to whether or not the public can ask for a public hearing on Type I permits. Mr. Stalheim stated that right now they can and with the update to the ordinance they could only by appeal to the Planning Commission. Mr. Molnar stated that one of the key issues is that items which are called up now are often due to property owners who don't have a whole lot of information but with the changes we are making in regard to the notification many of those issues will be cleared up because property owners will have the opportunity to discuss there issues with staff prior to a decision being made.

Mr. Morris stated as long as we have the process for reconsideration it is good, because the Commission can focus on factual errors and not be dealing with policy issues.

Ms. Mindlin stated the concern she has is lack of public access to even having an initial public hearing without paying a lot of money. She would feel more comfortable with this process if it cost a lot less to call up an item. Ms. Marsh said there are two questions which need to be answered; 1) is the process a better process for getting public input and moving the item along and 2) for the piece of it that includes an appeal, should there be a fee. These two questions should remain separate. Mr. Dimitre stated that he believes there is a fundamental change as currently people can ask for a hearing in front of the Commission and it doesn't cost them anything but with the change the only time you can get in front of the Commission is when you pay \$250 for an appeal. This is a fundamental shift in how we handle items. Particularly for those folks who don't have a lot of money. Mr. Fields stated that if you look at the net saving of time and paperwork we probably could have no appeal fee and still see a savings in the department.

Mr. Stromberg reiterated his earlier question of what is the downside of this process for the citizen. Mr. Stalheim stated that he can't see any downside other than the issue of appeal fees. In his opinion the improved notification process is nothing but beneficial.

Mr. Stromberg asked if staff included anything in the new Type I permits that wasn't in there already or wasn't part of the lower level items which have been brought up? Mr. Stalheim stated, yes, and this has been discussed on several different occasions. He gave a list of those permits. He reiterated that it still is under staff discretion to change some Type I permits to Type II. Mr. Molnar stated that typically all permits in the downtown area are bumped up to Type II, due to the higher levels of public interest.

Mr. Stromberg stated he would like to discuss the appeal fee issue. He stated that if the Commission passes this ordinance they way it is structured then the City Council will be the ones who will decide on the appeal fee. The Commission is not required to take a position regarding the fees, however, they can make a recommendation. Mr. Fields would like to recommend that they not add an appeal fee, with the understanding that Council can add a fee later if they need to. Ms. Marsh would like to recommend a Fifty-dollar fee just to make sure the people who are asking for things to be brought forth for a full hearing are serious about it. Mr. Dimitre stated that it seems like the best way to deal with this is to get the process in place, see how it works, and if we are getting too many appeals, the fee could be reconsidered. Mr. Morris stated he would go along with the fifty dollar fee because the reconsideration should be looked at first, which has no fee. If that falls through then an appeal happen but there should be a fee associated with the appeal, even if it is a minor fee. Mr. Stromberg stated that he would prefer to have no fee, see if we have a problem and then the Council could make the decision to have a fee. He believes that for many people fifty dollars is not a small amount.

**Dimitre/Fields m/s that they recommend to Council that there be no appeal fees. Voice Vote: 5 ayes. 1 no. Motion passes.**

Mr. Stromberg read the summary of the Type II Planning Action procedural modifications from the Staff Report.

Mr. Stromberg stated this was the only Type II procedural modification. He reminded the Commission that they did this process with the Verde Village permit. Mr. Stalheim stated that they did start this process but were unable to complete it because they ran out of time.

Mr. Stromberg asked Mr. Stalheim if he had any additional comments regarding this change. Mr. Stalheim stated that he thinks it is a tool that could be used it is not a requirement it is an option. He thinks it will increase the ability to have a different type of hearing which might allow a little bit more participation and might allow the Commission to have more information when they begin their deliberations. The record would be more complete.

Mr. Stromberg asked how this would affect the 120 day rule. Mr. Stalheim stated it shouldn't affect the rule because the process gives staff some flexibility on when the hearings take place.

Mr. Dawkins asked for some simple examples of the process. For example, with the Northlight permit which is just starting through the process, would the Commission be the ones doing the hearing? Mr. Stalheim stated no, that the initial, evidentiary hearing would be conducted by staff for information gathering only – no decision making.

Ms. Mindlin stated she has a concern about requiring the applicant and the public to come to two meetings to do the same thing. Mr. Stalheim stated that they are not required to attend both meetings. Mr. Fields said in public hearings where the public is heard in a more informal setting (rather than in front of the Commission) it allows people to articulate their ideas and concerns without it being such a formal, rigorous setting. Mr. Stalheim stated that additionally, with the informal hearings you have much more time to discuss issues, whereas in the formal public hearings you are limited by the agenda and the constraints of time of the meeting. Additionally, there is the possibility to stop the hearing and allow the parties to work out issues before they go on the record in a formal public hearing.

Mr. Morris stated that he likes the idea of an evidentiary hearing to allow all parties the opportunity to work out all the details without wasting time in front of the Commission attempting to do so.

All agreed that it would be useful to use this method and see how it works.

Mr. Stromberg read the summary of Type II Planning Actions Reconsideration and Appeal from the Staff Report. He stated that the reconsideration portion seems to be fairly straight-forward it is just that the Planning Director can do it and it is only for factual errors.

Mr. Stromberg asked for clarification regarding the appeal changes as to why this allows for a greater public comment period. Mr. Stalheim stated that it has been his experience that for appeals which go to Council as de novo we see entirely new information presented in front of the Council which never was brought in front of the citizen commissions. In some projects it comes before quite a few commissions; historic, tree, planning, etc. and so when you have a de novo process suddenly this whole new body of evidence can be presented to Council so rather than it becoming an appeal it really becomes a whole new public hearing process. On the record appeals really encourage the public to get involved early in the process so that they present all their information in front of the citizen commissions.

Mr. Stromberg also noted that the other part of this process is that the City Administrator would be authorized to selectively allow new information in. He questioned how that would work and if the State rules allow this. Mr. Stalheim stated that the State rules state each city has to adopt its own procedures. One of the things that came up was that sometimes there is new information which comes to light, which may not have been available at the original hearing process so it is necessary for those exemptions to be made. The reason for involving the City Administrator is to avoid any pre-judgment on the part of the Council. This also takes away the Council's ability to appeal to themselves but not their ability to call an item up.

Ms. Marsh asked what is contained in the public notice for a Type II or Type III permit. Mr. Stalheim stated that they send a notice of application and a notice of public hearing which has all the dates for all the meetings for all commissions the item will be sent to.

**Fields/Marsh m/s to recommend that appeals to Council be on the record rather than de novo.** DISCUSSION: Ms. Mindlin questioned why Mr. Fields think on the record is a good idea. Mr. Fields stated that we go through all the effort to develop a record and then no matter what is decided people can introduce new evidence at the last minute which causes these cases to drag on for months and months and finally a decision is made based on information that the Commission didn't even have the chance to hear or consider. It seems the process, rather than being extended longer and longer, needs to be compressed and clear. The obligation is to make the case, have the hearing, and if the decision is faulty then you appeal that decision but that is different than saying, well we didn't like the decision so we want to do it all over again. At a certain point a decision just has to be made. Mr. Stalheim stated that part of the reason to have a de novo hearing is to cure any procedural problems which occurred during the earlier process but that is why, if we make this change, we need to give the City Administrator authority to review these things to cure any procedural issues. **Voice Vote: all ayes. Motion passes.**

## V. UNFINISHED BUSINESS

None.

## VI. Other

A. Regional Problem Solving: Discussion and overview of population and urban reserve issues

Mr. Stalheim gave an overview of the current Regional Problem Solving (RPS) population and urban reserves issues. In 2003 the City of Ashland decided to notify the RPS process that they did not intend to propose and new urban reserves.

Mr. Stromberg asked for a definition of urban reserves. Mr. Stalheim stated urban reserves are land that the City identifies in the comprehensive plan as areas which will be urbanized in the future but which are not currently within the urban growth boundary.

Mr. Stromberg asked if it were true the City of Ashland can not expand the urban grown boundary unless the City first get approval from the State based on the urban reserves. Mr. Stalheim stated that the RPS regional plan will start affecting that, and he will discuss this more later in the presentation.

Mr. Stalheim gave information on the population allocation. In 2006 the County did a population allocation for cities in the county. The consulting firm of ECONorthwest assisted them and sent population allocation information to the cities. They identified that by 2026 the city would have 5,177 more people than currently and 12,260 by the year 2056. Three weeks later they sent responses to the allocations, and noted that Mr. Molnar had sent a memo stating that the figures allocated to Ashland are reasonable. With this response they also re-sent out the population allocation numbers which were still the same as before.

Less than 9 days later ECONorthwest sent out preliminary population allocations with no explanation as to why Ashland's figures had changed to 1,439 by 2026 and 2,176 by 2056. As a side note, Ashland population has consumed nearly 50% of the total population allocation we were given for then next 20 years in the last two years.

Mr. Stromberg asked for clarification on allocation versus prediction and if it means that, for example, Central Point took some of our growth. Mr. Stalheim stated this is exactly what happened. The total population allocation for Jackson County did not change between the two letters only Ashland's portion of that population.

After ECONorthwest's work, the County went through a process to amend their comprehensive plan. They adopted the same population allocation as the later ECONorthwest figures.

Ms. Mindlin asked if the lower numbers were the numbers presented to the Commission. Mr. Stalheim stated that the numbers were in the Economic Opportunity Analysis and it was that presentation which first got Planning Staff to start questioning the figures. They particularly questioned why population and employment numbers didn't seem to match up.

The Regional Plan is only reviewed every 10 years, with 2012 as the first year of review. There currently is no process in place for review of the population allocation and the county allocations (based on the lower numbers) has now been ratified by the State. If the Regional Plan needs to be amended because there is a new unallocated population the State says Jackson County has grown by more than previously estimated then all the other signatories need to consider the amendment. Basically, Ashland and all the other cities will have to sit down together and agree on the allocation numbers. Mr. Stalheim also reminded the group that the Regional Plan has no specific population allocations – they only have population for the entire region, not specific cities – the only place that does have these figures is the County comprehensive plan. The effect, though, is that the Regional Plan sets aside urban reserves and Ashland has requested none. In order to, in the future, ask for urban reserves to expand the urban growth boundary the population allocation issues then comes back into question.

Cities who choose to expand the urban growth boundary into land not designated in the urban reserves will be required to go through the RPS plan amendment process. Since Ashland had no urban reserves if we needed to expand, the City could not make that decision, we would need to go to RPS for approval. This change can only happen during the 10-year periodic review and, of course, at that time there will be a question of population allocation numbers.

Ms. Marsh asked for clarification on if we had said before that we could handle the 5,177 population growth within our existing UGB. If so, then what is our argument? Even if we get the 5,177 in population increase, we are still on the record as saying we could accommodate that increase. Mr. Stalheim stated that the difference is that Ashland has a higher density standards and he believes RPS is trying to aim for those standards. He thinks that our buildable lands inventory needs to be updated to ensure we still are accurate with our assumptions.

Mr. Stromberg asked if, when we do the inventory, we would be using the allocated figures? Mr. Molnar stated that when the County adopted their new population element they did put in a provision that they will review the coordinated population estimates every five years. This was the only concession they could make to us. They didn't say they would change anything,

but that they would be willing to look at the numbers in 2012. To answer Ms. Marsh's question, when Mr. Molnar went and spoke to the RPS policy committee months ago, to give updates and to re-affirm the City's position, the one concern RPS had was that almost 3 ½ years had gone by since the City gave their initial suggestion not to do any urban reserves and two items had come out which could affect that original position. These were, 1) we conducted our wetland inventories and one of the chief in-fill areas turned out to be a wetland (south of east main) and 2) Measure 37 was approved, which brings up many impacts on a community that weren't even considered in 2003. The question now is if there is more of a tool to the City to have identified urban reserves to accommodate growth within the style of the City rather than be at the unknown hands of Measure 37?

Mr. Stromberg asked if the State has other agencies who have told RPS that this draft of their plan is not acceptable and RPS has decided to push ahead anyway? Is there a chance RPS will have to re-work their plan? Mr. Stalheim stated the State submitted a letter to RPS policy group and RPS has reviewed that and has gone to Salem to talk to the State agency. He thinks there have been some adjustments so a change might not be necessary.

Mr. Stalheim stated that his recommendations are: 1) Regional Plan should review the population allocation prior to concluding that there should not be any more urban reserves for Ashland. Mr. Stalheim sent a letter to the RPS technical and policy group but has not been put in place. He believes Ashland should have first opportunity to add urban reserves, if Ashland so wishes. 2) Ashland should review the decision about urban reserves perhaps holding those areas while the Comprehensive Plan is reviewed and updated. The 5 or 10 year review in the Regional Plan will greatly limit Ashland's opportunities to consider adding to the Urban Growth Boundary. 3) An alternative would have the Regional Plan formally recognize that Ashland could come forward to add urban reserves, if it wishes after our public process, without having to wait for the periodic review of the Regional Plan in 2012.

Notes for future meetings:

Mr. Morris will be out of town for hearings board, Mr. Dawkins agreed to take his place.

ADJOURNMENT – The meeting was adjourned at 9:15 p.m.

Respectfully submitted by,  
Diana Shippet, Executive Secretary

**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
OCTOBER 9, 2007  
MINUTES

**I. CALL TO ORDER**

The meeting was called to order at 7:03 p.m. by Chair John Stromberg at the Ashland Civic Center, 1175 E. Main Street, Ashland, OR

**Commissioners Present:**

John Stromberg, Chair

Michael Dawkins

Tom Dimitre

John Fields

Pam Marsh

Mike Morris

**Absent Members (excused):**

Olena Black

Dave Dotterer

Melanie Mindlin

**Council Liaison:**

Cate Hattzell, Council Liaison, absent due to quasi-judicial agenda items.

**Staff Present::**

David Stalheim, Community Development Director

Bill Molnar, Planning Manager

Angela Barry, Assistant Planner

Amy Anderson, Assistant Planner

Sue Yates, Executive Secretary

**II. ANNOUNCEMENT**

Dawkins gave thanks to Bob Plain, Ashland Tidings reporter, who is leaving Ashland to take a job in Ithaca, New York.

**III. APPROVE AGENDA**

A. Dawkins/Dimitre m/s to approve the agenda. Voice Vote: Approved.

B. Stalheim announced that the Systems Development Committee (SDC) that formed every year is looking for members. Marsh has expressed an interest, but if any others are interested, contact Stalheim.

**IV. CONSENT AGENDA**

A. August 14, 2007 Hearings Board Minutes

B. September 11, 2007 Regular Meeting Minutes

On page 7 of the September 11, 2007 minutes, it was Marsh who called for the question, not Stromberg.

Dimitre/Dawkins m/s to approve the Consent Agenda. Voice Vote: Approved.

**V. PUBLIC FORUM**

COLIN SWALES, 461 Allison Street, thanked Bob Plain for his work on the Tidings, and to express the importance of the Daily Tidings to cover the activities of the Planning Commission meetings. Recording the decisions of the Planning Commission is very important.

BRENT THOMPSON, 582 Allison Street, talked about Regional Problem Solving (RPS). The last public meeting will be October 10th, 5:30 pm at the Talent Civic Center. Thompson is President of the Friends of Jackson County, P. O. Box 1443, Phoenix, OR 97535. The Friends of Jackson County was formed to protect farmland, forest land, open space and wildlife habitat, and to contain the sprawl of cities through infill programs. RPS has attempted to deal with growth in a planned way. We are getting to the point where a final plan will be approved. Friends of Jackson County has testified mostly with regard to the fact that no city yet has a fully evolved infill program. Thompson gave several examples of the densities per square mile of cities in Jackson County. It takes 5000 people per square mile to insure that a public transit system will be economically viable. Medford is of particular interest because they have one of the lowest densities in the county with about 2500 people per square mile. Ashland has a density of about 2700 per square mile, and has an infill program with a commitment not to add to the Urban Growth Boundary (UGB). There is still a lot more Ashland can do with infill.

**VI. UNFINISHED BUSINESS**

A. **PLANNING ACTION:** PA-2007-00250

**SUBJECT PROPERTY:** 281 Fourth St

**OWNER/APPLICANT:** Aaron Glover

**DESCRIPTION:** Request for a Conditional Use Permit for a theater use and a Type II Variance to parking for a property located at 281 Fourth St. **COMPREHENSIVE PLAN DESIGNATION:** Employment **ZONING:** E-1;

**ASSESSOR'S MAP #:** 39 1E 09BA; **TAX LOT:** 101

*The public hearing and record have been closed.*

Ex Parte Contact/Bias/Conflict of Interest/Site Visits

Dawkins and Morris had another site visit. Morris talked to two people that had testified at the last meeting, but they discussed only procedures. Dimitre, Marsh and Fields had no ex parte contacts or site visits. Stromberg received an e-mail last night from Ruth Alexander that he read to the Commissioners. A letter was inadvertently included in the packet from Bill Welch, Deluxe Awning, 260 4<sup>th</sup> Street, discussing scheduling of events and Variance criteria. The letter was received after the record was closed. No one challenged the ex parte contacts.

**STAFF REPORT**

Stalheim referred to the Staff Report Addendum dated October 9, 2007, outlining what has happened since the last public hearing. During the two weeks that the record was left open, the applicant met with Staff to work out strategies for the issues raised including noise, trash and security.

Staff has proposed the following:

1. Serving of Alcohol - The applicant has amended their request to allow the serving of alcohol at the facility. Staff is recommending a Condition prohibiting the service of alcohol except for catered events. They can, however, apply for a Conditional Use Permit to allow for the serving of alcohol.
2. Hours of Operation – Staff is recommending weeknight shows end at 11:00 p.m. in order to reduce the impact on surrounding residential uses.
3. Number of Patrons – the parking standards are based on the seats and event. In this case there are no seats, so Staff based it on total occupancy. The total occupancy allowed would be 150 people, allowing for about 30 performers or staff and 120 patrons.
4. Limit large events to a seven. Smaller events could be more frequent.
5. Monitor noise with an independent authority on a complaint basis.
6. Staff clarified that the Variance is for 13 spaces. The applicants are asking for a 50 percent reduction with on-street parking credits for two spaces. They talked about amending the Variance request to allow the shared parking to be further than 200 feet from the facility so they can consolidate the parking in one location.

The Commissioners have been given a modified set of findings for approval should they choose to approve the action.

- Condition 13 reads “That the applicant shall provide staff to patrol all areas within 200 feet of the premises for one hour past the end time of any event. This staff shall observe and report to the proper authorities, vandalism, outdoor use of alcohol or drugs, or other illegal activities, and shall be available to the public during these patrols to discuss any nuisance issues related to the theatre use.”
- Condition 14 reads “That security will be provided by an independent firm contracted by the applicant for all events that result in a total building occupancy of more than 100 people.”

Marsh asked who the “proper authorities” are. Barry thought the Ashland Police Department or the Code Enforcement Officer.

Marsh wondered what governs the operation at catered events. Stalheim said the OLCC gives out licenses and they have to be approved by the City. The real controlling factor will probably be the number of events allowed.

The owner of Deluxe Awning suggested 8:00 p.m. as a starting time for events. Stalheim said this is an Employment zone. He thinks Deluxe Awning is concerned about parking. The parking that is available has to be timed to the events. The parking should be able to be controlled through the parking agreements. Stalheim added that there is an overall parking issue in the Railroad District. The question is: Do they meet the criteria for this particular application? It is a little easier for Staff to support a 50 percent parking variance in the Historic District. They tried to condition the application so when the shared parking agreement is granted, that the hours are identified that parking is available.

Dimitre said at the last meeting the Commission was told this was an incomplete application. He is concerned whether or not the burden of proof has been met for the CUP. Stalheim said it has been challenging for Staff to evaluate this application. However, since the last hearing and their meeting with the applicant, and getting the applicants to submit information in writing as to what the applicants plan to do, it has become clearer and more supported by Staff. He will leave it to the Planning Commission to decide if it meets the burden of proof, the CUP criteria and Variance criteria.

Dimitre questioned if there is parking for staff (approximately 30 staff). Stalheim said there should be, but the parking standards are tied to the seats, not the staff.

There is no limit for the number of events drawing under 50 people.

## COMMISSIONERS' DISCUSSION AND MOTION

Dimitre would like to see this project go forward. However, he is having a hard time seeing how the Variance criteria have been met. Dawkins said he is looking at the reality of the parking situation in the Fourth Street area. Through his own observation, there is plenty of parking available at the times the applicants want to schedule events - this would fit into an unusual circumstance. For years we've wanted activity in the Railroad District and by being overly restrictive with this request, we tend to get way too caught up in the legality of it. We have given approval to several projects that are in the same neighborhood without any of the restrictions. Examples include: Unitarian Church and Roasting Company. There is reality and there are the rules.

Stromberg added that this is the type of use that has been conceived for the area if it meets the criteria for a CUP. Because of the historical nature of the area, it doesn't have the parking capacity it would have if it were a new building project. That is the unusual circumstance. Staff has offered a viable solution by expanding the distance that people would walk to the event by opening up larger commercial parking lots that would not be in use at night.

Morris is concerned because in the Railroad District, we have denied applications for lack of one parking space. It's a stretch to authorize a Variance for this many spaces. We don't yet have the spaces. It could be on multiple lots.

Fields said any decision on one planning action does not set a precedent for another.

Marsh has an easy time with the parking variance because it probably fits the criteria for an Administration approval. Staff raised the question of whether in the Historic District this application met that threshold because the building was built in 1957. She read the code and interprets it to being tied to the Historic District, given that buildings in the area are built next to each with no room for parking, and we want to keep those buildings instead of encouraging people to tear them down and change the nature of the street.

Fields/Marsh m/s to approve with the 17 attached Conditions (as modified).

Marsh commented that Condition 11 seems excessive (notifying neighbors of events). It was agreed that it would be permissible for Mobius to e-mail neighbors.

Dimitre asked if the review in a year of the CUP and Variance is mandatory or optional. What is the mechanism to make it happen? Stalheim said the intent it is mandatory and in a year it will be noticed to the neighbors to let them provide comment and see how the applicants are performing to the Conditions.

Fields/Dawkins m/s to call for the question. Voice Vote: Approved.

Roll Call: Dawkins, Marsh, Morris, Fields and Stromberg voted "yes" and Dimitre voted "no."

APPROVAL OF FINDINGS: Marsh /Dawkins m/s to approve the Findings. Roll Call: The Findings were unanimously approved.

- B. PLANNING ACTION: PA-2007-00980  
SUBJECT PROPERTY: Westwood/Strawberry 391E 08BD Tax Lot #102  
OWNER/APPLICANT: City of Ashland  
DESCRIPTION: A request for a Land Partition approval to create two parcels, including one flag lot for the property located near the intersection of Strawberry Lane and Westwood Street.

### Ex Parte Contact/Bias/Conflict of Interest/Site Visits

Dawkins and Morris had a site visit. Stromberg, Dimitre, Marsh and Fields did not have a site visit.

## STAFF REPORT

Anderson reviewed the Staff Report dated August 14, 2007. The application was administratively approved in July 2007 and called up for a public hearing by a neighbor expressing concern about mitigating the impact to the neighborhood. There are no criteria addressing this.

The proposal is to create two one-half acre properties. The application appears to satisfy the criteria for a Land Partition and Staff is recommending approval. Anderson suggested two additional Conditions. 1) That applicant's Exhibit G shall be recorded concurrently with the survey plat so it is recorded onto each parcel when they are created, and 2) (4.b) That the driveway curbs cuts be permitted, installed and inspected in accordance with the standards of the Public Works and Engineering Division.

## PUBLIC HEARING

**BRANDON GOLDMAN**, Housing Specialist, City of Ashland, 51 Winburn Way, noted that the City is held to the same standard as anyone else from the public. This application is not asking for any variances, exceptions to existing standards or anything out of the ordinary. Goldman reviewed the application that has been made part of the record.

The call-up letter did not articulate what adverse impacts the neighbors anticipate with conditions of approval. Goldman noted the points that go above and beyond the requirements of a Minor Land Partition. According to the Fire Department, the existing fire hydrant is within an adequate distance of the proposed flag drive, therefore, interior sprinkler systems would be required. However, the neighboring subdivisions have requirements for fire sprinklers in their Covenants, Conditions and Restrictions (CC&R's), therefore, that requirement has been added to the deed restriction to insure these homes would have fire sprinkler systems installed. To address compatibility issues with the surrounding neighborhood, the City imposed other CC&R-like limitations through the deed restriction (Exhibit G).

- Outbuildings should be architecturally compatible with the primary house that is developed.
- RV's cannot be stored within 30 feet of an adjoining property, and no trailers other than camping trailers or heavy equipment are to be permitted or stored on the lots.
- No outside repair of vehicles.
- The removal of rubbish and weed abatement is addressed.

No limitations or size requirements have been added in order to make the property as marketable as possible.

**KEN BARNES**, 523 Strawberry Lane, President of the Strawberry Meadows Homeowners' Association, said the deed restrictions are a result of ongoing dialogue to be responsive to the previous neighborhood and in an effort to create a very special place. The Association is requesting the deed restrictions be strengthened.

- Continue the deed restriction in perpetuity, not just for eight years.
- Clarify and strengthen the language concerning RV storage.
- Language to preserve the natural night lighting by using downward directed lights.
- Add architectural design standards.
- Address drainage. The City has deposited dirt on some of the land and the area of slope and drainage has shifted.
- Confusion about land swaps.

**CATHERINE DIMINO**, 423 Strawberry Lane, explained that her concern is that they maintain the unique character and the rural atmosphere of the neighborhood. She would like to see:

- Separation of lots with landscaping strategies, not fences.
- No expiration for the deed restrictions, or it should not expire while the Strawberry Meadows Homeowners' Association is effect. The reason for the deed restrictions at the initial partition that it be part of Strawberry Meadows.
- What is the time period for RV parking – 14 days over the course of one year, leave for one day and bring it back for 14 days?
- Make it part of the Strawberry Meadows Homeowners' Association.

**Rebuttal** – Goldman said the deed restriction is not part of the approval criteria. The eight year term probably originated because some of it is tied to the development of the property. They can look at an extension. With regard to night lighting, there are no statements in the deed restriction to address a dark sky. It's usually been street lighting that has been modified to be directed downward. Architectural standards are outside the realm of a Land Partition. Homeowners' associations are created to maintain common facilities. There are no common facilities or spaces to maintain with this application. Storm water drainage from both of the proposed lots would be directed to Westwood Street through the public utility easement provided on the south side of the proposed flag lot. Flag drives are required to be screened with a fence or sight obscuring vegetation at the time of development.

Goldman said the deed restriction has been drafted but a new one will have to be drafted and taken up with the Mayor and Council. The Commissioners wondered if they have the ability to make recommendations to the Council.

Fields/Marsh m/s to approve this planning action with the four attached Conclusions and Recommendations and the two added Conditions recommended by Staff. Roll Call: Unanimously approved.

**APPROVAL OF FINDINGS:** Morris/Dimitre m/s to approve the Findings for PA2007-00980. Roll Call: Unanimously approved.

Dimitre moved to recommend to the Council to remove the language in the first paragraph in the deed restriction (Exhibit G) "...until the last day of December of the year 2015 A.D.," making it a perpetual deed restriction.

Stalheim talked with the Assistant City Attorney and there should be some kind of contractual time, perhaps in the 30 year range.

The motion failed for lack of a second. Stromberg would add his name to Dimitre's comments. Stalheim will pass the Commissioner's comments along.

VII. TYPE III PLANNING ACTIONS

A. COMPREHENSIVE PLAN AMENDMENT

DESCRIPTION: Amendment to the Ashland Comprehensive Plan amending the Economic Chapter and adopting an Economic Opportunities Analysis (EOA) as a technical appendix to the Comprehensive Plan.

Molnar said this is the first public hearing that will ultimately lead to a legislative action by the Council to amend the Comprehensive Plan. As background, the City received a grant from the Dept. of Land Conservation and Development to conduct an Economic Opportunities Analysis. The consulting work was done by ECONorthwest out of Eugene. The grant was part of changes under Statewide Goal 9 that deals with economic development, amending the Administrative Rules to create a standard format for which cities compile economic information, culminating in looking at employment projections that would lead to looking at a city's land supply. It's similar to what is required of cities for residential development; that in a 20 year planning period, they are required to have enough land in their UGB to account for the employment need over the next 20 years.

The City finished the grant work and submitted it to the state. The EOA is currently being used to determine long-term economic development in the interchange areas, particularly the Croman property. The master plan for Croman needs to be consistent with what came out of the EOA as well.

Molnar showed PowerPoint slides that were shown previously, to refresh the Commission's memory of the details of the EOA. He is looking for the Commission to make a motion as follows:

*Move to recommend that the City Council amend the text of the Ashland Comprehensive Plan – Economy Element – to include language recognizing the need to conduct studies and produce technical reports and adopt by reference the 2007 Economic Opportunities Analysis (EOA).*

Last week, Marsh said, the Commission talked about the Regional Problem Solving report and the adoption of the 1400 figure for population growth that is included in the EOA. She has a concern about adopting as part of the Comprehensive Plan, a document memorializing the lower population figures, given we are unclear the derivation of those figures. Molnar said there are a couple of ways to approach that issue. The consultant was bound when doing the EOA to use the report adopted by the County as part of their Comprehensive Plan. He said there are two places in the plan – the appendices (A-23) and in the population – where they added a paragraph commenting on assuming a countywide average annual growth rate of about one percent. He would recommend deleting the next two sentences that discuss “Ashland, by contrast, has an assumed annual average growth rate of about .28 percent. This lower than average growth rate reflects the tighter land supply and higher housing costs relative to other cities.” Molnar said if those two sentences are deleted, the next sentence reads: “It is likely that Ashland will experience greater population growth than the county has forecasted. The City should monitor population growth over the next five years to determine impact of actual population growth on land needs.” Molnar believes that statement is relevant.

Fields said he is not certain what some of the other things in the report mean. He realizes the adopted statistics and the trends can change, but he is concerned the conclusions are just a snapshot. It seems there needs to be a summation of anything the Commission disagrees with. Molnar said the City has many other reports that are referenced as supplementary documents.

Morris said nowhere does it say, for example, that Table IX is an exact science. The consultants take their trends and current data and list the assumptions to extrapolate the information. He does not see a problem with that as long as they quantify their numbers. However, if the analysis goes to the Council and they take things out, then the document would be incomplete, leaving more questions.

Stromberg was hoping to add some commentary rather than altering it. Molnar said we are coming through for the next stage of technical assistance grants for the next step of developing an action strategy. Stromberg added we are starting to develop a rational framework that is fact-based. We want to have a friendly, cooperative relationship with the consultants. If we have concerns we need to find a tactful way to address them.

PUBLIC HEARING - No one came forth to speak and no written comments were received.

COMMISSIONERS' DISCUSSION AND MOTION

Fields would like to call this the “Olena motion.” Black (absent) has been begging for numbers in the direction of the Comprehensive Plan to create benchmarks to see where we are going and where we have been. Fields noted that numbers are subject to spin, however, we’ve wanted to build this level of detail.

Stromberg said if anyone else besides himself is interested in including comments he will try to make that happen. He said “Economic Opportunities Analysis” is a provocative and stimulating title. For example, he noticed the report talks about national and international trends of energy prices going higher and higher and being more volatile, and people changing their travel patterns as a result. Ashland’s whole economy is driven primarily by something that depends on people’s travel patterns and the cost of transportation, particularly gasoline. If we are looking at economic opportunities, he would like noted in the EOA the vulnerability in the economy because this is an issue he sees as something we need to move ahead with as soon as possible. Stromberg gave another example of the railroad mentioned in the report. To take it further, if we can find a way to run a light rail, we should be starting to work with that because that could be our salvation at a time when the economy could get clobbered. Additionally, the report mentions employment growth in the town with the City of Ashland as one of the largest employers. The implication is that employment growth is good for the community. But, employment growth for the City, increases costs of living and costs of doing business. From an economic point of view, we need to make that distinction. We shouldn’t just treat the City as if it is another business whose growth is an automatic thing. Stromberg was drawn to the concept of the comparative advantage of Ashland. We have unused capacity in lodging, restaurants and visitor oriented retail nine months out of the year. The winter is the shoulder season. There is something that is an economic asset. The capital investment is there. We can do more business if there is something that draws people here. Put that together with SOU (struggling, but emphasizing areas of environmental studies – things that are getting close to sustainability) and we have the Ashland brand. We should consider becoming a model city to draw people here to see real world examples of this shift into a non-petroleum or lower petroleum base. It would strengthen our existing businesses and our university.

*The public hearing closed.*

Morris/Fields m/s to approve the motion language suggested by Molnar.

Marsh/Morris m/s to amend the motion to add some type of notation or editing of the population statement as suggested by Molnar.  
Voice Vote: Approved.

Roll Call vote on the original motion: Unanimously approved.

#### VIII. OTHER BUSINESS

Fields would like to have an agenda item to discuss the City’s relationship with planning staff and the Commission’s expectation of Staff’s role in the community. Our job and our service are helping people understand the ordinance and how to actually get something done. There is a conflict with being clear and objective versus using a value judgment. What is staff’s role helping people come to terms with this very confusing subjective process? Everyone should get a fair shake. As we look at revising land use ordinances and as staff is changing, what is the role of planning staff, what is the Commission’s relationship with it and what are our expectations of moving ahead into the future? How do we make planning work again in Ashland? We have to give direction or come to a public consensus of what we expect of our system.

Morris said the previous Planning Director, John McLaughlin, used to always say planning is an art. It’s hard to regulate creativity. The Councilor’s trainer is quoted on his website, “If all you know is what you don’t want, you are just going to get more the same.” We need to define in the ordinance what we really want. Now we don’t seem to know what we want.

Stalheim said he’s seen several applications that have put Staff in an awkward position. The public criticizes Staff because they appear to be friendly to some and not to others. There is nothing wrong with helping an applicant. He has seen, for example, Variance criteria scrutinized heavily for one applicant, but not another. That leaves Staff hanging, trying to provide advice to the customers. Consistency and fairness is really important. It’s a very organic process, but how you get there is to back and verify the vision and put the rules in format to make that happen. Stalheim said his pitch is for a Hearings Officer. The Planning Commission has to current planning off their plate and do planning.

Marsh and Dimitre also requested that the issue of staff involvement be discussed at another meeting.

IX. ADJOURNMENT – The meeting was adjourned at 9:42 p.m.

*Respectfully submitted by,  
Sue Yates, Executive Secretary*

**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
OCTOBER 23, 2007  
MINUTES

**I. CALL TO ORDER**

The meeting was called to order at 7:00 p.m. by Chair John Stromberg at the Ashland Civic Center, 1175 E. Main Street, Ashland, OR

**Commissioners Present:**

John Stromberg, Chair  
Michael Dawkins  
Mike Morris  
Tom Dimitre  
John Fields  
Pam Marsh  
Dave Dotterer  
Melanie Mindlin  
**Absent Members:**  
Olena Black, excused

**Council Liaison:**

Cate Hartzell, Council Liaison, present

**Staff Present:**

David Stalheim, Community Development Director  
Bill Molnar, Planning Manager  
Sue Yates, Executive Secretary

**II. ANNOUNCEMENTS**

**A. Council Items** – On November 27, 2007, there will be a possible report on the Wetland & Riparian Ordinance and the Croman Master Plan. The arterial setback ordinance will be brought to a regularly scheduled meeting.

**B. Roles and Responsibilities** – Stromberg reported that the City Administrator is reviewing newly drafted language. She will decide if it is ready to move onto the Council for their review.

*Recessed to honor David Stalheim. Tonight is his last meeting. The meeting reconvened at 7:15 p.m.*

Stromberg thanked Stalheim for his service, his hard work and willingness to push things forward, for bringing an outside perspective to planning along with his dedication and commitment to planning. Stromberg noted how Stalheim has had an appreciation for his staff and their abilities. Dotterer added his appreciation for Stalheim's unique organizational ability to listen to lots of people and issues and put it all together into a coherent form.

**III. PUBLIC FORUM**

**BRENT THOMPSON, 582 Allison Street**, discussed the Safe Routes to School program. The state has \$3.7 million in grant money for improving safety on any of the pathways and walkways to school. The program allows for planning of anything within two miles of school to increase the probability of children walking to school. Whoever gets their request in first, gets the cash. He encouraged the Commission to put this item on their list of goals by moving it forward and contacting the program administrator, Julie Yip, 503-986-4196, to find out what we need to do to make this happen. Dotterer said Traffic Safety has been working on planning for safe sidewalks. Molnar said Derek Severson, Associate Planner, has been working on this too.

**IV. APPROVE AGENDA** – Dotterer/Dimitre m/s to approve the agenda. Voice Vote: The agenda was approved.

**V. TYPE III PLANNING ACTIONS**

**A. PLANNING ACTION: PA2007-01283**

**APPLICANT:** City of Ashland

**DESCRIPTION:** Proposed amendments to the Ashland Land Use Ordinance implementing portions of the recommendations in the Land Use Ordinance Review prepared by Siegel Planning Services. In addition other recommendations of the City Planning Director concerning land use decision-making procedures will be considered. Proposed changes affect the following chapters of the Ashland Land Use Ordinance (Title 18): Definitions, Districts and Zoning Map, Woodland Residential District, Rural Residential District, Single Family Residential District, Suburban Residential District, Low Density Multiple Family Residential District, High Density Multiple Family, North Mountain Neighborhood, Retail Commercial District, Employment District, Health Care Services Zone, Tree Preservation and Protection, Physical and Environmental Constraints, Southern Oregon University District, General Regulations, Site Design Review, Partitions, Parking, Sign Regulations, Procedures and Enforcement.

*Note: The public hearing and record have been closed.*

Stalheim reviewed his memo dated October 15, 2007 memo outlining the work of the subcommittee. In summary, if there any items on the list that the Commission would like to continue discussing, he suggested tabling those items tonight to a specific time and date. This will allow the items to be continued without requiring re-noticing. Be clear what is being left on the table and what is approved.

Dotterrer/Dimitre m/s to table 1) the proposed changes to residential ground floor in C-1 and E-1 zones, and 2) the proposed changes to Vision Clearance. These items will be discussed at the February 26, 2008 Planning Commission Study Session, 7:00 p.m. at the Council Chambers. Roll Call: The motion carried unanimously.

With regard to the Procedures revisions, Stromberg noted at the last meeting the sense of the Commission was that the Procedures were acceptable, except they added a recommendation to the Council that the appeal fee should be set at zero. And, wait to see if there are any problems concerning misuse of the appeals process before adding a charge. Marsh clarified it is the appeals process for Staff approval to the Planning Commission and not a recommendation to change the appeal fee for a Planning Commission decision to the City Council.

Accessory Residential Units - Marsh questioned the Accessory Residential Unit (ARU) in R-2 and R-3. It does not make sense to her to allow for an accessory residential unit up to 1000 square feet on a 5000 square foot R-1 lot, and yet in the R-2 (7500 square foot lot minimum) and R-3 zones (5000 square foot lot minimum) not be allowed an accessory residential unit. Morris responded that the committee looked back at what the ARU was intended to do and at the Housing Needs Analysis. We are looking at getting smaller units. R-2 and R-3 are higher density multi-family zones. If apartments are not going to be built on R-2 and R-3 then should 1000 square foot units be built? Marsh argued that it can only be up to 1000 square foot. It could include units less than 1000 square feet. It is clearly secondary to the primary unit on the lot. Morris said parking becomes an issue as two parking spaces are needed for anything more than 500 square feet. He looks at R-2 and R-3 not in density, but in number of units. He believes it's better to have more units at a smaller size. Marsh believes there is more diversity with varying square footages. Units over 500 square feet can be more flexible in size to accommodate more than one person. Dawkins said instead of needing larger spaces, he believes we will be moving to smaller spaces.

Molnar agreed it should be lowered to 500 square feet. Historically, most of the accessory residential units that have been built in R-1 have been on lots between 7000 and 8000 square feet. The ordinance was set up to allow R-1 lots that are oversized – not big enough to split – to have a second unit. In R-2, once you go over 500 square feet, lot coverage requirements and parking will become an issue. Under a Conditional Use Permit, issues like scale, bulk and coverage can be addressed. In R-2 and R-3 they would be subject to Site Review.

Fire Truck Turnaround – Stalheim said the Oregon Fire Code Standard is 150 feet, however, to allow flexibility, wording was added to extend the turnaround to a distance of 250 feet in length (bottom of page 27 of Draft 3 – strikeout version)

Appeal Fee -18.108.070.B.2.c.i. – page 41 of Draft 3 – strikeout version - Dimitre noted the appeal fee language is still there. He recalled the motion at the last meeting, was that the Planning Commission recommended that there be no fee. It seems the Council should add the language back in if that is what they want. Stalheim said no fees have been established because they are set by separate resolution or ordinance of the Council. It does, however, allow for a placeholder. Dimitre's concern is that someone could read into it that there is a fee. His intention was that the language would not be there. Dotterrer reads it that it authorizes the Council at a later date to establish a fee. It would be better to leave the wording in if we think the Council may want to establish a fee.

Dimitre/Dawkins m/s to delete the last part of 18.108.070.B.2.c.i. – “be accompanied by a fee established pursuant to City Council action...” and c.ii. Roll Call: Dimitre, Dawkins, Stromberg and Mindlin voted “yes” and Morris, Fields, Marsh and Dotterrer voted “no.” The motion failed.

Fields/Dawkins m/s to recommend to the Council passage of this package of ordinance revisions.

## DISCUSSION

Dotterrer referred to page 2 and 4, Definitions. The definition “Porous Solid Surfaces” should be changed to match the wording in “Coverage, lot or site” to “solid porous surfaces.”

Dotterrer asked for an explanation of 18.08.74, Story (page 3, Draft 3 – strikeout version). Stalheim said sometimes dormers will come out to the setback lines, but if the dormer is taken out all the way to the edge so it is part of the wall, and if there is a requirement for an increased setback for multiple stories, then that should be considered a story for purposes of setback. He

said they are trying to make it clear that they cannot take advantage of the half-story definition and create a huge wall face along the setback line.

Temporary Uses. Molnar explained there has always had a CUP for temporary uses in an E-1 and C-1. Often, proposals for temporary uses in other zoning districts have come up; a use incidental, seasonal or subordinate to the permitted use. This revision would allow some flexibility in the other zones to review a temporary use under a CUP process.

Retail Commercial District, Special Permitted Uses in C-1, 18.32.025 (page 13, Draft 3 – ~~strikeout~~ version) - Units of less than 500 square feet shall count as .75 of a unit is language that was already in the multi-family zoning districts. Some of the proposals in the Downtown and E-1 zones, applicants have been interested in providing smaller units because there was a need, but under our current ordinance, they would get docked for a full unit even if it was a full unit. This won't penalize someone for doing smaller units.

Appeal – 18.108.070.B.2.c.ii – This language is out of state law. It is Stalheim understands it would have to be a bona fide non-profit association and recognized by the City.

Evidentiary Hearing – The purpose is to collect evidence by way of public input to find out what the neighborhood issues are. The issues can get into the record and it allows Staff to respond. It is not a mandated process, but an optional tool. An alternative is a neighborhood meeting.

De novo or on the record appeal to Council – Stalheim said sometimes a de novo hearing is selected because with the political process, the elected officials feel citizens can come before the Council unencumbered – they can come before them with no restrictions. On the record, there will be restrictions. De novo hearings can cure any procedural issues that occur at lower hearings. In the proposed revisions there is wording to allow an “out” for the City Administrator if there are any procedural issues so they could do a limited de novo process. The Procedures are written so the mayor or the council can set rules ahead of time regarding how an appeal would be done. Most on the record appeals allow the parties to file written arguments and then allow the parties to the action. Other people do not get to testify. It would limit the action to what is in the record rather than allowing new evidence.

Stromberg said if an appeal is on the record, it makes the Planning Commission's decision more important.

Dotterrer suggested amending 18.108.110A.4. by substituting the word “may” with the word “will.”

**Roll Call: The motion carried unanimously.**

**VI. HEARINGS OFFICER DISCUSSION**

Stalheim gave a PowerPoint presentation reviewing the pros and cons and why he is recommending a hearings officer.

Stalheim said the Commission needs to prioritize their work. He reiterated that if the Commission wants to do aggressive long-range planning, they need the time to spend on it. It does not have to be permanent. He believes a Design Review Board would be a positive option. The officer usually charges a flat fee.

Dawkins said it's been asserted that if the Commission did not hear all the actions, there would have more time to work on other things. A couple of years ago, at the direction of Council, three members of the Planning Commission worked for over a year on developing a Downtown Plan that was rejected by the Council. There seems to be a fundamental problem with visioning and he's not so sure it's directly related to the fact the Commissioners are reviewing planning actions. By going through the cross-section of projects that come to them, they get the pulse of the community. Planning is an art. However, until we somehow have a clear vision from the Council, he would just rather not have a hearings officer.

Marsh lived in a community that had a Hearings Officer, an Architectural Review Board, and a Planning Commission. The hearings officer, a planner (not an attorney), worked very well for their city and she could support that. The recommendations passed tonight regarding Procedures is a baby step. If the commissioners can let go of Hearings Board and put more decisions in the hands of Staff, over time, we will find out how comfortable we are with that. Dotterrer agreed and will also wait and see what the Council does with the Planning Commission's Roles and Responsibilities. Dawkins concurred.

**VII. MEASURE 37/MEASURE 49**

Stalheim put the Measure 49 language in the packet for informational purposes only.

### VIII. WETLANDS AND RIPARIAN STANDARDS

Molnar reviewed the background for this upcoming ordinance. Over the past month, while this has been delayed, he has had the opportunity to talk to members of the Technical Advisory Committee, and go out into the field to see how the proposed ordinance can be applied to actual creek environments or wetlands. As a result, he is reformatting some areas of the proposed ordinance. It is not ready this evening, so he is just going to give an overview of each section. He would like the Commission to have a basic understanding of that they are doing prior to sending out a notice under the Measure 56 requirements. The purpose is to draft a new chapter in the Land Use Ordinance. Molnar gave a PowerPoint presentation.

#### Questions from Commissioners/Topics for Future Discussion

- Density transfer that would compensate commercial owners for sitting next to mitigation areas. To how many sites would this apply?
- State law allows communities to have alternative ways to determine where the buffer zone is in a riparian area. What are the options? Molnar said cities can include a 50 percent reduction in the setback and establish criteria. If the applicant is willing, they can go to a more discretionary review.
- Should there be provisions to treat a subdivider differently than a single existing homeowner?

Contact Staff with additional comments, questions or concerns.

### IX. OTHER

Marsh read in the local newspaper that the Council has approved a Transportation Committee. She wondered what kind of nexus we would like to have as a Planning Commission with that body. She personally wrote to the Mayor suggesting the Planning Commission reserve one seat on the committee for communication purposes.

Stromberg reported that he had just attended a meeting that will start the revision of the Transportation System Plan and there is a whole process lined up to move ahead. Does this have any connection to visioning/comprehensive plan process? Stalheim suggested Stromberg speak with Paula Brown, Public Works Director.

X. ADJOURNMENT – The meeting was adjourned at 9:40 p.m.

*Respectfully submitted by,  
Sue Yates, Executive Secretary*



**PLANNING ACTION: #2007-01756**

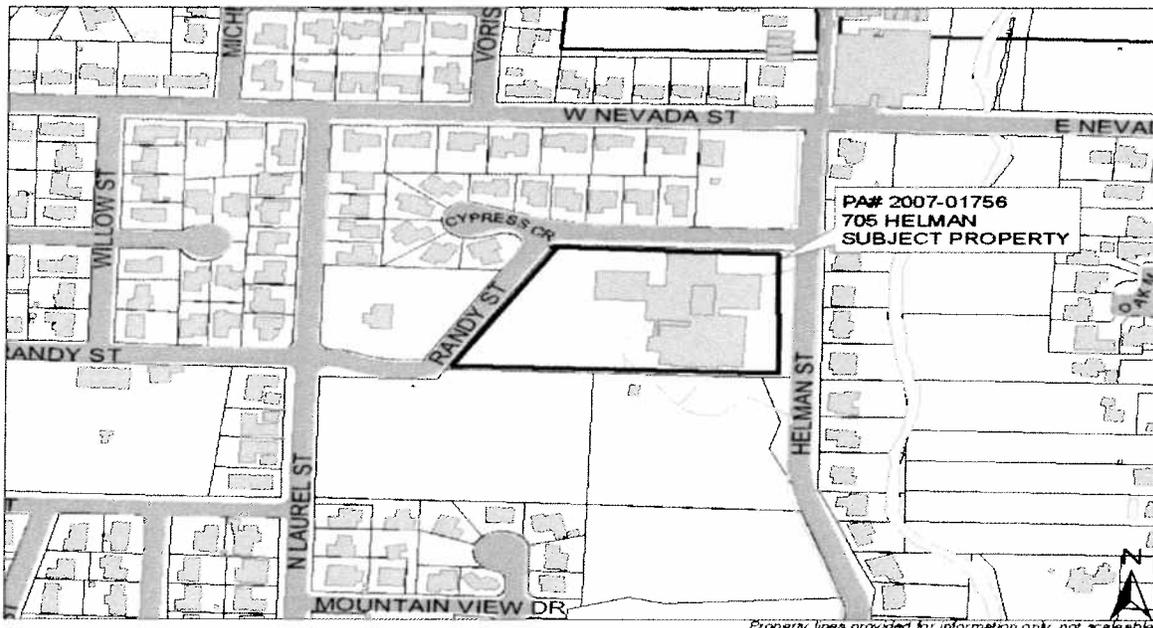
**SUBJECT PROPERTY: 705 Helman Street**

**OWNER/APPLICANT: Odgen Roemer Wilkerson Architecture**

**DESCRIPTION: A request for Site Review approval to construct an approximately 6,400 square foot gym addition and a 5,010 square foot library addition at Helman School, with related interior modifications and sitework, for the property located at 705 Helman Street. The application includes the removal of six trees; because these trees are less than 18-inches in diameter at breast height (d.b.h.) and located on public school property no tree removal permits are required. COMPREHENSIVE PLAN DESIGNATION: Single Family Residential; ZONING: R-1-5; ASSESSOR'S MAP #: 39 1E 04 BC; TAX LOT: 200, 600, 700.**

**NOTE:** The Ashland Tree Commission will also review this Planning Action on November 8, 2007 at 7:00 p.m. in the Community Development and Engineering Services building (Siskiyou Room) located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING: November 13, 2007, 7:00 PM, Ashland Civic Center**



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102-.35.104 ADA Title I).

## **SITE DESIGN AND USE STANDARDS**

### 18.72.070 Criteria for Approval

The following criteria shall be used to approve or deny an application:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options. (Ord. 2655, 1991; Ord 2836 S6, 1999)

# ASHLAND PLANNING DEPARTMENT STAFF REPORT

November 13, 2007

**PLANNING ACTION:** 2007-01756

**APPLICANT:** OgdenRoemerWilkerson Architecture, AIA

**LOCATION:** 705 Helman Street  
39 1E 04 BC Tax Lots 200, 600, 700 & 800

**ZONE DESIGNATION:** R-1-5

**COMPREHENSIVE PLAN DESIGNATION:** Single Family Residential

**APPLICATION DEEMED COMPLETE:** November 5, 2007

**120-DAY TIME LIMIT:** March 4, 2008

**ORDINANCE REFERENCE:**

18.20	R-1 Single-Family Residential District
18.61	Tree Preservation and Protection
18.70	Solar Access
18.72	Site Design and Use Standards
18.92	Off-Street Parking
18.96	Sign Regulations

**REQUEST:** A request for Site Review approval to construct an approximately 6,400 square foot gymnasium addition and a 5,010 square foot library addition at Helman School, with related interior modifications and sitework, for the property located at 705 Helman Street. The application includes the removal of six trees; because these trees are less than 18-inches in diameter at breast height (d.b.h.) and located on public school property no tree removal permits are required.

## **I. Relevant Facts**

### **1) Background - History of Application:**

There are no other planning actions of record for this property.

## 2) Detailed Description of the Site and Proposal:

**Site:** The subject property, the Helman School site, consists of four tax lots located between Laurel and Helman Streets, immediately to the south of the Randy Street right-of-way and the Benner Subdivision, and north of the Laurel Street P.U.D. and the undeveloped Reynolds property. The northernmost parcel, Tax Lot 200, is trapezoidal in shape and has an area of roughly 3.83 acres according to county assessment data. Tax Lot 600 is a roughly rectangular lot located in the southwestern corner of the site and has an approximate area of 3.47 acres. Tax Lot 700 is irregularly shaped and located in the southeastern portion of the site, with an approximate area of 2.46 acres. Tax Lot 800 is the smallest of the four parcels, located at the southeast portion of the Helman School campus immediately adjacent to the Helman Street right-of-way and with an approximate area of 0.17 acres.

In total, the Helman campus consists of roughly ten acres with street frontage along Helman, Randy and Laurel Streets. The existing campus consists of five buildings clustered near the northeastern portion of the site on Tax Lot 200, with a large paved parking lot to the east of the buildings, along Helman Street and an adjacent lot just to the north accessed off of Randy Street. A third parking area for staff, which is currently not paved, is located on the northwestern portion of the site near the intersection of Randy Street and Cypress Circle. Athletic facilities including basketball and tennis courts are located on the central portion of the site, and the southern portion consists of athletic fields.

There are some areas along Laurel and Randy Streets where there are noticeable grade changes between the sidewalk and the adjacent fields, but overall, the site is generally flat with slopes of approximately only one to four percent down to the east, toward Helman Street and the nearby Ashland Creek drainage.

There are numerous existing mature trees on the site; these are located primarily along the Laurel and Helman Street frontages at the perimeter of the campus, and adjacent to the existing buildings. The application notes that six trees in the area of construction disturbance are proposed to be removed; because these trees are less than 18-inches in diameter at breast height (d.b.h.) and located on public school property, no tree removal permits are required.

**Site Review Proposal:** The applicants are requesting Site Review approval to construct an approximately 6,400 square foot gymnasium addition and a 5,010 square foot library addition on the northwest portion of the Helman School campus, near Randy Street. The project is part of a \$46.8 million bond package approved by Ashland voters in November of 2006.

The proposed gymnasium is to be located approximately 45 feet to the east of the existing sidewalks on Randy Street, and south of the existing gravel staff parking lot. The gymnasium is to be constructed of painted pre-cast concrete panels, with aluminum windows placed high on the north and south walls and “green screen” wire mesh

treatment on the north and west elevations. A canopy covering is provided along the entire south wall of the building, roofed in standing seamed metal, to provide a protected outdoor hardscape area. The building's height is identified as 28 feet eight inches from finished grade to the top of the parapet.

The proposed library is located to the east of the new gymnasium, between the new gymnasium and the existing gym, which will become a cafeteria and multi-purpose space. The library is shown as having a smooth-finished, integrally colored concrete masonry unit block veneer base along the east, south and north elevations, with the remainder of the building finished in stucco. The roof and a canopy on the north side of the building are to be covered with standing-seamed metal roofing, and a row of clerestory windows on the north wall will take advantage of views to the north. The building's height is shown at 19 feet three inches from grade to the peak of the low-pitched roof.

The application includes related interior modifications to existing campus buildings consisting of the conversion of the existing gymnasium to a cafeteria and multi-purpose space, and the expansion of administrative offices into what is now the library. Other sitework includes the paving of an existing gravel staff parking lot near the intersection of Randy Street and Cypress Circle which will continue to provide parking but is now proposed to serve as the bus drop-off site for the school as well.

## **II. Project Impact**

Looked at broadly, the application involves the addition of two detached buildings, a gymnasium and a library, to an established permitted use. Placement of the new buildings is largely in response to the existing campus lay-out. Parking will be confined to existing parking lots, with an unimproved staff parking lot at the northwest corner of the site to be improved to city standards with the proposal. The application notes that existing utilities are in place serving the existing buildings and includes plans to address drainage for the newly created impervious surfaces, and parking calculations have been provided showing that the increased parking demand brought about by the larger gymnasium will be addressed with 36 spaces located in the lots off of Helman Street, 17 spaces in the lot off of Randy Street, and seven on-street parking credits available along Helman and Randy Streets. While required bicycle parking is based upon the number of students, and the proposal is not associated in an increase in the student body, the applicants have proposed to provide an additional 12 covered bicycle spaces adjacent to the new gymnasium.

The Helman School site is zoned R-1-5 Single-Family Residential. Public schools are an outright permitted use within this zoning district, and the existing use is well established within the neighborhood. Within the Single-Family Residential zoning districts, construction other than a single unit on a single lot triggers Site Review, and because the proposal involves the construction of new structures in excess of 2,500 square feet the

application would typically be subject to administrative approval under a Type I procedure. Ashland's Site Design and Use Standards do not provide a unique set of standards under which public buildings are considered, so historically, public buildings including schools and city facilities located in residentially zoned areas have been reviewed according to the Basic Site Review Standards for Commercial, Employment and Industrial Development, which focus primarily on the orientation of buildings, landscaping, and the screening of parking areas. In reviewing the initial application submittal, Planning Staff identified three primary issues which we did not feel had been sufficiently addressed to administratively approve the application. Staff believed that these issues could be satisfactorily resolved through relatively simple design modifications and imposing certain conditions, and therefore opted to schedule the item for a public hearing before the Planning Commission rather than delaying the application.

The specific issues of concern identified with the initial submittal were: 1) that the Basic Site Review Standards were not addressed in the written findings provided, and that the proposed new gymnasium building's west elevation did not meet these standards with regard to its orientation to the street and sense of entry; 2) that the proposed relocation of the bus drop-off site to the Randy Street lot was not addressed in the submittal materials; and 3) that the initial submittal materials included a large metallic mural/wall graphic of the school mascot without addressing the need for a Conditional Use Permit to allow the proposed signage. Planning Staff has been in frequent communication with the applicants since these issues were raised; the applicants have been agreeable to making the requested modifications to address the Basic Site Review Standards, additional submittal materials have been provided, and revised conceptual drawings of the gymnasium building are expected before the hearing.

### **Gymnasium Building**

Staff's primary concern with the initial submittal had to do with the fact that the Basic Site Review Standards were not addressed in the written findings, and that the proposed new gymnasium building did not meet these standards. Specifically, the Basic Site Review Standards call for buildings to have their primary orientation to the street rather than to parking areas, with entrances oriented to the street and accessed from a public sidewalk. As initially proposed, the west elevation of the new gymnasium building which was oriented to the Randy Street streetscape and adjacent residential neighborhood had no entrance, and presented a 63-foot wall of pre-cast concrete panels which was unadorned but for three "green screen" wire mesh panels. Staff was concerned that the proposed building effectively turned its back upon its neighbors, and the fact that the wall was situated at the bend in Randy Street at the logical terminus of a motorist, bicyclist, or pedestrian's line of sight as they approach the curve proceeding east from Laurel Street made the wall an even more prominent feature of the streetscape. As such Planning Staff felt strongly that its orientation and relationship to the street were a critical element of the design that needed to be better established to satisfy the standards.

In revised findings provided by the applicants, they emphasized the importance of aesthetically and functionally unifying the new buildings with the existing campus, and also explained the importance of limiting access to the campus buildings and minimizing the number of entry points for the safety and security of students. Subsequent discussions with the applicants also suggested that their design team was concerned that the grade differential from the sidewalk to the building site was a limiting factor in establishing a strong street presence. Planning Staff recognized the validity of these concerns, but felt strongly that the Basic Site Review Standards could still be better addressed with relatively simple modifications to the design while remaining within the applicants' design parameters.

Staff believes that the entrance at the southwest corner of the building could easily be shifted to the west side, with appropriate entrance covering and hardscape/landscape treatments to provide a clear orientation and sense of entry toward Randy Street. Additionally, a change in the façade materials around the entrance could be further used to identify and emphasize the entrance, and the area between the west side of the building and Randy Street could be landscaped to emphasize a connection to the Randy Street streetscape. Public space could be extended to this elevation and defined through hardscape treatment, and a walkway extended to provide a pedestrian connection to the newly defined westerly entrance. In discussing these modifications with the applicants and representatives of the School District, Staff emphasized that we were not seeking to establish the sort of street presence that would be required in a commercial district, but rather seeking a respectful recognition of the adjacent neighborhood through a more connected relationship to the streetscape. The applicants were eager to explore alternative design options, and have indicated that they would be working to provide revised conceptual drawings prior to the hearing.

### **Changes to On-Site Bus Staging**

The proposal includes the relocation of the school's bus drop-off site from the existing paved parking lot off of Helman Street, on the east side of the site, to what is now a gravel parking lot for staff on Randy Street, at the northwest corner of the site. As part of the proposal, the existing gravel lot will be paved and landscaped to allow for 14 head-in and three parallel parking spaces and a separate bus drop-off lane, with vehicular circulation limited to one-way north-to-south travel and entry and exit onto Randy Street. At the pre-application stage, Staff requested that this alteration to the bus drop-off be thoroughly addressed in the submittal materials to clarify the impacts of a change in the bus circulation pattern from the higher order collector street (Helman) to the lesser order neighborhood street (Randy). The findings which were initially submitted were silent with regard to these changes. Subsequent submittals indicate that five buses serve the school in the morning and again in the afternoon, and explain that three of these five buses currently exit the campus via Randy Street while two exit via Helman Street. The proposed change here would have all five buses staging at the newly paved lot off of Randy Street, but the circulation pattern would continue to have two buses exiting to Helman Street.

In further discussing these changes with representatives of the School District, it was explained that the existing parking lot off of Helman Street currently accommodates school parking, parent drop-off and bus drop-off. A primary goal of the project was to separate the bus traffic from the parent drop-off area in order to reduce the safety hazard created by children darting between buses and into the vehicle travel lane in order to reach their parents' cars. The applicants felt that relocating the bus staging site would have a significantly lesser impact to the Randy Street neighborhood, particularly given the existing bus circulation onto Randy, than would relocating a significantly larger number of parent vehicles to this Randy Street lot. Staff would concur with this assessment, and believes that the proposed relocation of the bus staging site to Randy Street and the associated changes in bus circulation will improve student safety without significantly altering the impacts of bus traffic to the surrounding streets.

Neighbors to the proposed Randy Street bus staging site have questioned the impact of idling buses in this new location, and Staff raised this issue with the applicants and the District. It was explained that District policy is to minimize bus idling to comply with the city's idling ordinance; bus drivers are directed to turn off the buses wherever possible. In addition, the entire fleet of buses has been converted to use an ultra-low sulfur green diesel fuel, and high-efficiency mufflers have been installed. These measures have reduced the emissions of the buses by as much as 95 percent, and recently earned the District a certificate of appreciation from the Oregon Department of Environmental Quality.

### **Proposed Signage**

The final element of the initial application submittal that was of concern to Planning Staff had to do with a large aluminum dragon, the Helman School mascot, shown on the north wall of the proposed gymnasium building. The application materials initially asserted that this was an architectural enhancement to the building rather than a mural or wall graphic, and therefore argued that its placement was not subject to the city's Sign Regulations found in AMC 18.96.

For Planning Staff, the proposed placement of the dragon mascot on the gymnasium's north wall clearly falls within the definition of a sign found in AMC 18.96.020.023, which reads, "*Any identification, description, illustration, symbol or device which is placed or affixed directly or indirectly upon a building, structure, or land.*" The placement of signage within residential zoning districts is allowed only within very limited circumstances under the Sign Regulations, however the ordinance includes a provision in AMC 18.96.150 that "(g)overnmental agencies may apply for a Conditional Use to place a sign that does not conform to this Code when the Commission determines that, in addition to the criteria for a conditional use, the sign is necessary to further that agency's public purpose." Planning Staff has advised the applicants that a Conditional Use Permit would need to be obtained prior to the placement of signage, and a condition to this effect has been added below.

### **Tree Removal & Protection**

The application includes the removal of six trees. These include two six-inch diameter at breast height (d.b.h.) plums and three pines of diameters from three to nine inches located in or adjacent to the proposed building footprints, and one ten-inch d.b.h. spruce located near Helman Street adjacent to required excavation for a proposed storm drain line. Because these trees are less than 18-inches in diameter at breast height (d.b.h.), are not street trees, and are located on public school property no tree removal permits are required. A Tree Protection plan has been provided showing tree protection details for five trees to be retained in the area of proposed disturbance adjacent to the existing gravel parking lot.

### **III. Procedural - Required Burden of Proof**

The criteria for Site Review approval are listed in AMC 18.72.070 as follows:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.

### **IV. Conclusions and Recommendations**

Overall, Planning Staff is supportive of the request for Site Review approval and believe that the proposal does a commendable job of integrating the new additions into the existing campus with the least possible disturbance of the site and its overall circulation patterns by locating the new buildings in a manner that is respectful of the existing campus lay-out and keeping the option of future enclosure of the campus buildings open. Staff also believes that the proposal represents significant enhancements to the overall campus both in separating the bus staging area from the parent drop-off site, and in further developing a central courtyard space between the existing buildings and the new.

After review of the initial application submittal, Staff had concerns with three primary areas of the proposal: 1) the orientation to the street/sense of entry for the proposed new gymnasium building's west elevation; 2) the impacts of the proposed relocation of the bus drop-off site to Randy Street; 3) the need for a Conditional Use Permit to allow proposed signage in the form of a large metallic dragon-mascot wall graphic. After discussing these items at length with the applicants and receiving additional materials, Staff believes that the relocation of the bus drop-

off site has been sufficiently addressed, and that the issue of signage can be dealt with through a separate Conditional Use Permit application. The applicants have expressed a willingness to revise the architectural drawings of the gymnasium to address the remaining concern with the orientation and sense of entry, and have indicated that Staff's recommendations will be incorporated into revised conceptual elevations to be provided for the hearing. Planning Staff is confident that the outstanding concerns with the gymnasium building can be satisfactorily addressed, and would recommend approval of the application provided that the revised drawings presented at the hearing effectively implement the recommendations detailed above to better orient the gymnasium to the Randy Street streetscape.

Should the Planning Commission choose to approve the Site Review after review of the revised drawings and testimony provided during the hearing, Planning Staff would recommend that the following conditions be attached to the approval:

- 1) That all proposals of the applicants shall be conditions of approval unless otherwise modified herein.
- 2) That Conditional Use Permit approval shall be obtained prior to the installation of any new signage, including but not limited to the proposed dragon mascot mural/wall graphic on the new gymnasium building, or modification of any existing signage.
- 3) That all exterior lighting shall be directed on the property and shall not illuminate adjacent properties.
- 4) That prior to the submittal of a building permit:
  - A) The applicants shall provide revised elevation drawings addressing the west elevation of the new gymnasium building to include an entrance with appropriate covering, material changes, and hardscape/landscape treatments to provide a clearer orientation and sense of entry toward Randy Street.
  - B) The proposed buildings shall comply with the Standard A Solar Setback in accordance with AMC 18.70.040.A. The building permit submittals shall include identification of the highest shadow producing point(s), identification of the height of the shadow producing point(s) from natural grade, the solar setback measurement(s) called out to the north property line, and calculations in the ordinance-required format to demonstrate compliance.
  - C) Lot coverage calculations shall be provided which differentiate new and existing coverage areas, including buildings, walkways, athletic courts, parking areas and all other proposed lot coverage.
  - D) All easements shall be identified on the building permit submittals.
  - E) The applicants shall submit an electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment. This plan must be reviewed and approved by the Electric Department prior to the submittal of a building permit application. Transformers and cabinets shall be located in areas least visible from streets, while considering the access needs of the Electric Department.

- F) That exterior building materials and paint colors shall be compatible with the existing buildings and surrounding neighborhood, and sample exterior building colors and materials shall be provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used.
- 5) That prior to the issuance of a building permit:
- A) That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify the Site Review approval shall be submitted and approved prior to issuance of a building permit.
  - B) That final utility and drainage plans for the project shall be reviewed and approved by the Engineering, Building and Planning Divisions. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins.
  - C) Tree protection fencing shall be installed according to the approved Tree Protection Plan prior to any site work, storage of materials or permit issuance. The tree protection shall be chain link fencing six feet tall and installed in accordance with 18.61.200.B.
  - D) A Tree Verification Permit shall be applied for and approved by the Ashland Planning Division prior to permit issuance, site work, and/or storage of materials. The Verification Permit is to confirm that the trees to be removed are properly identified and to verify the installation of tree protection fencing for the trees to be retained.
  - E) Revised landscape, irrigation and tree protection plans shall be provided for the review and approval of the Staff Advisor. The revised plan shall incorporate: 1) the recommendations of the Ashland Tree Commission, where consistent with the Site Design and Use Standards; 2) calculations demonstrating that the parking lot landscaping for the proposed bus staging and staff parking lot satisfies the seven percent landscaping requirement; 3) irrigation system and maintenance watering schedule details to meet the Site Design and Use Standards Water Conserving Landscaping Guidelines and Policies irrigation requirements; 4) modifications to the proposed landscaping between the west side of the proposed gymnasium building and Randy Street to emphasize a connection to the streetscape, including landscape materials, hardscape treatment to extend public space to the westerly elevation, and a walkway connection from the new entrance to the sidewalk. All landscape and hardscape elements shall be installed in accordance with the approved plan prior to the issuance of a certificate of occupancy for the newly constructed buildings.

- 6) That prior to the issuance of a certificate of occupancy for the newly constructed buildings:
  - A) The requirements of the Fire Department, including fire flow, fire apparatus access, fire hydrant distance and clearance, approved addressing, key box(es) and required fire department connections (FDCs) for fire sprinklers, shall be satisfactorily addressed.
  - B) Street trees, 1 per 30 feet of street frontage, shall be installed along the Randy Street frontage. All street trees shall be chosen from the adopted Street Tree List and shall be installed in accordance with the specifications noted in Section E of the Site Design and Use Standards. The street trees shall be irrigated.
  - C) All service and equipment installation shall be installed according to Ashland Electric Department specifications prior to certificate of occupancy.
  - D) The proposed new bicycle parking facilities adjacent to the gymnasium shall be installed according to the requirements of AMC 18.92.040, inspected, and approved by the Staff Advisor.

# Conference Report

CLIENT: Ashland School District  
PROJECT: Helman Elementary School  
LOCATION: Ashland, Oregon  
JOB NO. 0715  
DATE: October 30, 2007



UNLESS WRITTEN OBJECTION IS RECEIVED WITHIN SEVEN DAYS, WE ASSUME THE STATEMENTS CONTAINED HEREIN ARE ACCEPTED

PRESENT: Wilkerson, Ogden, DiChiro, Molnar, Severson, DeCock

- I. PURPOSE OF MEETING: Post-Application review meeting, to address staff concerns with specific issues raised in the course of reviewing the findings and application (findings and drawing).
- II. GENERAL DISCUSSION:
- A. Concern by neighbors over potential impact on view of Mt. Ashland in the distance. Staff asked ORW to look into this, and provide additional info (3D model views) to staff.
  - B. Staff has concerns over how the building addresses the street (primary orientation). The rear door of the gym must be moved from the south side to the west side (around the corner), in order to create a sense of entry. This door will be covered with a canopy.
  - C. Staff confirmed that this change will not trigger a requirement to raise the floor elevation above the surrounding grade. Staff also confirmed that a DIRECT connection to the sidewalk isn't required; the current connection to the sidewalk can be modified to provide entry to the relocated doors.
  - D. Bill asked about bus traffic patterns on Randy Street. David noted that currently, three of the five buses exit onto Randy. Ken noted that the bus routes won't change; only the parking / queuing area is relocated. Juli noted that buses aren't allowed to idle excessively – as required by city ordinance, are equipped with special mufflers, and there are NO EMISSIONS due to the use of "green diesel" fuel. The district has won an award for their practices in this regard.
- III. NEXT STEPS:
- A. Juli expressed a desire to maintain the current (Nov 13) approval schedule. Derek noted that separate notices will be required for the Conditional Use Permit, so this will need to be added as a Condition of Approval for this application.
  - B. ORW to construct a 3D computer model of the site, to show how sightlines from neighboring properties are affected by the building.
  - C. /DLR will relocate the door from the south side to the west side (around the corner), in order to create a sense of entry. This door will be covered with a canopy.
  - D. David will revise the findings to address the bus emissions.

Route to:	<input type="checkbox"/> Ogden	<input type="checkbox"/> Conley	<input type="checkbox"/> DeCock/DiChiro
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ccemail

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**Findings of Fact  
and Conclusions of Law**

**SITE PLAN REVIEW APPROVAL**

**for**

**HELMAN ELEMENTARY SCHOOL  
LIBRARY & GYM ADDITION  
705 Helman St, Ashland, Oregon**

**(Approximately 6,400 sf gym addition and 5,010 square foot library addition to existing elementary school, with related interior modifications and sitework)**

Tax Lots 200, 600 & 700, Assessors Map Page 39-1E-04BC

OCTOBER 26, 2007 (revised)

Submitted to  
CITY OF ASHLAND  
PLANNING COMMISSION AND PLANNING DEPARTMENT

Submitted for  
ASHLAND SCHOOL DISTRICT #5

Prepared by  
DLR GROUP and ORW ARCHITECTURE

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Findings of Fact and Conclusions of Law  
SITE PLAN REVIEW APPROVAL  
HELMAN ELEMENTARY SCHOOL LIBRARY & GYM ADDITION

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## A. PROJECT DIRECTORY

- A.1 Owner** Ashland School District #5  
885 Siskiyou Boulevard  
Ashland, OR 97520
- A.2 Applicant** OgdenRoemerWilkerson Architecture  
2950 East Barnett Road  
Medford, OR 97504
- A.3 Consultants** DLR Group  
421 SW Sixth Avenue  
Suite 1212  
Portland, OR
- OgdenRoemerWilkerson Architecture  
2950 East Barnett Road  
Medford, OR 97504
- Polaris Land Surveying  
P.O. Box 459  
Ashland, OR 97520
- ZCS, Civil & Structural Engineering  
900 Klamath Ave  
Klamath Falls, OR 97601
- Covey Pardee Landscape Architecture  
295 E Main St #8  
Ashland, OR 97520
- A.4 Property Description** Tax Lot 700, Assessors Map Page 39-1E-10DB
- A.5 Current Zoning** R-1-5, Single Family Residential
- A.6 Current Use** Elementary School (permitted use)
- A.7 Proposed Use** Elementary School (no change or increase in use)
- A.8 Request** Site Plan Review for new commercial development  
Conditional Use Permit for new signage

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## **B. PROJECT NARRATIVE**

### **B.1 Site Description**

The subject property is situated at the southwest corner of Helman Street and Randy Street in Ashland, on the site of the current Helman Elementary School. The site is bounded by Helman on the east, Randy on the north, Randy and Laurel on the west, and residential development on the south. The site is almost flat. Currently, the site is used as an elementary school, including well-used recreational fields.

### **B.2 Proposed Development**

This project is part of a \$46.8 million bond package approved by Ashland voters in November 2006. The bond package included a variety of projects, including this one with a budget of \$2,732,010.

The Project proposes a new 6,400 square foot gymnasium and a new 5,000 square foot library. The buildings will be located in such a way that will reinforce and further develop the courtyard design of the existing campus yet maintain the potential for adaptation to a new enclosed plan in the future.

The existing staff parking area located in the Northwest corner of the property along Randy Street will be improved. The bus drop off area will be relocated to this portion of the site to eliminate the current conflict with parent drop off. The existing gymnasium will now be utilized as a cafeteria and multi-purpose space, Administration will now expand into the existing library.

### **B.3 Site Coverage**

Current survey data indicates that the site comprises 10.01, or approximately 436,446 square feet. The gross building area footprint is 45,097. Thus, the building occupies 10.3% of the site. The balance of the site area is devoted to public sidewalks, lawns and gardens, staff and visitor parking, and recreational fields.

### **B.4 Available Public Facilities, Services, and Utilities**

The project site is well served by a full range of public utilities and transportation services, including municipal water, sanitary sewer service, electrical service, natural gas, underground storm drainage. The existing utilities which currently serve the school campus are adequate to serve this small addition as well.

Since the project site is located at the southwest corner of Helman Street and Randy Street, it is well served by public streets. The site is also served by a public transportation bus stop located

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nearby. Students arrive at the site by car (dropped off), on foot, or by school bus. Five buses serve the school in the morning and again in the afternoon.

Currently, the buses queue up in the visitor / staff parking lot on Helman Street. This arrangement causes traffic congestion and creates a safety hazard for children trying to reach parents' cars in the same parking lot. The new design separates the bus traffic from the car traffic, to resolve this safety issue. As indicated on the site plan, bus traffic will be relocated to the Randy Street parking lot. Currently, three of the five buses exit the campus via Randy Street (versus Helman), so the traffic impact of two additional buses is negligible. Bus routes will remain unchanged, so no new bus routes will be directed onto Randy Street.

Traffic impact on Randy Street also will be mitigated through the district's current transportation policies, which include strict limitations on bus idling (in accordance with Ashland city ordinance) and the use of "green diesel" fuel, which produces no emissions.

## **B.5 Review Criteria**

This project must comply with the City of Ashland Land Use Ordinance (ALUO). This project also must comply with the applicable sections of the 'City of Ashland Site Design and Use Standards' for projects subject to Basic Site Review.

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## B. ORDINANCE REQUIREMENTS

### C.1 Definitions (18.08)

#### 18.08.515 Mural.

*A graphic design on a building which represents a person, place, scene or other artistic endeavor. This definition does not include architectural enhancement of a building facade; however, this would be subject to the procedural and substantive design review portion of the Site Review Chapter. (Ord. 2097 S2, 1980)*

**Finding:** The above-referenced definition has been used in these findings and in the attached drawings, to distinguish between a prohibited wall graphic and an allowable architectural enhancement of a building façade.

**Conclusion:** The applicant concludes that the large dragon motif shown on the building façade does indeed a mural.

### C.2 R-1 Single-Family Residential District Regulations (18.20)

#### 18.20.020 Permitted Uses.

*The following uses and their accessory uses are permitted outright:*

*E. Public schools, parks, and recreational facilities.*

**Finding:** Public schools are an outright permitted use in this zone.

**Conclusion:** The applicant concludes that the library addition to the elementary school included in this project is an outright permitted use in this zoning district.

#### 18.20.040 General Regulations.

*A. Minimum lot area: Basic minimum lot area in the R-1 zone shall be five thousand (5,000) square feet, except six thousand (6,000) square feet for corner lots. R-1 areas may be designed for seventy-five hundred (7,500), or ten thousand (10,000) square foot minimum lot sizes where slopes or other conditions make larger sizes necessary. Permitted lot sizes shall be indicated by a number following the R-1 notation which represents allowable minimum square footage in thousands of square feet, as follows:*

<i>R-1-5</i>	<i>5,000 square feet</i>
<i>R-1-7.5</i>	<i>7,500 square feet</i>
<i>R-1-10</i>	<i>10,000 square feet</i>

*D. Standard Yard Requirements: Front yards shall be a minimum of, 15 feet excluding garages. Unenclosed porches shall be permitted with a minimum setback of eight feet or the width of any existing public utility easement, whichever is greater, from the front property line. All garages accessed from the front shall have a minimum setback of 20' from the front property line; side yards, six feet; the side yard of a corner lot abutting a public street shall have a ten*

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*foot setback; rear yard, ten feet plus ten feet for each story in excess of one story. In addition, the setbacks must comply with Chapter 18.70 which provides for Solar Access. (Ord. 2097 S5, 1980; Ord. 2121 Se, 1981, Ord. 2752, 1995)*

- E. Maximum Building Height: No structure shall be over thirty-five (35) feet or two and one-half (2 1/2) stories in height, whichever is less. Structures within the Historic District shall not exceed a height of 30 feet.*
- F. Maximum Coverage: Maximum lot coverage shall be fifty (50%) percent in an R-1-5 District, forty-five (45%) percent in an R-1-7.5 District, and forty (40%) percent in an R-1-10 District.*

**Finding:** The lot area is approximately 436,446 square feet. As shown on the site plan, the front, side, and rear yard setbacks are well in excess of 20’.

**Finding:** The building height is approximately 28’8”, measured from the top of the parapet to the finish grade.

**Conclusion:** The applicant concludes that the project meets the above requirements regarding lot size, yard requirements, maximum height, and maximum coverage.

### **C.3 Tree Preservation and Protection (18.61)**

#### Section 18.61.035

##### *Exempt Tree Removal Activities:*

*The following activities are exempt from the requirement for tree removal permits:*

*E. Removal of trees less than 18" DBH on any public school lands, Southern Oregon University, and other public land; but excluding Heritage trees and street trees within the public right of way.*

**Finding:** The project is located on public school lands.

**Finding:** The project site contains six existing trees that are less than 18” DBH (see Exhibit 2, Drawing L1.2 – Tree Protection Plan). These trees fall within the footprint of the proposed new building and reconfigured parking area, are not street trees, and are not within the public way.

**Conclusion:** The applicant concludes that a Tree Removal Permit is **NOT** required for removal of trees as shown on the attached Preliminary Landscape Plan and Tree Protection & Removal Plan prepared by Covey Pardee Landscape Architects.

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#### C.4 Solar Access (18.70)

##### 18.70.010 Purpose and Intent.

*The purpose of the Solar Access Chapter is to provide protection of a reasonable amount of sunlight from shade from structures and vegetation whenever feasible to all parcels in the City to preserve the economic value of solar radiation falling on structures, investments in solar energy systems, and the options for future uses of solar energy.*

**Finding:** The library and gym additions are both located on the south side of the existing buildings, and well away from the property lines.

**Conclusion:** The applicant concludes that solar access to adjacent properties will not be impeded, due to the size and location of the library and gym additions on the site.

#### C.5 Site Design and Use Standards (18.72)

##### 18.72.030 Application

*Site design and use standards shall apply to all zones of the city and shall apply to all development indicated in this Chapter, except for those developments which are regulated by the Subdivisions (18.80), the Partitioning (18.76), Manufactured Housing (18.84) and Performance Standards (18.88).*

**Finding:** This project is located in a R 1-5 (Single-Family Residential) zone.

**Conclusion:** The applicant concludes that the Site Use and Design Standards apply to this project.

##### 18.72.070 Criteria for Approval

*The following criteria shall be used to approve or deny an application:*

- A. *All applicable City ordinances have been met or will be met by the proposed development.*
- B. *All requirements of the Site Review Chapter have been met or will be met.*
- C. *The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.*

**Finding:** The proposed development meets or will meet all applicable City ordinances, applicable requirements of the Site Review Chapter, and applicable portions of the Site Design and Use Standards, as outlined in items A through C above.

- D. *That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-*

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*of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.*

**Finding:** Adequate capacity of City facilities and utilities are provided to the project site, as shown on the attached Striping & Utility Plan prepared by ZCS Engineering.

**Finding:** The utility service requirements for the existing building will not be impacted by the proposed addition.

**Conclusion:** The applicant concludes that this application meets all the criteria required for approval of the application.

*SECTION 18.72.110 Landscaping Standards.*

A. *Area Required. The following areas shall be required to be landscaped in the following zones:*

*R-1 - 45% of total developed lot area*

B. *Location. Landscaping shall be located so that it is visible from public right-of-way or provide buffering from adjacent uses. Landscaping shall be distributed in those areas where it provides for visual and acoustical buffering, open space uses, shading and wind buffering, and aesthetic qualities.*

C. *Irrigation. All landscaping plans shall either be irrigated or shall be certified that they can be maintained and survive without artificial irrigation. If the plantings fail to survive, the property owner shall replace them.*

**Finding:** As shown on the attached Preliminary Landscape Plan, the landscaping has been located according to criterion B above.

**Finding:** As shown on the attached Preliminary Landscape Plan, an irrigation system will be installed.

**Conclusion:** The applicant concludes that this project complies with the criteria for landscaping outlined above.

**C.6 Off-Street Parking (18.92)**

18.92.020 Automobile Parking Spaces Required

*Uses and standards are as follows:*

*D. Institutional and Public Uses. For institutional and public uses the following automobile parking spaces are required.*

*7. Schools, elementary and junior high. One and one-half space per classroom, or the requirements for public assembly areas as set forth herein, whichever is greater.*

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**Finding:** The existing building contains 16 classrooms, which would require 24 parking spaces.

**Finding:** The size of the new gymnasium is 4800 square feet. Using an occupant load of 240 (based on 20 square feet per person for assembly-type uses), 60 parking spaces are required.

**Finding:** For this project, the number of required parking spaces is based on the occupant load of the new gymnasium, since this number is greater than the parking space requirement based on the number of classrooms.

18.92.030 Disabled Person Parking Places.

*The total number of disabled person parking spaces shall comply with the following:*

<i>Total in Parking Lot</i>	<i>Required Minimum Number of Accessible Spaces</i>
<i>1 to 25</i>	<i>1</i>
<i>26 to 50</i>	<i>2</i>
<i>51 to 75</i>	<i>3</i>

**Finding:** Based on a total of 60 parking spaces provided, three accessible spaces are required.

**Finding:** The existing parking lot at the corner of Helman and Randy Streets contains three parking spaces designated for handicapped accessible use.

**Conclusion:** The applicant concludes that the required number of accessible parking spaces have already been provided on the site.

18.92.040 Bicycle Parking

*A. All uses, with the exception of detached single-family residences and uses in the C-1-D zone, shall provide a minimum of two sheltered bike parking spaces.*

*C. In addition, all uses which require off street parking, except as specifically noted, shall provide one bicycle parking space for every 5 required auto parking spaces. Fractional spaces shall be rounded up to the next whole space. Fifty percent of the bicycle parking spaces required shall be sheltered from the weather. All spaces shall be located in proximity to the uses they are intended to serve. (Ord. 2697 S1, 1993)*

**Finding:** Based on a total of 60 parking spaces required by the enlarged gymnasium, 12 bicycle parking spaces are required, including 6 covered spaces.

**Finding:** The attached Site Plan indicates 12 new bicycle parking spaces adjacent to the new gymnasium entrance, including six spaces that are covered by the building's overhang.

**Conclusion:** The applicant concludes that this project provides the required amount of bicycle parking based on the parking required for the enlarged gymnasium.

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*E. Elementary, Junior High, Middle and High Schools shall provide one sheltered bicycle parking space for every five students.*

**Finding:** Based on a maximum population of 330 students, 66 bicycle parking spaces are required. This project does not increase the number of students at the school.

**Finding:** There are currently 68 existing bicycle parking spaces located throughout the site, as noted on the attached Site Plan.

**Conclusion:** The applicant concludes that the existing bicycle parking spaces meet the bicycle parking requirement based on the student population.

18.92.050 Compact Car Parking

*Up to 50% of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 x 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."*

**Finding:** As indicated on the various floor plans included in Exhibit 2, parking for compact cars has been provided the size and quantities outlined above.

**Conclusion:** The applicant concludes that the design of the project meets the requirements of this section.

18.92.070 Automobile Parking Design Requirements

- A. *Size and Access. All required parking areas shall be designed in accordance with the parking layout chart at the end of this Chapter. Parking spaces shall be a minimum of 9 x 18 feet, except that 50% of the spaces may be compact spaces in accord with 18.92.050 and shall have a 22 foot back-up space except where parking is angled.*
- B. *Driveways and Turn-Arounds. Driveways and turn-arounds providing access to parking areas shall conform to the following provisions:*
1. *A driveway for a single dwelling shall have a minimum width of nine feet, and a shared driveway serving two units shall have a width of 12 feet.*
  2. *Parking areas of more than seven parking spaces per lot shall be provided with adequate aisles or turn-around areas so that all vehicles may enter the street in a forward manner.*
  3. *Parking areas of more than seven parking spaces shall be served by a driveway 20 feet in width and constructed to facilitate the flow of traffic on or off the site, with due regard to pedestrian and vehicle safety, and shall be clearly and permanently marked and defined. Parking areas of seven spaces or less shall be served by a driveway 12 feet in width.*
- D. *Vision Clearance. No signs, structures or vegetation in excess of two and one-half feet in height shall be placed in the vision clearance area. The vision clearance area is the triangle formed by a line connecting points 25 feet from the intersection of property lines. In the case of an intersection involving an alley and a street, the triangle is formed by a*

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*line connecting points ten (10) feet along the alley and 25 feet along the street. When the angle of intersection between the street and the alley is less than 30 degrees, the distance shall be 25 feet. No signs, structures or vegetation or portion thereof shall be erected within ten (10) feet of driveways unless the same is less than two and one-half feet in height. The vision clearance standards established by this section are not subject to the Variance section of this title.*

- E. *Development and Maintenance. The development and maintenance as provided below, shall apply in all cases, except single-family dwellings.*
1. *Paving. All required parking areas, aisles, turn-arounds and driveways shall be paved with concrete, asphaltic or comparable surfacing, constructed to standards on file in the office of the City Engineer.*
  2. *Drainage. All required parking areas, aisles and turn-arounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.*
  3. *Driveway approaches. Approaches shall be paved with concrete surfacing constructed to standards on file in the office of the City Engineer.*
  4. *Marking. Parking lots of more than seven spaces shall have all spaces permanently and clearly marked.*
  5. *Wheel stops. Wheel stops shall be a minimum of four inches in height and width and six feet in length. They shall be firmly attached to the ground and so constructed as to withstand normal wear. Wheel stops shall be provided where appropriate for all spaces abutting property lines, buildings, landscaping, and no vehicle shall overhang a public right-of-way.*
  6. *Walls and Hedges.*
    - a. *Where parking abuts upon a street, a decorative masonry wall or evergreen hedge screen of 30-42 inches in height and a minimum of 12" in width shall be established parallel to and not nearer than two feet from the right-of-way line. Screen planting shall be of such size and number to provide the required screening within 12 months after installation. The area between the wall or hedge and street line shall be landscaped. All vegetation shall be adequately maintained by a permanent irrigation system, and said wall or hedge shall be maintained in good condition. The required wall or screening shall be designed to allow for free access to the site and sidewalk by pedestrians.*
    - b. *In all zones, except single-family zones, where parking facilities or driveways are located adjacent to residential or agricultural zones, school yards, or like institutions, a sight-obscuring fence, wall, or evergreen hedge not less than five feet, nor more than six feet high shall be provided on the property line as measured from the high grade side. Said wall, fence or hedge shall be reduced to 30 inches within required setback area, or within 10 feet of street property lines, and shall be maintained in good condition. Screen plantings shall be of such size and number to provide the required screening within 12 months after installation. Adequate provisions shall be made to protect walls, fences or plant materials from being damaged by vehicles using said parking areas.*

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7. *Landscaping. In all zones, all parking facilities shall include landscaping to cover not less than 7% of the area devoted to outdoor parking facilities, including the landscaping required in subdivision 6(a) above. Said landscaping shall be uniformly distributed throughout the parking area, be provided with irrigation facilities and protective curbs or raised wood headers. It may consist of trees, plus shrubs, ground cover or related material. A minimum of one tree per seven parking spaces is required.*
8. *Lighting of parking areas within 100 feet of property in residential zones shall be directed into or on the site and away from property lines such that the light element shall not be directly visible from abutting residential property.*

**Finding:** As indicated on the attached Site Plan and Planting Plan, the size, layout, and location of the parking spaces, drive aisles, and turn-arounds conforms to the requirements of criteria A and B above.

**Finding:** As indicated on the attached Site Plan and Planting Plan, vision clearance areas will be maintained per criterion D above. Signs, structures, and vegetation within 10 feet of driveways and vehicular entrances will be less than 2 ½ feet in height.

**Conclusion:** The applicant concludes that the design of the project meets the requirements of this section.

18.92.090 Alterations and Enlargements.

*The required parking facilities shall be constructed when an existing building or dwelling is altered or enlarged by the addition or creation of guest rooms or dwelling units, or when a use is intensified by the addition of floor space, seating capacity, or change in use.(Ord. 2659, 1991; Ord. 2777, 1996)*

**Finding:** This project – the replacement of the existing gym with a larger gym - increases the occupant load of the gymnasium space.

**Finding:** This project requires the relocation and reconfiguration of the existing staff parking lot on Randy Street.

**Conclusion:** The applicant concludes that the ‘required parking facilities’ must to be provided in the reconfigured parking lot, since the existing use will be intensified by the enlarged gym.

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## C.7 Sign Regulations (18.96)

### 18.96.040 Prohibited Signs.

L. No wall graphics shall be permitted.

**Finding:** Based on the definition of 'mural' and 'wall graphic' in section 18.08 above, the dragon motif shown on the attached Building Elevations constitutes a mural, or wall graphic.

### SECTION 18.96.150 Governmental Signs.

*Governmental agencies may apply for a Conditional Use to place a sign that does not conform to this Code when the Commission determines that, in addition to the criteria for a conditional use, the sign is necessary to further that agency's public purpose. (Ord. 2557, 1985; Ord. 2440, 1988)*

**Finding:** Each of the Ashland public schools has a school mascot (e.g. Grizzly, Bobcat), which plays a prominent role in the school's identity. The mascot of Helman Elementary School is the DRAGON.

**Finding:** The 'dragon motif' mural promotes the school's role as a learning environment, fosters a sense of unity within the student body, reinforces the school's function as a focal point in the community, all of which are public purposes of the school district.

**Conclusion:** The applicant concludes that this mural meets the criteria for a conditional use, and requests approval of a Conditional Use Permit to allow installation of the mural on the building facade.

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## D. SITE DESIGN AND USE STANDARDS

### D.1 Ordinance Landscaping Requirements (II-A)

#### Ordinance Landscaping Requirements

The following percentages of landscaping are required for all properties falling under the Site Design and Use Standards.

<u>Zone</u>	<u>% Landscaping</u>
R-1-3.5	45%
R-2	35%
R-3	25%
C-1	15%
C-1-D	10%
E-1	15%
M-1	10%

These percentages are the minimum required. At times, more landscaping is required to meet the needs of other sections of the Site Review Ordinance, such as screening of parking areas, landscaping of setback areas and providing usable outdoor space. In general, all areas which are not used for building or parking areas are required to be landscaped. You should also be aware that, as a condition of approval of your project, you will be required to submit a site and species specific landscape plan to the Planning Division for Staff Advisor approval.

**Finding:** The project site is located within an R 1-5 zone.

**Conclusion:** The applicant concludes that this requirement is inapplicable. However, landscaped areas will be provided as required by Section 18.72 (above).

### D.2 Basic Site Review Standards (II-C-1)

*Approval Standard: Development in all commercial and employment zones shall conform to the following development standards:*

**Finding:** The project site is located within an R 1-5 zone, not in a commercial or employment zone.

**Finding:** The Pre-Application Conference Comment Sheet for this project dated July 18, 2007 contained a note stating specifically that the Basic Site Review Standards must be addressed in these findings.

**Conclusion:** Based on direction from staff, the applicant concludes that the Basic Site Review Standards must be addressed as part of this project, even though the project's location in a residential zone would otherwise exempt it from this requirement.

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II-C-1a) *Orientation and Scale*

1) *Buildings shall have their primary orientation toward the street rather than the parking area. Building entrances shall be oriented toward the street and shall be accessed from a public sidewalk. Public sidewalks shall be provided adjacent to a public street along the street frontage.*

**Finding:** The project site is surrounded by streets on three sides. The existing buildings are oriented toward ***Helman, which is the higher order street.*** Randy Street wraps the site on one side and the rear.

**Finding:** This project is part of a multi-building "California style" school campus with a ***primary orientation to Helman Street.*** The buildings are connected by exterior breezeways and courtyards which serve to unify the buildings, both aesthetically and functionally.

**Finding:** For the safety and security of the students, ***all visitors to the school must enter through the main entrance,*** which will remain in its existing location on Helman Street. Additionally, entrances should be placed so that they are visible from within the school campus, not 'tucked' away around the corner.

**Finding:** Sidewalks have been provided to the street and along the street frontage, as required for emergency egress to the public way.

**Conclusion:** The applicant concludes that the new gym and library, as part of the overall campus, have their primary orientation to the front of the site and the higher order street.

2) *Buildings that are within 30 feet of the street shall have an entrance for pedestrians directly from the street to the building interior. This entrance shall be designed to be attractive and functional, and shall be open to the public during all business hours.*

**Finding:** Both the library and gym addition are more than 30 feet from the street.

**Conclusion:** The applicant concludes that this standard does not apply to this project.

3) *These requirements may be waived if the building is not accessed by pedestrians, such as warehouses and industrial buildings without attached offices, and automotive service uses such as service stations and tire stores.*

**Finding:** Unlike the recreational fields behind the school (which are used by the general public), ***the gym is not accessed by pedestrians,*** but is for student use only. At the beginning of the school day, students do not arrive at the gym, but at their individual classrooms, as well as the cafeteria.

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**Finding:** Throughout the remainder of the campus, *exterior doors are not used as entrances to the school*, but are used to gain access to individual classrooms. The breezeways and courtyards downplay the significance of the numerous exterior doors, to avoid confusion with the main entrance.

**Finding:** The library and gym additions are designed to complement the existing campus-style development. Exterior breezeways and courtyards not only connect the new buildings to the existing ones, but also integrate their functions with the remainder of the campus, including the main entrance on Helman Street.

**Conclusion:** The applicant concludes that the campus' primary orientation to Helman Street has been appropriately maintained. Since the gym is not accessed by pedestrians or the general public, an orientation to the street and a strong sense of entry are not required, would not be appropriate or in keeping with the rest of the school's design, and could hinder student security and safety.

*II-C-1b) Streetscape*

*One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street.*

*II-C-1c) Landscaping*

- 1) Landscaping shall be designed so that 50% coverage occurs after one year and 90% coverage occurs after 5 years.*
- 2) Landscaping design shall utilize a variety of low water use and deciduous and evergreen trees and shrubs and flowering plant species.*
- 3) Buildings adjacent to streets shall be buffered by landscaped areas at least 10 feet in width, except in the Ashland Historic District. Outdoor storage areas shall be screened from view from adjacent public rights-of-way, except in M-1 zones. Loading facilities shall be screened and buffered when adjacent to residentially zoned land.*
- 4) Irrigation systems shall be installed to assure landscaping success.*
- 5) Efforts shall be made to save as many existing healthy trees and shrubs on the site as possible.*

*II-C-1d) Parking*

- 1) Parking areas shall be located behind buildings or on one or both sides.*

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- 2) *Parking areas shall be shaded by deciduous trees, buffered from adjacent non-residential uses and screened from non-residential uses.*

*II-C-1e) Designated Creek Protection*

- 1) *Designated creek protection areas shall be considered positive design elements and incorporated in the overall design of a given project.*
- 2) *Native riparian plant materials shall be planted in and adjacent to the creek to enhance the creek habitat.*

*II-C-1f) Noise and Glare*

*Special attention to glare (AMC 18.72.110) and noise (AMC 9.08.170(c) & AMC 9.08.175) shall be considered in the project design to insure compliance with these standards.*

*II-C-1g) Expansions of Existing Sites and Buildings*

*For sites which do not conform to these requirements, an equal percentage of the site must be made to comply with these standards as the percentage of building expansion, e.g., if a building area is expanded by 25%, then 25% of the site must be brought up to the standards required by this document.*

### **D.3 Parking Lot Landscaping and Screening Standards (II-D)**

*Approval Standard: All parking lots, which for purposes of this section include areas of vehicle maneuvering, parking, and loading shall be landscaped and screened as follows:*

*II-D-1) Screening at Required Yards*

- 1) *Parking abutting a required landscaped front or exterior yard shall incorporate a sight obscuring hedge screen into the required landscaped yard.*
- 1) *The screen shall grow to be at least 36 inches higher than the finished grade of the parking area, except for required vision clearance areas.*
- 2) *The screen height may be achieved by a combination of earth mounding and plant materials.*
- 3) *Elevated parking lots shall screen both the parking and the retaining wall.*

*II-D-2) Screening Abutting Property Lines*

- 1) *Parking abutting a property line shall be screened by a 5 feet landscaped strip. Where a buffer between zones is required, the screening shall be incorporated into the required buffer strip, and will not be an additional requirement.*

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II-D-3) Landscape Standards:

- 1) *Parking lot landscaping shall consist of a minimum of 7% of the total parking area plus a ratio of 1 tree for each seven parking spaces to create a canopy effect.*
- 2) *The tree species shall be an appropriate large canopied shade tree and shall be selected from the street tree list to avoid root damage to pavement and utilities, and damage from droppings to parked cars and pedestrians.*
- 3) *The tree shall be planted in a landscaped area such that the tree bole is at least 2 feet from any curb or paved area.*
- 4) *The landscaped area shall be planted with shrubs and/or living ground cover to assure 50% coverage within 1 year and 90% within 5 years. Landscaped areas shall be evenly distributed throughout the parking area and parking perimeter at the required ratio.*
- 5) *That portion of a required landscaped yard, buffer strip or screening strip abutting parking stalls may be counted toward required parking lot landscaping but only for those stalls abutting landscaping as long as the tree species, living plant material coverage and placement distribution criteria are also met. Front or exterior yard landscaping may not be substituted for the interior parking stalls.*

II-D-6) Other Screening

- 1) *Other screening and buffering shall be provided as follows:*

*Light and Glare Screen: Artificial lighting shall be so arranged and constructed as to not produce direct glare on adjacent residential properties or streets.*

**Finding:** All lighting will be engineered with the appropriate screening and orientation as to prevent direct glare on adjacent residential properties and streets.

**D.4 Street Tree Standards (II-E)**

*APPROVAL STANDARD: All development fronting on public or private streets shall be required to plant street trees in accordance with the following standards and chosen from the recommended list of street trees found in this section.*

II-E-1) Location for Street Trees

- 1) *Street trees shall be located behind the sidewalk except in cases where there is a designated planting strip in the right of-way, or the sidewalk is greater shall include irrigation, root barriers, and generally conform to the standard established by the Department of Community Development.*

**Finding:** The street trees are located behind the existing sidewalk.

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II-E-2) Spacing, Placement, and Pruning of Street Trees

*All tree spacing may be made subject to special site conditions which may, for reasons such as safety, affect the decision. Any such proposed special condition shall be subject to the Staff Advisor's review and approval. The placement, spacing, and pruning of street trees shall be as follows:*

- a) *Street trees shall be placed the rate of one tree for every 30 feet of street frontage. Trees shall be evenly spaced, with variations to the spacing permitted for specific site limitations, such as driveway approaches.*

**Finding:** The existing street trees exceed this requirement (spacing is less than 30 feet).

- b) *Trees shall not be planted closer than 25 feet from the curb line of intersections of streets or alleys, and not closer than 10 feet from private driveways (measured at the back edge of the sidewalk), fire hydrants, or utility poles.*

**Finding:** The existing street trees are closer than 25 feet to the curb in some cases. New street trees are located in accordance with the above standard.

- c) *Street trees shall not be planted closer than 20 feet to light standards. Except for public safety, no new light standard location shall be positioned closer than 10 feet to any existing street tree, and preferably such locations will be at least 20 feet distant.*

**Finding:** All new street trees shall be located at least 20 from existing light standards.

- d) *Trees shall not be planted closer than 2½ feet from the face of the curb except at intersections where it shall be 5 feet from the curb, in a curb return area.*

**Finding:** All new street trees shall be located at least 5 feet from the face of the curb.

- e) *Where there are overhead power lines, tree species are to be chosen that will not interfere with those lines.*

**Finding:** All tree species will be selected to avoid interference with existing overhead utility lines.

- f) *Trees shall not be planted within 2 feet of any permanent hard surface paving or walkway. Sidewalk cuts in concrete for trees shall be at least 10 square feet, however, larger cuts are encouraged because they allow additional air and water into the root system and add to the health of the tree. Space between the tree and such hard surface may be covered by permeable non-permanent hard surfaces such as grates, bricks on sand, or paver blocks.*

**Finding:** All new street trees are located more than 2 feet from the sidewalk.

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- g) *Trees, as they grow, shall be pruned to provide at least 8 feet of clearance above sidewalks and 12 feet above street roadway surfaces.*

**Finding:** Trees will be pruned to maintain required minimum clearances above sidewalks and roadways.

- h) *Existing trees may be used as street trees if there will be no damage from the development which will kill or weaken the tree. Sidewalks of variable width and elevation may be utilized to save existing street trees, subject to approval by the Staff Advisor.*

**Finding:** The existing trees which are to remain will be protected during construction to ensure their continued viability. Existing trees which are already damaged or diseased will be removed and replaced.

II-E-3) Replacement of Street Trees

*Existing street trees removed by development projects shall be replaced by the developer with those from the approved street tree list. The replacement trees shall be of size and species similar to the trees that are approved by the Staff Advisor.*

**Finding:** The existing trees to be removed will be replaced with trees from the approved list, and will be of an approved size and species.

II-E-4) Recommended Street Trees

*Street trees shall conform to the street tree list approved by the Ashland Tree Commission.*

**Finding:** The trees shown on this project were selected from the approved street tree list.

**Conclusion:** The applicant concludes that the street trees shown on this project conform to all applicable street tree development standards.

## E. SUMMARY CONCLUSIONS

Based on the foregoing findings of fact and conclusions of law, the applicant concludes that this application for Site Plan Review Approval has satisfied all of the relevant substantive standards and criteria contained in the Ashland Land Use Ordinance and the Ashland Site Design and Use Standards.

The applicant ultimately concludes that, based on the foregoing findings of fact and conclusions of law, the application for Site Plan Review Approval complies with all requirements of the City of Ashland and of the State of Oregon.

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## **Valley&State**



**Ashland school district bus mechanic Donnie Hayes works** on the exhaust system of a bus that was retrofitted with a new type of filter system that burns cleaner.

Orville Hector | Daily Tidings

### **DEQ recognizes city school bus green upgrades**

*Particulate emissions reduced again*

**By Alan Panebaker**  
Ashland Daily Tidings

The Ashland School District has been going green for more than two years, and yesterday, the Oregon Department of Environmental Quality offered a certificate of appreciation for reducing emissions by more than 90 percent on some district school buses.

"Now we just need to get more kids to ride the bus," Ashland School Board chair Heidi Parker said.

The idea to reduce school bus emissions came as Parker, the new chair, was elected in 2003. After barely missing a deadline for a government grant that would help with the cost of converting buses to biodiesel, Parker and business manager at the time Loren Lumen began investigating other options after a student from the Wilderness Charter School came to the school board with the idea of using biodiesel in 2004. Then Lumen looked into the idea of using green diesel — ultra low sulfur diesel. Although more expensive, the green diesel reduced particulate emissions by about half. Ashland school buses started using the new fuel in 2004 after working with the local DEQ. In the last three months, the district got a \$69,714 Oregon Clean School Bus Grant and retro-fitted six school buses with new mufflers that heat exhaust to 200 degrees Celsius and filter the particulate matter to a point where the machine burns as cleanly as natural gas. The district also bought a brand new bus with the upgraded technology. The new technology coupled with green diesel reduces emissions on those buses by up to 95 percent.

Ashland School District mechanic Donnie Hayes explained how a catalyst in the new mufflers in some buses contains precious metals that filter the ultra-heated exhaust and break down the particulates, so the pollution they cause is nearly unrecognizable.

"The air quality issue is in the entire Rogue Valley, not just Ashland," Stephanie Hallock, director of the Oregon Department of Environmental Quality said. "It's always been a problem, but now the area is in compliance with state regulations for particulate matter."

Air pollution problems in the Rogue Valley have traditionally been worse than most other areas in the state because of the geographic location in a valley, Hallock said, and in recent years growth and more cars have added to the problem. So when the school district, Rogue Valley Council of Governments and Rogue Disposal and Recycling upgraded their diesel engines, the DEQ offered them all appreciation for their efforts.

The shiny new mufflers on some district school buses may be reducing emissions, but the real issue for transportation in the district seems to be lack of students in the buses. District transportation coordinator Carol Savage said approximately 500 students in the district ride the bus to school on any given day. There are about 3,000 students in the district.

"A lot of people take their kids to school because it's convenient," Savage said. "Through middle school and elementary school, we'd like to see more ridership."

Getting high school kids to ride the bus has always been more difficult because of social factors, Savage said.

The district offers free transportation to students who live more than a mile away from their school and added a new route after Lincoln Elementary closed.

*Staff writer Alan Panebaker can be reached at 482-3456 x 227 or [apanebaker@dailytidings.com](mailto:apanebaker@dailytidings.com).*





0 25 50 100 Feet

*Property lines are for reference only, not scaleable*



**GENERAL CIVIL NOTES:**

- ALL WORK & MATERIALS SHALL CONFORM TO CURRENT OREGON U.P.C. 2002 STATE OF OREGON APWA / ODOT STANDARD SPECIFICATIONS FOR CONSTRUCTION & ALL APPLICABLE STATE, CITY, AND COUNTY REGULATIONS AND STANDARDS. CONTACT ENGINEER FOR DIRECTIVE IN THE EVENT OF CONFLICTING STANDARDS.
- ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE COORDINATED WITH THE GOVERNING AGENCIES INSPECTOR AND SHALL CONFORM TO THAT AGENCIES CURRENT ENGINEERING STANDARD SPECIFICATIONS & DETAILS.
- THE GENERAL CONTRACTOR AND ALL THEIR AFFILIATES SHALL VERIFY ALL DIMENSIONS, ELEVATIONS & LOCATIONS PRIOR TO CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES.
- ALL GRADE SURVEYING AND HORIZONTAL LAYOUT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL EXISTING SITE UTILITIES IDENTIFIED ON THIS PLAN ARE NOT INTENDED TO BE EXACT OR COMPLETE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IDENTIFY ALL UTILITIES AND PROTECT AS REQUIRED DURING THE COURSE OF CONSTRUCTION.
- CONTRACTOR SHALL NOTIFY ALL APPLICABLE REGULATORY AGENCIES AND UTILITY COMPANIES 48 HRS PRIOR TO BEGINNING WORK.
- ALL SITE EXCAVATION, TRENCH BACK FILL, PAVING LOT SUB-GRADE, FLAT WORK SUB-GRADE, CONSTRUCTION REQUIREMENTS, ETC., SHALL BE AS NOTED IN THE SITE PREPARATION NOTES.
- ALL SITE CONCRETE SHALL BE  $f_c = 3500$  psi @ 28 DAYS, 6% EXTRAIRAD AIR, 4" SLUMP (U.N.O.).
- ALL UTILITY SERVICES SHALL BE INSTALLED PER THE RESPECTIVE UTILITY CODES & STANDARDS.
- ALL SERVICES SHALL BE ADEQUATELY MARKED AS NOTED AS TO IDENTIFY THE SIZE, TYPE, & DEPTH OF THE SERVICE.
- ALL SERVICES SHALL BE PLUGGED AS NOTED TO ADEQUATELY ENSURE THAT NO FOREIGN MATERIALS ENTER THE LINE.
- CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE SIZE, TYPE, DEPTH OF MAIN, TYPE OF CONNECTION, AT MAKE, INSTALLATION DATE, LOCATION & SKETCH OF ALL UTILITY SERVICE INSTALLATIONS.
- CONTRACTOR SHALL OBTAIN ALL APPLICABLE PERMITS PRIOR TO CONSTRUCTION.
- ALL WATER LINES SHALL BE AS SPECIFIED ON THE PLAN SET.
- ALL SANITARY SEWER WASTE LINES SHOWN OUTSIDE THE BUILDING SHALL BE PVC SEWER PIPE CONFORMING TO ASTM D 3034 - SDR 35 WITH GASKET JOINTS.
- SANITARY LINES SHALL BE TESTED TO PASS A LOW PRESSURE AIR TEST OR WATER TEST CONFORMING TO PLUMBING CODE SPECIFICATIONS PRIOR TO FINAL ACCEPTANCE. ALL PARTS OF THE SYSTEM SHALL BE CLEANED PRIOR TO FINAL ACCEPTANCE. THE CONTRACTOR SHALL NOT ALLOW ANY FOREIGN MATERIAL TO ENTER THE EXISTING SYSTEM. THE CONTRACTOR SHALL PROVIDE THE TEST PERSONNEL AND MATERIAL TO PERFORM THE ABOVE TESTS. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH DOCUMENTATION OF THE ABOVE TESTS.
- STORM COLLECTION SYSTEM DESIGNED FOR SILT TIGHT COMPONENTS.
- ALL STORM PIPE IDENTIFIED AS "HOPE" SHALL BE Nancy Smith-Lof F477 (OR) ADS N-12. SEE PLAN SET FOR ADDITIONAL INFORMATION.
- ALL STORM COLLECTION SYSTEM CONNECTIONS AND COMPONENTS SHALL CONFORM TO PIPE MANUFACTURER REQUIREMENTS. GC TO COORDINATE STORM SYSTEM LAYOUT WITH ENGINEER AND STORM SYSTEM SUPPLIER. STORM SYSTEM COMPONENT SHOP DRAWINGS SHALL BE PROVIDED FOR ENGINEER'S REVIEW PRIOR TO CONSTRUCTION.
- ALL CATCH BASINS SHALL BE AS IDENTIFIED ON PLAN SET. ALL STORM SYSTEM CATCH BASINS SHALL BE PROVIDED WITH A MINIMUM 24" SETTLEMENT SUMP BELOW THE LOWEST PIPE INVERT. SEE PLAN SET FOR ADDITIONAL INFORMATION.
- ALL UNDERGROUND PIPING, CONDUIT AND OTHER UTILITIES SHALL BE BEDDED PER ODOT STD. DETAIL RD300 (OR AS OTHERWISE SPECIFIED BY PIPE MANUFACTURER). NOTIFY ENGINEER IN EVENT OF DISCREPANCIES.
- ALL LANDSCAPED AREAS SHALL BE AS NOTED ON THE LANDSCAPE PLANS.
- HOLD SUB-GRADE ELEVATIONS DOWN 12" WITHIN GROUND COVER PLANTING AREAS AND 6" WITHIN LAWN AREAS. REFER TO LANDSCAPE PLANS FOR ADDITIONAL INFORMATION PERTAINING TO TOP SOIL REQUIREMENTS.
- SEE PLAN SET & FOR ADDITIONAL INFORMATION.

**SITE PREPARATION NOTES:**

**CLEARING & GRUBBING -**  
REFER TO STRUCTURAL (FOUNDATION) PLANS FOR SPECIFIC SOIL EXCAVATION & BACK FILL REQUIREMENTS WITHIN BUILDING FOOTPRINT.

ALL AREAS BELOW ROADWAYS, PARKING AND WALKWAYS SHALL BE CLEARED AND GRUBBED OF ALL PAVEMENT, FOREIGN MATTER, DEBRIS, ORGANIC AND DISTURBED MATERIAL (U.N.O.). STRIPPING DEPTHS ACROSS THE SITE WILL VARY DEPENDING ON LOCATION AND PAVEMENT SECTION REQUIREMENTS. ALL EXPOSED MATERIAL SHALL BE MOISTURE CONDITIONED TO WITHIN 2% OF OPTIMUM.

ALL CLEARED AND GRUBBED MATERIAL SHALL BE REMOVED FROM SITE. GC SHALL COORDINATE DISPOSAL LOCATION.

ALL AREAS WITH ABANDONED UTILITY LINES, STORM DRAINS, UNDERGROUND TANKS, ETC. WHICH PROVIDE VOID SPACE BENEATH THE SURFACE SHALL BE LOCATED AND REMOVED PRIOR TO SITE GRADING.

ALL HOLES, DEPRESSIONS, AND UNDISTURBED NATIVE MATERIAL SHALL BE CLEARED OF ALL LOOSE AND ORGANIC MATERIAL THEN BACKFILLED AND COMPACTED WITH APPROVED STRUCTURAL FILL.

AFTER CLEARING THE ABOVE MENTIONED AREAS, ALL EXPOSED SUB-GRADE SHALL BE PROOF ROLLED WITH A LOADED DUMP TRUCK OR HEAVY NON-VIBRATORY ROLLER. SOBS SHALL BE REMOVED AND RE-COMPACTED OR REPLACED WITH IMPORTED APPROVED STRUCTURAL FILL IF THEY DO NOT DEMONSTRATE A FIRM UNYIELDING CONNECTION. ENGINEER SHALL APPROVE SUB-GRADE SURFACE PRIOR TO STRUCTURAL FILL IMPORT EXPLAINED TO THE RIGHT.

**SITE PREPARATION NOTES CONT:**

**STRUCTURAL FILL PLACEMENT & COMPACTION -**  
APPROVED STRUCTURAL FILL SHALL BE PLACED BENEATH AREAS RECEIVING ASPHALT AND/OR CONCRETE PAVEMENT.

ALL AREAS RECEIVING ASPHALT AND/OR CONCRETE SHALL BE PROVIDED WITH APPROVED WOVEN GEOTEXTILE FABRIC APPLIED DIRECTLY OVER SUB-GRADE DESCRIBED ABOVE.

STRUCTURAL FILL SHALL BE APPROVED BY ENGINEER PRIOR TO IMPORTING. ALL FILL SHALL BE FREE OF ORGANIC AND EXPANSIVE CLAY MATERIAL.

PLACEMENT LIFTS TO BE DETERMINED BY ENGINEER BASED ON MATERIAL PROPERTIES OF STRUCTURAL FILL CHOSEN AND TYPE OF COMPACTION EQUIPMENT USED. BASE ROCK PLACEMENT LIFTS SHALL NOT EXCEED 8". EACH LIFT SHALL BE NEARLY EQUAL IN THICKNESS AND COMPACTED TO A MINIMUM OF 95% OF ASTM D698. FILLS SHALL BE PLACED AT OR SLIGHTLY ABOVE THEIR OPTIMUM MOISTURE CONTENT.

ALL UTILITY TRENCH BACK FILL SHALL CONFORM TO CURRENT OREGON APWA / ODOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND CITY OF ASHLAND STD. DETAIL CD302. CONTACT ENGINEER OF RECORD IN THE EVENT OF A CONFLICT.

ALL UTILITY TRENCH BACK FILL SHALL CONFORM TO CURRENT OREGON APWA / ODOT STANDARD SPECIFICATIONS FOR CONSTRUCTION. REPORT TO ENGINEER OF RECORD IN THE EVENT OF DISCREPANCIES.

**EROSION CONTROL NOTE:**

PLAN SHEET CD 1 CONTAINS AN EROSION AND SEDIMENT CONTROL PLAN THAT MUST BE IMPLEMENTED AS PART OF THIS PROJECT. THE INFORMATION CONTAINED WITHIN THE REFERENCED PLAN SHEET SHALL BE CONSIDERED A MINIMUM AND SHALL BE MODIFIED AS REQUIRED BY THE CONTRACTOR TO CONTAIN ALL SEDIMENT ON SITE. SPECIAL ATTENTION SHALL BE TAKEN AT ALL EXISTING STORM DRAIN CATCH BASINS AND STORM DRAIN CHANNELS AS TO ELIMINATE ANY SEDIMENT TRANSFER INTO THE EXISTING STORM DRAIN SYSTEM.

ALL CONSTRUCTION SHALL BE MAINTAINED WITHIN THE DEVELOPMENT LIMITS OF THIS PHASE. REFER TO SHEET CD 1 FOR ADDITIONAL INFORMATION.

**UTILITY STATEMENT:**

EXISTING UNDERGROUND UTILITIES ILLUSTRATED IN THESE PLANS ARE APPROXIMATED BASED ON MAPS OBTAINED FROM THE CITY OF ASHLAND PUBLIC WORKS FILES, OR HAVE BEEN LOCATED BY A UTILITY LOCATE COMPANY. LAYOUT INDICATED IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. ALL LINES WITHIN PROJECTED WORK ZONE SHALL BE FIELD VERIFIED AS NOTED PRIOR TO CONSTRUCTION.

**PARKING SUMMARY:**

EXISTING	- 36 SPACES
PROPOSED	- 17 SPACES
ON STREET	- 7 SPACES
TOTAL	- 60 SPACES

**SITE IMPROVEMENTS SHEET INDEX:**

CD 0	SITE CIVIL COVER SHEET
CD 1	EROSION CONTROL PLAN - DEFERRED SUBMITTAL
C1.1	EXISTING CONDITIONS SURVEY & SITE DEMOLITION PLAN
C1.2	OVERALL SITE LAYOUT
C2.1	SITE GRADING & DRAINAGE PLAN
C3.1	GRADING AND DRAINAGE DETAILS
C4.1	SITE STRIPING & UTILITIES PLAN
C5.1	STORM DRAIN PLAN/PROFILE
L1.1	PLANTING PLAN
L1.2	TRACE PROTECTION & REPAIR PLAN

**SECTION ARROW:**



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**ZBIDEN • CARTER • SoudERS**  
ENGINEERING  
900 Hiawath Avenue, Hiawath Falls, OR 97001  
(541) 884-1461  
ZCS Project No. H44461-07

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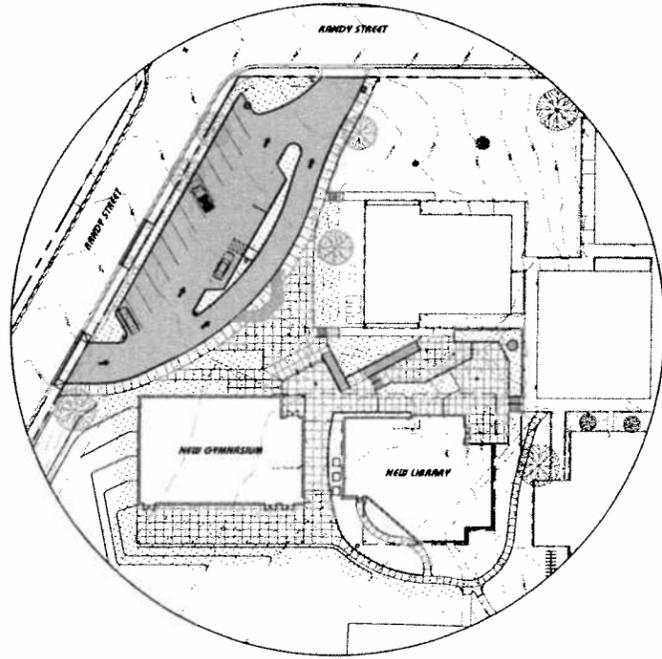


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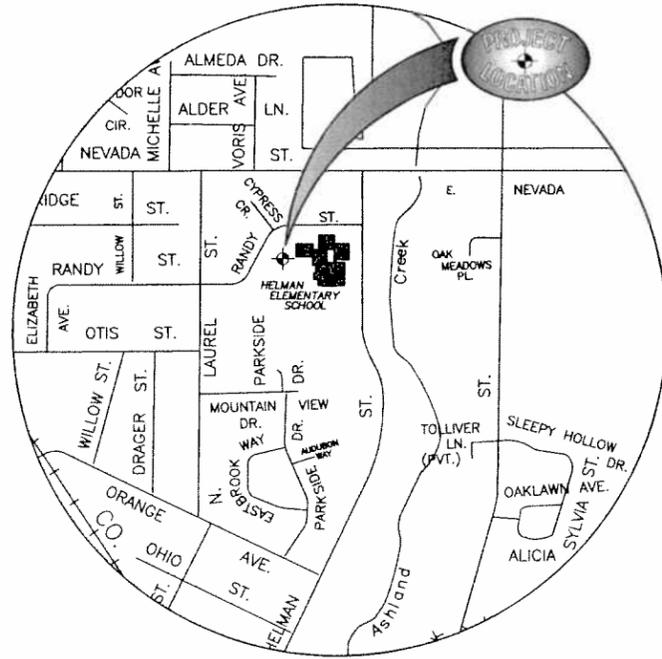
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# HELMAN ELEMENTARY SCHOOL

A PROJECT FOR:  
**ASHLAND SCHOOL DISTRICT**  
TRP LOT 200, SITUATED IN THE SW 1/4 NE 1/4 OF SECTION 04B,  
T30S, R1E, L1M - CITY OF ASHLAND, JOHNSON COUNTY, OREGON



**SITE PLAN**  
NTS



**VICINITY MAP**  
NTS

**LEGEND:**

- NEW HMA PAVING - SEE PLAN
- NEW ON-SITE CONCRETE - SEE PLAN
- NEW LANDSCAPING - BY OTHERS
- EXTG. IRRIGATION LINE
- EXTG. SANITARY SEWER
- NEW SANITARY SEWER
- EXTG. DOMESTIC WATER
- NEW DOMESTIC WATER
- EXTG. STORM SEWER
- NEW PRIVATE STORM SEWER
- EXTG. FENCE
- EXTG. SURFACE CONTOUR

**SYMBOLS (EXISTING):**

- IRRIGATION MANHOLE
- SANITARY SEWER MANHOLE
- SANITARY SEWER CLEAROUT
- STORM DRAIN MANHOLE
- FIRE HYDRANT
- IRRIGATION VALVE
- CATCH BASIN
- ELECTRICAL EQUIPMENT
- TELEPHONE BOX
- CONDUIT
- LIGHT
- DECIDUOUS TREE
- CONIFEROUS TREE
- SKIN

**SYMBOLS (NEW):**

- NEW GRADE SPOT ELEVATION
- CATCH BASIN
- CLEANOUT TO GRADE
- WATER METER
- SKIN
- ON-SITE LIGHTING
- WATER VALVE

**ABBREVIATIONS:**

- INVERT ELEVATION
- FINISHED FLOOR
- ASPHALT
- CONCRETE
- BACK OF SIDEWALK
- SIDEWALK
- GROUND
- TOP BACK OF CURB
- EXISTING
- NEW
- MATCH EXISTING

PRELIMINARY PRINT

70% CDS

CIVIL COVER SHEET  
ASHLAND SCHOOL DISTRICT  
HELMAN ELEMENTARY

CD 0.0

10-05-07

10-05-07

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# TOPOGRAPHIC SURVEY

PROVIDED FOR:  
ASHLAND SCHOOL DISTRICT  
TRX LOT 000, SITUATED IN THE SW 1/4 NE 1/4 OF SECTION 04B,  
T39S, R1E, U1M - CITY OF ASHLAND, JACKSON COUNTY, OREGON

**LEGEND:**

[Symbol]	EXTG. BUILDING LIMITS
[Symbol]	EXTG. CONCRETE SIDEWALKS
[Symbol]	EXTG. GRAVEL PAVING
[Symbol]	EXTG. IRRIGATION LINE
[Symbol]	EXTG. SANITARY SEWER
[Symbol]	EXTG. DOMESTIC WATER
[Symbol]	EXTG. STORM DRAIN
[Symbol]	EXTG. FENCE
[Symbol]	EXTG. SURFACE CONTOUR

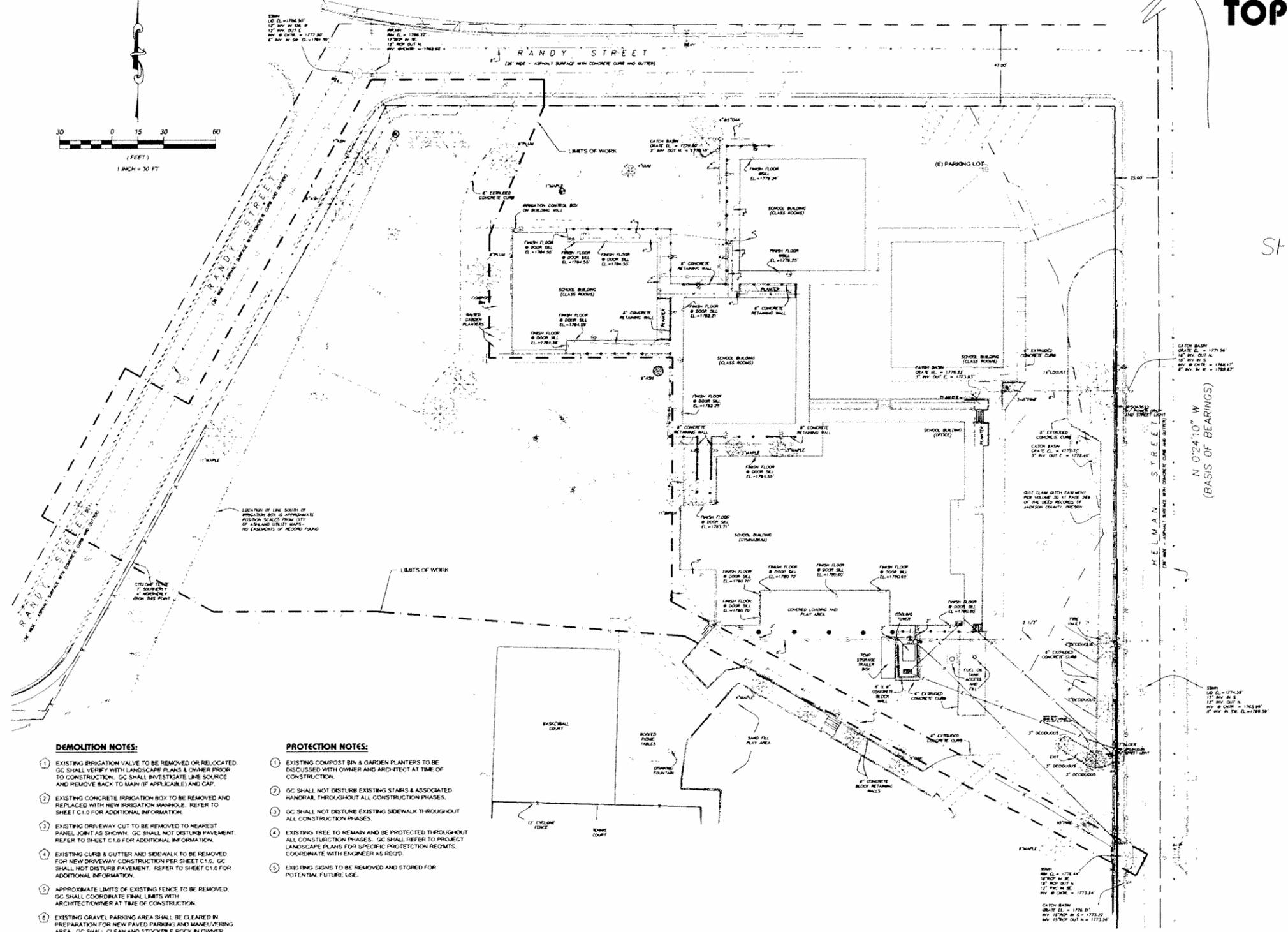
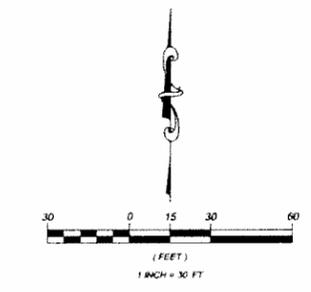
**SYMBOLS (EXISTING):**

[Symbol]	IRRIGATION MANHOLE
[Symbol]	SANITARY SEWER MANHOLE
[Symbol]	SANITARY SEWER CLEANOUT
[Symbol]	STORM DRAIN MANHOLE
[Symbol]	FIRE HYDRANT
[Symbol]	IRRIGATION VALVE
[Symbol]	CATCH BASIN
[Symbol]	ELECTRICAL EQUIPMENT
[Symbol]	TELEPHONE BOX
[Symbol]	HOSE BIB
[Symbol]	DECIDUOUS TREE
[Symbol]	CONIFEROUS TREE
[Symbol]	SIGN

**ABBREVIATIONS:**

(E)	EXISTING
(IE)	INVERT ELEVATION
(RM)	TOP OF MANHOLE
(CB)	CATCH BASIN
(SN)	SURVEY NUMBER AS FILED AT THE JACKSON COUNTY SURVEYOR'S OFFICE, OREGON

- SURVEY NOTES:**
- THE EXISTING TOPOGRAPHICAL SURVEY INFORMATION DEPICTED ON THIS SHEET AND THROUGHOUT THE PLAN SET WAS PROVIDED BY TERRASURVEY, INC. THE FOLLOWING SURVEY NOTES HAVE BEEN REPRODUCED FROM THE REFERENCED SURVEY DOCUMENT.
  - LOT LOCATED IN THE SW 1/4 NE 1/4 OF SECTION 04B, TOWNSHIP 39 SOUTH, RANGE 1 EAST, WILLAMETTE MERIDIAN, CITY OF ASHLAND, JACKSON COUNTY, OREGON.
  - CONTOUR INTERVAL = 1'
  - EXISTING UNDERGROUND UTILITIES ILLUSTRATED ON THIS SHEET ARE APPROXIMATED BASED ON MAPS OBTAINED FROM THE CITY OF ASHLAND PUBLIC WORKS FILES, OR HAVE BEEN LOCATED BY A UTILITY LOCATE COMPANY. LAYOUT INDICATED IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. ALL LINES WITHIN PROJECTED WORK ZONE SHALL BE FIELD VERIFIED AS REQUIRED TO CONSTRUCTION.
  - PROJECT DATUM IS BASED ON THE CITY OF ASHLAND BENCH MARK NO. 68, LOCATED IN THE TOP OF THE CURB AT THE NORTH-EAST CORNER OF HELMAN STREET AND HELMAN STREET. ELEVATION = 1762.21 FEET (NAVD 1929 56).
  - BASE OF BEARINGS: FOUND CITY OF ASHLAND BENCH CENTERLINE MONUMENTS AT THE SOUTHWEST CORNER OF THE LINE OF NEVADA STREET AND THE CENTER LINE OF HELMAN STREET AND THE CENTERLINE ANGLE POINT OF HELMAN STREET BEING NORTH 02°41'00" WEST PER SN 16744.



- DEMOLITION NOTES:**
- EXISTING IRRIGATION VALVE TO BE REMOVED OR RELOCATED. GC SHALL VERIFY WITH LANDSCAPE PLANS & OWNER PRIOR TO CONSTRUCTION. GC SHALL INVESTIGATE LINE SOURCE AND REMOVE BACK TO MAIN (IF APPLICABLE) AND CAP.
  - EXISTING CONCRETE IRRIGATION BOX TO BE REMOVED AND REPLACED WITH NEW IRRIGATION MANHOLE. REFER TO SHEET C1.0 FOR ADDITIONAL INFORMATION.
  - EXISTING DRIVEWAY CUT TO BE REMOVED TO NEAREST PANEL JOINT AS SHOWN. GC SHALL NOT DISTURB PAVEMENT. REFER TO SHEET C1.0 FOR ADDITIONAL INFORMATION.
  - EXISTING CURB & GUTTER AND SIDEWALK TO BE REMOVED FOR NEW DRIVEWAY CONSTRUCTION PER SHEET C1.0. GC SHALL NOT DISTURB PAVEMENT. REFER TO SHEET C1.0 FOR ADDITIONAL INFORMATION.
  - APPROXIMATE LIMITS OF EXISTING FENCE TO BE REMOVED. GC SHALL COORDINATE FINAL LIMITS WITH ARCHITECT/OWNER AT TIME OF CONSTRUCTION.
  - EXISTING GRAVEL PARKING AREA SHALL BE CLEARED IN PREPARATION FOR NEW PAVED PARKING AND MANEUVERING AREA. GC SHALL CLEAN AND STOCKPILE ROCK IN OWNER APPROVED LOCATION. COORDINATE WITH OWNER AT TIME OF CONSTRUCTION.
  - EXISTING CURB TO BE REMOVED AND DISPOSED OF IN AN APPROVED OFF-SITE LOCATION. COORDINATE AS REQD.
  - EXISTING TREE TO BE REMOVED AND DISPOSED OF IN AN APPROVED OFF-SITE LOCATION. COORDINATE AS REQD.
  - EXISTING ON-SITE SIDEWALK AND ASSOCIATED BASE ROCK (IF APPLICABLE) TO BE REMOVED AND DISPOSED OF IN AN APPROVED OFF-SITE LOCATION. COORDINATE AS REQD.

- PROTECTION NOTES:**
- EXISTING COMPOST BIN & GARDEN PLANTERS TO BE DISCUSSED WITH OWNER AND ARCHITECT AT TIME OF CONSTRUCTION.
  - GC SHALL NOT DISTURB EXISTING STAIRS & ASSOCIATED HANDRAIL THROUGHOUT ALL CONSTRUCTION PHASES.
  - GC SHALL NOT DISTURB EXISTING SIDEWALK THROUGHOUT ALL CONSTRUCTION PHASES.
  - EXISTING TREE TO REMAIN AND BE PROTECTED THROUGHOUT ALL CONSTRUCTION PHASES. GC SHALL REFER TO PROJECT LANDSCAPE PLANS FOR SPECIFIC PROTECTION REQD'S. COORDINATE WITH ENGINEER AS REQD.
  - EXISTING SIGNS TO BE REMOVED AND STORED FOR POTENTIAL FUTURE USE.

EXISTING CONDITIONS SURVEY & SITE DEMOLITION PLAN  
1" = 30'

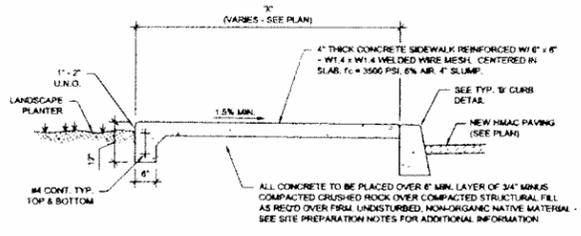
TERRASURVEY, INC.  
PROFESSIONAL LAND SURVEYORS  
274 FOURTH STREET  
ASHLAND, OREGON 97520  
(541) 482-6474  
terra@tsp.net  
JOB NO. 619-07-F

ZCS  
ZBINDEN • CARTER • SOUDERS  
ENGINEERING  
900 HIGHWAY 99 NORTH, ASTORIA, OREGON 97103  
(541) 326-7400  
ZCS Project No. 14-4401-07

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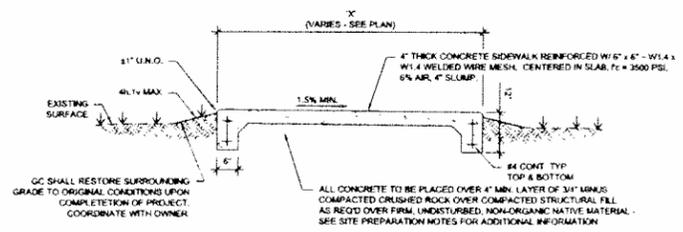
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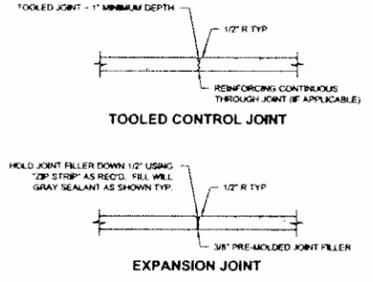
PROVIDE CONTRACTION JOINTS @ 8' o.c.  
 & 3/8" ISOLATION JOINTS @ 3/4" o.c.  
 (COORDINATE WITH PLAN)

**PRIVATE SIDEWALK SECTION 'A'**  
 1  
 C3.1 1/2" = 1'

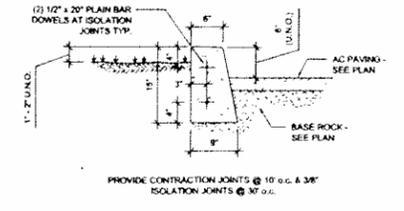


PROVIDE CONTRACTION JOINTS @ 8' o.c.  
 & 3/8" ISOLATION JOINTS @ 3/4" o.c.  
 (COORDINATE WITH PLAN)

**PRIVATE SIDEWALK SECTION 'B'**  
 2  
 C3.1 1/2" = 1'



**3 SIDEWALK JOINTING DETAILS**  
 C3.1 NTS



PROVIDE CONTRACTION JOINTS @ 10' o.c. & 3/8"  
 ISOLATION JOINTS @ 3/4" o.c.

**4 TYPE 'B' CURB**  
 C3.1 3/4" = 1'

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90% CDS

CIVIL DETAILS  
 ASHLAND SCHOOL DISTRICT  
 HELMAN ELEMENTARY

C3.1

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**ZCS**  
 ZBINDEN • CARTER • SOUDERS  
 ENGINEERING  
 900 Homoth Avenue, Homoth Falls, OR 97001  
 (541) 884-7421  
 ZCS Project No. 14-0401-07  
 05/05/07

PRELIMINARY -  
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 Architecture Planning Interiors

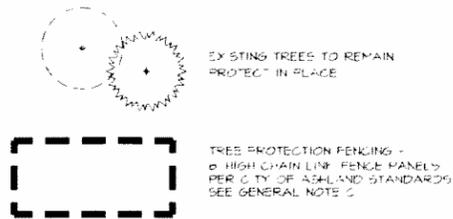
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LEGEND



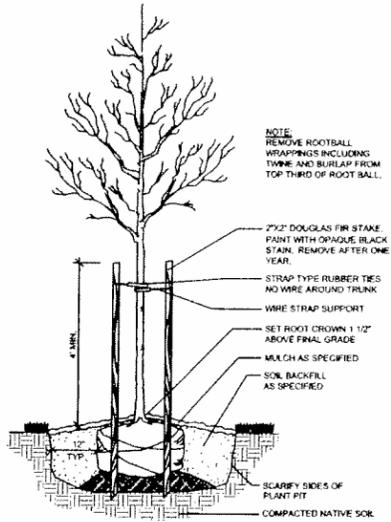
GENERAL NOTES

- A. VERIFY LOCATIONS OF ALL BELOW GRADE UTILITIES PRIOR TO BEGINNING WORK. CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL UTILITY LOCATIONS.
- B. TOPOGRAPHIC SURVEY OF EXISTING CONDITIONS PROVIDED BY TERRASURVEY, INC., TEL: 541-482-6474.
- C. GENERAL CONTRACTOR SHALL PROVIDE, INSTALL, AND MAINTAIN TREE PROTECTION FENCING ACCORDING TO CITY OF ASHLAND REQUIREMENTS.
- D. AN AUTOMATIC IRRIGATION SYSTEM WILL BE PROVIDED FOR ALL PLANTING AREAS AT THE TIME OF BUILDING PERMIT SUBMITTAL.

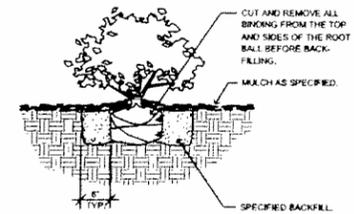
PLANT LIST

Key	Botanical Name	Common Name	Size / Comments
<b>Trees</b>			
ACHE	ACER HENRYI	HENRY MAPLE	1.75" CAL - B&B
ACPL	ACER PLATANOIDES 'CLEVELAND'	CLEVELAND NORWAY MAPLE	1.75" CAL - B&B
COED	CORNUS 'EDDIE'S WHITE WONDER'	DOGWOOD	1.75" CAL - B&B
CUBA	CUPRESSUS BAKERII	BAKERS CYPRESS	1.75" CAL - B&B
LATU	LAGERSTROEMIA TUSCARORA	TUSCARORA CRAPE MYRTLE	1.75" CAL - B&B
PAPE	PARROTIA PERSICA	PERSIAN PARROTIA	1.75" CAL - B&B
QUGA	QUERCUS GARRYANA	OREGON OAK	1.75" CAL - B&B
<b>Shrubs</b>			
BETH	BERBERIS T. 'CRIMSON PYGMY'	CRIMSON PYGMY BARBERRY	1 GAL
COHO	COTONEASTER HORIZONTALIS	ROCK COTONEASTER	1 GAL
IICO	ILEX CORNUTA 'BURFORDI NANA'	DWARF BURFORD CHINESE HOLLY	1 GAL/5 GAL NOTED
MAHE	MAHONIA NERVOSA	LONGLEAF MAHONIA	1 GAL
MAAO	MAHONIA AQUIFOLIUM	OREGON GRAPE	1 GAL /5 GAL NOTED
MACO	MAHONIA AQUIFOLIUM COMPACTA	COMPACT OREGON GRAPE	5 GAL
NADO	MAHONIA AQUIFOLIUM COMPACTA	COMPACT OREGON GRAPE	1 GAL
NADO	NANDINA DOMESTICA	HEAVENLY BAMBOO	1 GAL
POFR	POTENTILLA FRUTICOSA 'GOLD STAR'	GOLD STAR POTENTILLA	1 GAL
RISA	RIBES SANGUINEUM	RED-FLOWERING CURRANT	1 GAL
ROYE	ROSA 'YELLOW CARPET'	YELLOW CARPET ROSE	1 GAL
SPJA	SPIRAEA J. 'ANTHONY WATERER'	ANTHONY WATERER SPIRAEA	1 GAL
VITI	VBURNUM TINUS 'COMPACTUM'	COMPACT LAURESTINUS VIBURNUM	1 GAL/5 GAL NOTED
<b>Perennials</b>			
COAP	ARCTOSTAPHYLOS UVA-URSI 'MASS'	MASSACHUSETTS BEAR BERRY	1 GAL at 30" O.C.
	COTONEASTER A. 'TOM THUMB'	TOM THUMB COTONEASTER	1 GAL

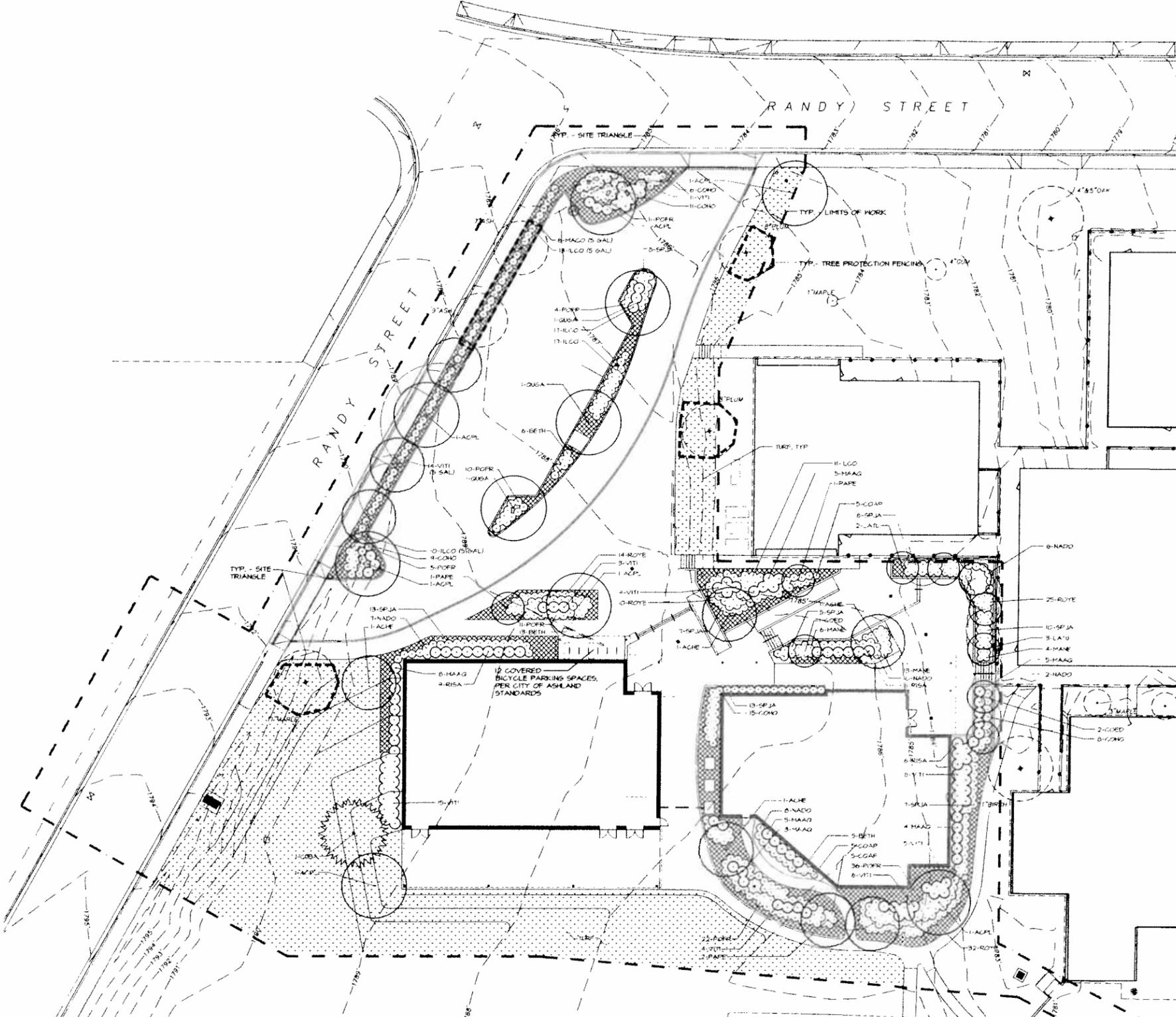
MACO



1 TREE PLANTING  
NOT TO SCALE



2 SHRUB PLANTING  
NOT TO SCALE



NOV 1 2007

NORTH

0 10' 20' 40'

295 EAST MAIN, #2  
P.O. BOX 999  
ASHLAND, OR 97132

541.582.2315 px  
541.582.1024 fx  
pdx@coveypardee.com

covey PARDEE  
LANDSCAPE ARCHITECTS

REGISTERED  
LANDSCAPE ARCHITECT  
Oregon  
295  
7/29/92

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30% CDs

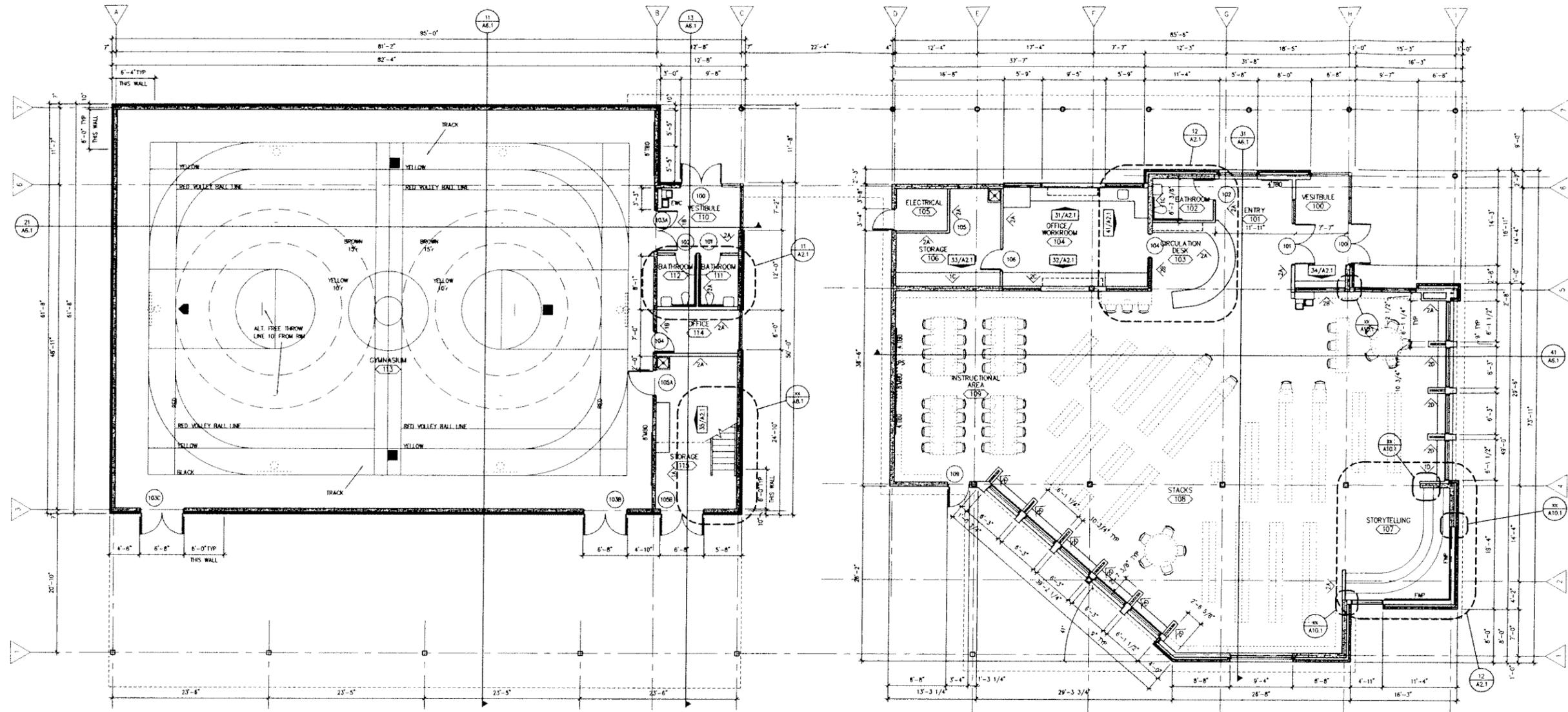
PRELIMINARY LANDSCAPE PLAN  
ASHLAND SCHOOL DISTRICT  
HELMAN ELEMENTARY

L1.1  
740710640  
REVISED 10-19-2007

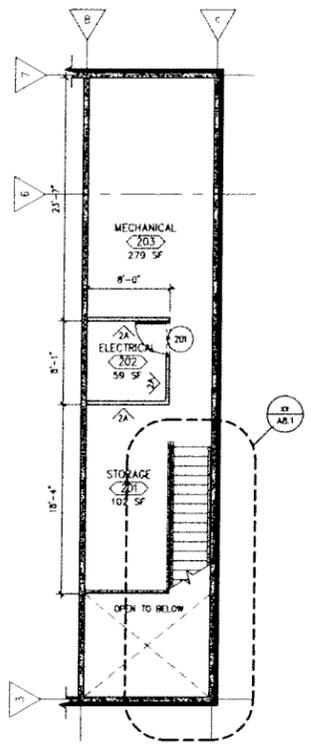
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04.08.2007 3:38pm  
ashland



 **FIRST FLOOR PLAN**  
SCALE: 1/8" = 1'-0"



 **SECOND FLOOR/MECHANICAL MEZZANINE PLAN**  
SCALE: 1/8" = 1'-0"

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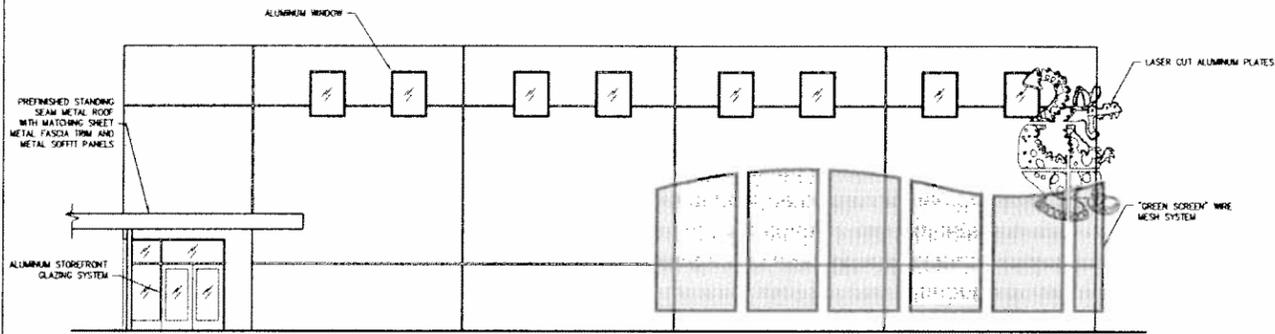
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PRINT

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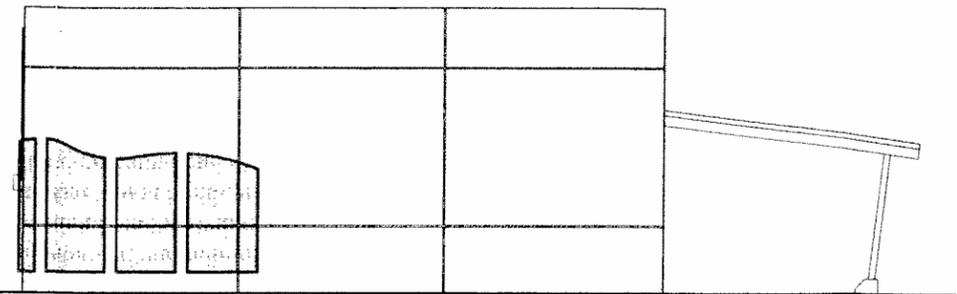
**FLOOR PLANS  
HELMAN ELEMENTARY SCHOOL  
LIBRARY AND GYM ADDITION**

**DLR Group**  
Architecture Planning Interiors

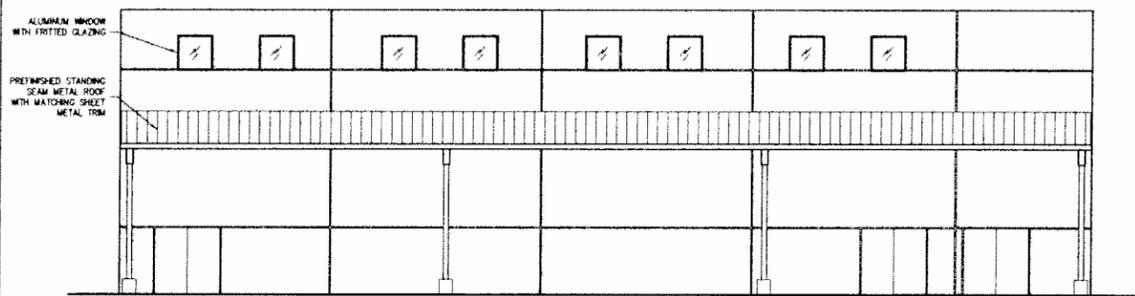
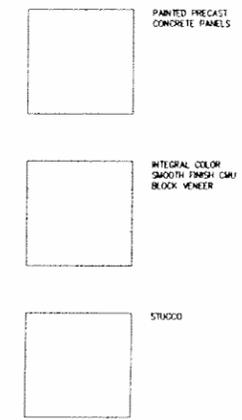
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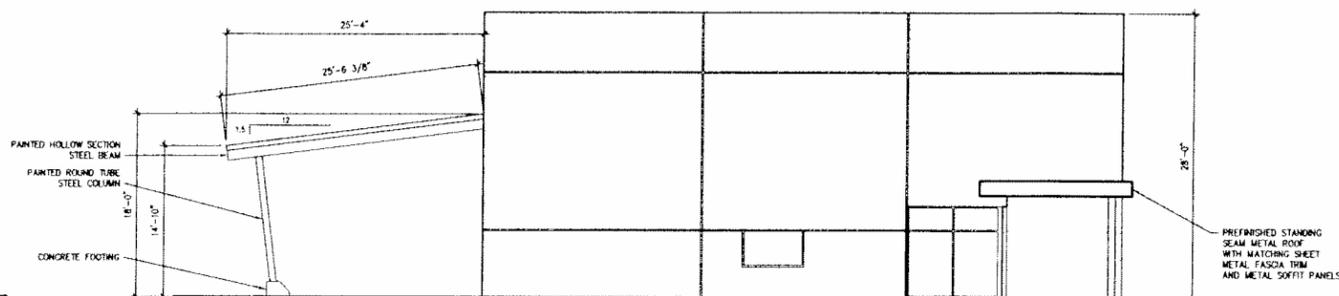
11 NORTH GYM ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



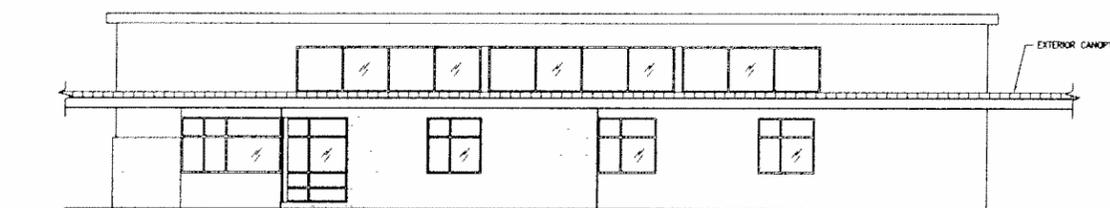
12 WEST GYM ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



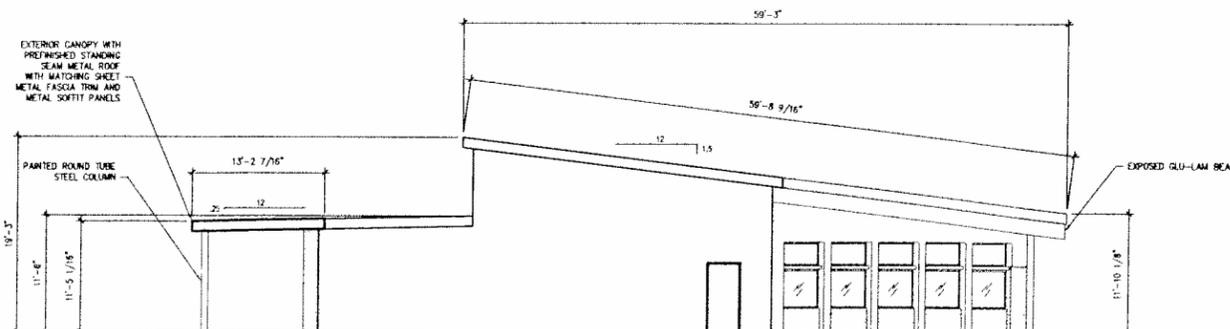
21 SOUTH GYM ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



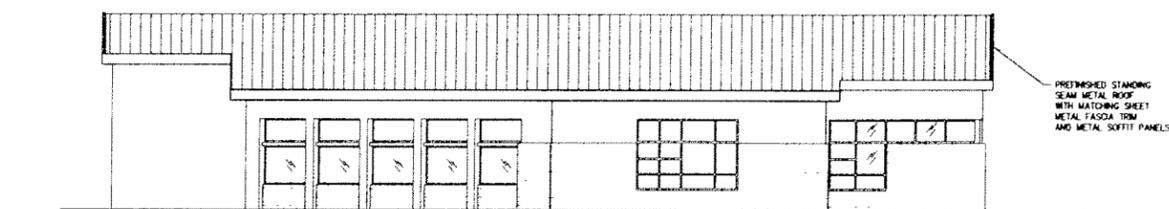
22 EAST GYM ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



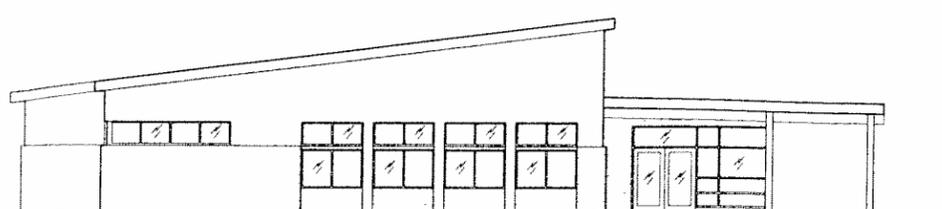
31 NORTH LIBRARY ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



32 WEST LIBRARY ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



41 SOUTH LIBRARY ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"



42 EAST LIBRARY ELEVATION  
AS.1 SCALE: 1/8" = 1'-0"

PRELIMINARY PRINT  
10-10-07  
NOT FOR CONSTRUCTION

90% CDS

BUILDING ELEVATIONS  
HELMAN ELEMENTARY SCHOOL  
LIBRARY AND GYM ADDITION

A5.1  
7/10/06-07  
10-06-2007

RECEIVED  
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City of Asheville

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Oct 10, 2007 3:50pm - Jordan



## **OUTLINE PLAN APPROVAL**

### **18.88.030.A Criteria for Approval**

The Planning Commission shall approve the outline plan when it finds the following criteria have been met:

- a. That the development meets all applicable ordinance requirements of the City of Ashland.
- b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.
- c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.
- d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.
- e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.
- f. That the proposed density meets the base and bonus density standards established under this Chapter.
- g. The development complies with the Street Standards. (Ord 2836, S2 1999)

## **SITE DESIGN AND USE STANDARDS**

### **18.72.070 Criteria for Approval**

The following criteria shall be used to approve or deny an application:

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options. (Ord. 2655, 1991; Ord 2836 S6, 1999)

## **CONDITIONAL USE PERMITS**

### **18.104.050 Approval Criteria**

A conditional use permit shall be granted if the approval authority finds that the proposed use conforms, or can be made to conform through the imposition of conditions, with the following approval criteria.

- A. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
- B. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.
- C. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:
  1. Similarity in scale, bulk, and coverage.
  2. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  3. Architectural compatibility with the impact area.
  4. Air quality, including the generation of dust, odors, or other environmental pollutants.
  5. Generation of noise, light, and glare.
  6. The development of adjacent properties as envisioned in the Comprehensive Plan.
  7. Other factors found to be relevant by the Hearing Authority for review of the proposed use.

## **TREE REMOVAL**

### **18.61.080 Criteria for Issuance of Tree Removal - Staff Permit**

An applicant for a Tree Removal-Staff Permit shall demonstrate that the following criteria are satisfied. The Staff Advisor may require an arborist's report to substantiate the criteria for a permit.

- A. **Hazard Tree:** The Staff Advisor shall issue a tree removal permit for a hazard tree if the applicant demonstrates that a tree is a hazard and warrants removal.
  1. A hazard tree is a tree that is physically damaged to the degree that it is clear that it is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within public rights of way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated or the damage alleviated. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard or a foreseeable danger of property damage to an existing structure and such hazard or danger cannot reasonably be alleviated by treatment or pruning.
  2. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.
- B. **Tree that is Not a Hazard:** The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:
  1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards. (e.g. other applicable Site Design and Use Standards). The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and
  2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and
  3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with other provisions of the Ashland Land Use Ordinance.
  4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit. (ORD 2883 added 06/04/2002)

# ASHLAND PLANNING DEPARTMENT STAFF REPORT

November 13, 2007

**PLANNING ACTION:** 2007-01398

**APPLICANT:** Havurah Friends Investment Group, LLC

**LOCATION:** 167, 185 and 203 North Mountain Avenue  
39 1E 09AA Tax Lots 1500, 1600, 1701, 1800 & 1700

**ZONE DESIGNATION:** R-1-5-P

**COMPREHENSIVE PLAN DESIGNATION:** Single Family Residential

**APPLICATION DEEMED COMPLETE:** November 5, 2007

**120-DAY TIME LIMIT:** March 4, 2008

**ORDINANCE REFERENCE:**

18.20	R-1 Single-Family Residential
18.61	Tree Preservation and Protection
18.72	Site Review
18.76.140	Lot Line Adjustments
18.88	Performance Standards Options
18.104	Conditional Use Permits

**REQUEST:** Planning Action #2007-01398 is a request for Outline Plan Approval to allow a 12-lot, 15-unit subdivision for the properties located at 167, 185 and 203 North Mountain Avenue. Also included are requests for: the modification of a previously approved Site Review and Conditional Use Permit (#2001-0039) for the Havurah Jewish Synagogue; Site Review approval to construct a two-story, six-unit residential building; a Tree Removal Permit for the removal of one nine-inch diameter pine tree from Tax Lot #1701; and a boundary line adjustment with Tax Lots 1600, 1701, 1800 and 1700. (This request would supersede the previous Outline Plan approval for a 14-lot, 13-unit subdivision granted under Planning Action #2006-01091.)

## I. Relevant Facts

### A. Background - History of Application

Planning Action #2001-039, a request for a Conditional Use Permit to remodel the Havurah Jewish Synagogue facility and add a sanctuary was approved by the Planning Commission in 2001.

Planning Action #2006-01091, a request for Outline Plan approval to subdivide the property at 203 North Mountain Avenue into 14 lots including 13 lots for single-family homes and one lot for open space purposes, was approved by the Planning Commission in 2006. That application included a Lot Line Adjustment to incorporate the western end of the parcel located at 185 North Mountain Avenue into the subdivision, and a Tree Removal permit to remove a 40-inch diameter at breast height Black Walnut tree in the North Mountain Avenue street right-of-way adjacent to the front of the property.

There are no other planning actions of record for this site.

## **B. Detailed Description of the Site and Proposal**

The subject properties are located at 167, 185 and 203 North Mountain Avenue, on the west side of North Mountain Avenue, directly north of the railroad right-of-way. The project site comprises four primary tax lots totaling 3.59 acres:

**Tax Lot #1500 (1.7 acres)** – Located at 203 North Mountain Avenue, the site of an existing 1,700 square foot single family home constructed in 1905 and oriented to North Mountain Avenue. The home is to be retained on Lot #1 of the proposed subdivision.

**Tax Lot #1600 (0.32 acres)** – An oddly-shaped, vacant parcel likely created from remnant railroad right-of-way adjacent to the north side of the railroad right-of-way.

**Tax Lot #1701 (1.04 acres)** – Located at 185 North Mountain Avenue, the site of the existing 5,246 square foot Havurah Jewish Synagogue facility. The existing building contains a small sanctuary, classrooms, assembly space, offices, bathrooms and a dining area.

**Tax Lot #1800 (0.55 acres)** - An oddly-shaped, vacant parcel likely created from remnant railroad right-of-way adjacent to the north side of the railroad right-of-way.

The proposal also involves a boundary line adjustment with a fifth property, Tax Lot #1700, located at 167 North Mountain Avenue and owned by Nancy Spencer. This .404 acre parcel is triangular in shape and contains an existing single family home, constructed in 1900, and a small associated accessory structure.

The site is moderately sloped averaging approximately a four percent downhill slope to the north. A drainage, identified as Mountain Creek in the City of Ashland Stormwater and Drainage Master Plan (June 2000), runs from south to north along the sites western edge. A wetland has been preliminarily delineated on the western side of the site, adjacent to Mountain Creek. The application includes a tree inventory which identifies eight trees sized six-inches in diameter at breast height (d.b.h.) and greater located in the North Mountain Avenue right-of-way, around the existing house and in the vicinity of the wetlands and Mountain Creek.

The subject parcels as well as the surrounding properties to the north and east are located in the R-1-5 Single-Family Residential zoning district. The area on the west side of Mountain

Creek and north of the railroad tracks is located in the E-1 Employment zoning district. The area immediately west of the subject site is vacant. To the south of the site, there is R-3 High Density Multi-Family Residential zoning, as well as E-1 Employment zoning in the area of the City's "B Street Yard."

### **1. Outline Plan for Performance Standards Options Subdivision**

The applicant is requesting Outline Plan approval to subdivide the property into 12 lots accommodating 15 residential units. The Havurah Jewish Synagogue and the existing home at 203 North Mountain are proposed to be preserved on their own individual lots, and one lot would accommodate commonly owned open space, including the identified wetland. Eight lots are proposed to be developed as single family residential units, with six of these as attached, zero lot line units and two detached residences. The remaining lot is proposed to contain a six-unit, two story condominium building. This proposal would supersede the previously approved 14-lot subdivision approved under Planning Action #2006-01091.

The proposal is to provide access to the subdivision by constructing a new public street connecting to N. Mountain Ave. and running along the north side of the site. When the properties to the north and west eventually develop, the street would be extended to the northwest and north to serve those properties. The frontage of the property located at 203 N. Mountain Ave. will be improved with parkrow, street trees and sidewalk installed to city standards.

The residences fronting on the newly proposed section of Clear Creek Drive will take vehicular access via driveways from the street. As a Commercial Collector Street, controlled access requirements call for a minimum of 75 feet between driveways. The application satisfies these requirements through the use of shared driveways serving the zero lot line units in order to minimize access points. Lot 10, the only proposed flag lot, will take access from Mountain Avenue via the Havurah's driveway, with residents to exit via the lot's flag drive to Clear Creek Drive. Off street parking for the development is to be provided in garages and required on-street parking spaces are available on North Mountain Avenue and the newly proposed portion of Clear Creek Drive.

The proposed open space is approximately 19,150 square feet in size, which is roughly 12 percent of the total site area and includes the wetlands adjacent to Mountain Creek and a bicycle and pedestrian path along the southern portion of the parent property, adjacent to the railroad right-of-way. A preliminary determination of the boundary of the wetlands is included in the application site plan, and identifies a wetland which is approximately 4,780 square feet in size. The proposal is to retain the wetland and include it in the open space area on the western end of the site. Approximately half of the open space area is comprised of the wetland and creek. The remainder of the open space area is comprised of the long narrow area adjacent to the railroad right-of-way. The application indicates that the existing wetland will be supplemented with the creation of additional wetlands and a bio-swale to enhance the natural environment and provide additional stormwater detention. Blackberry

overgrowth is to be removed, but the existing trees within the wetland and the wetland habitat retained as a natural open space corridor. A wetland mitigation planting plan has been provided. The applicants also propose to provide a public pedestrian pathway to connect a proposed bicycle and pedestrian easement along the railroad tracks to Clear Creek Drive through the open space adjacent to the wetland.

Existing and proposed public facilities are generally discussed in the application narrative, and sewer and water lines are identified, labeled and sized in the application. The application states and the plan notes that all primary utilities will be extended in the new street for eventual connection with the railroad property to the west. The application also states that the lines in the new street will be “upsized, in cooperation with the City, in order to accommodate future capacity needs.” Existing and proposed upgrades include:

- Existing six-inch water and ten-inch sanitary sewer lines are located within the North Mountain Avenue right-of-way, and proposed eight-inch water and sewer lines are shown routed within the new street.
- A conceptual drainage plan detailing an engineered storm drainage splitter and pollution control structure has been provided. This structure would direct lower stormwater flows via four-inch storm and french drain lines to a four-inch outlet emptying to a bio-swale adjacent to the proposed wetland, with higher flows directed via new 15-inch lines through the subdivision, to Mountain Avenue and on to the existing 24-inch line at Village Square Drive. The application notes that this design is intended to resolve flooding issues which have historically impacted the neighbors in the Parkridge and Starflower Subdivisions to the north.
- Existing overhead power along the North Mountain Avenue right-of-way is identified, street light locations along the new street shown, and connections to meters on the proposed homes identified.
- Paved access is provided by North Mountain Avenue, as well as by the proposed new street Clear Creek Drive running through the site. The proposal is to build a “half-street” improvement. The easterly 50 feet of the street would include a two travel lanes and a parkrow and sidewalk on the south side of the street. The remaining approximately 470 feet would include two travel lanes, parking on the south side of the street, and a parkrow and sidewalk on the south side of the street.
- The frontage of the property located at 203 North Mountain Avenue will be improved with a parkrow and sidewalk to city standards.

In Planning Action #2006-01091, the Planning Commission determined that, “...adequate key City facilities can be provided to serve the project including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City

facility to operate beyond capacity.” The current proposal is for only two more units than were approved in 2006, and Planning Staff believes that this finding remains applicable to the current proposal.

## **2. Lot Line Adjustment**

Because the existing lots are triangular, creating significant narrow areas that make efficient land use difficult, the applicants propose to adjust the boundary lines in order to square off the lots and create a more logical and efficient configuration. The adjustments proposed include a reciprocal exchange between the Havurah and the owner of the adjacent Tax Lot #1700, Nancy Spencer, and adjustments between the Havurah property on Tax Lot #1701 and the adjacent Tax Lots #1500, #1600, and #18001.

## **3. Modification of Havurah Jewish Synagogue Conditional Use Permit**

The only changes proposed for the Havurah site have to do with the reconfiguration of property lines, expansion and improvement of the parking lot, relocation of the *sukkah* (a shelter used during the Jewish holiday of Sukkot), and expansion of the playground area. No changes to the existing Havurah building are proposed. A modification of the existing Conditional Use Permit is required in order to consider the potential ramifications of the proposed site modifications with regard to the on-going operation of the Conditional Use.

The reconfiguration will square the rear of the property, making the space more accessible and useable, and add 4,536 square feet to the lot area to allow for the playground and parking lot to be enlarged and for the *sukkah*, which is currently separated from the main building by a parking lot, to be moved to near a large English Walnut tree. The current parking configuration on the site has 17 spaces in an asphalt lot at the side of the building, and a gravel lot at the rear of the property which accommodates approximately 20 additional unmarked spaces. In conjunction with the reconfiguration and squaring of the rear of the lot, the parking lot is to be paved and a total of 52 parking spaces provided, 50 of which will be for exclusive use of the Havurah. The applicants also propose to add a short sidewalk to provide a link between the Havurah property and the residential portion of the development, noting that this would allow overflow parking for Havurah events along Clear Creek Drive, thus reducing the on-street parking impacts along North Mountain Avenue.

## **4. Site Review**

Site Review approval is requested to allow the construction of a six-unit, two-story residential condominium building and its associated parking and landscaping on the flag lot designated as Lot 10, adjacent to the railroad right-of-way. Vehicular ingress to the building is to be provided via easement from North Mountain Avenue through the Havurah parking lot, with egress via the lot's 20-foot flag drive exiting onto the newly improved section of Clear Creek Drive. Parking for this building is to be accommodated in a partial basement garage, which will contain eight parking spaces.

Two additional visitor parking spaces are to be provided in the adjacent Havurah parking lot. The application notes that this building's design is intentionally compact to be compatible with the surrounding neighborhood, and from the railroad tracks the building will appear to be only a single-story. The six units will range in size from 682 to 1,219 square feet, averaging 875 square feet per unit.

## **5. Tree Removal Permit**

One tree is identified for removal as part of the project, a nine-inch d.b.h. Pine tree located on Tax Lot #1701, the Havurah property. The application notes that the removal is necessary in order to complete the parking lot improvements to city standards and allow adequate back-up movements, and states that an appropriate mitigation tree will be provided as one of the nine proposed parking lot trees.

The applicants propose to retain a 40-inch d.b.h. Walnut street tree which was to have been removed under the Planning Action #2006-01091 subdivision approval. This tree is located in the North Mountain Street right-of-way near the southeast corner of the 203 North Mountain Avenue property, and future removal would be subject to a Street Tree Removal Permit.

## **II. Project Impact**

The project requires a subdivision approval since it involves the creation of residential lots. In accordance with Chapter 18.108, applications for Outline Plan approval are required to be reviewed under the "Type II" process with a public hearing.

### **A. Outline Plan for Performance Standards Options Subdivision**

In Staff's review of the proposal, the application appears to meet the approval criteria for Outline Plan approval. Chapter 18.88, Performance Standards Options, allows a flexible lot layout and design approach in an effort to preserve natural features as well as encourage creative and energy efficient site and building design. To this end, the base density of the project is for the total project site area. While perimeter and front yard setbacks must conform to the requirements of the underlying zoning district, the Chapter provides for flexibility with regard to lot sizes, widths, depths and interior site setbacks.

#### **1. Development of Adjacent Land**

The Outline Plan approval criteria require "That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan."

The parcels to the north of the site are similar to 203 N. Mountain Ave. in that the zoning is Single-Family Residential (R-1-5), and the lots are configured as long narrow parcels with homes situated at the east end of the lots and fronting on N. Mountain Ave. There is developable area on these lots behind the homes at the rear of the parcels. As a result, the area between the subject site and the southern edge of

the Mountain Creek Estates Subdivision (located at the southwest corner of the intersection of N. Mountain Ave. and Hersey St.) has development potential. Additionally, the property to the west of Mountain Creek has development potential. The area west of Mountain Creek is zoned Employment (E-1), and the bulk of the area is a 19.53 acre parcel owned by Union Pacific Railroad Company. This site is commonly referred to as the railroad property. The lots across from the site on N. Mountain Ave., on the east side of the street, are zoned Single-Family Residential (R-1-5) and are largely developed except for a parcel directly north of the railroad right-of-way and tracks. The large parcel is approximately ten acres in size, is not in the city limits and has a residence with several accessory buildings located on the lot. Finally, the area to the south of the site and south of the railroad right-of-way and tracks is zoned High Density Multi-Family (R-3) and Employment (E-1). This area includes a mixture of multi-family developments, single-family homes and a public works yard operated by the City of Ashland.

In Staff's opinion, a primary issue to consider in the development of the subject site is providing for the connection and coordination of the future street system and utilities to serve the developable areas to the north and west of the subject site. The City of Ashland Transportation Plan Map was updated in June 2002 to include the conceptual street locations and dedications for the railroad property area including the subject site (see attached map). The application has based the street layout and type on the adopted Transportation Plan Map. In addition, the application states that the "utility lines (water, sewer, storm, electric, etc.) will be upsized, in cooperation with the City, in order to accommodate future capacity needs." In Staff's opinion, the proposed subdivision will not prevent adjacent land from being developed in accordance with the R-1 zoning district, Ashland Land Use Ordinance and Ashland Comprehensive Plan.

## **2. Public Facilities and Street Standards**

The Outline Plan approval criteria require "That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity."

Existing and proposed facilities are discussed in the application narrative and noted on the site plan. Of particular note, a conceptual drainage plan detailing an engineered storm drainage splitter and pollution control structure has been provided to address longstanding drainage issues which have adversely impacted neighbors to the north. The proposed drainage structure would direct lower stormwater flows via four-inch storm and french drain lines to a four-inch outlet emptying to a bio-swale adjacent to the proposed wetland, with higher flows directed via new 15-inch lines through the subdivision, to Mountain Avenue and on to the existing 24-inch line at Village Square Drive. Staff believes that the application demonstrates that public facilities and utilities are in place or can be extended to service the project.

Transportation requirements are further addressed by the approval criteria which states that "The development complies with the Street Standards."

The City of Ashland Transportation Plan Map was updated in June 2002 to include the conceptual street locations and dedications for the railroad property area including the subject site. The plan identified a collector street connecting North Mountain Avenue to Oak Street. The eastern end of this collector street is shown on the subject property, and would eventually extend over Mountain Creek, traverse the railroad property and connect to the existing end of Clear Creek Drive. The location of the proposed street conforms to the adopted Transportation Plan Map. In addition, the new street has been designed to a Neighborhood Commercial Collector standard. The Neighborhood Commercial Collector standard is consistent with the existing west end of the street, Clear Creek Drive.

The previous subdivision application in 2006 included a U-shaped alley to provide vehicular access to the proposed homes while limiting the traffic flow impacts to the new collector street. The alley reduced the number of access points on the street, thus reducing potential vehicular conflicts and creating a safer pedestrian corridor for the proposed new collector street. During the public hearing for that application, some Planning Commissioners expressed concerns that the placement of the alley necessitated garage placement that reduced useable rear yard areas for the homes and adversely impacted solar access. The current application addresses those concerns by removing the proposed alley, but has again attempted to limit the traffic flow impacts to the street and satisfy controlled access standards by utilizing zero lot line units with shared driveways along the proposed new street.

Additionally, the majority of the east end of the new collector street will be located on the subject site rather than being more equally split between the subject site and the property to the north. The applicant is building both travel lanes with parking, a parkrow and a sidewalk on one side. A typical residential Neighborhood Street is a total of 47 feet in width including sidewalks and parkrows on both sides of the street. The proposed "half street" improvement to a collector street standard is comparable with a total width of 41 feet in improvements. This construction of the "half street" improvement will allow the property to the north to simply finish off the parking, parkrow and sidewalk on the north side if the property develops.

Ashland's Street Standards require a bicycle lane on a Neighborhood Commercial Collector street such as the proposed Clear Creek Drive section when there will be more than 3,000 vehicle trips per day or actual motor vehicle travel speeds in excess of 25 mph. Staff believes that when the areas to the north and west are fully developed, the trips per day on the new east-west collector connecting North Mountain Avenue to Oak Street will meet or exceed 3,000 vehicle trips per day, and as such provisions for bicycle facilities were a significant area of discussion in Planning Action #2006-01091. When the west end of the street, from Oak Street, was developed as Clear Creek Drive, a decision was made to have a multi-use bicycle and pedestrian path on the north side of the railroad tracks instead of installing bicycle lanes on Clear Creek Drive. As a result, a ten-foot wide easement was

established from the alley off of the south side of Clear Creek Drive along the full length of the railroad property. This existing multi-use path easement ends on the western boundary of the subject site, and the previous action required a ten to 12-foot wide public pedestrian and bicycle easement along the site's southern boundary for a future multi-use path. The applicants have carried this element through into the current application, proposing the bicycle and pedestrian easement as part of their open space and providing a pedestrian connection from that easement to Clear Creek Drive through the portion of the open space near the wetland.

The Performance Standards Options Chapter AMC 18.88 includes additional standards which require that private drives serving four or more units, and located in an R-1 zoning district, to be improved to City street standards and dedicated to the City as a public street. The application proposes a 20-foot width private flag drive for Lot 10, which is to contain a six-unit residential condominium building. The applicants have proposed to split the vehicular impacts of this building so that vehicles will enter via the Havurah parking lot from North Mountain Avenue to a garage located in the building's basement, and only exiting vehicles will utilize the lot's flag drive. As such, the vehicle trips via the proposed flag drive will be limited to one-half of the building's total vehicle trips, the equivalent of three units. Given that the drive will serve the equivalent of three units, that the proposed building will be fully sprinkled for fire protection, that the flag drive will be paved to a full 20-foot width, and that a fire hydrant and fire department connection are proposed on the lot, Staff believes that this proposal adequately addresses the street standards.

In Staff's opinion, the proposal largely meets the requirements of the Street Standards, and the applicants have done an admirable job of balancing between the project needs and the long-range planning issues relating to the future development of properties to the north and west.

### **3. Natural Features and Open Space**

The Outline Plan approval criteria require "That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas and unbuildable areas."

The maintenance of open space and common area is further addressed in the approval criteria which states "That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ration of amenities as proposed in the entire project."

The subject site includes Mountain Creek, the associated wetlands and trees. Mountain Creek is identified as a Riparian Land Drainage on the adopted Comprehensive Plan maps. The tree inventory included in the application identifies eight trees in the development area of the site that are larger than six inches diameter at breast height and greater (d.b.h.). The proposal is to retain all but one of the trees

on site; the tree to be removed is a Pine identified for removal to complete the parking lot improvements to city standards and allow adequate back-up movements, and states that an appropriate mitigation tree will be provided as one of the nine proposed parking lot trees.

In Staff's opinion, the application has identified the natural features on the site, and has preserved the significant natural features in the open space for the subdivision. In addition, a building envelope is proposed for the lot nearest the wetland that provides for a separation of 26 feet to buffer the wetland. The development is required to provide five percent of the total site area in Open Space; the open space area proposed is approximately 19,150 square feet or 12 percent of the total project area.

Additionally, the open space area including Mountain Creek and the wetlands at the west end of the site is consistent with the City of Ashland Open Space Plan and the draft Railroad Property Master Plan. The City of Ashland Open Space Program plan identifies the area along this section of Mountain Creek, from the railroad tracks to Williamson Way as an open space and future trail connection. The draft Railroad Property Master Plan also shows a park and trail in this area. Both plans show a corridor running along both sides of the creek corridor as open space with a trail parallel and west of the creek. In the location of this site, both plans identify an area approximately 150 feet wide with approximately 75 feet on each side of the creek, as open space. The proposed open space area at the west end of the site exceeds these dimensions as it is approximately 105 feet in width.

Currently, storm drainage from south of the site is collected at a point north of the railroad tracks, piped under the tracks and directed in an open ditch to the wetland/creek area. This is an old, existing condition that does not contain the necessary easements. Additionally, the situation is not created by the existing or proposed development on the site, but rather the site itself is impacted by the facilities and water flowing on the property and further on to properties to the north. The applicant has worked with the Ashland Engineering Division to remedy the situation in conjunction with the development of the property, and is proposing an engineered solution that would direct lower stormwater flows via four-inch storm and french drain lines to a four-inch outlet emptying to a bio-swale adjacent to the proposed wetland, with higher flows directed via new 15-inch lines through the subdivision, to Mountain Avenue and on to the existing 24-inch line at Village Square Drive. This solution is intended to preserve inflows to the wetland while alleviating the drainage problems historically experienced during significant storm events.

#### **4. Base and Bonus Density Standards**

The Outline Plan approval criteria require "That the proposed density meets the base and bonus density standards established under this Chapter."

The site has a base density of 16 units (3.59 acres x 4.5 units = 16.155). While the applicants are proposing only 15 units with this application, satisfying the density

requirements of Chapter 18.88, they propose to address certain density bonus provisions of the ordinance by constructing all of the new units to meet Ashland's Earth Advantage standards and increasing the amount of open space from the required five percent to 12 percent. These proposals have been made because the total project area used in density calculations for the current application includes the Havurah lot, and its Conditional Use Permit impacts have also been considered in light of its lot area. The applicants have proposed to provide additional open space and build the new units to the Earth Advantage standards to ensure that the project is compliant with density standards while maintaining the Havurah's impact as no greater than that of the target use for the R-1-5 zone. *(See further discussion of these impacts with regard to the Conditional Use Permit below.)*

## **5. Other Applicable Ordinance Requirements**

The Outline Plan approval criteria require "That the development meets all applicable ordinance requirements of the City of Ashland." The remaining issues that are addressed under this criterion are the setback and parking requirements.

Chapter 18.88 requires that one space is provided per unit on the street for guest parking. The application identifies a total of 19 parking spaces available on the project's street frontages, with seven proposed spaces along North Mountain Avenue and 12 proposed spaces along the newly proposed section of Clear Creek Drive. The proposal meets the on-street parking requirement.

The site plan delineates the proposed lot lines, building envelopes, and setbacks. While noting that all proposed buildings will comply with solar access standards, calculations to this effect have been provided only for Lot 10. The Solar Ordinance requires that all land divisions which create new lots demonstrate that a 21-foot high structure can be placed with a setback which does not exceed 50 percent of the lot's north-south lot dimension. Given the relationship of the newly created lots to the street right-of-way to the north, Staff believes that this requirement will be met, but a condition has been added to require that calculations demonstrating compliance be provided with the Final Plan submittal.

### **B. Lot Line Adjustment**

AMC 18.76.140 states, "The adjustment of a lot line by the relocation of a common boundary, where the number of parcels is not changed and all zoning requirements are met, shall be accepted by the City, provided the requirements of Sections 18.76.090 through 18.76.130 are satisfied, in addition to Section 18.76.170, where the lot adjustment causes access to be changed to an exterior unimproved street." The proposed lot line adjustment involves the relocation of the common boundaries of the parent parcels to the subdivision, and the number of parent parcels prior to the subdivision is unchanged. All zoning requirements are met, and final map requirements of Sections 18.76.090 through 18.76.130 will be addressed concurrently with recording of the final subdivision plat. None of the resultant lots' access will be changed to an unimproved street.

### **C. Modification of Havurah Jewish Synagogue Conditional Use Permit Approval**

The changes proposed for the Havurah site involve reconfiguration of property lines, expansion and improvement of the parking lot, relocation of the *sukkah* (a shelter used during the Jewish holiday of Sukkot), and expansion of the playground area. No changes to the existing building are proposed, and the modification of the existing Conditional Use Permit is required simply to consider the on-going operation of the Havurah as a Conditional Use in light of the potential ramifications of the proposed changes to the site.

The criteria for approval of a Conditional Use Permit are intended largely to address impacts of the use on existing facilities and upon the livability of the surrounding neighborhood. Because the Havurah use is existing, the criteria most effected by the proposal have to do with the generation of traffic and effects on surrounding streets and with ensuring that the use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone.

As proposed, the existing parking lot which has 17 parking marked parking spaces and approximately 20 additional spaces in an unimproved area behind the building will be improved to city standards to accommodate a total of 52 parking spaces, 50 of which will be for the exclusive use of the Havurah. In addition, a pedestrian connection from the Havurah parking lot to the flag drive for Lot 10 is proposed to allow for potential overflow parking for Havurah events within the subdivision, shifting the impacts now experienced by neighbors during some events from Mountain Avenue to within the subject property. Staff believes that these modifications will enable the Havurah site to better accommodate the impacts of the established use.

In considering the impacts of the proposal relative to the target use of the subject property, Staff noted that the proposal takes the Havurah property, which is already considered as part of the established Conditional Use Permit, into account as part of the density calculations of the overall project site. Staff initially expressed some concerns that this could be seen as “double-dipping”. To address these concerns, the applicants have provided calculations demonstrating that the total vehicle trips of the proposed uses of the overall project site would be 171 trips: 50 per day for the Havurah based on 9.32 trips per 1,000 square feet of the 5,246 square feet of the Havurah; and 121 trips for the proposed residential uses. They note that the total proposed vehicle trips are comparable to the 181 vehicle trips generated by 19 single family residential units, and as such have proposed to construct all of the proposed buildings to Earth Advantage standards and to provide additional open space in order to qualify for a density bonus that would accommodate 19 units on the site, when only 15 units are proposed. The application goes on to explain that Havurah members conducted a two-month study to verify the accuracy of the vehicle trip calculations, which were based on Institute of Traffic Engineers’ standards, and found that the vehicle trips experienced would be between 159 and 179 per day. Staff believes that the application effectively demonstrates that combined modifications to the Havurah site and construction of the residential development with the density bonus measures proposed will have no greater adverse material effect on the livability of the impact area than would development of the subject lot to its target residential use.

#### **D. Site Review**

The first criterion for Site Review approval is that, "All applicable City ordinances have been met or will be met by the proposed development." Site Review approval is requested to allow the construction of a six-unit, two-story residential condominium building and its associated parking and landscaping on the flag lot designated as Lot 10, adjacent to the railroad right-of-way. No variances are requested. The application notes that this building's design is intentionally compact to be compatible with the surrounding neighborhood, and from the railroad tracks the building will appear to be only a single-story. The six units will range in size from 682 to 1,219 square feet, averaging 875 square feet per unit. The Performance Standards Options Chapter AMC 18.88 includes a requirement that private drives serving four or more units located in an R-1 zoning district be improved to street standards and dedicated as a public street. The application proposes a 20-foot width private flag drive for Lot 10, which is to contain a six-unit residential condominium building. The applicants have proposed to split the vehicular impacts of this building so that vehicles will enter via the Havurah parking lot from North Mountain Avenue to a garage located in the building's basement, and only exiting vehicles will utilize the lot's flag drive. As such, the vehicle trips via the proposed flag drive will be limited to one-half of the building's total vehicle trips, or the equivalent of three units. Given that the drive will serve the equivalent of three units Staff believes that this proposal addresses the intent of the standard, and would also note that the proposed building will be fully sprinkled for fire protection, that the flag drive will be paved to a full 20-foot width, and that a fire hydrant and fire department connection are proposed on the lot,

The second criterion for Site Review approval is that, "All requirements of the Site Review Chapter have been met or will be met." The application notes that a common, screened trash and recycling area is to be provided, that the landscaping shall be maintained and irrigated, and that lighting and glare standards abided by.

The third criterion for Site Review approval is, "The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter." Parking has been provided in the proposed building's basement, buffering its impact to surrounding lots, and landscape and irrigation plans have been provided to address these standards.

The fourth criterion for Site Review approval is "That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options." As noted in the review of the impacts of the proposed Performance Standards Subdivision's in light of the Outline Plan criteria, Staff believes that the application demonstrates that public facilities and utilities are in place or can be extended to service the project.

## **E. Tree Removal Permit**

One tree is identified for removal as part of the project, a nine-inch d.b.h. Pine tree located on Tax Lot #1701, the Havurah property. The arborist's report identifies this tree as being in fair condition, but the application notes that the removal is necessary in order to complete the parking lot improvements to city standards and allow adequate back-up movements, and states that an appropriate mitigation tree will be provided as one of the nine proposed parking lot trees. The Tree Ordinance requires Tree Removal Permits only for significant trees (those over 18 inches d.b.h.) on vacant R-1 lands, however in this instance the tree is located on the Havurah lot and subject to that property's Conditional Use Permit. The applicants have chosen to address tree removal requirements as a means to consider the impact of the tree's removal, and Planning Staff believes that the tree's size, the proposed mitigation plantings, and the benefits provided by the proposed parking lot expansion and improvement justify the request.

The applicants also propose to retain a 40-inch d.b.h. Walnut street tree which was to have been removed under the Planning Action #2006-01091 subdivision approval. This tree is located in the North Mountain Street right-of-way near the southeast corner of the 203 North Mountain Avenue property, and future removal would be subject to a Tree Removal Permit given the tree's size.

The Tree Commission had not reviewed either proposal at the time of this writing. Tree Commission comments will be provided at the hearing.

## **III. Procedural - Required Burden of Proof**

**The criteria for Outline Plan approval are described in 18.88.030.A as follows:**

- a. That the development meets all applicable ordinance requirements of the City of Ashland.
- b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.
- c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.
- d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.
- e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.

- f. That the proposed density meets the base and bonus density standards established under this Chapter.
- g. The development complies with the Street Standards.

**The criteria for a Conditional Use Permit are described in 18.104.050 as follows:**

- A. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
- B. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.
- C. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:
  - 1. Similarity in scale, bulk, and coverage.
  - 2. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  - 3. Architectural compatibility with the impact area.
  - 4. Air quality, including the generation of dust, odors, or other environmental pollutants.
  - 5. Generation of noise, light, and glare.
  - 6. The development of adjacent properties as envisioned in the Comprehensive Plan.
  - 7. Other factors found to be relevant by the Hearing Authority for review of the proposed use.

**The criteria for Site Review approval are described in 18.72.070 as follows:**

- A. All applicable City ordinances have been met or will be met by the proposed development.
- B. All requirements of the Site Review Chapter have been met or will be met.
- C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.
- D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options. (Ord. 2655, 1991; Ord 2836 S6, 1999)

**The criteria for Issuance of Tree Removal are described in 18.61.080 as follows:**

A. Hazard Tree: The Staff Advisor shall issue a tree removal permit for a hazard tree if the applicant demonstrates that a tree is a hazard and warrants removal.

1. A hazard tree is a tree that is physically damaged to the degree that it is clear that it is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within public rights of way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated or the damage alleviated. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard or a foreseeable danger of property damage to an existing structure and such hazard or danger cannot reasonably be alleviated by treatment or pruning.
2. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.

B. Tree that is Not a Hazard: The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:

1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards. (e.g. other applicable Site Design and Use Standards). The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and
2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and
3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property.

The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with other provisions of the Ashland Land Use Ordinance.

4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.

#### **IV. Conclusions and Recommendations**

Staff believes the applicants have taken an admirable approach by designing a development that balances project needs with long-range planning items. The proposal has several

positive components that will benefit the residents of the development as well as the future railroad property neighborhood including the use of shared driveways to reduce potential conflict points on the future commercial collector street, an oversized open space that is consistent with the open space program and the draft Railroad Property Plan, and potentially providing the last link for a multi-use path easement along the north side of the railroad tracks.

In Staff's opinion, the proposal is consistent with the approval criteria for a 12-lot, 15-unit Performance Standards subdivision; modification of a previously approved Site Review and Conditional Use Permit for the Havurah Jewish Synagogue; Site Review to construct a two-story, six-unit residential building; the removal of one nine-inch diameter pine tree from Tax Lot #1701; and a boundary line adjustment between Tax Lots 1600, 1701, 1800 and 1700. Staff would accordingly recommend approval of the application with the following conditions attached:

- 1) That all proposals of the applicant are conditions of approval unless otherwise modified here.
- 2) That all conditions of the previous Conditional Use Permit approval for the Havurah Jewish Synagogue facility (Planning Action #2001-039) shall remain in effect unless specifically modified herein.
- 3) That prior to Final Plan approval:
  - a) Engineering for the utility plan including but not limited to the water, sewer, storm drainage and electric facilities shall be submitted. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, fire hydrants, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins, and locations of all primary and secondary electric services including line locations, transformers (to scale), cabinets, meters and all other necessary equipment. Transformers and cabinets shall be located in areas least visible from streets, while considering the access needs of the Electric Department. Any required private or public utility easements shall be delineated on the utility plan.
  - b) An Electric Distribution Plan shall be coordinated with the Ashland Electric Department, and shall be included in the utility plan with the Final Plan application.
  - c) Required pedestrian-scaled streetlights shall consist of the City of Ashland's residential streetlight standard, and shall be included in the utility plan and engineered construction drawings for the street improvements.
  - d) A drainage plan including final engineering of the storm drain splitter and pollution control device, any on-site storm water detention systems, and off-site storm drain system improvements shall be provided. The permanent maintenance of on-site storm water detention systems must be addressed through the obligations of the Homeowners' Association and approved by the Public Works Department and Building Division.

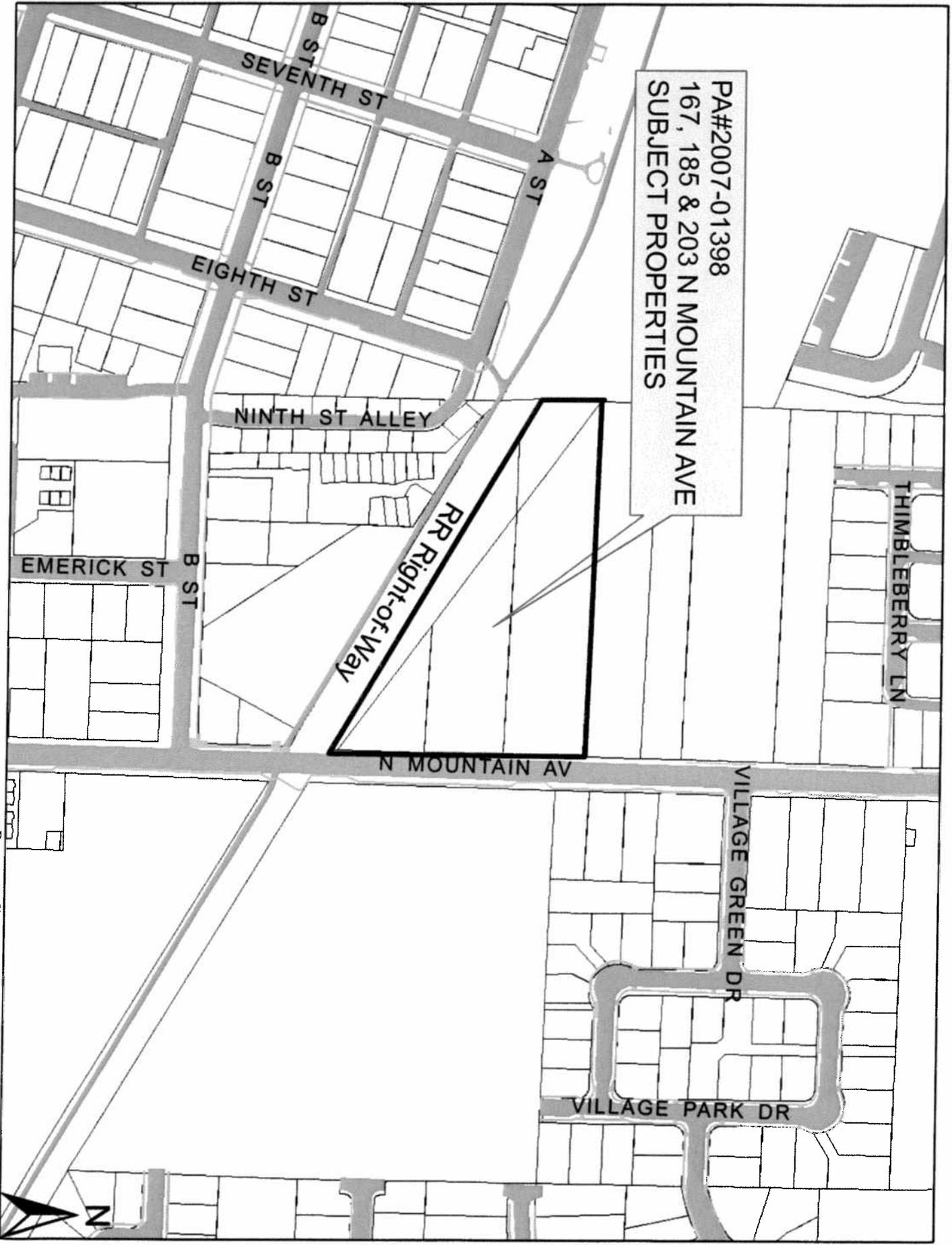
- e) The engineering for proposed street improvements shall be provided at Final Plan application. The engineering drawings shall address the treatment of the northern edge of the street and direction of storm drainage, and address the necessary curb radius and travel lane width for truck traffic use.
  - f) The boundaries of the wetland and the western property line of Lot 10 shall be delineated on site, and inspected by the Staff Advisor.
  - g) The final wetland determination/delineation report shall be prepared and submitted and the necessary state and federal permits received prior to the Final Plan application. If the final wetland determination/delineation report submitted for state and federal review differs significantly from the preliminary determination (i.e. larger area or numerous wetland areas), the Outline Plan shall be modified prior to an application for Final Plan approval.
  - h) The recommendations of the Ashland Tree Commission, where consistent with the Site Design and Use Standards, shall be incorporated and with final approval by the Staff Advisor, shall be incorporated into the Landscaping, Irrigation, and Tree Protection and Removal Plans.
  - i) A draft copy of the CC&R's for the Homeowners Association shall be provided. CC&R's shall describe responsibility for the maintenance of all common area and open space improvements including landscaping, irrigation, parkrows, and street trees. CC&R's shall note that any deviation from the approved Tree Protection Plan must receive written approval from the City of Ashland Planning Department.
  - j) Fence heights within side and rear yard areas adjoining the open space shall not exceed four feet. Stipulations with regards to fencing shall be described in the project CC&R's, and the location and height of fencing shall be identified at the time of building permit submittals. Fence permits shall be obtained prior to installation.
  - k) Lot coverage shall be based on overall coverage for the project site, and shall be limited to the building envelopes, driveways and walkways depicted in the approved site plan. Allocation of specific lot coverage amounts to individual lots, and demonstration that the overall subdivision complies with the lot coverage allowed in the R-1-5 zone, shall be provided.
  - l) Project phasing details shall be clarified in the final plan submittal.
- 4) That prior to the signature of the final survey plat:
- a) All easements for sewer, water, electric, streets, parking, public pedestrian access and mutual access shall be indicated on the final survey plat as required by the City of Ashland.
  - b) A drainage way easement shall be indicated on the final survey plat for the width and length of the western open space area including Mountain Creek and the associated wetlands.

- c) A public pedestrian and bicycle easement, a minimum of ten feet in width, shall be indicated on the final survey plat for future development as a multi-use path parallel to the railroad right-of-way and along the length of the southern boundary of the site, connecting the western site boundary to N. Mountain Ave. In addition, the western open space shall include a north-south path connecting the future multi-use path parallel to the railroad right-of-way and the new street.
- d) Street trees, located one per 30 feet of street frontage, shall be installed in the parkrow along street frontages as part of the subdivision infrastructure improvements. Street trees shall be chosen from the Recommended Street Tree List and shall be installed in accordance with the specifications noted in the Recommended Street Tree List. The street trees shall be irrigated.
- e) Calculations shall be submitted demonstrating that a 21-foot high structure can be placed on the new lot with a Standard A Solar Setback that does not exceed 50 percent of the lot's north-south lot dimension, or a solar envelope and written description of its effects demonstrating compliance with Solar Standard A shall be submitted as required in Chapter 18.70.050 of the Solar Ordinance.
- f) Common area and open space improvements (i.e. landscaping, irrigation, wetland ponds, bio-swale, etc.) shall be installed or bonded for in accordance with the procedures in the Subdivision chapter. The project landscape architect shall inspect the common area and open space improvements for conformance with the approved plan, and shall submit a final report on the inspection and items addressed to the Ashland Planning Division. The City shall receive written verification from the Oregon Division of State Lands acknowledging that the wetlands have been constructed and planted per the approved plan.
- g) That public improvements to the proposed new section of Clear Creek Drive shall be installed from the northeast corner of the project west to the eastern boundary of the proposed Lot 12. Improvements shall be consistent with those described in the application, including paving, on-street parking, curb and gutter, storm drain system, seven-foot planting strip and 6-foot wide sidewalk. A reserve strip ("street plug") shall be provided along the northern boundary of the street improvements.
- h) Subdivision infrastructure improvements, including but not limited to utilities, streets, street lights, sidewalks, street trees, multi-use path, temporary curbing, and temporary fencing shall be installed according to approved plans prior to the signature of the final survey plat.
- i) Electric services shall be installed underground to serve Lots 1-10. At the discretion of the Staff Advisor, a bond may be posted for the full amount of underground service installation (with necessary permits and connection fees paid) as an alternative to installation of service prior to signature of the final survey plat. In either case, the electric service plan shall be reviewed and approved by the Ashland Electric Department and Ashland Engineering Division prior to installation.

- j) That the sanitary sewer laterals and water services including connection with meters at the street shall be installed for Lots 1-10.
  - k) That the requirements of the Fire Department, including that approved addressing shall be installed prior to combustible construction and that a fire apparatus access, fire apparatus turn-around, and fire hydrants shall be installed, shall be addressed.
- 5) That prior to the issuance of a building permit:
- a) Individual lot coverage calculations including all impervious surfaces shall be submitted with each building permit. Building footprints, walkways, driveways, parking areas, and any impervious surfaces shall be counted for the purpose of lot coverage calculations.
  - b) The setback requirements of 18.88.070 shall be met and identified on the building permit submittals including but not limited to the required width between buildings as described in 18.88.070.D.
  - c) That all new structures shall meet Solar Setback A in accordance with Chapter 18.70 of the Ashland Land Use Ordinance. Solar setback calculations shall be submitted with each building permit and include the required setback with the formula calculations and an elevation or cross-section clearly identifying the height of the solar producing point from natural grade.
  - d) That a Verification Permit in accordance with 18.61.042.B shall be applied for and approved by the Ashland Planning Division prior to removal of the Pine tree, prior to site work, storage of materials and/or the issuance of an excavation or building permit. The Verification Permit is to inspect the tree to be removed and the installation of the tree protection fencing. The tree protection for the trees to be preserved shall be installed according to the approved Tree Protection Plan prior to site work or storage of materials. Tree protection fencing shall be chain link fencing a minimum of six feet tall and installed in accordance with 18.61.200.B.
  - e) That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in substantial conformance with those approved as part of this application, an application to modify this Site Review approval shall be submitted and approved prior to issuance of a building permit.
  - f) That exterior building materials and paint colors shall be compatible with the surrounding area, and sample exterior building colors shall be provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with II-B-6a) of the Multi-Family Site Design and Use Standards.

- g) That all of the proposed new units shall be constructed to Conservation Division Earth Advantage standards as proposed by the applicants. Evidence of compliance with these standards shall be provided for each new unit prior to the issuance of a building permit.
- 6) That prior to the issuance of a certificate of occupancy:
- a) Fire Sprinklers shall be installed in the six-unit building on Lot 10 as proposed by the applicants.
  - b) All exterior lighting shall be directed on the property and shall not illuminate adjacent properties.
  - c) The inverted u-racks shall be used for the bicycle parking. All bicycle parking shall be installed in accordance with the standards in 18.92.040.I and J prior to the issuance of the certificate of occupancy. The building permit submittals shall verify that the bicycle parking spacing and coverage requirements are met in accordance with 18.92.040.I.
  - d) That the screening for the trash and recycling enclosure shall be installed in accordance with the Site Design and Use Standards prior to the issuance of a Certificate of Occupancy. An opportunity to recycle site of equal or greater size than the solid waste receptacle shall be included in the trash enclosure in accordance with 18.72.115.B.

PA#2007-01398  
167, 185 & 203 N MOUNTAIN AVE  
SUBJECT PROPERTIES

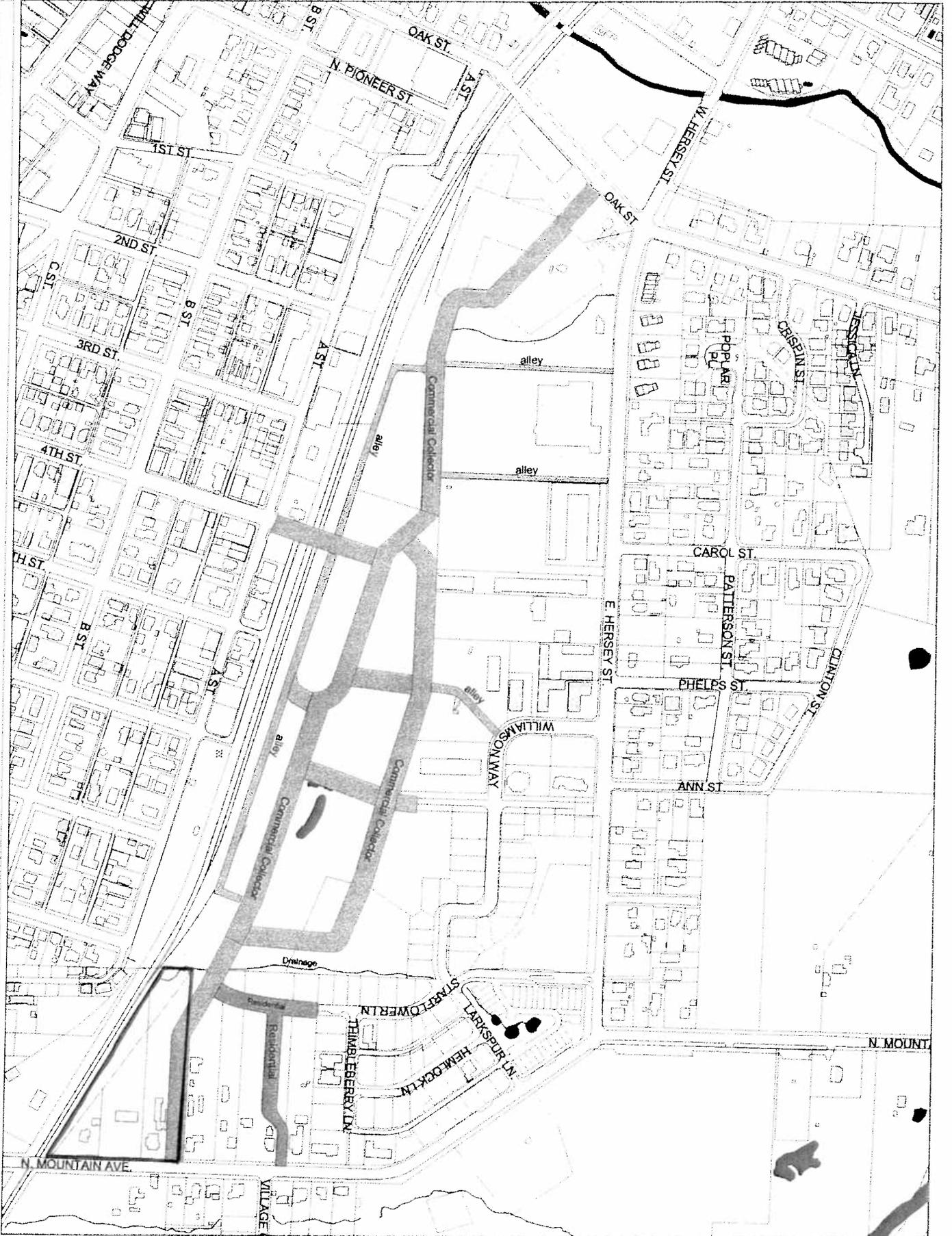


Property lines are for reference only, not scalable



CITY OF  
ASHLAND

Proposed Street Dedications, amendments to the City of Ashland Transportation Plan Map.





**ADDRESS & LEGAL DESCRIPTION:** 167, 185 and 203 North Mountain Avenue,  
391E 09AA 1500, 1600, 1700, 1701 and 1800

**PROJECT INFORMATION:**

**APPLICANTS:**

Havurah Friends Investment Group, LLC  
Jewish Community Havurah of S.O.  
Ashland, OR 97520  
Tel: 482-3961

**LAND USE PLANNING:**

Urban Development Services, LLC  
700 Mistletoe Road, Suite 204  
Ashland, OR 97520  
Tel: 482-3334

**DRAFTING & DESIGN**

Medinger Construction Company, Inc.  
P.O. Box 702  
Ashland, OR 97520  
Tel: 482-3961

**ARBORIST:**

Upper Limb-it Tree Service  
P.O. Box 881  
Ashland, Oregon 97520  
Tel: 482-3667

**SURVEYOR:**

Polaris Land Survey  
P.O. Box 459  
Ashland, Oregon 97520  
Tel: 482-5009

**WETLAND BIOLOGIST:**

Northwest Biological  
324 Terrace Street  
Ashland, Oregon 97520  
Tel: 488-1061

**COMPREHENSIVE PLAN DESIGNATION:**

Single Family Residential

**ZONING DESIGNATION:**

R-1-5-P

**LOT STATISTICS:**

Lot Area:	391E 09AA	#1500 = 1.682 acres
		#1600 = 0.318 acres
		#1701 = 1.039 acres
		#1800 = 0.549 acres
		Total: = 3.59 acres

Lot Coverage:	R-1-5-P District Permitted:	50%
	Proposed:	48.77%

Parking and Alley:	15.55%
Structures:	25.33%
Drive & Sidewalks:	7.89%
Landscaping:	51.23%

Density (R-1-5-P):  
 Base Density: 4.5 units per acre (4.5 units X 3.59 acres) = 16.15 units  
 Density Bonus: Conservation Housing - Earth Advantage (15%) = 2.421 units  
 Density Bonus: Recreational Space = 7% (initial 5% excluded) = 1.130 units  
 Total Permitted Density: = 19.697 units  
 Proposed Density: = 15 units

**APPLICABLE ORDINANCES:**

Single Family Residential, Chapter 18.20  
 Performance Standards Option, Chapter 18.88  
 Tree Preservation & Protection, Chapter 18.61  
 Conditional Use Permits, Chapter 18.104  
 Site Design & Use Standards, Chapter 18.72  
 Site Design & Use Standards (booklet)

**ADJACENT ZONING/USE:**

West: E-1, Employment  
 East: R-1-5-P, Single Family Residential  
 South: R-1-5-P, Single Family Residential &  
 R-3, High Density, Multi-Family Residential  
 North: R-1-5-P, Single Family Residential  
*Subject Site: R-1-5-P, Single Family Residential*

**PLANNING ACTION:**

The applicants wish to obtain approval for five entitlements:

- 1) An Outline Plan Subdivision for a 12 lot subdivision with one common open space lot, one lot for an existing single family home, one lot for the existing Havurah Jewish Synagogue, eight lots single family units and one lot for a six unit two story condominium building (NOTE: the proposed application would supercede the recently approved 14-lot / 13-unit residential approved in 2006 (PA-2006-01091).
- 2) Lot Line Adjustments with Tax Lots #1600, #1700, #1701 and #1800.
- 3) A Modification to the Conditional Use Permit granted for the Havurah Jewish Synagogue in 2001 (PA-2001-039); and
- 4) A Site Review Permit for the construction of a six-unit two-story residential building and its associated parking and landscaping improvements.
- 5) A Tree Removal Permit for the removal of one 9" dbh Pine Tree on Tax Lot #1701 (Havurah Property).

**APPLICANTS:** Since the Planning Commission's October 2006 approval (2006-01091), the subject property was purchased by a number of members from the adjacent Havurah Jewish

Synagogue and Larry Medinger, Medinger Construction to form Havurah Friends Investment Group, LLC. The group has been meeting weekly with the primary desire to create a more sustainable living and social environment than the previous land use approval.

**PROPERTY DESCRIPTION:** The subject properties are located at 167, 185 and 203 North Mountain Avenue – directly north of the railroad tracks. To the north and east of the property are single family residences. To the west is the vacant Union Pacific Railroad property. The property involves five parcels, including a minor Lot Line Adjustment with the Spencer property (Tax Lot #1700) located at 167 North Mountain. The four primary parcels are Tax Lots #1500 (1.70 acres), #1600 (.32 acres), #1701 (1.04 acres) and #1800 (.55 acres) for a total of 3.59 acres. All of the parcels are somewhat oddly shaped with acute angles primarily due to the traversing railroad right-of-way (see map insert below).

203 North Mountain Avenue (Tax Lot #1500): The property at 203 North Mountain Avenue is currently occupied by an historic single family home located near the street frontage. The home will remain in place and is identified on the site plan as being on Lot #1. According to Jackson County assessor records, the home was constructed in 1905 and is approximately 1,700 square feet in area. The applicants believe the home is structurally sound, has some historical merit and has architectural qualities that should be retained.

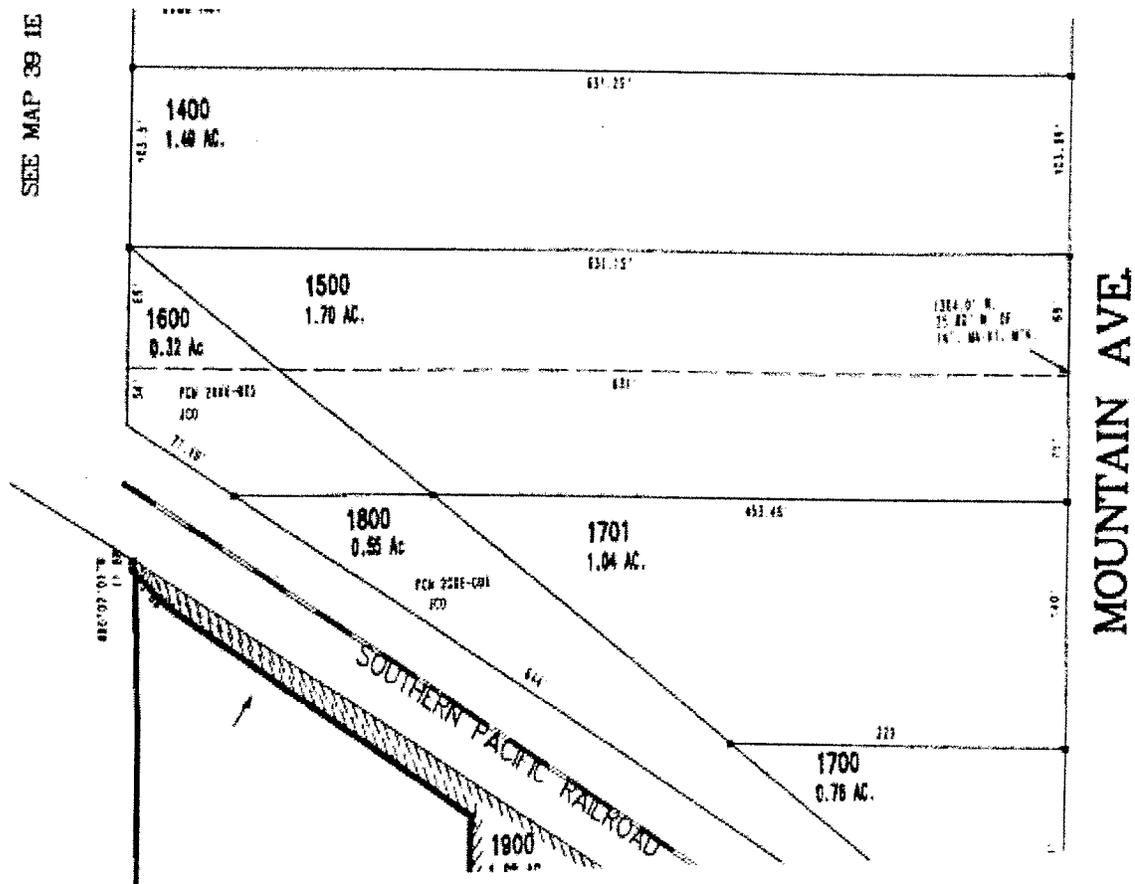
185 North Mountain Avenue (Tax Lot #1701): 185 North Mountain is the site of the Havurah Jewish Synagogue facility. The property is approximately 47,144 square feet in size and includes an approximate 5,426 building housing a small sanctuary, classrooms, assembly space, offices, bathrooms and dining area. In May of 2001 the building was remodeled and the sanctuary added. At that time, the front of the property was enhanced with not only new landscaping, but also an attractive building façade. The Synagogue's parking sits to the side and rear of the building with vehicular access from North Mountain Street.

167 North Mountain Avenue (Tax Lot #1700): The property located at 167 North Mountain is owned by Nancy Spencer. The applicants have been working with Mrs. Spencer in order to correct oddities in their shared property lines in an attempt to further the comprehensive planning of this area. The property is triangular shaped and is .404 acres (17,594 sq. ft.). An existing single family residence, constructed in 1900, sits in the middle of the property facing Mountain Avenue. A small structure sits to the north and slightly to the rear of the single family residence. A number of trees sit on the property but most are along the north property line (shared property line of the Havurah site).

Tax Lots #1600 and #1800: These two parcels are oddly shaped vacant and remnant properties likely created from surplus railroad right-of-way. The parcel lines will be adjusted or merged into the adjacent properties making logical and efficient boundaries. A small wetland area at the far western end of the property consisting of approximately 4,780 square feet exists and will be incorporated into the Subdivision's open space. Within the wetlands there are groups of trees and plants indicative of wetland vegetation. According to the project's Wetland Biologist, the wetland's hydrology comes from an adjacent storm water culvert extending underneath the railroad tracks at the end of Eight Street and another

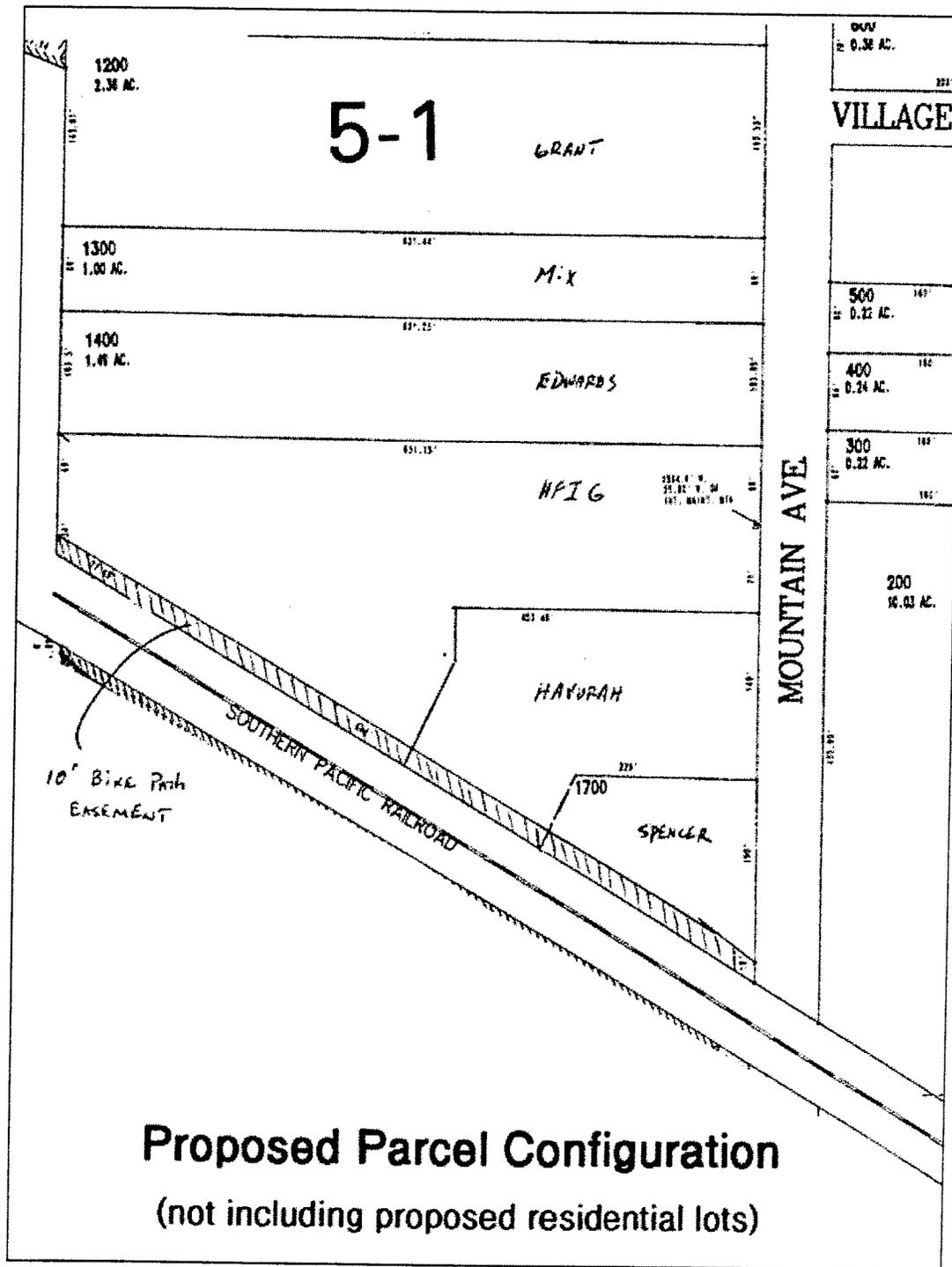
extending from the City's equipment storage yard along "B" Street. The storm water is collected from both the Ashland Railroad District area and the Emerick/Eureka Street area.

Unfortunately, no public easements or maintenance agreements exist for either storm drain source to cross the property and likely only exist due to grandfathered circumstances. In addition, there have been many documented cases where excess storm water from these sources have caused flooding problems onto many neighbors to the north – including the homes in the Parkridge and Starflower Subdivisions to the north. The lot containing the wetland is identified on the Site Plan as Lot #12 and is intended to remain as a private open space.



### Existing Parcel Configuration

*The above illustration is from the Jackson County Assessor's Maps and shows the subject property (Tax Lots 1500, 1600, 1701 & 1800). The illustration also shows Ralph Edwards' property (Tax Lot 1400) and Nancy Spencer's property (Tax Lot 1700).*



*The above illustration shows the proposed lot line adjustments for the subject property, Havurah property and Mrs. Spencer's property. Please see attached Site Plan exhibits for proposed residential lot details.*

**BACKGROUND:** In October of 2006, a very similar application was approved by the Ashland Planning Commission (PA-2006-01091) for a 14-lot / 13-unit subdivision. That application did not include Tax Lots #1700 or #1701.

In 2002, Tax Lot #1500 (large parcel) was identified on the City's adopted Transportation Plan Map as being the "primary parcel" providing ingress and egress to the Ashland Railroad property. The street is designated and planned to be a Commercial Collector Street and will eventually link with Clear Creek Drive which connects with Oak Street. In addition, the subject wetland has been identified by the City as a riparian corridor and more recently by Fishman & Associates Consulting (draft).

In 2001, a Conditional Use Permit (PA-2001-039) was granted by the Ashland Planning Commission to remodel the existing Havurah Jewish Synagogue and to add a sanctuary. At that time, the front of the property was enhanced with not only new landscaping, but also an attractive building façade.

No other land use planning or building history could be found for the subject properties.

**PROJECT PROPOSAL:** The applicants are requesting the following four entitlements:

Outline Plan Subdivision (1) & Lot Line Adjustment (2): With the inclusion and/or adjustments of adjacent tax lots, the applicants contend the proposed subdivision is far superior than the previous plan. With the inclusion of the additional properties – properties that due to their location or physical circumstances, would likely be ill-considered remnants in the future and autonomous to the neighborhood. The properties as proposed will contribute to the overall neighborhood planning and usefulness of infill lands. There are five primary differences between the 2006 plan and the 2007 plan:

First, the previous application was for a 14-lot / 13-unit residential subdivision and included Tax Lots #1500, #1600 and #1800 for a total area of 2.57 acres. The current proposal is for a 12-lot / 15-unit residential subdivision that now also includes Tax Lot #1701 for a total area of 3.59 acres. The size of the property increased by one acre, increased by two units and now incorporates the Havurah Jewish Synagogue property.

Second, the previous application was for 13 single family units (1 existing / 12 new) with each unit ranging in size from 1,800 sq. ft. to 2,000 sq. ft. The previous units were to be predominately two-story buildings. The proposed application is for 9 single family units (1 existing / 8 new) ranging from 1,458 square feet to 2,008 square feet and predominantly single story buildings. The six condominium units range in size from 682 square feet to 1,219 square feet with an overall average size of 875 square feet.

Third, the previous plan provided a public alley serving 11 of the 13 units and the current plan identifies a private driveway (flag lot driveway) extending from the new street and a private driveway extending from North Mountain Avenue through the a reconfigured parking lot. One of the primary issues with the 2006 application was some of the Planning Commissioner's concerns that the looped alley tended to consume the rear yards and the resultant garages consumed the southern exposure of the homes preventing solar access. Such a commitment to an extensive alley system also contributed to excessive impermeable surfaces creating additional storm water run-off. While alleys are more preferable in the

majority of planning circumstances, on small or odd shaped parcels they can also create other types of constraints or impacts as believed by the current applicants. In the 2007 application, the applicants have designed the street facing units so that the garages are either recessed from the front façade or maintain access off the short flag driveway in order to retain a traditional streetscape with porches, visible entrances and articulated facades in an attempt to provide for a more human scale streetscape.

Fourth, the previous application identified the site's wetlands as being retained with no improvements proposed. The current application still retains the site's wetlands, but now proposes to "add" additional wetlands to enhance the natural environment and to provide additional storm water detention. The existing and added wetlands will remain within a single parcel owned in common by the project's Home Owner's Association.

Finally, although the 2006 application considered the future needs of its adjacent neighbor to the north (Tax Lot #1400), the 2007 application has expanded upon these considerations by including the neighbors to the south (Tax Lots #1700 and #1701). Such inclusions have allowed the applicants to participate in comprehensively planning and eventually implementing land use policies such as an integrated street and pedestrian network, preservation and creation of new wetlands, shared parking, reduced dependency on automobiles, improved public safety and reduced curb cuts (access points) along primary streets.

Conditional Use Permit Modification (3): The Havurah Friends Investment Group (HFIG) are largely members of the Havurah Jewish Synagogue hoping to not only minimize development impacts on the Havurah, but to improve the Havurah's property so that parking and pedestrian circulation can be significantly improved. In addition, the members hope to occupy the six-plex condominium building with family and elderly members of the Havurah creating an on-site living community.

Nevertheless, religious facilities in the City of Ashland are Conditional Uses and subject to the Conditional Use Permit Criteria (18.104.050) whenever a *change in site configuration or use* is proposed. In this application's case, the Havurah Jewish Synagogue is *not* proposing any changes to their building, a change in membership or a change to their typical activities. The only changes proposed for the Havurah is the reconfiguration of property lines, parking lot expansion, parking lot improvements, Sukkah relocation and playground expansion. However, because the Havurah's property is now part of the development proposal and the subdivision's density is based upon the entire property, the application includes an explanation and findings of fact justifying the modification.

The following modifications are proposed for the Havurah property:

First, the Havurah's property boundaries will be adjusted so that its current acute configuration in the rear will be squared making the space more accessible and useable. The current property configuration makes it difficult to use the rear of the property as the Sukkah structure is separated by a parking lot. A portion of the parking lot is gravel and not consistent

with standard parking lot configurations nor does it meet current Planning and Building code regulations.

Secondly, the Havurah's property boundaries will be increased in area by an additional 4,536 square feet. The combination of the lot line adjustment and additional property allows for the playground area to be enlarged and the Sukkah structure to be moved near the English Walnut Tree along the north property line. Also, the enlarged area now incorporates a community garden and a large English walnut tree currently on Tax Lot #1701. Note: With a previous application, the walnut tree was proposed to be removed in order to incorporate an alley into the subdivision, but was later planned to be within a private yard. This specimen tree's ultimate survival has always been a concern for the membership but with its incorporation into the Havurah's property, it can continue to flourish.

Third, the parking lot will be reconfigured to increase on-site parking and reduce the demand for off-site parking during special events in an attempt to better accommodate members and reduce impact to neighbors. Currently, the existing parking lot is partially paved with 17 delineated parking spaces and a gravel parking area accommodating another 20+/- parking spaces. Unfortunately, the parking count varies wildly because of the odd property line configuration in combination with the inability to easily delineate parking spaces on an unpaved surface. With the proposed changes to the property boundaries and the completion of parking lot improvements, the parking will be safe and accessible with an increase in parking spaces from 37+/- to 52 parking spaces.

Also, the site plan includes a short sidewalk linking the single-family phase of the development to the Havurah's property. The intent of the sidewalk is to provide the Havurah's membership with a secondary option for overflow parking during special events. In this case, the applicants contend a significant portion of on-street parking along North Mountain Avenue will now use Clear Creek Drive. And, depending upon their parking location either travel along the new pedestrian sidewalk or along the new sidewalk along North Mountain Avenue - thus reducing the dependency of North Mountain Avenue and increasing pedestrian safety.

Fourth, the parking lot will be fully improved to City Site Design and Use Standards which includes storm water management, striping, curbing, landscaping and added parking lot trees for shading and to minimize urban heat gains. More importantly, because the parking lot will be improved to current City Planning and Building (ADA) standards, the parking lot will be safer to traverse due to an orderly design and vehicular circulation pattern.

Lastly, by including the Havurah property into the Outline Plan Subdivision proposal, the property is being modified due to the allocation of the lot's permitted density. In this case, the Havurah property has a base density of 4.5 units to the acre or 4.67 units. The combination of the lots (except for Tax Lot #1700) allows a base density of 16 units (3.59 acres X 4.5). In addition, it should be noted that although the applicants are only proposing 15 units (1 existing / 14 new), they are proposing to meet certain density bonus provisions of Chapter 18.88 such as constructing each of the new units to meet Ashland's Earth Advantage standards and increasing the amount of open space from the required 5% to 12%. In doing so,

the permissible density permitted would be close to 20 single family dwelling units. Nevertheless, the applicants are only proposing a total of 15, nine of which will be single family homes and six of which will be small attached units intended to provide on-site living for members of the Havurah.

Site Review Permit (4): A Site Review Permit is being requested for the construction of the six-unit two-story residential building and its associated parking and landscaping improvements which also includes the Havurah's parking lot. The improvements are subject to the City of Ashland Site Design and Use Standards.

Tree Removal Permit (5) A Tree Removal Permit is required to remove one tree on Tax Lot #1701. The subject tree is a 9" d.b.h Pine Tree. The tree's removal is necessary in order to complete the parking lot improvements. The tree will be replaced within the landscape islands of the parking lot. The replacement tree was chosen from the City's adopted Street Tree List for its urban environment.

### **PROJECT DETAILS:**

Site Plan: The attached site plan identifies the proposed lot configurations, circulation patterns, sidewalks, trees, parking, open space areas, easements and building envelopes showing building footprints, yard areas, porch locations and garage locations. The concept of the subdivision is intended to be "neo-traditional" with street friendly facades, inviting building entrances and recessed garages where possible. Overall, the proposed site layout is similar to the previous subdivision's approval with all of the new single family homes facing the new street connecting North Mountain Avenue and the property to the west (Clear Creek Drive). However, unlike the previous application, there is not an east/west alley as this design was believed to consume too much rear yard space and limit solar access onto the residences.

Circulation: Access to the units will occur from either North Mountain Avenue, the new street (Clear Creek Drive) or the short flag driveway. The new street extending through the site from North Mountain Avenue will eventually connect with the Railroad Property to the west and eventually with Oak Street as well as Hersey Street. The street's location is exactly as identified on the City's Transportation Plan Map which is intended to identify primary street locations in order to comprehensively plan for the City's future transportation needs. The subject street is identified as a Neighborhood Commercial Collector which will carry traffic from the future Railroad Property neighborhood to North Mountain Avenue. The overall width of the street right-of-way will be 63' with 41' of the right-of-way on the applicants' property. The remaining 22' will be dedicated and improved with the eventual development of the north fronting parcel (Tax Lot 1400). The improvements to be done as part of this subdivision will include a ½ street improvement + 10' in order to accommodate ingress and egress movements. The design of the street right-of-way show two 10' travel lanes, 8' on-street parking lanes on each side, curbs, 7' parkrows and 6' wide sidewalks. Until the adjacent property to the north develops or the Railroad Property to the west develops, the street will only extend into the property approximately 400' with the combination of the proposed alley allowing the street to function like a dead-end street with the alley's entrance being a temporary fire truck turn around. The design of the street as well as the temporary turn around

has been preliminarily approved by the Ashland Fire Department with final civil drawings being submitted with the project's Final Plan phase.

In addition to the new street, the plan includes a unique circulation pattern intended to reduce automobile dependency on a single street or intersection, increase pedestrian connectivity, increase alternative parking scenarios during larger religious services and spread the vehicular circulation patterns. In particular, the revised plan identifies a hybrid "one-way" system (vehicular only) for the tenants of the six-plex building. In this circumstance, the tenants will enter from the Havurah's parking lot, parking in their designated space on the ground floor of the building and exit to Clear Creek Drive via the short flag driveway. This allows for an equal percentage of vehicular trips between the Havurah's driveway and the flag driveway. Lastly, the flag driveway allows for the tenants of Lots #6 and #7 to use the flag driveway, as permitted by AMC 18.76.060 B, without having to force their driveways and garages to extend directly from Clear Creek Drive – a standard Transportation Plan policy and Planning Code directive. At the time of final plat, all private property circulation patterns will be recorded through easements and identified on the plat.

The existing house's driveway off North Mountain Avenue will be removed and relocated off the new street. The North Mountain Avenue frontage street will also be completed with planting strips, street trees and sidewalks.

Pedestrian Circulation: The project includes two valuable pedestrian connections or "shortcuts" intended to not only reduce trip generation, but also to spread parking demand. For instance, the pedestrian connection traversing through the open space lot links Clear Creek Drive to the Railroad District and such uses and amenities as Railroad Park, restaurants, shopping, the central bike path, etc. As one evaluates the City's Transportation System Plan or the draft circulation plan for this area, it becomes obvious the connection will play an important role in reduced vehicle trip miles and create an opportunity to link the two neighborhoods together. Without this path, pedestrians would take an extra 10 minutes to walk a very circuitous route (approximately 2,500 feet – or ½ a mile) to get to Railroad Park vs. taking the proposed path which is approximately 200 feet or a 1 minute walk. The applicants contend this path will eventually be considered a unique and important pedestrian amenity as the Alice Peil Walkway (behind the City's Community Development Offices across from Lithia Park) is considered today.

In addition, the private pedestrian connection via the short flag drive linking Clear Creek Drive and the Havurah Synagogue provide members an alternative parking location during special events instead of solely parking along North Mountain Avenue. Also, the path will encourage future residents to use the path providing a short and convenient link from their homes to the Synagogue.

Density: The property is 3.59 acres in size, zoned R-1-5 with a base density of 4.50 units per acre for an allowed density of 16.15 dwelling units. The applicants are proposing 15 units (1 existing / 14 new). However, as noted above, the applicants are proposing to meet certain density bonus provisions of Chapter 18.88 such as constructing each of the new units to meet Ashland's Earth Advantage standards and increasing the amount of open space from the

required 5% to 12%. In doing so, the permissible density permitted would be close to 20 (19.7) single family dwelling units. Again, the applicants are only proposing a total of 15 units, nine of which will be single family homes and six will be modestly sized attached units clustered under one roof.

Base Density: 4.5 units per acre (4.5 units X *3.59 acres)	=	16.15 units
Proposed Density:	=	15 units
Density Bonus: Conservation Housing - Earth Advantage (15%)	=	2.421 units
Density Bonus: Recreational Space = **7% (initial 5% excluded)	=	1.130 units
Total Permitted Density:	=	19.697 units

\*One of the primary intentions with this application is to reshape the lot lines in order to create lots that are efficient and as logical as possible. As such, a Lot Line Adjustment with Tax Lot #1700 is proposed in an attempt to improve property line configuration. The adjusted property area is intended to be a reciprocal amount with the area granted to the Havurah and equal amount will be granted back to Tax Lot #1700 (Nancy Spencer, Property Owner).

** Total proposed open space areas (bike path and wetland area)	=	19,150 sq. ft.
Recreational Space = 5% required open space ( <i>not included per 18.88.040. B.3.b.2.</i> )		
156,299 sq. ft. total area X .05	=	7,815 sq. ft.
Recreational Space = 7% additional open space ( <i>included per 18.88.040. B.2.b.2.</i> )		
156,299 sq. ft. total area X .07	=	11,335 sq. ft.

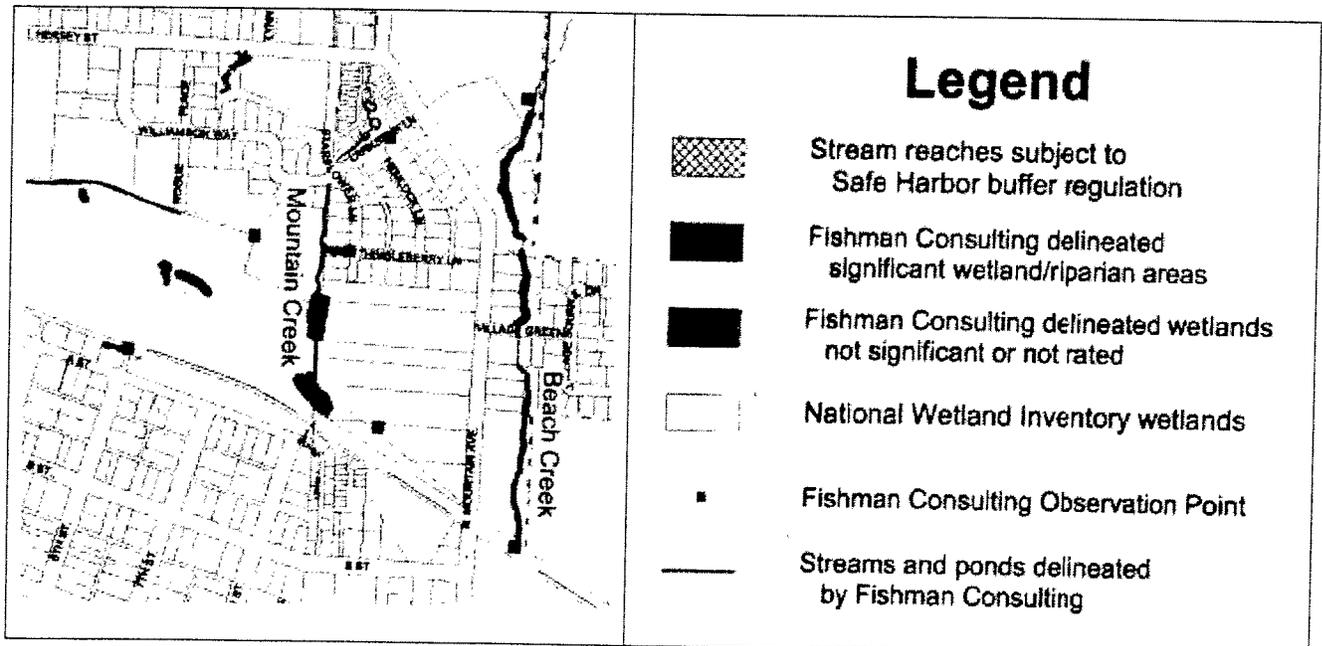
Wetlands / Open Space: The site plan has been designed to recognize the site’s natural features such as the small wetland area within the site’s western boundary (Tax Lot #1600). The wetland has been identified on the Draft Fishman Wetland Report and further delineated by the project’s Biologist, Scott English. Within the wetlands there are groups of trees and plants indicative of wetland vegetation. According to the project’s Biologist and Public Work’s staff, the wetland’s hydrology comes from adjacent storm water culvert(s) extending underneath the railroad tracks at the end of Eight Street and another extending in a “man-made open ditch” from the City’s Corporation Yard along “B” Street. The storm water essentially comes from three areas (Sixth, Eighth and Emerick Streets) and is not only the hydrological source for the wetlands, but also when combined creates the primary water source for what is called “Mountain Creek” which extends down to Heresy Street and eventually into Bear Creek. Unfortunately, the water is dispersed on the property without conventional mechanisms such as legal agreements or even volume-to-capacity analysis, but instead appears to have occurred from unknown grandfathered circumstances.

Nevertheless, the “open ditch” leading from the City’s Corporation yard off “B” Street, under a culvert, and then extending directly to the eastern edge of the wetlands “may also be” classified as a wetland by the Division of State Lands (DSL), but can not be officially determined a “wetland” or an “upland wetland” until an application is submitted to the DSL. Unfortunately, depending upon the type of application made to DSL (delineation and mitigation vs. delineation), because of work loads and staffing constraints this process could take up to 120 days (delineation and mitigation) or a year (solely a delineation). As such, the applicants are proposing to expedite DSL’s review process by removing the open ditch and

replacing its overall area (approximately 1,100 square feet) to the area just north of the natural wetlands. In this case, the hydrology from the ditch will be collected from its current source point at the culvert, drained through a pipe and then released in the same location and intensity as current location near the natural wetland. The new wetlands will be sized accordingly (approximately 1,700 square feet) and designed to accommodate native plant and wildlife. Final landscaping details of the wetlands mitigation plan will be included with a Final Plan application submittal. In addition, the plan is to include an open bio-swale within the open space lot (Lot #12) to assist in the filtration of the storm water prior to it entering the new wetlands. The bio-swale will be planted with native plant species conducive to intermittent levels of hydrology.

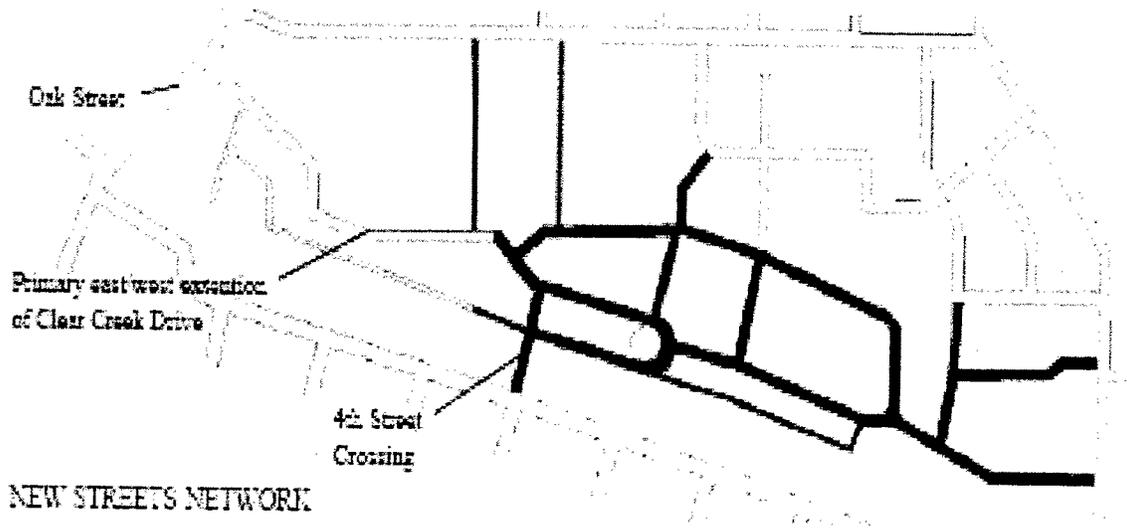
Separating the wetlands and the bio-swale will be a “public” pedestrian path extending from the street to the bicycle easement and the railroad’s right-of-way. The path will be improved with a decomposed granitic surface for a firm pedestrian surface. The path’s immediate purpose will provide an opportunity for the residents and public to walk along the wetlands and bioswale, but in the long term provide an important short-cut connection to the Central Bike Path, Railroad Park and the Railroad District.

Within the natural wetlands, the applicants’ intentions are to remove the invasive Blackberry overgrowth, retain all trees within the wetland area, and retain the wetland habitat as a natural open space corridor. Maintenance of the wetlands and bio-swale, such as the continual eradication of the Blackberries, will be provided by the project’s Home Owner’s Association. It’s understood that maintenance of the wetland areas such as the removal of Blackberries will be done with hand tools and small machinery (weed eaters, brush hogs, hand tools, etc.) and specifically not done with heavy equipment (backhoes, tractors, etc.). It’s the applicants’ contention that both wetlands and engineered storm drain improvements will be valuable to the neighborhood and community. Final engineering details will be provided at time of Final Plat.

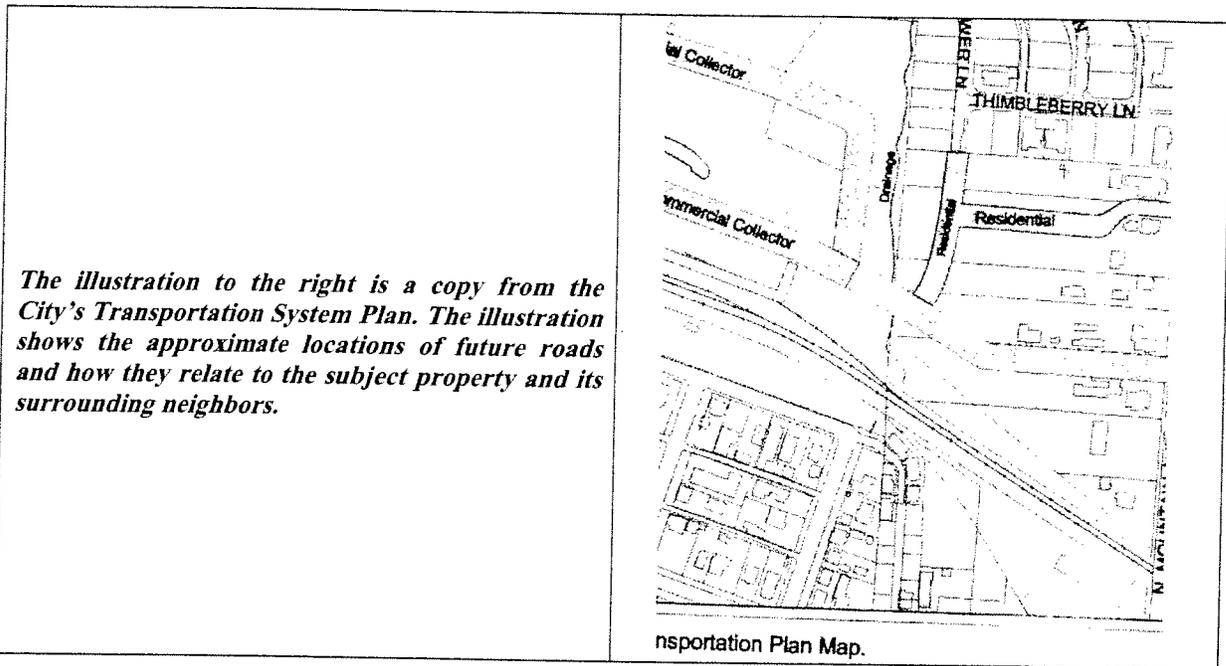


*The above illustration is a copy from the City's "draft" Fishman Wetland Report. The illustration shows the subject wetlands and observation point on the subject property (below right hand corner).*

Future Road Extension: The City's Transportation Plan Maps as well as the maps within the Draft Railroad Property Plan identify a road extending from North Mountain Avenue through the subject property, angling through a small portion of Tax Lot #1400 to the north, over Mountain Creek and to the Railroad property. The submitted Outline Plan shows the proposed road being consistent with those plans. However, due to the unknown development timing or possibilities of Tax Lot #1400, the applicants are only proposing to install the road to where it starts to angle onto another property. The remaining section, would be then be designed and developed in accordance with the City's Transportation Plan by the adjacent owner when he/she develop. This is proposed for a number of reasons: 1) This section of road is not in the control of the applicants; 2) The property owner of Tax Lot #1400 may have an alternative alignment that is preferable to his/her development plans; 3) It may be determined that an alternative alignment is preferable to future Planning Commissioners (due to site constraints such as trees, topography, creek alignment, etc.); and 4) Fairness - primarily due to the fact the applicants willingness to install  $\frac{3}{4}$  of the road and all the necessary utilities on their property that "will not" have to be paid by, but will eventually available to, the developer of Tax Lot #1400.



*The above illustration is from the "draft" Railroad Master Plan and shows the proposed network of streets within the Railroad Property with the subject property within illustration's below right hand corner.*



Utilities: After some preliminary engineering and surveying, it appears all primary utilities will extend within the new road eventually connecting with the Railroad Property to the west. The utility lines (water, sewer, storm, electric, etc.) will be upsized, in cooperation with the City, in order to accommodate future capacity needs. The lines will extend from North Mountain Avenue and temporarily be “stubbed” at the western end of the street until those adjacent parcels develop. The applicants have met with the Ashland Public Works Department, Fire Department and Ashland Electric Department to insure coordination is occurring. At the time of Final Plan, Civil Engineered drawings will be completed showing the details of the utility locations, easements, type of service, depths and sizes of pipes. Currently all utilities but for storm water exist within the North Mountain Street right-of-way, none of which are at capacity according to City department representatives. However, as noted above there is no public storm water line servicing this entire area which has led to flooding issues for the neighbor’s to the north. It is the applicant’s intention, in concert with the Public Works Department, to resolve these issues by extending a new storm water line into the existing storm line in Ashland Village Drive to the north.

House Designs: The proposed homes will be traditional in appearance with predominantly single-story volumes. The homes will have recessed or alley accessible garages, front porches and be designed with a sense of human scale entries for visitors and residents. The garages along Clear Creek Drive have setbacks ranging from 29’ to 34’ (20’ is the minimum requirement) from the back of walk and 41’ to 46’ from the back of curb. In addition, the homes porches sit between 10’ to 15’ in front of the garages in an attempt to create an articulated and neighborly friendly streetscape. Conceptual illustrations of the homes are attached to this application.

Condominium Design: The six-unit condominium sits at the rear of the subject property adjacent to the railroad right-of-way (Lot #10). The six-unit building is 1½ stories in height with the ½ story being a partial basement. From the railroad tracks, the building appears as a

single story building and from the front and sides of the building it appears as a single story building on a raised foundation. The building's design is relatively compact with the intention to be compatible with the surrounding neighborhood. The building's primary entrance faces the Havurah Synagogue and parking lot. The building will have residential architectural elements such as hipped 5.5/12 roofs, hardiplank siding, a three car garage, roof dormers (for ventilation), and trim around the openings and covered entries. But for the rear, the building has multiple wall and roofline off-sets.

The basement floor shows eight parking spaces (including one handicap space), the ingress and egress points, small independent storage spaces, seven bike parking spaces, a stairwell, an elevator and a small equipment room. The first floor (technically considered first floor by Building Code), show the proposed units, hallway, laundry room, stairwell, elevator and a lobby. All units will meet the accessibility standards established by the Building Code and the Americans with Disabilities Act (ADA). All of the units, but for one will be single bedroom units and range in size from 682 square feet to 1,219 square feet with an overall average of 875 square feet per unit.

Solar Access: All proposed buildings within the development will comply with the City's Solar Access Standards, Chapter 18.70.050 A., including the six-unit condominium building. All of the homes fronting onto Clear Creek Drive will shadow their front yards and a small portion of the right-of-way. The site plan shows how the six-plex building will be set back from Lots #6 and #7 in order to provide full solar access. At the time of building permit submittal, the plans will reflect how each building complies with this Chapter.

Neighborhood Planning: The applicants have made contact with the neighbors in an attempt to present their plans and to try and incorporate potential issues or concerns that may arise. As evidenced within the application materials, the applicants have considered the neighbors' concerns and have planned accordingly for future utilities, road construction, access points and boundary line configurations.

Lot Coverage: Within the R-1-5 zoning regulations, the maximum lot coverage for an individual parcel is 50% of the lot's area. The current proposal has an overall coverage of 47.8%. However, in Performance Standards Developments (cluster developments), where homes are clustered together on small lots in order for natural features to be retained, the coverage issue for individual lots during the building permit process become vague and difficult for staff to administer. As such, for clarification purposes, the applicants are proposing the lot coverage regulations be applied to the entire subdivision (less than 50%) with the understanding that each lot's impervious surface area will be regulated to the identified driveways and building envelopes as noted on the site plan.

Temporary Fire Truck Turn-Around: Since the proposed street does not yet connect with the street network planned for the Railroad Property, the applicants have designed the subdivision to accommodate a "temporary" fire truck turn-around in consultation with the Ashland Fire Department. As evidenced on the site plan, the far alley entrance and the end of the street create a turn around area that can easily accommodate fire and other large trucks. In addition, the corners at the entry of the alley have been increased to further accommodate the truck

movements. All final details of the turn-around and street improvements will be provided at the time of Final Plan review.

Parking: Each single family lot is required to have at least 2 on-site parking spaces and one guest parking space on-site or off. With over 700' of improved street frontage along Clear Creek Drive, it is estimated that "at least" 12 new on-street parking spaces will be created with an additional seven parking spaces along North Mountain Avenue for a total of 19 on-street parking spaces. In addition, each of the single family units have a two car garage and the six multi-family units (1 two-bedroom and 5 one-bedroom units) have a total of eight enclosed parking spaces within the basement and two exterior parking spaces within the Havurah's parking lot leased via a parking agreement.

The Havurah's parking is based upon the number of seats within the Synagogue (1 space per four seats). Currently, there are 143 seats requiring 36 parking spaces, but during special events additional "temporary" chairs are available. In these circumstances, an additional 40+/- chairs are available requiring another 10 parking spaces for a total parking space demand of 46. With the lot line adjustment and parking lot modification proposal, the improved parking lot will have 52 on-site parking spaces, 50 of which will be dedicated solely to the Havurah.

Phasing: The subdivision is expected to have two phases with the first phase being the separation of Lot #1 (existing house) from the rest of the subdivision. The lot boundaries and easements of Lot #1 would be recorded as shown in order for the home to be sold. In addition, the existing driveway will remain in its current location until Clear Creek Drive is completed. Once the new street is completed, the existing driveway will be removed and a new curb installed.

#### **APPLICABLE CRITERIA:**

**18.88 OUTLINE PLAN SUBDIVISION:** In response to the City of Ashland's criteria for an Outline Plan, Procedure for Approval, Section 18.88.030 A.4., the Planning Commission shall approve the Outline Plan when it finds the following criteria have been met:

**a. That the development meets all applicable ordinance requirements of the City of Ashland.**

All applicable City ordinances have been met with the submitted application. No exceptions or variances are being requested with this application.

**b. That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.**

All key facilities are available to service the proposed project. The applicants have met with all of the utility departments to verify if there were any capacity issues and none were forthcoming. The discussions primarily focused on the future connection of the road and all

utilities that will be necessary to service the future developments of the Railroad Property. The applicants have also discussed the proposal with the Ashland Fire Department and they had no concerns over the proposed temporary turn around area or apparatus access to the five-plex. All preliminary engineering has been completed with final engineering pending Final Plan submission.

**c. That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.**

The site plan has been designed to recognize the site's natural features such as the small wetland area within the site's western boundary (Tax Lot #1600). The wetland has been identified on the Draft Fishman Wetland Report and further delineated by the project's Biologist, Scott English. Within the wetlands there are groups of trees and plants indicative of wetland vegetation. According to the project's Biologist and Public Work's staff, the wetland's hydrology comes from adjacent storm water culvert(s) extending underneath the railroad tracks at the end of Eight Street and another extending in a "man-made open ditch" from the City's Corporation Yard along "B" Street. The storm water essentially comes from three areas (Sixth, Eighth and Emerick Streets) and is not only the hydrological source for the wetlands, but also when combined creates the primary water source for what is called "Mountain Creek" which extends down to Heresy Street and eventually into Bear Creek. Unfortunately, the water is dispersed on the property without conventional mechanisms such as legal agreements or even volume-to-capacity analysis, but instead appears to have occurred from unknown grandfathered circumstances.

Nevertheless, the "open ditch" leading from the City's Corporation yard off "B" Street, under a culvert, and then extending directly to the eastern edge of the wetlands "may also be" classified as a wetland by the Division of State Lands (DSL), but can not be officially determined a "wetland" or an "upland wetland" until an application is submitted to the DSL. Unfortunately, depending upon the type of application made to DSL (delineation and mitigation vs. delineation), because of work loads and staffing constraints this process could take up to 120 days (delineation and mitigation) or a year (solely a delineation). As such, the applicants are proposing to expedite DSL's review process by removing the open ditch and replacing its overall area (approximately 1,100 square feet) to the area just north of the natural wetlands. In this case, the hydrology from the ditch will be collected from its current source point at the culvert, drained through a pipe and then released in the same location and intensity as current location near the natural wetland. The new wetlands will be sized accordingly (approximately 1,700 square feet) and designed to accommodate native plant and wildlife.

In addition, the plan is to include an open bio-swale within the open space lot (Lot #12) to assist in the filtration of the storm water prior to it entering the new wetlands. The bio-swale will be planted with native plant species conducive to intermittent levels of hydrology.

Separating the wetlands and the bio-swale will be a “public” pedestrian path extending from the street to the bicycle easement and the railroad’s right-of-way. The path will be improved with a decomposed granitic surface for a firm pedestrian surface. The path’s immediate purpose will provide an opportunity for the residents and public to walk along the wetlands and bioswale, but in the long term provide an important short-cut connection to the Central Bike Path, Railroad Park and the Railroad District.

Within the natural wetlands, the applicants’ intentions are to remove the invasive Blackberry overgrowth, retain all trees within the wetland area, and retain the wetland habitat as a natural open space corridor. Maintenance of the wetlands and bio-swale, such as the continual eradication of the Blackberries, will be provided by the project’s Home Owner’s Association. It’s understood that maintenance of the wetland areas such as the removal of Blackberries will be done with hand tools and small machinery (weed eaters, brush hogs, hand tools, etc.) and specifically not done with heavy equipment (backhoes, tractors, etc.). It’s the applicants’ contention that both wetlands and engineered storm drain improvements will be valuable to the neighborhood and community. Final engineering details will be provided at time of Final Plat.

**d. That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.**

The proposed development will not prevent adjacent land from being developed as shown on the Comprehensive Plan. The City’s Transportation Plan Maps as well as the maps within the Draft Railroad Property Plan identify a road extending from North Mountain Avenue through the subject property, angling through a small portion of Tax Lot #1400 to the north, over Mountain Creek and to the Railroad property. The submitted Outline Plan shows the proposed road being consistent with those plans.

**e. That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.**

The proposed subdivision will create a Home Owners Association (HOA) with adequate Conditions, Covenants, and Restrictions (CC&R’s) that provides the management structure to provide the necessary maintenance of the project’s open spaces, pathways and parking spaces. A copy of the HOA and CC&R’s will be provided at the time of Final Plan approval.

**f. That the proposed density meets the base and bonus density standards established under this Chapter.**

The property is 3.59 acres in size, zoned R-1-5 with a base density of 4.50 units per acre for an allowed density of 16 dwelling units. The applicants are proposing to construct 15 units and intend to construct the new structures under the City’s Earth Advantage program. The Earth Advantage program is a comprehensive program designed to make homes more energy efficient than code built homes while at the same time provide a healthier indoor quality, is resource efficient and environmentally responsible. Because the Earth Advantage program is

menu driven and “point” based, the development of the subdivision (from initial earth disturbance to finish construction) will be completed under the oversight of the City’s Conservation Department.

**g. The development complies with the Street Standards.**

The development complies with the Ashland Street Standards for a Neighborhood Collector Street. The planned right of way will be 63’ in width with the applicants developing 2/3 (41’) of the street with the development. The remaining 1/3 will be developed at the time the adjacent property to the north (Tax Lot 1400) is subdivided. The design of the street right-of-way show two 10’ travel lanes, 8’ on-street parking lanes on each side, curbs, 7’ parkrows (planting strips) and 6’ wide sidewalks. Street trees will be installed within the parkrows in an attempt to create a tree canopied street. North Mountain Avenue will be improved with street trees, planting strip and a sidewalk in accordance with the City’s Street Standards.

**18.104 CONDITIONAL USE PERMITS:**

**A. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.**

Response: The proposal is for a modification of a Conditional Use Permit for an existing religious facility. Religious facilities are permitted uses within the R-1-5 zoning district with approval of a Conditional Use Permit, based upon the applicable criteria. The applicants are not proposing an expansion of the religious facility, but a modification of the property line and site improvements to the parking lot. The existing synagogue conforms to the zone’s setback dimensions, lot coverage percentages, solar access requirements, parking configurations and access provisions noted in Chapter 18.20.040 as well as relevant Comprehensive Plan policies.

*B. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.*

Response: Adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage and adequate transportation can and will be provided. Considering the application is for a very similar application as the one recently approved (PA-2001-039) where this criterion was determined to be met, the applicants contend the current “modified” application also complies. Nevertheless, the only physical change to the property will be the improvement of the parking lot which will increase the site’s storm water. The applicants propose to collect the storm water and pipe the water to the new system to be installed with the subdivision. Final engineering details will be provided with the Final Plan application with all plans designed by a Civil Engineer.

*C. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:*

*1. Similarity in scale, bulk, and coverage.*

Response: The existing synagogue already exists and no exterior building alterations are proposed. The new residential buildings are to be designed with a variety of shapes and styles in an attempt to create a more vibrant streetscape and living environment. Each of the buildings, including the six-unit condominium building, will be similar in scale, bulk and coverage to some of the surrounding buildings within the impact area (200'). The site's surrounding impact area consists of a variety of uses and housing types, including detached, attached and cluster housing. In particular, the properties directly across the railroad tracks to the south include the City's corporation yard (typical yard debris, construction materials, out buildings, etc.), the Barchet Subdivision (two-story attached housing complex on a flag lot), and the 9<sup>th</sup> Street Alley Subdivision (a residential Planned Unit Development with a combination of small single story and two-story houses). Overall, the area has similar building volumes (scale and bulk) as the proposed buildings.

*2. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.*

Response: As noted previously, the Havurah Jewish Synagogue is *not* proposing any changes to their building, a change in membership or a change to their typical activities. The only changes proposed for the Havurah is the reconfiguration of property lines, parking lot expansion, parking lot improvements, Sukkah relocation and playground expansion. However, because the Havurah's property is now part of the development proposal and the subdivision's density is based upon the entire property, the application includes an explanation and findings of fact explaining the modification.

Residential Vehicle Trips: The proposed traffic generated by 9 single family units and 6 multi-family units, in addition to the existing traffic generated by the Havurah, according to Robert Kortt, Traffic Engineer, are very minimal and would not cause any of the surrounding intersections to operate beyond capacity. Additionally, because of the added new street and the proposed pedestrian paths, the traffic generated from the 14 "new" units is equivalent to less than 112 vehicle trips (Institute of Traffic Engineers (ITE) 7<sup>th</sup> Edition, Trip Generation Manual) which is less than 5 trips per hour in a 24 hour time frame. In addition, because the applicants are hoping to market the housing units to elderly family members, other Havurah members and the pedestrian improvements previously mentioned and identified on the site plan, it is the applicants' belief the number of vehicle trips generated by the proposal should be less than the national averages.

Overall, the following illustrates the total number of vehicle trips expected by the residential units based upon national averages (ITE 7<sup>th</sup> Edition, Trip Generation Manual):

Single Family Unit (9):	86 vehicle trips (9.55 per unit)
Multi-Family Housing (6):	35 vehicle trips (5.86 per unit)
Total:	121 vehicle trips (trips that primarily will use the new Clear Creek Drive intersection)

Havurah Vehicle Trips: According to the ITE, religious institutions (Havurah) generate generally average 9.32 vehicle trips per 1,000 square feet of gross floor area. In this case, the Havurah has a total of 5,426 square feet and would presumably generate 50 vehicle trips per day (Scenario #1 below).

This appears to be very consistent with a recent study completed by members of the Havurah (Scenario #2 below) who found that over a two month period the average number of vehicle trips generated was 1,127 vehicle trips (*Note: 2 vehicle trips is 1 incoming vehicle and 1 outgoing vehicle*) or 38 trips per day in a 30 day month. In addition, the study included two special events which (average one per month) and at times created overflow parking demand along North Mountain Avenue. In these circumstances, the applicants found the vehicle trips generated during special events added an average of 10 cars or 20 vehicle trips.

In conclusion, the combination of the subdivision’s residential trips and the Havurah’s vehicle trips, even when including special events, is *very* consistent with the trips generated by a 19 unit single-family development as illustrated below in either scenario:

Single-Family Development                      \*19 units X 9.55 trips = **181 vehicle trips**  
*\* Based upon the site’s permitted base density and proposed density bonus provisions. In this scenario, it would be assumed the Havurah didn’t exist and the entire property was developed as a 19-unit residential subdivision.*

Scenario #1 (ITE Average)	Residential Trips:	121 vehicle trips
	Havurah Trips:	50 vehicle trips
	Total:	<b><u>171 vehicle trips</u></b>

Scenario #2 (Havurah’s Study)	Residential Trips:	121 vehicle trips
	Havurah Trips:	38 vehicle trips
	Special Events:	20 vehicle trips*
	Total:	<b><u>179 vehicle trips</u></b>

\* Special events do not occur on a daily basis, but have been included in Scenario #2 as if they did to clearly illustrate how under a “worst-case” scenario the proposed Conditional Use Permit Modification is equal or less impacting when compared to the zone’s target uses.

### 3. Architectural compatibility with the impact area.

Each of the buildings, including the 6-unit condominium building, will be architecturally compatible with the surrounding buildings in the impact area (200’). The site’s surrounding impact area consists of a variety of uses and housing types, including detached, attached and cluster housing. Most of the housing types within the impact area have a similar architectural

flavor often referred to as non-traditional vernacular or contemporary neighborhood vernacular as they do not represent a typical architectural style (i.e., Gothic, Italian, Craftsman, etc.). Overall, the applicants believe each proposed building type will be architecturally compatible with the surrounding buildings.

*4. Air quality, including the generation of dust, odors, or other environmental pollutants.*

Response: The proposed residential uses are “permitted” uses and will not generate dust, odors, or other environmental pollutants. The existing Havurah Synagogue is a “conditional use” but is *not* proposing any changes to their building, a change in membership or a change to their typical activities. The only changes proposed for the Havurah is the reconfiguration of property lines, parking lot expansion, parking lot improvements, Sukkah relocation and playground expansion. All of the changes proposed will improve air quality, specifically dust, but in no case will it generate an increase in dust, odors, or other environmental pollutants.

*5. Generation of noise, light, and glare.*

Response: The proposed residential uses are “permitted” uses and will not generate noise, light or glare beyond what’s typically generated by a single family residence. The existing Havurah Synagogue is a “conditional use” but is *not* proposing any changes to their building, a change in membership or a change to their typical activities. The only changes proposed for the Havurah is the reconfiguration of property lines, parking lot expansion, parking lot improvements, Sukkah relocation and playground expansion.

*6. The development of adjacent properties as envisioned in the Comprehensive Plan.*

Response: The proposed modifications will not negatively impact adjacent properties from developing in accordance with the City’s Comprehensive Plan or land use related documents. In fact, the applicants contend the development of this property and the installation of the new street connecting the Railroad property to the west and North Mountain is “crucial” for the long range economic and employment needs of the City. The subject street is part of the City’s Transportation Plan, but without the development of this property, it is likely the only possible connection and street improvement would be through public acquisition - a very expensive endeavor. However, with the development proposal, the street will be engineered and installed at private expense, will be approved under public street standards, and will be designed to comprehensively relate to adjacent developments now and in the future.

*7. Other factors found to be relevant by the Hearing Authority for review of the proposed use.*

Response: There are a number of policies and generalizations in the City’s Comprehensive Plan and other housing related documents that promote “mixed-use” planning efforts. However, unlike the most referenced or understood mixed-use definition (housing over retail), the applicants are proposing a “mixture” of housing types such as single family detached, single family attached and multi-family mixed in with typically associated residential “accessory” uses such as a churches or parks.

The applicants contend the mixture of uses is not only appropriate, but also a very important social opportunity that typical zoning practices tend to eliminate. Ashland has always been unique in this endeavor unlike other communities primarily due to leadership and community values memorialized in the City's Comprehensive Plan and other housing related documents. Overall, these can be found in the Comprehensive Plan under Sections 2.04.01, 2.04.03, 5.23, 5.26.4, 5.26.5, 6.07, 6.09, 6.10, 6.11.b, 6.11.2a, 6.11.3b, 6.11.3c., etc.

**18.61.080 CRITERIA FOR ISSUANCE OF TREE REMOVAL - STAFF PERMIT: An applicant for a Tree Removal-Staff Permit shall demonstrate that the following criteria are satisfied. The Staff Advisor may require an arborist's report to substantiate the criteria for a permit.**

The applicants are requesting a Tree Removal Permit to remove *one* tree greater than 6" dbh: 9" (dbh) Pine Tree (within Havurah parking lot)

In addition, the applicants have retained the services of Upper Limb-It Tree Service, Tom Myers, to assess the health of the site's trees. Of all of the sites' trees, only the mature 40" (dbh) Black Walnut along North Mountain is in really poor condition most likely due to the continuous topping because of the overhead electric lines. However, unlike the previous application proposal to remove the tree (PA-2006-01091), the applicants are proposing to leave the tree, meandering around the tree with the new sidewalk, until it is either removed by the City or dies naturally.

The criteria are as follows:

**A. Hazard Tree: The Staff Advisor shall issue a tree removal permit for a hazard tree if the applicant demonstrates that a tree is a hazard and warrants removal.**

Not applicable as the Pine Tree is not a hazard tree.

**B. Tree that is Not a Hazard: The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:**

**1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards. (e.g. other applicable Site Design and Use Standards). The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and**

The applicant is proposing to remove the 9" (dbh) Pine Tree to allow for the parking lot to be designed in a logical manner consistent with the City's Site Design and Use Standards. Specifically, back-up movements would be impeded. Preliminary design attempts were completed in an attempt to save the tree, but due to a variety of reasons (location, size, adjacency to similar type trees, vehicle back-up maneuvering problems, etc.). Approximately nine replacement trees will be added to the site's parking lot creating a significant increase in the parking lot's landscaping and shading opportunities.

**2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and**

The site around the tree is relatively flat and void of other natural features. The removal of the tree will not have a negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.

**3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property.**

The Pine Tree is approximately 15' in height and is limited in size and canopy. The removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The tree will be replaced with nine trees that create a diverse canopy of trees.

**The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with other provisions of the Ashland Land Use Ordinance.**

No exceptions are proposed with this application.

**4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.**

See section below referencing and addressing AMC 18.61.084.

**18.61.084 Mitigation Required: An applicant may be required to provide mitigation for any tree approved for removal. The mitigation requirement shall be satisfied by one or more of the following:**

**A. Replanting on site. The applicant shall plant either a minimum 1 ½-inch caliper healthy and well-branched deciduous tree or a 5-6 foot tall evergreen tree for each tree removed. The replanted tree shall be of a species that will eventually equal or exceed the removed tree in size if appropriate for the new location. The tree shall be planted and maintained according to the specifications in the City Tree Planting and Maintenance Guidelines as approved by the City Council.**

The applicant will replace the one Pine tree with nine trees to be planted within the parking lot. These trees will be irrigated and maintained by members of the Havurah. In addition, all trees and irrigation services will be completed by a professional landscape firm.

**B. Replanting off site. If in the City's determination there is insufficient available space on the subject property, the replanting required in subsection A shall occur on other property in the applicant's ownership or control within the City, in an open space tract that is part of the same subdivision, or in a City owned or dedicated open space or park. Such mitigation planting is subject to the approval of the authorized property owners. If planting on City owned or dedicated property, the City may specify the species and size of the tree. Nothing in this section shall be construed as an obligation of the City to allow trees to be planted on City owned or dedicated property.**

Applicant is not proposing to replant trees off site but is proposing to plant trees on-site as described in Section 18.61.084 A. above.

**C. Payment in lieu of planting. If in the City's determination no feasible alternative exists to plant the required mitigation, the applicant shall pay into the tree account an amount as established by resolution of the City Council.**

Applicant is proposing to plant four new trees on site as described in Section 18.61.084 A. above.

#### **18.72.070 SITE DESIGN & USE STANDARDS:**

**A. All applicable City Ordinances have been met or will be met by the proposed development.**

To the applicant's knowledge all City regulations are or will be complied with. The applicants are not requesting any exceptions or variances.

**B. All requirements of the Site Review Chapter have been met or will be met.**

As noted below, all requirements listed in the Site Review Chapter (18.72) have or will be complied with. Specifically, the landscaping will be irrigated and maintained, the trash/recycling area will be screened and light and glare concerns will be addressed with down lighting and screening where necessary.

**C. The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.**

As noted below, all requirements listed in the Site Design Standards (booklet) for Multi-Family developments have or will be complied with. Specifically, the applicants have addressed the pertinent requirements of the Multi-Family Residential Standards, Parking Lot Landscaping & Screening Standards and Street Tree Standards.

**D. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property. All improvements in the street right-of-way shall comply with the Street Standards in Chapter 18.88, Performance Standards Options.**

All utilities associated with the development of this property will either be directed towards or come from North Mountain Avenue or the newly installed Clear Creek Drive. All key facilities are available to service the proposed project. The applicants have met with all of the utility departments to verify if there were any capacity issues and none were forthcoming. The discussions primarily focused on the future connection of the road and all utilities that will be necessary to service the future developments of the Railroad Property. The applicants have also discussed the proposal with the Ashland Fire Department and they had no concerns over the proposed temporary turn around area or apparatus access to the five-plex. All preliminary engineering has been completed with final engineering pending Final Plan submission.

Note: Based upon a last minute request from the Ashland Fire Department, the Fire Hydrant and FDC (Fire Department Connection) has been adjusted to the west side of the private driveway (located directly behind lot #7). The submitted conceptual Utility Plan identifies the hydrant and FDC on the east side, but will be relocated on the west side, as identified on all other plans, at the time of Final Plan review.

**II-D PARKING LOT LANDSCAPING & SCREENING STANDARDS: All parking lots, which for purposes of this section include areas of vehicle maneuvering, parking, and loading, shall be landscaped and screened as follows:**

**II-D-1) Screening at Required Yards**

- 1) Parking abutting a required landscaped front or exterior yard shall incorporate a sight obscuring hedge screen into the required landscaped yard.**
- 2) The screen shall grow to be at least 36 inches higher than the finished grade of the parking area, except for required vision clearance areas.**
- 3) The screen height may be achieved by a combination of earth mounding and plant materials.**
- 4) Elevated parking lots shall screen both the parking lot and retaining wall.**

The Havurah Jewish Synagogue building and its front yard landscaping exists and is not proposed to be changed. All screening standards have been complied with. The rear parking area and playground area are proposed to be modified in order to increase parking as well as upgrade the parking area to conform to Planning and Building Code provisions.

**II-D-3) Landscape Standards:**

**1) Parking lot landscaping shall consist of a minimum of 7% of the total parking area plus a ratio of 1 tree for each seven parking spaces to create a canopy effect.**

Approximately 14% of the parking area consists of landscaping. Of the 52 open parking spaces, a total of 11 trees exists or are to be planted which is essentially 1 tree for every 5 parking spaces.

**2) The tree species shall be an appropriate large canopied shade tree and shall be selected from the street tree list to avoid root damage to pavement and utilities, and damage from droppings to parked cars and pedestrians.**

The proposed parking lot trees and their placements have been chosen by a professional landscape architect with the intent to provide shade and a canopy effect over parked automobiles. All trees were chosen from the City's adopted street tree list which includes a variety of tree species recommended for parking lots.

**3) The tree shall be planted in a landscaped area such that the tree bole is a least 2 feet from any curb or paved area.**

All trees to be planted near a hard surface area will be setback at least 2'.

**4) The landscaped area shall be planted with shrubs and/or living ground cover to assure 50% coverage within 1 year and 90% within 5 years.**

The proposed parking lot landscaping plans were completed by a professional landscape architect with the intent to provide for a nicely landscaped site that also complies with the above standard.

**5) Landscaped areas shall be evenly distributed throughout the parking area and parking perimeter at the required ratio.**

The submitted landscape plan shows the parking lot landscaping being evenly distributed throughout the parking lot.

**6) That portion of a required landscaped yard, buffer strip or screening strip abutting parking stalls may be counted toward required parking lot landscaping but only for those stalls abutting landscaping as long as the tree species, living plant material, coverage and placement distribution criteria are also met. Front or exterior yard landscaping may not be substituted for the interior landscaping required for interior parking stalls.**

The project complies with the above standard.

#### **II-D-6) Other Screening**

**1) Other screening and buffering shall be provided as follows:**

**Refuse Container Screen:** Refuse containers or disposable areas shall be screened from view by placement of a solid wood fence or masonry wall from five to eight feet in height. All refuse materials shall be contained within the refuse area.

The project’s refuse area is located along the private driveway, adjacent to the six-plex building and behind a screened fence that will be at least five feet in height.

**Service Corridor Screen:** When adjacent to residential uses, commercial and industrial service corridors shall be screened. Siting and design of such service areas shall reduce the adverse effects of noise, odor and visual clutter upon adjacent residential uses.

No service corridors or large delivery service doors are proposed with this application.

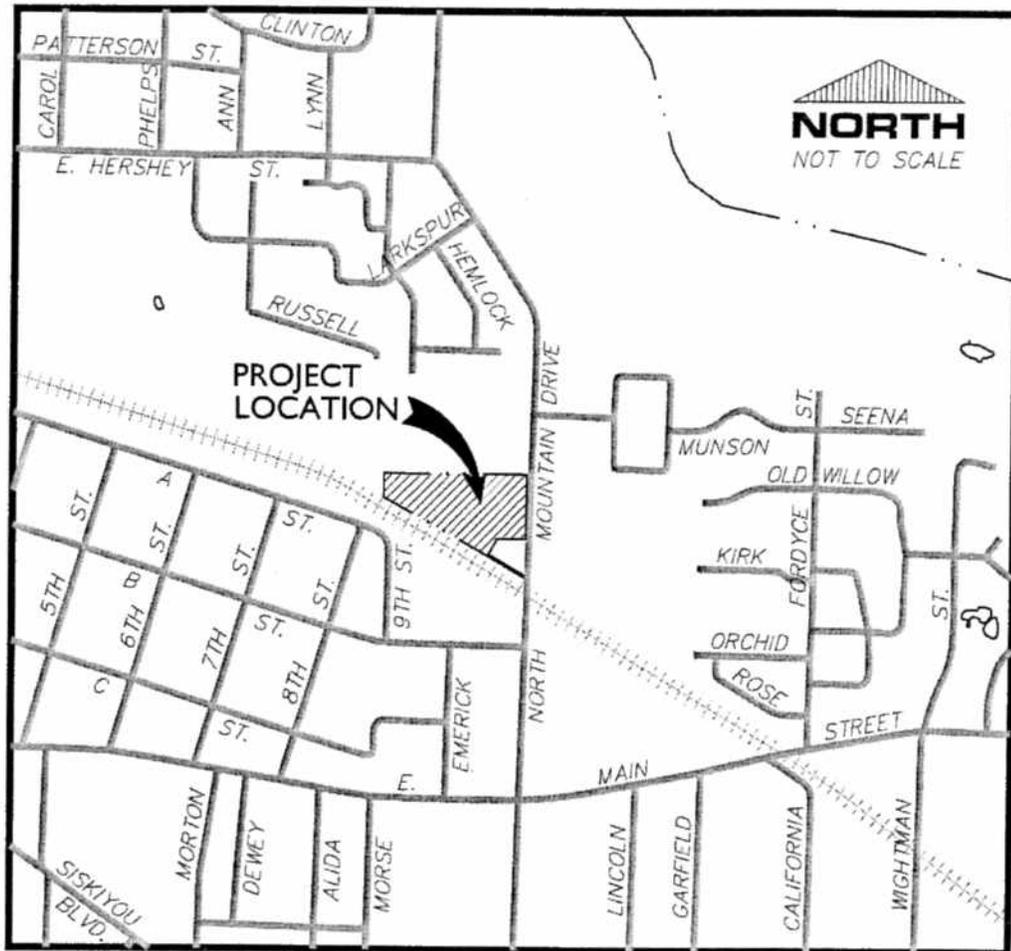
**Light and Glare Screen:** Artificial lighting shall be so arranged and constructed as to not produce direct glare on adjacent residential properties or streets.

No offsite direct light or glare will be directed towards any residential uses as all on-site lighting is intended to be directed either on the building or for the site’s adjacent pedestrian sidewalks.

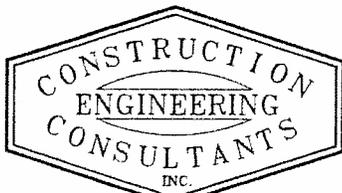
**18.76.140 LOT LINE ADJUSTMENTS:** The adjustment of a lot line by the relocation of a common boundary, where the number of parcels is not changed and all zoning requirements are met, shall be accepted by the City, provided the requirements of Sections 18.76.090 through 18.76.130 are satisfied, in addition to Section 18.76.170, where the lot adjustment causes access to be changed to an exterior unimproved street.

The requirements of Sections 18.76.090 through 18.76.130 and Section 18.76.170 have or will be met at the time of recording the Final Plat. Again, the purpose behind the Lot Line Adjustment is to reconfigure the property lines between the subject properties so that each is as logical and efficient as possible.

	<u>8½ “ X 11”</u>	<u>11” X 17”</u>
	Findings (pages 1 – 29)	Survey (page 38)
<b>E</b>	Vicinity Map (30)	Lot Line Adjustment Site Plan (39)
<b>X</b>	Existing Conditions Map (31)	Site Plan (proposed) (40)
<b>H</b>	Site Development Plan (32)	Conceptual Grading & Drainage Plan (41)
<b>I</b>	Wetland Cross-Section (33)	Conceptual Utility Plan (42)
<b>B</b>	Stormwater Pollution Control Detail (34)	Landscape Plan (43)
<b>I</b>	Erosion & Sediment Control Plan (35)	Irrigation Plan (44)
<b>T</b>	Tree Inventory (36)	Typical SFR Streetscape Plan (45)
<b>S</b>	Tree Protection Narrative (37)	Typical SFR Detached/Attached Plan (46)
<b>:</b>		Condominium Elevations (47)
		Condominium Floor Plans (48)
		Wetland Mitigation plan (49)



VICINITY MAP



P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5288 • FAX (541) 779-3139

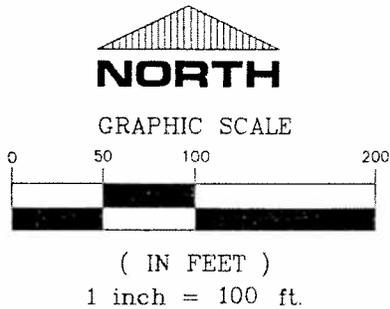
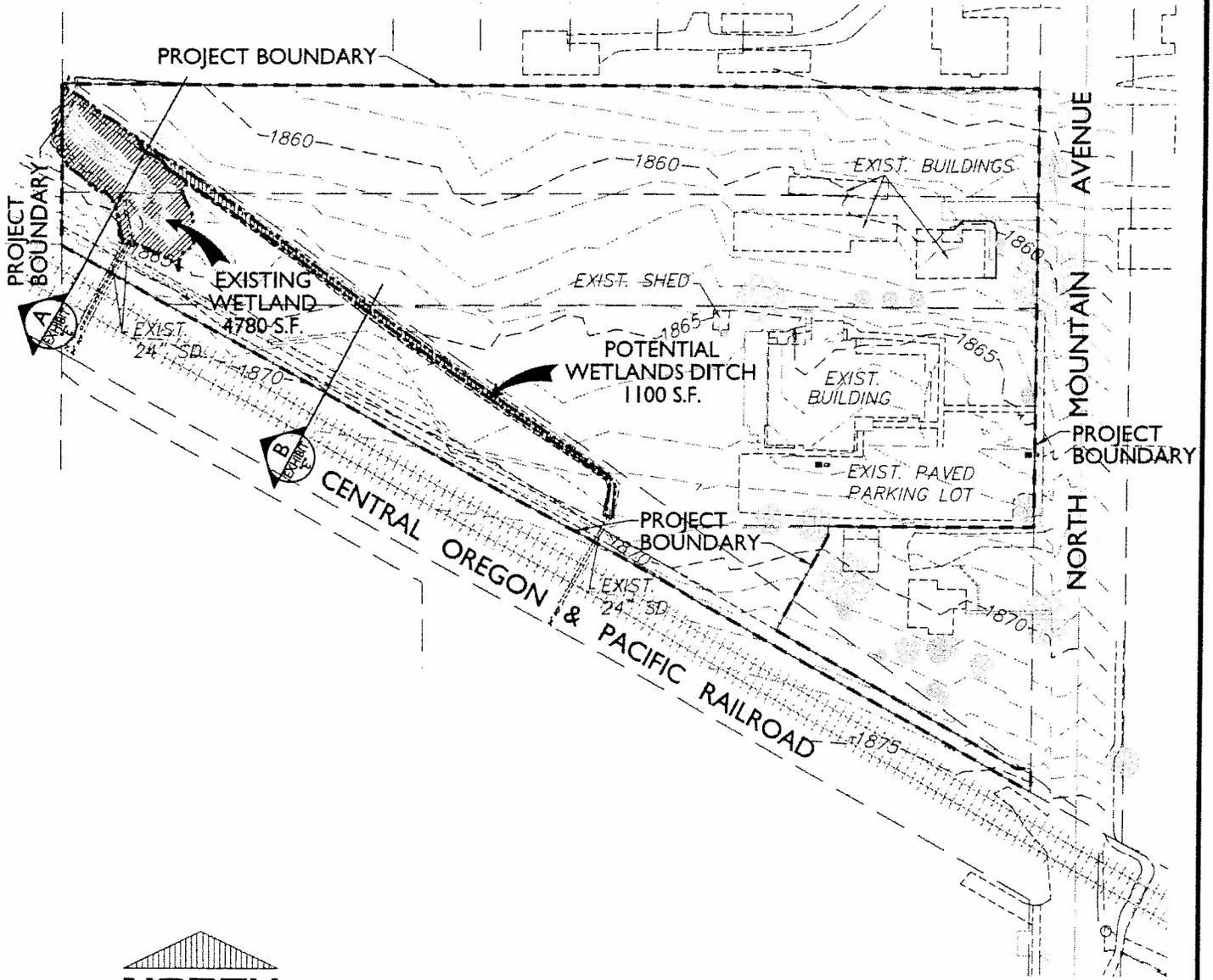
NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

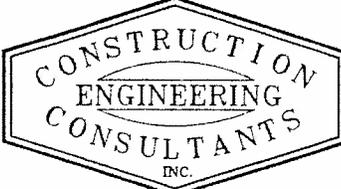
CITY OF ASHLAND

ALEPH SPRINGS SUBDIVISION

SITE VICINITY MAP

EXHIBIT "A"



  
**CONSTRUCTION  
ENGINEERING  
CONSULTANTS**  
 INC.

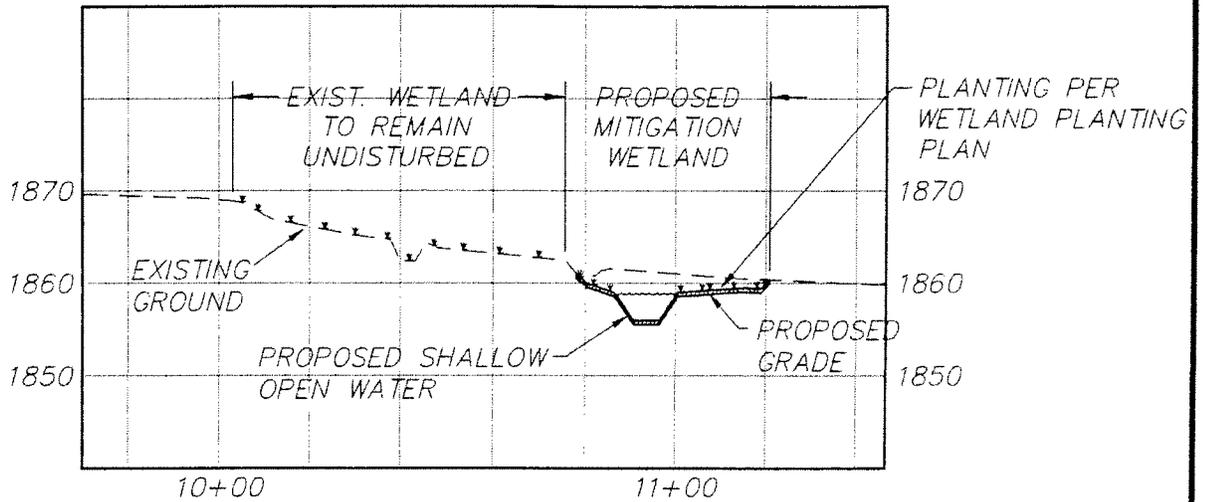
P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5268 • FAX (541) 779-3138

NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

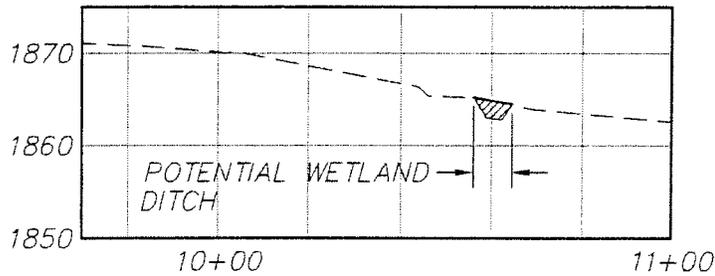
**CITY OF ASHLAND**  
 ALEPH SPRINGS SUBDIVISION  
 EXISTING CONDITIONS MAP  
 EXHIBIT "B"

31

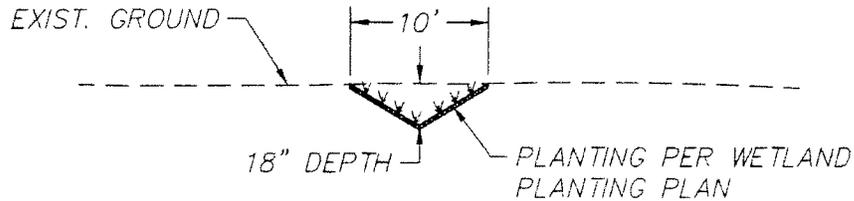




**WETLAND SECTION** (A)



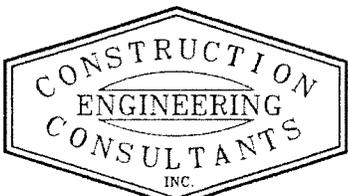
**POTENTIAL WETLAND SECTION** (B)



**BIO-SWALE SECTION** (C)

WETLAND CUT-FILL VOLUMES		
	CUT	FILL
TOTAL SITE	1570 C.Y.	800 C.Y.
WETLAND - NEW	130 C.Y.	0 C.Y.
POTENTIAL WETLAND DITCH	0 C.Y.	109 C.Y.

SCALES  
 HORIZONTAL: 1" = 40'  
 VERTICAL: 1" = 20'



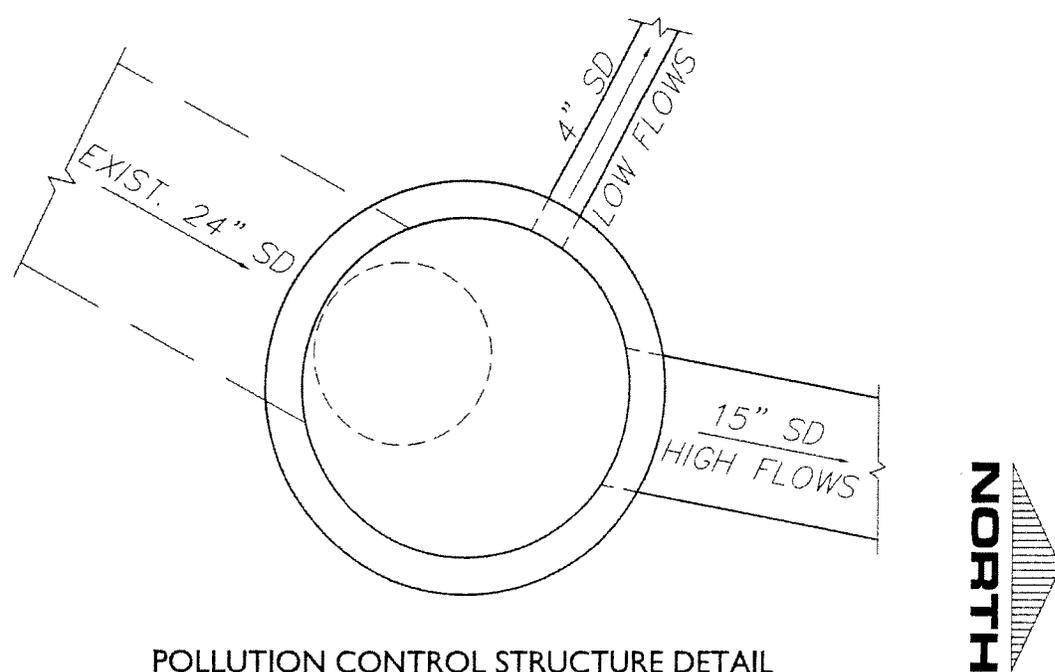
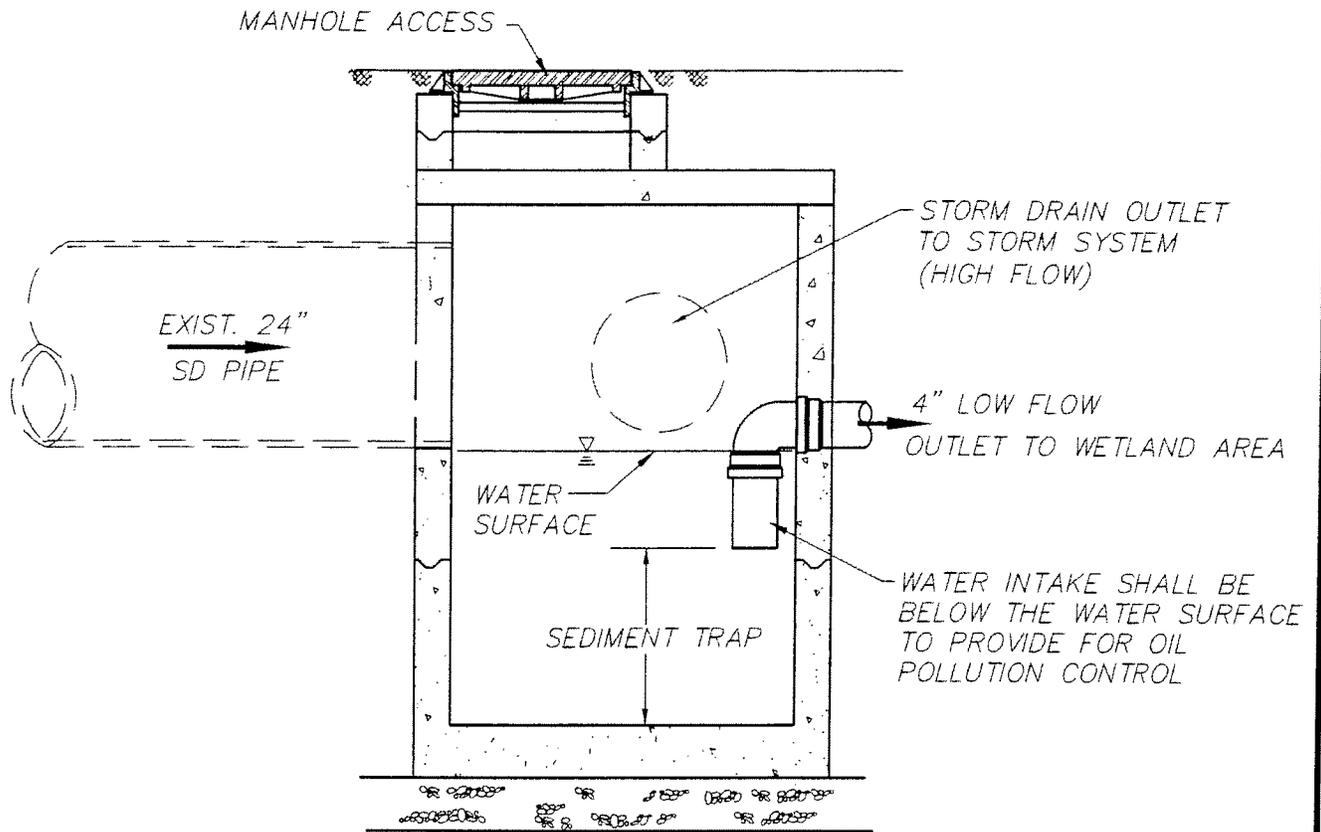
P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5268 • FAX (541) 779-3139

NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

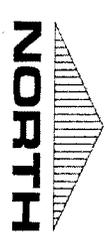
CITY OF ASHLAND

ALEPH SPRINGS SUBDIVISION  
 WETLAND CROSS-SECTION

EXHIBIT "E"



**POLLUTION CONTROL STRUCTURE DETAIL**  
 NOT TO SCALE



**CONSTRUCTION  
 ENGINEERING  
 CONSULTANTS  
 INC.**

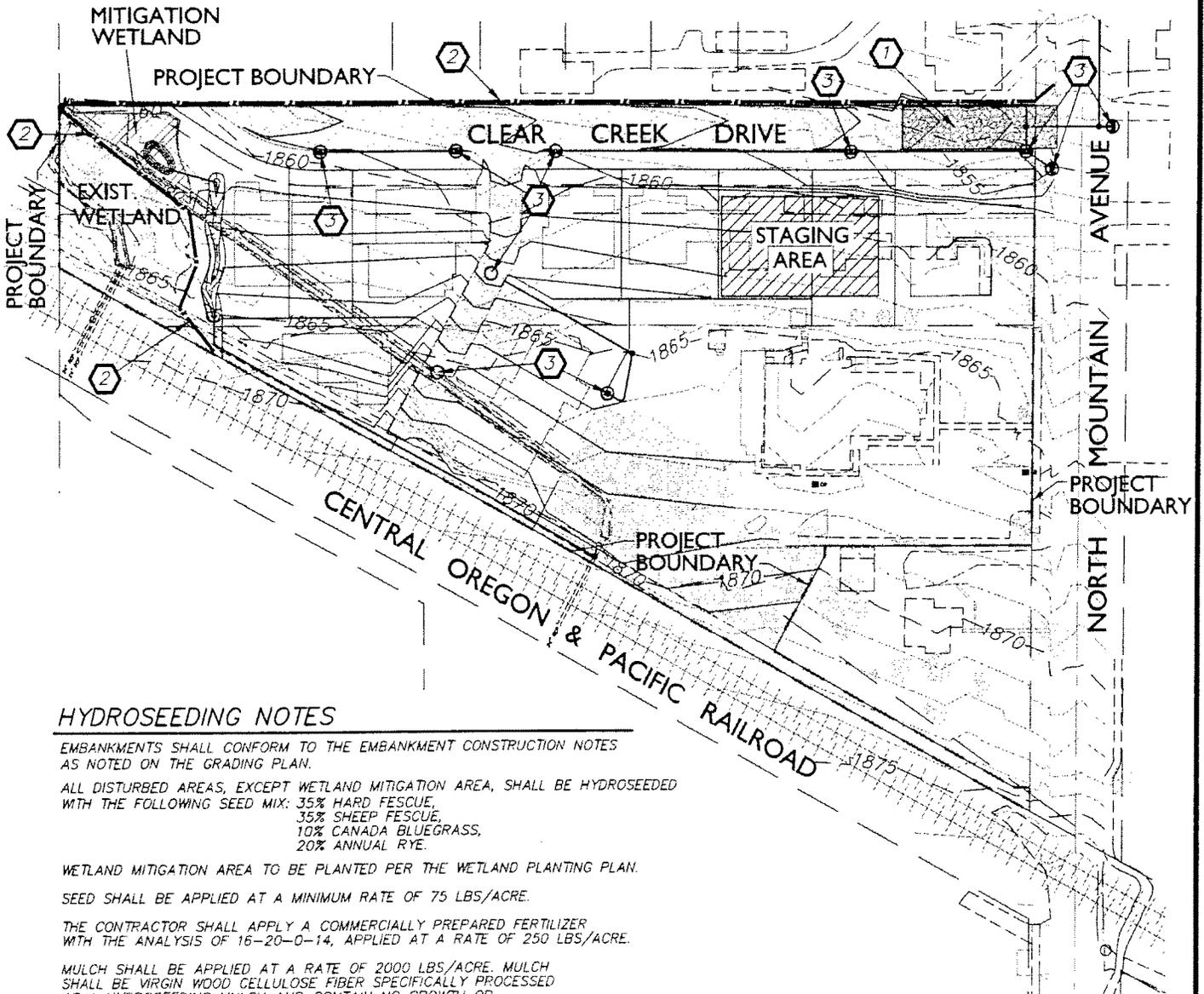
P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5268 • FAX (541) 779-3139

NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION  
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 PH/FAX (541) 488-1061

**CITY OF ASHLAND**

ALEPH SPRINGS SUBDIVISION  
 STORMWATER SPLITTER & POLLUTION  
 CONTROL STRUCTURE DETAIL

EXHIBIT "F" 34



**HYDROSEEDING NOTES**

EMBANKMENTS SHALL CONFORM TO THE EMBANKMENT CONSTRUCTION NOTES AS NOTED ON THE GRADING PLAN.

ALL DISTURBED AREAS, EXCEPT WETLAND MITIGATION AREA, SHALL BE HYDROSEEDED WITH THE FOLLOWING SEED MIX: 35% HARD FESCUE, 35% SHEEP FESCUE, 10% CANADA BLUEGRASS, 20% ANNUAL RYE.

WETLAND MITIGATION AREA TO BE PLANTED PER THE WETLAND PLANTING PLAN.

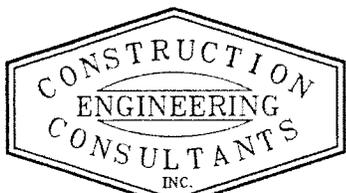
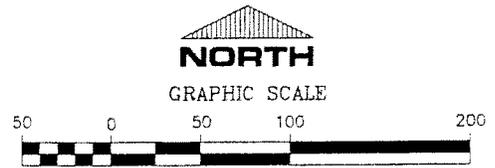
SEED SHALL BE APPLIED AT A MINIMUM RATE OF 75 LBS/ACRE.

THE CONTRACTOR SHALL APPLY A COMMERCIALY PREPARED FERTILIZER WITH THE ANALYSIS OF 16-20-0-14, APPLIED AT A RATE OF 250 LBS/ACRE.

MULCH SHALL BE APPLIED AT A RATE OF 2000 LBS/ACRE. MULCH SHALL BE VIRGIN WOOD CELLULOSE FIBER SPECIFICALLY PROCESSED AS A HYDROSEEDING MULCH AND CONTAIN NO GROWTH OR GERMINATION INHIBITING FACTORS.

**CONSTRUCTION NOTES**

- ① ROCKED CONSTRUCTION ENTRANCE FOR 100' MINIMUM TO PREVENT TRACKING OF SOIL ONTO ROADWAY, 8" MIN. THICKNESS OF 3"-6" CLEAN ROCK ON FILTER FABRIC. LINE AND GRADE SHALL CONFORM TO FUTURE ROAD IMPROVEMENTS.
- ② FILTER FABRIC SEDIMENTATION FENCE INSTALLED AT THE LOCATIONS SHOWN.
- ③ PROPOSED STORM DRAIN INLETS SHALL BE PROTECTED BY FILTER FABRIC INLET BARRIERS.



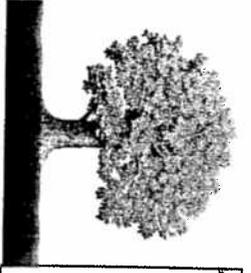
P.O. BOX 1724 • MEDFORD, OREGON 97501  
PH. (541) 779-5268 • FAX (541) 779-3138

NORTHWEST BIOLOGICAL CONSULTING  
HABITAT RESTORATION  
324 TERRACE STREET  
ASHLAND, OR 97520  
SCOTT ENGLISH  
PH/FAX (541) 488-1061

CITY OF ASHLAND  
ALEPH SPRINGS SUBDIVISION  
EROSION & SEDIMENT CONTROL PLAN

EXHIBIT "G"

35



**Upper Limb-it  
Tree Service**  
 PO Box 881  
 Ashland, OR 97520  
 Phone 541-482-3667

185 & 203 North Mountain Avenue

8/9/2007

Tree #	Species	DBH	Height	Crown Radius	Tree protection zone radius in feet	relative tolerance to construction	Condition
1	Juglans nigra (1)	40	30	15	50.0	poor	poor
2	Pinus nigra	14	30	15	7.0	good	fair
3	Juglans regia	25	28	25	31.3	poor	good
4	Acer saccharum	12	30	9	9.0	moderate	good
5	Acer saccharum	11	30	9	8.3	moderate	good
6	Pyrus communis	11	24	9	8.3	moderate	good
7	Pinus nigra	12	20	10	6	good	good
8	Pinus nigra (2)	10	20	10	6	good	good

**Notes**

- 1 60% of trunk wounded, topped
- 2 To be removed due to location in parking lot

## **TREE PROTECTION PLAN**

The primary contact person for the implementation of the Tree Protection Plan will be Larry Medinger. He will designate a site superintendent as the secondary contact. Office number for Larry Medinger and site superintendent is 482-3961. Cell numbers available upon request.

The Tree Protection Plan for 203 North Mountain shall consist of chain-link fence protection of each tree shown on the Site Plan as per specifications set forth in AMC 18.61.230. Tree species, sizes, and locations in or adjacent to the construction area are shown on said Site Plan. All protection measures will be instituted prior to start of construction activities and inspected and approved by the Staff Advisor or his agent.

**TOPOGRAPHIC SURVEY**

LOCATED AT

**185 NORTH MOUNTAIN AVENUE  
ASHLAND, OREGON**

LYING SITUATE WITHIN

NORTHEAST QUARTER OF SECTION 9,  
TOWNSHIP 39 SOUTH, RANGE 1 EAST, W.M.  
CITY OF ASHLAND, JACKSON COUNTY, OREGON

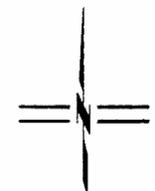
FOR

**MEDINGER CONSTRUCTION CO., INC.**

P.O. Box 702  
Ashland, Oregon 97520

**LEGEND**

- BRASS CAP MONUMENT FOUND
- ▲ IRON PIN MONUMENT FOUND
- SURVEY CONTROL POINT
- PROPERTY BOUNDARY LINE
- - - PROPOSED LOT LINE
- BOUNDARY LINE
- CENTERLINE
- EASEMENT LINE
- FENCE LINE
- FLOWLINE
- WATER LINE
- BURIED NATURAL GAS LINE
- BURIED PHONE LINE
- STORM DRAIN LINE
- SANITARY SEWER LINE
- OVERHEAD POWER LINE
- FIBER OPTIC LINE
- BURIED CABLE TV
- CONTOUR LINE
- GUY ANCHOR
- POWER POLE
- POWER PEDESTAL
- WATER METER
- WATER VALVE
- FIRE HYDRANT
- HOSE BIBB
- CATCHBASIN
- CURB INLET
- SANITARY SEWER MANHOLE
- STORM SEWER MANHOLE
- CLEANOUT
- PHONE MANHOLE
- PHONE PEDESTAL
- CATV PEDESTAL
- GAS VALVE
- MAILBOX
- SIGN POST
- LIGHT POLE
- ★ AREA LIGHT



REGISTERED  
PROFESSIONAL  
LAND SURVEYOR

*Shirley J. ...*

OREGON  
JULY 14, 1998  
SHIRLEY J. ...  
2883 LS

RENEWAL DATE: 6/30/2007

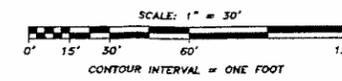
SURVEYED BY:

**POLARIS LAND SURVEYING LLC**  
P.O. BOX 459  
ASHLAND, OREGON 97520  
(541) 482-5009

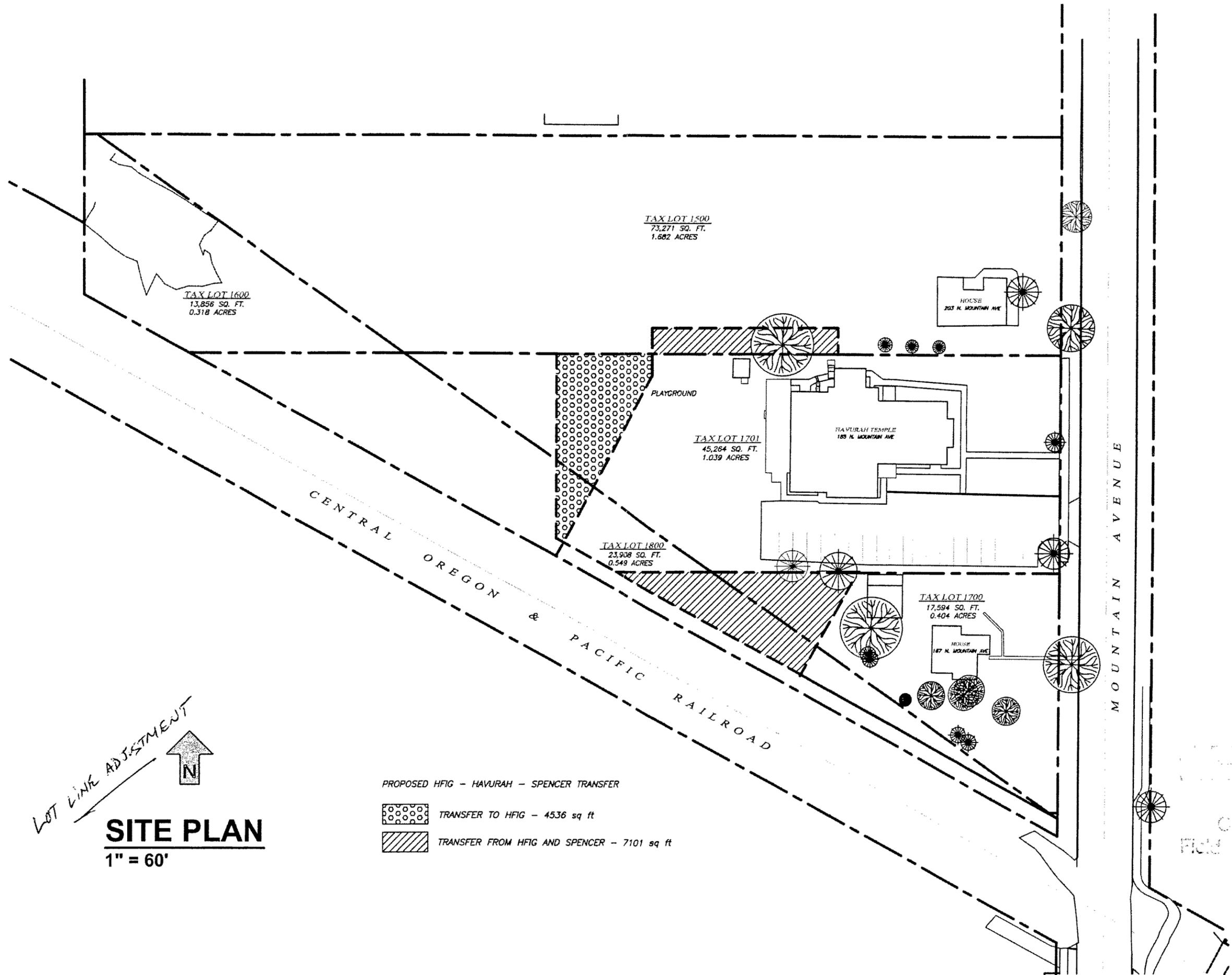
DATE: JULY 26, 2007  
PROJECT NO. 300-05

**SURVEY NOTES**

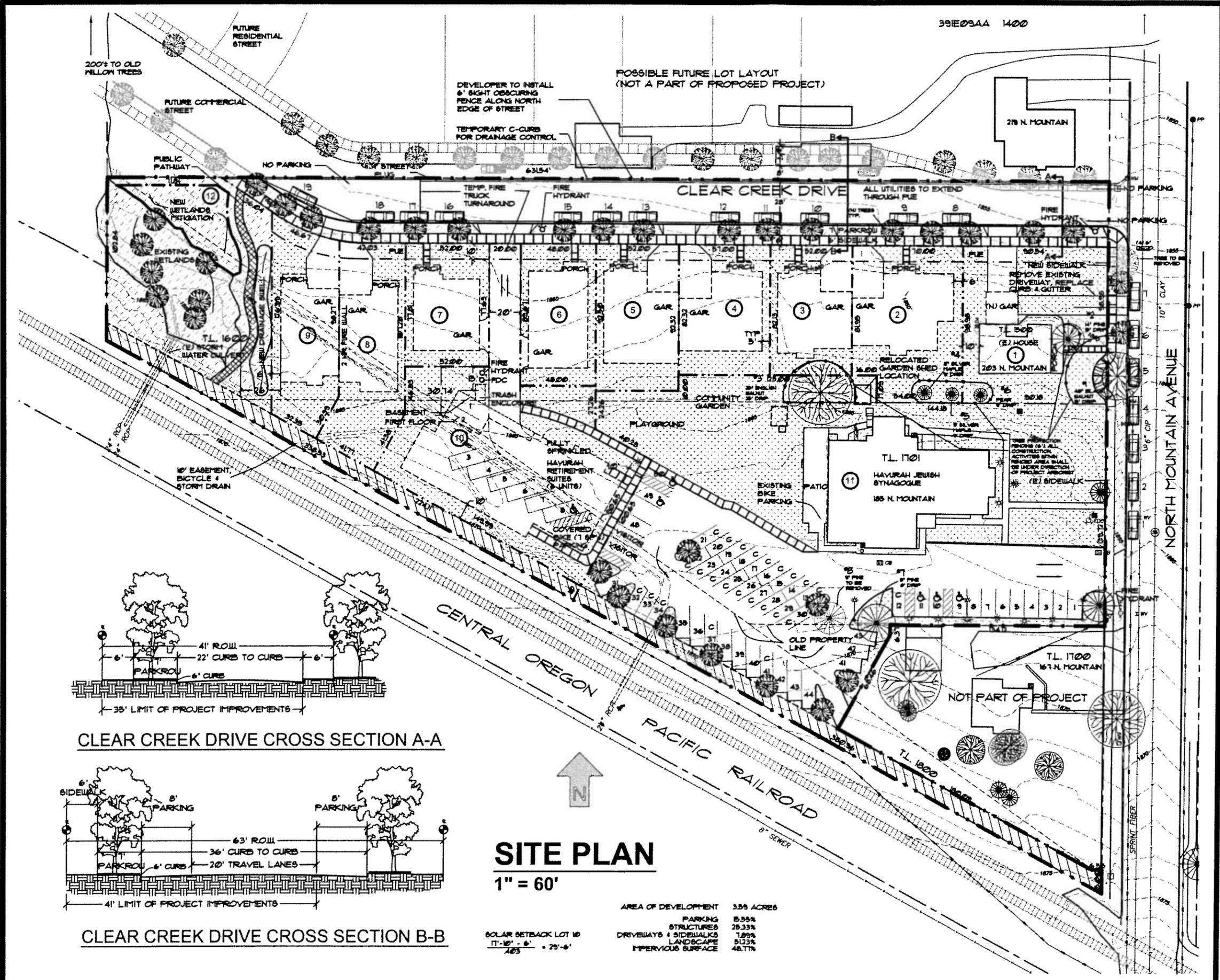
1. THE BASIS OF VERTICAL CONTROL FOR THIS SURVEY IS A CITY OF ASHLAND BENCHMARK BRASS DISC IN THE TOP OF A CONCRETE CURB LOCATED AT THE NORTHWEST INTERSECTION OF MOUNTAIN AVENUE & EAST MAIN STREET. BENCHMARK ELEVATION = 1903.48' BASED ON THE NATIONAL GEODESIC VERTICAL DATUM OF 1928, ADJUSTED IN 1959 (NGVD 2005).
2. EXPOSED UTILITY SHOWN HEREON WERE FIELD LOCATED IN THE PERFORMANCE OF THIS SURVEY. BURIED UTILITY LOCATIONS WERE DETERMINED BY UTILIZING A COMBINATION OF FIELD SURVEYED PAINT MARKS AND "AS-BUILT" RECORD DRAWINGS FURNISHED BY THE RESPECTIVE UTILITY COMPANY REPRESENTATIVES, ARE APPROXIMATE AND SHOWN HEREON FOR CLARIFIC PURPOSES ONLY. FIELD VERIFICATION OF ALL BURIED UTILITIES MUST BE PERFORMED PRIOR TO ANY EXCAVATION OR CONSTRUCTION ACTIVITIES.



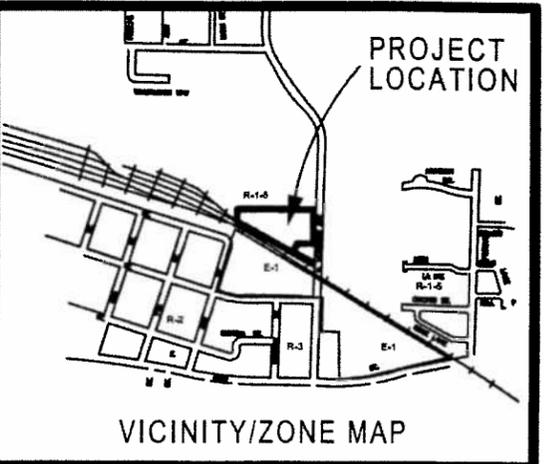
Assessor's Map No. 39 1E 09 AA, Tax Lot 1500, 1600, 1701 & 1800



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 City of Astoria  
 Field Office Court



391E093AA 1400



VICINITY/ZONE MAP

**PROJECT INFORMATION:**

**PROJECT NAME:**  
ALEPH SPRINGS SUBDIVISION  
N. MOUNTAIN AVE. & CLEAR CK. DR.  
ASHLAND, OREGON

**PROPERTY OWNERS & APPLICANTS:**  
HAVURAH FRIENDS INVESTMENT GROUP, LLC.  
JEWISH COMMUNITY HAVURAH OF SOUTHERN OREGON

**LAND USE PLANNING**  
URBAN DEVELOPMENT SERVICES, LLC  
700 MISTLETOE ROAD, SUITE 204  
ASHLAND, OR 97520  
TEL: 482.3334

**WETLAND CONSULTANT:**  
NORTHWEST BIOLOGICAL CONSULTING  
324 TERRACE STREET  
ASHLAND, OR 97520  
541.488.1061

**DRAFTING & DESIGN:**  
MEDINGER CONSTRUCTION COMPANY, INC.  
PO BOX 702  
ASHLAND, OR 97520  
TEL: 482-3961

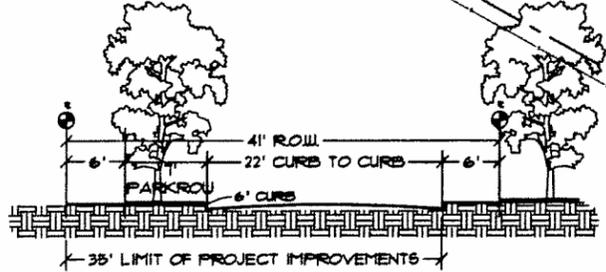
**SURVEYOR:**  
POLARIS LAND SURVEY  
P.O. BOX 459  
ASHLAND, OREGON 97520  
TEL: 482-5009

**ARBORIST:**  
UPPER LIMB-IT TREE SERVICE  
P.O. BOX 881  
ASHLAND, OREGON 97520  
TEL: 482-3667

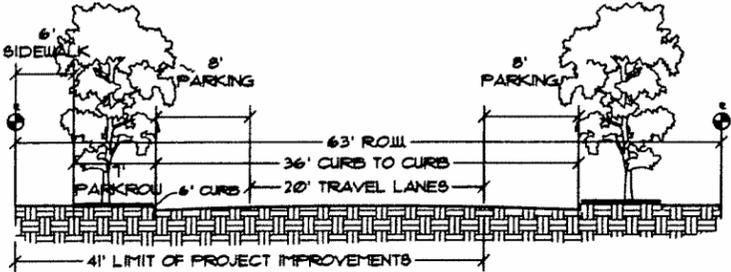
**CIVIL ENGINEER:**  
CONSTRUCTION ENGINEERING CONSULTANTS  
132 W MAIN ST. SUITE 201  
MEDFORD, OREGON  
779-5268

DATE: 11-OCTOBER-07

CLEAR CREEK DRIVE CROSS SECTION A-A



CLEAR CREEK DRIVE CROSS SECTION B-B



**SITE PLAN**

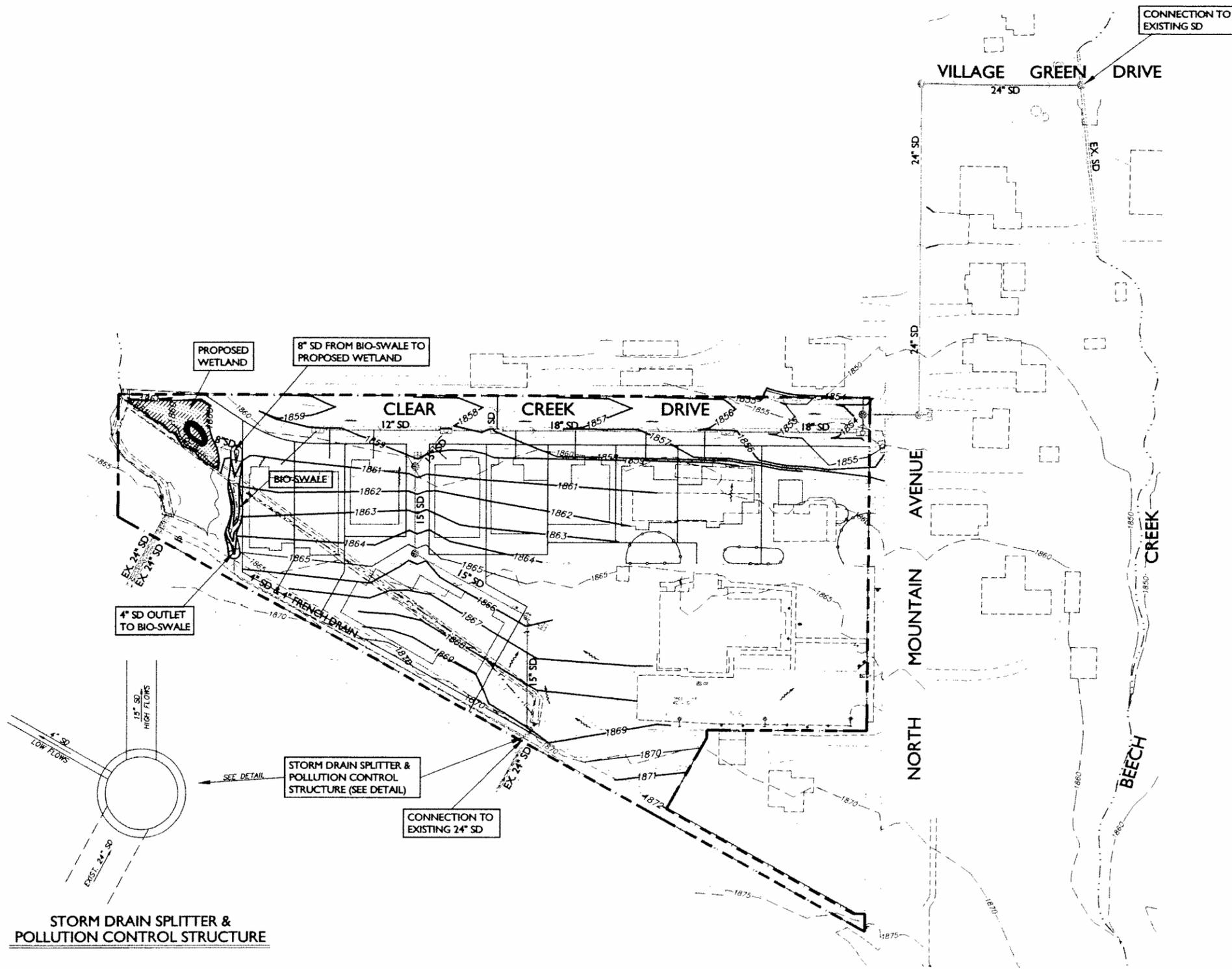
1" = 60'

AREA OF DEVELOPMENT	3.95 ACRES
PARKING STRUCTURES	8.55%
DRIVEWAYS & SIDEWALKS	25.33%
LANDSCAPE	1.99%
IMPERVIOUS SURFACE	51.23%
	48.77%

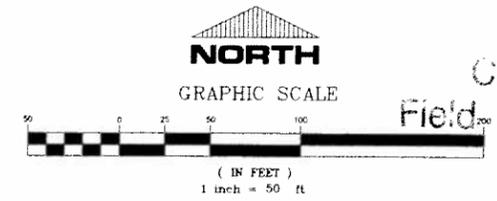
SOLAR SETBACK LOT 10  
11'-10" - 6' - 25'-6"  
403

**PROJECT KEY:**

- PROJECT BOUNDARY ————
- EXISTING PROPERTY LINES - - - - -
- NEW PROPERTY LINES - . . . . .
- ABANDONED PROPERTY LINES - - - - -
- NO PARKING ZONE ————
- PRIVATE LANDSCAPE [Patterned Box]
- OTHER LANDSCAPE [Patterned Box]



- LEGEND**
- PROPOSED STORM MANHOLE
  - PROPOSED CURB INLET
  - PROPOSED CATCH BASIN
  - PROPOSED STORM MAIN
  - EXISTING STORM MAIN
  - DRAINAGE DIRECTION



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 City of Ashland  
 Field Office

**STORM DRAIN SPLITTER & POLLUTION CONTROL STRUCTURE**

**EXHIBIT C.1**



P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5260 • FAX (541) 779-3139

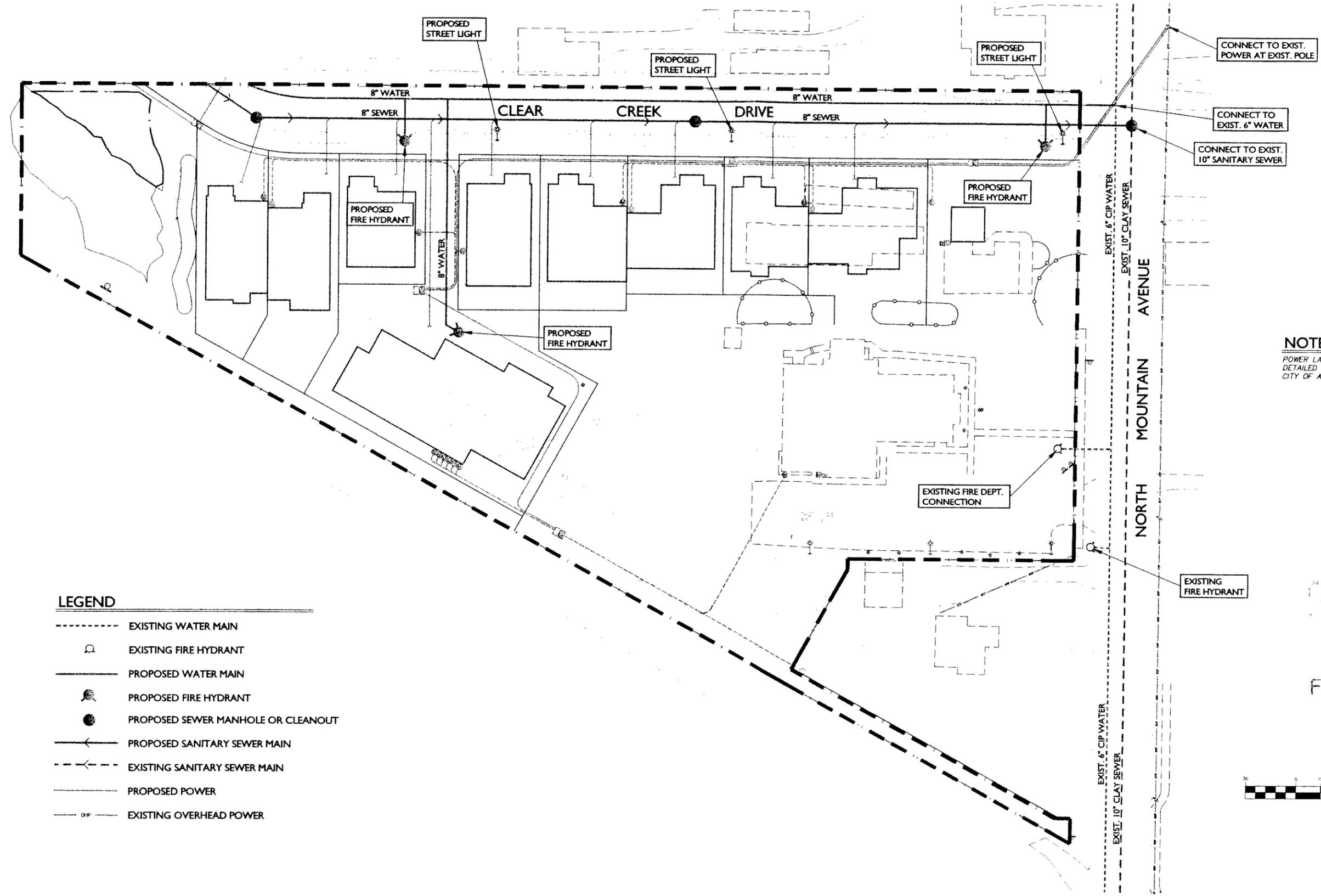
DRAWN BY: XXX	DATE: XX/XX
CHECKED BY: XXX	DATE: XX/XX
	DATE:
	DATE:
	DATE:

NO.	REVISION	DATE	BY



**CITY OF ASHLAND**  
 ALEPH SPRINGS SUBDIVISION  
 CONCEPTUAL  
 GRADING & DRAINAGE PLAN

PROJECT NO.  
 DRAWING NO.  
**41**

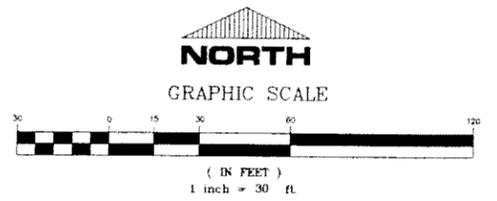


**NOTE:**  
 POWER LAYOUT SHOWN FOR REFERENCE ONLY.  
 DETAILED POWER PLAN TO BE PREPARED BY  
 CITY OF ASHLAND ELECTRIC DEPARTMENT.

- LEGEND**
- EXISTING WATER MAIN
  - ⊕ EXISTING FIRE HYDRANT
  - PROPOSED WATER MAIN
  - ⊕ PROPOSED FIRE HYDRANT
  - PROPOSED SEWER MANHOLE OR CLEANOUT
  - ← PROPOSED SANITARY SEWER MAIN
  - - - EXISTING SANITARY SEWER MAIN
  - PROPOSED POWER
  - OF — EXISTING OVERHEAD POWER

**RECEIVED**

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 Field  Office  Court



**EXHIBIT C.2**



P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5268 • FAX (541) 779-3139

DRAWN BY:	DLG	DATE:	08/07
CHECKED BY:	MWK, PMH	DATE:	08/07
		DATE:	
		DATE:	
		DATE:	

NO.	REVISION	DATE	BY



**CITY OF ASHLAND**  
 ALEPH SPRINGS SUBDIVISION  
 CONCEPTUAL  
 UTILITY PLAN

PROJECT NO.  
 DRAWING NO.  
**42**



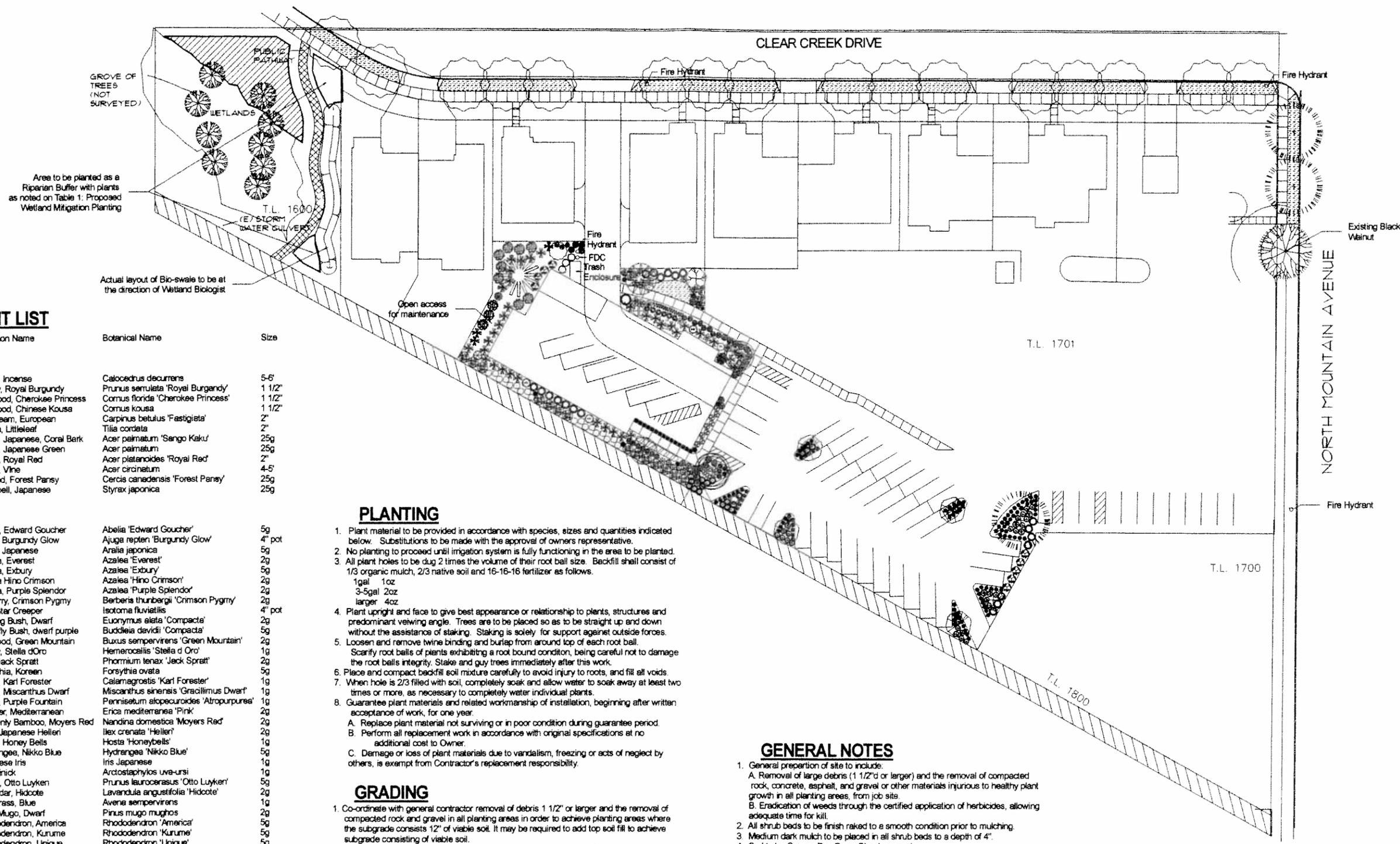
**Madara Design Inc**  
 Landscape Architecture, Design & Consultation  
 2994 Wells Fargo Rd  
 Central Point, Or 97502  
 541-664-7055

**ALEPH SPRINGS SUBDIVISION**  
 N. MOUNTAIN AVENUE  
 Ashland, Or

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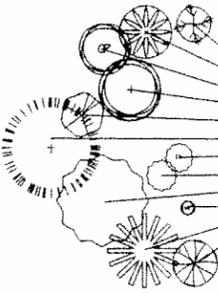
DRAWN BY: TM  
 CHECKED BY: TM  
 10.10.07  
 SCALE:  
 1" = 60'

**L1**  
 43

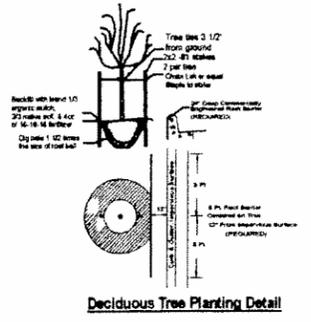


**PLANT LIST**

Quantity	Common Name	Botanical Name	Size
<b>Trees</b>			
2	Cedar, Incense	Calocedrus decurrens	5-6'
1	Cherry, Royal Burgundy	Prunus serrulata 'Royal Burgundy'	1 1/2"
1	Dogwood, Cherokee Princess	Cornus florida 'Cherokee Princess'	1 1/2"
1	Dogwood, Chinese Kousa	Cornus kousa	1 1/2"
4	Hornbeam, European	Carpinus betulus 'Fastigiata'	2"
3	Linden, Littleleaf	Tilia cordata	2"
1	Maple, Japanese, Coral Bark	Acer palmatum 'Sango Kaku'	25g
1	Maple, Japanese Green	Acer palmatum	25g
16	Maple, Royal Red	Acer platanoides 'Royal Red'	2"
3	Maple, Vine	Acer circinatum	4-5'
1	Redbud, Forest Pansy	Cercis canadensis 'Forest Pansy'	25g
1	Snowbell, Japanese	Styrax japonica	25g
<b>Shrubs</b>			
9	Abelia, Edward Goucher	Abelia 'Edward Goucher'	5g
27	Ajuga, Burgundy Glow	Ajuga reptans 'Burgundy Glow'	4" pot
1	Aralia, Japanese	Aralia japonica	5g
8	Azalea, Everest	Azalea 'Everest'	2g
1	Azalea, Exbury	Azalea 'Exbury'	5g
6	Azalea Hino Crimson	Azalea 'Hino Crimson'	2g
8	Azalea, Purple Splendor	Azalea 'Purple Splendor'	2g
29	Barberry, Crimson Pygmy	Berberis thunbergii 'Crimson Pygmy'	2g
111	Blue Star Creeper	Isotoma fluviatilis	4" pot
18	Burning Bush, Dwarf	Euonymus alata 'Compacta'	2g
1	Butterfly Bush, dwarf purple	Buddleia davidii 'Compacta'	5g
3	Boxwood, Green Mountain	Buxus sempervirens 'Green Mountain'	2g
3	Daylily, Stella d'Oro	Hemerocallis 'Stella d'Oro'	1g
1	Flax, Jack Spratt	Phormium tenax 'Jack Spratt'	2g
1	Forsythia, Korean	Forsythia ovata	5g
4	Grass, Karl Forester	Calamagrostis 'Karl Forester'	1g
2	Grass, Miscanthus Dwarf	Miscanthus sinensis 'Gracillimus Dwarf'	1g
6	Grass, Purple Fountain	Pennisetum alopecuroides 'Atrypurpurea'	1g
14	Heather, Mediterranean	Erica mediterranea 'Pink'	2g
9	Heavenly Bamboo, Moyers Red	Nandina domestica 'Moyers Red'	2g
22	Holly, Japanese Helleri	Ilex crenata 'Helleri'	2g
6	Hosta, Honey Bells	Hosta 'Honeybells'	1g
1	Hydrangea, Nikko Blue	Hydrangea 'Nikko Blue'	5g
3	Japanese Iris	Iris Japanese	1g
25	Kinnikinnick	Arctostaphylos uva-ursi	1g
20	Laurel, Otto Luyken	Prunus laurocerasus 'Otto Luyken'	5g
7	Lavender, Hidcote	Lavandula angustifolia 'Hidcote'	2g
6	Oat Grass, Blue	Avena sempervirens	1g
5	Pine, Mugo, Dwarf	Pinus mugo mughos	2g
1	Rhododendron, America	Rhododendron 'America'	5g
1	Rhododendron, Kurume	Rhododendron 'Kurume'	5g
1	Rhododendron, Unique	Rhododendron 'Unique'	5g
14	Rosemary, Huntington Carpet	Rosmarinus 'Huntington Carpet'	1g
8	Sedum, Brilliant	Sedum spectabile 'Brilliant'	1g
38	Viburnum, David	Viburnum davidii	5g
11	Viburnum, Spring Bouquet	Viburnum tinus	5g



Turf Grass Lawns  
 Annual Color Spot



**PLANTING**

- Plant material to be provided in accordance with species, sizes and quantities indicated below. Substitutions to be made with the approval of owners representative.
- No planting to proceed until irrigation system is fully functioning in the area to be planted.
- All plant holes to be dug 2 times the volume of their root ball size. Backfill shall consist of 1/3 organic mulch, 2/3 native soil and 16-16-16 fertilizer as follows:  
 1gal 1oz  
 3-5gal 2oz  
 larger 4oz
- Plant upright and face to give best appearance or relationship to plants, structures and predominant viewing angle. Trees are to be placed so as to be straight up and down without the assistance of staking. Staking is solely for support against outside forces.
- Loosen and remove twine binding and burlap from around top of each root ball. Scarify root balls of plants exhibiting a root bound condition, being careful not to damage the root balls integrity. Stake and guy trees immediately after this work.
- Place and compact backfill soil mixture carefully to avoid injury to roots, and fill all voids.
- When hole is 2/3 filled with soil, completely soak and allow water to soak away at least two times or more, as necessary to completely water individual plants.
- Guarantee plant materials and related workmanship of installation, beginning after written acceptance of work, for one year.  
 A. Replace plant material not surviving or in poor condition during guarantee period.  
 B. Perform all replacement work in accordance with original specifications at no additional cost to Owner.  
 C. Damage or loss of plant materials due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility.

**GRADING**

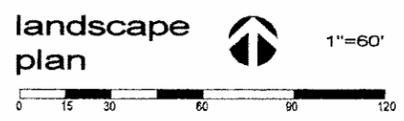
- Co-ordinate with general contractor removal of debris 1 1/2" or larger and the removal of compacted rock and gravel in all planting areas in order to achieve planting areas where the subgrade consists 12" of viable soil. It may be required to add top soil fill to achieve subgrade consisting of viable soil.
- General contractor responsible to provide a sub-grade within 1" of finish grade in shrub areas and within 3" of finish grade in lawn areas. All subgrades to be adequately firm without being overly compacted.
- Sub grading to consist of grading existing soil to a smooth even grade, no undulation greater than plus or minus 1" within any 10 lineal feet of distance. Sub grading to begin 3" below top of surrounding hardscapes, curbs, sidewalks, etc., and to slope smoothly across top of any berming. Berming to rise to a peak equidistant between surrounding hardscapes sloping equally on both sides.
- Landscape to place 3" of compacted (4" loose) clean topsoil to achieve finish grade in lawn areas. Imported topsoil shall be 10%-70% Sand, 20%-80% silt, and 5%-25% clay, free of alkali, nematodes, harmful chemicals, debris, waste materials, rocks over 1" in diameter, and noxious weeds. Additional soil may be necessary depending on available native soil.
- Placement and grading of any soil to be done in coordination with suitable weather condition so as to prevent damage to soil structure.
- Finish grade in shrub areas to be a smooth even grade mounded 2" high in the middle of beds and ending 3" below surrounding areas. All finish grading to promote positive drainage away from structures and to be done in such a way as to eliminate puddling or collection of water. Exceptions will be as noted on plans.
- Landscape contractor responsible for addressing any drainage problems encountered during the course of construction, with owners representative.
- Provide owner with a per cubic yard placed cost for additional soil.

**GENERAL NOTES**

- General preparation of site to include:  
 A. Removal of large debris (1 1/2" or larger) and the removal of compacted rock, concrete, asphalt, and gravel or other materials injurious to healthy plant growth in all planting areas, from job site.  
 B. Eradication of weeds through the certified application of herbicides, allowing adequate time for kill.
- All shrub beds to be finish raked to a smooth condition prior to mulching.
- Medium dark mulch to be placed in all shrub beds to a depth of 4".
- Sod to be Oregon Rye Grass Blend or equal.
- Sod preparation:  
 A. Bring areas to an even, smooth grade removing hard or soft areas that might impede drainage or cause puddling.  
 B. Install as per growers recommendations.
- INCLUDE 120 DAYS OF MAINTENANCE from the day of acceptance. Including but not limited to:  
 A. Maintain planting area in a healthy, weed free condition through a minimum of bi-weekly visits.  
 B. Mow lawns weekly per suppliers recommendations.  
 C. Replace any material showing signs of stress.  
 D. Monitor irrigation for correct timing.  
 E. Provide owner with complete list of instructions for continued care.
- Plan is diagrammatic and measurements should be confirmed on-site. Any changes are the responsibility of the contractor to co-ordinate with the owners representative.

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**Madara Design Inc**  
 Landscape Architecture, Design & Consultation  
 2804 Wells Fargo Rd  
 Central Point, Or 97502  
 541-664-7055

**ALEPH SPRINGS SUBDIVISION**  
 N. MOUNTAIN AVENUE  
 Ashland, Or

All drawings and written material appearing herein, constitutes the original and copyrighted work of Madara Design Inc. and the same may not be reproduced, used, or disclosed without written consent. Madara Design Inc. is to assume responsibility for revisions to plans due to: unknown grade variations, differences in dimensions, or on-site changes by others.  
 DRAWN BY: TM  
 CHECKED BY: TM

10.10.07  
 SCALE:  
 1" = 60'

**L2**  
 44

**Irrigation Parts**

- Valve number
- Galons Per Minute/ Drip Valve size in inches
- Hunter PGV electric control valve size as noted
- Backflow device 1" Wilkins 950-XLT Double check valve assembly.
- Hunter PRO-C Control Clock model PC-900 Mounted on outside of building
- 1/2" Manual Drain located at low point in mainline. Place in 6" round Valve Box
- Mainline PVC Pipe Sc 40 all pipe not noted 1 1/4" in size
- Laterals PVC Pipe Class200 All pipe not noted is 1" in size
- All PVC mainlines to be buried a minimum of 18" deep with clean backfill. Laterals to be buried a minimum of 15" deep with clean backfill.
- Sleeve 4" PVC CI 125 except as noted

**IRRIGATION NOTES**

Layout of sprinkler heads in shrub areas to be based on a head to head coverage configuration starting from the perimeter and working towards the interior of the space to be watered.  
 -----NO EXCEPTIONS-----

Design is based on 25gpm at 50psi available pressure per owners information 1" water meter provided by others. Confirm on-site before proceeding.

Steering to be installed by others.

Mainline should be located in area with least conflict with surrounding utilities. Mainline location on plan for ease of interpretation.

Rain Bird 1806 series heads in shrub areas. Use nozzles as noted.

When necessary Nelson MP Rotator nozzles will be used on 1806 heads. Nozzles will be dependent on actual distances encountered on-site. Use nozzle appropriate to range. Spray patterns indicated on plan may not be accurate for the area to be covered and may require the use of variable arc nozzles to achieve appropriate coverage. All sprinkler heads to be placed 6" from adjacent hardscape and 12" from any building. Some heads as noted on plan are to be additional distances. Adjust radius and arc to prevent overspray on adjoining walks and paving.

Place manual drain valves as needed at any low points in mainline to allow for complete drainage of mainline.

Control wires, 14ga minimum, to be located below all piping in any ditch. Any wire splices are to be located in a minimum of a 10" round valve box and noted on as-built. Power control wires to be red, common to be white. Tape bundled wire together every 5' before placing in ditch.

Clock location is approximate and will be determined on site.

Irrigation plan is diagrammatic and not necessarily accurate for all areas. Base map provided by others.

Provide owner with accurate as-built locating all actual valves, wire splices, main line and any steering.

Provide owner with preliminary watering schedule for the established landscape.

Timing of irrigation adjacent to Clear Creek Drive to be coordinated to avoid overlapping of valve timing.

Provide owner with complete set of written instructions for operation of sprinkler system including spring start up, clock operation, and winterization.

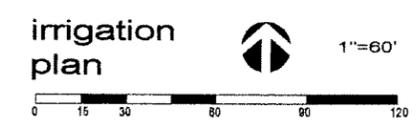
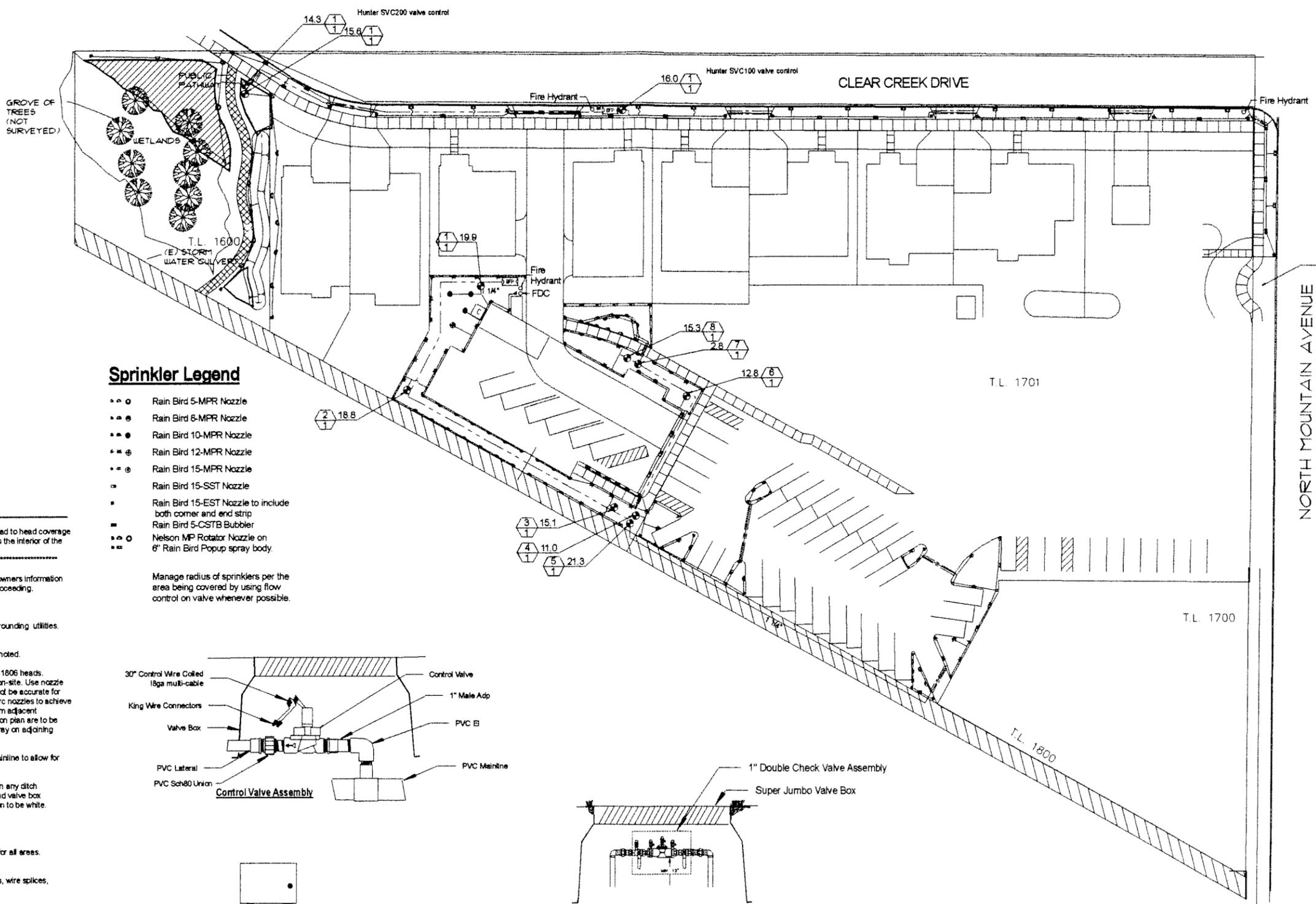
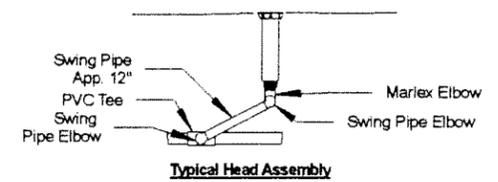
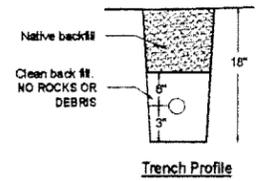
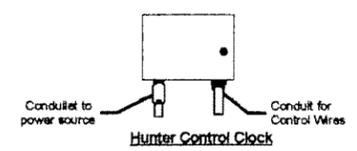
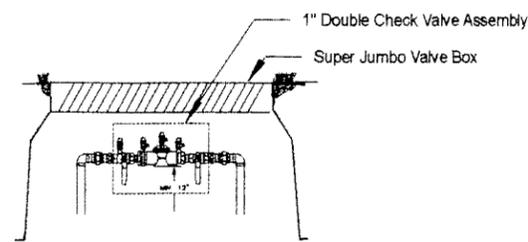
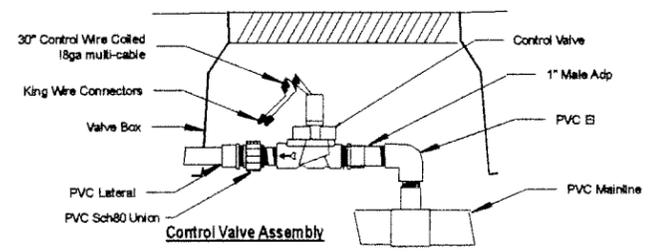
Walk owner through the entire system describing the operating instructions.

Guarantee irrigation system for one year from date of acceptance.  
 A. Replace any materials found to be defective or not operating to manufactures standards during guarantee period.  
 B. Perform all replacement work in accordance with original specifications at no additional cost to Owner.  
 C. Damage or loss of materials due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility.

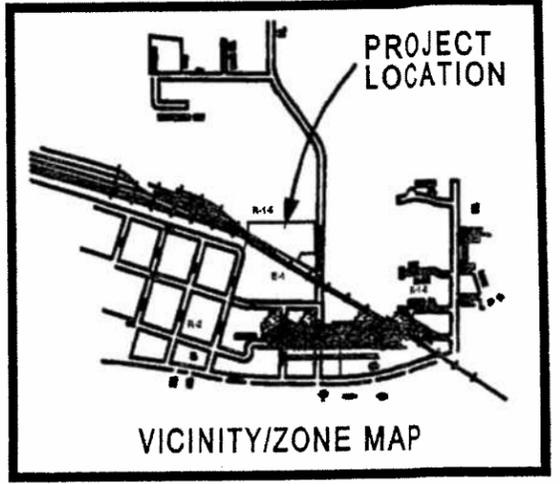
**Sprinkler Legend**

- Rain Bird 5-MPR Nozzle
- Rain Bird 6-MPR Nozzle
- Rain Bird 10-MPR Nozzle
- Rain Bird 12-MPR Nozzle
- Rain Bird 15-MPR Nozzle
- Rain Bird 15-SST Nozzle
- Rain Bird 15-EST Nozzle to include both corner and end strip
- Rain Bird 5-CSTB Bubbler
- Nelson MP Rotator Nozzle on 6" Rain Bird Popup spray body.

Manage radius of sprinklers per the area being covered by using flow control on valve whenever possible.



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**TYPICAL STREETSCAPE AT SINGLE FAMILY ATTACHED**

**PROJECT INFORMATION:**

**PROJECT NAME:**  
ALEPH SPRINGS SUBDIVISION  
N. MOUNTAIN AVE. & CLEAR CK. DR.  
ASHLAND, OREGON

**PROPERTY OWNERS & APPLICANTS:**  
HAVURAH FRIENDS INVESTMENT GROUP, LLC.  
JEWISH COMMUNITY HAVURAH OF SOUTHERN OREGON

**LAND USE PLANNING**  
URBAN DEVELOPMENT SERVICES, LLC  
700 MISTLETOE ROAD, SUITE 204  
ASHLAND, OR 97520  
TEL: 482.3334

**WETLAND CONSULTANT:**  
NORTHWEST BIOLOGICAL CONSULTING  
324 TERRACE STREET  
ASHLAND, OR 97520  
541.488.1061

**DRAFTING & DESIGN:**  
MEDINGER CONSTRUCTION COMPANY, INC.  
PO BOX 702  
ASHLAND, OR 97520  
TEL: 482-3961

**SURVEYOR:**  
POLARIS LAND SURVEY  
P.O. BOX 459  
ASHLAND, OREGON 97520  
TEL: 482-5009

**ARBORIST:**  
UPPER LIMB-IT TREE SERVICE  
P.O. BOX 881  
ASHLAND, OREGON 97520  
TEL: 482-3667

**CIVIL ENGINEER:**  
CONSTRUCTION ENGINEERING CONSULTANTS  
132 W MAIN ST, SUITE 201  
MEDFORD, OREGON  
779-5268

DATE: 11-OCTOBER-07

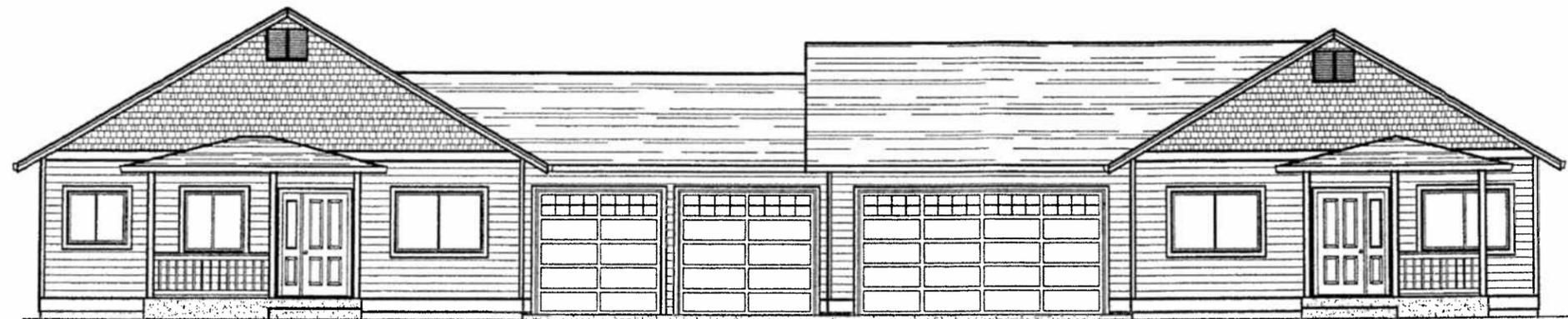
**ELEVATIONS**

1" = 10'

STREETSCAPE

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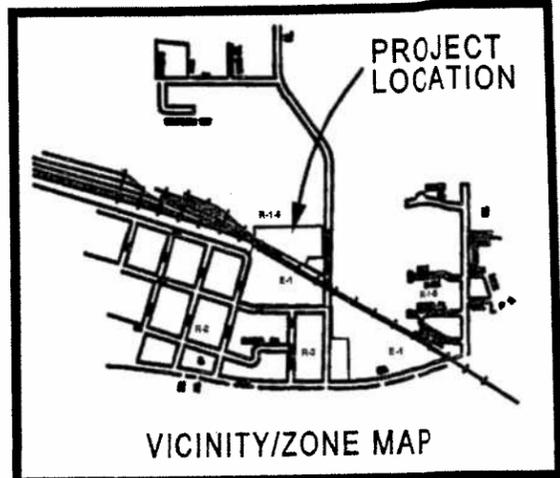


○ SINGLE FAMILY ATTACHED



ARCHIT. COMP. ROOFING  
 GABLE VENTS  
 ASSORTED GABLE SIDING  
 5/4" SPRUCE DETAIL TRIM  
 HORIZ HARDI-PLANK

○ SINGLE FAMILY DETACHED



**PROJECT INFORMATION:**

PROJECT NAME:  
 ALEPH SPRINGS SUBDIVISION  
 N. MOUNTAIN AVE. & CLEAR CK. DR.  
 ASHLAND, OREGON

PROPERTY OWNERS & APPLICANTS:  
 HAVURAH FRIENDS INVESTMENT GROUP, LLC.  
 JEWISH COMMUNITY HAVURAH OF SOUTHERN OREGON

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 541.488.1061

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 132 W MAIN ST, SUITE 201  
 MEDFORD, OREGON  
 779-5268

DATE: 11-OCTOBER-07

**ELEVATIONS**

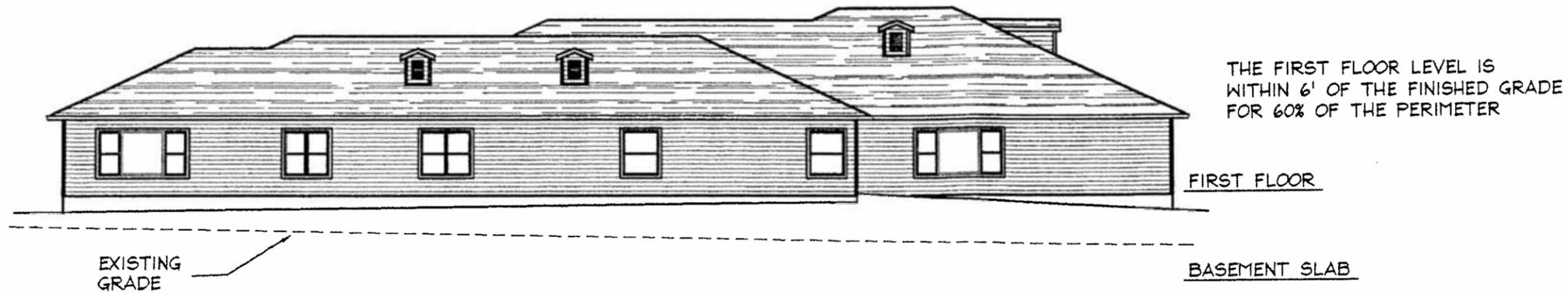
1" = 10'

SINGLE FAMILY

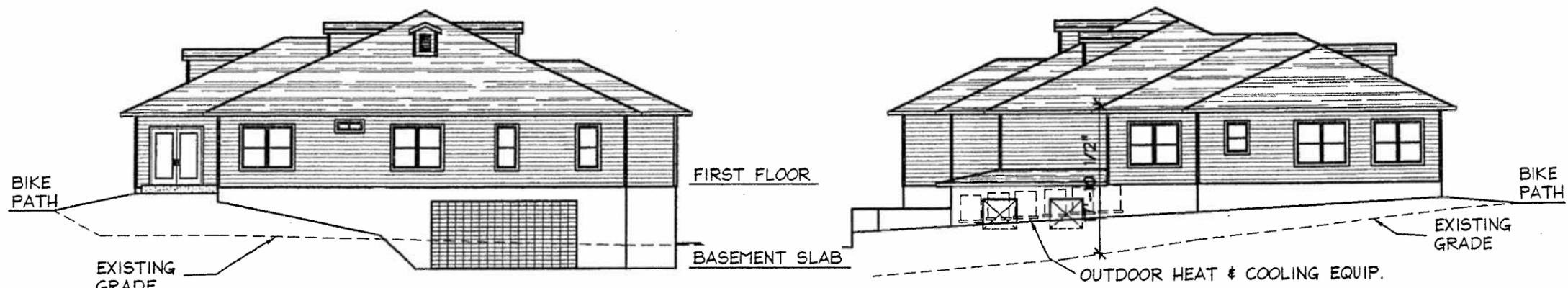
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46

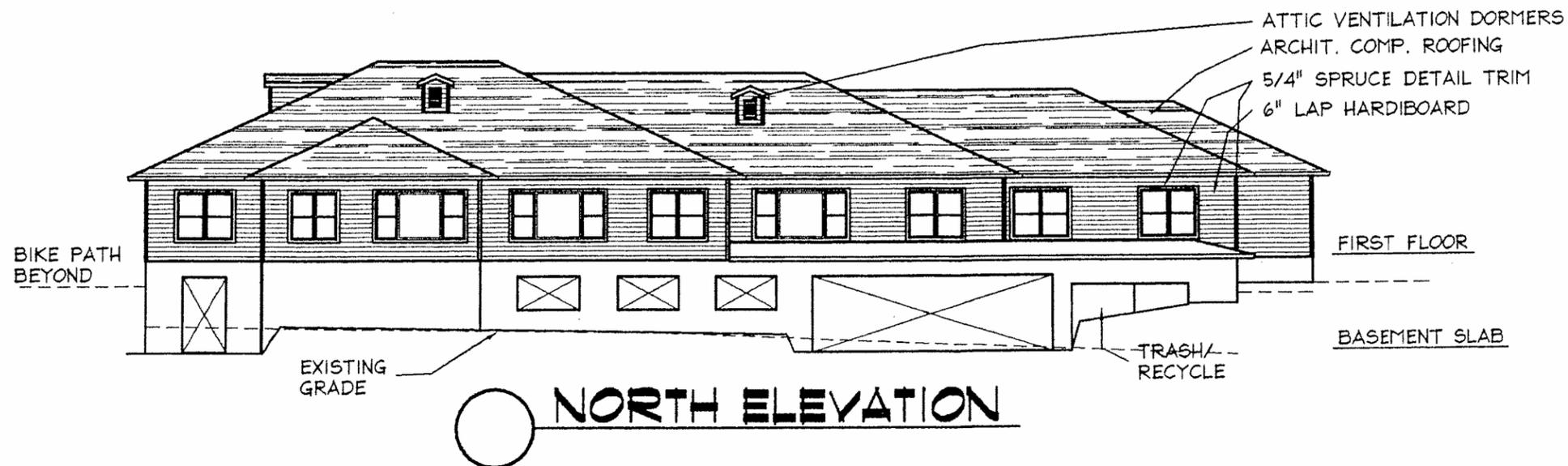


**SOUTH ELEVATION**

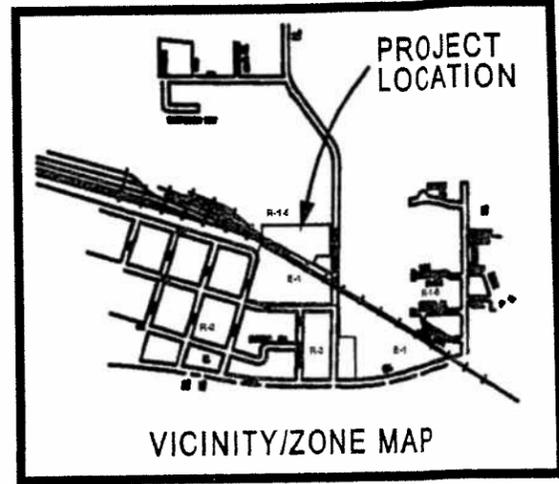


**EAST ELEVATION**

**WEST ELEVATION**



**NORTH ELEVATION**



**PROJECT INFORMATION:**

PROJECT NAME:  
ALEPH SPRINGS SUBDIVISION  
N. MOUNTAIN AVE. & CLEAR CK. DR.  
ASHLAND, OREGON

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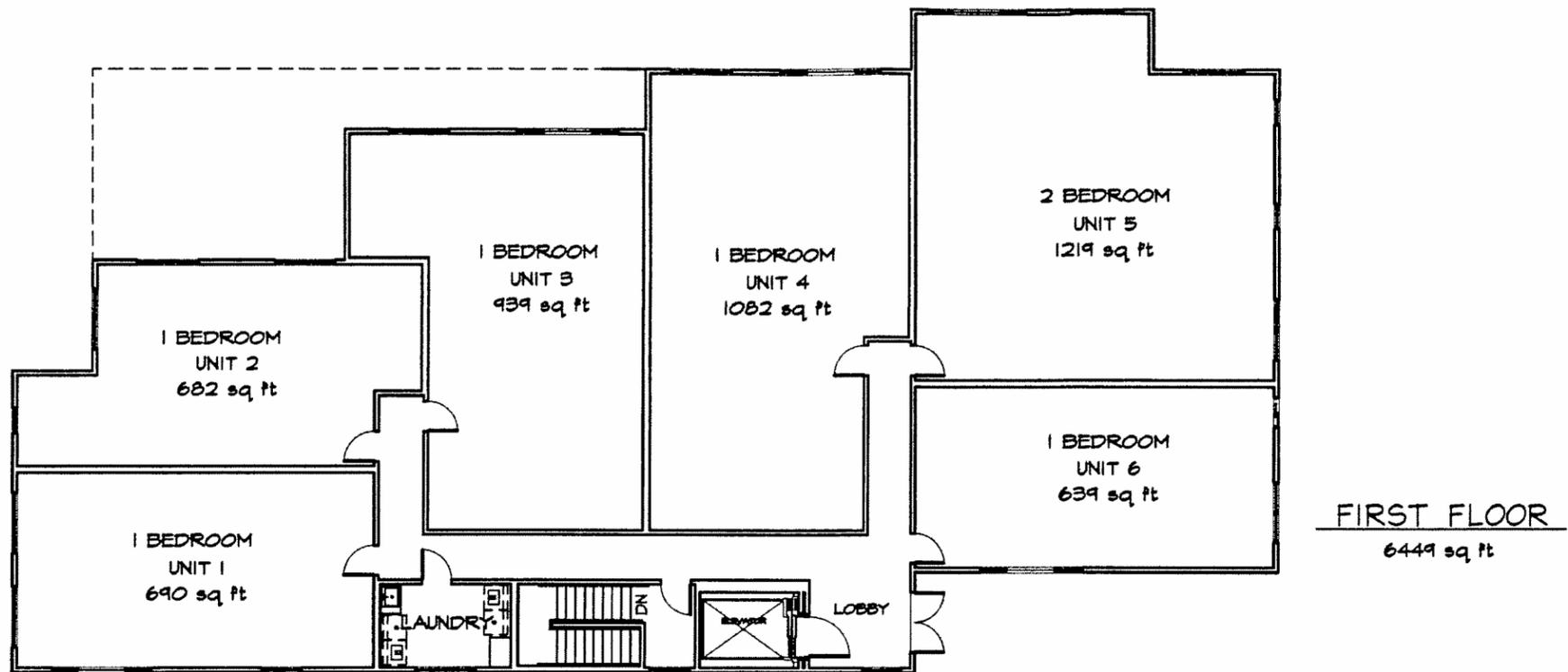
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**ELEVATIONS**  
1/16" = 1'-0"

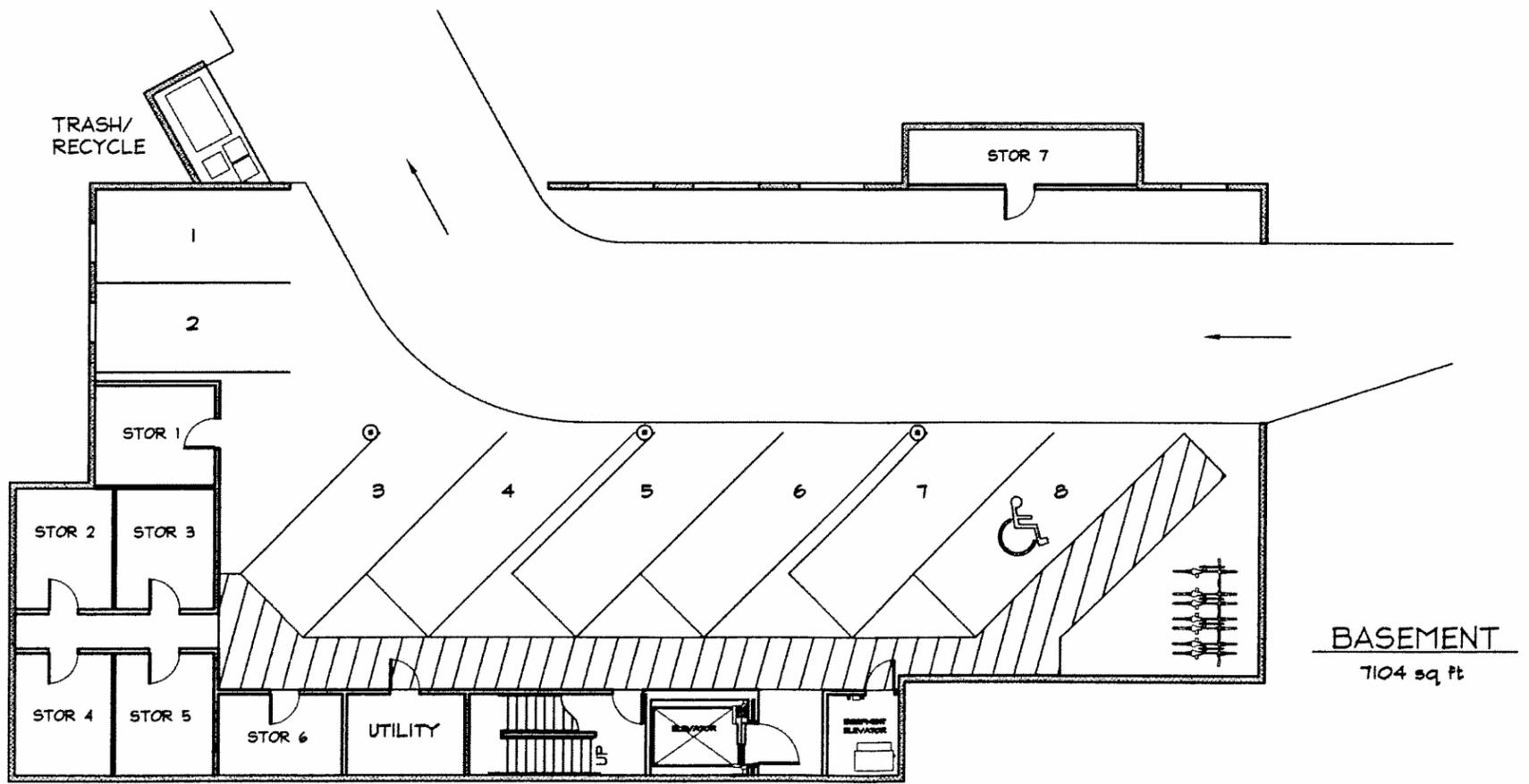
6 UNIT CONDOMINIUM

City of Ashland  
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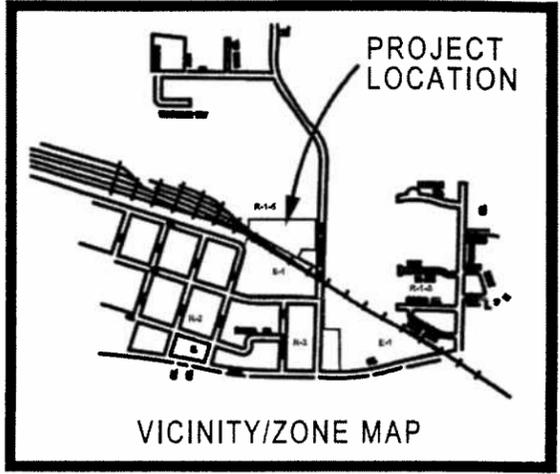
47



**FIRST FLOOR**  
6449 sq ft



**BASEMENT**  
7104 sq ft



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541.488.1061

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779-5268

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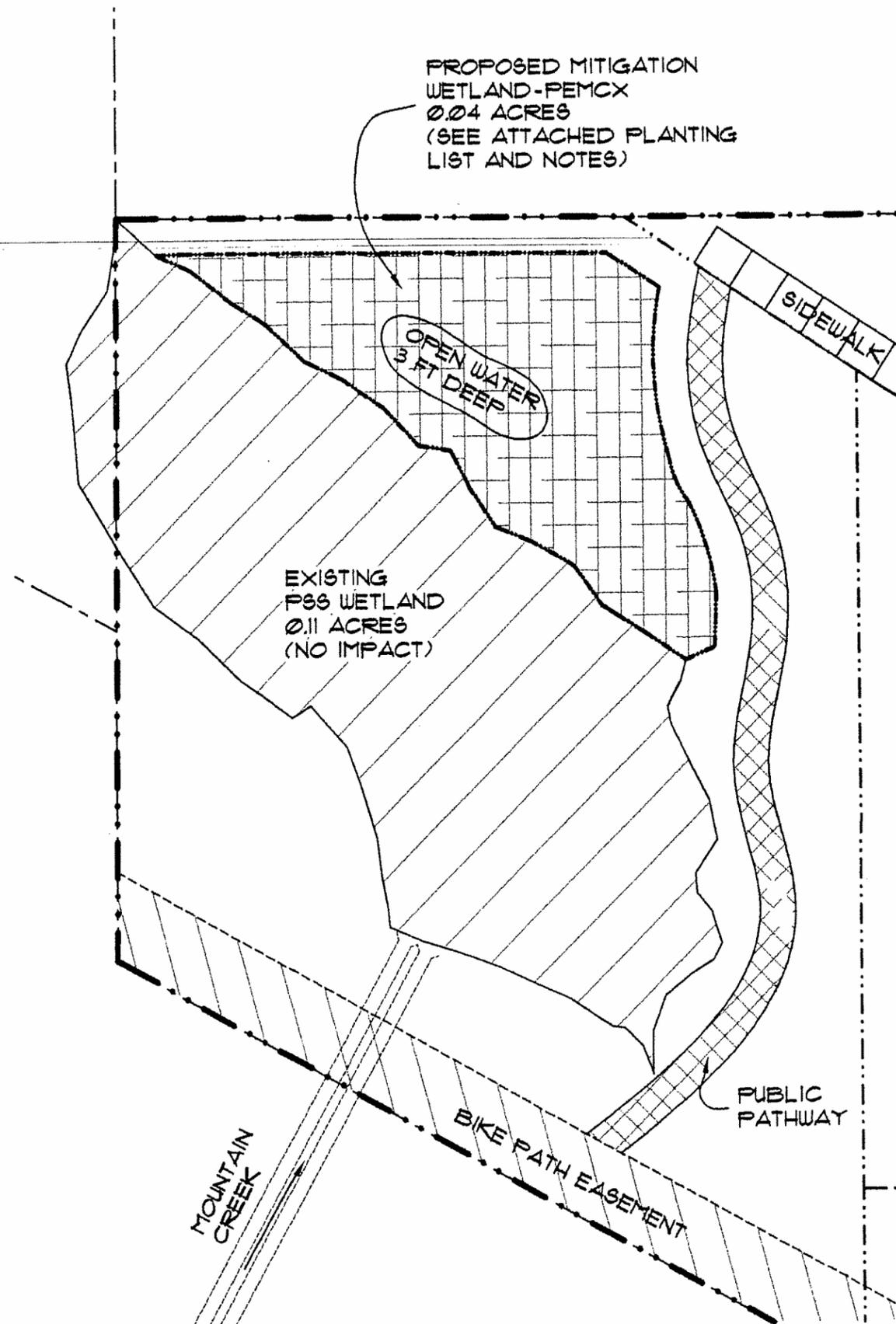
**FLOOR PLANS**  
1/16" = 1'-0"

**6 UNIT CONDOMINIUM**

City of Ashland  
Field [ ] to [ ] Court

40

# WETLAND MITIGATION PLANTING PLAN



## PLANTING PLAN

### EMERGENT WETLAND PLANTS

- SPREADING RUSH, *JUNCUS PATENS* (FACW)
- DENSE SEDGE, *CAREX Densa* (OBL)
- MONKEY FLOWER, *MIMULUS GATTATUS* (OBL)
- SOFT RUSH, *JUNCUS EFFUSUS* (FACW+)
- BULRUSH, *SCIRPUS ACUTUS* (OBL)
- CREeping SPIKERUSH, *ELOCHATIS PALUSTRIS* (OBL)
- ARROUHEAD, *SAGGITARIA LATIFOLIA* (OBL)

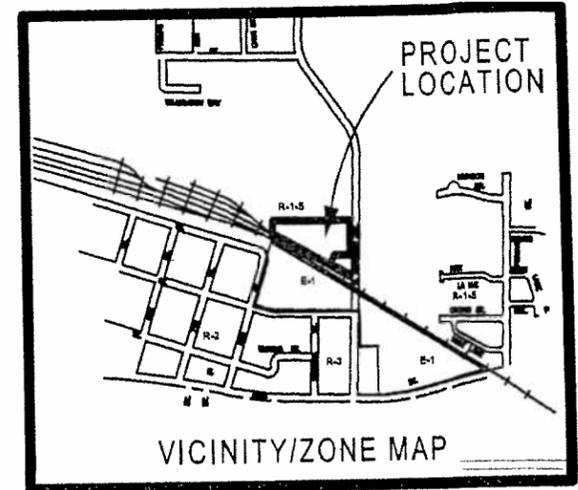
### RIPARIAN BUFFER

- RED OSIER DOGWOOD, *CORNUS SP.*
- SPIREA, *SPIRIA DOUGLASII* (FACW)
- WILLOW, *SALIX SP.* (FACW)
- OREGON ASH, *FRAXINUS LATIFOLIA*
- WHITE ALDER, *ALNUS SP.*



1" = 20'

FIGURE \_\_\_\_\_



## PROJECT INFORMATION:

**PROJECT NAME:**  
ALEPH SPRINGS SUBDIVISION  
N. MOUNTAIN AVE. & CLEAR CK. DR.  
ASHLAND, OREGON

**PROPERTY OWNERS & APPLICANTS:**  
HAVURAH FRIENDS INVESTMENT GROUP, LLC.  
JEWISH COMMUNITY HAVURAH OF SOUTHERN OREGON

**LAND USE PLANNING**  
URBAN DEVELOPMENT SERVICES, LLC  
700 MISTLETOE ROAD, SUITE 204  
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TEL: 482.3334

**WETLAND CONSULTANT:**  
NORTHWEST BIOLOGICAL CONSULTING  
324 TERRACE STREET  
ASHLAND, OR 97520  
541.488.1081

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MEDINGER CONSTRUCTION COMPANY, INC.  
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TEL: 482-3961

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P.O. BOX 881  
ASHLAND, OREGON 97520  
TEL: 482-3667

**CIVIL ENGINEER:**  
CONSTRUCTION ENGINEERING CONSULTANTS  
132 W MAIN ST. SUITE 201  
MEDFORD, OREGON  
776-5268

DATE: 2-AUGUST-07

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**NORTHWEST BIOLOGICAL CONSULTING**

HABITAT RESTORATION - ENVIRONMENTAL PLANNING

Cal. Engineering Contractors Lic. #599428

**WETLAND DELINEATION REPORT**

**AELPH SPRINGS, HAVURAH SHIR HADASH  
JEWISH RENEWAL COMMUNITY**

**ASHLAND, OREGON**

Submitted To: Department of State Lands  
Wetlands Program  
775 Summer Street, NE  
Salem, Oregon 97301

U.S. Army Corps of Engineers  
1600 Executive Parkway, Suite 210  
Eugene, Oregon 97401

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SEP 18 2007

C. J. [unclear] [unclear]

Submitted By: Northwest Biological Consulting  
324 Terrace Street  
Ashland, Oregon 97520  
Phone (541) 488-1061

In Conjunction With: Thompson Ecological Consulting  
6378 Pioneer Road  
Medford, OR 97501  
Phone (541) 776-8505

July 6, 2007

P.O. Box 671- 324 Terrace St, Ashland, OR, 97520- Ph/Fax (541) 488-1061

**WETLAND DELINEATION / DETERMINATION REPORT COVER FORM**

This form constitutes a request for a jurisdictional determination by the Department of State Lands. It must be fully completed and signed, and attached to the front of reports submitted to the Department for review and approval.

**Wetlands Program Manager/Oregon Department of State Lands  
775 Summer Street NE, Suite 100  
Salem, OR 97301-1279**

<input checked="" type="checkbox"/> Applicant <input type="checkbox"/> Owner Name, Firm and Address: <b>Larry Mendinger</b> PO Box 702 Ashland, Oregon 97520	Business phone # <b>(541) 482-3961</b> Home phone # (optional) FAX # <b>(541) 482-6486</b> E-mail: _____
<input type="checkbox"/> Authorized Legal Agent, Name and Address: <b>See Above</b>	Business phone # _____ FAX # _____ E-mail: _____

I either own the property described below or I have legal authority to allow access to the property. I authorize the Department to access the property for the purpose of confirming the information in the report, after prior notification to the primary contact.

Typed/Printed Name: Larry Mendinger Signature: *Larry Mendinger* For Larry Mendinger  
 Date: **August 18, 2007** Special instructions regarding site access: \_\_\_\_\_

**Project and Site Information** (for latitude & longitude, use centroid of site or start & end points of linear project)

Project Name: <b>Aelph Springs</b>	Latitude: <b>42 11 50.08 N</b>	Longitude: <b>122 42 3.35 W</b>
Proposed Use: <b>Residential Housing</b>	Tax Map # <b>391E09AA</b>	
Project Street Address (or other descriptive location): <b>185 Mountain Avenue</b>	Township <b>39S</b> Range <b>1E</b> Section <b>09</b> QQ <b>AA</b>	
City: <b>Ashland</b> County: <b>Jackson</b>	Tax Lot (s) <b>1500,1600, and 1800</b>	
	Waterway: <b>Mountain Creek</b> River Mile: _____	
	NWI Quad(s): <b>Ashland</b>	

**Wetland Delineation Information**

Wetland Consultant Name, Firm and Address: <b>Scott English, Northwest Biological Consulting</b> 324 Terrace Street Ashland, Oregon 97520	Phone # <b>(541) 488-1061</b> FAX # <b>see above</b> E-mail address: <b>nwbiolog@gmail.com</b>
The information and conclusions on this form and in the attached report are true and correct to the best of my knowledge.	
Consultant Signature: <u><i>Scott English</i></u>	Date: <b>August 18, 2007</b>
Primary Contact for report review and site access is <input checked="" type="checkbox"/> Consultant <input type="checkbox"/> Applicant/Owner <input checked="" type="checkbox"/> Authorized Agent	
Wetland/Waters Present? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Total Wetland Acreage: <b>0.13</b>

**Delineation Purpose:**

<input checked="" type="checkbox"/> R-F permit application submitted with delineation <input type="checkbox"/> Mitigation bank site <input type="checkbox"/> Industrial Land Certification Program site <input type="checkbox"/> R-F application will be submitted within 90 days	<input type="checkbox"/> Sale, purchase, lease etc. <input type="checkbox"/> Partition, re-plat, lot line adjustment <input type="checkbox"/> Habitat restoration project <input type="checkbox"/> Other: _____
<b>Other Information:</b>	
Has previous delineation/application been made on parcel? <input type="checkbox"/> Y <input checked="" type="checkbox"/> N	<input type="checkbox"/> If known, previous DSL # _____
Does LWI, if any, show wetland on parcel? <input type="checkbox"/> Y <input checked="" type="checkbox"/> N	<input type="checkbox"/> LWI wetland code: _____

**For Office Use Only**

DSL Reviewer: _____	Report Tier: <input type="checkbox"/> 1 <input type="checkbox"/> 2 <input type="checkbox"/> 3	DSL WD # _____
Date Delineation Received: ___ / ___ / ___	DSL Project # _____	DSL Site # _____
Scanned: <input type="checkbox"/> Final Scan: <input type="checkbox"/>	DSL WN # _____	DSL App. # _____

## **WETLAND DELINEATION SUMMARY**

**PROJECT:** Aelph Springs, Jewish Renewal Community, Wetland Delineation

**SUBJECT SITE:** Taxlots 1500, 1600 and 1800 on Taxlot Map No. 391E09AA

**ACREAGE OF STUDY AREA:** 2.57 Acres

**LOCATION:** 185 N. Mountain Ave., Ashland, Oregon 97520

**LAT/LONG:** 42° 11' 50.08"N / 122° 42' 3.35" W

**CLIENT/OWNER:** Havurah Shir Hadash, Jewish Renewal Community

**METHOD:** 1987 *Corps of Engineers Wetland Delineation Manual*

**STUDY PERIOD:** June 5-6, 2007

**ELEVATION:** Approximately 1,870 to 1,885 feet above mean sea level (from Jackson County Smartmap)

**TOPOGRAPHIC FEATURES:** Plane, sloping northeast

**WATERWAYS:** Mountain Creek and Drainage Ditch

**ESSENTIAL FISH HABITAT:** None

**WETLAND TYPES:** Palustrine Scrub-Shrub (PSS) and PEMCx Drainage Ditch

**WETLAND ACREAGE:** 0.11 PSS and 0.02 PEMCx Drainage Ditch

**PROPOSED USE:** Residential Housing Development

**PRESENT/PAST USE:** Jewish Renewal Community Center and Child Day Care

**ADJ. LAND-USE:** Residential, Public

FILED

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## **INTRODUCTION**

The following report was prepared at the request of the Havurah Shir Hadash, Jewish Renewal Community Center to assist them in the planning process of a residential housing development, called Aelph Springs. Construction of the residential development potentially affects state and federally jurisdictional wetlands, necessitating the completion of a wetland delineation report. The project site, located within the City of Ashland, comprises all of taxlots 1500, 1600, and 1800 on Jackson County Assessor's Map No. 391E09AA (Figure 3), for a total of 2.57 acres. Zoning for all taxlots within the project site is R-1-5 Single Family Residential.

## **PHYSICAL SETTING**

The project lies within a residential area in the southeast portion of Ashland, Oregon. A Union Pacific Railroad (UPR) railway adjoins the western boundaries of taxlots 1600 and 1800 (Figures 1 & 5).

The project site lies on an alluvial plane in the Rogue Valley that slopes perceptibly to the northeast toward Bear Creek. Mountain Creek enters the northwest corner of taxlot 1600 via two 24-inch culverts (Figure 5; Photo 2) and flows off-site to the north. Storm runoff and irrigation return water from upslope areas southwest of the project site flows through a ditch alongside the UPR railway and is piped under the RR tracks and enters taxlot 1800 via a single 24-inch culvert. A man-made ditch within the study area carries the water northwest where it converges with Mountain Creek (Figure 5; Photo 3 and 4). A palustrine scrub-shrub wetland (PSS) is located in the northwest corner of taxlot 1600 and is associated with Mountain Creek and the confluence with the man-made ditch. The drainage ditch is considered a potential PEMCx wetland due to the soils, vegetation, and hydrology found within the OHW zone. (Figure 5; Photos 1 and 2).

## **METHODS**

### **COLLECTION OF BACKGROUND INFORMATION**

Prior to conducting field surveys, a preliminary background investigation was undertaken to obtain the following information for the project site:

ENCLOSURE

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- Taxlot Information
- Regional Climate and Precipitation Information
- Wetlands of Local Significance
- Soils information
- Draft Geotechnical Investigation Report
- Site History
- Aerial Photographs
- Flood Zone Information

Taxlot information, Federal Emergency Management Agency (FEMA) flood zone designations, the National Wetlands Inventory and aerial photographs were acquired by accessing the Jackson County Geographic Information Services web page:

(<http://web.jacksoncounty.org/fca/index.cfm>) and downloading the appropriate GIS data.

Climate and Precipitation information for Jackson County was obtained from the National Oceanic and Atmospheric Administration (NOAA) National Weather Service Forecast Office website (<http://www.weather.gov/climate/index.php?wfo=mfr>) and compared to the USDA WETS tables

(<ftp://ftp.wcc.nrcs.usda.gov/support/climate/wetlands/or/41029.txt>) data for Ashland in order to determine percentage of normal rainfall during each site visit.

Detailed soils descriptions for the study area were obtained from the Soil Survey of Jackson County Area (Soil Conservation Service 1993).

Present and historic land use information was obtained during site visits.

## FIELD METHODS

Field data was collected on June 5 and 7 of 2007. Ten data plots were established to document indicators of wetland and upland conditions. For each data plot, the routine method was used to visually estimate percent vegetative cover for each plant species observed within a 5-foot radius for herbaceous groundcover and a 30-foot radius for trees and shrubs. Due to the occasionally small diameter or narrow width of certain features, such as the channels of ditches, the sampling radius was sometimes modified to avoid overlap into non-representative plant communities. All species were then ranked (based

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on spatial cover for each vegetative layer) and then the dominant species were selected and evaluated.

Soils and hydrology were evaluated at each of these points using a spade to examine the upper part (16 or more inches) of the soil profile whenever possible. Hydrology below the ordinary high water line of the ditch was determined by the observation of a single primary indicator (saturated soils within 12 inches of the soil surface). Due to less than normal rainfall for two weeks prior to the June 5 field survey (See Table 2), primary hydrologic indicators either were not present or were weakly present within the PSS wetland; however, secondary indicators (oxidized rhizospheres and local soil survey data) were observed and used to determine wetland hydrology. Ashland received 0.32 inches of rain between June 5 and 6, partially recharging the hydrology of the PSS wetland soils. Data Plots No. 5 and 6 were examined on June 7 and a single primary hydrologic indicator (saturation within 12 inches of the soil surface) was used to determine wetland hydrology. All field observations were recorded on wetland data sheets and are included in Appendix C.

All sample plot locations, watercourses and wetland boundaries were marked with colored pin flagging and then surveyed with a mapping grade GPS with real-time differential correction and sub-meter accuracy. The GPS data were then overlaid onto a 2001 Jackson County aerial photograph along with the boundaries of the project taxlots, and 5-ft elevational contours (Figure 5). The width of the lines representing Mountain Creek and the un-named ditch represent their ordinary high water line (OHW). Wetland acreages were calculated using the GPS data and GIS software. Accuracy of the mapped wetland boundaries and the alignments of Mountain Creek and the un-named ditch were estimated at <1 meter due to the accuracy of the differentially corrected GPS data (Figure 5).

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## CLIMATE AND PRECIPITATION

### Climate

The study area is situated in the southern part of the Bear Creek watershed in Ashland. This portion of the Bear Creek watershed is situated between the Coast Range, Klamath Mountains, and the Cascade Range in a subtle rain shadow. Summer months are generally warm and dry with infrequent rain and thunderstorms and occasional heat waves, while winter and spring months are generally cool and mild with moderate rainfall, occasional freezing temperatures, and infrequent snowfall.

### Precipitation

Table 1 compares precipitation recorded at the Ashland WETS Station to for the Water Year 2006 to 2007 to historical precipitation collected from 1971 to 2000. Average rainfall for the Ashland WETS Station is 19.75 inches. For the water year 2006-2007, the Ashland WETS Station recorded 17.52 inches of rainfall, which is 89 percent of the yearly average (Table 1).

	June 2006	July 2006	Aug 2006	Sept 2006	Oct 2006	Nov 2006	Dec 2006	Jan 2007	Feb 2007	Mar 2007	April 2007	May 2007
Observed	1.07	0.00	0.15	0.21	0.41	4.37	4.22	1.82	2.11	1.13	1.64	0.39
30-Year Average	0.92	0.51	0.61	0.88	1.46	2.85	2.80	2.48	1.92	2.09	1.77	1.55
Percent of Average	116	0	25	24	28	153	151	73	110	54	93	25

Table 2 compares precipitation data for two weeks prior to each field survey. As shown, field conditions for the first field visit on June 5 were somewhat drier than normal. The subsequent June 7 field visit was conducted under wetter conditions due a mild storm that brought 0.32 inches of rain between June 5 and June 7.

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Study Interval	Two Weeks Prior to Study Interval	Observed Precipitation	Average Precipitation (Period 1971 – 2000)	Percent of Average
June 5, 2007	May 22 to June 4, 2007	0.37	0.64	58
June 7, 2007	May 24 to June 6, 2007	0.5	0.62	81

## **SITE HISTORY AND ALTERATIONS**

The current alignment of Mountain Creek and the ditch that brings stormwater and irrigation runoff onto taxlot 1800 appear to be fairly old. The aerial photo (Figure 6) is dated 2001 and the June 2007 field surveys did not reveal any significant alterations since that time that would potentially affect site hydrology. For purposes of this wetland delineation report, environmental conditions are considered normal.

## **LOCAL AND NATIONAL WETLANDS INVENTORIES**

The City of Ashland is in the final phases of completing a local wetlands inventory (LWI). City of Ashland GIS Specialist Lea Light was contacted on June 7 regarding the LWI and stated that the current draft did not include any wetlands for the project site; however, Mountain Creek is included in the GIS dataset for the Jackson County Smartmaps (Figure 4).

The Jackson County Smartmap NWI data also did not show any wetlands for the project site.

## **HYDROLOGY**

### Palustrine Scrub-Shrub (PSS) Wetland (0.11 acres)

The PSS wetland primarily receives hydrology in the form of direct precipitation and to a lesser extent, surface flows from the un-named ditch and Mountain Creek. The wetland lies in a shallow depression and collects rainwater sufficient for the persistence of hydrophytic vegetation (Figure 5; Photo 1). The un-named ditch provides some hydrology during the irrigation season and rain events; however, this source is minimal,

since the landform slopes toward the northeast and the ditch only connects with the northeastern boundary of the wetland. During the late spring surveys, Mountain Creek did not appear to supply any hydrology to the PSS wetland due the low geographical position of its incised channel relative to the wetland. However, during heavy storm events, Mountain Creek does overflow into the wetland.

#### Mountain Creek

Mountain Creek has a vertically incised channel that averages 3 feet deep within the study area and has an ordinary high water line that is also 3 feet wide and 16 inches in depth (Figure 5; Photo 2). A call was placed to Brent Crowe of Oregon Department of Fish and Wildlife on June 18 regarding its fisheries status. Mr. Crowe stated that no recent surveys have been conducted within Mountain Creek and its fisheries status at this time is uncertain. Scott English is familiar with the drainage and adds that there are several passage barriers, including waterfalls and pipes that would preclude fish passage from Bear Creek into the project site.

#### Un-named Ditch (PEMCx wetland, 0.02 acres)

The un-named ditch carries stormwater runoff and irrigation return flows from upslope roads and residential areas southeast of the project site. This water is conveyed along the railroad right-of-way, and then passes under the UPR tracks and into taxlots 1600 and 1800, eventually joining Mountain Creek within Taxlot 1600 (Figure 5; Photo 4). The ditch has an unconsolidated bottom and the average width of the bed is 30 inches with OHW at 2-4 inches. The ditch appears to be constructed in upland soils and old fill, perhaps from the adjacent railroad. The bed was inundated to 3 inches on June 5 and saturated on June 7 with free water at the surface. The un-named ditch also connects on its west bank to the PSS wetland (Figure 5).

## **SOILS**

Two soils are mapped as occurring on the project site: 100A-Kubli loam, 0 to 3 percent slopes and 33A-Coker clay, 0 to 3 percent slopes (Figure 4). The following soil

descriptions were taken from the Soil Survey of Jackson County Area, Oregon [State Soil Scientist, Natural Resources Conservation Service (formerly Soil Conservation Service 1993)].

100A-Kubli loam, 0 to 3 percent slopes

*This very deep, somewhat poorly drained soil is on stream terraces. It formed in alluvium derived dominantly from granitic rock and underlain by clayey sediment.*

*The vegetation in areas that have not been cultivated is mainly grasses and forbs.*

*Typically, the surface layer is very dark brown loam about 9 inches thick. The next layer is very dark grayish brown loam about 6 inches thick. The subsoil is dark grayish brown loam about 16 inches thick. The upper 16 inches of the substratum is brown clay. The lower part to a depth of 60 inches is brown clay loam. Included in this unit are small areas of Medford soils, Barron and Central Point soils on the higher terraces and alluvial fans, and Gregory soils and poorly drained or very poorly drained, loamy soils near drainageways and on concave slopes. Also included are small areas of Kubli soils that have slopes of more than 3 percent. Included areas make up about 10 percent of the total acreage.*

*Permeability is moderate to a depth of 31 inches in the Kubli soil and very slow below that depth. Available water capacity is about 10 inches. The water table, which is perched above the layer of clay, is at a depth of 0.5 foot to 3.0 feet from November through April.*

33A-Coker clay, 0 to 3 percent slopes.

*This very deep, somewhat poorly drained soil is on alluvial fans. It formed in alluvium derived dominantly from tuff and breccia.*

*The native vegetation is mainly grasses, sedges, and forbs.*

*Typically, the surface layer is very dark gray clay about 20 inches thick. The next layer is very dark gray and dark grayish brown, calcareous clay about 26 inches thick. The subsoil to a depth of 70 inches is dark grayish brown, calcareous clay. The depth to bedrock is 60 inches or more. Included in this unit are small areas of Brader, Carney, Darow, and Debenger soils on convex slopes; Cove, Gregory, and Padigan soils on concave slopes near drainageways; Medford and Phoenix soils and soils that are similar to the Coker soil but have bedrock at a depth of 40 to 60 inches. Also included are small areas of Coker soils that have slopes of more than 3 percent. Included areas make up about 20 percent of the total acreage.*

*Permeability is very slow in the Coker soil. Available water capacity is about 9 inches. The effective rooting depth is limited by the water table, which is at a depth of 0.5 foot to 1.5 feet from December through April.*

## VEGETATION

### Ditch and Mountain Creek

Vegetation within the ditch (PEMCx) below the normal high water line was predominantly hydrophytic and included narrow-leaf cattail (*Typha angustifolia*; OBL) meadow foxtail (*Alopecurus pratensis*; FACW), buttercup (*Ranunculus repens*; FACW), and American speedwell (*Veronica americana*; OBL). Above the normal high water line, the vegetation was predominantly upland and included tall fescue (*Festuca arundinacea*; FAC-) and Himalayan blackberry (*Rubus discolor*; FACU). Brome grasses and prickly lettuce (*Lactuca serriola*; FACU-) were also present in lesser amounts.

Vegetation was virtually absent beneath the normal high water line of Mountain Creek owing to low light penetration and an incised channel. Above the normal high water line, an herbaceous layer was absent. Himalayan blackberry formed a dense shrub layer and weeping willow comprised the overstory.

### Palustrine Scrub-Shrub Wetland

The Palustrine Scrub-Shrub (PSS) Wetland had an overstory of weeping willow and a shrub layer of Himalayan blackberry in areas with sufficient light penetration. An herbaceous layer was typically absent; however, purple vetch was present at the margin.

### Uplands

Upland habitats comprised the majority of the site. Mediterranean barley (*Hordeum hystrix*; FACU) comprised the dominant vegetation in the southern part of the project site (Taxlot 1800) and tall fescue was dominant in the north (Taxlot 1500). Himalayan blackberry and purple vetch (*Vicia americana*; NI) were dominant in uplands of Taxlot 1800.

All ten data plots were taken in soils mapped as 33A-Coker clay, 0 to 3 percent slopes; and Kubli loam, 0 to 3% slopes.. Both soils are not considered hydric.

## RESULTS AND CONCLUSIONS

### Palustrine Scrub-Shrub Wetland

The Palustrine Scrub-Shrub (PSS) Wetland was delineated in the northwest corner of Taxlot 1600 and comprised 0.11 acres (Figure 5). This wetland extends offsite to the north and several feet west. The west side of the wetland is defined by the vertical channel of Mountain Creek. Lands west of Mountain Creek are overwhelmingly dominated by Himalyan blackberry. Weeping willows define the dominant wetland vegetation while Himalayan blackberry and tall fescue define the vegetation of the surrounding uplands (Figure 5).

### Ditch (PEMCx) Palustrine Emergent seasonally Flooded, Excavated

The un-named ditch, below OHW contained hydric soils, dominant hydrophytic vegetation, and wetland hydrology and initially meets the criteria as a PEMCx wetland. The railway ditch was designed and constructed to carry stormwater from upslope roads and housing infrastructure and irrigation return flows from Talent Irrigation District (TID) water managed by the City of Ashland. No fish are present in the 30 inch wide ditch. The ditch empties into Mountain Creek (Figure 5).

### Mountain Creek

Mountain Creek is a perennial watercourse within a deep (3-5 foot) incised channel and a sandy bottom. The average width of the channel is 3 feet and the ordinary high water line is also 3 feet at 16 inches above the bed of the channel. Mountain Creek is a state and federally jurisdictional watercourse (Figure 5).

## LIMITATIONS

This report is not a jurisdictional document until reviewed and confirmed by the DSL and/or the Corps (the Corps is currently only reviewing wetland delineations when accompanied by a permit application to fill a wetland). This report documents the investigation, best professional judgment, and conclusions of the investigators. It should be considered a preliminary Jurisdictional Determination and “used at your own risk” until it has been reviewed and approved by the DSL in accordance with Oregon Administrative Rules (OAR) 141-090-0005 through 141-090-0055.

## LITERATURE CITED

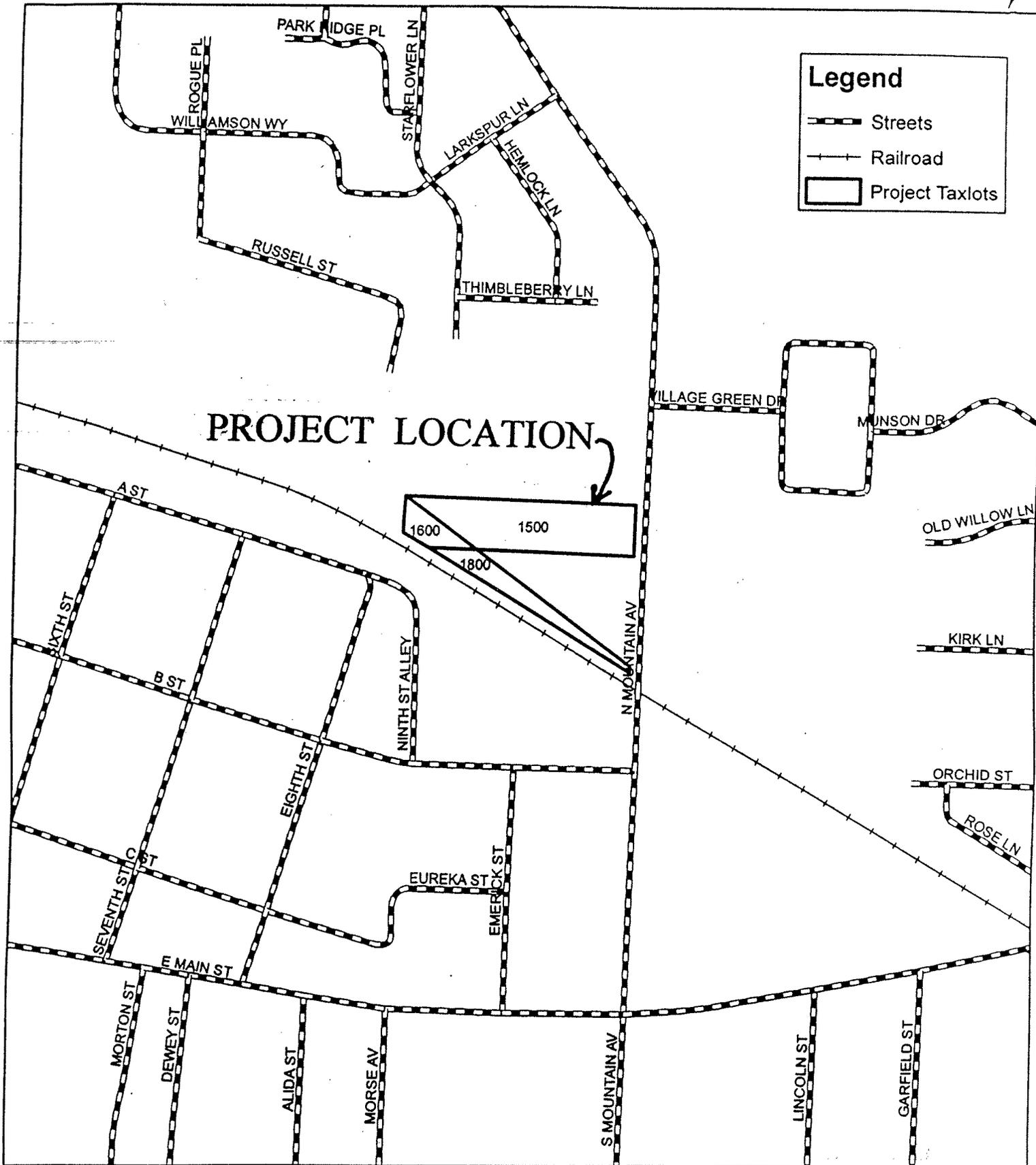
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**APPENDIX A**  
**FIGURES**

SEP 18

# Figure 1: Location Havurah Shir Hadash Wetland Delineation

1:4,200 1 inch = 350 feet

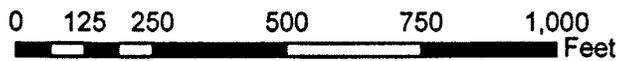


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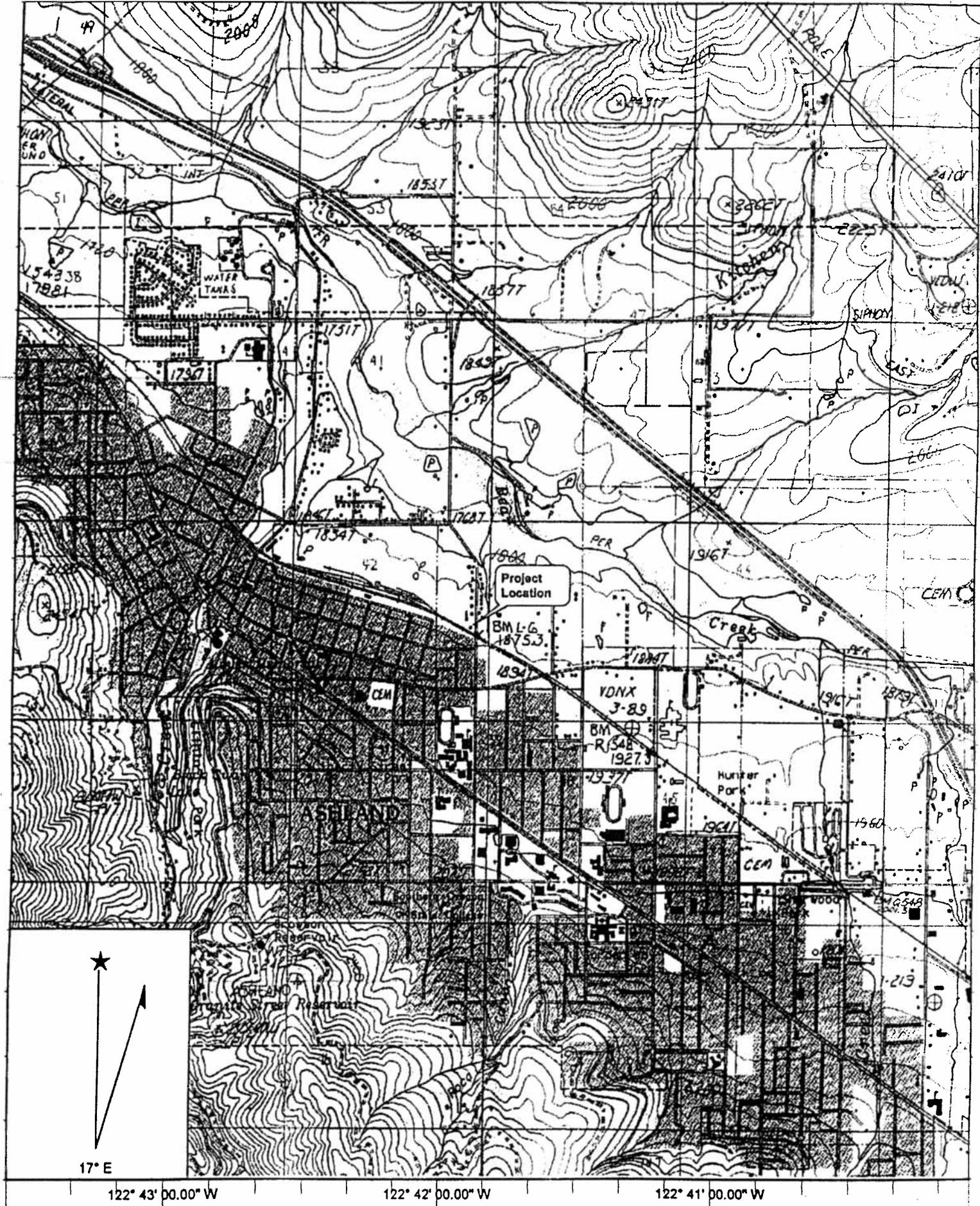
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Name: ASHLAND  
 Date: 6/5/2007  
 Scale: 1 inch equals 2000 feet

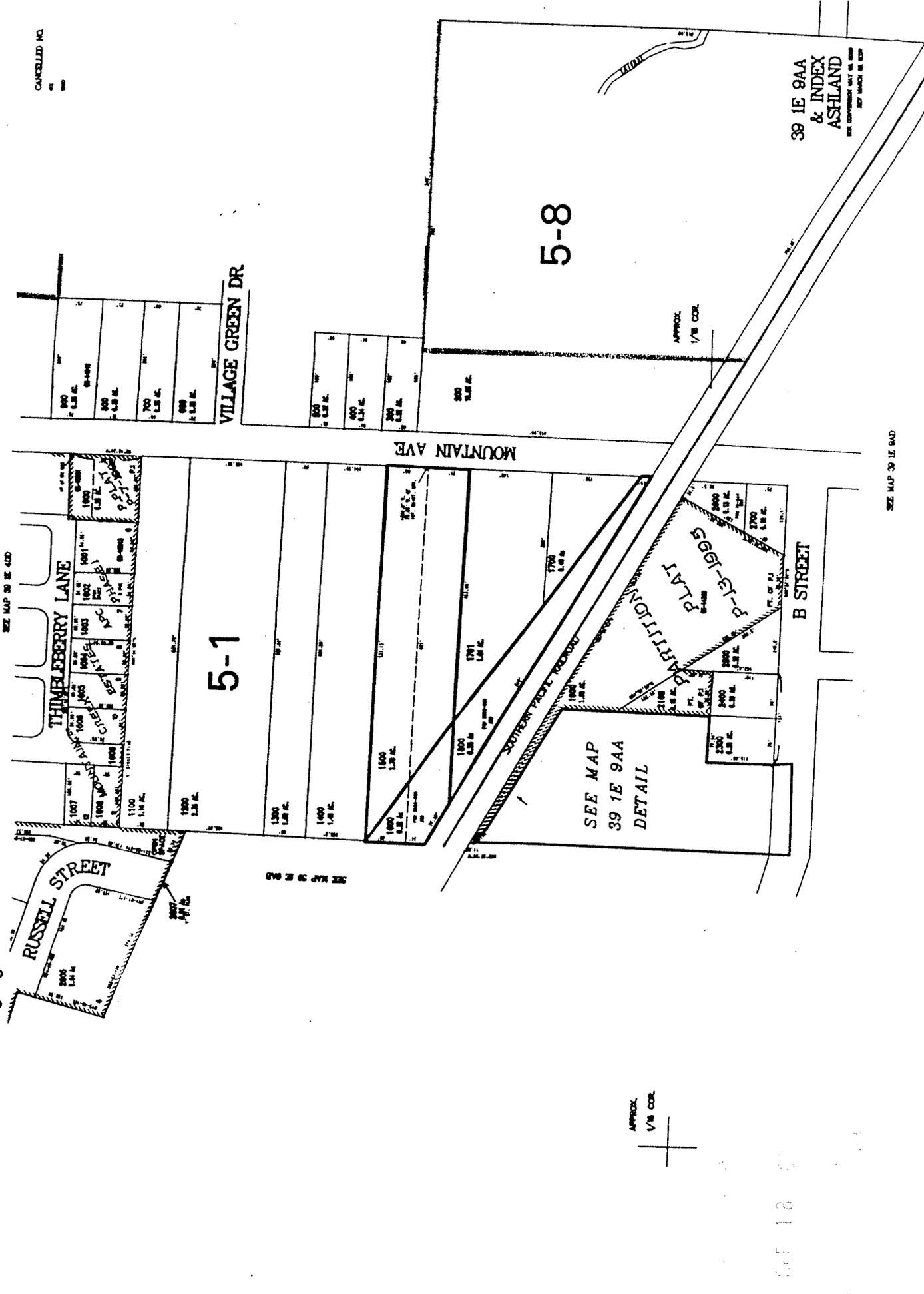
Figure 2

Location: 042° 11' 55.20" N 122° 41' 48.99" W  
 Caption: Figure 2. U.S.G.S. 7.5-Minute Quadrangle Map, Project Location

NE 1/4 NE 1/4 SEC. 9 T38S R1E WM  
 JACKSON COUNTY

39 1E 9AA  
 & INDEX  
 ASHLAND

Figure 3: Taxlot Map from Jackson County  
 Taxlots 1500, 1600 and 1800 highlighted



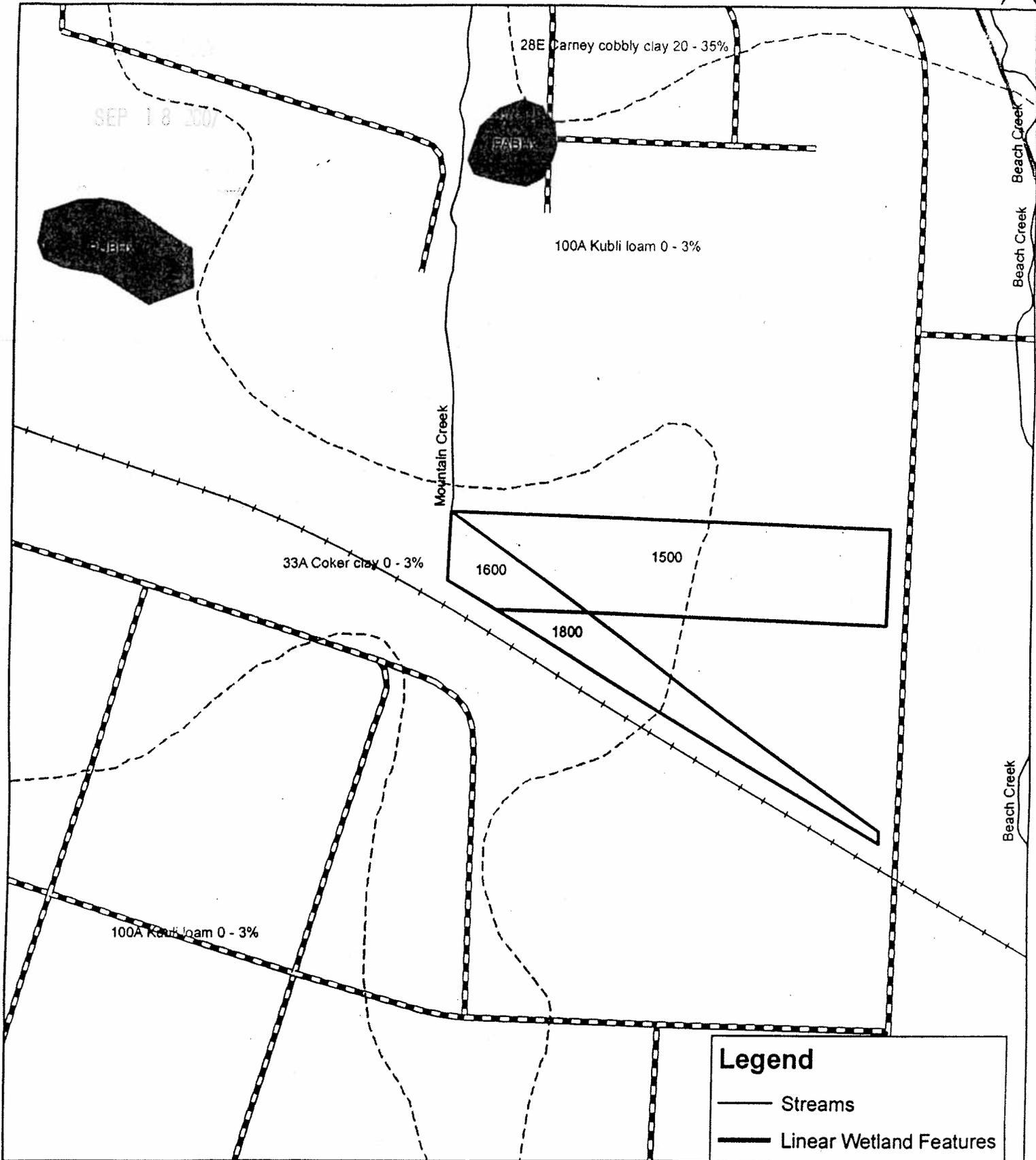
SEE MAP 39 1E 9AA

APPROVAL  
 1/8 COR.

SEE MAP 39 1E 9AA

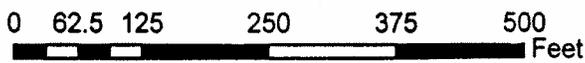
# Figure 4: NWI & Soils Havurah Shir Hadash Wetland Delineation

1:2,220 1 inch = 185 feet



**Legend**

- Streams
- Linear Wetland Features
- Wetland Features
- - - Soils
- + + + Railroad
- · - · Streets



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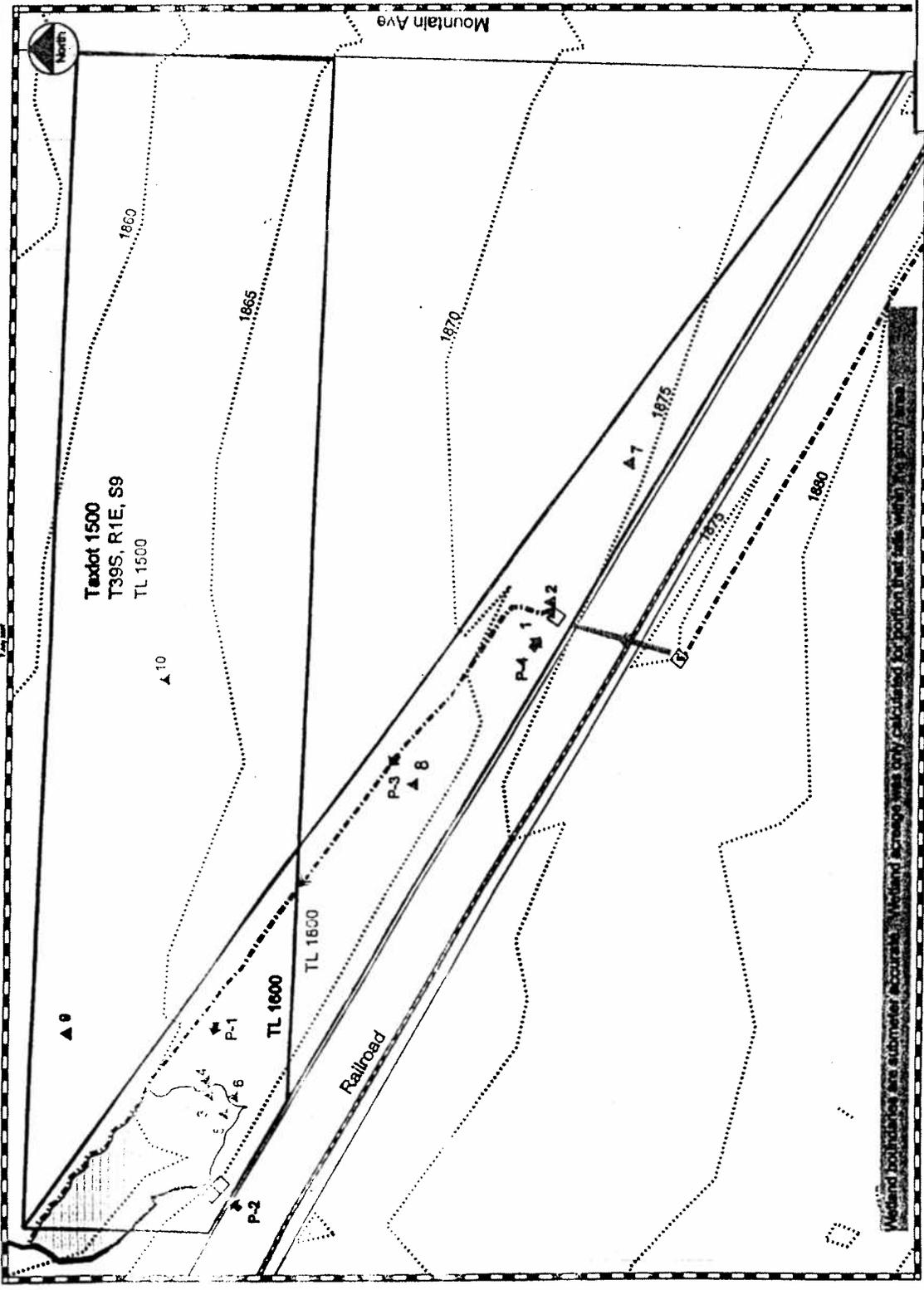
SEP 18 2007

Figure 6

Wetland Delineation  
Hevurah Shur Hadash  
Jewish Renewal Community  
Ashland, Oregon

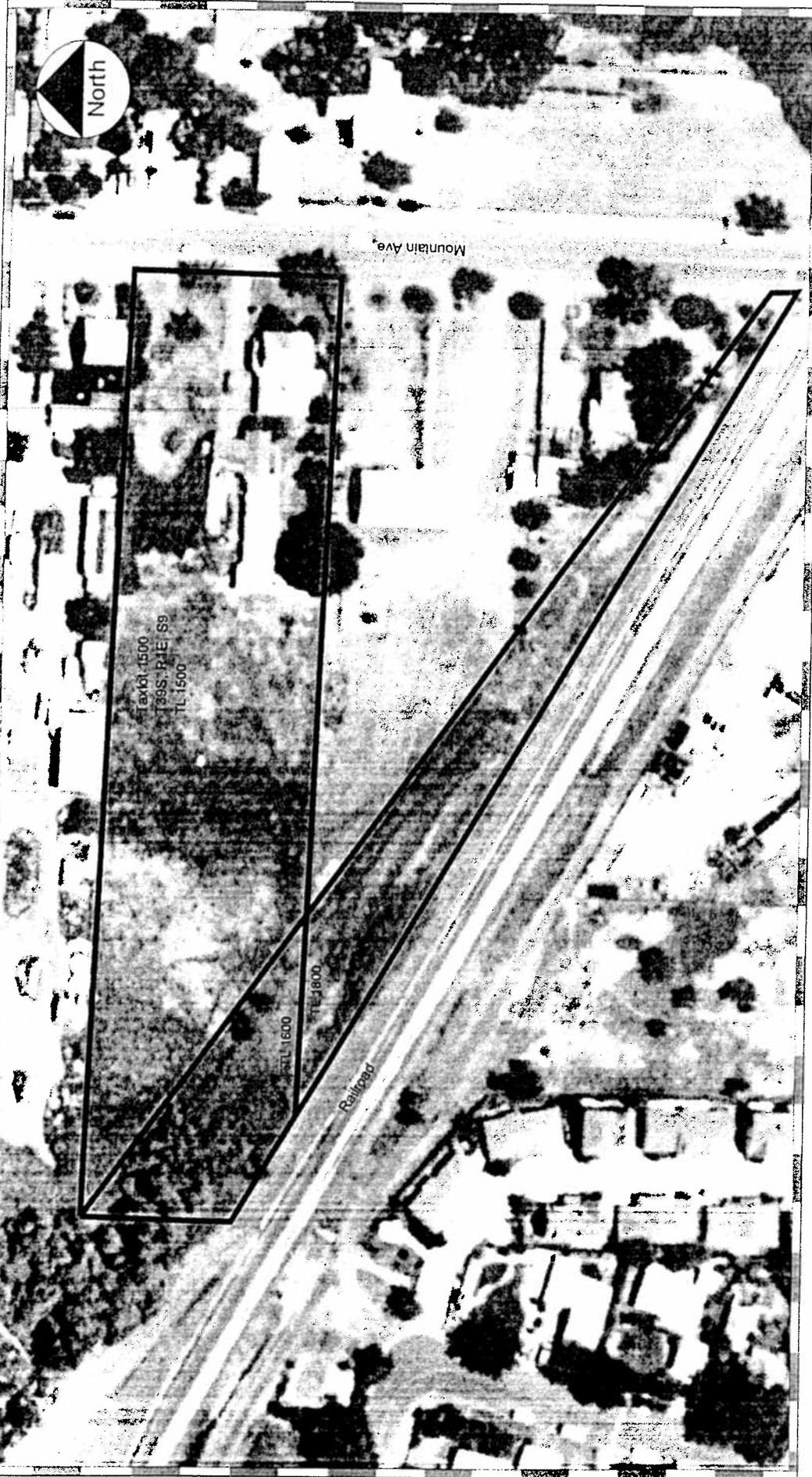
- LEGEND**
- Study Area Boundary
  - PMS Wetland 0.11 Acres
  - Mountain Creek OHW
  - Ditch PEMC, 0.02 Acres
  - Below Ground Location of Culvert
  - 3:1 Contour
  - Railroad
  - Pole Point
  - Data Plot
  - Flow Direction
  - 24-inch Culvert

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2 July 2007



LEGEND



Study Area

Figure 6

Aerial Photo

Havurah Wetland Delineation

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2007-10-10

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**APPENDIX B  
SITE PHOTOGRAPHS**



Photo 1. PSS Wetland Facing West



Photo 2. Mountain Creek facing northwest.

1000-1000-00

SEP 18 AMV

1000



Data Plots 1 and 2. Culvert is in lower right-hand corner.



Photo 4. Alignment of ditch facing northwest.

**APPENDIX C**  
**WETLAND DATA SHEETS**

307 10 10

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: JACKSON City: ASHLAND Date: 7-7-07 File # 8  
 Project/Contact: HAVIRAH Det. by: Thompson/English  
 Plant Community: \_\_\_\_\_ Plot # 10  
 Plot location: SEE REPORT  
 Recent Weather: CLEAR  
 Do normal environ. conditions exist? Y  N  If No, explain: \_\_\_\_\_  
 Has Vegetation Soil Hydrology been significantly disturbed? No  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: 130 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. Lolium perenne FACU 60  
 2. Agropyron sp (wheat) NOL 20  
 3. Poa pratensis FACU+ 20  
 4. Bromus sp (unknown) 10  
 5. \_\_\_\_\_  
 6. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

Map Unit Name: Coker Clay 0-3% Slopes SOILS Drainage Class: Somewhat Poorly Drained  
 On Hydric Soils List? Y  N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions	Texture
<u>0-14"</u>	<u>10 yr 3/4</u>	<u>none</u>	<u>none</u>	<u>Clay loam</u>
<u>REFUSAL @ 14"</u>				
<u>MORE CLOSELY RESEMBLES KUBLI</u>				

Hydric Soil Indicators:  
 Histosol  
 Histic Epipedon  
 Sulfidic Odor  
 Reducing Conditions (tests positive)  
 Gleyed or low chroma colors  
 Redox features within 10" (e.g., concentrations)  
 Concretions/Nodules (w/in 3"; > 2mm)  
 High organic content in surface (in Sandy Soils)  
 Organic streaking (in Sandy Soils)  
 Organic pan (in Sandy Soils)  
 Listed on Hydric Soils List (and soil profile matches)  
 Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)  
 Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO

HYDROLOGY

Recorded Data  
 Recorded Data Available  Aerial Photos  Stream gauge  Other  No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: >14" Depth to free water: \_\_\_\_\_

Primary Hydrology Indicators:  
 Inundated  
 Saturated in upper 12 inches  
 Water Marks  
 Drift Lines  
 Sediment Deposits  
 Drainage Patterns  
 Secondary Hydrology Indicators (2 or more required):  
 Oxidized Root Channels (upper 12")  
 Water-stained Leaves  
 Local Soil Survey Data  
 FAC-Neutral Test  
 Other: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

SEP 18 2007

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: JACKSON City: ASHLAND Date: 7-7-07 File # \_\_\_\_\_  
 Project/Contact: HAYURAH Det. by: \_\_\_\_\_  
 Plant Community: GRASSLAND Plot # 9 Thompson/English  
 Plot location: FEE REPORT  
 Recent Weather: CLEAR  
 Do normal environ. conditions exist? Y X N \_\_\_\_\_ If No, explain: \_\_\_\_\_  
 Has Vegetation \_\_\_\_\_ Soil \_\_\_\_\_ Hydrology \_\_\_\_\_ been significantly disturbed? No  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. Festuca arundinacea FAC 100  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

4. \_\_\_\_\_  
 5. \_\_\_\_\_  
 6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO X Comments: \_\_\_\_\_

Map Unit Name: Coker Clay 0-3% Slopes <sup>SOILS</sup> Drainage Class: Somewhat Poorly Drained  
 On Hydric Soils List? Y X N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions	Texture
<u>0-18"</u>	<u>10YR 3/1</u>	<u>none</u>	<u>none</u>	<u>clay</u>

Hydric Soil Indicators:

- \_\_\_ Histosol
- \_\_\_ Histic Epipedon
- \_\_\_ Sulfidic Odor
- \_\_\_ Reducing Conditions (tests positive)
- X Gleyed or low chroma colors
- \_\_\_ Redox features within 10" (e.g., concentrations)

- \_\_\_ Concretions/Nodules (w/in 3"; > 2mm)
- \_\_\_ High organic content in surface (in Sandy Soils)
- \_\_\_ Organic streaking (in Sandy Soils)
- \_\_\_ Organic pan (in Sandy Soils)
- \_\_\_ Listed on Hydric Soils List (and soil profile matches)
- \_\_\_ Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)
- \_\_\_ Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_

HYDROLOGY

Recorded Data  
 \_\_\_ Recorded Data Available X Aerial Photos \_\_\_ Stream gauge \_\_\_ Other \_\_\_ No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: >18" Depth to free water: \_\_\_\_\_

- Primary Hydrology Indicators:
- \_\_\_ Inundated
  - \_\_\_ Saturated in upper 12 inches
  - \_\_\_ Water Marks
  - \_\_\_ Drift Lines
  - \_\_\_ Sediment Deposits
  - \_\_\_ Drainage Patterns

- Secondary Hydrology Indicators (2 or more required):
- \_\_\_ Oxidized Root Channels (upper 12")
  - \_\_\_ Water-stained Leaves
  - \_\_\_ Local Soil Survey Data
  - \_\_\_ FAC-Neutral Test
  - \_\_\_ Other: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO X Comments: Slightly moist

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO X Comments: \_\_\_\_\_

SEP 18 2007

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Jackson City: Ashland Date: 7-7-07 File # 8  
 Project/Contact: UPR FALGS HAWKMAE Det. by: Thompson (English)  
 Plant Community: SEE REPORT Plot # 8  
 Plot location: SEE REPORT  
 Recent Weather: Clear  
 Do normal environ. conditions exist? Y  N  If No, explain: \_\_\_\_\_  
 Has Vegetation  Soil  Hydrology  been significantly disturbed? NO  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover

1. Tubus discolor FACV 20
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_
5. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: 120 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. Lolium perenne FACV 40
2. Agropyron sp. (Wheat) NBL 10
3. Bromus sp (FACV EST) 40
4. Festuca arundinacea FAC- 10
5. Vicia americana NIE 20
6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

SOILS

Map Unit Name: 33A Coker Clay Drainage Class: \_\_\_\_\_

On Hydric Soils List? Y  N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-18"</u>	<u>10YR 3/2</u>	<u>none</u>	<u>none</u>	<u>Sandy Loam</u>

Hydric Soil Indicators:

- |   |  |
|---|--|
| <input type="checkbox"/> Histosol   | <input type="checkbox"/> Concretions/Nodules (w/in 3"; > 2mm)                                    |
| <input type="checkbox"/> Histic Epipedon                                  | <input type="checkbox"/> High organic content in surface (in Sandy Soils)                        |
| <input type="checkbox"/> Sulfidic Odor                                    | <input type="checkbox"/> Organic streaking (in Sandy Soils)                                      |
| <input type="checkbox"/> Reducing Conditions (tests positive)             | <input type="checkbox"/> Organic pan (in Sandy Soils)  |
| <input type="checkbox"/> Gleyed or low chroma colors                      | <input type="checkbox"/> Listed on Hydric Soils List (and soil profile matches)                  |
| <input type="checkbox"/> Redox features within 10" (e.g., concentrations) | <input type="checkbox"/> Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration) |
|   | <input type="checkbox"/> Supplemental indicator (e.g., NRCS field indicator): _____              |

Criteria Met? YES \_\_\_\_\_ NO

HYDROLOGY

Recorded Data  
 Recorded Data Available  Aerial Photos \_\_\_\_\_ Stream gauge \_\_\_\_\_ Other \_\_\_\_\_ No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: >18" Depth to free water: \_\_\_\_\_

- |   |   |
|---|---|
| <u>Primary Hydrology Indicators:</u>                  | <u>Secondary Hydrology Indicators (2 or more required):</u> |
| <input type="checkbox"/> Inundated                    | <input type="checkbox"/> Oxidized Root Channels (upper 12") |
| <input type="checkbox"/> Saturated in upper 12 inches | <input type="checkbox"/> Water-stained Leaves               |
| <input type="checkbox"/> Water Marks                  | <input type="checkbox"/> Local Soil Survey Data             |
| <input type="checkbox"/> Drift Lines                  | <input type="checkbox"/> FAC-Neutral Test                   |
| <input type="checkbox"/> Sediment Deposits            | <input type="checkbox"/> Other: _____                       |
| <input type="checkbox"/> Drainage Patterns            |   |

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: JACKSON City: ASHLAND Date: 7-7-07 File # \_\_\_\_\_  
 Project/Contact: \_\_\_\_\_ Det. by: Thompson - English  
 Plant Community: UPL VEG. Plot # 7  
 Plot location: \_\_\_\_\_  
 Recent Weather: CLEAR  
 Do normal environ. conditions exist? Y  N \_\_\_\_\_ If No, explain: \_\_\_\_\_  
 Has Vegetation \_\_\_\_\_ Soil \_\_\_\_\_ Hydrology \_\_\_\_\_ been significantly disturbed?  
 Explain: NO

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: 40 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. Festuca arundinacea FAC- 10  
 2. Bromus sp. FAC(PS) 20  
 3. Cichorium intybus NOL 5  
 4. Daucus carota NOL 5  
 5. Vicia americana NI 50  
 6. Agropyron sp. (unknown) 10

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_  
 Criteria Met? YES \_\_\_\_\_ NO  Comments: Only 40% are indicator sp.

SOILS

Map Unit Name: Kubli Loam Drainage Class: \_\_\_\_\_  
 On Hydric Soils List? Y \_\_\_\_\_ N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions	Texture
<u>0-10"</u>	<u>10YR 3/2</u>	<u>none</u>	<u>none</u>	<u>Sandy loam + rocks</u>
<u>Refusal @ 10"</u>				
<u>APPEARS TO BE FILL MATERIAL FROM RAIL ROAD</u>				

Hydric Soil Indicators:  
 Histosol  
 Histic Epipedon  
 Sulfidic Odor  
 Reducing Conditions (tests positive)  
 Gleyed or low chroma colors  
 Redox features within 10" (e.g., concentrations)  
 Concretions/Nodules (w/in 3": > 2mm)  
 High organic content in surface (in Sandy Soils)  
 Organic streaking (in Sandy Soils)  
 Organic pan (in Sandy Soils)  
 Listed on Hydric Soils List (and soil profile matches)  
 Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)  
 Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO

HYDROLOGY

Recorded Data  
 Recorded Data Available  Aerial Photos  Stream gauge  Other  No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: > 10" Depth to free water: \_\_\_\_\_

Primary Hydrology Indicators:  
 Inundated  
 Saturated in upper 12 inches  
 Water Marks  
 Drift Lines  
 Sediment Deposits  
 Drainage Patterns  
 Secondary Hydrology Indicators (2 or more required):  
 Oxidized Root Channels (upper 12")  
 Water-stained Leaves  
 Local Soil Survey Data  
 FAC-Neutral Test  
 Other: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO  Comments: UPLAND FILL.



DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Tackson City: Ashtland Date: 6-7-07 File # \_\_\_\_\_  
 Project/Contact: HAWKRAH Det. by: Thompson  
 Plant Community: SCARP STRAW Plot # 5  
 Plot location: SEE REPORT  
 Recent Weather: RAIU  
 Do normal environ. conditions exist? Y X N \_\_\_\_\_ If No, explain: \_\_\_\_\_  
 Has Vegetation \_\_\_\_\_ Soil \_\_\_\_\_ Hydrology \_\_\_\_\_ been significantly disturbed? NO  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. Salix babylonica FR 100  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. 0  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover  
 1. Rubus discolor FACU 40  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

4. \_\_\_\_\_  
 5. \_\_\_\_\_  
 6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 50  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_ Comments: \_\_\_\_\_

33A Coter Clay 0-3% Slopes SOILS Drainage Class: Somewhat Poorly Drained

On Hydric Soils List? Y X N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-16"</u>	<u>10YR 2/1</u>	<u>none</u>	<u>none</u>	<u>Sandy Clay loam</u>

Hydric Soil Indicators:

- |   |  |
|---|--|
| <input type="checkbox"/> Histosol   | <input type="checkbox"/> Concretions/Nodules (w/in 3"; > 2mm)                                    |
| <input type="checkbox"/> Histic Epipedon                                  | <input type="checkbox"/> High organic content in surface (in Sandy Soils)                        |
| <input type="checkbox"/> Sulfidic Odor                                    | <input type="checkbox"/> Organic streaking (in Sandy Soils)                                      |
| <input type="checkbox"/> Reducing Conditions (tests positive)             | <input type="checkbox"/> Organic pan (in Sandy Soils)  |
| <input checked="" type="checkbox"/> Gleyed or low chroma colors           | <input type="checkbox"/> Listed on Hydric Soils List (and soil profile matches)                  |
| <input type="checkbox"/> Redox features within 10" (e.g., concentrations) | <input type="checkbox"/> Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration) |
|   | <input type="checkbox"/> Supplemental indicator (e.g., NRCS field indicator): _____              |

Criteria Met? YES X NO \_\_\_\_\_

HYDROLOGY

Recorded Data

Recorded Data Available  Aerial Photos \_\_\_\_\_ Stream gauge \_\_\_\_\_ Other \_\_\_\_\_ No Recorded Data Available

Field Data

Depth of inundation: \_\_\_\_\_ Depth to Saturation: Surface Depth to free water: >16"

Primary Hydrology Indicators:

- Inundated  
 Saturated in upper 12 inches  
 Water Marks  
 Drift Lines  
 Sediment Deposits  
 Drainage Patterns

Secondary Hydrology Indicators (2 or more required):

- Oxidized Root Channels (upper 12")  
 Water-stained Leaves  
 Local Soil Survey Data  
 FAC-Neutral Test  
 Other: \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_ Comments: \_\_\_\_\_

DETERMINATION

WETLAND? YES X NO \_\_\_\_\_ Comments: \_\_\_\_\_

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Jackson City: Ashtland Date: 6-5-07 File # 7-8  
 Project/Contact: Hawuach Det. by: Thompson  
 Plant Community: Grassland Plot # 4  
 Plot location: See Report  
 Recent Weather: Rain  
 Do normal environ. conditions exist? Y X N If No, explain: \_\_\_\_\_  
 Has Vegetation Soil Hydrology been significantly disturbed? No  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_
5. \_\_\_\_\_

Herb Stratum 90  
 Total Plot Cover: 100 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. Festuca arundinaceae FAC- 90
2. Vicia americana NE 10

3. \_\_\_\_\_
4. \_\_\_\_\_
5. \_\_\_\_\_
6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO X Comments: \_\_\_\_\_

33A Map Unit Name: Coker clay 0-3% Slopes SOILS Drainage Class: Somewhat Poorly Drained

On Hydric Soils List? Y ✓ N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-16"</u>	<u>10YR 7/1</u>	<u>com/sm/5YR 5/8/Matrix</u>		<u>Silty Clay Loam</u>

- Hydric Soil Indicators:
- Histosol
  - Histic Epipedon
  - Sulfidic Odor
  - Reducing Conditions (tests positive)
  - Gleyed or low chroma colors
  - Redox features within 10" (e.g., concentrations)
  - Concretions/Nodules (w/in 3"; > 2mm)
  - High organic content in surface (in Sandy Soils)
  - Organic streaking (in Sandy Soils)
  - Organic pan (in Sandy Soils)
  - Listed on Hydric Soils List (and soil profile matches)
  - Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)
  - Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_

HYDROLOGY

Recorded Data  
 Recorded Data Available  Aerial Photos  Stream gauge  Other  No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: > 16" Depth to free water: \_\_\_\_\_

- Primary Hydrology Indicators:
- Inundated
  - Saturated in upper 12 inches
  - Water Marks
  - Drift Lines
  - Sediment Deposits
  - Drainage Patterns
- Secondary Hydrology Indicators (2 or more required):
- Oxidized Root Channels (upper 12")
  - Water-stained Leaves
  - Local Soil Survey Data
  - FAC-Neutral Test
  - Other: \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_ Comments: DRY THROUGHOUT PROFILE

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO X Comments: DOES NOT MEET VEG. REQUIREMENT

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Jackson City: Ashland Date: 6-5-07 File # 17-8  
 Project/Contact: Havurah Det. by: Thompson  
 Plant Community: Scrub-shrub Plot # 3  
 Plot location: \_\_\_\_\_  
 Recent Weather: \_\_\_\_\_  
 Do normal environ. conditions exist? Y \_\_\_ N \_\_\_ If No, explain: \_\_\_\_\_  
 Has Vegetation \_\_\_ Soil \_\_\_ Hydrology \_\_\_ been significantly disturbed?  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum  
 Total Plot Cover: 100 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. Salix 100  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: 0 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover  
 1. \_\_\_\_\_  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: 50 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover  
 1. Rubus discolor FACU 50  
 2. \_\_\_\_\_  
 3. \_\_\_\_\_  
 4. \_\_\_\_\_  
 5. \_\_\_\_\_

4. \_\_\_\_\_  
 5. \_\_\_\_\_  
 6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 50  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_ Comments: \_\_\_\_\_

33A SOILS  
 Map Unit Name: Coker clay 0-3% Slopes Drainage Class: Somewhat Poorly Drained

On Hydric Soils List? Y X N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-10"</u>	<u>10YR 2/1</u>	<u>com/med 15YR 4/6 Matrix</u>		<u>Sandy clay loam</u>
<u>10-16"</u>	<u>10YR 2/1</u>	<u>few faint</u>		<u>Sandy clay loam</u>

Hydric Soil Indicators:

- Histosol
- Histic Epipedon
- Sulfidic Odor
- Reducing Conditions (tests positive)
- Gleyed or low chroma colors
- Redox features within 10" (e.g., concentrations)
- Concretions/Nodules (w/in 3"; > 2mm)
- High organic content in surface (in Sandy Soils)
- Organic streaking (in Sandy Soils)
- Organic pan (in Sandy Soils)
- Listed on Hydric Soils List (and soil profile matches)
- Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)
- Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES X NO \_\_\_\_\_

HYDROLOGY

Recorded Data

Recorded Data Available  Aerial Photos \_\_\_\_\_ Stream gauge \_\_\_\_\_ Other \_\_\_\_\_ No Recorded Data Available \_\_\_\_\_

Field Data

Depth of inundation: \_\_\_\_\_ Depth to Saturation: Surface Depth to free water: \_\_\_\_\_

Primary Hydrology Indicators:

- Inundated
- Saturated in upper 12 inches
- Water Marks
- Drift Lines
- Sediment Deposits
- Drainage Patterns

Secondary Hydrology Indicators (2 or more required):

- Oxidized Root Channels (upper 12")
- Water-stained Leaves
- Local Soil Survey Data
- FAC-Neutral Test
- Other: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO \_\_\_\_\_ Comments: Barely Saturated - Recent Rain

DETERMINATION

WETLAND? YES X NO \_\_\_\_\_ Comments: \_\_\_\_\_

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Jackson City: Ashland Date: 6-5-07 File # 7-8  
 Project/Contact: Havural Det. by: Thompson  
 Plant Community: Rudrals / Grassland Plot # 2  
 Plot location: See Report  
 Recent Weather: Rain  
 Do normal environ. conditions exist? Y  N  If No, explain: \_\_\_\_\_  
 Has Vegetation  Soil  Hydrology  been significantly disturbed? No  
 Explain: \_\_\_\_\_

VEGETATION

Tree Stratum Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_

Herb Stratum Total Plot Cover: 60 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. Vicia americana NE 40
2. Festuca arundinacea FAC- 60
3. Lactuca scariola FACU T
4. \_\_\_\_\_
5. \_\_\_\_\_
6. \_\_\_\_\_

Sapling/Shrub Stratum Total Plot Cover: \_\_\_\_\_ 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_
5. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 0  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES \_\_\_\_\_ NO  Comments: \_\_\_\_\_

1004 Map Unit Name: Kubli Loam 0-3% slopes SOILS Drainage Class: Somewhat Poorly Drained

On Hydric Soils List? Y  N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-6"</u>	<u>10YR 3/2-3/1</u>	<u>none</u>	<u>none</u>	<u>Loam</u>

Root Refusal @ 6"

Hydric Soil Indicators:

- |   |  |
|---|--|
| <input type="checkbox"/> Histosol   | <input type="checkbox"/> Concretions/Nodules (w/in 3"; > 2mm)                                    |
| <input type="checkbox"/> Histic Epipedon                                  | <input type="checkbox"/> High organic content in surface (in Sandy Soils)                        |
| <input type="checkbox"/> Sulfidic Odor                                    | <input type="checkbox"/> Organic streaking (in Sandy Soils)                                      |
| <input type="checkbox"/> Reducing Conditions (tests positive)             | <input type="checkbox"/> Organic pan (in Sandy Soils)  |
| <input type="checkbox"/> Gleyed or low chroma colors                      | <input type="checkbox"/> Listed on Hydric Soils List (and soil profile matches)                  |
| <input type="checkbox"/> Redox features within 10" (e.g., concentrations) | <input type="checkbox"/> Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration) |
|   | <input type="checkbox"/> Supplemental indicator (e.g., NRCS field indicator): _____              |

Criteria Met? YES \_\_\_\_\_ NO

HYDROLOGY

Recorded Data  
 Recorded Data Available  Aerial Photos  Stream gauge  Other  No Recorded Data Available

Field Data  
 Depth of inundation: \_\_\_\_\_ Depth to Saturation: > 6" Depth to free water: \_\_\_\_\_

- |   |   |
|---|---|
| <input type="checkbox"/> Inundated                    | <input type="checkbox"/> Oxidized Root Channels (upper 12") |
| <input type="checkbox"/> Saturated in upper 12 inches | <input type="checkbox"/> Water-stained Leaves               |
| <input type="checkbox"/> Water Marks                  | <input type="checkbox"/> Local Soil Survey Data             |
| <input type="checkbox"/> Drift Lines                  | <input type="checkbox"/> FAC-Neutral Test                   |
| <input type="checkbox"/> Sediment Deposits            | <input type="checkbox"/> Other: _____                       |
| <input type="checkbox"/> Drainage Patterns            |   |

Criteria Met? YES \_\_\_\_\_ NO  Comments: Dry

DETERMINATION

WETLAND? YES \_\_\_\_\_ NO  Comments: Outside of ditch. (See Report Figures)

DEPARTMENT OF STATE LANDS WETLAND DETERMINATION DATA FORM—Quick Method

County: Jackson City: Ashland Date: 6-5-07 File # 7-8  
 Project/Contact: Havurah Det. by: Thompson  
 Plant Community: \_\_\_\_\_ Plot # 1  
 Plot location: See Report  
 Recent Weather: Rain  
 Do normal environ. conditions exist? Y  N \_\_\_\_\_ If No, explain: \_\_\_\_\_  
 Has Vegetation \_\_\_\_\_ Soil \_\_\_\_\_ Hydrology \_\_\_\_\_ been significantly disturbed?  
 Explain: NO

VEGETATION

Tree Stratum  
 Total Plot Cover: 0 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_

Herb Stratum  
 Total Plot Cover: 0 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status / Raw % Cover

1. BARE GROUND 100%
2. \_\_\_\_\_
3. \_\_\_\_\_

Sapling/Shrub Stratum  
 Total Plot Cover: 5 50% \_\_\_\_\_ 20% \_\_\_\_\_  
 Status/Raw % Cover

1. Fraxinus latifolia FACW 5
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_
5. \_\_\_\_\_

4. \_\_\_\_\_
5. \_\_\_\_\_
6. \_\_\_\_\_

Percent of Dominant Species that are OBL, FACW, FAC (not FAC-): 100  
 Other Hydrophytic Vegetation Indicators: \_\_\_\_\_

Criteria Met? YES  NO \_\_\_\_\_ Comments: \_\_\_\_\_

100A Map Unit Name: Kubli Loam, 0-3% Slopes SOILS Drainage Class: Somewhat Poorly Drained

On Hydric Soils List? Y  N \_\_\_\_\_ Has hydric inclusions? Y \_\_\_\_\_ N \_\_\_\_\_

Depth Range of Horizon	Matrix Color	Redox Concentrations* * abund./size/contrast/color/location (matrix or pores/peds)	Redox Depletions*	Texture
<u>0-16"</u>	<u>10YR 2/1</u>	<u>none</u>	<u>none</u>	<u>Silt</u>

Hydric Soil Indicators:

- Histosol
- Histic Epipedon
- Sulfidic Odor
- Reducing Conditions (tests positive)
- Gleyed or low chroma colors
- Redox features within 10" (e.g., concentrations)

- Concretions/Nodules (w/in 3"; > 2mm)
- High organic content in surface (in Sandy Soils)
- Organic streaking (in Sandy Soils)
- Organic pan (in Sandy Soils)
- Listed on Hydric Soils List (and soil profile matches)
- Meets hydric soil criteria 3 or 4 (ponded or flooded for long duration)
- Supplemental indicator (e.g., NRCS field indicator): \_\_\_\_\_

Criteria Met? YES  NO \_\_\_\_\_

HYDROLOGY

Recorded Data

Recorded Data Available  Aerial Photos  Stream gauge  Other  No Recorded Data Available

Field Data

Depth of inundation: 3' Depth to Saturation: \_\_\_\_\_ Depth to free water: \_\_\_\_\_

Primary Hydrology Indicators:

- Inundated
- Saturated in upper 12 inches
- Water Marks
- Drift Lines
- Sediment Deposits
- Drainage Patterns

Secondary Hydrology Indicators (2 or more required):

- Oxidized Root Channels (upper 12")
- Water-stained Leaves
- Local Soil Survey Data
- FAC-Neutral Test
- Other: \_\_\_\_\_

Criteria Met? YES  NO \_\_\_\_\_ Comments: \_\_\_\_\_

DETERMINATION

WETLAND? YES  NO \_\_\_\_\_ Comments: Data plot in drainage ditch, which flows to Mountain Creek (see Report Figures)



US Army Corps  
Of Engineers (Portland District)

# Joint Permit Application Form

DATE STAMP



AGENCIES WILL ASSIGN NUMBERS

Corps Action ID Number \_\_\_\_\_ Oregon Department of State Lands No \_\_\_\_\_

**SEND ONE SIGNED COPY OF YOUR APPLICATION TO EACH AGENCY**

District Engineer  
ATTN: CENWP-OP-GP  
PO Box 2946  
Portland, OR 97208-2946  
503-808-4373

State of Oregon  
Department of State Lands  
775 Summer Street NE Suite 100  
Salem, OR 97301-1279  
503-378-3805

(1) Applicant Name and Address	Larry Medinger PO Box 702 Ashland, Oregon 97501	Business Phone # (541) 482-3961 Home Phone# FAX # (541) 482-6846 E-mail:
Authorized Agent Name and Address (signature required in Block 9) <input type="checkbox"/> Consultant <input type="checkbox"/> Contractor	Same as Above	Business Phone # Same as Above Home Phone# FAX # E-mail:
Property Owner Name and Address (if different than applicant) <sup>1</sup>	Havurah Friends Investment Group, LLC	Business Phone # N/A Home Phone# FAX # E-mail:

(2) **PROJECT LOCATION**

Street, Road or other descriptive location		Legal Description (attach tax lot map)		
185 N. Mountain Avenue Ashland Oregon 97520		Quarter/Quarter NW 1/4	Section 9	Township 39S Range 1E
In or Near (City or Town) Ashland	County Jackson	Tax Map # 391E09AA		Tax Lot # <sup>2</sup> 1500,1600 & 1800
Wetland Name (pick one) Adjacent to Mt. Creek	River Mile (if known) N/A	Latitude 42 11' 08" N		Longitude 122 42' 3.35"W
Do you consent to allow Corps or Dept. of State Lands staff to enter into the above-described property? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No				

(3) **PROPOSED PROJECT INFORMATION**

Activity Type:  Fill  Excavation (removal)  In-Water Structure  Maintain/Repair an Existing Structure

Brief Description: A portion of an existing potential wetland (0.02 acres) located within a drainage ditch will be filled and mitigated on-site to allow for the development of Residential Property. 0.05 acres of wetlands will be created for Wetland Mitigation, at a ratio of 1.5 to 1 to offset the impacts of filling in the ditch which may contain jurisdictional wetlands..

Fill				Removal			
<input type="checkbox"/> Riprap	<input type="checkbox"/> Rock	<input type="checkbox"/> Gravel	<input type="checkbox"/> Sand	<input checked="" type="checkbox"/> Silt	<input type="checkbox"/> Riprap	<input type="checkbox"/> Rock	<input type="checkbox"/> Gravel
<input checked="" type="checkbox"/> Clay	<input type="checkbox"/> Organics	<input checked="" type="checkbox"/> Other: soil		<input checked="" type="checkbox"/> Clay	<input checked="" type="checkbox"/> Organics	<input type="checkbox"/> Other:	
Cy Annually	Cy Total Project	800		Cy Annually	Cy Total Project	1570	
Cy in wetlands or below OHW or high tide line	109			Cy in wetlands or below OHW or high tide line	0		
Impact Area in Acres or Dimensions (feet)				Impact Area in Acres or Dimensions (feet)			
Acres	0.02	l'	w'	Acres	0	l'	w'
Is the disposal area: Upland? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No				Wetland / Waterway? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			

NOTE: Italicized areas are not required by the Corps for a complete application, but are requested by DSL.

<sup>1</sup> If applicant is not the property owner, permission to conduct the work must be attached.

<sup>2</sup> Attach a copy of all tax maps with the project area highlighted.

Are you aware of any state or federal Endangered Species on the project site?  Yes  No If yes, please explain in the  
 Are you aware of any Cultural Resources on the project site?  Yes  No project description (on page  
 Is the project site within a national Wild & Scenic River?  Yes  No 2, block 4)  
 Is the project site within a State Scenic Waterway?  Yes  No

**(4) PROPOSED PROJECT PURPOSE & DESCRIPTION**

**Project Purpose and Need:**

*(Include a description of the public, social or economic benefits of the project along with any supporting formal actions of a public body, as appropriate.)*

The project will offer housing development property which fulfills a necessary need for the city of Ashland and provides economic benefits to the community by way of jobs. This project will comply with the City of Ashland ordinances and planning regulations.

**Project Description:**

*(Include volumes and acreages of all fill and removal activities in waterway or wetland separately, permanent and temporary impacts, types of materials, staging areas, access routes and general construction methods.)*

The project will require 0.02 acres of wetland fill, the volume totaling 109 cu. yds. in a jurisdictional wetland.)

Permanent impacts include the loss of open space and reduction of wildlife habitat within the Ashland area.

Temporary impacts include soil disturbance during construction and the initial time it takes for mitigation wetlands to reestablish.

The types of materials required are wetland and riparian plants, erosion control products, and soil amendments.

The staging and access areas will be limited to upland areas near the entrance to the site (See Exhibit G).

The general construction methods will include clearing/grubbing of exotic and noxious weeds, installation of BMPs, staking and layout of the project, stockpiling of hydric soil to be placed in mitigation wetlands, filling and compaction of 0.02 acres of ditch wetlands with soil / rock, construction of bioswale, and planting / grading of 0.05 acres of mitigation wetlands.

Refer to the attached Exhibits A-G for more details.

**Project Drawings:**

How many project drawing sheets are included with this application? (must be on separate 8½ X 11 sheets) 8

Note: A complete application must include a location map, site plan and cross-section drawings:

**Location map** (must be legible with street names)

**Site plan** (include project footprint, proposed contours, ordinary high water or wetland boundary, proposed impacts, staging areas, temporary impacts, location of cross section(s), as applicable)

**Cross section(s)** (include existing and proposed elevations, ordinary high water or wetland boundary)

Will any construction debris, runoff, etc., enter a wetland or waterway?  Yes  No

If yes, describe the type of discharge and show the discharge location on the site plan.

Estimated Start Date

Fall 2007

Estimate Completion Date Fall 2009

(5)

**PROJECT IMPACTS AND ALTERNATIVES**

Describe alternative sites and project designs that were considered to avoid impacts to the waterway or wetland. *(Include alternative design(s) with less impact and reasons why the alternative(s) were not chosen.)*

Some of the initial design called for avoiding the ditch but the lack of developent land necissitated the filling of the wetland ditch. Wetland mitigation to occur on site as part of an existing wetland and open space area.

*Describe any changes the proposed project may have on the direction of stream flow, etc. (hydrological characteristics) of the waterway.*

The proposed project will change the hydrological characteristics of the small ditch wetland by filling 0.02 acres (1,100sq. ft) of this ditch and creating the mitigation wetland along the Mountain Creek Corridor by expanding the existing PSS wetland to include 0.05 acres of PEMCx wetlands. See Exhibits A-G for more details.

Describe what measures you will use (before and after construction) to minimize impacts to the waterway or wetland. *(Include an erosion control plan, storm water management plan to manage post construction storm water, work area isolation plan or description of other best management practices (BMP's) as appropriate.)*

The following are measures that will be utilized to minimize construction impacts to wetlands, and manage post construction storm water:

1. See erosion control plan, Figure .
3. See attached work area isolation plan (included in erosion control plan).
4. In addition, BMPS such as: silt fence, straw wattles, hydroseeding, palm mats, and graveled equipment road access areas will be utilized to minimize erosion and sediment impacts to wetlands (Refer language and typical sketches).

NOTE: If necessary, use additional sheets.

(6)

**ADDITIONAL INFORMATION**

Adjoining Property Owners and Their Address and Phone Numbers *(if more than 5, attach printed labels)*

See attached list of property owners

Has the proposed activity or any related activity received the attention of the Corps of Engineers or the Department of State Lands in the past, e.g., wetland delineation, violation, permit, lease request, etc.?  Yes  No

If yes, what identification number(s) were assigned by the respective agencies:  
Corps # \_\_\_\_\_ State of Oregon # \_\_\_\_\_

(7)

**CITY/COUNTY PLANNING DEPARTMENT AFFIDAVIT (to be completed by local planning official)**

(7) CITY/COUNTY PLANNING DEPARTMENT AFFIDAVIT (to be completed by local planning official)

I have reviewed the project outlined in this application and have determined that:

- This project is not regulated by the comprehensive plan and land use regulations.
- This project is consistent with the comprehensive plan and land use regulations.
- This project will be consistent with the comprehensive plan and land use regulations when the following local approval(s) are obtained.

Conditional Use Approval     Development Permit     Other Subdivision  
MODIFICATION

This project is not consistent with the comprehensive plan. Consistency requires a  
 Plan Amendment     Zone Change     Other \_\_\_\_\_

An application  has  has not been filed for local approvals checked above.

Signature of local planning official: [Signature] Title: PLANNING MANAGER City/County: CITY OF ASHLAND Date: 8/3/2007  
 Comments:

(8) COASTAL ZONE CERTIFICATION

If the proposed activity described in your permit application is within the Oregon coastal zone, the following certification is required before your application can be processed. A public notice will be issued with the certification statement, which will be forwarded to the Oregon Department of Land Conservation and Development for its concurrence or objection. For additional information on the Oregon Coastal Zone Management Program, contact the department at 635 Capitol Street NE, Suite 150, Salem, Oregon 97301 or call 503-373-0050.

CERTIFICATION STATEMENT

I certify that, to the best of my knowledge and belief, the proposed activity described in this application complies with the approved Oregon Coastal Zone Management Program and will be completed in a manner consistent with the program.

Print /Type Name \_\_\_\_\_ Title \_\_\_\_\_  
 Applicant Signature \_\_\_\_\_ Date \_\_\_\_\_

(9) SIGNATURE FOR JOINT APPLICATION

Application is hereby made for the activities described herein. I certify that I am familiar with the information contained in the application, and, to the best of my knowledge and belief, this information is true, complete, and accurate. I further certify that I possess the authority to undertake the proposed activities. I understand that the granting of other permits by local, county, state or federal agencies does not release me from the requirement of obtaining the permits requested before commencing the project. I understand that payment of the required state processing fee does not guarantee permit issuance. The fee for the state application must accompany the application for completeness. Amount enclosed \$620.

~~Larry Mendinger~~ LARRY MEDINGER Developer REPRESENTING: HAVURAH FRIENDS INVESTH. GROUP  
 Print /Type Name \_\_\_\_\_ Title \_\_\_\_\_  
[Signature] Date August 8, 2007  
 Applicant Signature \_\_\_\_\_

I certify that I may act as the duly authorized agent of the applicant.

Print /Type Name \_\_\_\_\_ Title \_\_\_\_\_  
 Authorized Agent Signature \_\_\_\_\_ Date \_\_\_\_\_

I certify that the applicant has my permission to conduct the project on my property. PROP. OWNER AND DEVELOPER  
~~Larry Mendinger~~ LARRY MEDINGER Developer HAVURAH FRIENDS INVESTMENT GROUP  
 Print /Type Name \_\_\_\_\_ Title REPRESENTATIVE  
[Signature] Date August 8, 2007  
 Property Owner Signature \_\_\_\_\_

(10) SUPPLEMENTAL IMPACT AND RESOURCE REPLACEMENT INFORMATION

<sup>3</sup> If the project is on a state-owned waterway, you must contact the Land Management Division of the Department of State Lands for approval to proceed with this application. See [www.oregonstatelands.us](http://www.oregonstatelands.us) for a list of state-owned waterways.

Impact area is  Ocean  Estuary  River  Lake  Stream  Freshwater Wetland

Has a wetland delineation been completed for this site?

Yes  No

If yes, by whom: Northwest Biological Consulting, I

Has the wetland delineation been approved by DSL or the COE?

Yes  No

(If yes, attach concurrence letter.)

Describe the existing **physical and biological character** of the wetland/waterway site by area and type of resource.  
(Use separate sheets and photos, if necessary.)

0.02 acres of palustrine emergent seasonally flooded, excavated ditch (PEMCx) wetlands are present on the 2.60 acre site. The HGM class is Riverine Flow Through. The site has a long history of land disturbance related to agricultural use but it has not been used for agriculture in recent years. The topography is characterized by a fairly flat meadow that lies adjacent to Mountain Creek. The wetland ditch contains irrigation return water and is a major conduit for carrying storm water. The wetland vegetation is dominated by Phalaris arundinacea, and to a lesser degree by Scirpus acutus, Juncus effusus, Typha latifolia, Holcus lanatus, and Eleocharis bella. The upland meadow community is dominated by Geranium dissectum, Lolium perenne, Elymus caput-medusea, Poa bulbosa, Bromus tectorum Festuca pratensis, and Hordeum depressum. The soil types on the site are: 33A Coker Clay, 0-3% slopes, found on alluvial fans, and 100A Kubli Loam, 0-3% slopes found on stream terraces. Approximately 1/4 of the site is within the flood plain of Mountain creek, and the remainder is upland meadow. The functional assessment of the wetland includes: limited nutrient cycling, limited wildlife habitat, and flood plain functions during events that exceed bank full.

*For wetlands, include vegetation, Cowardin and Hydrogeomorphic Method (HGM) class, position in the landscape, prior disturbance, soil types, hydro period, summary of functional assessment.*

*For waterways, include a description of the bank slopes, riparian vegetation, channel morphology, stream substrate, prior disturbance, erosion, water flows and general appearance.*

*Describe the existing navigation, fishing and recreational use of the waterway or wetland.*

There is no navigation, fishing, or recreational use of the wetlands

#### **Resource Replacement Mitigation**

*Describe measures to be taken to replace wetland/waterway resources where reasonably expected adverse impacts including wetland functions cannot otherwise be avoided or minimized. See list below for required information.*

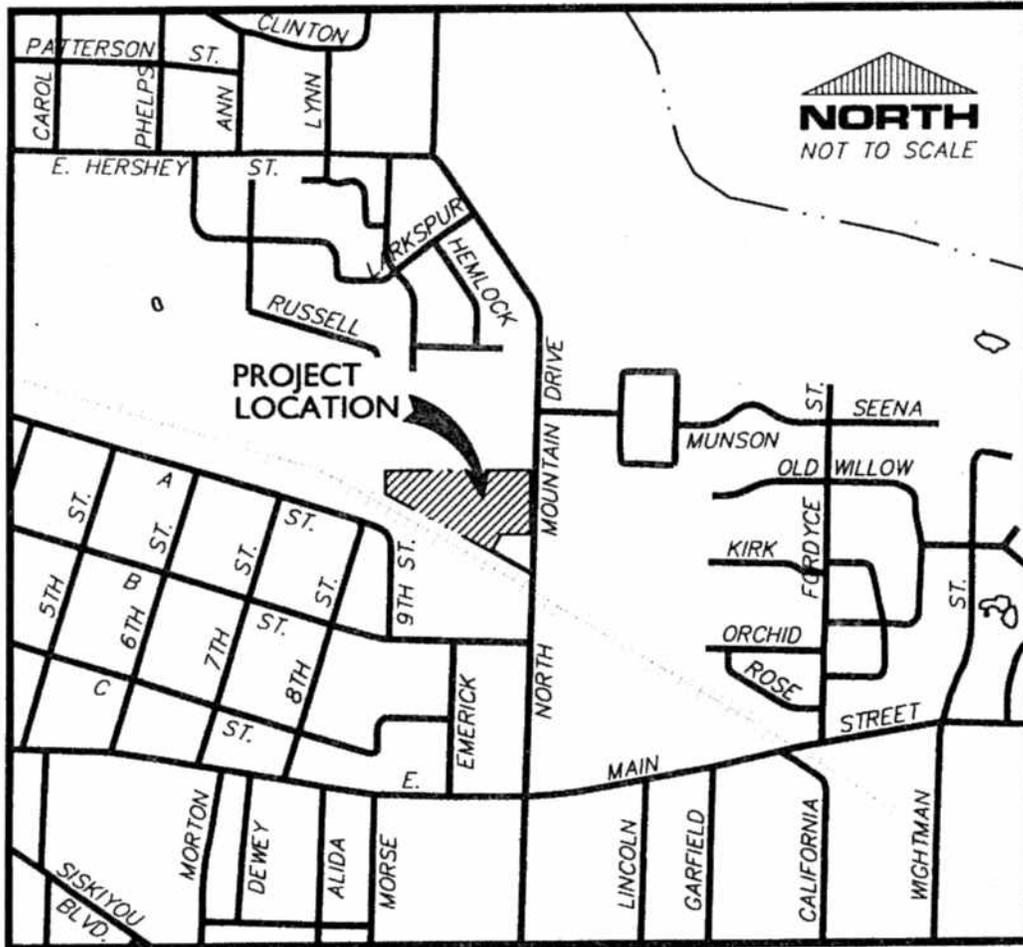
*For Freshwater Wetland Impacts include a Compensatory Wetland Mitigation (CWM) Plan*

*For Waterway Impacts include a Compensatory Mitigation (CM) Plan*

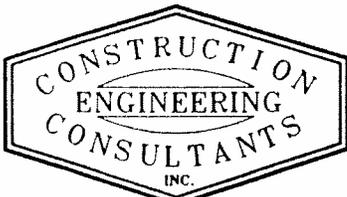
*For Temporary Impacts include a Rehabilitation Plan*

*For Estuarine Impacts include an Estuarine Resource Replacement Plan*

Refer to the attached Compensatory Wetland Mitigation (CWM) Plan



**VICINITY MAP**



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PH. (541) 779-5266 • FAX (541) 779-3139

**NORTHWEST BIOLOGICAL CONSULTING  
HABITAT RESTORATION**

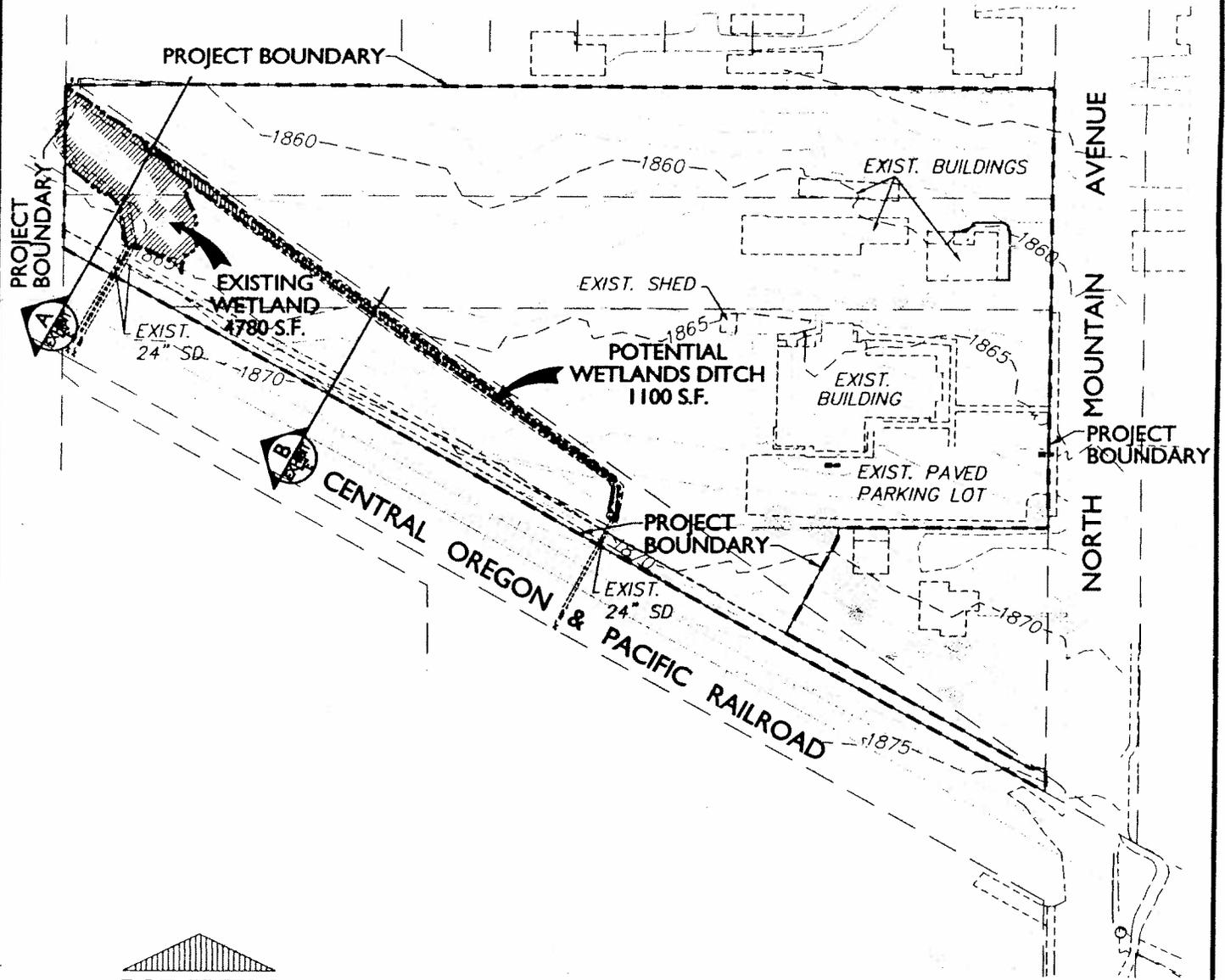
324 TERRACE STREET  
ASHLAND, OR 97520  
SCOTT ENGLISH  
PH/FAX (541) 488-1061

**CITY OF ASHLAND**

**ALEPH SPRINGS SUBDIVISION**

**SITE VICINITY MAP**

**EXHIBIT "A"**

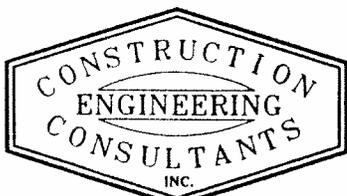


GRAPHIC SCALE



( IN FEET )

1 inch = 100 ft.



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PH. (541) 779-5268 • FAX (541) 779-3139

NORTH-WEST BIOLOGICAL CONSULTING  
HABITAT RESTORATION

324 TERRACE STREET  
ASHLAND, OR 97520  
SCOTT ENGLISH

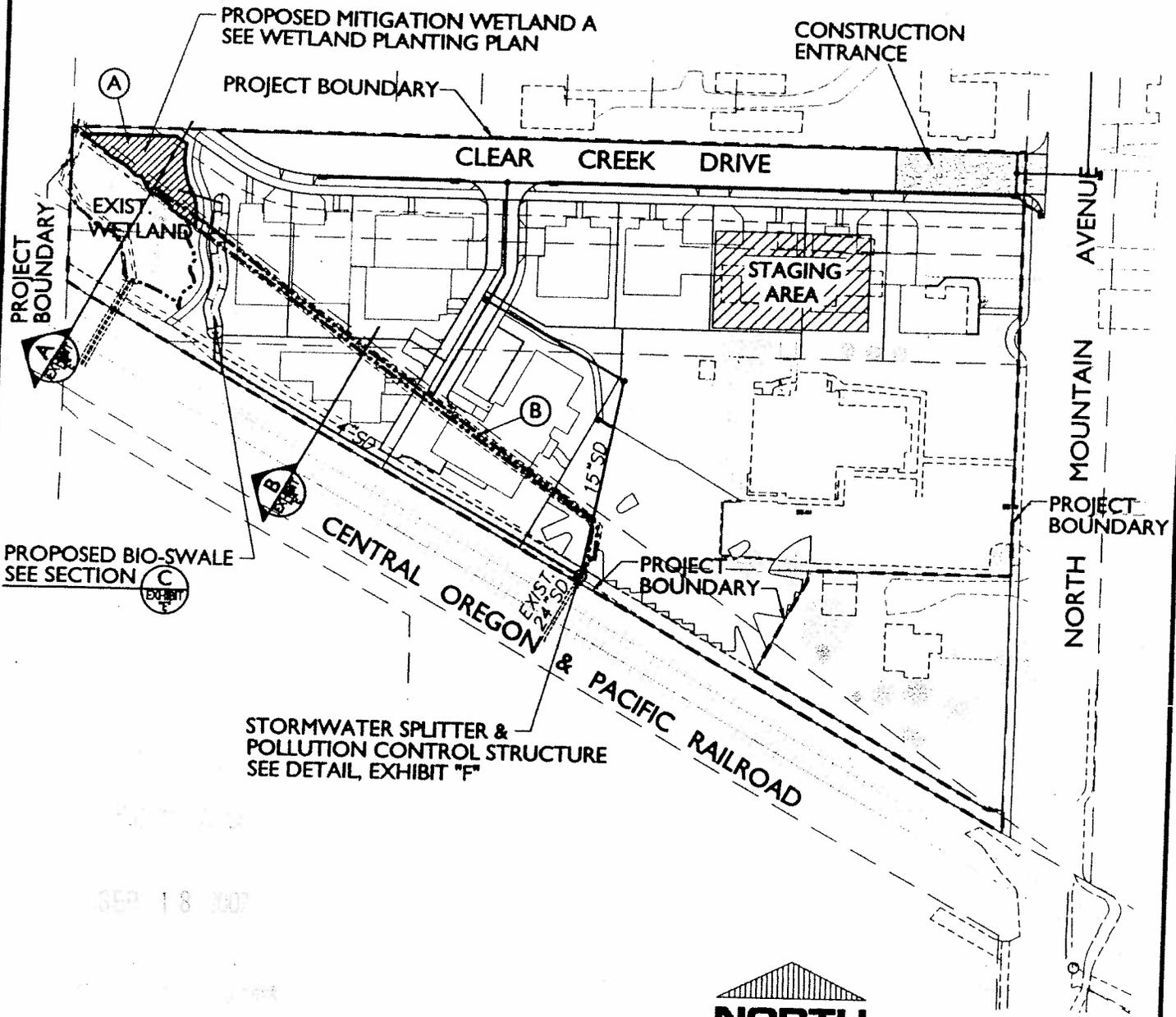
PH/FAX (541) 488-1061

CITY OF ASHLAND

ALEPH SPRINGS SUBDIVISION

EXISTING CONDITIONS MAP

EXHIBIT "B"

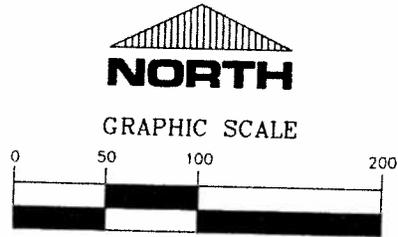


**MITIGATION WETLAND**

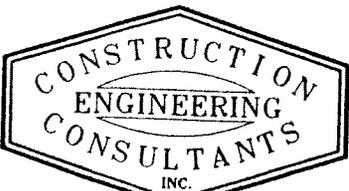
(A) 1988 S.F.

**POTENTIAL WETLAND IMPACTS**

(B) 1100 S.F.



( IN FEET )  
1 inch = 100 ft.



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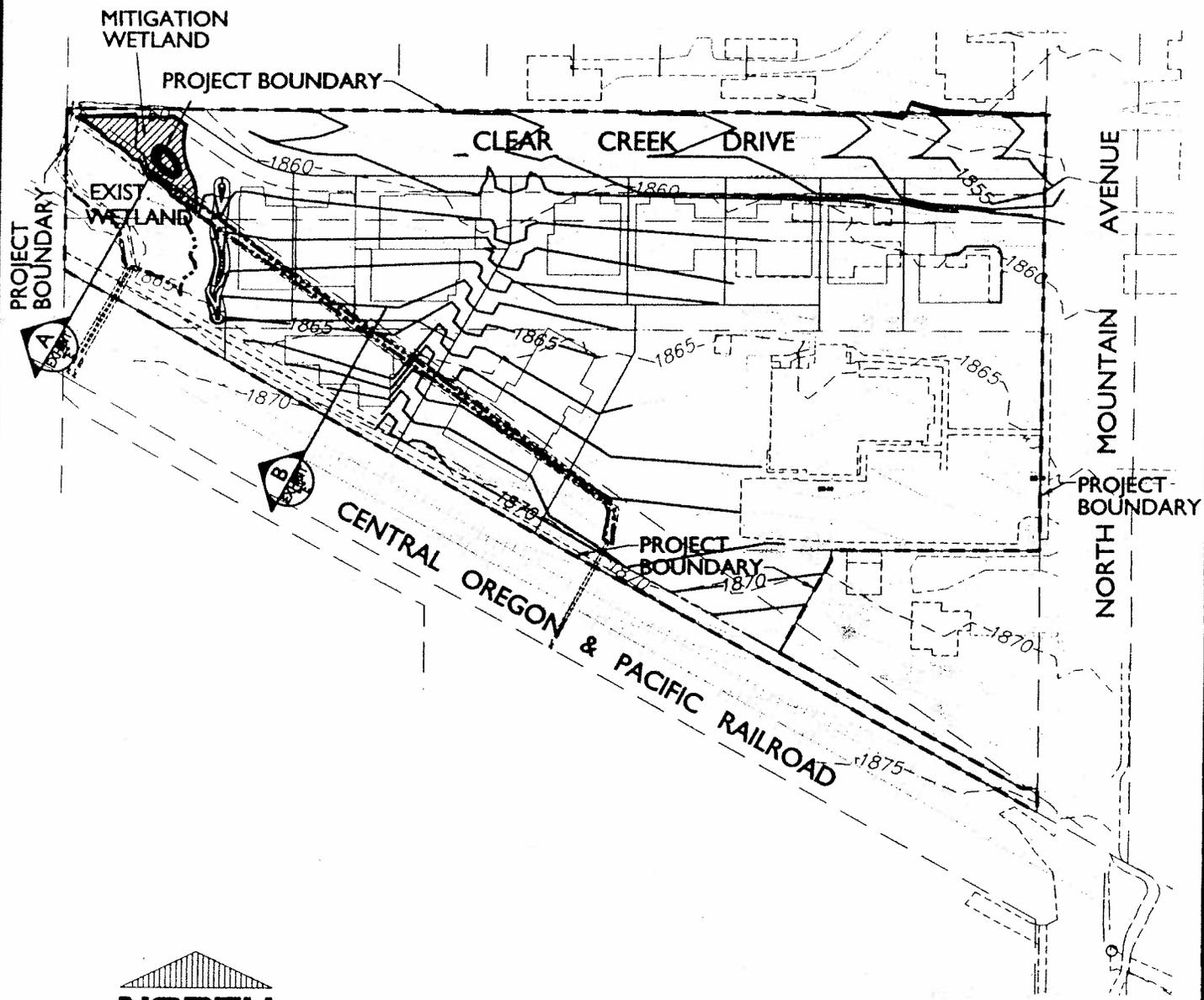
**NORTHWEST BIOLOGICAL CONSULTING  
HABITAT RESTORATION**

324 TERRACE STREET  
ASHLAND, OR 97520  
SCOTT ENGLISH  
PH/FAX (541) 488-1061

**CITY OF ASHLAND**

**ALEPH SPRINGS SUBDIVISION  
SITE DEVELOPMENT PLAN**

*EXHIBIT "C"*

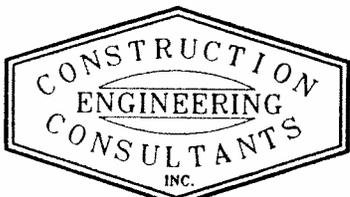


GRAPHIC SCALE



( IN FEET )

1 inch = 100 ft.



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 PH. (541) 779-5288 • FAX (541) 779-3139

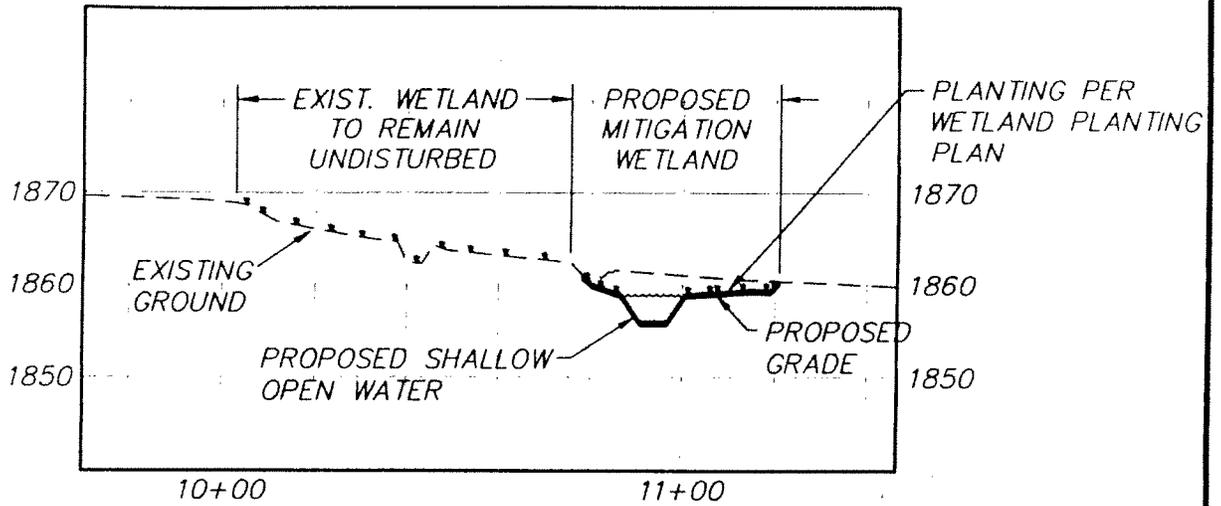
**NORTHWEST BIOLOGICAL CONSULTING**  
**HABITAT RESTORATION**  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

**CITY OF ASHLAND**

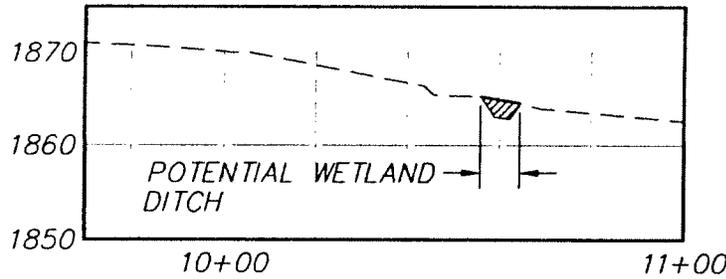
**ALEPH SPRINGS SUBDIVISION**

**GRADING PLAN**

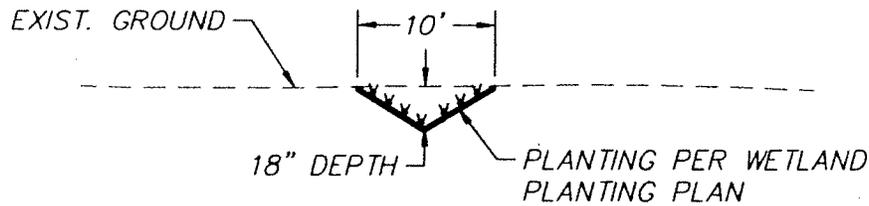
**EXHIBIT "D"**



**WETLAND SECTION A**



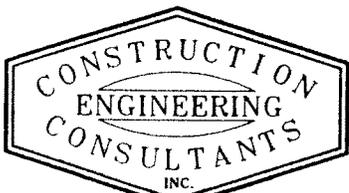
**POTENTIAL WETLAND SECTION B**



**BIO-SWALE SECTION C**

WETLAND CUT-FILL VOLUMES		
	CUT	FILL
TOTAL SITE	1570 C.Y.	800 C.Y.
WETLAND - NEW	130 C.Y.	0 C.Y.
POTENTIAL WETLAND DITCH	0 C.Y.	109 C.Y.

SCALES  
 HORIZONTAL: 1" = 40'  
 VERTICAL: 1" = 20'



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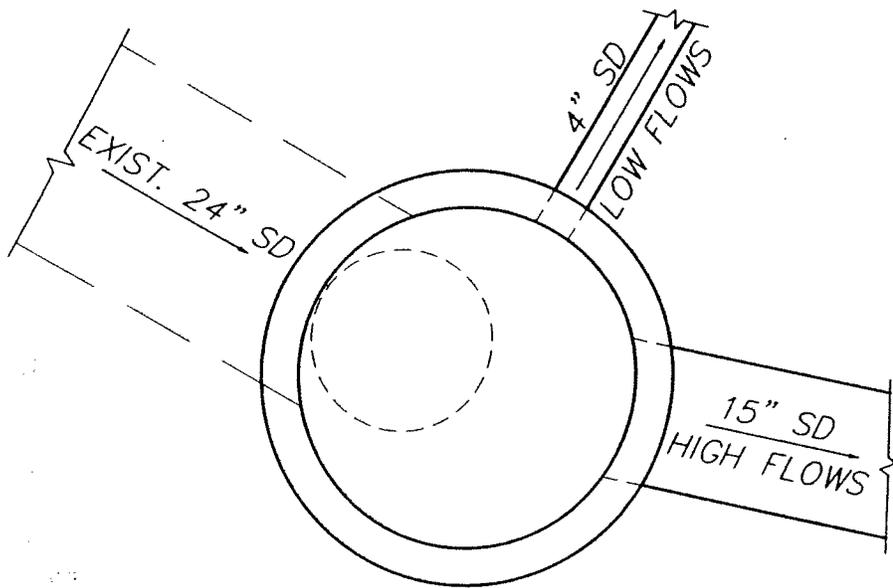
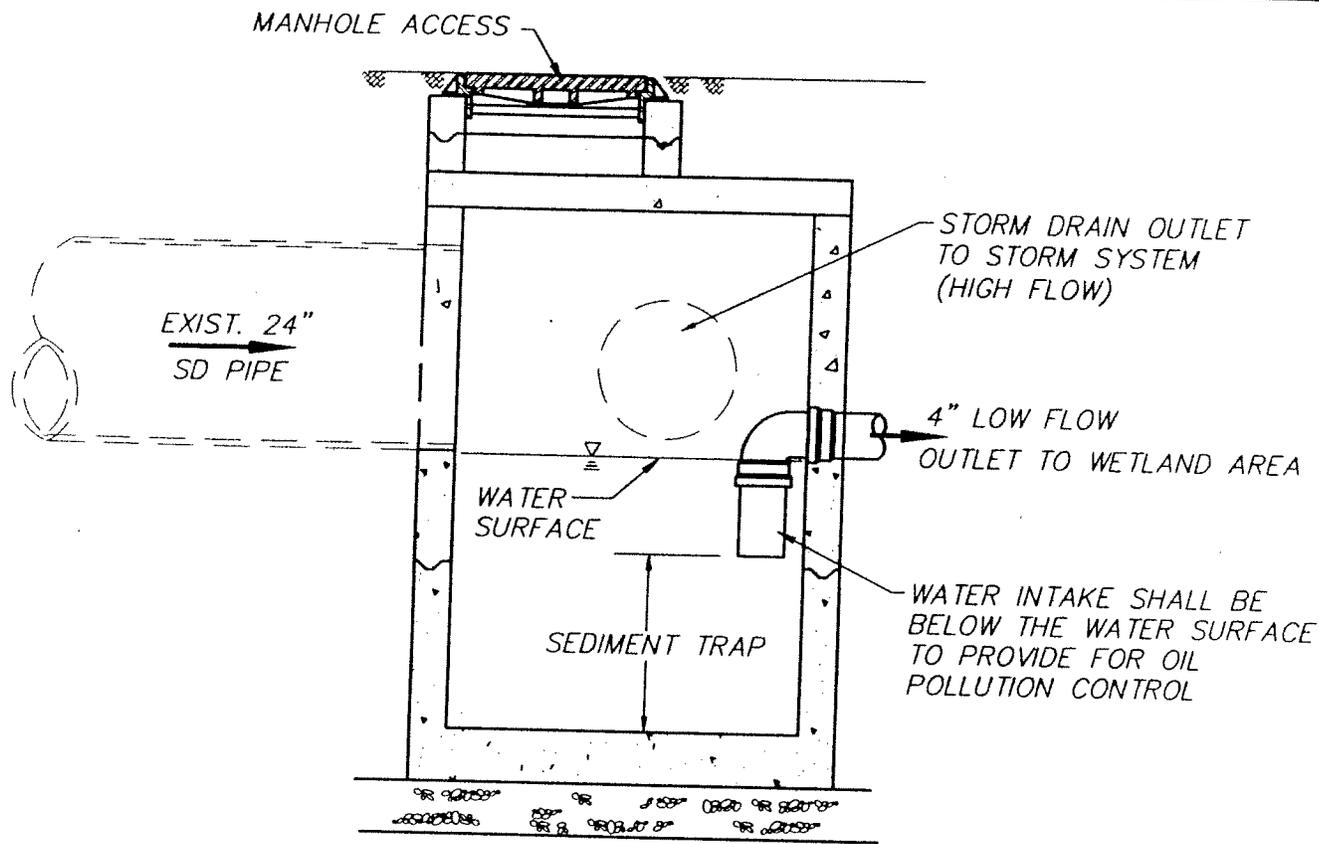
NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

**CITY OF ASHLAND**

**ALEPH SPRINGS SUBDIVISION**

**WETLAND CROSS-SECTION**

**EXHIBIT "E"**



**POLLUTION CONTROL STRUCTURE DETAIL**  
 NOT TO SCALE

**CONSTRUCTION  
 ENGINEERING  
 CONSULTANTS  
 INC.**

P.O. BOX 1724 • MEDFORD, OREGON 97501  
 PH. (541) 779-5268 • FAX (541) 779-3139

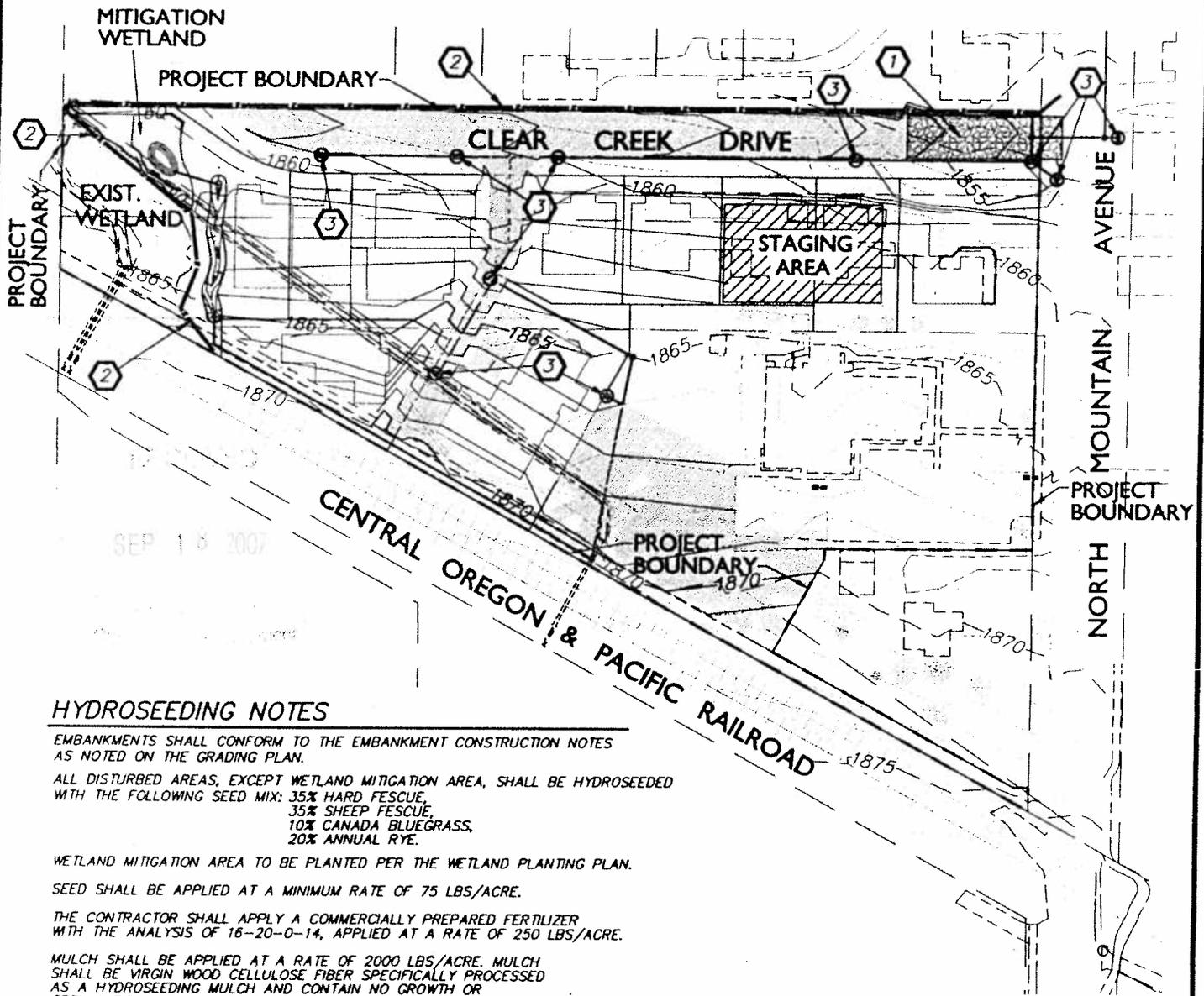
**NORTHWEST BIOLOGICAL CONSULTING  
 HABITAT RESTORATION**

324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

**CITY OF ASHLAND**

ALEPH SPRINGS SUBDIVISION  
**STORMWATER SPLITTER & POLLUTION  
 CONTROL STRUCTURE DETAIL**

EXHIBIT "F"



**HYDROSEEDING NOTES**

EMBANKMENTS SHALL CONFORM TO THE EMBANKMENT CONSTRUCTION NOTES AS NOTED ON THE GRADING PLAN.

ALL DISTURBED AREAS, EXCEPT WETLAND MITIGATION AREA, SHALL BE HYDROSEEDED WITH THE FOLLOWING SEED MIX: 35% HARD FESCUE, 35% SHEEP FESCUE, 10% CANADA BLUEGRASS, 20% ANNUAL RYE.

WETLAND MITIGATION AREA TO BE PLANTED PER THE WETLAND PLANTING PLAN.

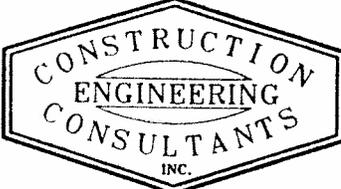
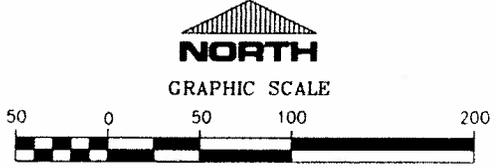
SEED SHALL BE APPLIED AT A MINIMUM RATE OF 75 LBS/ACRE.

THE CONTRACTOR SHALL APPLY A COMMERCIALY PREPARED FERTILIZER WITH THE ANALYSIS OF 16-20-0-14, APPLIED AT A RATE OF 250 LBS/ACRE.

MULCH SHALL BE APPLIED AT A RATE OF 2000 LBS/ACRE. MULCH SHALL BE VIRGIN WOOD CELLULOSE FIBER SPECIFICALLY PROCESSED AS A HYDROSEEDING MULCH AND CONTAIN NO GROWTH OR GERMINATION INHIBITING FACTORS.

**CONSTRUCTION NOTES**

- ① ROCKED CONSTRUCTION ENTRANCE FOR 100' MINIMUM TO PREVENT TRACKING OF SOIL ONTO ROADWAY, 8" MIN. THICKNESS OF 3"-6" CLEAN ROCK ON FILTER FABRIC. LINE AND GRADE SHALL CONFORM TO FUTURE ROAD IMPROVEMENTS.
- ② FILTER FABRIC SEDIMENTATION FENCE INSTALLED AT THE LOCATIONS SHOWN.
- ③ PROPOSED STORM DRAIN INLETS SHALL BE PROTECTED BY FILTER FABRIC INLET BARRIERS.



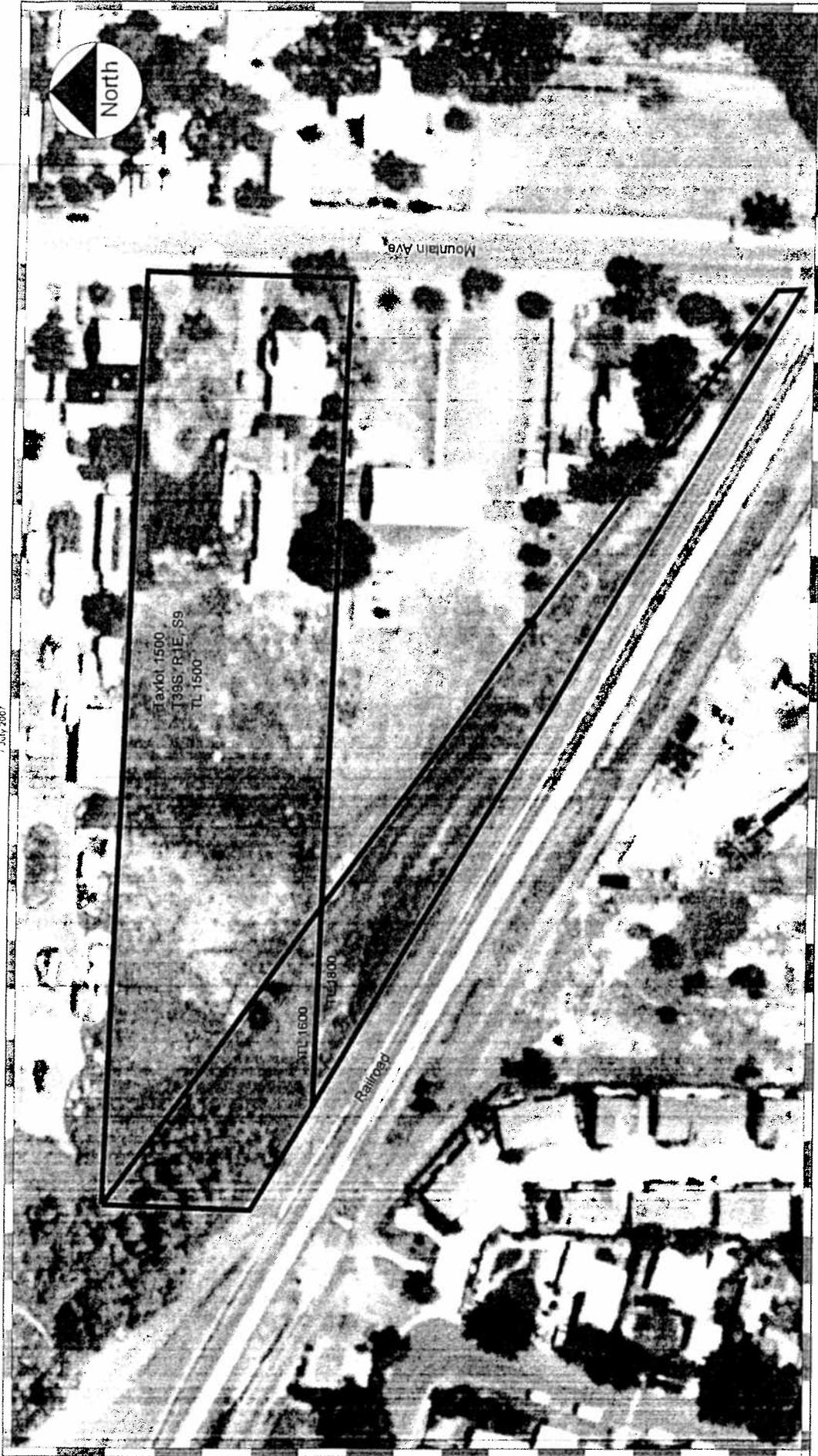
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**CITY OF ASHLAND**  
ALEPH SPRINGS SUBDIVISION  
EROSION & SEDIMENT CONTROL PLAN

EXHIBIT "G"

7 JULY 2007



**Figure 6**  
**Aerial Photo**  
**Havurah Wetland Delineation**  
**Ashland, Oregon**

**LEGEND**



Study Area

**Northwest Biological Consulting**



Habitat Restoration,  
 Environmental Planning  
 CA Engineering Contractor  
 OR General Contractor

324 Terrace St. P.O. Box 671 nwbiolog.com  
 Ashland, OR 97520 Ph/Fax (541) 488-1061



**NORTHWEST BIOLOGICAL CONSULTING**

HABITAT RESTORATION - ENVIRONMENTAL PLANNING

Cal. Engineering Contractors Lic. #599428

## **Wetland Mitigation Plan for Aleph Springs City of Ashland, Jackson County, Oregon**

**Submitted To:      Division of State Lands  
                            Wetland Program  
                            775 Summer St. NE Suite 100  
                            Salem, Oregon 97301**

**&**

**Army Corps of Engineers  
1600 Executive Parkway, Suite 210  
Eugene, Oregon 97401**

**Completed By:      Northwest Biological Consulting  
                            324 Terrace Street  
                            Ashland, Oregon 97520**

**August 11, 2007**

## **Section 1: CWM Site Information:**

The 2.57 acre CWM site owner is Havurah Friends Investment Group, 185 N. Mountain Ave, Ashland, Oregon 97520. The Authorized Agent is Larry Mendinger, PO Box 702, Ashland, Oregon. Ph. (541) 482-3961, Fax (541) 482-6846.

Location is T39S, R1E, S09AA, Tax lots 1500, 1600, and 1800. See table 1 and exhibits A-I for more information and details.

## **Section 2: CWM Baseline Information:**

A wetland Delineation was completed in July of 2007, and is part of this submittal. The HGM class on site is Riverine Flow Through. Two Cowardin classes of wetlands occur on site: Palustrine Emergent, Seasonally Flooded, Excavated (PEMCx ) wetland, located in the drainage ditch (0.02 acres), and Palustrine Scrub Shrub ( PSS.) located adjacent to Mountain Creek (0.11 acres).

The following baseline information relates to a **Wetland Creation Plan to Satisfy the Requirements for CWM.**

- The existing 0.11 acre PSS wetland would not be impacted but the 0.02 acre PEMCx wetland in the drainage ditch will be filled. Please refer to the attached wetland delineation which provides more details on the PSS and PEMCx wetlands. We propose to create a PEMCx and HGM Riverine Flow mitigation wetland on site to compensate for the loss of the 0.02 acre PEMCx wetland. The existing PEMCx wetland plant community (0.02 acres), occurs below Ordinary High Water (OHW) in the ditch and consists of : narrow-leaf cattail (OBL), meadow foxtail (FACW), American speedwell (OBL), and buttercup (FACW) with a few scattered patches of exotic reed canary grass (FACW). The PEMCx water source is the drainage ditch water which supplies some irrigation return water during the growing season and storm runoff water during rain events. The wetland water is available permanently and the created mitigation wetlands are expected to maintain wetland hydrology throughout the year. During the growing season the estimated flow in the ditch is approximately 25 to 50 gallons per minute.
- The proposed construction of roads in the housing development will require piping the storm water and filling in the existing PEMCx wetland which occurs below OHW in the ditch.
- The proposed CWM will reverse the degradation of the impacted ditch wetlands by the following:
  1. The total area of the PEMCx impacted wetlands, 0.02 acres will be replaced by the creation of 0.05 acres of the above described wetland type. This is slightly more than the required 1 to 1.5 replacement ratio.
  2. In addition, the created wetlands will be planted with all native emergent and riparian

plants to increase the plant diversity and provide better biological filtration of the irrigation and urban runoff water.

3. The replacement wetlands will have a larger cross-sectional area and have the ability to provide sediment- trapping via the construction of a stormceptor like treatment system.

4. The replacement - wetlands will have more emergent wetland area and provide better nutrient removal and wildlife habitat.

5. And finally, the proposed created wetlands will be constructed along the edge of the existing PSS wetland and will be planted with a variety of riparian trees and shrubs to provide shade (cooling) and habitat. Native emergent wetland plants will be planted throughout the created wetlands. Exotic species such as blackberries and reed canary grass will also be removed and replaced with native plants.

The area of total wetlands on the site is 0.13 acres and the proposed wetland creation amounts to 0.05 acres to compensate for a total wetland impact of 0.02 acres. Please refer to the following **Exhibits A-I and table 1 for more details.**

## **Section 3: CWM Plan Description**

### **The goals, objectives, and success criteria are as follows:**

The **goals and objectives** are to create 0.05 acres of high quality wetlands to replace the 0.02 acres of PEMCx wetlands which would be impacted by fill and removal related to the piping and filling of the drainage ditch, in the Aleph Springs housing development. In addition, the goals and objectives are to create a riparian edge of native trees and shrubs and emergent wetland vegetation as part of an open space plan that extends for approximately 300 lineal feet along the edge of the created wetland. Also, it is the intent of this project to create wetlands in conjunction with stormceptor collection points and a bio-swale to slow the runoff and filter the nutrient rich water through various wetland bio-filtration features within the project.

The **success criteria** will be based on the area of created wetlands as well as the 80% survival of all plants, trees, and shrubs planted in the created wetlands and along the project riparian edge which is not part of the required mitigation but is in the spirit of the project. The created wetland area will be verified by a wetland delineation to be performed before the final monitoring year. In addition, we propose a minimum of 75% cover of dominant wetland plants within a 3 year time span and removal of 70% of the non-native weedy species such as blackberries and reed-canary grass. A 5 year monitoring plan will provide the data necessary to evaluate the success criteria.

The **CWM in general terms** will increase some of the lost wetland plant diversity and remove some of the exotic plants and replace them with native emergent plants and riparian vegetation. In addition, the proposed riparian and upland trees and shrubs will introduce a shade component which will help to maintain cooler water temperatures during the hot summer months and provide wildlife habitat. Recreational benefits and local education will also help to provide some of the functions that were lost in this system.

**A summary of anticipated lost function replacement is as follows:**

1. Increase in wildlife habitat by providing moderate plant diversity and replacing weedy species with native plants. Some minor waterfowl and wading bird habitat would be provided by the creation of a small shallow seasonal open- water component.
2. Aquatic habitat downstream in Mountain Creek will be improved by planting emergent vegetation, and riparian trees/shrubs which will provide shade and cooler water before discharging to mountain creek.
3. Food chain support will be improved, see above.
4. Sediment trapping will increase due to the placement and maintenance of stormceptors.
5. Minor flood attenuation is expected to be increased due to the detention provided by the creation of a bio-swale and the increased cross-sectional area of the created wetlands.
6. Nutrient uptake and overall water quality will be improved by removing exotic wetland plants and increasing the emergent plant area and bio-filtration benefits of the thatch and detention provided by the proposed CWM and the bio-swale.
7. Passive recreation and education will be provided by the proposed viewing areas and trail which will allow residents to view the wetlands and associated wildlife. In addition, a kiosk will be constructed on site as an interpretive education sign which explains the functions and values of wetlands.

The **rationale for site selection** is based on the uplands adjacent to PSS wetlands, which consists of blackberries, tall fescue, and weedy forbs such prickly lettuce( *Lactuca serriola*). Also, the removal of reed canary grass and blackberries and the planting of native wetland and riparian plants will help restore some of the biological functions and complexity of the site.. In addition, the availability of a permanent supply of water from the drainage ditch makes the site wetland ideal and insures that wetland hydrology will always be present.

**The proposed water source** is the irrigation return flows and storm water discharges which vary during the growing season from 25 to 100 gallons per minute. In addition, an irrigation system will be installed that will provide supplemental water during the growing season for 5 years. We expect that the wetlands will remain saturated to a depth of at least 6 inches in the emergent zones and wetted perimeter during the entire year from the irrigation water, storm water, and supplemental irrigation water. In addition, we anticipate that the seasonal shallow open water will maintain a depth of at least 2 feet during the growing season and beyond.

We do not anticipate any **CWM site constraints**, and feel that the site can be transformed from an ordinary drainage ditch with exotic vegetation to a PEMCx wetlands and riparian buffer with vegetation providing habitat, water quality benefits, better flood management and recreation for the community.

**The proposed HGM and Cowardin class** is Riverine Flow Through and PEMCx as discussed previously.

The **long-term maintenance and protection** of the CWM will be accomplished by the

development in the form of a deed restriction, designed by the consultant and developer.

The CWM meets the wetland creation ratio as was discussed previously.

## Section 4: Functional Replacement

The Cowardin and HGM class of impact wetland is PEMCx, Riverine Flow through. The unnamed ditch is a highly urbanized drainage that functions primarily to carry storm runoff and irrigation water. The ditch has a history of urban use and disturbance such as channelization, introduction of exotic invasive weedy vegetation, and ditch maintenance. The functions and values of the existing ditch wetland are limited due to the highly disturbed nature of the ditch and environs. The proposed CWM wetland will provide better function and value than the existing 0.02 acre PEMCx wetland.

In-kind emergent PEMCx wetlands will be created in uplands adjacent to the existing 0.11 acre PSS wetland. Blackberries, prickly lettuce, and other introduced invasive species will be removed from the existing uplands and not be planted in the mitigation wetlands. Riparian vegetation is minimal at the existing upland and the proposed CWM will include a diversity of native riparian trees and shrubs which will be planted along the edges and uplands of the created emergent wetlands. The depth of the seasonal open water in the CWM area will average around 2 feet. The small shallow open water element is found in nearby PEMCx wetlands located along Mountain Creek. Please refer to the attached CWM and riparian planting list (Table 1) for more details.

The in-kind Cowardin and HGM replacement wetland will be PEMCx, Riverine Flow Through, 0.05 acres. The replacement wetland will be the same type as the impact wetland but larger and more complex and will provide greater wetland functions and values compared to the impact wetland. The in-kind CWM is based upon the impact wetland type and similar emergent wetlands which are located nearby within the Mountain Creek drainage. The proposed planting list is based upon emergent and riparian species found in the impact wetland and other nearby wetlands. Please refer to the NWI map in the wetland delineation for more details.

The intended result of the mitigation would be increased sediment trapping, nutrient uptake, and flood storage, by means of expanding the cross-sectional area and size of the emergent - zones of the proposed replacement wetlands and installing a bioswale and stormceptor like system to trap sediments and other pollutants.

Functional assessment of impact wetlands The impact wetland area (0.02 acres), generally has only minimal functions for storm water retention, aquatic and wildlife habitat, sediment trapping, and nutrient intake/water quality benefits. In addition, the impact wetlands provide no passive or active recreation or education benefits. The impact PEMCx wetlands and adjacent area is dominated by blackberries and tall fescue grass. There are no endangered or threatened species habitat or unique or rare wetlands on the

site or on adjacent lands.

**The functional assessment of the impact site wetlands expected to be adversely affected** Adverse effects to the impact wetlands associated with the piping and filling of the wetland ditch will be limited to the following functions:

- Loss of PEMCx area of 0.02 acres due to grading, filling, pipe installation, etc.
- Temporary impacts to wildlife during construction time (about 60 days).
- Short-term increase in turbidity during construction
- Temporary loss of nutrient removal benefits of emergent plants while new plants are becoming established

**Functional assessment of existing wetlands at CWM site** includes the following:

- Limited flood attenuation due to narrow excavated ditch
- No recreation values
- No education values
- Limited nutrient removal
- Limited wildlife value due to lack of cover and riparian vegetation
- Limited aquatic habitat value and no fish present
- Limited sediment removal due to lack of stormceptor and bioswale.

**Functional assessment of predicted post treatment wetlands at CWM site, and net gain of specific functions** includes:

- Net increase of flood attenuation due to stormceptor and bioswale as well as design of CWM wetlands
- Addition of recreation values for residents of development due to construction of viewing area and trails
- Addition of educational benefits due to access trails with wetland kiosk
- Gain of nutrient removal due to the expanded emergent plants, bio-thatch zone, increased retention time of flow-through water, and maintenance of stormceptors, and bioswale.
- Increase of water quality standards due to sediment removal and shade offered by the planting of riparian vegetation
- Net gain of aquatic and wildlife habitat due to the cover and food provided by the native emergent and riparian vegetation, and removal of much of the introduced and exotic weedy plants
- More sediment removal due to the CWM design and installation of stormceptors.

**Net gain or loss of specific functions as a result of CWM** includes:

- There would be a temporary loss of existing impact wetlands due to the ditch replacement but that would only last for several months until the CWM vegetation is planted and grows in the CWM. Some temporary sediment input from construction is expected but will be minimized by the installation of BMP's and other measures
- The net gain of specific wetland functions is described above.

#### **Section 5: Construction Plans:**

Please refer to the attached site plan, grading plan, construction methods, cross sections, planting plan, and other details for more information (Exhibits A-H). The implementation schedule will begin in the uplands this spring (2008), with Bmp's and erosion control methods in place. The ditch construction will also be completed this spring. Initial grading of the wetlands and some of the planting will occur this spring. Nursery orders for wetland and riparian plants will be placed this fall for delivery in the spring. The spring and summer of 2008 will see the final creation of the wetlands and the planting of the remaining emergent wetlands and the riparian corridor. Refer to the erosion control plan in the fill/removal permit for more information.

#### **Section 6: Reference Sites:**

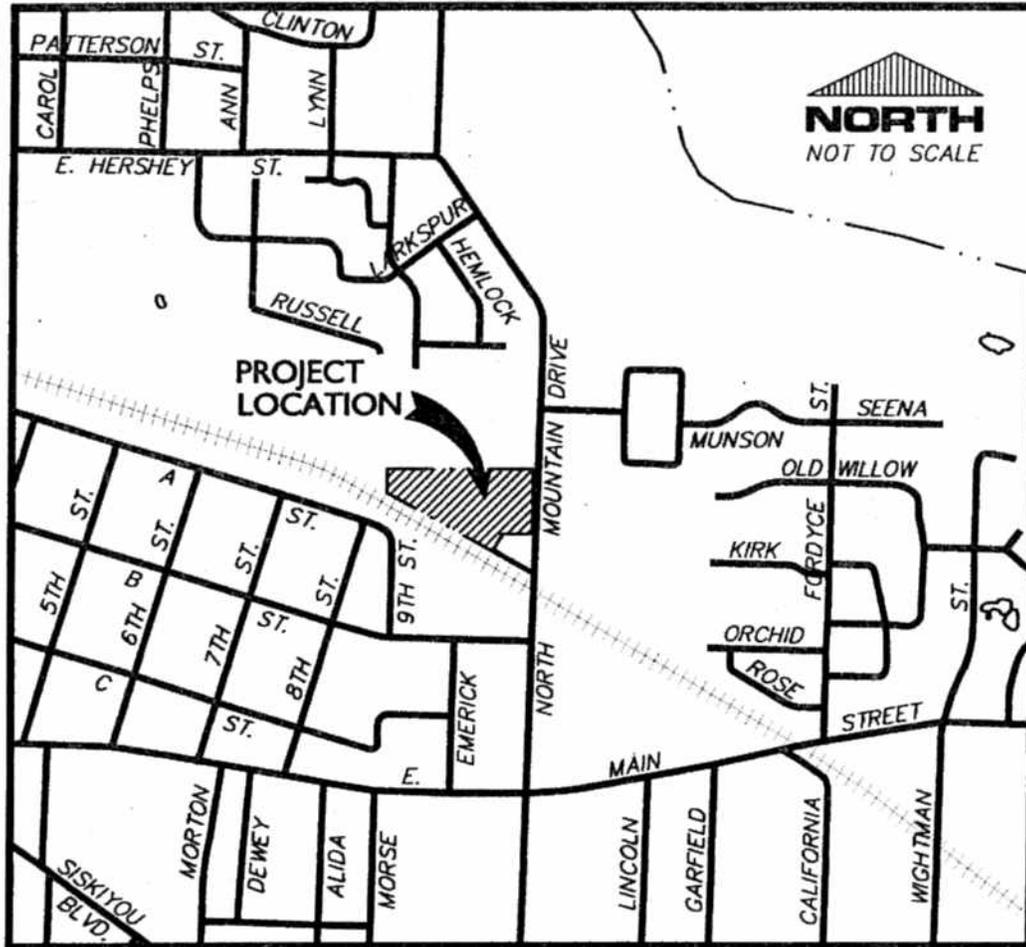
Reference sites are located about ¼ mile upstream on Mountain Creek that illustrate somewhat similar site possibilities. However, our goals are to provide a riparian buffer and wetland system which would be somewhat different than other wetland mitigation projects that were created upstream.

#### **Section 7: Monitoring Plan:**

A monitoring plan will be established to monitor the goals, objectives, and success criteria discussed previously. Overall, hydrology and vegetation survival will be documented from April through September of 2008. Photographic sites will be established to maintain a visual record of the changes in the wetland conditions. Also, flow estimates will be taken during the growing season at the CWM to help estimate monthly flows during the growing season. The monitoring report will be submitted to the DSL and ACE every fall following completion of the project for five years.

#### **Section 8: Protection and Security Instruments:**

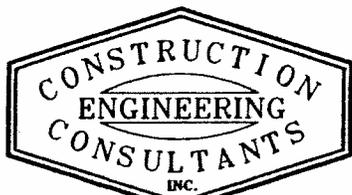
These requirements will be followed as per instructions from DSL and ACE as necessary, and part of the issuance of the permit, and approval of the final mitigation plan (CWM).



VICINITY MAP

REVISED

SEP 18 2001



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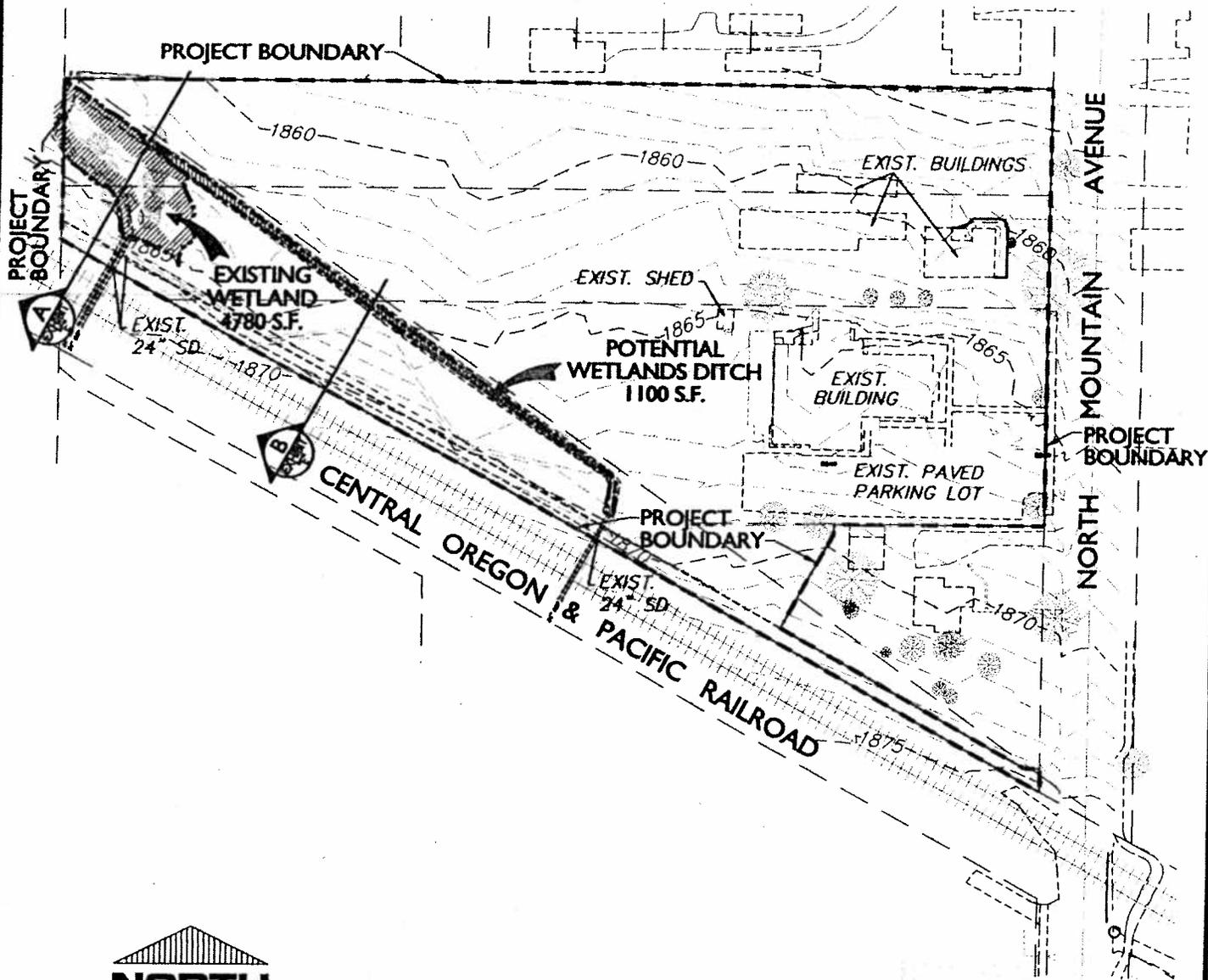
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CITY OF ASHLAND

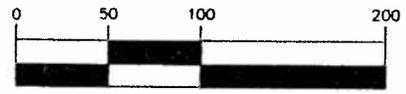
ALEPH SPRINGS SUBDIVISION

SITE VICINITY MAP

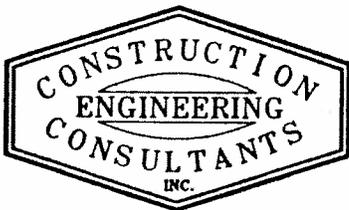
EXHIBIT "A"



GRAPHIC SCALE



( IN FEET )  
1 inch = 100 ft.



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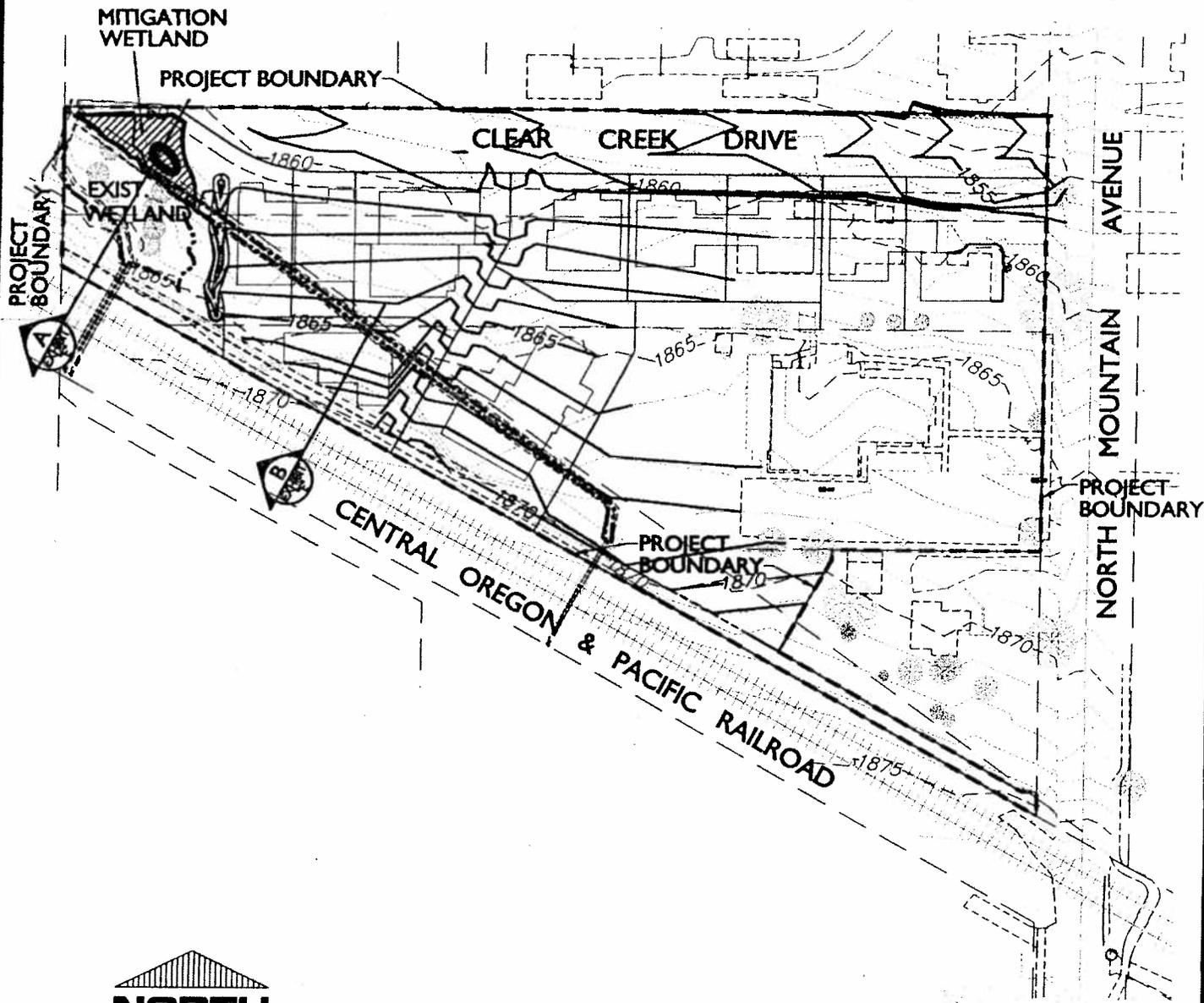
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**CITY OF ASHLAND**

**ALEPH SPRINGS SUBDIVISION**  
**EXISTING CONDITIONS MAP**

*EXHIBIT "B"*





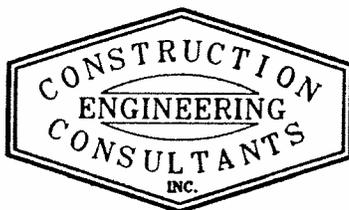
SEP 18 2000



GRAPHIC SCALE



( IN FEET )  
1 inch = 100 ft.



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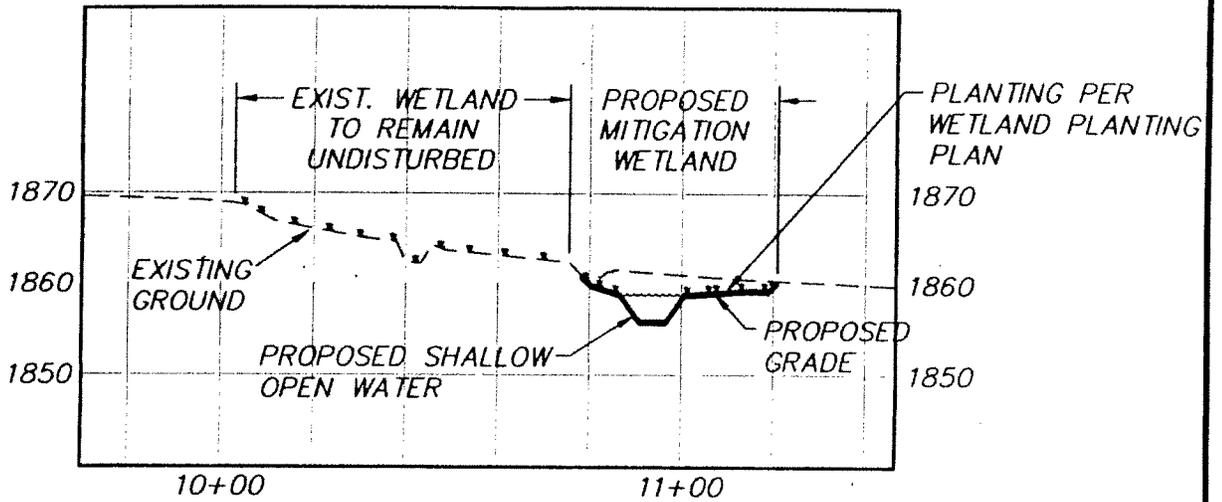
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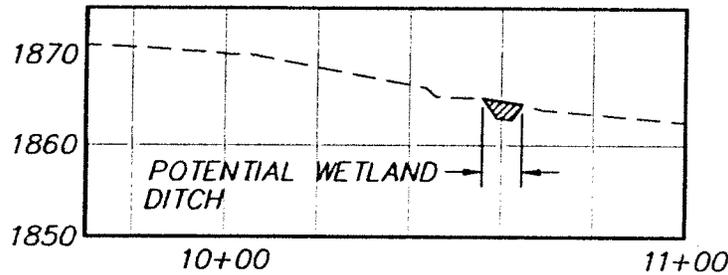
**ALEPH SPRINGS SUBDIVISION**

**GRADING PLAN**

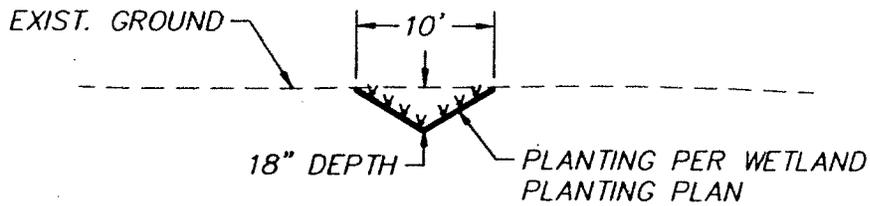
**EXHIBIT "D"**



**WETLAND SECTION** (A)



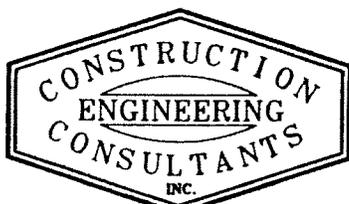
**POTENTIAL WETLAND SECTION** (B)



**BIO-SWALE SECTION** (C)

WETLAND CUT-FILL VOLUMES		
	CUT	FILL
TOTAL SITE	1570 C.Y.	800 C.Y.
WETLAND - NEW	130 C.Y.	0 C.Y.
POTENTIAL WETLAND DITCH	0 C.Y.	109 C.Y.

SCALES  
 HORIZONTAL: 1" = 40'  
 VERTICAL: 1" = 20'



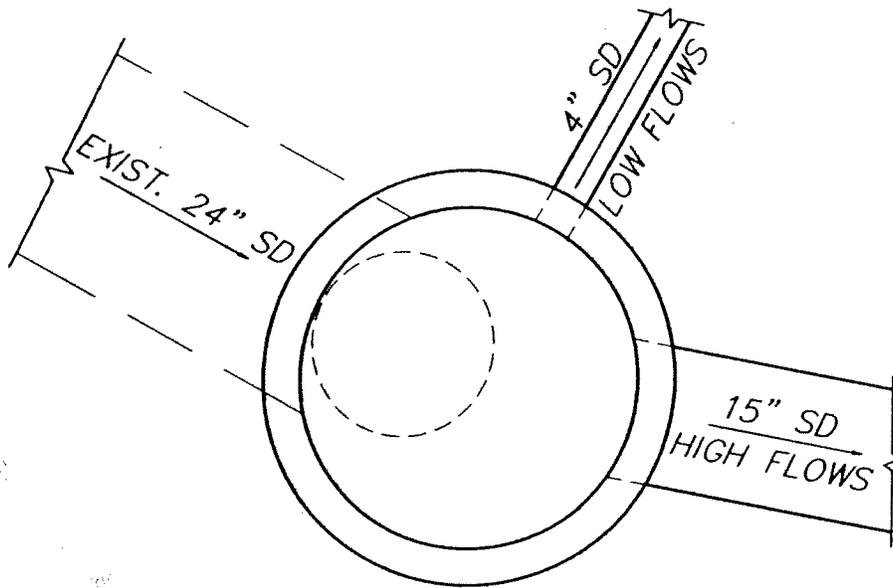
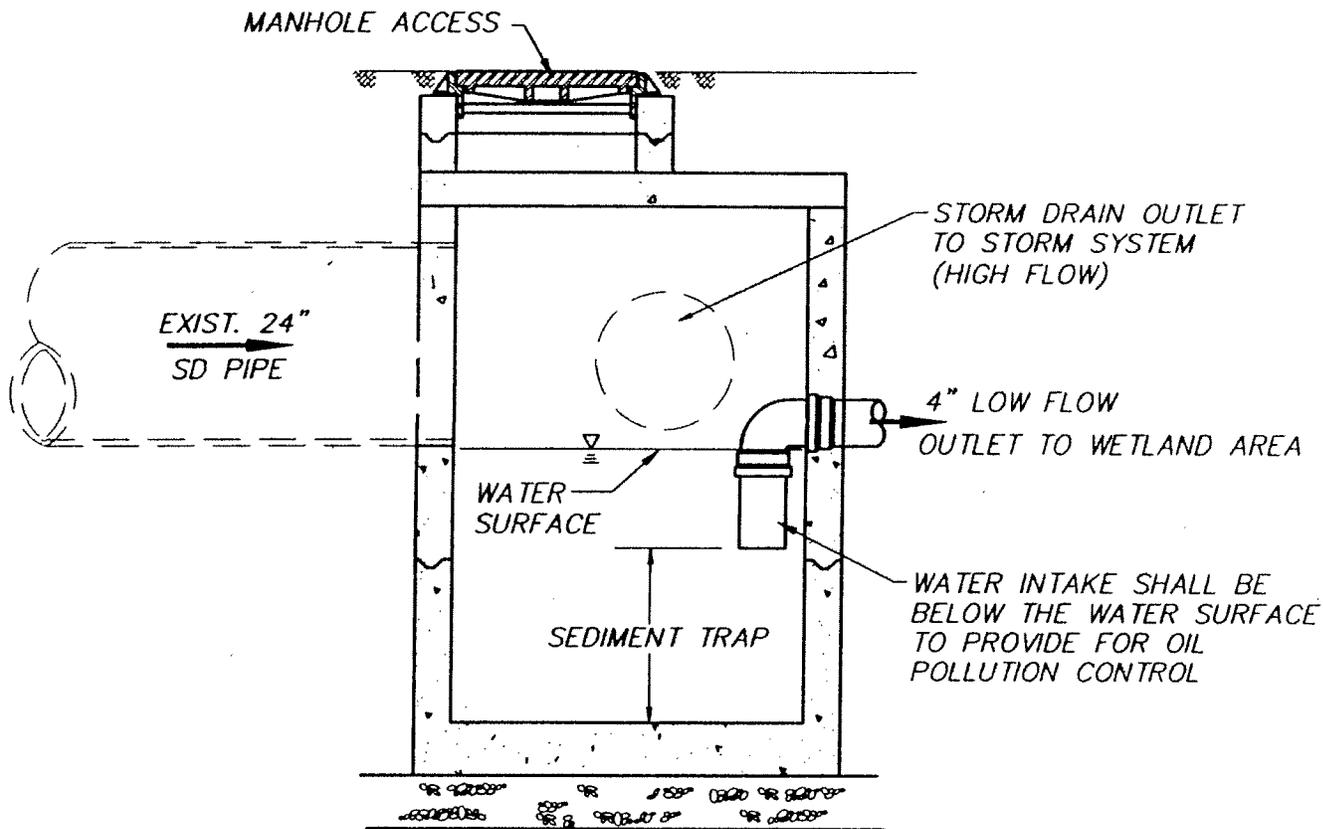
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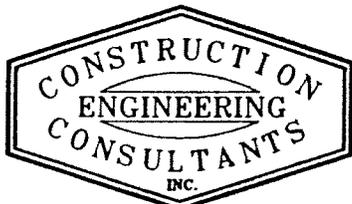
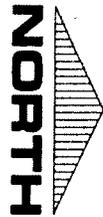
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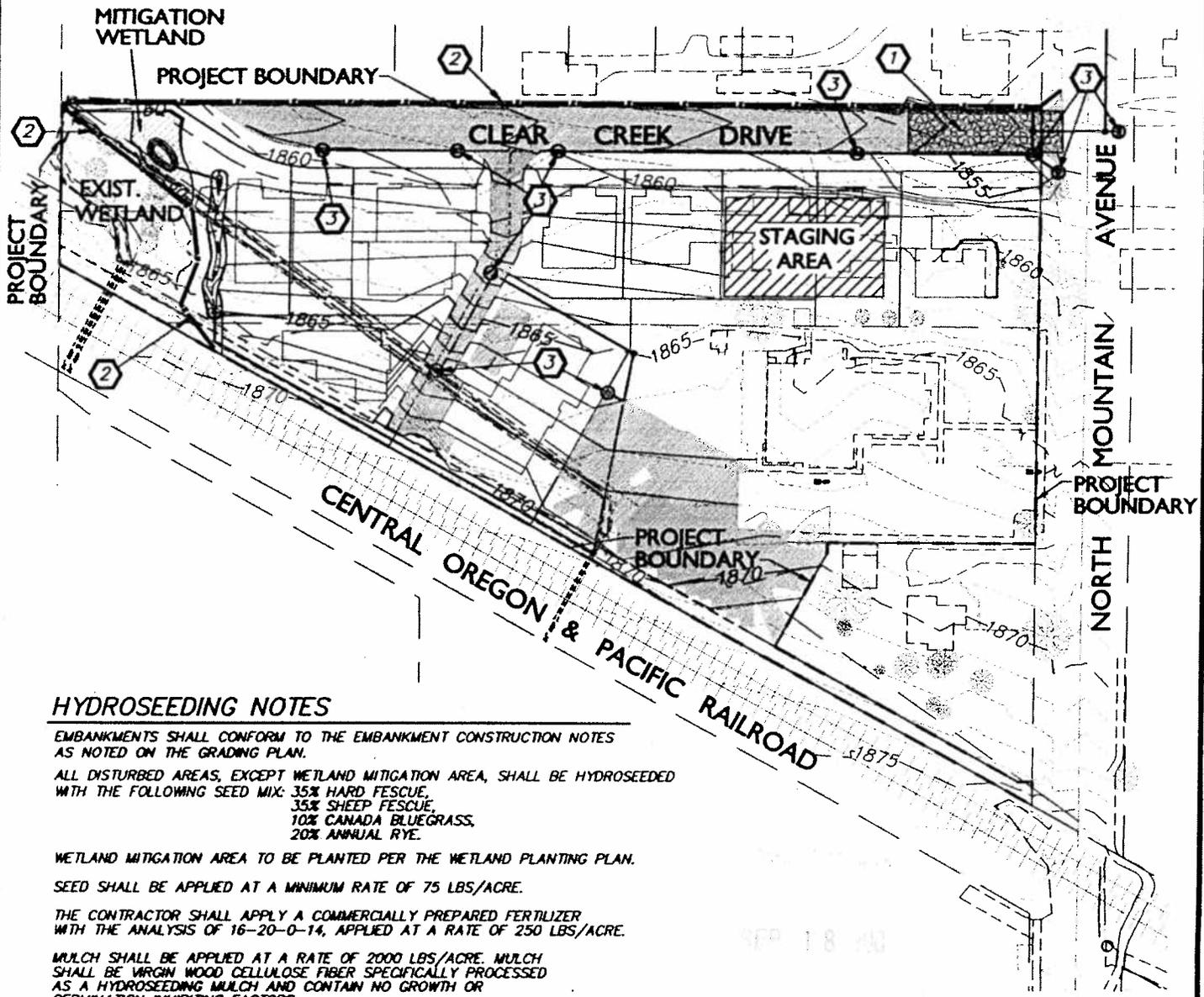
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CONTROL STRUCTURE DETAIL

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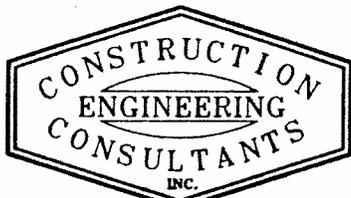
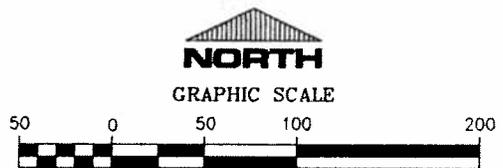
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**HABITAT RESTORATION**  
 324 TERRACE STREET  
 ASHLAND, OR 97520  
 SCOTT ENGLISH  
 PH/FAX (541) 488-1061

**CITY OF ASHLAND**  
**ALEPH SPRINGS SUBDIVISION**  
**EROSION & SEDIMENT CONTROL PLAN**  
**EXHIBIT "G"**

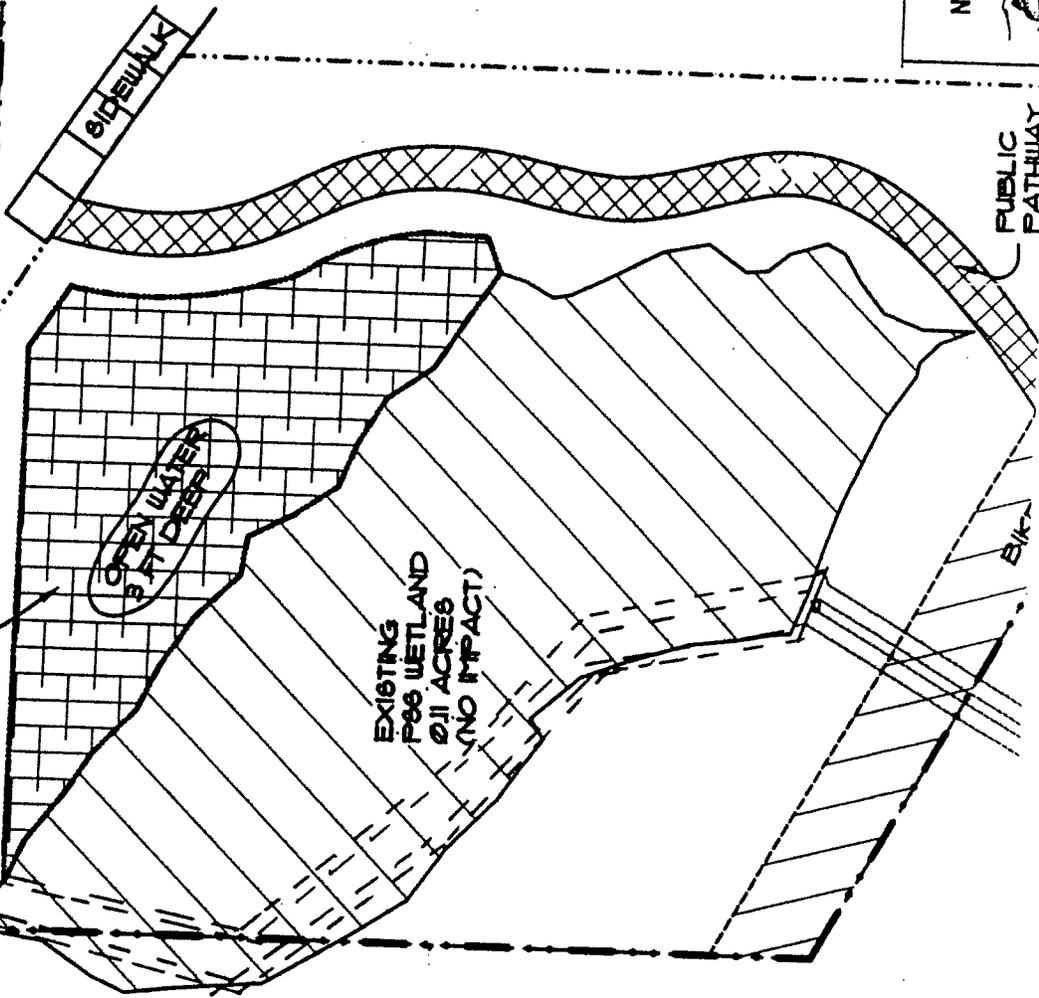


# WETLAND MITIGATION PLANTING PLAN

PROPOSED MITIGATION  
WETLAND - REMCX  
0.5 ACRES  
(SEE ATTACHED PLANTING  
LIST AND NOTES)

OPEN WATER  
3 FT DEEP

EXISTING  
P06 WETLAND  
0.11 ACRES  
(NO IMPACT)



## PLANTING PLAN

### EMERGENT WETLAND PLANTS

- SPREADING RUSH, JUNCUS PATENS (FACU)
- DENSE SEDGE, CAREX DENSA (OEL)
- MONKEY FLOWER, MIMULUS GUTTATUS (OEL)
- SOFT RUSH, JUNCUS EFFUSUS (FACU)
- BULLRUSH, SCIRPUS ACUTUS (OEL)
- CREeping SPICEWASH, ELOCHATIS PALUSTRIS
- ARROWHEAD, SAGGITARIA LATIFOLIA (OEL)

### RIPARIAN BUFFERS

- RED OAK, QUERCUS DOUGLASSII (FACU)
- WILLOW, SALIX SP. (FACU)
- OREGON ASH, FRAXINUS LATIFOLIA
- WHITE ALDER, ALNUS SP.

Northwest Biological Consulting  
Habitat Restoration,  
Environmental Planning  
CA Engineering Contractor  
OR General Contractor  
324 Terrace St. P.O. Box 671 nwbiolog.com  
Ashland, OR 97520 P/N/Fax (541) 488-1061

# EXHIBIT I---CWM PLANTING PLAN

**Table 1: Proposed Wetland Mitigation Planting, Type, Size and Quantity for Aleph Springs**

Common / Scientific Name / Indicator Status	Planting Condition / Container Size	Anticipated Quantity
<b>EMERGENT AND RIPARIAN BUFFER (0.05 acres)</b>		
Red-Osier Dogwood ( <i>Cornus stolonifera</i> , FACW)	1 gallon, bareroot	10
Pacific Crabapple ( <i>Malus fusca</i> , FACW)	1 gallon, bareroot	5
Douglas Hawthorn ( <i>Crataegus douglasii</i> , FAC)	1 gallon, bareroot	3
Spiraea ( <i>Spiraea douglasii</i> , FACW)	1 gallon, bareroot	10
Nootka Rose ( <i>Rosa nutkana</i> , FAC)	1 gallon, bareroot	5
Pacific Ninebark ( <i>Physocarpus capitatus</i> , FACW-)	1 gallon, bareroot	10
Willow ( <i>Salix</i> sp., FACW)	cuttings, live stakes	20
Spreading Rush ( <i>Juncus patens</i> , FACW)	bareroot	50
Dense Sedge ( <i>Carex densa</i> , OBL)	bareroot	50
Small-Fruited Bulrush ( <i>Scirpus microcarpus</i> , OBL)	plugs, bareroot	20
Hard-Stem Bulrush ( <i>Scirpus acutus</i> , OBL)	plugs, bareroot	50
Meadow Barley ( <i>Hordeum prachyantheum</i> , FACW-)	seed	1.0 lb
Spike Bentgrass ( <i>Agrostis exarata</i> , FACW)	seed	.5 lb
Tufted Hairgrass ( <i>Deschampsia cespitosa</i> , FACW)	seed	.5 lb
American Sloughgrass ( <i>Beckmannia syzigachne</i> , OBL)	seed	.5 lb
Northwestern Mannagrass ( <i>Glyceria occidentalis</i> , OBL)	seed	.5 lb
Creeping Spikerush ( <i>Elocharis palustris</i> , OBL)	plugs, bareroot	25
Soft-Stem Bulrush ( <i>Scirpus validus</i> , OBL)	plugs, bareroot	25
Monkey-Flower, Common ( <i>Mimulus gattatus</i> , OBL)	plugs, bareroot	25
Soft Rush ( <i>Juncus effusus</i> , FACW+)	plugs, bareroot	25
Cussick's Sedge ( <i>Carex cusickii</i> , OBL)	plugs, bareroot	25
Slough Sedge ( <i>Carex obtusa</i> , OBL)	plugs, bareroot	25
Water Parsley ( <i>Oenanthe sarmentosa</i> , OBL)	plugs, bareroot	25
Duck Weed ( <i>Lemna</i> sp., OBL)	from stock	200
<b>RIPARIAN BUFFER (250 lineal feet)</b>		
Oregon ash ( <i>Fraxinus latifolia</i> , FACW)	1 gallon, bareroot	5
Douglas Fir ( <i>Pseudotsuga menziesii</i> , FACU)	1 gallon	3
Ponderosa Pine ( <i>Pinus ponderosa</i> , NL)	1 gallon	3
Serviceberry ( <i>Amelanchier alnifolia</i> , FACU)	1 gallon, bareroot	5
Bigleaf Maple ( <i>Acer macrophyllum</i> , FACU)	1 gallon, bareroot	3
Ocean Spray ( <i>Holodiscus discolor</i> , NL)	1 gallon, bareroot	3
Black Current ( <i>Ribes americanum</i> , FAC)	1 to 2 gallon	3
Clustered Wild Rose ( <i>Rosa pisocarpa</i> , FAC)	1 gallon, bareroot	2
Elegant Rose ( <i>Rosa elegant</i> , FACU)	1 gallon, bareroot	2
Red Fescue ( <i>Festuca rubra</i> var. <i>rubra</i> , FAC+)	seed	.5 lb
Blue Wildrye ( <i>Elymus glaucus</i> , FAC+)	seed	.5 lb
California Brome ( <i>Bromus</i> sp., FAC+)	seed	.5 lb
Common Yarrow ( <i>Achillea millefolium</i> , FACU)	seed	.25 lb
California Poppy ( <i>Eschscholzia californica</i> , NL)	seed	.5 lb
Lupine ( <i>Lupinus</i> sp., FAC, FACU)	seed	.5 lb

## PLANTING NOTES:

1. All the plants are native to Southern Oregon and Are frequently found in wetland and riparian habitat within the Ashland area.
2. Soil from the excavated mitigation area will be salvaged and placed back into the mitigation area prior to planting.
3. The 0.04 acre upland will be grubbed by small equipment to remove patches of blackberries, and other noxious weeds.
4. A variety of native emergent wetland plants will be planted in the wetland area cleared of noxious vegetation.
5. Erosion control measures and other BMPs will be in place all during the construction of the created wetlands.
6. In general, the emergent wetland species will be planted on 2 foot centers. The shrubs will be planted along an edge every 5 feet or so, and the trees will be spaced every 10 to 15 feet along the riparian and upland edges.
7. The seeds will be scattered around the wetland, riparian, and upland areas in the fall.
8. Wetland hydrology will be provided by grading to access the subsurface water table and additional water will be supplied by piping the irrigation return water and treated storm drain water into the created wetland.
9. A drip irrigation water system will be constructed to maintain the riparian and upland plants installed along the edges of the mitigation wetland..
10. Please refer to **attached figures** for more details.

2011-10-19

2011-10-18

2011-10-18

Havurah Shir Hadash  
חבורת שיר חדש  
P.O. Box 1262  
Ashland, OR 97520



RECEIVED

OCT 29 2007

City of Ashland  
Field  Office  Court

<http://www.havurahshirhadash.org>  
shalomrav@aol.com or (541) 621-0782

To: Ashland Planning Commission  
Date: October 26, 2007  
Re: Aleph Springs project

Dear Commission Members:

I am the rabbi and spiritual leader of the synagogue known as Havurah Shir Hadash, located at 185 North Mountain Avenue in Ashland since 1998.

I am writing in the hope that you will look favorably on all the details of the Aleph Springs proposal which will encompass land on three sides of our current synagogue, playground, and parking area. Following are the reasons why this project has been wholeheartedly supported by members of our synagogue in two community-wide meetings held over the last year and a half.

- The current plan will have enormous benefits to both our community and to the "community" of homes that are proposed to be built;
- Under the masterful supervision of Larry Medinger the plan calls for a very green campus that will allow Havurah families and Aleph Springs residents to interact in mutually beneficial ways;
- Compared with the original plan, the current plan is environmentally friendly, and allows for an "open campus" feel for both members and residents;
- The exquisite design of the Aleph Springs project allows for maximum interaction of people of all ages while serving the unique needs of the elders.

With the development of the land around our synagogue I certainly realize that we will no longer have a quaint, rural feel to our property, but I cannot imagine a better use of the land, and a more mutually beneficial development than the one proposed. If you have any questions, please do not hesitate to call me. on my cell phone at 621-0782.

Shalom,

Rabbi David Zaslow

PA-2007-01398  
203 MTN AV N

**Francene Orrok PhD & G Timothy Orrok PhD**  
**1030 Ivy St. Ashland OR 97520 541 488-2495 [franorg@zintech.org](mailto:franorg@zintech.org)**

**26 October, 2007**

Planning Department  
City of Ashland  
51 Winburn Way  
Ashland OR 97520

Re: Aleph Springs

After twenty years at 1030 Ivy Lane, we are ready to move off the hill, downsize, and be closer to town. We are proud to be part of a project that supports the Ashland values and spirit that have been such an important part of our lives, a plan that is both environmentally friendly and encourages a sense of community. A plan that extends one's living space beyond the boundaries of an isolated small fenced lot.

We are especially attracted to the plan making full use of Southern exposure, providing sunny backyards and warm, light-filled living during the day. The shared common walls increase energy efficiency, decrease Western exposure, and allow for more green space. The open expansive green spaces, including a community garden, promote interaction between residents and Havurah members, between elders and children, and encourage healthy caring concern for each other.

As a bonus, we're just off the bike path and can even walk downtown. This plan is a win-win for us, for the Havurah, and for Ashland.

Sincerely



G Timothy Orrok



Francene Orrok

**RECEIVED**  
OCT 26 2007  
**City of Ashland**

Stanley Anthony Shulster

Katheryn J. McElrath

*Bella Vista Vineyard*

165 Pilot View Drive,

Ashland, Oregon 97520

[541] 552-9999

stan@ashlandwireless.net

Dear Planning Commission,

Re: Aleph Springs

I am not a member of Havurah Shir Hadash, but I am one of the founding members of the Havurah Friends Investment Group (HFIG).

This community, when built and underway will be a great asset to the community of Ashland. I am very excited about the possibility of older folks living in a safe, comfortable and beautiful setting. One of the main benefits will be that the school aged children will have a chance for a close relationship with seniors. A child does an art project, or learns a song, or bakes some cookies.....that child can, without getting into a car, share these things with seniors who are on the same "campus". I know lots of kids at the Havurah School will adopt a grandma or grandpa from among the Aleph Springs residents. It is vital that children integrate with elders and learn to love and accept them, even if their dental plates may slip or their wigs are off kilter. The elders will have a chance to share a meal with the kids, to show them skills and to teach kids from their wealth of life experiences.

Ashland will be proud of this project. I expect that, to some extent, the artificial social barriers between children and elders will be decreased. The lives of elders will be much enriched by the bonds they form with the children of the congregation. The seniors and children can be from any social, racial or religious background.

I urge you to grant approval for this project.

Sincerely,



Stanley Anthony Shulster

**RECEIVED**

NOV 5 2007

City of Ashland

Field  Office  Court

**PA #2006-01091 SUBDIVISION FINDINGS – FOR REFERENCE**  
**BEFORE THE PLANNING COMMISSION**  
**September 12, 2006**

IN THE MATTER OF PLANNING ACTION #2006-01091, A REQUEST FOR )  
AN OUTLINE PLAN APPROVAL UNDER THE PERFORMANCE STANDARDS )  
OPTIONS CHAPTER 18.88 FOR A 13-UNIT SINGLE-FAMILY RESIDENTIAL ) **FINDINGS,**  
SUBDIVISION FOR THE PROPERTY LOCATED AT 203 N. MOUNTAIN AVE. ) **CONCLUSIONS**  
A TREE REMOVAL PERMIT IS REQUESTED TO REMOVE A 40-INCH ) **AND ORDERS**  
DIAMETER AT BREAST HEIGHT BLACK WALNUT TREE IN THE )  
N. MOUNTAIN AVE. RIGHT-OF-WAY. )  
)  
)

**APPLICANT:** Urban Development Services, LLC

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**RECITALS:**

- 1) Tax lot 1500 of 39 1E 09AA is located at 203 N. Mountain Ave. and is zoned R-1-5-P Single-Family Residential. Tax lot 1701 of 39 1E 09AA is located at 185 N. Mountain Ave., and is zoned R-1-5-P Single-Family Residential. Tax lots 1600 and 1800 of 39 1E 09AA are vacant and without addresses, and are zoned R-1-5-P Single-Family Residential.
- 2) The applicant is requesting Outline Plan approval for a 13-unit single-family residential subdivision under the Performance Standards Options. The application includes a Tree Removal Permit to remove a 40-inch diameter at breast height Black Walnut tree in the N. Mountain Ave. street right-of-way. Site improvements are outlined on the plans on file at the Department of Community Development.
- 3) The criteria for Outline Plan approval under the Performance Standards Options are described in Chapter 18.88 as follows:
  - a) *That the development meets all applicable ordinance requirements of the City of Ashland.*
  - b) *That adequate key City facilities can be provided including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity.*
  - c) *That the existing and natural features of the land; such as wetlands, floodplain corridors, ponds, large trees, rock outcroppings, etc., have been identified in the plan of the development and significant features have been included in the open space, common areas, and unbuildable areas.*
  - d) *That the development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan.*

*e) That there are adequate provisions for the maintenance of open space and common areas, if required or provided, and that if developments are done in phases that the early phases have the same or higher ratio of amenities as proposed in the entire project.*

*f) That the proposed density meets the base and bonus density standards established under this Chapter.*

4) The criteria for a Tree Removal Permit are described in Chapter 18.88.050.F as follows:

*A. Hazard Tree: The Staff Advisor shall issue a tree removal permit for a hazard tree if the applicant demonstrates that a tree is a hazard and warrants removal.*

*1. A hazard tree is a tree that is physically damaged to the degree that it is clear that it is likely to fall and injure persons or property. A hazard tree may also include a tree that is located within public rights of way and is causing damage to existing public or private facilities or services and such facilities or services cannot be relocated or the damage alleviated. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard or a foreseeable danger of property damage to an existing structure and such hazard or danger cannot reasonably be alleviated by treatment or pruning.*

*2. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.*

*B. Tree that is Not a Hazard: The City shall issue a tree removal permit for a tree that is not a hazard if the applicant demonstrates all of the following:*

*1. The tree is proposed for removal in order to permit the application to be consistent with other applicable Ashland Land Use Ordinance requirements and standards. (e.g. other applicable Site Design and Use Standards). The Staff Advisor may require the building footprint of the development to be staked to allow for accurate verification of the permit application; and*

*2. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks; and*

*3. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property.*

*The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone. Nothing in this section shall require that the residential density be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that*

*would lessen the impact on trees, so long as the alternatives continue to comply with other provisions of the Ashland Land Use Ordinance.*

*4. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to AMC 18.61.084. Such mitigation requirements shall be a condition of approval of the permit.*

5) The Planning Commission, following proper public notice, held a public hearing on September 12, 2006 at which time testimony was received and exhibits were presented. The Planning Commission approved the application subject to conditions pertaining to the appropriate development of the site.

Now, therefore, The Planning Commission of the City of Ashland finds, concludes and recommends as follows:

## SECTION 1. EXHIBITS

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

## SECTION 2. CONCLUSORY FINDINGS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the Staff Report, public hearing testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal to develop a 13-unit single-family residential subdivision meets all applicable criteria for an Outline Plan approval described in the Performance Standards Options Chapter 18.88, and that the proposed removal of a 40-inch diameter at breast height Black Walnut tree in the N. Mountain Ave. street right-of-way meets all applicable criteria for a Tree Removal Permit in Chapter 18.61.

2.3 The Planning Commission finds that adequate key City facilities can be provided to serve the project including water, sewer, paved access to and through the development, electricity, urban storm drainage, police and fire protection and adequate transportation; and that the development will not cause a City facility to operate beyond capacity. Water, sanitary sewer, and electric services are available from N. Mountain Ave. and will connect through the site in the

new street serving the project. Storm drain facilities will be extended from Village Green Dr. to N. Mountain Ave., and then south in N. Mountain Ave., and through the new street and alley system.

Paved access is provided by N. Mountain Ave, as well as by the proposed new street running through the site. The new street will be built as a "half-street" improvement. The easterly 70 feet of the street will include a two travel lanes, and a parkrow and sidewalk on the south side of the street. The remaining 410 feet will include two travel lanes, parking on the south side of the street, and a parkrow and sidewalk on the south side of the street.

The City of Ashland Transportation Plan Map was updated in June 2002 to include the conceptual street locations and dedications for the railroad property area including the subject site. The plan identifies a collector street connecting N. Mountain Ave. to Oak St. The eastern end of the collector street is on the subject site. Going from east to west, the street will cross over Mountain Creek, traverse the railroad property and connect to the existing end of Clear Creek Drive. The location of the street in the proposed project conforms to the adopted Transportation Plan Map. In addition, the new street is designed to a Neighborhood Commercial Collector standard. The Neighborhood Commercial Collector standard is consistent with the existing west end of the street, Clear Creek Drive.

The street system serving the project includes a u-shaped alley that provides access to 11 of the 13 homes. The alley reduces street access points from the project on the new street to three points, thereby addressing safety and traffic flow impacts that would result from having 13 individual driveways on the new collector street. Traffic flow impacts on the new street area a safety issue because in the future when the area is built out the new collector street will accommodate a higher level of vehicle trips per day. The alley reduces the number of access points on the street which reduces the potential vehicular conflicts and creates a safer pedestrian corridor.

The Street Standards require a bicycle lane on a Neighborhood Commercial Collector street when there will be more than 3,000 vehicle trips per day or actual motor vehicle travel speeds in excess of 25 mph. Approximate projections show that when the railroad property west of the subject site is fully developed, the trips per day on the new east-west collector connecting N. Mountain Ave. to Oak St. will be at or exceed 3,000 vehicle trips per day. As a result, the new street included in the application should have bicycle lanes. However, when the west end of the street was developed as Clear Creek Drive, a decision was made to have a multi-use bicycle and pedestrian path on the north side of the railroad tracks instead of installing bicycle lanes on Clear Creek Drive. As a result, a ten-foot wide easement was established from the alley off of the south side of Clear Creek Drive along the full length of the railroad property. This existing multi-use path easement ends on the western boundary of the subject site. The project continues this easement from the current end point through to N. Mountain Ave. The Planning Commission finds that in lieu of installation of a bicycle lane in the street, the ten to 12-foot wide public pedestrian easement along the sites southern boundary for a future multi-use path will

provide adequate bicycle facilities. Additionally, a multi-use path through the open space adjacent to the wetlands will connect the path adjacent to the railroad tracks to the subdivision and the new street. The approved Site Plan date stamped September 11, 2006 delineates the path through the wetland open space as public, and the applicant testified at the public hearing on September 12, 2006 for PA 2006-01091 that a public pedestrian easement would be granted for this same path. The public path connection between the multi-use path adjacent to the railroad tracks and the new street is consistent with Connectivity standard 9 in the Ashland Street Standards Required Street Layout and Design Principles which require "Where conditions preclude a street connection, a continuous nonautomotive connection in the form of a multi-use path or trail shall be provided."

2.4 The Planning Commission finds that development of the land will not prevent adjacent land from being developed for the uses shown in the Comprehensive Plan. The parcels to the north of the site are similar to 203 N. Mountain Ave. in that the zoning is Single-Family Residential (R-1-5), and the lots are configured as long narrow parcels with homes situated at the east end of the lots and fronting on N. Mountain Ave. There is developable area on these lots behind the homes at the rear of the parcels. As a result, the area between the subject site and the southern edge of the Mountain Creek Estates Subdivision (located at the southwest corner of the intersection of N. Mountain Ave. and Hersey St.) has development potential. Additionally, the property to the west of Mountain Creek has development potential. The area west of Mountain Creek is zoned Employment (E-1), and the bulk of the area is a 19.53 acre parcel owned by Union Pacific Railroad Company. This site is commonly referred to as the railroad property. The lots across from the site on N. Mountain Ave., on the east side of the street, are zoned Single-Family Residential (R-1-5) and are largely developed except for a parcel directly north of the railroad right-of-way and tracks. The large parcel is approximately ten acres in size, is not in the city limits and has a residence with several accessory buildings located on the lot. Finally, the area to the south of the site and south of the railroad right-of-way and tracks is zoned High Density Multi-Family (R-2) and Employment (E-1). This area includes a mixture of multi-family developments, single-family homes and a public works yard operated by the City of Ashland.

The primary issue considered by the Planning Commission in evaluating the project was that the proposed development provide for the connection and coordination of the future street system and utilities to serve the developable areas to the north and west of the subject site. The City of Ashland Transportation Plan Map was updated in June 2002 to include the conceptual street locations and dedications for the railroad property area including the subject site. The application has based the street layout and type on the adopted Transportation Plan Map, which coordinates with a street system extending north and west to serve the developable areas. In addition, the application states that the "utility lines (water, sewer, storm, electric, etc.) will be upsized, in cooperation with the City, in order to accommodate future capacity needs."

2.5 The Planning Commission finds the density meets the base density standards established under the Performance Standards Options for the Single-Family Residential (R-1-5-P) zone. The property's R-1 zoning designation and lot area of 2.57 acres permit a base density of 11.57 units

(2.57 acres \* 4.5 units/acre = 11.57 units). The project will use the conservation density bonus to increase the number of units to 13 (11.57 units base density x 1.15 conservation density bonus = 13.30 units).

2.6 The Planning Commission finds that the significant natural features on the property are located on the western end of the site and include a section of Mountain Creek, and the associated wetland and trees. The section of Mountain Creek and the associated wetland and trees adjacent to the creek will be preserved in an open space for the subdivision. Mountain Creek is identified as a Riparian Land Drainage on the adopted Comprehensive Plan maps.

The development is required to provide five percent of the total site area in Open Space. The open space area at the western end of the property is approximately a third of an acre in size and 11.6 percent of the total site area.

The building envelope for the lot closest to the wetlands, Lot 11, shows the building footprint on the side (west) property line. The west property line of Lot 11 is shown at 17 feet from the closest point in the wetland. Since the envelopes are relatively narrow, the actual building footprint will likely correspond to the building envelope. The Planning Commission finds that the western side yard setback is needed on Lot 11 to create an additional buffer to the wetlands from the residence for three reasons. The construction activities tend to take place well outside the actual building footprint which could impact the wetland area. Second, in the long run, the owner of Lot 11 will need space to maintain the side of the home. The lot configuration may lend itself to Lot 11 using the open space as de facto yard area. This opens the door to more possible infringement on the wetland area. Finally, since the wetland is defined by a draft delineation, there is a possibility that the wetland boundaries may change and potentially be closer to Lot 11. A condition has been added requiring the Final Plan document to include a minimum of six feet for the west side yard for Lot 11 to provide additional buffer from the eastern edge of the wetland to the footprint of the residence.

2.7 The Planning Commission finds that the development meets all applicable ordinance requirements of the City of Ashland with the attached conditions of approval. The Site Plan dated September 11, 2006 delineates the proposed building envelopes, setbacks, solar setbacks and driveway locations. The setbacks on the perimeter of the subdivision and for the front yards are required to meet the standard setback requirements of the Single-Family Residential zoning district, and the proposal meets this requirement.

The driveway aprons are separated by 24 feet as required in the street standards. The proposed subdivision meets the on-site parking requirements. In addition to the two off-street parking spaces that are required for each unit, one guest space is provided on the street for each unit. There are parking spaces available on the new street and the N. Mountain Ave. frontages. Additionally, Lots 12 and 13 will each provide a third guest space on the lots as required in Chapter 18.76 for flag lots.

New residential construction in newly created lots is required to meet Solar Setback Standard A in accordance with 18.70.050. Chapter 18.70 requires that newly created lots with slopes less than 15 percent to be configured so that the future homes will meet Solar Setback A. Solar Setback A is the most stringent standard which requires that new structures can not shade the property to the north more than a six foot fence would at the north property line. The solar setbacks for Lots 1 – 11 appear to meet Solar Setback Standard A, but because the calculation is dependent on the final home design including the height of the shadow producing point and the distance of the point from natural grade. As a result, the final determination is made at the building permit submittal.

The Planning Commission finds that the request to use a less demanding solar setback standard for Lots 12 and 13 is not supported by the evidence in the record. The application describes a “solar envelope” for Lots 12 and 13 which is delineated on the site plan. The proposed envelope would allow the shadowing of the garages, rear yards and residences on Lots 7 – 10. The Commission finds that the variance to the solar setback standard will substantially diminish the solar access of the habitable structures on Lots 7-10, and there are not unique or unusual circumstances which apply to this site that do not typically apply elsewhere. In addition to the north-south dimensions of the lots, the alley to the north of Lots 12 and 13 provides an additional 16 feet of right-of-way that could be shaded.

2.8 The Planning Commission finds the request for a Tree Removal Permit for one tree adjacent to the site, a 40-inch dbh Walnut, in the N. Mountain Ave. right-of-way near the southeast corner of the property meets the approval criteria in 18.61.080. The arborist report identifies the tree as in poor condition and describes the species as having a poor tolerance to construction. The application goes on further to say that the poor condition of the walnut tree is most likely due to continuous topping for overhead electric lines. The written findings identify the walnut tree as a future hazard tree due to the poor condition and proximity to the street and sidewalk. The removal of the walnut tree will be mitigated with an appropriate tree from the Recommended Street Tree Guide.

### SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal to develop a 13-unit single-family residential subdivision is supported by evidence contained within the record.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #2006-01091. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #2006-01091 is denied. The following are the conditions and they are attached to the approval:

- 1) That all proposals of the applicant are conditions of approval unless otherwise modified here.

- 2) That all easements for sewer, water, electric and streets shall be indicated on the final survey plat as required by the City of Ashland.
- 3) That a drainage way easement shall be indicated on the final survey plat for the width and length of the western open space area including Mountain Creek and the associated wetlands.
- 4) That the boundaries of the wetland and the western property line of Lot 11 shall be delineated on site, and inspected by the Ashland Planning Division prior to the Final Plan approval.
- 5) That the street right-of-way for the undeveloped western end of the street shall be dedicated as public right-of-way and if determined necessary by Ashland Engineering Division, a street plug shall be established at the western boundary of the street right-of-way.
- 6) That a public pedestrian and bicycle easement, a minimum of ten feet in width, shall be indicated on the final survey plat for future development as a multi-use path parallel to the railroad right-of-way and along the length of the southern boundary of the site, connecting the western site boundary to N. Mountain Ave. In addition, a public pedestrian and bicycle easement shall be provided linking the subject development to the future multi-use path.
- 7) That engineering for the utility plan including but not limited to the water, sewer, storm drainage and electric facilities shall be submitted with the Final Plan application. The utility plan shall include the location of connections to all public facilities in and adjacent to the development, including the locations of water lines and meter sizes, fire hydrants, sewer mains and services, manholes and clean-outs, storm drainage pipes and catch basins, and locations of all primary and secondary electric services including line locations, transformers (to scale), cabinets, meters and all other necessary equipment. Transformers and cabinets shall be located in areas least visible from streets, while considering the access needs of the Electric Department. Any required private or public utility easements shall be delineated on the utility plan.
- 8) That the Electric Distribution Plan shall be coordinated with the Ashland Electric Department, and shall be included in the utility plan with the Final Plan application.
- 9) That the Tree Protection Plan shall be revised to be coordinated with the final utility plan, and shall be submitted with the Final Plan application.
- 10) That if the storm drainage plan includes on-site storm water detention systems and/or off-site storm drain system improvements, the engineering shall be submitted with the Final Plan application. The permanent maintenance of on-site storm water detention systems must be addressed through the obligations of the Homeowners' Association and approved by the Public Works Department and Building Division.
- 11) That the impact of redirected storm drainage to the wetland shall be evaluated at the Final Plan application. Adequate water flow shall be maintained to retain the current size wetland.

- 12) That a grading plan addressing general areas of cut and fill shall be submitted with the Final Plan application.
- 13) That the required pedestrian-scaled streetlight shall consist of the City of Ashland's residential streetlight standard, and shall be included in the utility plan and engineered construction drawings for the street improvements submitted with the Final Plan submittals.
- 14) The engineering for proposed street improvements shall be provided at Final Plan application. The engineering drawings shall address the treatment of the northern edge of the street and direction of storm drainage, and address the necessary curb radius and travel lane width for truck traffic use.
- 15) That the Final Plan application shall delineate vision clearance areas at the intersections of streets and alleys throughout the project in accordance with 18.92.070.D. Structures, signs and vegetation in excess of two and one-half feet in height shall be placed in the vision clearance areas.
- 16) Subdivision infrastructure improvements, including but not limited to utilities, public streets, street trees and irrigation and open space landscaping and irrigation shall be installed or an irrevocable letter of credit posted for the full cost of construction prior to signature of the final survey plat. If an irrevocable letter of credit is posted for common area and open space improvements, the Final Plan application shall include a phasing plan for the common area and open space improvements including but not limited to landscaping, irrigation and pathway improvements. The project landscape architect shall inspect the common area and open space improvements for conformance with the approved plan, and shall submit a final report on the inspection and items addressed to the Ashland Planning Division. The phasing plan shall include a schedule for a final inspection including the project landscape architect with the Ashland Planning Division of the common areas and open spaces prior to issuance of the ninth building permit.
- 17) That the street name shall be reviewed and approved by Ashland Engineering for compliance with the City's resolution for street naming.
- 18) That the final wetland determination/delineation report shall be prepared and submitted with the Final Plan application, and the necessary state and federal permits received prior to the Final Plan application. If the final wetland determination/delineation report submitted for state and federal review differs significantly from the preliminary determination (i.e. larger area or numerous wetland areas), the Outline Plan shall be modified prior to an application for Final Plan approval.
- 19) That the recommendations of the Ashland Tree Commission, with final approval by the Staff Advisor, shall be incorporated into the Tree Protection Plan submitted with the Final Plan submittals.

- 20) That one tree shall be planted in the parkrow in the N. Mountain Ave. right-of-way in accordance with 18.61.084 as mitigation for the removal of the 40-inch walnut. The landscaping plan provided at the time of the Final Plan application shall include and identify the mitigation trees.
- 21) That a Verification Permit in accordance with 18.61.042.B shall be applied for and approved by the Ashland Planning Division prior to removal of the walnut tree and prior to site work, storage of materials and/or the issuance of an excavation or building permit. The Verification Permit is to inspect the tree to be removed and the installation of the tree protection fencing. The tree protection for the trees to be preserved shall be installed according to the approved Tree Protection Plan prior to site work or storage of materials. Tree protection fencing shall be chain link fencing a minimum of six feet tall and installed in accordance with 18.61.200.B.
- 22) That a size and species specific landscaping plan for the parkrows and open spaces shall be provided at the time of the Final Plan application. The western open space shall include a north-south path connecting the future multi-use path parallel to the railroad right-of-way and the new street and improved with asphalt, concrete or a comparable all weather surface.
- 23) That street trees, located one per 30 feet of street frontage, shall be installed in the parkrow along street frontages as part of the subdivision infrastructure improvements. Street trees shall be chosen from the Recommended Street Tree List and shall be installed in accordance with the specifications noted in the Recommended Street Tree List. The street trees shall be irrigated.
- 24) Fence heights within side and rear yard areas adjoining the open space shall not exceed four feet. Stipulations with regards to fencing shall be described in the project CC&R's.
- 25) That a draft copy of the CC&R's for the Homeowners Association is provided at the time of Final Plan application. CC&R's shall describe responsibility for the maintenance of all common area and open space improvements, parkrows and street trees. CC&R's shall note that any deviation from the Tree Protection Plan must receive written approval from the City of Ashland Planning Department.
- 26) That the Final Plan application shall demonstrate compliance with a maximum lot coverage of 50 percent by either using a total site area calculation or including lot coverage calculations in square footage and percentage for each lot.
- 27) That the Final Plan application shall include a minimum of six feet for the west side yard for Lot 11.
- 28) The setback requirements of 18.88.070 shall be met and identified on the building permit submittals including but not limited to the required width between buildings as described in 18.88.070.D.
- 29) That all new structures on Lots 1 – 13 shall meet Solar Setback A in accordance with Chapter

18.70 of the Ashland Land Use Ordinance. Solar setback calculations shall be submitted with each building permit and include the required setback with the formula calculations and an elevation or cross-section clearly identifying the height of the solar producing point from natural grade.

- 30) Individual lot coverage calculations including all impervious surfaces shall be submitted with the building permits. Pervious driveway and parking areas shall be counted as impervious surfaces for the purpose of lot coverage calculations.
- 31) That the Final Plan submittals shall address the fire access, turnaround and fire hydrant requirements of Chapter 18.76.
- 32) That the Tree Protection Plan shall be revised to include the trees within 15 feet of the site, and shall be submitted with the Final Plan application.
- 33) That the electric transformer shall not be located in the parkrow as shown the application Site Plan date stamped September 11, 2006, and shall be addressed in the utility plan for the Final Plan application.
- 34) That the flag lot lines shall meet the requirements of 18.76.060, and shall be addressed in the Final Plan application.

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Planning Commission Approval

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Date

**PA #2006-01091 TREE COMMISSION RECOMMENDATIONS – FOR REFERENCE**

ASHLAND TREE COMMISSION  
PLANNING APPLICATION REVIEW  
September 7, 2006

**PLANNING ACTION 2006-01091** Outline Plan approval under the Performance Standards Options Chapter 18.88 to subdivide the property into 14 lots including 13 lots for single-family homes and one lot for open space purposes for the property located at **203 N. Mountain Ave.** A Lot Line Adjustment is included in the proposal to incorporate the western end of the parcel located at **185 N. Mountain Ave** (Havurah Shir Haddash Jewish Temple) into the subdivision. A Tree Removal Permit is requested to remove a 40-inch diameter at breast height Black Walnut tree in the N. Mountain Avenue street right-of-way adjacent to the front of the property located at **203 N. Mountain Ave.** **COMPREHENSIVE PLAN DESIGNATION:** Single Family Residential (P Overlay); **ZONING:** R-1-5-P; **ASSESSOR'S MAP #** 39 1E 09 AA; **TAX LOTS:** 1500, 1600, 1701, 1800. **APPLICANT:** Urban Development Services, LLC

Recommendation:

- 1) Tree Commission recommends using pervious paving techniques for driveway on Lot 3 under dripline of English walnut.
- 2) Minimize driveway width on Lot 3 at alley to further reduce encroachment into root zone of English walnut.
- 3) That a curb, keystone wall, boulders or other form of parking obstacle be installed along driveway of Lot 3 and along south edge of alley to prevent parking under English walnut.
- 4) All future landscaping and irrigation under dripline of English walnut shall be implemented under supervision of consulting project Arborist.
- 5) Tree Commission supports applicant's efforts to retain Black Walnut under consultation with project Arborist, but supports the possible future removal if preservation proves to be ill fated or potentially hazardous.
- 6) That all tree preservation, protection and pruning techniques recommended by the consulting project arborist shall become conditions of approval.

# PA #2001-039 HAVURAH FINDINGS – FOR REFERENCE

## ASHLAND PLANNING COMMISSION

### FINDINGS & ORDERS

May 8, 2001

**PLANNING ACTION 2001-039** is a request for a Conditional Use Permit and Site Review to construct an approximately 3200 square foot addition to the existing religious sanctuary building located at 185 North Mountain.

Comprehensive Plan Designation: Single-Family Residential; Zoning: R-1-5;

Assessor's Map: 39 1E 09AA; Tax Lot 1701

**APPLICANT:** Havurah Shir Hadash

On Wednesday, Month-Day, 2000, a meeting was held in the Planning Office to review the application. In attendance were Maria Harris and Mark Knox, Associate Planners and Bill Molnar, Senior Planner serving as Staff Advisor.

The property consists of approximately 1.08 acres and is located on the west side of North Mountain Avenue, the second parcel north of the railroad tracks. The existing 2,240 square foot sanctuary for the Havurah Shir Hadash is situated in the central part of the property, with an unimproved gravel parking area located to the south and west of the building.

North Mountain Avenue is a paved City arterial with curb and gutter and public storm drainage. City water, sewer and electric facilities are currently serving the property.

The proposal includes a request to construct a 3,186 square foot addition to the existing building, bringing the total gross floor area to 5,426 square feet. The new construction will occur to the front and the rear of the structure, with the existing building centered in between. As described in the applicant's findings of fact, the new sanctuary space shall be oriented to the east toward North Mountain Avenue, while additional classrooms, a conference room and bathroom will be added to the rear of the existing building.

Several improvements to the site have been proposed to make the project more in line with current City of Ashland site design standards. The front entry becomes more pronounced through the installation of a set of double doors and transom. To further draw attention to the entrance, a covered porch will be constructed with an open framed extension. The existing gravel parking spaces located between the street and building shall be removed and replaced with landscaping.

Public improvements shall include the installation of a public sidewalk and planting strip along the street frontage of the property. A concrete walkway will be constructed from the sidewalk a lead to all three building entrances situated upon the south side of the building.

The criteria for approval of a conditional use permit are as follows:

*A. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.*

*B. That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.*

*C. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone:*

*1) Similarity in scale, bulk, and coverage.*

*2) Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.*

*3) Architectural compatibility with the impact area*

*4) Air quality, including the generation of dust, odors, or other environmental pollutants.*

*5) Generation of noise, light, and glare.*

*6) The development of adjacent properties as envisioned in the Comprehensive Plan.*

*7) Other factors found to be relevant by the Hearing Authority for review of the proposed use.*

The criteria for site review approval are as follows:

- A. *All applicable City ordinances have been met or will be met by the proposed development.*
- B. *All requirements of the Site Review Chapter have been met or will be met.*
- C. *The development complies with the Site Design Standards adopted by the City Council for implementation of this Chapter.*
- D. *That adequate capacity of City facilities for water, sewer, paved access to and through the development, electricity, urban storm drainage, and adequate transportation can and will be provided to and through the subject property.*

The application with the attached conditions complies with all relevant approval standards identified above.

Planning Action 2001-039 is approved with the following conditions. Further, if any one or more of the following conditions are found to be invalid for any reason whatsoever, then Planning Action 2001-039 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified here.
2. That the site plan be revised prior to the issuance of a building permit to include the installation of a seven foot planting strip and six foot wide sidewalk along the full street frontage of the property. The Staff Advisor may approve modifications in order to adjust to the location of existing trees.
3. That medium to large canopy shade trees, one per seven parking spaces, be installed along the south property line prior to a certificate of occupancy or final inspection. The landscape median adjacent to the adjacent to the south property line shall have a minimum width of seven feet and shall be protected through the installation of wheel stops. These changes shall be noted on a revised site plan at the time at which a building permit application is made.
4. That a revised site, size and species specific landscaping plan incorporating the recommendations of the Ashland Tree Commission be submitted for review prior to the issuance of a building permit. Landscaping to be installed in accordance with the approved plan prior to the issuance of a certificate of occupancy.
5. That street trees, 1 per 30 feet of street frontage be installed prior to the issuance of a certificate of occupancy. All street trees shall be chosen from the adopted Street Tree List

and shall be installed in accordance with the specifications noted in Section E of the Site Design and Use Standards. Tree specie shall be noted on the revised site plan.

6. That an irrigation system plan be include for review and approval by the Staff Advisor prior to issuance of a building permit.

If no appeal is filed, this request will become final when reviewed by the Ashland Planning Commission on May 8, 2001.

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Bill Molnar, Senior Planner

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Date

## PC Goals - revised 9/25/07 - PROPOSED

1. Visioning/Comp Plan/Economic Dev. Strategy - Combo
  - Start in early 2008, before election season
  - Visioning contains\_all\_components (Ec Dev, Transp, Housing, etc.) in integrated way ("The whole is more than....etc.")
  - Comp Plan Revision derives from Visioning
  - Streamline Comp Plan Rev
  - Need to prep for 2008 this fall
  - PC may do Transportation component
  - Resolution of CPAC included
  
2. Ordinance Review
  - OR1: Son of Siegel (low hanging fruit, etc.)
    - = WM, JF, MM, JS propose for 10/4
    - = Pass on 10/4 and move to CC for action in Oct/Nov
  
  - OR2: Procedure Changes
    - = Complete public hearings 10/4
    - = Wait to deliberate until PC org changes completed
  
  - OR3: Policy Issues, Incomplete items (e.g. vision clearance)
    - = Add to Pending list
  
3. Riparian Ordinance/Wetlands Inventory
  - Confront problems
  - Determine approach
  - Work steadily until done (recommend to CC)
  
4. Economic Opportunities Analysis
  - Pass on to CC with qualifying language and/or commentary
  
5. Crowman Plan
  - Immediate action

- Determine PC role and how to meet it
6. Arterial Setback
    - Continue on planned path (next meeting Nov. 27)
  7. PC Roles & Responsibilities
    - City Administrator/PD working with CC for acceptable wording
    - PC reviews before CC action
  8. PC Training & Candidate Prep
    - Needed institutionalized activity
    - Assign to committee?
  9. Regional Problem-solving Issues (DS report)
    - Oct. 4th?

To: Planning Commission  
From: John Stromberg

Subj: Updating PC Goals

Nov. 6, 2007

I put this item on our agenda for Nov. 13th for the following reasons:

- During the months since we last reviewed our goals we've completed work on some of them and also new issues have come up.
- Our current goals list was formulated largely in response to Dave Stalheim's proposals. Now that he's gone we may want to rethink our goals and/or priorities.
- We need to discuss our goals with Bill, both with regard to our choices and priorities and also to the work load they put on the staff.
- This new version will go to the Council as input for its own goals and will provide a basis for discussion between the PC and CC about the community's planning needs.

Also if any of our goals require City funding (beyond operation of the Planning Department) then we need to make sure the Council recognizes this fact and takes it into account in formulating the Council's own goals since they should guide the budget process.

Δ

In the packet for Nov. 13th you have notes from our last discussion of our goals. On the following pages is a draft update in which I've tried to assemble all the items with which we've been working, to serve as a starting point for our discussion. If I've left anything out or you would like to add new items, please let me know.

If we don't complete our work on updating our goals on Nov. 13th we'll carry over this item to subsequent PC sessions.

## **Planning Commission Goals - Nov. 6, 2007 - DRAFT**

### **Priority 1**

- A. Community Visioning - Comp Plan Update - Economic Development Planning - Transportation Planning

### **Priority 2**

- A. Riparian Ordinance
- B. Croman Master Plan
- C. Arterial Front Yard Setback
- D. PC Roles and Responsibilities

### **Priority 3**

- A. Ordinance Revision Follow-up
  - Support to City Council decision process
  - Implementation and evaluation of procedural changes, if adopted
  - Vision Clearance\*
  - Residential vs Commercial proportions in C-1, E-1\*
- B. PC Roles and Responsibilities
- C. PC Training and Candidate Prep
- D. Public Hearings Guide Editing
- E. Economic Opportunities Analysis - Comments to Council
- F. Regional Problem Solving Growth Allocation - Next Steps

\* The Ordinance Revision hearings process has been continued to Jan., 2008, to allow further consideration of these items without requiring additional noticing.

## **Proposed but not adopted**

- A. Variance and Performance Standards Criteria. Are the variance and performance standards criteria and process discouraging creative design and causing developers and architects to play it safe rather than strive for something excellent? Do we require too much information at the conceptual application stage for performance standards, rather than allowing creativity and options to flourish and be considered?
- B. Design-related Land Use Issues. The Site Design and Use Standards were adopted in 1992. The city is seeing different development concepts than originally envisioned, including mixed-use development and transitions to residential neighborhoods that should be addressed.
- C. Co-housing Overlay. Allow for a residential development that has shared parking area, rather than parking at every home, in a single family zone. Also neo-urban design standards that call for parking in back may cause more paving, less yards and less access to solar.
- D. Mixed-Use Housing. Review mixed use housing to encourage housing for business owners and workers to revitalize commercial areas, rather than high end housing.
- E. Secondary Uses. Address the issue of secondary uses dominating target uses in various zones. For example: residential in commercial and employment zones; and bed and breakfast inns in residential zones.
- F. Manufactured Homes. Should manufactured homes be allowed within historic districts. Also, are current standards for manufactured homes consistent with limitations that the state has placed on how local government can regulate manufactured homes?
- G. Urban Growth Area. Two property owners have requested that the City add their property to the urban growth area: 15 acres owned by Larry Medinger and Madeline Hill adjacent to Mountain Meadows, and the farm owned by Ron Roth along Bear Creek near Interchange 19.

## Planning Commission Goals - Nov. 11, 2007 - Comments - DRAFT

### Priority 1

A. Community Visioning >> Comp Plan Update - Economic Development Planning - Transportation Planning First stage needs to be completed in 2008. Visioning leads but incorporating elements of land use (Comp Plan), Economic Development and Transportation. Comp Plan Update, rather than complete Revision, may be sufficient. Issue of CPAC to be resolved in the process. Comp Plan Update may roll into Downtown/ Railroad Plan.

### Priority 2

A. Wetland and Riparian Ordinance Discussion guide being prepared for Nov. 27th Study Session. This item will retain top priority until completed.

B. Croman Master Plan Near ready to start. May schedule initial meeting with consultants in lieu of December Study Session. May involve new PC activity, i.e. performing outreach with community stakeholders.

C. Arterial Front Yard Setback Part 1: Lithia Way on either December or January Study Session agenda.

D. PC Roles and Responsibilities Waiting for City Administrator to clear with Council, then will return to PC for review of any rewording, then back to Council for action. Target for Council approval: Dec., 2007.

### Priority 3

A. Ordinance Revision Follow-up  
- Support to City Council decision process - if requested  
- Implementation (4 mo.s) and evaluation (6-12 mo.s) of procedural changes, if adopted  
- Vision Clearance\* - waiting for revision by Engineering and ground-truthing by PD  
- Residential vs Commercial proportions in C-1, E-1\* - priority may depend on progress with Visioning et al

B. PC Training and Candidate Prep Not a glamorous topic but have to 'professionalize' participation in the Planning Commission. For what we need to do, OJT is not enough.

D. Public Hearings Guide Editing Current version may cover all legal 'exposure' but is not yet user-friendly. New draft responsibility of the Chair (help, anyone?)

E. Economic Opportunities Analysis - Comments to Council Report goes to Council in Dec.. Get any suggestions to Chair by Nov. 20th.

F. Regional Problem Solving Growth Allocation - Next Steps After consultation with City Administrator and Com Dev Director, PC may want to make recommendation to Council.

\* The Ordinance Revision hearings process has been continued to Jan., 2008, to allow further consideration of these items without requiring additional noticing.

### **Proposed but not adopted**

- A. Variance and Performance Standards Criteria Are the variance and performance standards criteria and process discouraging creative design and causing developers and architects to play it safe rather than strive for something excellent? Do we require too much information at the conceptual application stage for performance standards, rather than allowing creativity and options to flourish and be considered?
- B. Design-related Land Use Issues The Site Design and Use Standards were adopted in 1992. The city is seeing different development concepts than originally envisioned, including mixed-use development and transitions to residential neighborhoods that should be addressed.
- C. Co-housing Overlay Allow for a residential development that has shared parking area, rather than parking at every home, in a single family zone. Also neo-urban design standards that call for parking in back may cause more paving, less yards and less access to solar.
- D. Mixed-Use Housing Review mixed use housing to encourage housing for business owners and workers to revitalize commercial areas, rather than high end housing.
- E. Secondary Uses. Address the issue of secondary uses dominating target uses in various zones - For example: residential in commercial and employment zones; and bed and breakfast inns in residential zones.
- F. Manufactured Homes Should manufactured homes be allowed within historic districts? Also, are current standards for manufactured homes consistent with limitations that the State has placed on how local government can regulate manufactured homes?
- G. Urban Growth Area Two property owners have requested that the City add their property to the urban growth area: 15 acres owned by Larry Medinger and Madeline Hill adjacent to Mountain Meadows, and the farm owned by Ron Roth along Bear Creek near Interchange 19.
- H. Water Supply Planning There may be a gap re management and policy/planning issues. Some related regulations are in the LUO. Should the PC at least push for a specific delegation of responsibility?