

CITY OF ASHLAND

HISTORIC COMMISSION MEETING

AGENDA

October 4, 2017 at 6:00 P.M.

- I. **REGULAR MEETING - CALL TO ORDER:** 6:00 p.m. – SISKIYOU ROOM in the Community Development/Engineering Services Building, located at 51 Winburn Way
- II. **APPROVAL OF MINUTES:** Historic Commission regular meeting of September 6, 2017
- III. **PUBLIC FORUM:** Business from the audience not included on the agenda. (Total time allowed for Public Forum is 15 minutes. Speakers are limited to 5 minutes or less, depending on the number of individuals wishing to speak.)
- IV. **COUNCIL LIAISON REPORT:**
Rich Rosenthal
- V. **PLANNING ACTION REVIEW:**
PLANNING ACTION: PA- 2017-01294
SUBJECT PROPERTY: 128 Central Avenue
APPLICANT: Rogue Planning & Development Services
OWNERS: Robert Baldwin
DESCRIPTION: A request for Site Design Review for an eight-unit multi-family development for the property located at 128 Central Avenue. The existing house contains two units. The proposal has been revised to add five units at the rear of the property and one unit above the reconstructed garage at the front of the property. The application also includes requests for a Conditional Use Permit to exceed the Maximum Permitted Floor Area in the Skidmore Academy Historic District by 21 percent (985 square feet) and for a Tree Removal Permit to remove seven trees six-inches in diameter-at-breast-height or greater. **COMPREHENSIVE PLAN DESIGNATION:** High Density Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 4500.

PLANNING ACTION: PA- 2017-01310
SUBJECT PROPERTY: 981 Siskiyou Blvd.
OWNERS: MH Smith Inc
APPLICANT: MNM Fund LLC
DESCRIPTION: A request for Site Design Review and Tree Removal Permit approval for a three-unit multi-family residential development for the property located at 981 Siskiyou Blvd. The proposal includes retaining the existing residence and constructing two units at the rear of the property. The Tree Removal Permit request is to remove two trees to the west of the existing home including a maple and an ash tree. **COMPREHENSIVE PLAN DESIGNATION:** Low-Density Multi-Family Residential; **ZONING:** R-2; **ASSESSOR'S MAP:** 39 1E 09DA; **TAX LOT:** 8100.

PLANNING ACTION: PA- 2017-01605
SUBJECT PROPERTY: 147 Van Ness Avenue
OWNERS: Deborah Dryden & Robert Hirschboeck
APPLICANT: Conscious Construction
DESCRIPTION: A request for Site Design Review for a three-unit multi-family development for the property located at 147 Van Ness Avenue. The proposal includes the existing residence as one unit, the conversion of an existing guest cottage as a second unit and the construction of a new third unit with attached single vehicle garage off of the alley. **COMPREHENSIVE PLAN DESIGNATION:** High Density Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 2900.

VI. **NEW ITEMS:**

- Review board schedule.
- Project assignments for planning actions.

VII. **DISCUSSION ITEMS:**

VIII. **COMMISSION ITEMS NOT ON AGENDA:**

IX. **OLD BUSINESS:**

X. **ADJOURNMENT:**

**ASHLAND HISTORIC COMMISSION
DRAFT Meeting Minutes**

September 6, 2017

Community Development/Engineering Services Building – 51 Winburn Way – Siskiyou Room

CALL TO ORDER:

Commission Chair, Shostrom called the meeting to order at 6:03pm in the Siskiyou Room at the Community Development and Engineering Offices located at 51 Winburn Way, Ashland OR 97520.

Commissioners Present:	Council Liaison:
Skibby	Rich Rosenthal
Emery	Staff Present:
Leonard	Maria Harris; Planning Dept.
Swink	Regan Trapp; Secretary
Von Chamier	Nathan Emerson; Planning Dept.
Whitford	
Shostrom	
Giordano	
Commissioners Absent:	

APPROVAL OF MINUTES:

Leonard motioned to approve minutes from August 2, 2017. Swink seconded. Voice vote; All AYES. Motion passed.

PUBLIC FORUM:

Melissa Mitchell-Hooge of the Ashland, Save our Schools and Playground Group and Jim Young of Lithia Arts Guild addressed the Commission regarding saving Briscoe School. Ms. Mitchell-Hooge submitted an informational packet to the Commission for their review. **(see attached, exhibit A)** Ms. Mitchell-Hooge emphasized, that in order to utilize the building they will need to increase the parking. She went on to say that this building has lots of historic value and there are many different things this building could be utilized for within the City. Ms. Mitchell-Hooge commented that Briscoe School is big enough to accommodate City Hall and other departments under one roof. The Commission was very responsive and supportive of the idea of the City saving this building.

COUNCIL LIAISON REPORT:

Rosenthal gave the Council Liaison report. Items discussed were:

- Consent agenda- New PW director, Paula Brown will be taking over for Mike Faught.
- Public art was approved for the Theater Corridor Project. Installation will be in the Spring of 2018.
- Approval of 1st reading of greenhouse gas targets.
- Senior Center citizen input in regards to the changes being enforced by Parks and Recreation Commission.

Shostrom read aloud the procedure for public hearings.

PLANNING ACTION REVIEW:

PLANNING ACTION: PA- 2017-01294

SUBJECT PROPERTY: 128 Central Avenue

APPLICANT: Rogue Planning & Development Services

OWNERS: Robert Baldwin

DESCRIPTION: A request for Site Design Review for an eight-unit multi-family development for the property located at 128 Central Avenue. The existing house contains two units, and the proposal would add six 475-square foot units at the rear of the property in two triplex structures accessed from the alley. The application also includes requests for a Conditional Use Permit to exceed the Maximum Permitted Floor Area in the Skidmore Academy Historic District by nine percent (395 square feet) and for a Tree Removal Permit to remove seven trees six-inches in diameter-at-breast-height or greater. **COMPREHENSIVE PLAN DESIGNATION:** High Density Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 4500.

Von Chamier stated that her firm has been assigned the project but she will have no issue in making an unbiased decision. Harris stated that she felt that this would be a conflict of interest and to be on the safer side, Von Chamier should dismiss herself from this presentation.

Von Chamier recused herself from planning action review for PA-2017-01294.

Harris gave the staff report for PA-2017-01294.

Harris spoke about the letters from the neighbors that were submitted before the meeting and asked the Commissioners to review them prior to making a decision.

Shostrom opened the public hearing to the applicants.

Amy Gunter, applicant's representative, Rogue Planning and Development, 1424 S. Ivy St, Medford, addressed the Commission regarding this project. Ms. Gunter stated that there are no storm drain facilities in the neighborhood and the on-site retention would most likely be in the parking area. She went on to say that the alley is required by code to be paved.

Ron Davies, owner of 157, 159, & 171 Helman Street addressed the Commission regarding this project. Mr. Davies has concerns about the paving of the alley which is required by code. He is concerned about potential overflow onto his property and wants to know what kind of sidewalk and gutters will be added. Mr. Davies stressed the importance of keeping the lot coverage normal

Ms. Gunter addressed the Commission regarding Mr. Davies concerns.

Shostrom closed the public hearing and opened to the Commission for comments.

Giordano motioned to approve PA-2017-01294 with recommendations. Swink seconded. Voice vote; All AYES. Motion passed.

Recommendations of Historic Commission for PA-2017-01294:

- The Historic Commission believes the scale and massing is similar to buildings along the alley. However, the Commission believes a portion of the development could be two story and designed to meet the Historic District Design Standards in order to preserve trees.
- Use smooth siding, not textured hardy plank.
- Add 3 to 4-inch framing between single hung windows.

- Do not use white windows.
- Increase size of porch posts to larger than 4 x 4 – wrap 4 x 4 or use 6 x 6.
- Use decorative or flat balustrade on porch railing of new units, do not use proposed 2 x 2.

PLANNING ACTION: PA- 2017-01310

SUBJECT PROPERTY: 981 Siskiyou Blvd.

OWNERS: MH Smith Inc.

APPLICANT: MNM Fund LLC

DESCRIPTION: A request for Site Design Review, Conditional Use Permit and Tree Removal Permit approval for a three-unit multi-family residential development for the property located at 981 Siskiyou Blvd. The proposal includes retaining the existing residence and constructing two units at the rear of the property. The Conditional Use Permit request is to exceed the maximum permitted floor area for the historic district by 164 square feet. The Tree Removal Permit request is to remove three trees to the west of the existing home including an Italian cypress, maple and an ash tree. **COMPREHENSIVE PLAN DESIGNATION:** Commercial; **ZONING:** C-1; **ASSESSOR'S MAP:** 39 1E 09DA; **TAX LOT:** 8100.

There was no ex-parte contact or conflict of interest indicated by the Commission.

Harris gave the staff report for PA-2017-01310.

Shostrom opened the public hearing to the applicants.

Amy Gunter, applicant's representative, Rogue Planning and Development, 1424 S. Ivy St, Medford, and Melanie Smith, applicant of MNM Fund and General Contractor, Ashland, addressed the Commission regarding this project.

Dominic Smith, neighbor residing at 965 Siskiyou Blvd, Ashland, addressed the Commission regarding the project. Mr. Smith stated that his property line is 2 feet into the property at 981 Siskiyou and halfway into the tree that is slated for removal. He wants to know if anyone had done any measuring on the lot in order to start this project. Mr. Smith does not want the tree removed as it would have him looking right through the windows of the property being built and is looking into paying a surveyor to see where the property lines actually lie.

Melanie Smith, MNM Funds, addressed the Commission regarding Mr. Smith's concerns. Ms. Smith stated that her measurements are based on fence lines and where the neighboring property lines lie.

Shostrom closed the public hearing and opened to the Commission for comments.

Shostrom motioned to deny PA-2017-01310. Leonard seconded. Voice vote; All AYES. Motion passed.

Recommendations of Historic Commission for PA-2017-01310:

- The Historic Commission determined the proposal does not meet the Historic District Design standards and therefore does not meet the approval criteria for a Conditional Use Permit to exceed the allowed maximum permitted floor area (MPFA). The Commission does not recommend approving the application.
 - **Height, Scale, Massing, Form** (AMC 18.4.2.050.B 2, 3, 4, 9)
The Historic Commission's concern is that the proposed structure is out of scale with historic buildings in the vicinity. Specifically, the proposed design is box-like and has very little architectural detail such as variations in massing. Additional items that were identified by the commission follow.
 - The front entrances should be well-defined and would typically have a landscape buffer between the entrance and driveway (Entrances AMC 18.4.2.050.B.10).
 - The slider windows are not historically compatible (AMC 18.4.2.050.C.2.g).

- Needs historically compatible architectural details such as water table, belly band and double hung windows.
- Square footage used in MPFA calculations is inconsistent with unit sizes outlined in application and may not be accurate.

PLANNING ACTION: PA-2017-01417

SUBJECT PROPERTY: 228 B St

OWNER: Hani Hajje

APPLICANT: Kerry KenCairn

DESCRIPTION: A request for Site Design Review, Conditional Use Permit approval, and Tree Removal Permit for the property located at 228 B Street. The proposal involves the development of a two-story, 1008-square foot detached addition to accommodate two additional Traveler Accommodation units. A Tree Removal Permit is also requested to remove five Leyland Cypress trees, one 7" in diameter pine tree, and one 9" in diameter flowering plum tree. The requested approval would allow operation of a seven unit traveler accommodation including six guest units and one owners unit. **COMPREHENSIVE PLAN DESIGNATION:** Multifamily Residential; **ZONING:** R2; **ASSESSOR'S MAP:** 39 1E 16AD; **TAX LOT #:**6200

Von Chamier recused herself since she works with Kerry KenCairn who is the applicant on this project. Emery stated he has worked with the applicant on several instances but will not affect his decision on this project.

Harris gave the staff report for PA-2017-01417.

Shostrom opened the public hearing to the applicants.

Kerry Kencairn, applicant, 147 Central Ave, Ashland, OR, addressed the Commission regarding this project. Ms. Kencairn gave the background on the project and stated that they have tried their best to bring it into compliance with the Historic District Design Standards. Ms. Kencairn expressed that the Historic Commission review board did not like the original placement of the stairs so the applicant moved them to accommodate bike parking and the water heater.

Shostrom closed the public hearing and opened to the Commission for comments.

Shostrom motioned to approve PA-2017-01417 with recommendations. Swink seconded. Voice vote; All AYES. Motion passed.

Recommendations of Historic Commission for PA-2017-01417:

- Porch on south elevation – add beam across the top and wrap post or use 6 x 6 posts.
- Add belly band at second level to match carriage house.
- Add water table band or skirt at bottom.
- Match balustrades and hand rails of main house for staircase.
- Consider tucking stairs tight to building or covering.

PLANNING ACTION: PA-2017-01649

SUBJECT PROPERTY: 84 Dewey St

APPLICANT: Taylored Elements Construction

OWNER: Kathy Hill

DESCRIPTION: A request for a Site Design Review to allow for the conversion and addition to an existing guest house to be a 263 sq.ft. accessory resident unit (ARU) on the subject property.

COMPREHENSIVE PLAN DESIGNATION: Multi-Family Residential; **ZONING:** R-2; **ASSESSOR'S MAP:** 39 1E 09AC; **TAX LOT:** 11300.

There was no ex-parte contact or conflict of interest indicated by the Commission.

Harris gave the staff report for PA-2017-01649.

Shostrom opened the public hearing to the applicants.

Kathy Hill, 84 Dewey Street, Ashland, addressed the Commission regarding this project and expressed that she would be happy to answer any questions they had.

Shostrom closed the public hearing and opened to the Commission for comments.

Whitford motioned to approve PA-2017-01649 with recommendations. Leonard seconded. Voice vote; All AYES. Motion passed.

Recommendations of Historic Commission for PA-2017-01649:

- Match corner boards and frieze boards on front (street) elevation of house.
- Match siding on house.
- Consider steeper roof pitch on new structure.

NEW ITEMS:

- Review board schedule.
- Project assignments for planning actions

DISCUSSION ITEMS:

Harris conveyed that only City staff are allowed on an applicant's private property and when Commissioners are doing their site visits they need to be sure to stay in the public right of way.

COMMISSION ITEMS NOT ON AGENDA:

There were no items to discuss.

OLD BUSINESS:

There were no items to discuss.

Review Board Schedule

September 7 th	Terry, Keith, Piper
September 14 th	Terry, Tom, Bill
September 21 st	Terry, Bill, Piper
September 28 th	Terry, Bill, Dale
October 5 th	Terry, Keith, Sam

Project Assignments for Planning Actions

PA-2014-00710	143 Nutley	Swink & Whitford
PA-2014-02206	485 A Street	Whitford
PA-2015-00878	35 S. Pioneer	Leonard
PA-2015-01695	399 Beach	Skibby
PA-2016-00847	252 B Street	Whitford
PA-2016-01027	276 B Street	Shostrom & Leonard
PA-2016-02103	133 Alida	Swink
PA-2016-02095	563 Rock St.	Whitford
PA-2016-02114	556 B	Von Chamier
PA-2017-00013	15, 35, 44 & 51 S. Pioneer Street	ALL
PA-2017-00235	114 Granite	Leonard
PA-2017-00325	746 C Street	Von Chamier
PA-2017-00200	165 Water	ALL
PA-2017-00707	550 E. Main	Skibby
PA-2017-00838	250 Alta Ave	Emery

PA- 2017-01054	220 Hargadine	Shostrom & Whitford
PA-2017-00969	244 Hargadine	Shostrom & Whitford
PA-2017-01279	692 B Street	Leonard
PA-2017-01294	128 Central	Emery & Swink
PA-2017-01417	228 B Street	Whitford
PA-2017-01310	981 Siskiyou Blvd	Shostrom
PA-2017-01649	84 Dewey Street	Von Chamier

ANNOUNCEMENTS & INFORMATIONAL ITEMS:

Next meeting is scheduled October 4, 2017 at 6:00 pm

There being no other items to discuss, the meeting adjourned at 9:28pm

Respectfully submitted by Regan Trapp

Ashland School District

Facility & Capacity Assessment Report

Briscoe Elementary School (Closed Facility)
 Address: 265 N Main Street
 Area: 33,980 square feet
 Buildings: Original Building 1948
 Music Addition 1997
 Enrollment 2004: 0 students



FINDINGS

Architectural - Findings

Briscoe Elementary School is currently a closed school that is being leased to two different businesses. A portion of the school is housing the Migrant Headstart. The other portion is housing a community of local artist studios. Overall the school is in moderate condition. There is no off street parking and very limited playground areas. The classroom capacity of the facility can support 275 students. The multipurpose room can support 235 for eating and over 500 in an assembly. The adjacent kitchen is extremely inadequate and the stage is not used for performances. The library and offices are inadequate with current capacity. The whole facility lacks conference and meeting spaces for staff, parents and students.

Mechanical and Plumbing Systems - Findings

HEATING

The building still has the original steam boiler. The piping ranges from new replacement to old original black iron, so ranges from good to bad condition. All piping is asbestos insulated. The school does have two steam boilers but one has been out of commission for years. The second boiler was decommissioned because it needed burner work. To get it operating and recertified would cost more than the boiler is worth. With only one operable boiler, a failure could result in freezing pipes and building closure.

The heating controls are the original pneumatic equipment. The pneumatic controls are outdated but we have been able to keep the components functioning, however finding parts is getting more and more difficult.

The rooms are heated by wall units in most of the building and there is an air handler with coils in the gymnasium. All are outdated and parts are no longer available. The original building has no cooling other than a few window mounted units the renters (OCDC) have installed. Also note the renters (OCDC) have split some classrooms so had to add wall mounted heat pumps to cover heating as the original heating system was not able to function with the rooms split. The Music Building addition has roof top Trane gas fired heaters with AC (package units).

Pumps and circulation for water heating system are old but are in fair condition. Steam traps need annual attention and return pumps have been replaced so are in good condition.

Water heating systems range from old to new and all are in good condition. All the piping is original equipment, galvanized pipe and in poor condition.

Plans for the Heating/Cooling System

1. The boilers are outdated and approaching failure. We recommend replacing them with condensing boilers. This would also include asbestos removal and all boiler room piping removal. Estimated cost: \$500,000
2. All the room heaters are steam equipment, outdated and parts no longer available. Everything needs replaced with something compatible with hot water systems. The piping is asbestos wrapped so would have to be removed/abated at the same time. This project would also include a lot of room surfacing removal and replacement. Estimated cost: \$500,000
3. The Music Building heaters already need to be replaced. There are three units. Estimated cost: \$35,000
4. Add DDC controls for the HVAC system. Estimated cost: \$250,000

Plumbing Systems and Fixtures

The water supply for domestic hot and cold water is the original galvanized pipe. Many of the pipes have failed, especially on the hot water side. We have replaced many of them but major pipe failure may only be a day away. All the hot and cold water piping is asbestos insulated. Because of the condition of the galvanized pipe the water is terrible in taste, color and odor. Filters have been installed on many devices.

The plumbing fixtures, sinks, toilets, faucets, and bathroom partitions are for the most part original equipment. We have changed out some fixtures as they have failed.

Plans for Plumbing Systems and Fixtures

1. All the galvanized pipe needs replaced, both hot and cold water sides. Through the years non-compatible pipe and fittings have been added, causing electrolysis, which creates more failures in the system. All the pipe is asbestos insulated which adds cost to any repairs or replacement. Replacement would include asbestos removal, wall surfacing removal/replacement, painting and probable concrete work. Estimated cost: \$350,000
2. All the toilets, faucets and sinks are past their expected life span and need to be replaced with efficient units. Estimated cost: \$250,000
3. Replace the bathrooms partitions. Estimated cost: \$35,000

ELECTRICAL SYSTEMS - FINDINGS

The school is original equipment main and sub panels, old and outdated. Many panels are at full capacity. We find on these old panels they don't "break" the power source when there is a problem. Much of the old system is the old style fuses which are getting harder to find for failure replacements. The outlets and switches range from old to new and need replaced as well.

Plans for the Electrical Systems

1. Change out all the original main and sub panels to Square D, QO main and sub panel. Estimated cost: \$350,000

ROOFING - SYSTEM

The roofing was in good condition until our roofing consultant recommended coating them to expand the life span. The roof coating has failed for the most part and has damaged the roofs. The Music Building is single ply rubber and needs to be replaced in the next few years.

Plans for the Roofing System

1. Replace all the roofs with plied up asphalt with new, more efficient insulation. Estimated cost: \$750,000

WINDOWS

All the windows are wood sash and past their life span. All are rotten and in poor condition.

Plans for Windows

1. They need to be replaced with new vinyl high efficiency windows and new wood trim with some structure replacement. The windows are lead containing in the paint and possibly asbestos in the window glazing so removal environmental costs are high. Estimated cost: \$550,000

INTERIOR FINISHES AND HARDWARE

Most of the interior floors are 9x9 asbestos floor tiles or asbestos sheet goods. OCDC has removed some during the years and the district has as well. Some of the carpet is old and needs to be replaced.

The school is in need of paint both interior and exterior.

All interior and exterior doors are worn out, and their hardware all needs to be replaced.

Plans for Interior Finishes and Hardware

1. Remove all the asbestos floor tile and asbestos sheet goods. Replace with VCT or sheet goods. Estimated cost: \$550,000
2. Paint the complete interior and exterior. Estimated cost: \$75,000
3. Replace all interior doors with solid core birch and replace all exterior doors with new commercial door and jamb assemblies. Estimated Cost: \$225,000

Briscoe Forum FAQs

What's this all about?

In 2003, Briscoe Elementary School was closed due to declining enrollment throughout Ashland. For nearly 14 years, Ashland School District (ASD) has leased this property out to various tenants. ASD and the district facilities committee are collecting information to make a recommendation to the School Board about the best path forward with Briscoe.

So why are we talking about this now?

The Ashland School Board has a responsibility to use our community resources and tax dollars to best serve our students. This is part of a larger, district-wide review of all district assets in preparation for a potential bond issue in 2018.

Give me some reasons that the district might want to keep Briscoe as it currently is.

The Briscoe building is a beautiful school with a long history. The open space and playground next to it is precious open space and enjoyed by neighborhood residents. Additionally, the current tenants (Oregon Child Development Center and Lithia Arts Guild, among others) all contribute to the community. Finally, the rental income each year exceeds expenses by somewhere between \$30,000 to \$147,000.

Those are some good reasons! So why would the district consider letting this property go?

The Briscoe building itself has over \$3M in deferred maintenance, according to a 2014 internal review. Between the asbestos, leaking windows, outdated electrical, and ancient boiler (just to name a few) this building is close to its "end of useful life." Even with the short-term income each year from renters, the math just doesn't work in the long-term to make this building "profitable" for the district as a landlord. And this doesn't factor in the liability the district has, if someone were to get hurt or injured on the property.

Couldn't we just use future bond money to repair Briscoe?

That is a choice that the school board could make, and is central to the discussion and decision-making around this. Doing this would mean that taxpayer money would be re-directed from our active schools and students into a rental property.

But what if we need to use Briscoe in the future? What if student enrollment goes way up?

Unfortunately, that's not likely. The high point for ASD enrollment in the past 25 years occurred in 1993, when 3,546 students attended Ashland schools. By the time Briscoe was closed in 2003, that number had dropped to 3,008. Enrollment for 2016 landed at 2896, including 285 students who live outside the school district boundaries.

The most recent demographer report (February 2017) indicates that enrollment will continue to decline further over the next 10 years, unless the district continues to accept students from outside Ashland. Even if enrollment were to surge in the more

distant future, the district still has capacity in our current active schools and properties to accommodate that.

What factors will the district and school board consider in deciding what to do?

The overarching question for decision-makers is this: *How can Ashland School District best use its funding and resources to perform our mission and serve our students?*

We want to focus on our core mission of education.

We want to be good neighbors and conscientious community partners.

We want to responsibly plan for the future – both short and long-term – so that we can continue to provide the best education possible for all students.

How can I let you know how I feel about all this?

The district is hosting a community forum on **May 4, 2017** to share data and hear your thoughts and ideas. You can also email Briscoe@ashland.k12.or.us.

Briscoe Community Forum

When: May 4, 2017

Where: Ashland High School Library

Time: 7:00 PM until 8:30 PM

Where can I get more information?

Visit the ASD website at www.ashland.k12.or.us and click the link for “Briscoe Forum.” Here, you’ll find the most recent demographers report, enrollment history, an overview of deferred maintenance, and other information.

When will a decision be made?

The last of the data, an engineering report with a better estimate of maintenance and repairs, is expected in June. At that point, the district and the facilities committee will consider all the information and make a recommendation to the Ashland School Board.

Decision Timeline

May 4, 2017: Community forum

Early June: engineering report expected

Mid-June: Facilities committee meeting to review data and make recommendation to school board. (Date TBD)

June and/or July: School board considers recommendation

Hello Ad Hoc City Hall Committee,

Thank you for your service on the Committee. It's a tough job, and an important one.

I'd like to follow up with you on my comments at the last meeting, with questions:

Are you interested in preserving the Briscoe building? Located in the Skidmore Academy Historic District, the oldest residential area in Ashland, Briscoe School is on the National Register of Historic Places in Jackson County.

It seems clear that City Hall in the current building should be preserved, and should continue to be used for civic functions. For one, it's an iconic building, and for another, we need to maintain the City's title. But could we keep City Hall as is, with seismic upgrades, possibly used for the Chamber, and move all other public functions to Briscoe?

Has the Committee studied what could fit in Briscoe? Could it house the Muni Court (doubling as Council Chambers,) Parks and Rec, and all staff currently working in the Com Dev building?

Have cost savings been examined if the Police used the whole Civic Center building (obviating the need for an expansion of the police department), the sale/lease of the Com Dev building, the sale/lease of the Grove, the sale/lease of the present Chamber building, and leasing the upper floor of City Hall as offices (providing for possible future expansion for new City staff)?

Has anyone studied whether funds are available from government, foundation grants and individual donations to preserve Briscoe in order to bring down the costs to the voters?

Parking is a huge issue.

Does it make sense to build off-street parking structures at \$20,000-\$40,000 when the need for parking is great in the short term, but will markedly decrease in the next 5, 10, 15 years due to better public transit, more bikes and electric bikes, increased concern with our carbon footprint, and more staff working remotely?

Would it be possible to get around the Parking Code by creating an exception for municipal buildings?

Melissa's parking plan is one option, but maybe we don't need to change the Briscoe field at all. The City could claim the 25+ angled spaces on Laurel as staff and visitor parking, claim the spaces on the Briscoe side of Manzanita, and possibly build new (\$5,000 to \$7,000) angled off-street parking on the Briscoe side of High Street? As Melissa has shown, there is ample additional street parking very close to Briscoe.

Has anyone talked to the Methodist Church about using its parking lot for a few evenings per month for large public meetings?

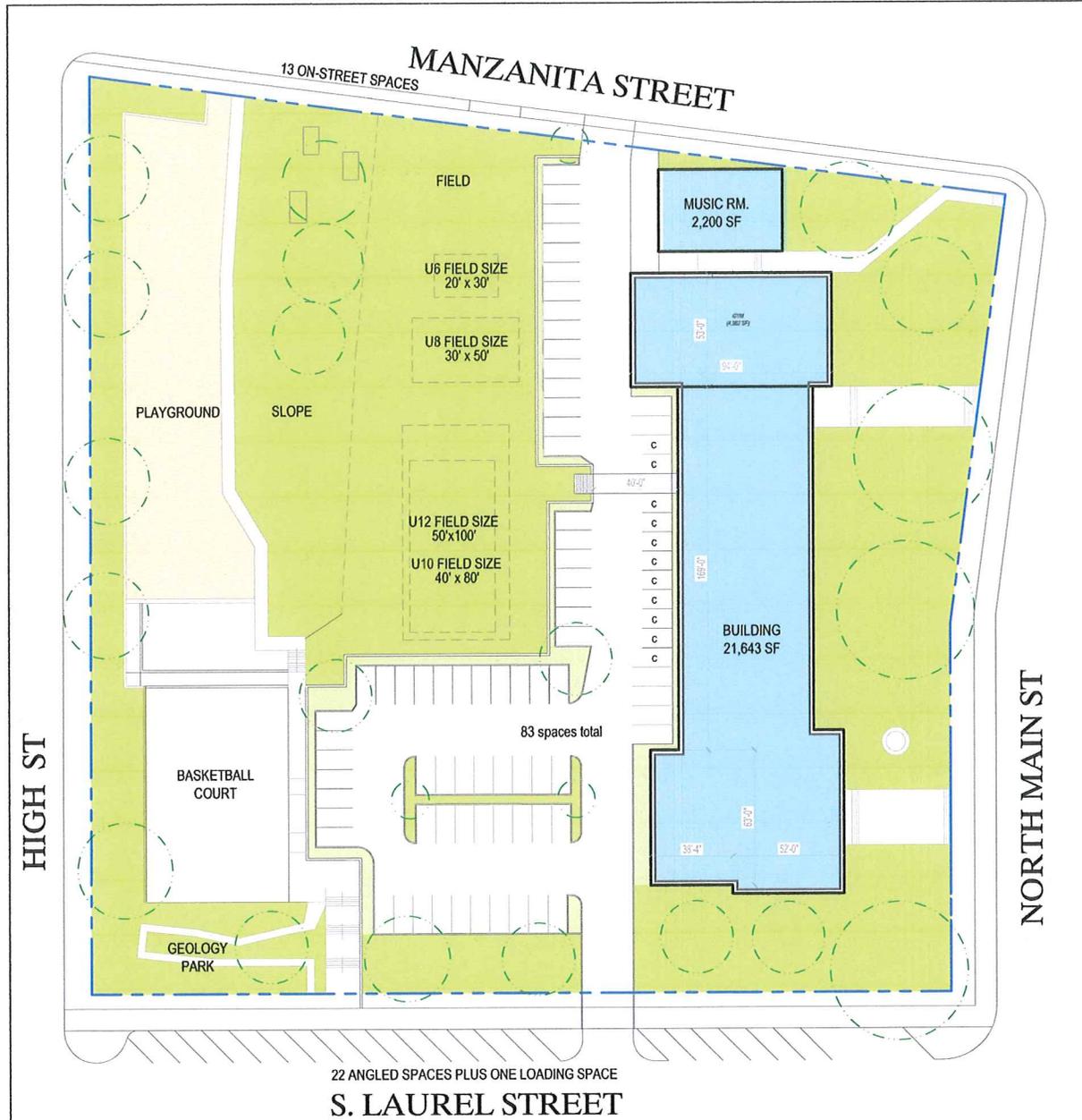
Shouldn't the parking issue be considered in light of new City priorities, including the approved Climate Energy Action Plan and Parking Plan? Has the Committee examined the CEAP and Parking Plans? Do you find it ironic that the City Hall Committee meetings are held away from downtown (practically at Briscoe) because there is no parking downtown?

Are benefits to Staff one of the criteria? Would staff rather move to the Civic Center, which is isolated and far from downtown, with few amenities nearby? Or would they prefer Briscoe, which has its own park, is two blocks from downtown and is easily walkable to cafes, restaurants, the current City Hall, and Lithia Park?

Would the merchants support more traffic and less parking downtown? Would voters support expensive new construction and parking lots when instead they could preserve two beautiful, historic buildings?

A lot of questions. But I think they all deserve answers from the Committee.

Thank you for your consideration,
Sharon

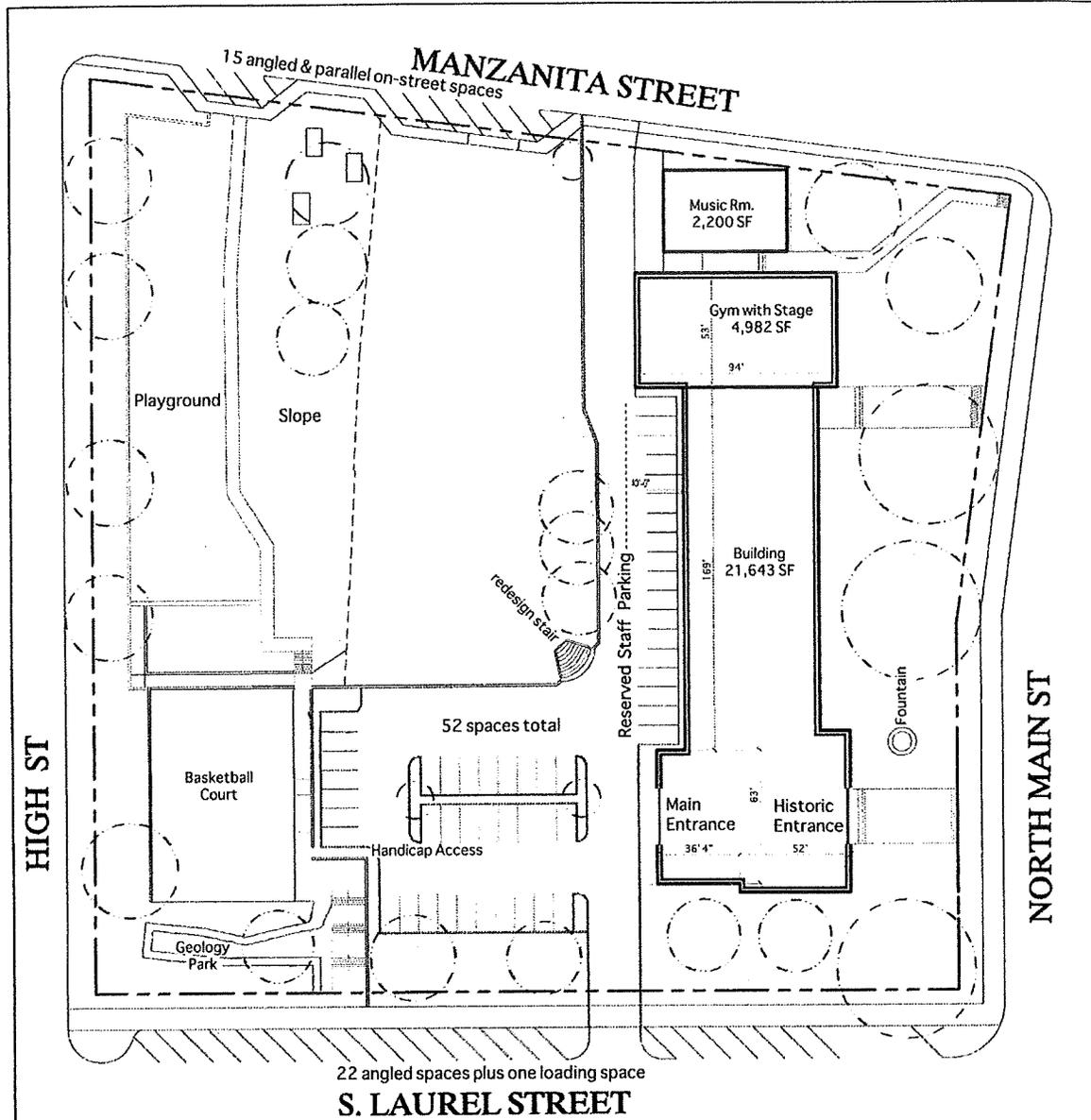


PARKING OPTION D - LOT ADJACENT TO BUILDING, PLUS NEW LOT WHERE SOUTH WING OF BUILDING IS REMOVED
 83 SPACES ONSITE - 37 ON STREET
 FIELD IS NARROWED BY 24 TO 27 FEET ALONG FULL LENGTH



BRISCOE SCHOOL

ASHLAND SCHOOL DISTRICT



PARTNERSHIP OPTION WING OF BUILDING REMOVED
 52 SPACES ONSITE ~ 38 ON STREET
 PARK OPEN SPACE KEPT 99% INTACT


 NEAR SCALE
 AUGUST 21, 2017

Briscoe ~ City Hall

Lithia Arts Guild of Oregon

**JACKSON COUNTY SCHOOL DISTRICT #5
Ashland School District – Ashland, Oregon**

Resolution # 2017-07B

WHEREAS, the Board-appointed Ashland School District Facilities Committee, the Superintendent and the Finance Director have collaboratively conducted an extensive assessment of the history and condition of the Briscoe school property; and

WHEREAS, the Board of Directors has solicited community input through public comment at meetings of the Facilities Committee and the Board, as well as two community forums dedicated to an exploration of possible uses for the property; and

WHEREAS, the Board commissioned an updated demographic study with enrollment projections for the Ashland School District in the coming decade indicating that enrollment is expected to remain flat or increase only slightly; and

WHEREAS, the condition of Briscoe would require extensive renovation to be usable as a school site by the District; and

WHEREAS, the community has expressed strong interest in maintaining the open space around the school as public park and playground space, and the City of Ashland has a commensurate goal for the community;

NOW, THEREFORE, BE IT RESOLVED by the Board of Directors of the Ashland School District as follows:

ONE: the Board accept the recommendation of the Facilities Committee that the District no longer invest resources in Briscoe beyond maintaining the facility as necessary for the needs of the current tenants;

TWO: the District move towards responsibly divesting the property.

THREE: in so doing, the Board will continue to pursue opportunities to retain public ownership of the property.

Dated this fourteenth day of August 2017

Board Chair, Deneice Covert Zeve

Superintendent, Kelly Raymond

SAVE OUR SCHOOLS & PLAYGROUNDS

MISSION STATEMENT

"The local, grassroots community group, Save Our Schools & Playgrounds, supports Ashland schools and the preservation of school playgrounds and playing fields for use as neighborhood parks."



REVISED NOTICE OF APPLICATION

PLANNING ACTION: PA- 2017-01294

SUBJECT PROPERTY: 128 Central Avenue

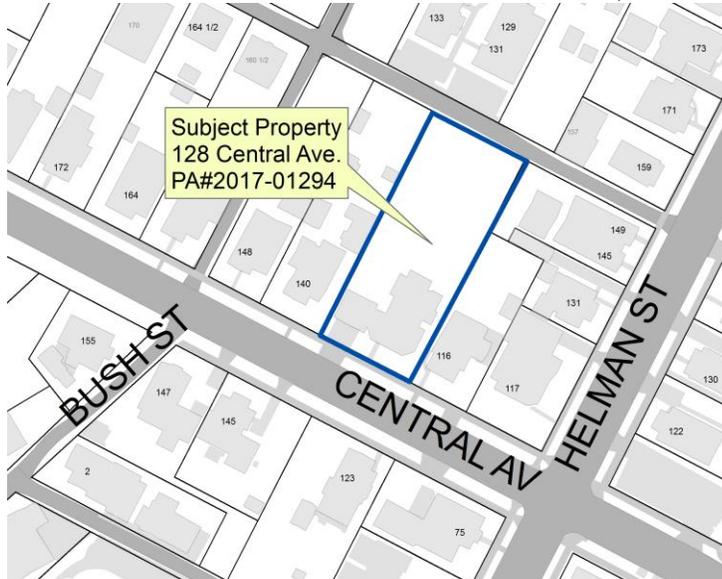
OWNER/APPLICANT: Robert Baldwin/Rogue Planning & Development Services

DESCRIPTION: A request for Site Design Review for an eight-unit multi-family development for the property located at 128 Central Avenue. The existing house contains two units. The proposal has been revised to add five units at the rear of the property and one unit above the reconstructed garage at the front of the property. The application also includes requests for a Conditional Use Permit to exceed the Maximum Permitted Floor Area in the Skidmore Academy Historic District by 21 percent (985 square feet) and for a Tree Removal Permit to remove seven trees six-inches in diameter-at-breast-height or greater. **COMPREHENSIVE PLAN DESIGNATION:** High Density Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP #:** 391E 04CC; **TAX LOT:** 4500.

The Ashland Historic Commission will also review this Planning Action on **Wednesday October 4, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way. The Ashland Tree Commission will also review this Planning Action on **Thursday, October 5, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

NOTICE OF COMPLETE APPLICATION: September 25, 2017

DEADLINE FOR SUBMISSION OF WRITTEN COMMENTS: October 9, 2017



The Ashland Planning Division Staff has received a complete application for the property noted above.

Any affected property owner or resident has a right to submit written comments to the City of Ashland Planning Division, 51 Winburn Way, Ashland, Oregon 97520 prior to 4:30 p.m. on the deadline date shown above.

Ashland Planning Division Staff determine if a Land Use application is complete within 30 days of submittal. Upon determination of completeness, a notice is sent to surrounding properties within 200 feet of the property submitting application which allows for a 14 day comment period. After the comment period and not more than 45 days from the application being deemed complete, the Planning Division Staff shall make a final decision on the application. A notice of decision is mailed to the same properties within 5 days of decision. An appeal to the Planning Commission of the Planning Division Staff's decision must be made in writing to the Ashland Planning Division within 12 days from the date of the mailing of final decision. (AMC 18.5.1.050.G)

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Department to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Division, Community Development & Engineering Services Building, 51 Winburn Way, Ashland, Oregon 97520.

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division at 541-488-5305.

SITE DESIGN AND USE STANDARDS

18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. *Exception to the Site Development and Design Standards.* The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

CONDITIONAL USE PERMITS

18.5.4.050.A

A Conditional Use Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.
3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.
 - a. Similarity in scale, bulk, and coverage.
 - b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
 - c. Architectural compatibility with the impact area.
 - d. Air quality, including the generation of dust, odors, or other environmental pollutants.
 - e. Generation of noise, light, and glare.
 - f. The development of adjacent properties as envisioned in the Comprehensive Plan.
 - g. Other factors found to be relevant by the approval authority for review of the proposed use.
4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.
5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.
 - a. WR and RR. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - b. R-1. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
 - d. C-1. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
 - e. C-1-D. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 1.00 gross floor to area ratio, complying with all ordinance requirements.
 - f. E-1. The general office uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
 - g. M-1. The general light industrial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, complying with all ordinance requirements.
 - h. CM-C1. The general light industrial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
 - i. CM-OE and CM-MU. The general office uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area, complying with all ordinance requirements.
 - k. CM-NC. The retail commercial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area ratio, complying

with all ordinance requirements.

- I. HC, NM, and SOU. The permitted uses listed in chapters 18.3.3 Health Care Services, 18.3.5 North Mountain Neighborhood, and 18.3.6 Southern Oregon University District, respectively, complying with all ordinance requirements.

TREE REMOVAL PERMIT

18.5.7.040.B

1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
 - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. **Tree That is Not a Hazard.** A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
 - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
 - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
 - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
 - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

September 21, 2017

**Site Design Review Application
For
Multi-Family Site Review
AMENDED FINDINGS**

Request:

A request for a Multi-Family Site Design Review for an eight-unit development was submitted on July 19, 2017. The application was deemed complete on August 29, 2017.

The development previously was proposed to consist of the two-unit residence at the front of the property, and six new units in two tri-plex structures at the rear of the property accessed via the alley. The exiting garage adjacent to the front residence accessed via Central was proposed to be retained. A Conditional Use Permit to exceed the Maximum Permitted Floor Area in the Skidmore Academy Historic District by nine percent was proposed. The application required a tree removal request to remove six trees.

Following the September 6th Historic Commission meeting, and the September 7th Tree Commission meeting, design modifications were made to accommodate for the large Ponderosa Pine on the west side of the property.

Modified Proposal:

The request is still for an eight-unit multi-family development. The proposal consists of the existing, duplexed residence at 128 Central, a new unit above the reconstructed garage accessed via Central Avenue, two small cottages and a triplex structure. The cottages and the tri-plex are accessed via from the rear alley. The new units are all proposed to be one-bedroom units with less than 500 square feet of gross habitable floor area.

No modifications are proposed to the existing, historic contributing residence, excepting the installation / removal / relocation of the electric meters at the rear of the structure. The residence has a large wrap around front porch and a strong orientation towards Central Avenue. There are architectural details such as turned posts, subdued Queen Anne details with diamond shingle pattern in the gable ends, spindle bracketry under the eaves, trim details, etc. that give the structure architectural interest.

To the west of the residence is a 24 X 24, two vehicle garage that was constructed sometime in the 1960s. This structure lacks any distinguishable characteristics. The garage will be replaced with a new, two-vehicle garage with unit above.

At the rear of the property accessed via the alley, in place of the previously proposed triplex along the west property line, two separate cottages area proposed. Unit A is 488 square feet and has a small front

porch that faces the alley and parking area. Unit A has been shifted 14-feet to the west to accommodate the large 26-inch DBH Ponderosa Pine tree. Unit B is to the south of Unit A. It is proposed to be 495 square feet. Unit B is proposed six-feet from the west property line, it is outside of the dripline of the Ponderosa pine tree. Unit B also has a front porch that faces east, into the courtyard area.

Across the courtyard area, a tri-plex consisting of three units is proposed along the east property line. Units C and D face the internal courtyard. The end unit, Unit E faces the alley and the parking area and units. Units C and D are 495 square feet and Unit E is 488 square feet.

The proposed development exceeds the maximum permitted floor area by 21 percent and requires a Conditional Use Permit for the increased area.

Maximum Permitted Floor Area:

Adjusted Lot Area:	8,856 SF
Allowed MPFA for Eight Units:	4,605.12 SF
Up to 25% increase with CUP:	+1,151.28 SF
Total Maximum Allowed MPFA:	5,756.40 SF
Existing Residence:	2,054 SF
Rebuilt Garage Under Unit "F":	576 SF
Proposed New Units:	2,960 SF
Total Proposed MPFA:	5,590 SF or 21% OVER

The garage is 9-feet, 11-inches away from the residence. The porch cover nearly touches the eve of the existing garage. The garage, as a separate structure was excluded from the Maximum Permitted Floor Area (MPFA) calculations. With the revised layout with the unit above the garage the floor area of the garage is included in the MPFA. The previous request was to exceed the MPFA by nine percent, the amended proposal increases the request to 21 percent overage.

All units are proposed to have exterior details reflective of the historic contributing structure, the structures have historically appropriate rhythms of openings and windows and door areas. With the number of units proposed within the allowed density and the historically appropriate proposed design, the request for a minor provision of additional floor area is reasonable. The proposed development is similar in bulk, scale and coverage as other multi-family developments in the vicinity.

The proposed modifications do not alter the required number of parking spaces. The parking is provided adjacent to the alley, in the garage and as on-street credits.

The proposal requires ten (10) bicycle parking spaces. A covered rack for six bicycle parking spaces is proposed at the rear of the property. The bicycle parking structure may need to be relocated depending upon the location of the electrical transformer. Regardless, a secure, covered structure for the parking

of six bicycles will be provided. The other spaces are on the porch of the front residence using the Cycloc product or similar and in the garage.

The trash and recycle enclosure area is proposed adjacent to the east property line. No landscape buffer is proposed along the east property line due to the refuse area fencing providing a buffer. A eleven-foot landscape buffer is proposed along the west property line.

The required open space area is 1,312 square feet. The proposed open spaces consist of approximately 485 square foot of porch area for the triplex unit, cottages and upper level units. The historic residence has a 372-square foot porch. There is a 378-square foot deck at the rear, on the west side of the structure. A common patio area is between the triplex units and the front house. More than 1,400 square feet in area has been provided in "functional" open space. Other landscape areas are provided that will have shrubbery, bark mulch and other plantings. The landscaping plan provides two new street trees, a shade tree in the eleven-foot landscape buffer along the west property line, and four new trees in the landscape areas.

A detailed survey of the trees has been provided. There are a variety of large stature trees that have generally been well taken care of. A Tree Protection and Preservation Plan has been provided. There are 14 trees greater than six-inches in diameter at breast height (DBH) on the property. Of those, ten are in the development area. There are seven trees proposed for removal. The property owner is requesting an exception to not plant "trees that will achieve similar size and stature at maturity" to replace the Ponderosa and the Deodar cedar trees. Conifer trees are excellent specimen in areas where there is room to grow but are not necessarily compatible with six new residential units, within an eight unit development.

There are three trees within 15-feet of the proposed development areas on the property to the east. There are two trees within 15-feet of the proposed development on the property to the west.

Following the Commission meetings, the project was redesigned to remove on the units from the triplex, leaving two cottages and allowing area for the preservation of the large ponderosa pine near the west property line. This unit was shifted to the front of the property and is now proposed above a front loading garage.

At the Tree Commission meeting, recommendation was made that the parking area shade tree and landscape buffer get relocated to the east property line to provide shade for the adjacent property to the east. Parking area shade trees are required for the new development, having the shade tree along the west property line provides shade for the proposed parking area. With the required amount of electrical facility installation to service not only the existing property but adjacent properties in the area, it would be inappropriate to plant a tree in conflict with the electric power. The secure trash and recycle area is also along the east property line and will be screened with fencing and is required to be solid surface, a tree would also conflict with these standards.

Additionally, six-inch caliper mitigation trees are not proposed. Upon consultation with the project landscape architect and arborist, six-inch trees do not survive as well as two-inch caliper trees, they are difficult to obtain, and they are more costly.

To accommodate the electrical site work necessary on the site and provide adequate room for essential functions of development including required separations between buildings, site grading, storm water improvements, a few of the sites trees, and, two of the larger trees on site require removal. Findings address the tree removal criteria are attached. The street tree is proposed for removal so that two larger stature trees can be replanted in the parkrow.

Along the east property line there is a power pole that has lines that run parallel to the alley. From the pole near the northeast corner a secondary service pole that feeds the existing residence and the property to the west. When the site develops, the electric service is required to be undergrounded. The service upgrades will require a below ground vault. The proposal is to place the vault near the alley and the northeast property line. The neighbors service would be undergrounded to their residence along the similar electrical path as the overhead. The route of the line depends on the service location. The City of Ashland Electric Department has been consulted and has reviewed the proposed layout.

The water service will be installed from Central Ave, along the east property line as close to the residence as possible to reduce the impacts on the large cedar tree on the adjacent property to the east.

Sanitary sewer service will go the alley.

A sidewalk / path system is proposed through the development in order to provide a safe walking route to the public sidewalk on Central Avenue and from the units to the parking area and to the trash / recycle enclosure area. The unit above the garage will have access via a gravel walkway system that connects to the driveway and to the sidewalk.

It is the applicant's understanding that the alley will be required to be paved from the west property line to the east at the intersection of the alley and Helman.

Conclusion:

In conclusion, the applicant finds that the proposed modifications to the development further comply with the standards for multi-family development and the permitted uses in the R-3 zone. The units proposed are a desirable size for individuals, and some couples comfortable with the small unit size. The property is near downtown, and there is an array of amenities provided within walking distance of the neighborhood. According to Walkscore, the property is Very Walkable and has a score of 87. <https://www.walkscore.com/score/128-central-ave-ashland-or-97520>

There are six, small, new energy efficient units proposed combined with the generous site amenities including mature trees, covered, secure bicycle parking, automobile parking, in close proximity to

downtown and within walking distance of many businesses, bus routes, and the ambiance of an established neighborhood.

New, smaller stature trees and ground cover landscaping is proposed that is compatible with a multi-family development. Adequate outdoor space exists to allow for tenant outdoor space that is functional to the residents. The porches provide valuable square footage of private outdoor living area that is more than ten percent of the floor area of the new units. The common patio area provides for a common area where tenants can interact with others or provide a small gathering area. These combined areas encompass more than eight percent of the site area.

The applicant finds that all of the applicable City of Ashland requirements have been met or can be met through the imposition of conditions of approval.

On the following pages, the criteria from the Ashland Land Use Ordinance as it pertains to Site Design Review and Conditional Use Permit criteria have been addressed. The City of Ashland criteria are in Times New Roman font and the applicant's findings are in Calibri font.

**Site Development Design Standards Approval Criteria:
Ashland Municipal Code 18.5.2.050**

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

The subject property is zoned R-3, High Density Multiple Family Residential. The parcel is 16,400 square feet (.376 ac) and meets minimum lot area and minimum lot dimensions in the R-3 zone.

The request is still for an eight-unit multi-family development. The proposal consists of the existing, duplexed residence at 128 Central, a new unit above the reconstructed garage accessed via Central Avenue, two small cottages and a triplex structure. The cottages and the tri-plex are accessed via from the rear alley. The new units are all proposed to be one-bedroom units with less than 500 square feet of gross habitable floor area.

The existing setbacks are slightly non-conforming to the front property line (19-feet, 1 1/2 - inches façade of the garage). The replacement garage is proposed at 20-feet from the front property line. This is to preserve the cedar tree that is to the north of the existing garage. A 10-inch DBH holly tree is proposed to be removed.

All proposed construction meets or exceeds minimum setbacks. The cottage on the west side is setback 14-feet to accommodate the area to preserve the 26-inch DBH Ponderosa Pine tree that is near the west property line. The units are separated from the front residence by more than 12-feet. There is more than 12-feet of separation between the new structures. The existing nine-foot, six-inch of separation between the historic residence and the garage will be replicated with the new construction, this slightly less than the required separation 12-feet. This requires an exception.

The proposed dwellings have traditional styling that reflects the character of the historic contributing structure. Similar siding, reveal, and Queen Anne details in the gable end are similar to the front residence. There are varying roof forms, with the stepping down the slight grade to break up the gable line and the mass of the structures. The proposed porches add architectural interest and add variation to the horizontal plane. There are numerous windows to allow for ample natural light into the units. The porch posts are proposed to be square instead of turned like the historic structures porch posts.

The garage is presently oriented towards Central and has no distinguishing architectural features, the proposed garage with unit above is setback further, and has design features reflective of the existing residence and the proposed residences.

The solar setback standards are met with the development because the structures are single story, more than 25-feet from the 16-foot wide right-of-way and there is only a four percent slope. The units will not cast a shadow beyond the width of the right-of-way.

*Allowed Density 18.2.5.080: .376 X 20 = 7.5
Proposed Density: Seven residential units (6.5);*

Six units less than 500 sf (6X.75 = 4.5)
Duplexed front Residence

The required open space area is 1,312 square feet. The proposed open spaces consist of approximately 485 square foot of porch area for the triplex unit, cottages and upper level units. The historic residence has a 372-square foot porch. There is a 378-square foot deck at the rear, on the west side of the structure. A common patio area is between the triplex units and the front house. More than 1,400 square feet in area has been provided in "functional" open space. Other landscape areas are provided that will have shrubbery, bark mulch and other plantings. The landscaping plan provides two new street trees, a shade tree in the eleven-foot landscape buffer along the west property line, and four new trees in the landscape areas. Other landscape areas are provided that will have shrubbery, bark mulch and plantings.

At the rear of the property, accessed via the alley, there are six parking spaces proposed. An offloading zone for an ADA van accessible space is provided adjacent to the ten-foot buffer along the west property line. A parking lot shade tree is proposed to be planted in the buffer on the west side to provide shade for the parking spaces.

Lot Coverage: Proposed impervious areas including existing building footprints, proposed building footprints, pathways, driveways, deck (excluding 200 SF) is 9,071 SF of the 16,400 SF lot for a total lot coverage of 55 percent, this is less than the maximum of 75 percent in the zone.

Parking: The proposed development requires ten (10) vehicle parking spaces.

Six units less than 500 sf:	6
Front Duplex Residence:	4
Total:	10

The two-vehicle garage will provide the two parking spaces for the primary residence. The upper unit is proposed to utilize two, on-street parking credits. In accordance with AMC 18.4.3.060.A. for on-street credits the property has more than 44-feet of continuous curb along the frontage of the property. Central Avenue is a fully improved Neighborhood Street with curb, gutter sidewalk and park row. The property is not a corner lot. Lastly, the property is more than 200-feet from downtown and the SO zone.

At the rear of the property, accessed via the alley, six head-in parking spaces are proposed. An offloading zone for an ADA van accessible space is provided adjacent to the five-foot buffer along the west property line.

Ten bicycle parking spaces are required. There is a six-bike rack near the parking area adjacent to the alley, two spaces in the garage and two on the covered porch. U-shaped bicycle rack staples will be used

to accommodate for the six spaces, a wall mounting system is proposed for the two spaces on the porch. See attached specifications for CycLoc.

Energy Usage: All of the units within the proposed development will be constructed to the current energy codes and building code standards. A detailed analysis of the actual energy consumption has not been determined but the square footage, use of ductless heat pump systems, LED lighting all contribute to the low energy consumption anticipated in the triplex units.

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

The property is in the Skidmore Historic District. Development is subject to AMC 18.4.2.050. The design complies with the applicable overlay zone requirements.

18.4.2.050 Historic District Development

B. Historic District Design Standards.

The property is occupied by a historic contributing, two-story duplexed residence. The circa 1895, James Duncan House is a historic contributing structure. It is not proposed to be altered as part of this proposal. To the west of the residence is a 24 X 24, two vehicle garage that was constructed sometime in the 1960s. This structure lacks any distinguishable characteristics. The garage will be replaced with a new, two-vehicle garage with unit above.

Height: The proposed structures are similar to average heights of structures in the vicinity. The proposed garage with unit above is similar to the height of the existing structure. The single story cottage and triplex structures are substantially shorter than the historic contributing structure.

Scale: The scale of the proposed structures are within the range of other multi-family dwellings in the vicinity. The limited square footage of the structures, the separations between the structures, the incorporation of single and two story construction and the overall layout of the proposed multi-family development is consistent with the scale of multifamily development in the vicinity and future development potential on adjacent properties.

Massing: The proposal to incorporate the unit over the garage is to preserve a large Ponderosa Pine and to vary the massing of the development. The unit over the garage has a 6.5-foot deep porch that steps half of the second story above the garage back and reduces the massing on Central. The removal of the triplex along the west side of the rear portion of the property and the proposal for detached cottages in combination with the triplex, the mass of the development is spread through the property. Additionally, through the incorporation of a stepped gable with a hipped gable over the middle unit the ridge line has been broken up to reduce the mass. The multiple gable ends and the incorporation of a covered porch, the mass of the triplex has been varied.

Setback: The proposed setbacks comply and exceed the minimum standards. The garage will be setback to 20-feet, though this is the minimum, it is necessary to save additional trees on the property. The proposed units have an 12.5-foot separation between the structures in the central courtyard.

Roof: The proposed roof pitches are 8:12. This pitch is similar to the various roof pitches in the vicinity. The roof forms are broken into a series of masses to reflect the subtle grade changes and to prevent a continuous ridge line. There are smaller gables for the porch roofs that add interest to the roof lines. The roof materials are proposed as composition shingles.

Rhythm of Openings: The units have a consistently spaced window pattern that is consistent with the rhythm of openings found on the historic residence. The windows are proposed as single hung, vinyl windows. The windows will not be bright white.

Base or Platform: The concrete foundation stem wall will be exposed for 12" to 36". This provides a sense of a base and makes the structure appear grounded. Unless dictated by code or to allow for the construction of ADA accessible units, there will be a single stair on the majority of the units to the deck of the porch. To limit the amount of excavation in proximity to the Ponderosa pine tree that may necessitate two or more risers.

Form: The form of the proposed development including the triplex is consistent with multi-family dwelling development and is sensitive to the Skidmore Academy Historic District. The units are small and single story, reducing visual impacts. The unit over the garage is setback behind the façade of the front porch of the historic home slightly. The unit is lower in overall height than the existing historic structure.

Entrances: The primary residence has an entrance that generally faces the public street. Due to the substantial setback of the triplex units from the public streets, there is not a requirement for a visible entrance. There are very prominent entrances provided on the units. The north unit of the triplex and the cottage each face the alley with an entry porch. The other units face the internal courtyard. These units also have porches which distinguish the entrance. The unit above the garage will have an exterior entrance at the rear of the structure. A single man door will provide access to the proposed roof top deck.

Imitation: The proposed triplexes have elements of the existing historic contributing structure on the property and brings in the Queen Anne stylings but has a more subdued look vs. the historic contributing structure. The siding exposure and reveal are similar to the existing structure. The color will match or be complementary. The units are proposed to have square posts instead of the turned posts, the crisscross brackets will not be mimicked but the diamond shingle detail in the gable end is proposed to be carried over to the new units. Though there are a variety of housing

styles in the vicinity. Vernacular I-home, craftsman and American bungalow cottage style construction are found throughout the Skidmore Academy historic district. The proposed units have elements of those design styles as well.

Garage Placement: A street facing garage is existing. A replacement garage is proposed that complies with the minimum setbacks. A unit is proposed above the garage. The unit will be accessed via the rear. The existing garage has a single door. The proposed garage will have two, eight-foot doors that will not detract from the historic residence.

C. Site Development and Design Standards.

The proposed site development complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

The proposed layout does not provide for any hidden areas that are not survey able by tenants of the site. The use of low level lighting, low growing vegetation and open space orientation, the design provides areas of safety for the tenants.

Building Orientation.

Building Orientation to Street. Dwelling units shall have their primary orientation toward a street. Where residential buildings are located within 20 feet of a street, they shall have a primary entrance opening toward the street and connected to the right-of-way via an approved walkway.

The primary residence on the site has its primary orientation towards Central Avenue. A large front porch wraps around the front of the residence. The garage is slightly recessed behind the eave of the roofline of the historic structure. The garage unit access is at the rear of the structure. The rooftop deck provides orientation of the unit towards Central Avenue. The rear cottages and triplex units are not visible to the public street and are not oriented towards Central. The units are oriented towards the alley upon which they front and towards their shared courtyard.

Limitation on Parking between Primary Entrance and Street. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or on one or both sides.

No parking is proposed between the building and the street. All parking is located to the side of the structure in the garage and at the rear of the property.

Build-to Line. Where a new building is proposed in a zone that requires a build-to line or maximum front setback yard, except as otherwise required for clear vision at intersections, the building shall comply with the build-to line standard.

The setbacks are existing for the primary residence and the detached garage. The proposed replacement garage is setback 20-feet from the front property line.

Garages. Alleys and Shared Drives. Where a lot abuts a rear or side alley, or a shared driveway, including flag drives, the garage or carport opening(s) for that dwelling shall orient to the alley or shared drive, as applicable, and not a street.

New vehicular access to the site is from the alley and not from the street. The property has utilized on-street, garage and driveway parking. There has never been alley access. It is being created to accommodate for the new development.

A front-loading garage exists. The garage is proposed to be re-constructed with a unit above. The access from Central will be retained.

Building Materials. Building materials and paint colors should be compatible with the surrounding area. Very bright primary or neon-type paint colors, which attract attention to the building or use, are unacceptable.

The building materials are compatible with the surrounding area. The materials are mixture of modern with classic elements. The units are proposed to have concrete stem wall that forms the base. The units are proposed to have horizontal, hardi-plank siding with five-inch exposure, with diamond shingle treatment in the gable ends which reflects the treatment in the historic home gable. Square porch posts, and wood and metal railings are also proposed. The roofing is proposed as composite shingles. The structures are proposed to be white like the primary residence.

Streetscape. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

The landscaping plan provides two new street trees. The street trees will be planted in accordance with the street tree standards from 18.4.4.030.E.

Landscaping and Recycle/Refuse Disposal Areas. Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

A common refuse area will be provided in a screened area adjacent to the alley in the northeast corner. It is anticipated that the electric vault, if in the ground can have the dumpster rolled over it. The property owner would prefer an above ground pedestal to the south of the trash area in a location that is protected from damage and is screened from the alley while still providing adequate service for the property. This is being discussed with the City of Ashland Electric Division. An electrician has not been selected for the job since it has not obtained approval so the exact sizing of the equipment is unknown and still in the early design stages.

There is an 11-foot landscape buffer along the west property line. This will allow for adequate area to provide a large stature deciduous tree to shade the parking area.

Open Space.

1. Recreation Area. An area equal to at least eight percent of the lot area shall be dedicated to open space for recreational use by the tenants of the development.

An area of more than 1,400 square feet in area is devoted to the open space areas available for recreational use by the tenants.

2. Surfacing. Areas covered by shrubs, bark mulch, and other ground covers that do not provide suitable surface for human use may not be counted towards this requirement.

The areas that account for the open space and recreation areas are surfaced with decking, porch flooring, and paver / concrete patio area.

3. Decks and Patios. Decks, patios, and similar areas are eligible for open space.

The open spaces are a mixture of common patio area and semi-private deck and porch areas.

4. Play Areas. Play areas for children are required for projects of greater than 20 units that are designed to include families. Play areas are eligible for open space.

A play area is not provided in the eight-unit development.

18.4.3.080 Vehicle Area Design

A. Parking Location

The proposed parking is not located between the building and the street. The parking is located adjacent to the alley.

B. Parking Area Design. Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in 18.4.3.080.B. See also, accessible parking space requirements in section 18.4.3.050 and parking lot and screening standards in subsection 18.4.4.030.F.

1. Parking spaces shall be a minimum of 9 feet by 18 feet.

Three of the six proposed parking spaces are 9 feet by 18 feet.

2. Up to 50 percent of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 feet by 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."

Three of the six proposed parking spaces are 8 feet by 16 feet. They will be painted "compact car only".

3. Parking spaces shall have a back-up maneuvering space not less than 22 feet, except where parking is angled, and which does not necessitate moving of other vehicles.

All proposed parking has a back-up space of 22-feet. The alley will be improved to accommodate the backing up dimension of 22-feet.

Shared Use of Driveways and Curb Cuts.

The driveway to the existing street facing garage is a non-conforming situation. The new parking is proposed to be accessed via the public alley at the rear of the property.

b. Developments subject to a planning action shall remove all curb cuts and driveway approaches not shown to be necessary for existing improvements or the proposed development. Curb cuts and approaches shall be replaced with standard curb, gutter, sidewalk, and planter/furnishings strip as appropriate.

The Central Avenue curb cut is necessary for existing improvements and will be retained.

c. If the site is served by a shared access or alley, access for motor vehicles must be from the shared access or alley and not from the street frontage.

All new access is from the alley. The garage access from Central Avenue is pre-existing non-conforming it is necessary to retain. The existing upstairs unit has always utilized on-street

parking. The new units will be served by the alley and all new access for motor vehicles will be from the alley and not from the street.

5. Alley Access. Where a property has alley access, vehicle access shall be taken from the alley and driveway approaches and curb cuts onto adjacent streets are not permitted.

No new curb cuts are proposed. The existing access from Central is pre-existing non-conforming. The new vehicle access is from the alley.

E. Parking and Access Construction. The development and maintenance as provided below, shall apply in all cases, except single-family dwellings.

1. Paving. All required parking areas, aisles, turn-arounds, and driveways shall be paved with concrete, asphaltic, porous solid surface, or comparable surfacing, constructed to standards on file in the office of the City Engineer.

The parking areas are proposed to be paved with concrete, asphaltic or porous solid surface depending on the storm water detention design.

2. Drainage. All required parking areas, aisles, and turn-arounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.

Drainage for the parking area will be provided to prevent sheet flow or drainage of waters into the public rights-of-way or onto abutting private property.

3. Driveway Approaches. Approaches shall be paved with concrete surfacing constructed to standards on file in the office of the City Engineer.

Driveway approach is pre-existing and is concrete.

4. Marking. Parking lots of more than seven spaces shall have all spaces permanently and clearly marked.

The parking spaces will be marked with paint or wheel stops.

5. Wheel stops. Wheel stops shall be a minimum of four inches in height and width and six feet in length. They shall be firmly attached to the ground and so constructed as to withstand normal wear. Wheel stops shall be provided where appropriate for all spaces abutting property lines, buildings, landscaping, and no vehicle shall overhang a public right-of-way.

Wheel stops, if provided will comply with this section.

6. Walls and Hedges

a. Where a parking facility is adjacent to a street, a decorative masonry wall or evergreen hedge screen between 30 and 42 inches in height and a minimum of 12 inches in width shall be established parallel to and not nearer than two feet from the right-of-way line.

The parking is not adjacent to a street.

A sight-obscuring fence, wall, or evergreen hedge will be provided along the side property lines to screen the parking spaces.

A 198 SF landscape buffer is provided.

18.4.3.090 Pedestrian Access and Circulation

A continuous walkway system extends throughout the development, and to existing public sidewalks. The walkway provides a safe, reasonably direct, and convenient walkway connection between primary building entrances and all adjacent streets. The building entrances are connected to one another to the greatest extent practicable. The walkway connects the on-site parking areas, common areas, and connect to the public sidewalk on Central.

18.4.4.030 Landscaping and Screening

The proposed landscaping plan and the irrigation plan that will be submitted with the building permits complies with the Irrigation and Water Conserving Landscaping requirements of the City of Ashland. The conceptual landscaping plan submitted with the application has been designed so that plant coverage of 50 percent after one year, and 90 percent within five years of planting is met. The eleven-foot landscape buffer will have a larger stature shade tree. A hedge is not proposed due to the presence of a solid panel fence adjacent to a parking space along the property line to the west. This fence will be replaced as its present location is not along the property line. Two-inches of mulch will be provided in all non-turf areas after planting.

There is a short and scraggly street tree that will be replaced with two new street trees. The street trees will be two-inch caliper at the time of planting.

Low level landscape lighting for the paths will be provided throughout the open space. Each unit will have recessed can lights in the porch roof, or a shrouded yard light that provides down-lighting and security for the unit but will not directly illuminate adjacent properties. There is a six-

foot fence as a buffer. No plant materials are proposed that prevent surveillance of the open space or the porches.

Tree Preservation, Protection, and Removal

18.4.5.030 Tree Protection: *A detailed site survey of the trees has been provided. There are 14 trees greater than six-inches in diameter at breast height. Of those, ten are in the area of the development.*

The trees along the property lines of the adjacent properties are protected by a six-foot tall fence. This fence will be reconstructed along the property lines following site development. For the trees on the site, six-foot chain link panels are proposed to be installed at the dripline or in accordance with the tree protection plan that's has been provided with the application. Tree #5, an excellent condition, 26-inch DBH Ponderosa Pine is now proposed for preservation. The triplex that was originally proposed in the location of this tree has been redesigned to be two separate cottages. The tree is approximately six-feet from the property line, Unit A is now 14-feet from the property line and though it encroaches into the dripline of the Ponderosa, the project arborist does not believe that the impacts will have major implications on the health of the tree.

Excavation within the root zone area of a number of the sites trees will be necessary so the tree protection fencing is not able to encircle the dripline but will need to follow excavation routes. The project arborist / landscape professional will be on-site during excavation.

A lot of consideration was made as to how to layout and the number of units allowed by code, provide a much-needed housing size inventory, while preserving the maximum number of trees. The proposed layout takes advantage of long linear nature of the property, provides for the necessary parking improvements, and utility installation is accommodated for. The required separations between structures, and necessary aesthetic spacing. Functional open spaces, provision for trash and refuse areas all contributed to the proposed layout.

18.5.7 Tree Removal:

B. Tree Removal Permit.

a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.

There are fourteen trees on site, ten in the area of development. Of the ten trees, seven trees greater than six-inches in diameter at breast height (DBH) proposed for removal.

The trees proposed for removal are a double stemmed (12" and 14" DBH) apple, a 24" DBH apple, two, 12" Deodar cedar trees, and a 24" DBH Ponderosa pine.

The removal of these trees is proposed to allow for the development of the site with the allowed density, compatible with historic district design standards, in an economical footprint. The development of the site also requires substantial modifications to the site to accommodate for electrical infrastructure and storm water detention and drainage compliance.

Tree #9, the Birch is one of dwindling number of healthy birch trees in Ashland. It is unfortunately, in a location that will be substantially negatively impacted by the site development. With the excavation near the trunk, substantial grade alterations within the dripline to accommodate required parking, and walkways, utility installation for sanitary sewer, electrical, storm water detention and drainage facilities. The presence of the amount of irrigation the tree receives will be substantially altered and further its demise.

Tree #10 is a 24" DBH Ponderosa Pine. There are overhead power conflicts with this tree. When the site development happens, at the pole located just to the northeast of the tree, an underground vault is required for the proposed development and excavation to provide the neighbor to the east at 116 Central with underground service. The subject property and the property at 116 Central have a secondary overhead service the follows the east property line through the Ponderosa and the Deodars. This Ponderosa will be netatvley impacted by the substantial amount of excavation that will be necessary to install the required electrical infrastructure for the subject lot and adjacent properties.

The two 12" DBH Decors are both too close in proximity to the proposed units and both are within the foot print. These trees are also into the overhead powerlines that exist, and the new power must be installed in the same location but underground.

b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.

The removal of the trees will not have impacts on erosion, soil stability, flow of surface waters, and protection of adjacent trees or existing windbreaks. The structures, landscaping and other site improvements are necessary on the site that will cover the exposed soil. There are no surface waters.

c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.

There are a significant number of deciduous and confer trees within 200-feet of the property. The removal of the three trees will not have a negative impact on the densities, sizes, canopies or

species diversity. The proposed layout allows for the preservation of a substantial number of the sites large stature conifer trees. The removal of some allows for better preservation of other trees.

d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.

The proposal complies with residential densities. The high density multiple family residential site has a large number of larger stature conifer trees that do great in open landscapes with substantial amounts of irrigation. Any development on the site will substantially alter the soils porosity, permeable surfaces and in turn have a negative impact on the trees. The proposal allows for many of the sites trees to be preserved while achieving desired densities.

e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

Due to the nature of the development, high-density multi-family, no conifer trees are proposed. Seven deciduous trees are proposed in the landscaping. The trees shall be planted and maintained per the specifications of the Recommended Street Tree Guide.

D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Adequate city facilities exist to service the new units.

Water: *There is an existing eight-inch water main in Central Avenue that serves the property. The existing ¾ inch meter will be used for the residences at the front of the parcel adjacent to Central Avenue. The meters for the new units are proposed on east side of the parkrow to parallel the east property line back to the area past the Black Walnut where the lines will branch off to service the units. A common area meter will also be provided.*

Sanitary Sewer: *There is a four-inch line in the public alley to the north of the site. This is adequate to service the new units.*

Electrical: There is underground power in the south alley. All electrical service on the site will be served by two electrical transformers installed on the property. The triplexes will have a three-pack meter installed on the east side of the east triplex and the west side of the western triplex.

Storm Sewer: There are no storm sewer services in the vicinity excepting uphill from the majority of the development across the Central Avenue right-of-way. The project civil engineer is working with the public works department to devise a bubbler system that will comply with the building code and the public works standards for storm water treatment and detention.

Central Avenue is paved with curb, gutter sidewalk and parkrow along the frontage of the property which provides paved access to the development. Street trees are proposed in the parkrow. The 16-foot wide alley at the rear of the property is gravel. If this surface needs to be paved, the applicant is willing to pave to Helman Streets intersection with the alley.

The apartment buildings will have Fire Sprinklers installed as required by the Oregon Fire Code and Oregon Residential Structural Specialty Code.

E. Exception to the Site Development and Design Standards. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

An exception to the existing 9-foot, 6-inch setback between the historic residence and the reconstructed garage is necessary. The reduced separation allows for the relocation of one the units from the rear of the property in a tri-plex format in order to preserve large stature conifer tree. The site unique in that it is a multi-family lot that has a high number of dwelling units permitted by density but is heavily treed. The layout allows for the preservation of the large Ponderosa pine tree, and two large Cedar trees. Additionally, the reduced separation allows for a fullwidth, two vehicle garage that has two, single vehicle garage doors. Two doors is more historically compatible than a single, 16-foot door. The existing historic structure is more than 12-feet from the east property line which is 2X the minimum setback. This reduces the setback area between the historic structure and the west property line where the garage exists and the reconstructed garage is proposed.

18.5.4.050 Conditional Use Permit Approval Criteria

The conditional use permit request is to exceed the Maximum Permitted Floor Area in the historic district by nine-percent. The proposed development exceeds the maximum permitted floor area by nine percent and requires a Conditional Use Permit for the increased area.

Maximum Permitted Floor Area:

<i>Adjusted Lot Area:</i>	<i>8,856 SF</i>
<i>Allowed MPFA for Eight Units:</i>	<i>4,605.12 SF</i>
<i>Up to 25% increase with CUP:</i>	<i>+1,151.28 SF</i>
<i>Total Maximum Allowed MPFA:</i>	<i>5,756.40 SF</i>

<i>Existing Residence:</i>	<i>2,054 SF</i>
<i>Rebuilt Garage Under Unit "F":</i>	<i>576 SF</i>
<i>Proposed New Units:</i>	<i>2,960 SF</i>
<i>Total Proposed MPFA:</i>	<i>5,590 SF OR 21% OVER</i>

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.

The use of the site is residential which is consistent with the relevant comprehensive plan policies and does not violate and city, state or federal law.

2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.

As addressed above, there is adequate capacity of City facilities for water, sewer, electricity, urban storm drainage and paved access to and through the development.

3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.

The additional square footage will not have a greater adverse material effect on the livability of the impact area when compared to the development of the site as eight residential units. The proposal complies with the target use of the zone, the proposal complies with the site design standards applicable in the zone.

a. Similarity in scale, bulk, and coverage.

The proposed triplexes are similar in scale, bulk and coverage as other multi-family developments in the zone. The proposed single-story structures are similar in height as the multi-family development across the alley. The coverage on the subject property is less than that of the multi-family development across the alley. All of the separations between buildings including setbacks and lot coverages are met which provides for a reduced bulk and coverage.

The unit over the garage will be setback 20-feet from the front property line, the façade of the garage is behind the roofline of the porch of the historic residence. A portion of the upper floor is recessed an additional six-feet, five-inches to allow for the outdoor upper level porch, this reduces the street fronting mass of the new garage with unit above.

b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.

The square footage proposed to exceed the maximum permitted floor area will not have an impact on the generation of traffic. The use of the property is multi-family as envisioned in the code. The request is to fulfill the allowed density on the property. The units are small and have adequate parking as required by code proposed. The property is within walking distance of downtown, grocery and other retail stores, restaurants and bar. The employment center of town and near the bus route on N Main Street. The units are in a walkable neighborhood that has sidewalks throughout.

c. Architectural compatibility with the impact area.

The proposed dwellings have traditional styling that reflects the character of the historic contributing structure. Similar siding, reveal, and the Queen Anne style siding detail in the gable end are similar to the front residence. There are varying roof forms, with the stepping down the slight grade to break up the gable line and the mass of the tri-plex structure.

The two small cottages are also architecturally compatible with the impact area. The existing garage is setback less than 20-feet from the front property line. The proposed garage with unit above is setback more than 20-feet, the deck provides for an openspace, recesses the façade of the upper story behind the façade of the historic residence.

The proposed porches add architectural interest and add variation to the horizontal plane. There are numerous windows to allow for ample natural light into the units. The porch posts are proposed to square versus the turned posts the historic residence. The homes in the impact area are a mixture of vernacular, ranch style, bungalow and craftsman. The proposed buildings have elements of each of those design styles.

d. Air quality, including the generation of dust, odors, or other environmental pollutants.

The additional square footage for the development of the site will not impact the air quality in the impact area any more than the allowed multi-family development of the site at seven units.

e. Generation of noise, light, and glare.

The proposed increase in square footage will not impact the generation of light, glare or noise any more than the allowed multi-family development of the site with seven units would.

f. The development of adjacent properties as envisioned in the Comprehensive Plan.

The additional square footage will not prevent the multi-family development of the adjacent properties as envisioned in the Comprehensive Plan.

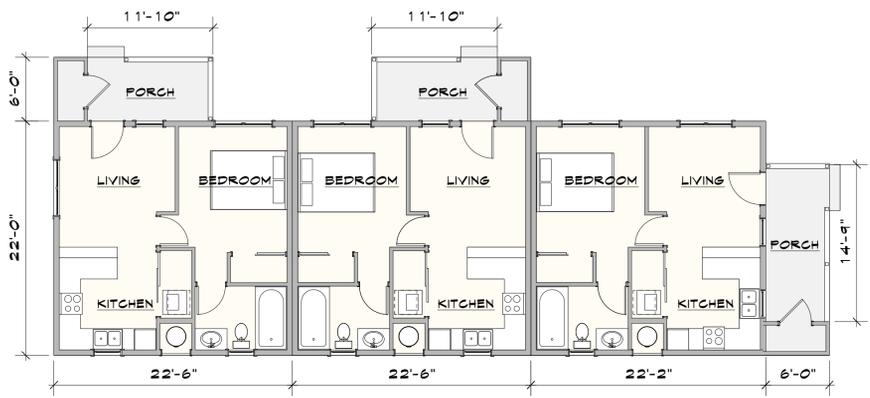
g. Other factors found to be relevant by the approval authority for review of the proposed use.

It can be found that all of the standards for the development of the site have been met.

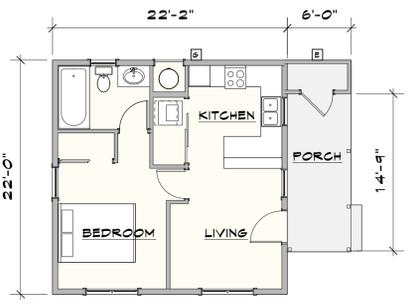
4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.

The conditional use to increase the MPFA square feet is an allowable use in the zone.

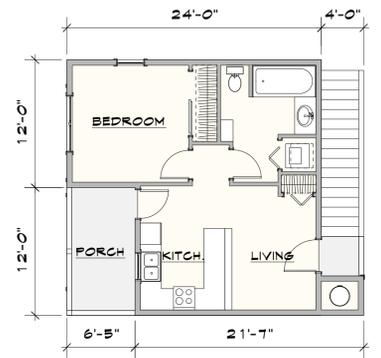
PRELIMINARY ONLY - NOT FOR CONSTRUCTION



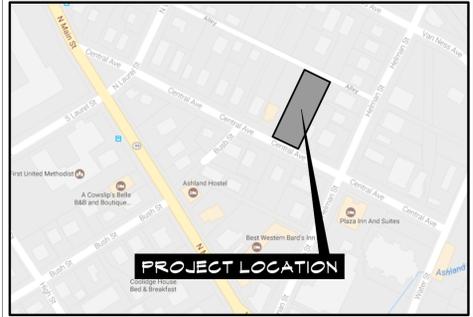
TRI-PLEX FLOOR PLAN
SCALE: 1/8" = 1'-0"



UNIT A(&B) PLAN
SCALE: 1/8" = 1'-0"



GAR. UNIT PLAN
SCALE: 1/8" = 1'-0"



VICINITY MAP
NOT TO SCALE

OWNER: ANDREW KRUG; ROBERT BALDWIN, AGENT
ADDRESS: 128 CENTRAL AVE., ASHLAND, OR. 97520
PHONE: 541-613-7625
APN: S9 1E 04 CC TAX LOT #4500
SIZE: 10,219 SQ. FT.
ZONING: R-3 (CITY OF ASHLAND)
OVERLAYS: HISTORIC DISTRICT

DESCRIPTION OF PROJECT:
CONSTRUCTION ONE TRIPLEX; TWO STAND-ALONE UNITS & ONE UNIT ABOVE RE-BUILT GARAGE, FOR A TOTAL OF SIX DWELLING UNITS; EACH WITH LESS THAN 500 SQ. FT. OF CONDITIONED SPACE.

(E) MAIN HOUSE TO REMAIN = 2
FIVE (N) UNITS OFF ALLEY = 5
ONE (N) UNIT OVER GARAGE = 1
TOTAL DWELLING UNITS = 8

LOT COVERAGE:
TOTAL LOT = 16,400 SQ. FT. OR 100%
(E) HOUSE (FOOTPRINT) = 2,400 SQ. FT.
(E & N) GARAGE (FOOTPRINT) = 576 SQ. FT.
(N) UNITS A & B (FOOTPRINT) = 1,179 SQ. FT.
(N) TRI-PLEX (FOOTPRINT) = 1,761 SQ. FT.
(N) PAVING / PARKING OFF ALLEY = 1,215 SQ. FT.
FRONT DRIVEWAY & HARDSCAPE = 1,346 SQ. FT.
TOTAL PROPOSED COVERAGE = 9,071 SQ. FT. OR 55.3%

LANDSCAPING:
SEE "L" SHEETS - MIN. 25% OF SITE TO BE LANDSCAPED.

PARKING REQ'D:
MAIN HOUSE, 4 SPACES REQ'D
2 TRIPLEXES, 6 SPACES REQ'D
TOTAL PARKING: 10 SPACES REQ'D

PARKING PROVIDED:
2 EXISTING SPACES, IN GARAGE, TO REMAIN
6 NEW SPACES OFF ALLEY
2 ON-STREET SPACE REQUESTED
TOTAL OF 10 SPACES PROVIDED

SOLAR SETBACK:
SLOPE = .05; HEIGHT = 15.53 FT
N/S DIM = 30'/3'45" = 75 FT MIN; S2 FT ACTUAL - USE "A"
SSE = 15.53-6'/3'45" = 24.25 FT MIN; S 1.93 FT PROVIDED

MPFA CALCULATION:
LOT AREA: 16,400 SF
ADJUSTED LOT AREA: 8,256 SF
ALLOWED MPFA FOR 8 UNITS: 4,605.12 SF
UP TO 25% INCREASE OF MPFA WITH CUP: +1,151.28 SF
TOTAL ALLOWED MPFA WITH CUP = 5,756.40 SF

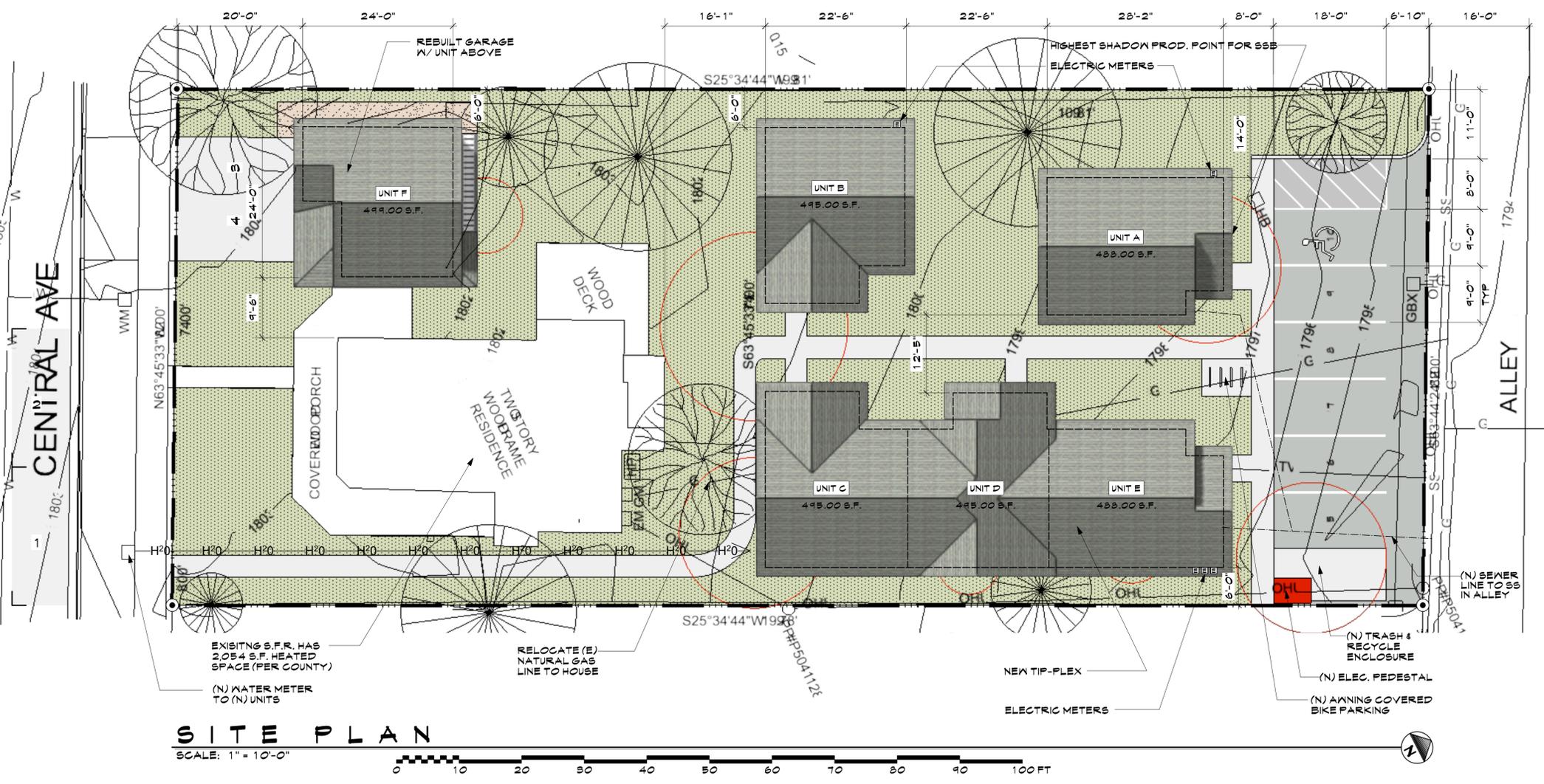
EXISTING SFR: = 2,054 SF
REBUILT GARAGE UNDER UNIT "F" = 576 SF
PROPOSED NEW UNITS: = 2,960 SF
TOTAL PROPOSED MPFA = 5,590 SF OR 21% OVER

**IMPORTANT NOTE: SCALE AS NOTED ONLY
VALID WHEN PRINTED AT 100%**

INDEX OF SHEETS:

SHEET 1	SITE PLAN • VICINITY MAP
SHEET 2	TRI-PLEX FLOOR PLAN
SHEET L-1.0	EXTERIOR ELEVATIONS
SHEET L-2.0	TREE PROTECTION
SHEET L3.0	PRELIM. IRRIGATION PLAN
	PRELIM. PLANTING PLAN

(E) = EXISTING
(N) = NEW



SITE PLAN
SCALE: 1" = 10'-0"



REVISIONS

NEW MULTI-UNIT DEVELOPMENT FOR:
Robert Baldwin
128 Central Ave • Ashland, Oregon 97520

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PETER L. CIPES
Building Design
317 North Main Street • Ashland, Oregon 97520 • 541-488-1096

A -
B D
PROFESSIONAL MEMBER
SINCE 1998

DATE: 09-20-17

DRAWN BY: PLC

SHEET

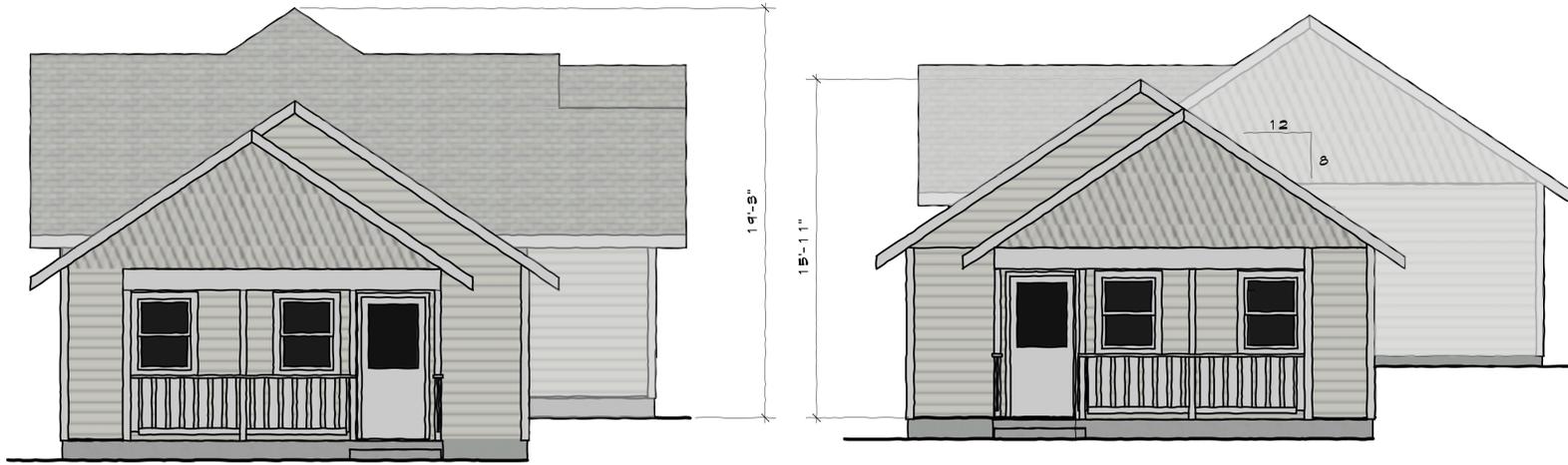
1

OF 5 SHEETS

PRELIMINARY ONLY - NOT FOR CONSTRUCTION

NOTES:

1. IN GENERAL, ALL EXTERIOR MATERIALS & COLORS SHALL MATCH (E) MAIN HOUSE.
2. USE WHITE LAP SIDING W/ 5" (MAX) EXPOSURE ON WALLS.
3. USE 1 X CORNER TRIM.
4. USE WHITE DIAMOND SHINGLES ON GABLE END WALLS.
5. BASIC SINGLE HUNG WINDOWS.



NORTH ELEVATION

SCALE: 1/4" = 1'-0"



GARAGE - SOUTH ELEVATION

SCALE: 1/4" = 1'-0"



TRI-PLEX / WEST ELEVATION

SCALE: 1/4" = 1'-0"



REVISIONS

NEW MULTI-UNIT DEVELOPMENT FOR:
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 128 Central Ave • Ashland, Oregon 97520

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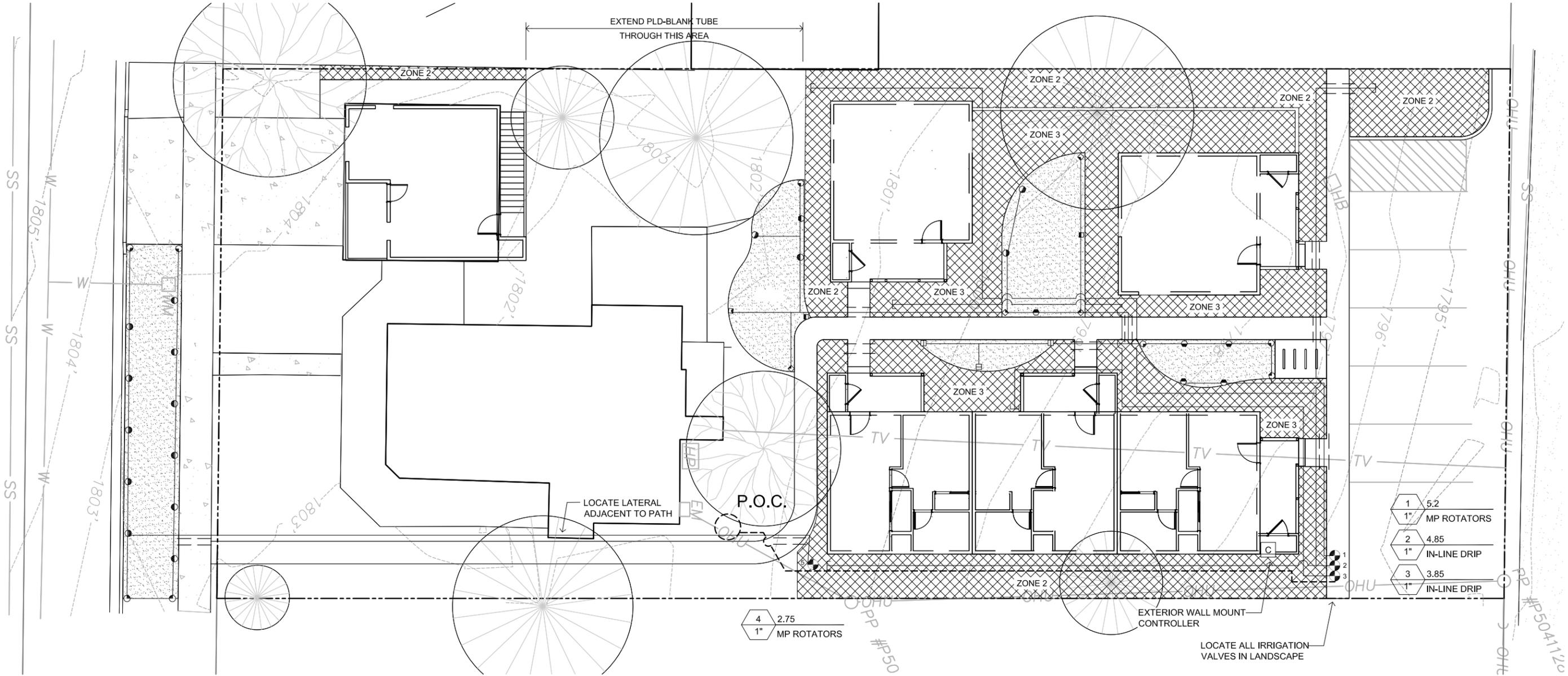
DRAWN BY: PLC

SHEET

2

OF 5 SHEETS

**IMPORTANT NOTE: SCALE AS NOTED ONLY
 VALID WHEN PRINTED AT 100%**



- 1 5.2
1" MP ROTATORS
- 2 4.85
1" IN-LINE DRIP
- 3 3.85
1" IN-LINE DRIP

- 4 2.75
1" MP ROTATORS

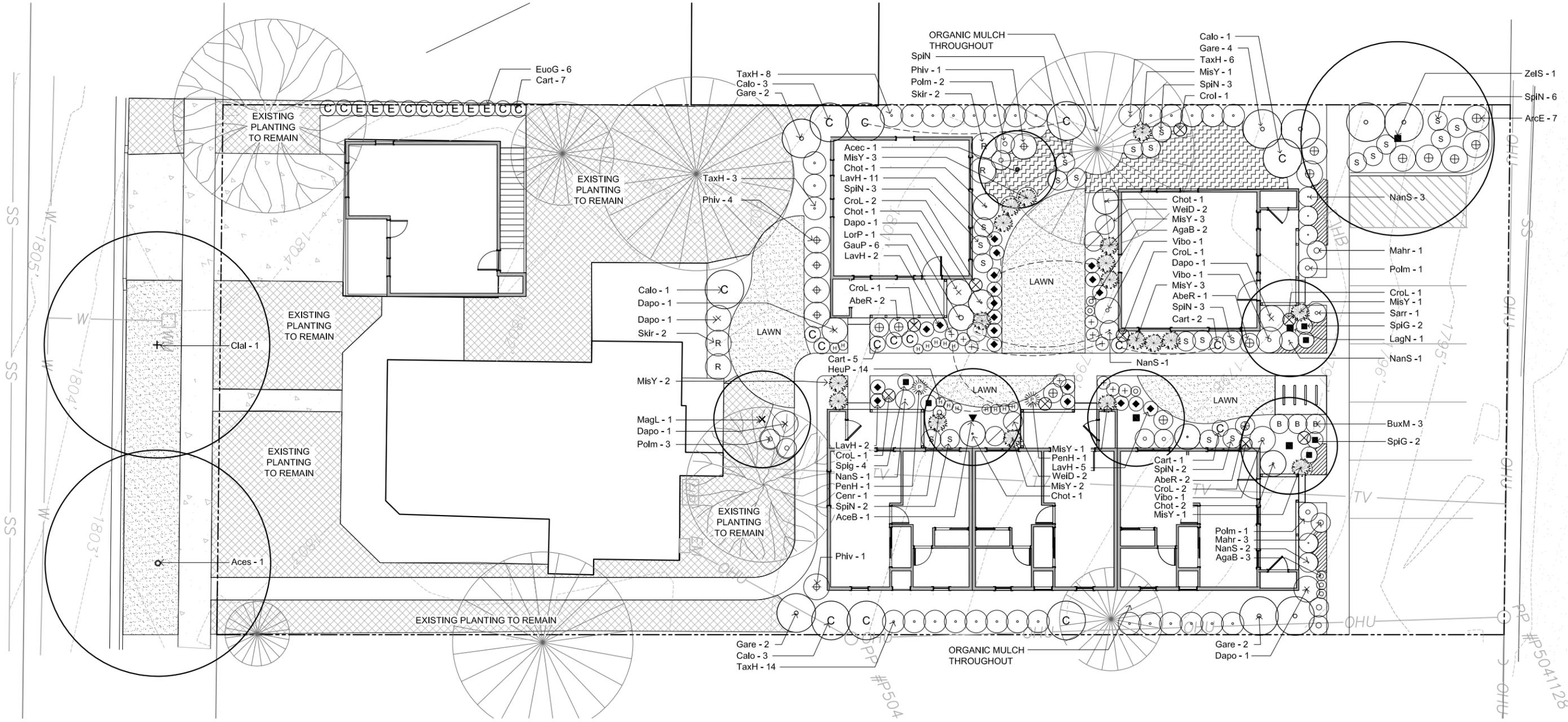
IRRIGATION LEGEND

SYM.	ITEM
	HUNTER ICV-101G (SPRAY)
	HUNTER PCZ-101 (DRIP)
	MAIN LINE: SCH. 40 PVC (1/2")
	LATERAL LINES SHALL BE SCHEDULE 40 PVC. (1")
	SLEEVES - SCH. 40, MIN. SIZE SHALL BE 2x DIA. OF PASSING PIPE.
	ISOLATION GATE VALVE - LINE SIZE
	QUICK COUPLING VALVE: HUNTER 44RC
	CONTROLLER: HUNTER IC-M SERIES WITH SOLAR-SYNC
	ZONE I.D.
	15.0 G.P.M 1.5" Shrubs APPLICATION
	VALVE SIZE
	P.O.C. POINT OF CONNECTION
	FLUSH VALVE
	DRIP IRRIGATION: HUNTER PLD-04-18

IRRIGATION HEAD LEGEND

SYMBOL	DESCRIPTION	MODEL (*) (**)	NOZZLE	RAD.	FLOW RATE (GPM)
	1/4, 1/2, FULL	Hunter PRS30	MP800-SR	6'-8'	0.16, 0.32, 0.63
	1/4, 1/2, 3/4, FULL	Hunter PRS30	MP-Rotator 1000	8'	0.11, 0.21, 0.44
	END, CENTER	Hunter PRS40	MP-Rotator Strip	Strip	0.19, 0.38



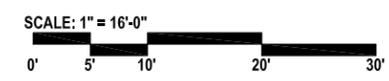


PRELIMINARY PLANT LEGEND			
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE
TREES			
Acec	<i>Acer circinatum</i>	VINE MAPLE	2" cal
AceB	<i>Acer palmatum 'Bloodgood'</i>	BLOODGOOD JAPANESE MAPLE	2" cal
Aces	<i>Acer saccharum 'Bonfire'</i>	BONFIRE SUGAR MAPLE	2" cal
Clal	<i>Cladrastus lutea</i>	KENTUCKY YELLOW WOOD	2" cal
LagN	<i>Lagerstroemia indica 'Natchez'</i>	NATCHEZ CRAPE MYRTLE	2" cal
MagL	<i>Magnolia grandiflora 'Little Gem'</i>	LITTLE GEM SOUTHERN MAGNOLIA	2" cal
ZeIW	<i>Zelkova serrata 'Schmidtlow'</i>	WIRELESS ZELKOVA	2" cal
SHRUBS			
AbeR	<i>Abelia x 'Rose Creek'</i>	ROSE CREEK ABELIA	3 gal
AgaB	<i>Agastache 'Blue Fortune'</i>	BLUE FORTUNE HYSSOP	1 gal
ArcE	<i>Arctostaphylos uva-ursi 'Emerald Carpet'</i>	EMERALD CARPET MANZANITA	1 gal
BuxM	<i>Buxus x 'Green Mountain'</i>	GREEN MOUNTAIN BOXWOOD	2 gal
Calo	<i>Calycanthus occidentalis</i>	CALIFORNIA SPICEBUSH	5 gal
Cart	<i>Carex testacea</i>	NEW ZEALAND SEDGE	1 gal
Cent	<i>Centranthus ruber</i>	RED VALERIAN	a gal
Chot	<i>Cholsya ternata</i>	MEXICAN ORANGE	5 gal
CisB	<i>Cistus x 'Brillancy'</i>	BRILLIANCY ROCK ROSE	5 gal
CroL	<i>Crocsmia 'Lucifer'</i>	LUCIFER CROCOSMIA	1 gal
Dapo	<i>Daphne odora 'Marginata'</i>	VARIEGATED WINTER DAPHNE	3 gal
DapB	<i>Daphne x burkwoodii 'Carol Mackl'</i>	CAROL MACKI DAPHNE	2 gal
EuoG	<i>Euonymus japonicus 'Green Spire'</i>	GREEN SPIRE EUONYMUS	3 gal
Gare	<i>Garrya elliptica</i>	COAST SILKTASSEL	5 gal
GauP	<i>Gaura lindheimeri 'Passionate Blush'</i>	PASSIONATE BLUSH GAURA	1 gal
Heup	<i>Heuchera x 'Peach Flambe'</i>	PEACH FLAMBE CORAL BELL	1 gal

LavH	<i>Lavendula angustifolia 'Hidcote'</i>	HIDCOTE LAVENDER	1 gal
LorP	<i>Loropetalum chinensis 'Purple Pixle'</i>	PURPLE PIXIE FRINGE FLOWER	5 gal
Mahr	<i>Mahonia repens</i>	CREeping MAHONIA	1 gal
MisY	<i>Miscanthus 'Yakushima'</i>	DWARF MAIDEN GRASS	1 gal
NanS	<i>Nandina domestica 'Sienna Sunrise'</i>	SIENNA SUNRISE HEAVENLY BAMBOO	3 gal
PenH	<i>Pennisetum alopecuroides 'Hamel'</i>	DWARF FOUNTAIN GRASS	1 gal
Phiv	<i>Philadelphus x virginialis 'Mini Snowflake'</i>	DWARF MOCK ORANGE	3 gal
Polm	<i>Polystichum munitum</i>	SWORD FERN	1 gal
Sarr	<i>Sarcococca ruscifolia</i>	FRAGRANT SWEET BOX	3 gal
Skir	<i>Skimmia reevesiana</i>	REEVES SKIMMIA	2 gal
SpiG	<i>Spiraea japonica 'Goldmound'</i>	GOLDMOUND SPIREA	2 gal
SpiN	<i>Spiraea japonica 'Neon Flash'</i>	NEON FLASH SPIREA	2 gal
TaxH	<i>Taxus x media 'Hicksii'</i>	HICKS YEw	10 gal
Vibd	<i>Viburnum davidii</i>	DAVID VIRBURNUM	3 gal
Vibo	<i>Viburnum opulus compacta</i>	COMPACT CRANBERRYBUSH	5 gal
WeID	<i>Weigela florida 'Dark Horse'</i>	DARK HORSE WEIGELA	3 gal
WeIM	<i>Weigela florida 'Minuet'</i>	MINUET WEIGELA	3 gal
GROUND COVER			
	<i>Liriope muscari</i>	Lilyturf	1 GAL @ 18" OC
	LAWN	LAWN	SOD
	<i>Waldsteinia fragaroides</i>	BARREN STRAWBERRY	1 GAL @ 3' OC
	EXISTING TO REMAIN		

PRELIMINARY PLANTING NOTES

- ALL LANDSCAPE PLANTING AREAS SHALL RECEIVE CLEAN, SANDY LOAM TOPSOIL TO A MINIMUM DEPTH OF 12" OR AS NOTED ON THE PLAN.
- ALL PROPOSED STREET AND SITE TREES WILL HAVE A MINIMUM OF (2) CUBIC FEET OF SOIL VOLUME FOR EACH SQUARE FOOT OF TREE CANOPY AT MATURITY. SOIL VOLUME WILL BE ACHIEVED BY MEANS OF TOPSOIL IN PLANTERS AND STRUCTURAL SOIL UNDER IMPERVIOUS SURFACES.
- ALL PLANTING AREAS SHALL RECEIVE 3" OF UNSETTLED ORGANIC MULCH.
- THE IRRIGATION SYSTEM WILL PROVIDE A FULLY AUTOMATIC IRRIGATION CONTROLLER AND BACKFLOW PREVENTION DEVICE THAT WILL MEET THE CITY OF ASHLAND REQUIREMENTS.
- THE PROPOSED IRRIGATION SYSTEM WILL CONSIST OF LOW VOLUME DISTRIBUTION.





Drawn By:
SGB

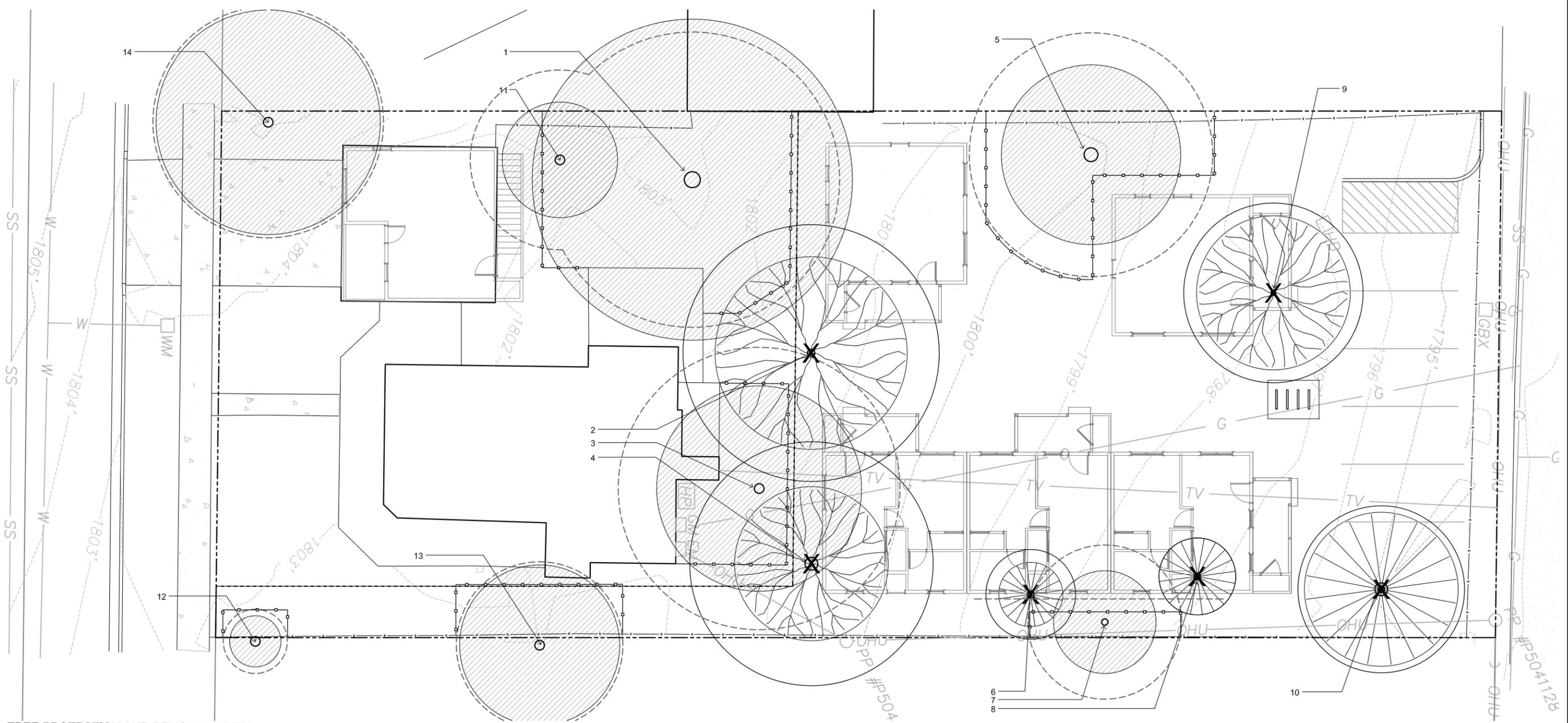
BALDWIN RESIDENCE
128 CENTRAL AVENUE
ASHLAND, OR, 97520

JOB NO. 1710
REVISION DATE
REV. - 1 : 09.26.17

TREE PROTECTION PLAN

ISSUE DATE:
JULY 18, 2017

L 1.0



TREE PROTECTION AND REMOVAL NOTES

- PRIOR TO DELIVERING EXCAVATION EQUIPMENT OR COMMENCING ANY CONSTRUCTION ACTIVITIES ON THE SITE, THE GENERAL CONTRACTOR SHALL CONTACT THE LANDSCAPE ARCHITECT FOR A PRE-CONSTRUCTION MEETING WITH THE LANDSCAPE ARCHITECT AND EXCAVATION SUPERVISOR PRIOR TO COMMENCING ANY WORK ON THE SITE. THE LANDSCAPE ARCHITECT SHALL BE NOTIFIED BY THE CONTRACTOR 48 HRS. IN ADVANCE FOR ALL SITE VISITS REQUESTED. CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL FROM THE OWNER'S REPRESENTATIVE THAT CONSTRUCTION MAY BEGIN AFTER ALL OF THE DESCRIBED FENCING IS IN PLACE. FENCING SHALL REMAIN IN PLACE UNTIL THE PROJECT IS COMPLETED.
- FENCES MUST BE ERECTED TO PROTECT TREES TO BE PRESERVED AS SHOWN IN DIAGRAM. FENCING SHALL BE 6' TALL TEMPORARY CHAIN LINK PANELS INSTALLED WITH METAL CONNECTIONS TO ALL PANELS AREA INTEGRATED. THESE FENCES SHALL BE INSTALLED SO THAT IT DOES NOT ALLOW PASSAGE OF PEDESTRIANS AND/OR VEHICLES THROUGH IT. FENCES DEFINE A SPECIFIC PROTECTION ZONE FOR EACH TREE OR GROUP OF TREES. FENCES ARE TO REMAIN UNTIL ALL SITE WORK HAS BEEN COMPLETED. FENCES MAY NOT BE RELOCATED OR REMOVED WITHOUT THE PERMISSION OF THE LANDSCAPE ARCHITECT.
- CONSTRUCTION TRAILERS, TRAFFIC AND STORAGE AREAS MUST REMAIN OUTSIDE FENCED TREE PROTECTION ZONES AT ALL TIMES.
- ALL PROPOSED UNDERGROUND UTILITIES AND DRAIN OR IRRIGATION LINES SHALL BE ROUTED OUTSIDE THE TREE PROTECTION ZONE. IF LINES MUST TRANSVERSE THE PROTECTION AREA, THEY SHALL BE TUNNELED OR BORED UNDER THE TREE ROOTS. NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY IF ANY PROJECT PLANS CONFLICT WITH THIS REQUIREMENT.
- NO MATERIALS, EQUIPMENT, SPOIL, OR WASTE OR WASHOUT WATER MAY BE DEPOSITED, STORED, OR PARKED WITHIN THE TREE PROTECTION ZONE (FENCED AREA).
- NOTIFY THE LANDSCAPE ARCHITECT IF TREE PRUNING IS REQUIRED CONSTRUCTION CLEARANCE.
- ANY HERBICIDES PLACED UNDER PAVING MATERIALS MUST BE SAFE FOR USE AROUND TREES AND LABELED FOR THAT USE.
- IF INJURY SHOULD OCCUR TO ANY TREE DURING CONSTRUCTION, NOTIFY THE LANDSCAPE ARCHITECT IMMEDIATELY. ALL DAMAGE CAUSED BY CONSTRUCTION TO EXISTING TREES SHALL BE COMPENSATED FOR BY THE OFFENDING PARTY, BEFORE THE PROJECT WILL BE CONSIDERED COMPLETE.
- WATERING SCHEDULE: WATERING PROTECTED TREES SHALL FOLLOW THESE STANDARDS, HOWEVER PERIODS OF EXTREME HEAT, WIND, RAINFALL OR DROUGHT MAY REQUIRE MORE OR LESS WATER THAN RECOMMENDED IN THESE NOTES.
 - MOST SPECIES: 1 TIME PER MONTH DURING IRRIGATION SEASON (USUALLY MARCH THROUGH SEPTEMBER)
 - QUERCUS/OAK: DEEP WATER IN MAY AND SEPTEMBER. DO NOT WATER DURING OTHER MONTHS. FOR OAKS ALREADY IN THE VICINITY OF IRRIGATED CONDITIONS, AUTOMATIC SPRINKLERS OR REGULAR WATERING SHALL NOT BE ALLOWED TO SPRAY ON OR WITHIN 3 FEET OF THE TRUNK. THE WATER SHALL NOT BE ALLOWED TO POOL OR DRAIN TOWARDS THE TRUNK.
 - WATERING METHOD: HAND WATERING SYSTEMS, RECOMMENDED FOR TREES THAT ARE PART OF A DEVELOPMENT PROJECT THAT MUST BE WATERED TO INSURE TREE SURVIVAL DURING THE COURSE OF CONSTRUCTION UNTIL AUTOMATIC IRRIGATION IS INSTALLED.
- EROSION CONTROL DEVICES SUCH AS SILT FENCING, DEBRIS BASINS, AND WATER DIVERSION STRUCTURES SHALL BE INSTALLED ON THE UPHILL SIDE OF THE TREE PROTECTION ZONE TO PREVENT SILTATION AND/OR EROSION WITHIN THE TREE PROTECTION ZONE.
- BEFORE GRADING, PAD PREPARATION, OR EXCAVATION FOR THE FOUNDATIONS, FOOTINGS, WALLS, OR TRENCHING, ANY TREES WITHIN THE SPECIFIC CONSTRUCTION ZONE SHALL BE ROOT PRUNED 1 FOOT OUTSIDE THE TREE PROTECTION ZONE BY CUTTING ALL ROOTS CLEANLY AT A 90 DEGREE ANGLE TO A DEPTH OF 24 INCHES. ROOTS SHALL BE CUT BY MANUALLY DIGGING A TRENCH AND CUTTING EXPOSED ROOTS WITH A SAW, VIBRATING KNIFE, ROCK SAW, NARROW TRENCHER WITH SHARP BLADES, OR OTHER APPROVED ROOT-PRUNING EQUIPMENT.
- ANY ROOTS DAMAGED DURING GRADING OR CONSTRUCTION SHALL BE EXPOSED TO SOUND TISSUE AND CUT CLEANLY AT A 90 DEGREE ANGLE TO THE ROOT WITH A SAW. PLACE DAMP SOIL AROUND ALL CUT ROOTS TO A DEPTH EQUALING THE EXISTING FINISH GRADE WITHIN 4 HOURS OF CUTS BEING MADE.
- IF TEMPORARY HAUL OR ACCESS ROADS MUST PASS OVER THE ROOT AREA OF TREES TO BE RETAINED, A ROAD BED OF 6 INCHES OF MULCH OR GRAVEL SHALL BE CREATED TO PROTECT THE SOIL. THE ROAD BED MATERIAL SHALL BE REPLENISHED AS NECESSARY TO MAINTAIN A 6 INCH DEPTH.
- SPOIL FROM TRENCHES, BASEMENTS, OR OTHER EXCAVATIONS SHALL NOT BE PLACED WITHIN THE TREE PROTECTION ZONE, EITHER TEMPORARILY OR PERMANENTLY.
- NO BURN PILES OR DEBRIS PILES SHALL BE PLACED WITHIN THE TREE PROTECTION ZONE. NO ASHES, DEBRIS, OR GARBAGE MAY BE DUMPED OR BURIED WITHIN THE TREE PROTECTION ZONE.
- MAINTAIN FIRE-SAFE AREAS AROUND FENCED AREA. ALSO, NO HEAT SOURCES, FLAMES, IGNITION SOURCES, OR SMOKING IS ALLOWED NEAR MULCH OR TREES.
- DO NOT RAISE THE SOIL LEVEL WITHIN THE DRIP LINES TO ACHIEVE POSITIVE DRAINAGE. EXCEPT TO MATCH GRADES WITH SIDEWALKS AND CURBS, AND IN THOSE AREAS, FEATHER THE ADDED TOPSOIL BACK TO EXISTING GRADE AT APPROXIMATELY 3:1 SLOPE.
- REMOVE THE ROOT WAD FOR EACH TREE THAT IS INDICATED ON THE PLAN AS BEING REMOVED.
- EXCEPTIONS TO THE TREE PROTECTION SPECIFICATIONS MAY ONLY BE GRANTED IN EXTRAORDINARY CIRCUMSTANCES WITH WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO ANY WORK COMMENCING.
- AS A PROTECTIVE MEASURE TO COMPENSATE FOR CONSTRUCTION IMPACTS, TWO TO SIX WEEKS PRIOR TO CONSTRUCTION, ALL RETAINED TREES SHOWN ON THIS PLAN SHALL RECEIVE AN APPLICATION OF MYCORRHIZAL PRODUCT PER MANUFACTURER'S INSTRUCTIONS. THIS MYCORRHIZAL PRODUCT IS A SPECIALLY FORMULATED NATURAL ROOT BIOSTIMULANT WHICH ENHANCES THE ABSORPTIVE SURFACE AREA OF THE TREES' ROOT SYSTEMS. THIS PROMOTES AND IMPROVES NUTRIENT AND WATER UPTAKE CAPABILITIES OF THE REMAINING ROOT STRUCTURE. DISTRIBUTE MYCORRHIZAL EVENLY WITHIN THE ACTIVE ROOT ZONE OF RETAINED TREES. APPLY 30 GALS. OF SOLUTION PER TREE 6" DBH AND GREATER, A MINIMUM OF 4" BELOW SOIL SURFACE IN QUANTITIES OF 1/2 GALLON AT EACH POINT OF APPLICATION. LOCATE THE ACTIVE ROOT ZONES WITH LANDSCAPE ARCHITECT PRESENT. MYCORRHIZAL IS AVAILABLE FROM MYCORRHIZAL APPLICATION, INC., PHONE (641) 476-3986.

TREE LEGEND

#	SPECIES	DBH (INCHES)	HEIGHT IN FEET	CROWN FEET IN RADIUS	TREE PROTECTION ZONE RADIUS IN FEET	TOLERANCE TO CONSTRUCTION	CONDITION	NOTES
1	Cedrus deodara	30	70	25	23	GOOD	Excellent	PROTECT
2	Malus spp.	12,14	30	20	10	GOOD	Excellent	REMOVE
3	Juglans spp.	18	30	16	22	POOR	Good	PROTECT
4	Malus spp.	24	30	19	18	GOOD	Good	REMOVE
5	Pinus ponderosa	26	70	14	19	GOOD	Excellent	PROTECT
6	Calocedrus decurens	12	35	7	12	MODERATE	Good	REMOVE
7	Calocedrus decurens	12	35	8	12	MODERATE	Good	PROTECT
8	Calocedrus decurens	12	35	6	12	MODERATE	Good	REMOVE
9	Betula spp.	6, 9	35	14	9	MODERATE	Excellent	REMOVE
10	Pinus ponderosa	24	70	13	18	GOOD	Fair	REMOVE
11	Calocedrus decurens	14	40	18	14	MODERATE	Good	PROTECT
12	Calocedrus decurens	4, 5	18	8	5	MODERATE	Good	PROTECT
13	Chamaecyparis law.	17	65	24	13	GOOD	Good	PROTECT
14	Ailanthus altissima	24	40	35	18	GOOD	Decline	PROTECT



THE TREE PROTECTION ZONE FOR EACH TREE IS BASED ON THE GUIDELINES ESTABLISHED BY: Matheny, N. & Clark, J. 1998. *Trees and Development: A Technical Guide to Preservation of Trees During Land Development.* p. 72.





REVISED NOTICE OF APPLICATION

PLANNING ACTION: PA- 2017-01310

SUBJECT PROPERTY: 981 Siskiyou Blvd.

OWNER/APPLICANT: MH Smith Inc./MNM Fund LLC

DESCRIPTION: A request for Site Design Review and Tree Removal Permit approval for a three-unit multi-family residential development for the property located at 981 Siskiyou Blvd. The proposal includes retaining the existing residence and constructing two units at the rear of the property. The Tree Removal Permit request is to remove two trees to the west of the existing home including a maple and an ash tree. **COMPREHENSIVE PLAN**

DESIGNATION: Low-Density Multi-Family Residential; **ZONING:** R-2; **ASSESSOR'S MAP #:** 391E 09DA; **TAX LOT:** 8100

The Ashland Historic Commission will also review this Planning Action on **Wednesday October 4, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way. The Ashland Tree Commission will also review this Planning Action on **Thursday, October 5, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

NOTICE OF COMPLETE APPLICATION: September 25, 2017

DEADLINE FOR SUBMISSION OF WRITTEN COMMENTS: October 9, 2017



The Ashland Planning Division Staff has received a complete application for the property noted above.

Any affected property owner or resident has a right to submit written comments to the City of Ashland Planning Division, 51 Winburn Way, Ashland, Oregon 97520 prior to 4:30 p.m. on the deadline date shown above.

Ashland Planning Division Staff determine if a Land Use application is complete within 30 days of submittal. Upon determination of completeness, a notice is sent to surrounding properties within 200 feet of the property submitting application which allows for a 14 day comment period. After the comment period and not more than 45 days from the application being deemed complete, the Planning Division Staff shall make a final decision on the application. A notice of decision is mailed to the same properties within 5 days of decision. An appeal to the Planning Commission of the Planning Division Staff's decision must be made in writing to the Ashland Planning Division within 12 days from the date of the mailing of final decision. (AMC 18.5.1.050.G)

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Department to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Division, Community Development & Engineering Services Building, 51 Winburn Way, Ashland, Oregon 97520.

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division at 541-488-5305.

SITE DESIGN AND USE STANDARDS

18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. *Exception to the Site Development and Design Standards.* The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

TREE REMOVAL PERMIT

18.5.7.040.B

1. *Hazard Tree.* A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
 - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. *Tree That is Not a Hazard.* A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
 - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
 - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
 - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
 - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
 - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

September 19, 2017

**Site Design Review Application
For
Multi-Family Site Review
AMENDED FINDINGS**

Modified Request:

A request for a Site Design Review Permit for three unit, multiple family residential development. The existing residence will remain with the proposed addition of two units to the rear.

Following the September 6, 2017 Historic Commission and the September 7, 2017 Tree Commission modifications were made to the previous application.

These include the redesign of the proposed new duplex unit to be reduced in size to 1,296 SF, this is within the allowed maximum permitted floor area for three units; and preservation of the Italian Cypress.

To clarify the use of the basement of the existing residence. The basement had previously been used as a separate residence. Through the renovations of the existing residence and the installation of a central HVAC system, new water heater, etc., the basement has been converted to a utility / storage area and cannot be rented out as a separate unit as stated by the Historic Commission at the September 6, 2017 meeting.

The non-conformity of the existing driveway width (restricted to less than 15-feet due to presence of existing building and Italian Cypress), and the non-conforming separation between the existing driveway approach and the intersection of Siskiyou Boulevard and Mountain Avenue will not be altered.

The proposed development complies with the maximum permitted floor area.

Maximum Permitted Floor Area:

Lot Area:	9,720 SF
Adjusted Lot Area:	7,095.6 SF
Allowed MPFA for three units:	2,980.15 SF
Proposed new units:	1,227 SF
Existing Residence:	1,253 SF (600 SF basement/utility room not included)
Total Proposed MPFA:	2,480 SF

There are 170 square foot and 175 square foot private patio areas of along the east property side of the structure. These areas are approximately 8-feet by approximately 20-feet, see site plan for exact dimensions. The patio areas will have 6-foot tall screening fencing around the boundaries of the patio areas. The space provided is necessitated by the lot layout. With the long and narrow lot, solar access

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standards, separation between buildings, number of required parking spaces, etc., the side-yard area is the most conducive to having private patio area for the tenants that is not adjacent to the parking or is not directly adjacent to each unit.

The front house has a 125-square foot dining porch, and substantial front yard area. At the 20-foot front yard setback in the front yard to the east side yard, screening fencing will be installed to provide a more private area in the open space.

The existing dwelling and the new units require five parking spaces. Five parking spaces are proposed at the rear of the property accessed via the existing driveway along the west property line. The driveway is proposed to be surfaced at 10 - 11-feet wide with gravel under the “canopy” of the Italian Cypress. The existing driveway clearance is 14-feet, 11-inches with 11-feet of paved width to the rear of the existing residence. This is a non-conforming situation with the existing structure, Italian Cypress and the limited driveway width. The driveway along the frontage of the new units is proposed to be 12-feet of pavement with 15-feet of clearance. Findings addressing the non-conformity criteria for the width of the driveway are attached.

The driveway accesses the five-vehicle parking area. The existing driveway is less than the required 15-feet in width for driveways considered flag-drives. Past the existing residence, the driveway will be 15-feet clear width with 12-feet of paved width. The parking area has 22-feet of back-up area to accommodate vehicle turn around. The required five-foot wide landscape buffer will be provided around the parking lot. A shade tree will be provided for in the parking lot landscape buffer.

The proposal requires four bicycle parking spaces. The bicycle parking will be provided within the fenced private yard in the front lawn area to the east of the porch for the front unit and in a bicycle parking structure for the new units at the rear of the property.

The primary target tenant will be Southern Oregon University students where there is a high probability of one of the possible five tenants would not have an automobile. Also, in the event that the tenant does have an automobile, the location of the dwellings, their proximity to SOU, the commercial areas along Siskiyou Boulevard, and direct access to three RVTB bus stops that are within 800-feet of the property, allows for reliance on multi-modal transportation and not only automobile use.

The existing residence is oriented towards Siskiyou Boulevard. The new units will have small entry stoops and will be primarily oriented towards the driveway. Each unit will have access to the east yard area where a patio and lawn area will be provided.

On the following pages, the criteria from the Ashland Land Use Ordinance as it pertains to Site Design Review, non-conforming situations, and tree removal criteria have been addressed. The City of Ashland criteria are in Times New Roman font and the applicant’s findings are in Calibri font.

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Non-conforming Situations:

18.1.4.040 Nonconforming Developments

A. *Exempt Alterations.* Repair and maintenance of a nonconforming development (e.g., paved area, parking area, landscaping) are allowed subject to approval of required building permits if the development is not enlarged or altered in a way that brings the nonconforming site less in conformity with this ordinance.

The proposed driveway width exists. The existing residence on the site restricts the driveway width to less than 15-feet of clear width and less than 12-feet paved width as required for driveways greater than 50-feet in length. Additionally, the existing Italian Cypress that had been requested for removal but was recommended to be retained by the Tree Commission at their September 7, 2017 regular meeting limits the property owner's ability to widen the existing driveway more than the existing 9-feet at the tree to 11 feet of 'surfaced width' to the south of the tree. Once past (north of) the existing residence, the driveway does have the required width of 15-feet clear width and 12-feet of paved width which brings the nonconforming site closer to conformance.

When the Cypress tree dies and is removed, the driveway could be widened to 11-feet of paved width.

**Site Development Design Standards Approval Criteria:
Ashland Municipal Code 18.5.2.050**

A. **Underlying Zone.** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

The subject property is zoned R-2, Low Density Multiple Family Residential. The parcel is 9,720 square feet (.223 ac) and meets minimum lot area and minimum lot dimensions in the R-2 zone. The existing setbacks are slightly non-conforming to the front property line, (19-feet, 6 1/2 - inches to front porch of existing residence) no modifications are proposed.

The proposal is to have a total of three residential dwelling units. The first unit is the existing front residence. At the rear of the property, to the north of the existing residence, accessible from the shared driveway are Units #2 and #3 within a 1,227-square foot duplex structure.

The proposed duplex is setback 8 ½ feet from the east side, and setback 17-feet 6 7/16 - inches from the west side to accommodate for the driveway and a walkway to the units. The new duplex is separated from the front residence by 9 ½ -feet. The new residence is 18-feet, 3-inches tall at the wall adjacent to the existing residence, the 9 ½ foot separation is the minimum allowed by the code.

The proposed structure has traditional styling that reflects the character of the historic contributing structure. Similar siding, reveal, and low pitch roof are like the front residence. There are varying roof

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forms to break up the mass of the structures. There are numerous windows to allow for ample natural light into the units.

The solar setback standards are met with the development because the structures are more than 45-feet from the rear property line.

Allowed Density 18.2.5.080: .223 X 13.5 = 3
Proposed Density: 3

The required open space area is 777.6 square feet. The proposed open spaces consist of approximately 345 square foot of private patio areas for the duplex units and, 210.5 square feet of porch and deck area (side porch, dining porch and rear deck). There is also yard area to the side of and behind the required front yard area that will be screened as private yard area. This accounts for an additional 500 square feet of useable, recreation space for the tenants of the residences.

Lot Coverage: Proposed impervious areas including existing building footprints, proposed building footprints, pathways, driveways, patios, deck (excluding 200 SF) is 5,666 SF of the 9,720 SF lot for a total lot coverage of 58.29 percent, this is less than the maximum of 65 percent in the zone.

Parking: The proposed development requires five (5) vehicle parking spaces.

One bedrooms >500 sf = 1.5 X 2 = 3
Three bedroom unit = 2
Total = 5

At the rear of the property, five head-in parking spaces are proposed. There is 22-feet of backup area behind the parking space. The parking area will be solid surface, there are five-foot landscape buffers proposed on the three sides of the parking area.

Three bicycle parking spaces are required. Bicycle parking spaces for the front residence are to the east of the structure, small covered areas are proposed under the windows in the fenced side yard. At the rear of the new duplex unit seven bicycle parking spaces are proposed. This accommodates the two required bicycle parking spaces and an additional five bicycle parking spaces.

Energy Usage: The units within the proposed development will be constructed to the current energy codes and building code standards. A detailed analysis of the actual energy consumption has not been determined but the square footage, use of ductless heat pump systems, LED lighting all contribute to the low energy consumption anticipated in the duplex units.

B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3).

18.4.2.050 Historic District Development
B. Historic District Design Standards.

Roughly 2/3 of the property is in the local historic district, the rear portion of the property is outside of the historic district. Since the property is in the local Siskiyou-Hargadine District but outside of the National Historic District boundaries there is little historic documentation information available on the property.

The property is occupied by a historic, single-story residence with walkout basement. The 1940s craftsman bungalow has a mixture of vinyl, and metal siding materials, sliding windows, mixture of door types, a metal roof, that all greatly alter the integrity of the structure. Generally, the form remains with the front stoop with large columns, the dining porch to the side and the roof pitches and angle which relate the structure to its original construction. It has been utilized as a rental for more than 25-years and has serious neglect. The structure is presently under renovation per a 2017 building permit.

The property to the north is outside of the historic district. The property to the west of the site is occupied by a bungalow style, 1925, duplexed residence and detached accessory structures. This property is zoned commercial. The properties to the east that are in the historic district are occupied by a converted single-family residence with no historic 'presence'. This property is zoned R-2, same as subject property but has been operated under a conditional use permit for a medical office use for more than 20 years. The property further to the east is a 1970s medical office building that has been used as medical, dental and other personal service type of establishments. The properties to the south are the campus of Southern Oregon University where the historic Swedenburg House is located.

The proposed, new dwellings will have limited visibility from any public rights-of-way. The structures are proposed to have traditional styling that reflects the character of the historic structure. Similar siding, reveal, to the front residence is proposed. The roof form, shape, pitch and materials are similar to the existing residence. There are numerous windows to allow for ample natural light into the units. An 8 – 12-inch wide belly band, 4-inch corner boards, 5-inch reveal siding, 1X4 trim provide architectural details often found on historic structures. It should be noted that the duplex is not highly visible from the public right-of-way and architectural compatibility should be somewhat flexible due to lack of view from Siskiyou Boulevard and lack of historical compatibility due to the existing and potential developments on the adjacent parcels.

Height: The proposed structures are similar to average heights of structures in the vicinity. There are a variety to structure in the vicinity. Though most are single story, due to the topography, the structure is similar in height to a story and half. The proposed structure with an average height of 19-feet, 4-inches is just slightly taller than the existing residence on the site at the rear of the property where the structure is nearly two-stories with a walkout basement.

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The proposed structure is more than 60-feet from the street and behind the residence. The property falls away from Siskiyou and the proposed structure is at a lower elevation than the sidewalk along the frontage of the property.

Scale: The scale of the proposed duplex is within the range of other dwellings in the vicinity. The limited square footage of the duplex at 1,227 square feet is in line with the allowed square footages on the other multi-family zoned properties in the vicinity as well.

Massing: Through the incorporation of more than one roof form and various gables, the mass of the new structure has been reduced. The structure is substantially away from and lower than Siskiyou Boulevard, the mass will be limited to the property and the large separation from the public right-of-way limits the visual impact.

Setback: The proposed residence is behind the historic residence.

Roof: The proposed pitch is a mixture of 7:12 and 3:12 dormers and hipped roofs. The pitch is similar to the various roof pitches in the vicinity. The roof materials are proposed metal to match. The existing residence has a metal roof, the properties to the northeast also have metal roofs.

Rhythm of Openings: The existing structures window and door placements are not proposed to be altered. There are a mixture of fixed pane and vinyl sliders. The proposed windows in the duplex are proposed as single hung on the ground floor and due to egress requirements, casement windows are necessary. The rhythm of openings is traditional and dictated by the floor plan of the residence. The proposal is consistent with rhythm of openings found on residential structures in the vicinity.

Base or Platform: The concrete foundation stem wall will be exposed for 12" to 36". This provides a sense of a base.

Form: The form of the duplex is consistent with multi-family dwelling development and is sensitive to the Siskiyou-Hargadine Historic District.

Entrances: The primary residence has an entrance that generally faces the public street. Due to the substantial setback of the duplex units from the public streets, there is not a requirement for a visible entrance. There are prominent entrances provided on the units. The west side of the units face the driveway and a covered entry provides emphasis to the entrances.

Imitation: The proposed duplex unit has elements of the existing historic structure on the property and reflects elements of the craftsman styling. The siding exposure and reveal are similar to the existing structure. The color will match or be complementary. Though there are a variety of housing and building styles in the vicinity, craftsman and American bungalow cottage style

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construction is found in the immediate vicinity. The proposed units have elements of those design styles as well.

Garage Placement: *There is no garage.*

C. Site Development and Design Standards.

The proposed site development complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.

The proposed layout does not provide for any hidden areas that are not survey able by tenants of the site. The use of low level lighting, low growing vegetation and open space orientation, the design provides areas of safety for the tenants.

Building Orientation.

Building Orientation to Street. Dwelling units shall have their primary orientation toward a street. Where residential buildings are located within 20 feet of a street, they shall have a primary entrance opening toward the street and connected to the right-of-way via an approved walkway.

The front residence has its primary orientation towards Siskiyou Boulevard. A covered front stoop clearly indicates the front of the residence and where the front door is located. The duplex will not be very visible from the public street and they are not oriented towards Siskiyou Boulevard.

Limitation on Parking between Primary Entrance and Street. Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or on one or both sides.

No parking is proposed between the building and the street. All parking is located at the rear of the property.

Build-to Line. Where a new building is proposed in a zone that requires a build-to line or maximum front setback yard, except as otherwise required for clear vision at intersections, the building shall comply with the build-to line standard.

All setbacks are existing for the primary residence.

Garages. Alleys and Shared Drives. Where a lot abuts a rear or side alley, or a shared driveway, including flag drives, the garage or carport opening(s) for that dwelling shall orient to the alley or shared drive, as applicable, and not a street.

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No garage is present. The property does not abut any shared driveways. The property does abut a private parking lot, that property owner was unwilling to provide access to the proposed parking area at the rear of the subject property.

Building Materials. Building materials and paint colors should be compatible with the surrounding area. Very bright primary or neon-type paint colors, which attract attention to the building or use, are unacceptable.

The building materials are compatible with the surrounding area. The materials are mixture of modern with classic elements. The duplex is proposed to have concrete stem wall that forms the base, horizontal siding with five-inch exposure. Similar style windows and the roofing is proposed as metal roofing to match existing.

Streetscape. One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

A new street tree selected from the Recommended Street Tree Guide and planted in accordance with the street tree standards from 18.4.4.030.E. will be planted in the parkrow.

Landscaping and Recycle/Refuse Disposal Areas. Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

The refuse area will be provided at the rear of the duplex structure.

A five foot landscape buffer is provided adjacent to the property lines and around the parking area. A parking lot shade tree has been provided.

The proposed landscaping complies with AMC 18.4.4.

18.4.3.080 Vehicle Area Design

A. Parking Location

The proposed parking is not located between the building and the street. The parking is located at the rear of the property.

B. Parking Area Design. Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in 18.4.3.080.B. See also, accessible parking space requirements in section [18.4.3.050](#) and parking lot and screening standards in subsection [18.4.4.030.F](#).

1. Parking spaces shall be a minimum of 9 feet by 18 feet.

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Three of the five proposed parking spaces are 9 feet by 18 feet.

2. Up to 50 percent of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 feet by 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."

Two of the five proposed parking spaces are 8 feet by 16 feet. They will be painted "compact car only".

3. Parking spaces shall have a back-up maneuvering space not less than 22 feet, except where parking is angled, and which does not necessitate moving of other vehicles.

All proposed parking has a back-up space of 22-feet.

Shared Use of Driveways and Curb Cuts.

The driveway onto Siskiyou Boulevard is a non-conforming situation due to the proximity to the intersection and the proximity to the property on the adjacent lot to the east. The non-conformity is not altered with the proposal.

There is a parking lot to the east that is accessed via Doctor's Alley. That property owner was approached regarding a vehicular easement to utilize their parking lot for egress from the subject property. They declined. Therefore, the driveway is the only way to access the parking area at the rear of the property.

b. Developments subject to a planning action shall remove all curb cuts and driveway approaches not shown to be necessary for existing improvements or the proposed development. Curb cuts and approaches shall be replaced with standard curb, gutter, sidewalk, and planter/furnishings strip as appropriate.

The Siskiyou Boulevard curb cut is necessary for access to existing and proposed site improvements and will be retained.

c. If the site is served by a shared access or alley, access for motor vehicles must be from the shared access or alley and not from the street frontage.

There are no shared accesses or alleys for the site. The adjacent property owners have been approached to obtain access through their properties. They declined and so all access is restricted to the existing driveway.

5. Alley Access. Where a property has alley access, vehicle access shall be taken from the alley and driveway approaches and curb cuts onto adjacent streets are not permitted.

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No new curb cuts are proposed. The existing access from Siskiyou Boulevard is pre-existing non-conforming.

E. Parking and Access Construction. The development and maintenance as provided below, shall apply in all cases, except single-family dwellings.

1. Paving. All required parking areas, aisles, turn-arounds, and driveways shall be paved with concrete, asphaltic, porous solid surface, or comparable surfacing, constructed to standards on file in the office of the City Engineer.

The parking areas are proposed to be paved with concrete, asphaltic or porous solid surface depending on the storm water detention design.

2. Drainage. All required parking areas, aisles, and turn-arounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.

Drainage for the parking area will be provided to prevent sheet flow or drainage of waters into the public rights-of-way or onto abutting private property.

3. Driveway Approaches. Approaches shall be paved with concrete surfacing constructed to standards on file in the office of the City Engineer.

Driveway approach is pre-existing and is concrete.

4. Marking. Parking lots of more than seven spaces shall have all spaces permanently and clearly marked.

The parking spaces will be marked with paint or wheel stops.

5. Wheel stops. Wheel stops shall be a minimum of four inches in height and width and six feet in length. They shall be firmly attached to the ground and so constructed as to withstand normal wear. Wheel stops shall be provided where appropriate for all spaces abutting property lines, buildings, landscaping, and no vehicle shall overhang a public right-of-way.

Wheel stops, if provided will comply with this section.

6. Walls and Hedges

a. Where a parking facility is adjacent to a street, a decorative masonry wall or evergreen hedge screen between 30 and 42 inches in height and a minimum of 12 inches in width shall be established parallel to and not nearer than two feet from the right-of-way line.

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The parking is not adjacent to a street.

A sight-obscuring fence, wall, or evergreen hedge will be provided to screen the parking spaces.

Five-foot landscape buffers are proposed around the perimeter of the parking area.

18.4.3.090 Pedestrian Access and Circulation

A continuous walkway system is unable to extend through the site due to the layout of the existing residence, the private outdoor space for the residence, the proposed layout and the location of the existing driveway. An Exception to the Design Standards has been requested.

18.4.4.030 Landscaping and Screening

The proposed landscaping plan, and the irrigation plan that will be submitted with the building permits will comply with the Irrigation and Water Conserving Landscaping requirements of the City of Ashland. The conceptual landscaping plan submitted with the application has been designed so that plant coverage of 50 percent after one year, and 90 percent within five years of planting is met. A hedge is not proposed due to the presence of a solid panel fence adjacent to a parking space along the property line to the west. This fence will be repaired and replaced as necessary. Two-inches of mulch will be provided in all non-turf areas after planting.

A street tree with two-inch caliper at the time of planting will be planted in the parkrow.

Each unit will have a shrouded yard light that provides down-lighting and security for the unit but will not directly illuminate adjacent properties. There is a six-foot fence as a buffer. No plant materials are proposed that prevent surveillance of the open space or the porches.

Tree Preservation, Protection, and Removal

18.4.5.030 Tree Protection: *The trees along the west, north and east property lines on the adjacent neighbor's property are protected by a six-foot tall fence.*

The Italian Cypress will be pruned to increase clearance. Preservation fencing can be placed directly adjacent to the tree but will not extend past the tree "canopy" area due to the present constraints on the driveway access from the tree.

18.5.7 Tree Removal:

B. Tree Removal Permit.

a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.3.10.

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There is an 11-inch Maple and a 7-inch Maple proposed for removal.

The removal of the trees is to allow for the development of the site with the allowed density, compatible with historic district design standards, in an economical footprint. The development of the site also requires substantial modifications and the driveway needs to be extended to accommodate the access and parking.

b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.

The removal of the trees will not have impacts on erosion, soil stability, flow of surface waters, and protection of adjacent trees or existing windbreaks. The structures, landscaping and other site improvements are necessary on the site that will cover the exposed soil. There are no surface waters.

c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.

There are a significant number of deciduous trees within 200-feet of the property. The removal of the trees will not have a negative impact on the densities, sizes, canopies or species diversity. The trees are adjacent to the driveway, within the widened area and directly adjacent the existing structure.

d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.

The proposal complies with residential density allowed for the property of three units. Any development on the site will substantially alter the soils porosity, permeable surfaces and in turn have a negative impact on the trees.

e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

Two deciduous trees are proposed in the landscaping. The trees shall be planted and maintained per the specifications of the Recommended Street Tree Guide.

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D. City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

Adequate city facilities exist to service the new units.

***Water:** There is an existing eight-inch water main in Siskiyou Boulevard that serves the property. The existing ¾ inch meter will be continued to be used for the residence at the front of the parcel. New services for the new units are proposed within the driveway.*

***Sanitary Sewer:** There is an existing line from the property to Mountain Avenue. The property owner to the west has given permission to retain the existing but no new impacts to cross their lot with the new sanitary sewer line. The new development is proposed will pump up to Siskiyou Boulevard unless alternative arrangements are secured through easements across adjacent properties.*

***Electrical:** There is overhead power on Siskiyou Boulevard. The duplex will have a dual-pack meter installed on the south side of the structure. It is not the intention of the property owner to underground unless required by building codes.*

***Storm Sewer:** The storm drainage facilities are being designed by an engineer. The system will comply with the Standards. The new development is proposed will pump up to Siskiyou Boulevard unless alternative arrangements are secured through easements across adjacent properties.*

Siskiyou Boulevard is paved with curb, gutter sidewalk and parkrow along the frontage of the property which provides paved access to the development. A street tree is proposed in the parkrow.

The new building will have Fire Sprinklers installed if required by the Oregon Fire Code and Oregon Residential Structural Specialty Code.

E. Exception to the Site Development and Design Standards. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or

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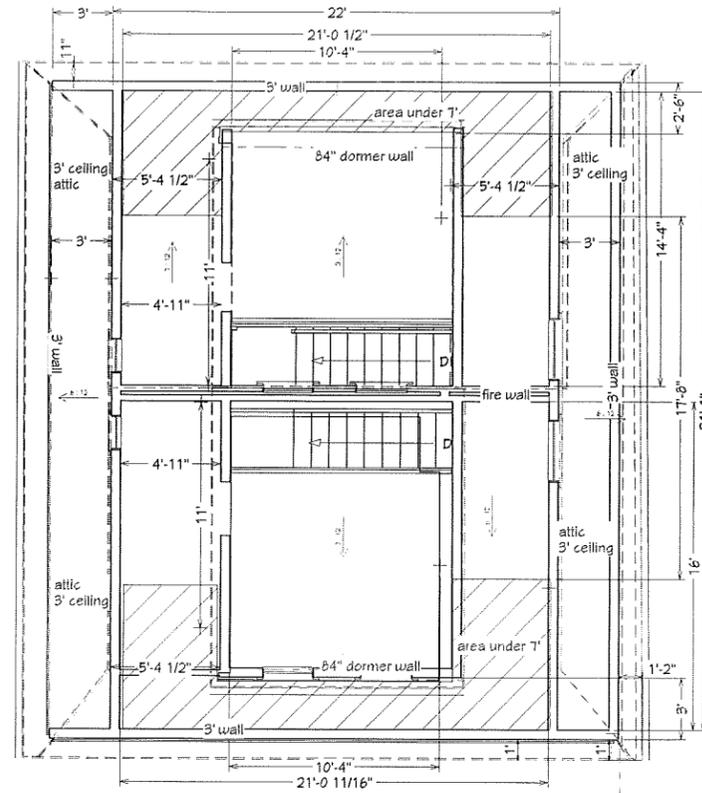
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There is demonstrable difficulty in meeting the standard from AMC 18.4.3.090 for pedestrian access and circulation by installing a continuous walkway system through the development. The lot is very narrow, the existing structure is pre-existing, the narrow driveway is a preexisting non-conforming situation and there is not room to provide a pathway system from the entrances of the units to the public sidewalk outside of the driveway. The only yard areas are the sideyards and the pathway would prevent the private patio areas from being developed. The exception will not have any impact on any of the adjacent properties. The exception is consistent with the stated purpose of the Site Development and Design Standards. The site is being developed to the allowed density with smaller, one-bedroom units that comply with all of the site spacing standards and setbacks.

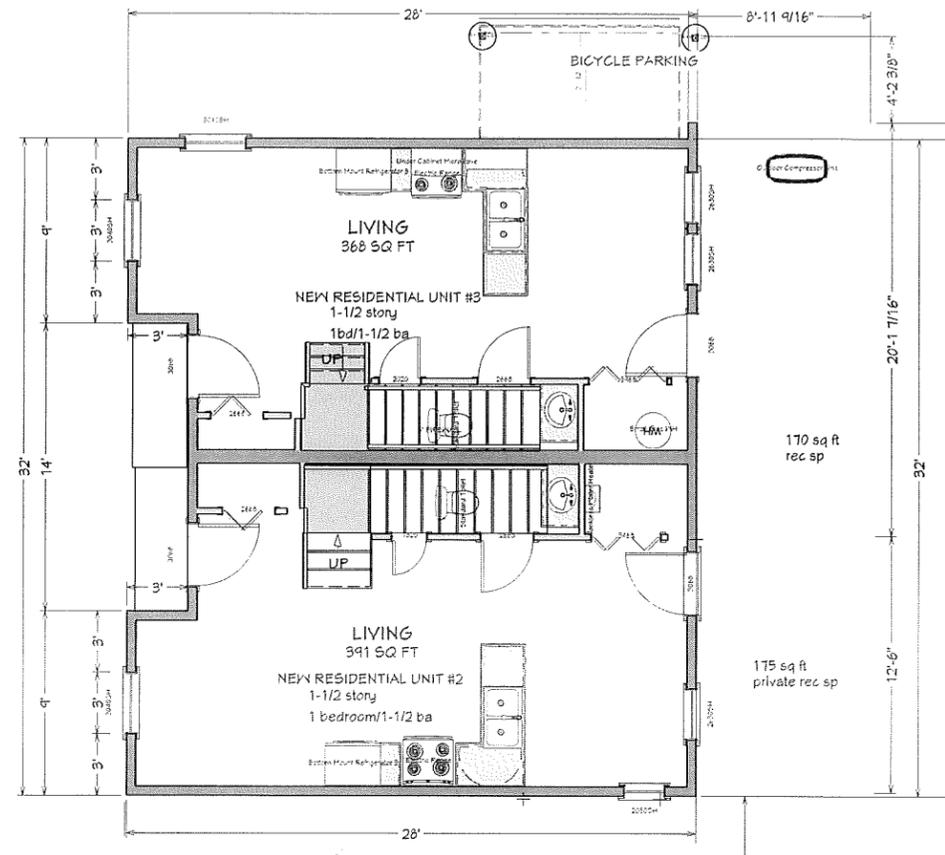
2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

The proposed duplex is similar in scale, bulk and coverage as other multi-family developments in the zone. There is limited development that is comparable to the proposed development due to the uses of the adjacent properties as commercial types of use. The proposed structure is similar in height as the allowed heights of development in the area, and is similar in height as the existing historic structure. All of the separations between buildings including setbacks and lot coverages are met which provides for a reduced bulk and coverage.

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2nd Floor
373 sq ft over T'



First Floor
896 sq ft

SQ FOOTAGE
 PROPOSED: 1269 SQ FT
 854 SQ FT, 1ST FLOOR
 373 SQ FT, 2ND FLOOR OVER T' CEILING HEIGHT
 EXISTING: 1553 sq ft
 1253 SQ FT, 2ND/MAIN FL
 TOTAL BUILDINGS FAR: 2,480 SQ FT

REVISION TABLE	NUMBER	DATE	REVISOR	DESCRIPTION

OWNER:
 MNM Fund LLC
 PO Box 964
 Ashland OR 97520
 PROJECT ADDRESS:
 451 Siskiyou Blvd, Ashland

FLOOR PLANS

DRAWINGS PROVIDED BY:
 Madeline Smith
 P O Box 964
 Ashland OR 97520
 541-301-4470

DATE:
 9/19/2017
 SCALE:
 1/4" = 1'0"
 SHEET:
 FP 15



REVISION TABLE		
NUMBER	DATE	DESCRIPTION

OWNER:
 MNM Fund, LLC
 P.O. Box 464
 Ashland, OR 97520
 PROJECT ADDRESS:
 4811 Siskiyou Blvd., Ashland

OVERVIEW FOR SITE DESIGN
 CONCEPT ONLY
 BUILDING NOT ACCURATE

DRAWINGS PROVIDED BY:
 K. James Smith
 P.O. Box 464
 Ashland, OR 97520
 541-301-4470

DATE:

9/19/2017

SCALE:

NTS

SHEET:

P 16

07/2017

REVISION TABLE	NUMBER	DATE	REVISION BY	DESCRIPTION

OWNER:
MNM Fund LLC
P.O. Box 964
Ashland OR 97520
PROJECT ADDRESS:
981 Siskiyou Blvd, Ashland

ELEVATIONS
EAST AND WEST

DRAWINGS PROVIDED BY:
Melanie Smith
P.O. Box 964
Ashland OR 97520
541-301-4970

DATE:

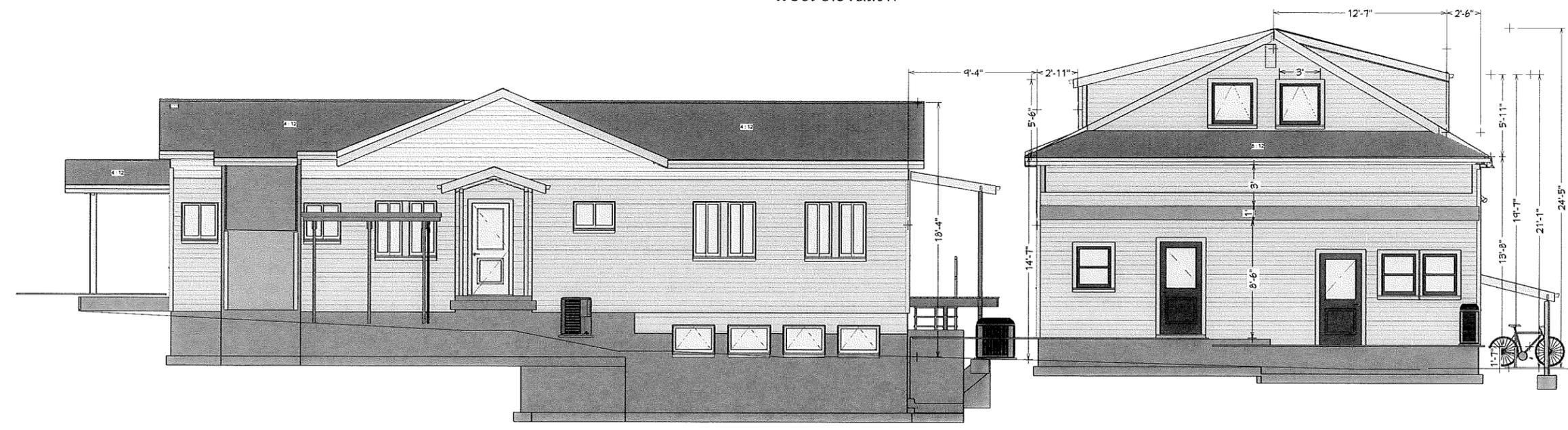
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SCALE:

SHEET:



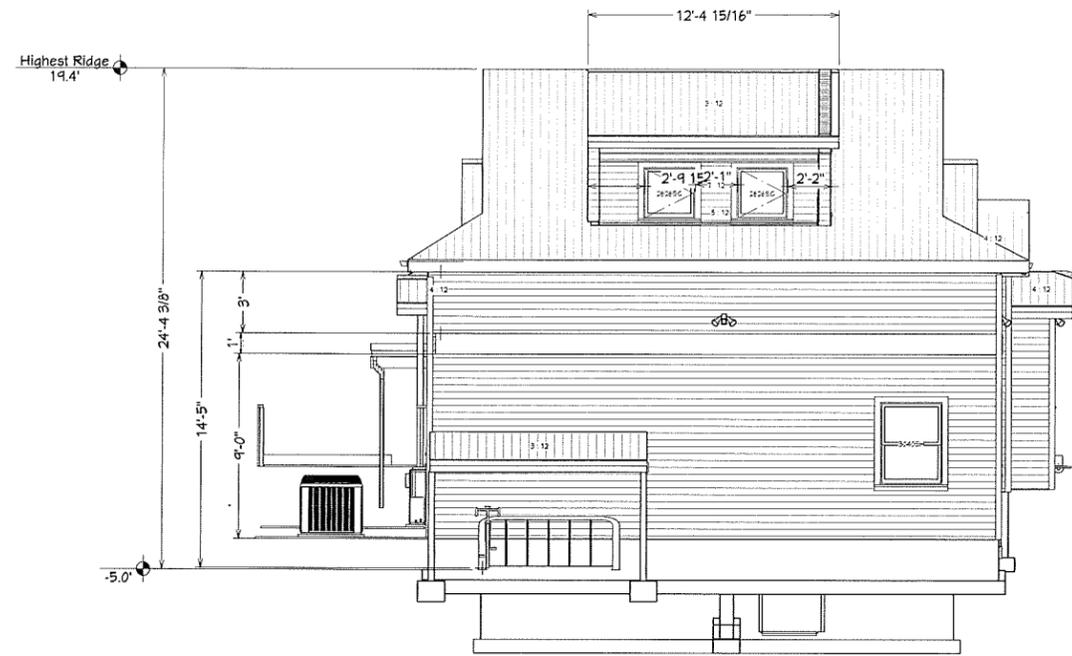
west elevation



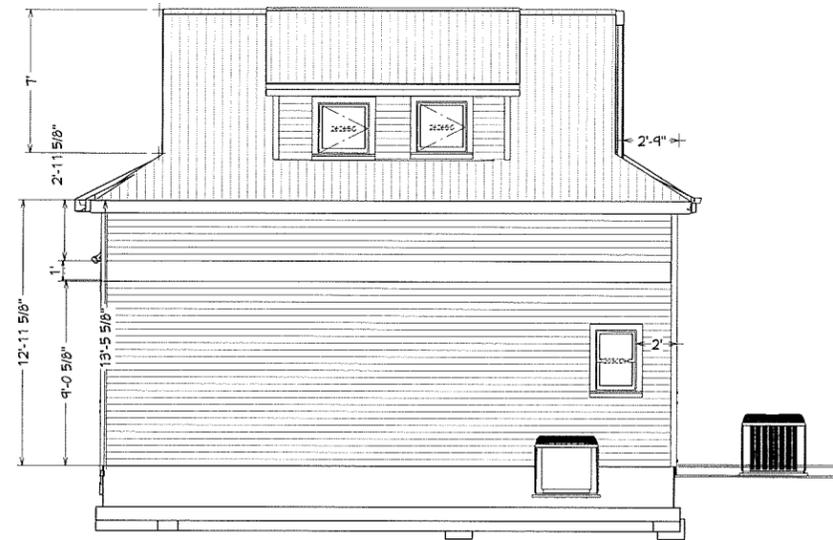
EAST ELEVATION

EXTERIOR FINISH OF NEW BUILDING:
SIDING: 9\"/>

9/19/2017
Melanie Smith



NORTH ELEVATION



SOUTH ELEVATION

EXTERIOR FINISH OF NEW BUILDING
 SIDING: 5" SMOOTH FINISH HARDI-TYPE LAP
 CORNER BOARDS: 1x4 PRIMED SPRUCE, SMOOTH SIDE OUT
 WINDOW AND DOOR WRAP: 1x4 PR SPRUCE, SMOOTH SIDE OUT
 WINDOWS: WHITE VINYL SINGLE HUNG ON FIRST FLOOR
 CASEMENT ON SECOND FLOOR TO MEET EGRESS
 EXTERIOR DOORS: 1/2 LITE RAISED PANEL EXTERIOR DOORS
 12" BELLY BAND AT SECOND FLOOR JOIST LEVEL
 ROOF: TO MATCH FRONT RESIDENCE, COLOR MAY CHANGE



REVISION TABLE	
NUMBER	DATE

OWNER:
 MNX Fund LLC
 P O Box 964
 Ashland OR 97520
 PROJECT ADDRESS:
 481 Siskiyou Blvd, Ashland

ELEVATIONS

DRAWINGS PROVIDED BY:
 Melanie Smith
 P O Box 964
 Ashland OR 97520
 541-301-4170

DATE:

9/19/2017

SCALE:

SHEET:

SEP 20 2017



NOTICE OF APPLICATION

PLANNING ACTION: PA- 2017-01605

SUBJECT PROPERTY: 147 Van Ness Avenue

OWNERS: Deborah Dryden & Robert Hirschboeck

APPLICANT: Conscious Construction

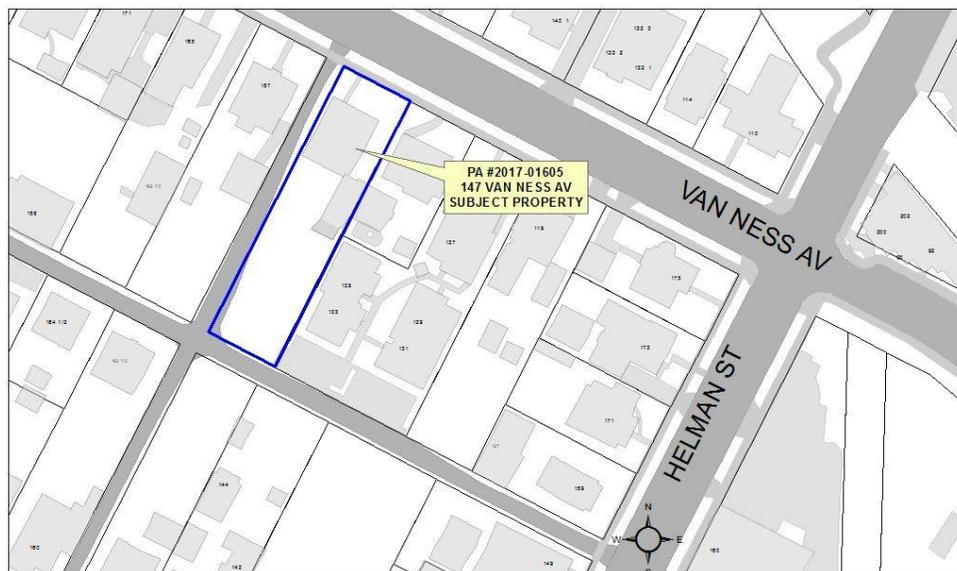
DESCRIPTION: A request for Site Design Review for a three-unit multi-family development for the property located at 147 Van Ness Avenue. The proposal includes the existing residence as one unit, the conversion of an existing guest cottage as a second unit and the construction of a new third unit with attached single vehicle garage off of the alley. **COMPREHENSIVE PLAN DESIGNATION:** High Density Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 2900.

NOTE: The Ashland Historic Commission will also review this Planning Action on **Wednesday October 4, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

NOTE: The Ashland Tree Commission will also review this Planning Action on **Thursday, October 5, 2017 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

NOTICE OF COMPLETE APPLICATION: September 21, 2017

DEADLINE FOR SUBMISSION OF WRITTEN COMMENTS: October 5, 2017



The Ashland Planning Division Staff has received a complete application for the property noted above.

Any affected property owner or resident has a right to submit written comments to the City of Ashland Planning Division, 51 Winburn Way, Ashland, Oregon 97520 prior to 4:30 p.m. on the deadline date shown above.

Ashland Planning Division Staff determine if a Land Use application is complete within 30 days of submittal. Upon determination of completeness, a notice is sent to surrounding properties within 200 feet of the property submitting application which allows for a 14 day comment period. After the comment period and not more than 45 days from the application being deemed complete, the Planning Division Staff shall make a final decision on the application. A notice of decision is mailed to the same properties within 5 days of decision. An appeal to the Planning Commission of the Planning Division Staff's decision must be made in writing to the Ashland Planning Division within 12 days from the date of the mailing of final decision. (AMC 18.5.1.050.G)

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Department to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Division, Community Development & Engineering Services Building, 51 Winburn Way, Ashland, Oregon 97520.

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division at 541-488-5305.

SITE DESIGN AND USE STANDARDS

18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. *Exception to the Site Development and Design Standards*. The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
 - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
 - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.



Planning Division
51 Winburn Way, Ashland OR 97520
541-488-5305 Fax 541-488-6006

ZONING PERMIT APPLICATION

FILE # PA-2017-01605

DESCRIPTION OF PROJECT Site design review for a 3 unit multi family development

Pursuing LEED® Certification? YES NO

DESCRIPTION OF PROPERTY

Street Address 147 Van Ness Ave

Assessor's Map No. 39 1E 04CC Tax Lot(s) 2900

Zoning R-3 Comp Plan Designation Single Family

APPLICANT

Name Conscious Construction Phone 541-973-8889 E-Mail Jason@designbuildashland.com

Address PO Box 3205 City Ashland Zip 97520

PROPERTY OWNER

Name Deborah Dryden/Robert Hirschboeck Phone _____ E-Mail _____

Address 71 Scenic St. City Ashland Zip 97520

SURVEYOR, ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OTHER

Title Amy Hunter Name Rogue Planning + Development Phone 541-951-4020 E-Mail _____

Address 1424 S. Ivy St. City Nedford Zip 97520

Title _____ Name _____ Phone _____ E-Mail _____

Address _____ City _____ Zip _____

I hereby certify that the statements and information contained in this application, including the enclosed drawings and the required findings of fact, are in all respects, true and correct. I understand that all property pins must be shown on the drawings and visible upon the site inspection. In the event the pins are not shown or their location found to be incorrect, the owner assumes full responsibility. I further understand that if this request is subsequently contested, the burden will be on me to establish:

- 1) that I produced sufficient factual evidence at the hearing to support this request;
- 2) that the findings of fact furnished justifies the granting of the request;
- 3) that the findings of fact furnished by me are adequate; and further
- 4) that all structures or improvements are properly located on the ground.

Failure in this regard will result most likely in not only the request being set aside, but also possibly in my structures being built in reliance thereon being required to be removed at my expense. If I have any doubts, I am advised to seek competent professional advice and assistance.

X Jason Eaton _____
Applicant's Signature Date 8/22/17

As owner of the property involved in this request, I have read and understood the complete application and its consequences to me as a property owner.
X Robert W. Hirschboeck _____
Property Owner's Signature (required) Date 8/22/17

(To be completed by City Staff)

Date Received 8/21/17 Zoning Permit Type Type 1 Filing Fee \$ 1,115⁰⁰

OVER ⇨

August 18, 2017

**Multi-Family Site Review
147 VAN NESS**

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AUG 21 2017
City Of Ashland

Property Owner: Deborah Dryden
Robert Hirschboek
71 Scenic Street
Ashland, OR 97520

Applicant/Designer: Conscious Construction
PO BOX 3205
Ashland, OR 97520

Planning Consultant: Amy Gunter
Rogue Planning & Development Services
1424 S Ivy Street
Medford, OR 97520

Subject Property

Property Address: 147 Van Ness

Map & Tax Lots: 39 1E 04CC Tax Lots: 2900
Zoning: R-3 (High Density Multiple Family)
Adjacent Zones: R-3
Lot Area: 9,987 square feet

Max Lot Coverage: 6,491.5 square feet
+200 sf porous solid surface: 6,691.5 square feet

Maximum Permitted Floor Area: 3,062 square feet
Allowed Density 18.2.5.080: .229 X 20 = 4.58
Proposed Density: 3

Outdoor Recreation – max 8%: 799 sf

Request:

A request for a Site Design Review for three-unit, multi-family development consisting of the existing residence, conversion of existing guest cottage, and the construction of new unit with attached single vehicle garage.

Property Description:

The subject property is the west half of Lot 9, Block 6, Original Ashland. It is on the south side of Van Ness, to the west of the intersection of Helman Street and Van Ness. The property is bordered on the west side by a 12.5-foot wide right-of-way, gravel alley that connects Van Ness to Central Avenue. The south property line is bound by a 16-foot wide right-of-way, gravel alley that connects Helman Street and Laurel Street.

The lot is 50 feet by 199.74 feet for 9,987 square feet in area. The lot and the adjacent properties are zoned R-3, High Density, Multi-Family Residential. The site slopes approximately four percent to the north. Along the rear property line, the alley that parallels Van Ness is four feet above the grade of the site. All of the site access is from the north / south alley that parallels the west side of the property.

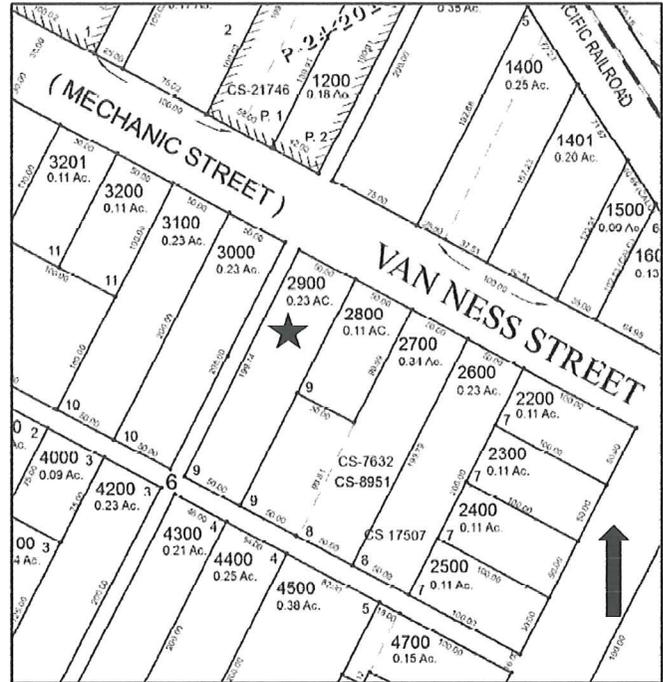
For the purposes of the solar setback standards, the new unit is exempt as Van Ness has less than a 45-degree angle and is considered the north property line.

The site is occupied by non-historic, non-contributing "ranch" style structure. The details from the Historic Resources Inventory are attached.

The trees on the subject property, and those along the east property line are shown on the site plan

Van Ness has a 60-foot wide right-of-way and is improved with pavement, curb and gutter, eight-foot parkrow and six-foot sidewalk along the frontage of the property. The gravel alley along the west side of the property is 12.5 feet wide. There is also a gravel alley along the south property line that has a 16-foot wide right-of-way.

There are overhead power lines and a cobra head street light in the parkrow along the frontage of the property. There are no street trees within the parkrow.



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Proposal:

The request is to construct a new, single story unit with attached single vehicle garage to the rear of the small residence on the site. This structure is a 551-square foot, one bedroom, one bath cottage with an attached single vehicle garage. The cottage has a 6' X 14' (84 square foot) front porch and a 12' X 15' (180 square foot) rear patio. A rear yard area is also provided for the tenant.

There is an existing 676 square foot, one story with sleeping loft guest cottage to the rear of the residence that is directly adjacent to the east property line (396 ground floor / 280 loft). This guest cottage that is proposed to be converted to a unit is a studio unit. This structure directly abuts a small structure on the adjacent property to the east at 143 Van Ness. There are no openings along the east property line that is shared with the adjacent property to the east.

The circa 1955, 944 square foot cottage has a 90-square foot front porch facing Van Ness. The structure is setback 25-feet from the front property line, has a five-foot setback to the west (side) property line and is separated from the converted cottage by more than ten feet.

The driveway access from Van Ness is proposed to be retained.

The proposed development requires five (5) vehicle parking spaces.

Four (4) spaces are provided on-site. One space is in the existing driveway, two on-site to the south of the 8" DBH Morus, accessed via that N/S alley. Another space is provided in the 416-square foot garage proposed for the new cottage, this space is also accessed from the N/S alley. One space is requested as on on-street parking credit.

The proposed development requires five (5) bicycle parking spaces. Two are provided for on the existing covered front porch of the residence, one to the rear of the converted guest house and two in the garage for the new unit.

A common trash / recycle enclosure area will be provided that is screened from the alley right of way that is accessible for Recology.

The existing residence has its orientation towards Van Ness. The existing guest cottage is oriented to the south yard area. The new cottage with attached garage is proposed at a slight angle to the N/X running alley. The angle is to take advantage of the properties solar orientation and to have less of an impact from the neighbor's trees onto the solar panels.

The proposal complies with Maximum Permitted Floor Area in the historic district.

MPFA:

Adjusted Lot Area:	7,290.5 SF
Allowed MPFA for 3 units:	3,062 SF
Proposed new unit:	967 SF
Existing Structures:	1,620 SF
Total Proposed Floor Area:	2,587 SF

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The new unit is low in stature, will have trim, siding, window style, corner boards, porch columns and architectural details reflective of the historic contributing structures in the vicinity. The new constructed unit is proposed to be reflective of an American bungalow style structure, but with a more modern, shed-style, single roof. The existing residence and the existing cottage are proposed to remain as is in the historically appropriate development style of 1955 cottages. The proposed development is similar in bulk, scale and coverage as other multi-family developments in the vicinity.

There are ample open spaces provided. These consist of approximately 550 square foot of porches and patios. There remaining open space will be provided in the yard area. There is approximately 4,979 square feet of landscape area.

On the following pages, the criteria from the Ashland Land Use Ordinance as it pertains to Site Design Review and Conditional Use Permit criteria have been addressed. The City of Ashland criteria are in Times New Roman font and the applicant's findings are in Calibri font.

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Findings of Fact:

Site Design Review - 18.5.2.050

A. Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

The subject property is zoned R-3, High-Density Multiple Family Residential. The lot has 9,987 square feet and has a potential density of 4.5 units.

The property complies with the maximum permitted floor area (MPFA) of 3,062-square feet. The proposed structure complies with the setbacks in the zone. The existing structure, in particular, the guest house to be converted to a studio unit, does not meet setbacks. This is a pre-existing non-conforming setback. The setback is not proposed to be modified or altered as part of this proposal. The separation between the existing structures is also not proposed to be modified. The proposed, new structure complies with the solar setbacks because Van Ness is the north property line. The structures on the property comply with building height, orientation, and coverage standards. The lot coverage is proposed at 4,938 square feet in area, this is 49 percent of the allowed 75 percent coverage. The surface parking space for the new units are more than five feet from the property line and more than eight feet from any residence, since it is accessed from the alley and complies with spacing standards no planted buffers are proposed.

B. Overlay Zones.

The property is within the Skidmore Academy Historic District.

Though more modern in design, the overall design compliments that the existing architectural character of the structures in the immediate vicinity of the property. Van Ness Street was formerly Mechanic Street and Central was formerly Factory Street. These historic street names are reminiscent of the types of uses of uses on the street, and the character of the homes on the streets. Residences in the immediate vicinity are simpler in design than other homes in Ashland's Historic Districts. There are few Italianate, Queen Anne or even craftsman embellishments found in the immediate vicinity. The material choices are reflective of the neighborhood pattern and harken a time of industrial and vernacular design. The proposed structure is a smaller, rear yard, cottage that will be minimally visible (if at all during the spring and summer when the trees are leafed out) from the Van Ness right-of-way. Much like historic site layouts found in the area, the proposed structure is designed to create a highly functional residence and yard that is oriented for livability and maximum solar gain. The proposed design can be found to comply with the applicable overlay zone requirements.

18.4.2.050 Historic District Development

B. Historic District Design Standards.

The property is the east half of a lot that was historically platted in Lot 9, Block 6 of Original Ashland.

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The proposed new construction is visually unobtrusive from a public right-of-way, and the proposed design though more modern, the proposed design does not obscure or eliminate character defining features of historic buildings in the vicinity.

Height: The proposed structure is similar to average heights of structures in the vicinity. Though lower in height than many of the residences in the area, the structure is not substantially shorter or taller than the historic contributing structures in the vicinity.

Scale: The scale of the proposed structures is within the range of other multi-family dwellings in the vicinity. The limited square footage of the 499-square foot unit with the attached 416-square foot garage, the size of the structure is similar in scale to properties in the immediate vicinity.

Massing: The proposed new construction has a relatively small footprint that is consistent with the size and mass of other structures in the vicinity. There is more than one shed proposed to alter the massing of the small structure.

Setback: The proposed unit complies with the setbacks in the zone and exceeds the minimum setbacks allowed by code.

Roof: The proposed pitch is a low pitch, single gable, metal roof for the installation of solar panels. The pitches are similar to the various roof pitches in the vicinity. The roof is proposed to be installed with metal roofing.

Rhythm of Openings: The proposed new construction will have consistently spaced window pattern that is consistent with the rhythm of openings found on adjacent historic structures. The structure is proposed to have side by side sliding windows.

Base or Platform: The concrete base of the new construction is minimally exposed and is consistent with garage construction. The property fencing will block the view of most of the base.

Form: The form of the ARU is consistent with more modern, unit over garages found in the Railroad Historic District. The form is similar to a craftsman aesthetic with modern styling. The step back of the roof top patio reduces the mass of the unit on the alley.

Entrances: The primary residence has an entrance that faces the public street. Due to the substantial setback of the ARU and its alley frontage, it is oriented towards the rear of the property.

Imitation: The proposed unit is a contemporary craftsman with a single-gable, shed roof. The structure has elements of the historic contributing structures in the vicinity. These include the use of horizontal, wood siding, stucco and metal. There are a variety of housing styles in the vicinity.

The majority of the structures in the vicinity consist of 20th Century vernacular structures and “modern” period ranch style structures. There are elements of craftsman and American bungalow cottage style construction found in the immediate vicinity.

Garage Placement: The garage is accessed via the alley along the west side of the property.

C. Site Development and Design Standards.

Multi-Family Site Design Review - 18.4.2.030

Building Orientation to the Street: *The primary residence faces Van Ness Street. The new unit is proposed towards the rear of the property and is oriented through the design and slight angle towards the north / south alley.*

Limitation on Parking between Primary Entrance and Street. *There is no parking between the primary entrance and the street.*

Building Materials: *The proposed ARU is a simple, contemporary craftsman style structure with a low-pitch, single gable roof. The structure is proposed to be constructed of a combination of horizontal, wood siding and stucco. These materials are found in the vicinity. Stucco, though not common is found in historic structures on Helman Street, and at the intersection of Van Ness and Helman.*

Build-to Line. *All setbacks are existing for the primary residence and the converted. There are no changes to the front yard setback. The setbacks for the new construction comply with the minimum setbacks required in the zone.*

Garages. *The proposed new unit will have an attached garage, it is behind the existing residence. New vehicular access to the site is from the alley and not from the street.*

Landscaping and Open Space: *The site is required to have 799 square feet of open space and recreation area. There is substantially more than 799 square feet of open space throughout the site. There is private open space, in the form of the 110-square foot deck for the converted guest house unit, the patio at the rear of the proposed, newly constructed residence and the yard areas account for substantially more than 689 square feet in areas for recreating.*

The areas that account for the open space and recreation areas are surfaced with decking, porch flooring, and yard area.

The open spaces are a mixture of private patio areas and yard areas.

A play area is not provided.

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The proposed development demonstrates compliance with the landscape plan requirements. There is 4,938 square feet of impervious surfaces allowing for substantial areas of landscaping. The proposed landscaping plan and the irrigation plan that will be submitted complies with the Irrigation and Water Conserving Landscaping requirements of the City of Ashland. The conceptual landscaping plan submitted with the application has been designed so that plant coverage of 50 percent after one year, and 90 percent within five years of planting is met.

The proposed development is outside of the tree protection zones. Tree protection fencing will be installed at the dripline of the trees. Street trees that comply with the spacing standards will be planted along the frontage of the property within the parkrow.

**18.4.3.030 General Automobile Parking Requirements and Exceptions
Minimum Number of Off-Street Automobile Parking Spaces.**

Two off-street parking spaces are required for the primary residence. One parking space is required for the converted studio unit and two spaces for the new, 551-square foot unit.

18.4.3.080 Vehicle Area Design

A. Parking Location

The property presently has a single vehicle parking space in the driveway and an on-street parking credit. With the proposed development, three new parking spaces are proposed. These are all to the side or rear of the primary structure and not between the primary residence and the street.

B. Parking Area Design. Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in 18.4.3.080.B. See also, accessible parking space requirements in section 18.4.3.050 and parking lot and screening standards in subsection 18.4.4.030.F.

1. Parking spaces shall be a minimum of 9 feet by 18 feet.

There is an existing surface parking space in the single vehicle driveway that accesses the site from Van Ness. This space is more than nine-feet wide and deeper than 18-feet. Two head-in, single vehicle, surface parking spaces are proposed. These spaces are nine-feet wide and 18-feet deep. A third space is proposed in the garage for the new unit.

2. Up to 50 percent of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 feet by 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."

No compact car parking is proposed.

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3. Parking spaces shall have a back-up maneuvering space not less than 22 feet, except where parking is angled, and which does not necessitate moving of other vehicles.

All proposed parking has a back-up space of 22-feet or is situated in manner that allows for maneuvering consistent to the necessary spacing standards for a 45 degree, diagonal parking space. The driveway for the garage and the alley accommodates the necessary back up dimensions.

b. Developments subject to a planning action shall remove all curb cuts and driveway approaches not shown to be necessary for existing improvements or the proposed development. Curb cuts and approaches shall be replaced with standard curb, gutter, sidewalk, and planter/furnishings strip as appropriate.

There is an existing curb cut that accesses the site from Van Ness. This driveway serves an existing parking space, this non-conforming situation is proposed to be retained.

c. If the site is served by a shared access or alley, access for motor vehicles must be from the shared access or alley and not from the street frontage.

All new vehicular access is from the alley.

5. Alley Access. Where a property has alley access, vehicle access shall be taken from the alley and driveway approaches and curb cuts onto adjacent streets are not permitted.

There is an existing, single vehicle driveway that provides one of the required parking spaces. This driveway is originally and is requested to be retained. No new curb cuts are proposed. New vehicle access is from the alley.

18.4.3.090 Pedestrian Access and Circulation

Due to the existing structure locations, trees, and decks, there is not adequate room within the yard areas to provide for a continuous walkway through the property. There will be a walkway that leads from the unit to the garage driveway which is connected to the alley. Pedestrian traffic will continue to be within the alley right-of-way.

D. City Facilities.

The proposal complies with the standards for public facilities, and adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation will be provided to the property.

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The existing residence and guest house are served by water, sewer and electric service. There are no storm drain facilities on the site. With the development of the new unit, the electric service on the property will be relocated from the aerial, overhead drop that presently services the site, and a new service connection from the power pole in the alley. Three electric meters for the units will be installed on the south side of the ARU facing the south property line.

There is enough capacity to service the units. The existing water service to the property will continue to service the front two units. A new service will be installed for the new unit.

In discussion with the Public Works Department, the storm drainage for the newly constructed unit, will be directed to the alley along the west property line. The existing structures are not connected to storm drain facilities and are not required to connect until expanded or enlarged, if ever the case.

The property is connected to sanitary sewer, there is adequate capacity within the City's facilities for the new unit.

E. Exception to the Site Development and Design Standards.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or

There is demonstrable difficulty in meeting the standard from AMC 18.4.3.090 for pedestrian access and circulation by installing a continuous walkway system through the development. The lot is narrow, the front residence, and the landscape areas including trees are pre-existing. These improvements span the width of the site and prevent pedestrian pathways from the units, through the property. The narrow lot and the improvements are a preexisting, non-conforming situation and there is not room to provide a pathway system from the entrances of the new units to the public sidewalk outside of the driveway and the alley.

The exception will not have any impact on any of the adjacent properties. The exception is consistent with the stated purpose of the Site Development and Design Standards. The site is being developed to the allowed density with smaller, one-bedroom unit and conversion of an existing guest house to a small studio unit. All new construction complies with all of the site spacing standards and setbacks.

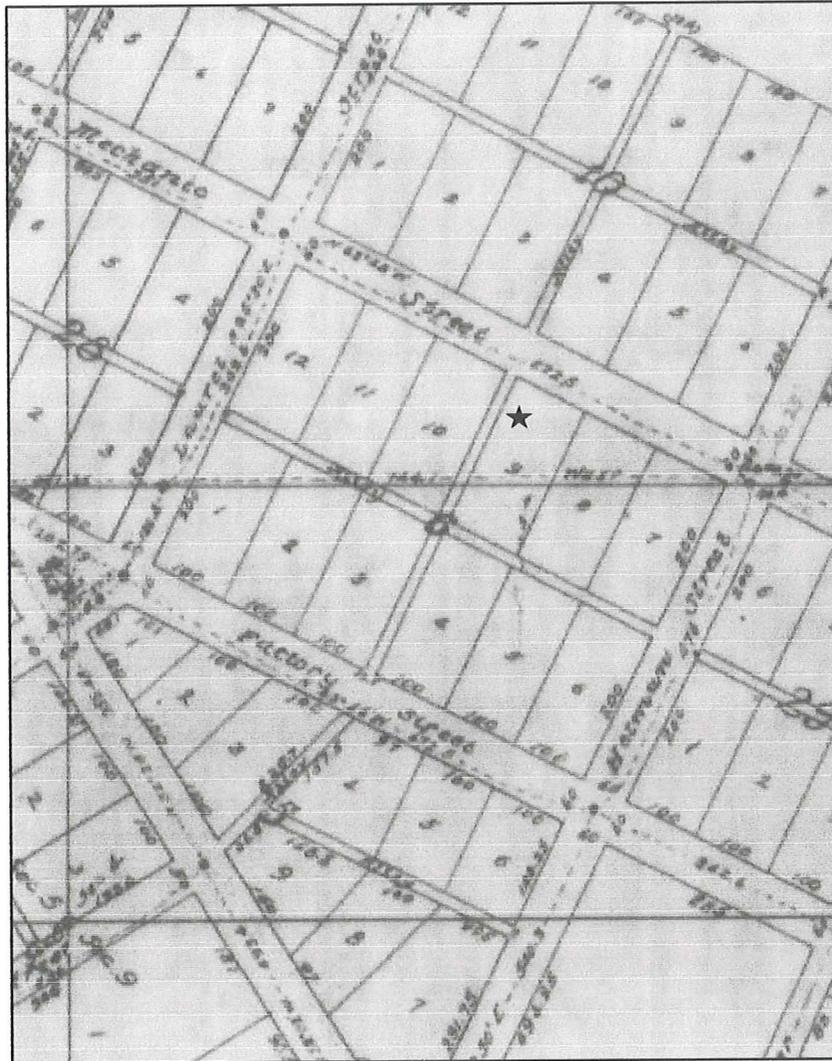
Attachments:

- A) Original Ashland Map 1888 (clip)
- B) Assessor's Map
- C) Historic District Inventory
- D) Preliminary Proposed Site Plan

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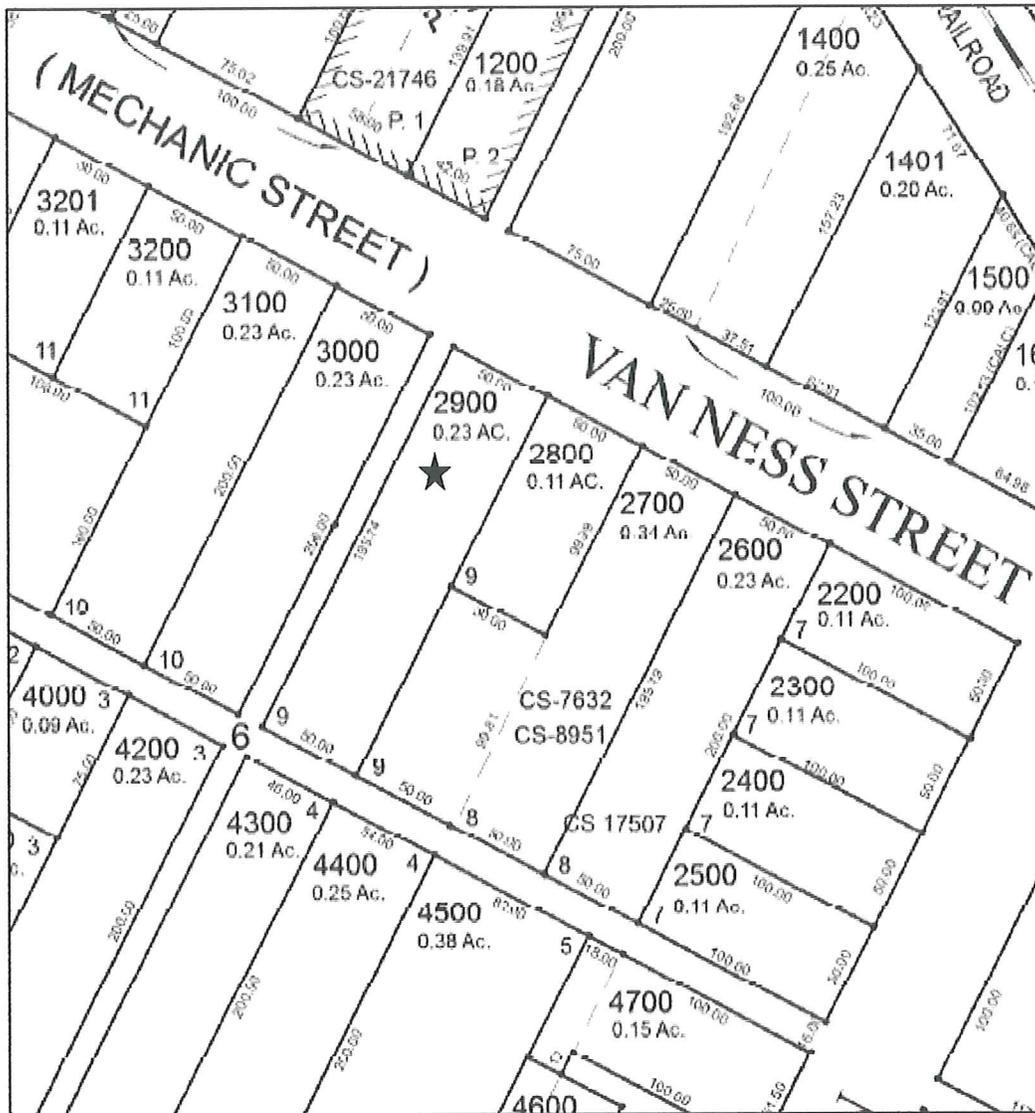
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Original Ashland 1888: Lot 9; Bock 6

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1/31/2011

City Of Ashland

National Register of Historic Places Skidmore Academy Historic District Inventory

22.0

GORDY, RAE & MARGARET HOUSE
147 VAN NESS AVE
Modern Period: Ranch House

1955c
391E04CC 2900
Non-Historic, Non-Contributing

This property was long owned by Elisha and Della Splawn, who lived at 180 Central. The single-story ranch style dwelling was probably built after 1955, when the property was sold. (JCD 283:395). In 1964 Rae and Margaret Gordy lived at the this address according to the city directory.

PROJECT INFORMATION

PROPERTY OWNER: DEBORAH DRYDEN AND ROBERT HIRSCHBOEK
71 SCENIC STREET ASHLAND OR

DESIGNER/CONTRACTOR: CONSCIOUS CONSTRUCTION
PO BOX 3205 ASHLAND OR
541 973 8889

SITE AND BUILDING DATA:

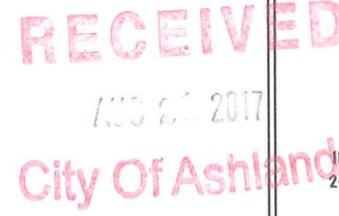
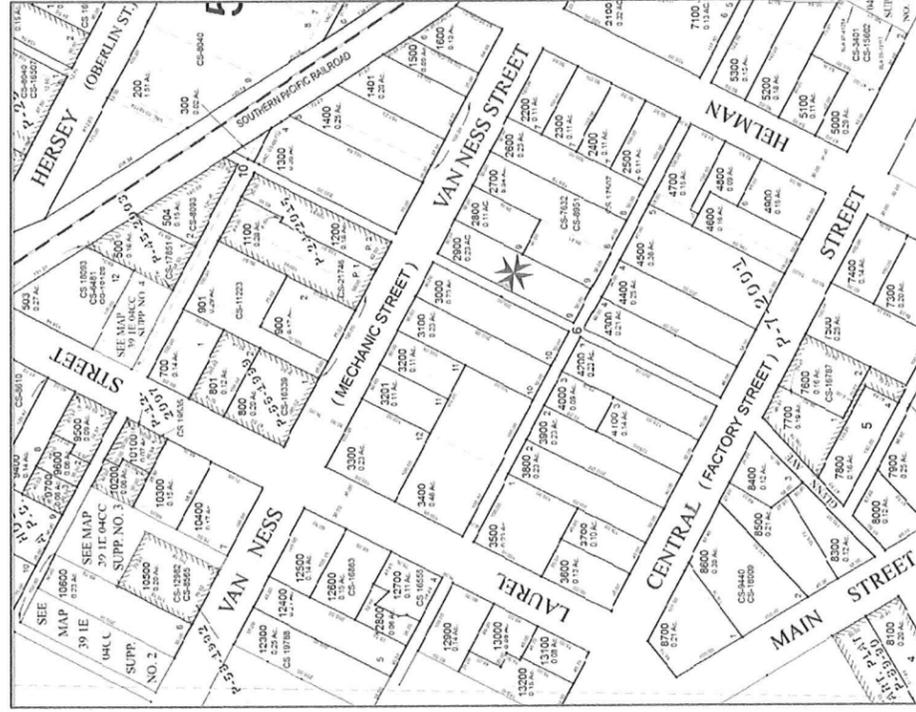
ADDRESS: 147 VAN NESS AVE ASHLAND OR
LOCATION: MAP 39 IE 04 TAX LOT 2900
LOT SIZE: 0.23 ACRE - 9987 SF
ZONING: R-3
ADJACENT ZONES: R-3
EXISTING 1 STORY RESIDENCE: 944 SF
EXISTING 2 STORY COTTAGE: 676 SF
PROPOSED 1 STORY COTTAGE: 551 SF
PROPOSED GARAGE: 416 SF
TOTAL SF ALL STRUCTURES: 2587 SF

LOT COVERAGE:

EXISTING RESIDENCE 944 SF
EXISTING STUDIO 396 SF
EXISTING GRAVEL DRIVEWAY 765 SF
EXISTING PATHS/PATIOS/PORCH 335 SF
PROPOSED RESIDENCE 551 SF
PROPOSED GARAGE 416 SF
PROPOSED GRAVEL PARKING 1040 SF
PROPOSED PATHS/PATIO/PORCH 491 SF
TOTAL IMPERVIOUS 4938 SF <49%>
TOTAL SF OF LANDSCAPE AREAS 5049 SF <51%>

PARKING:

ON-STREET CREDIT 1
EXISTING DRIVEWAY 1
PROPOSED PARKING 3
TOTAL # SPACES 5



VICINITY MAP-NITS

DRYDEN AND HIRSCHBOEK PROJECT
147 VAN NESS STREET
ASHLAND, OREGON

AUGUST 16, 2017

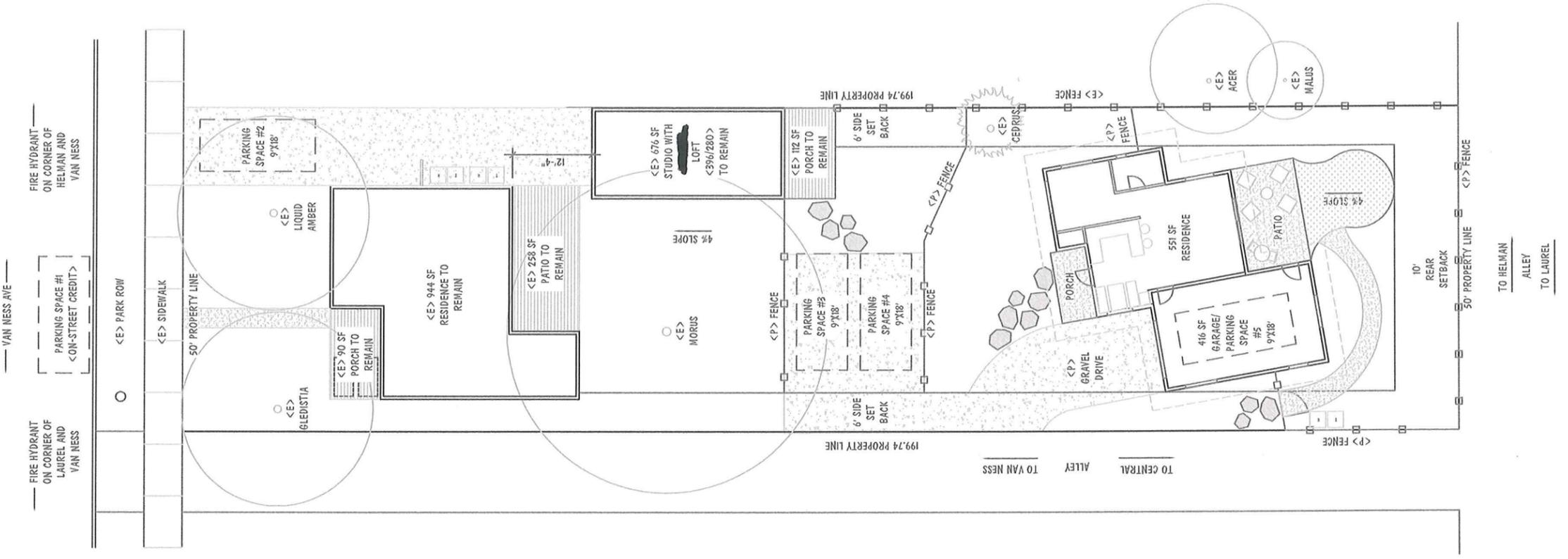
L-1.0

FULL SITE PLAN

REVISION DATE:

DRAWN BY:
KAE

SCALE:
1/4" = 20'-0"
1/8" = 10'-0"

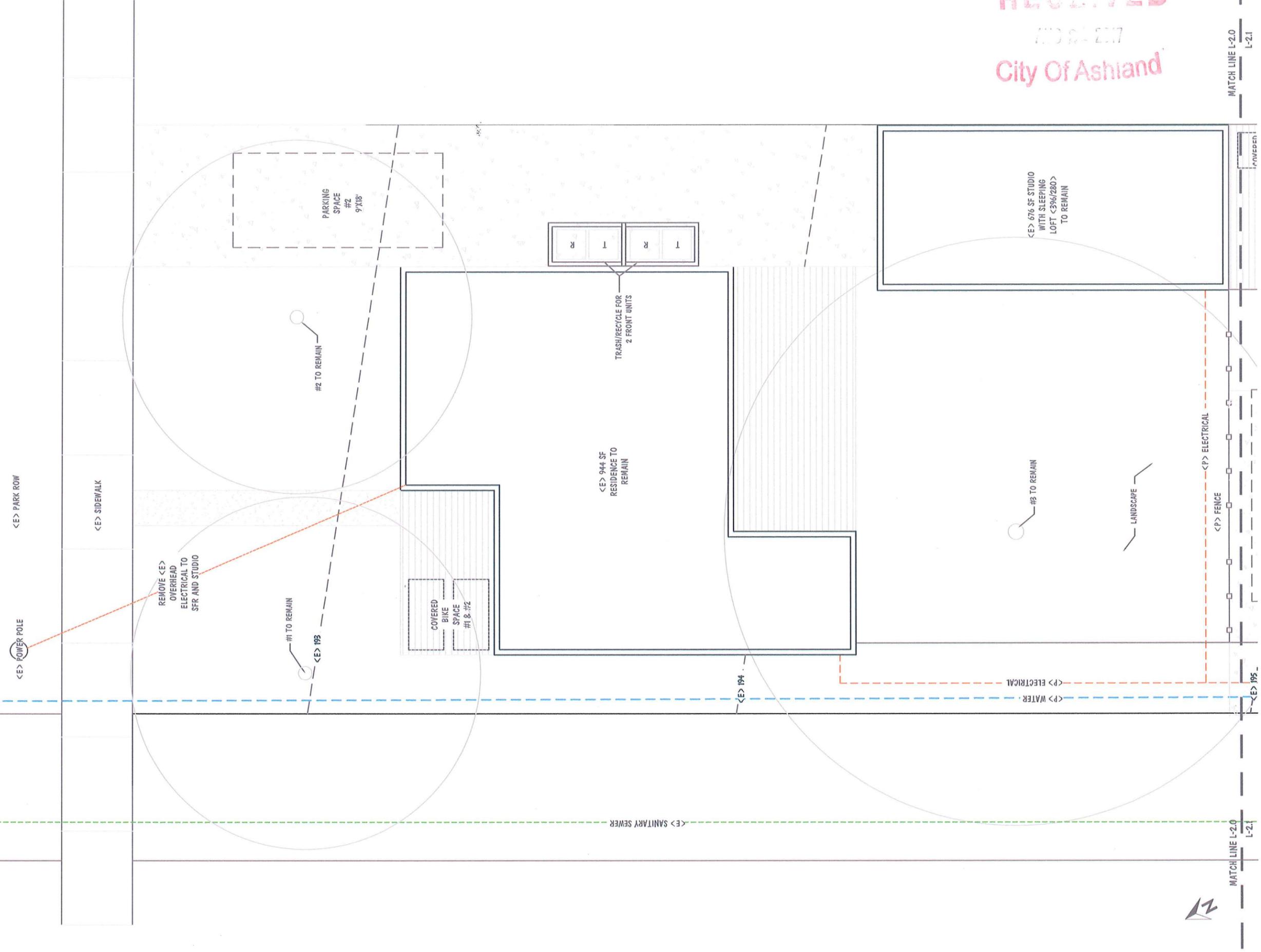


PLANS PROPERTY OF:
CONSCIOUS CONSTRUCTION, INC.
PO BOX 3205
ASHLAND, OREGON
541 973 8889

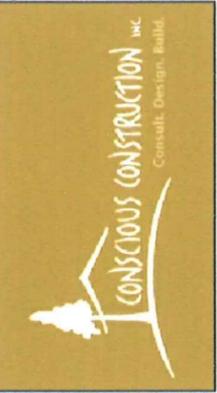
RECEIVED
 AUG 24 2017
 City Of Ashland

NOTES:
 *ALL DISTURBED AREAS SHALL BE COVERED WITH STRAW TO PREVENT EROSION
 *BIO-BAGS SHALL BE PLACED AT ALL POINTS OF POTENTIAL RUN-OFF TO MINIMIZED SEDIMENT IN STORM DRAIN

PARKING SPACE #1
 <ON-STREET CREDIT>
 9'X18'



PLANS PROPERTY OF:
 CONSCIOUS CONSTRUCTION, INC.
 PO BOX 3205
 ASHLAND, OREGON
 541 973 8889



REVISION DATE:

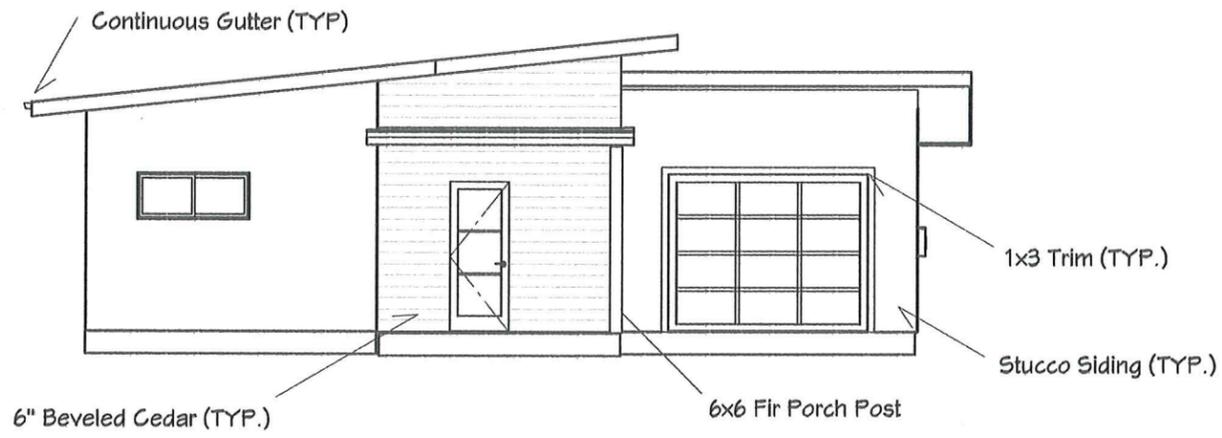
DRAWN BY:
 KAE

SCALE:
 11X17 1"=8'-0"
 24X36 1"=4'-0"

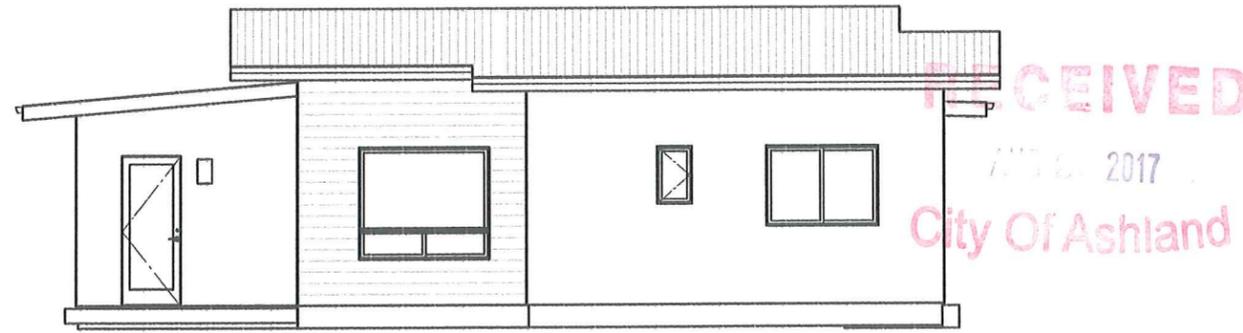
DRYDEN AND HIRSCHBOEK PROJECT
 147 VAN NESS STREET
 ASHLAND, OREGON

AUGUST 16, 2017

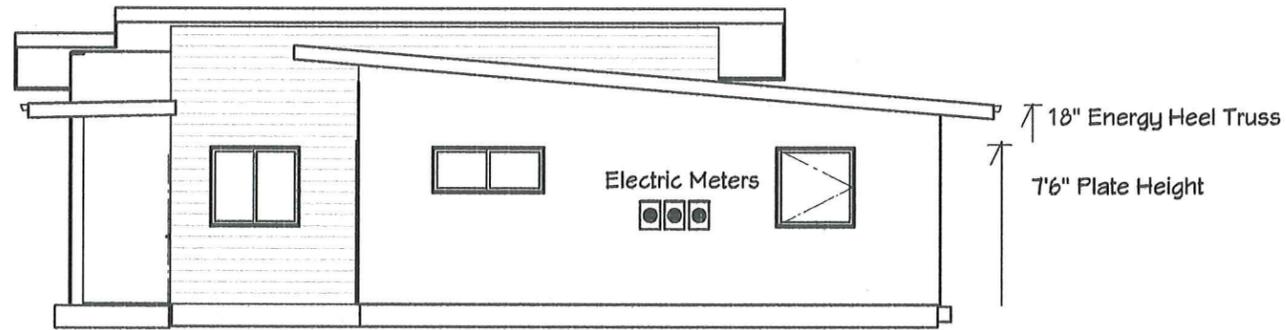
L- 2.0
 DRAINAGE,
 EROSION, AND
 UTILITY PLAN



NORTH



EAST



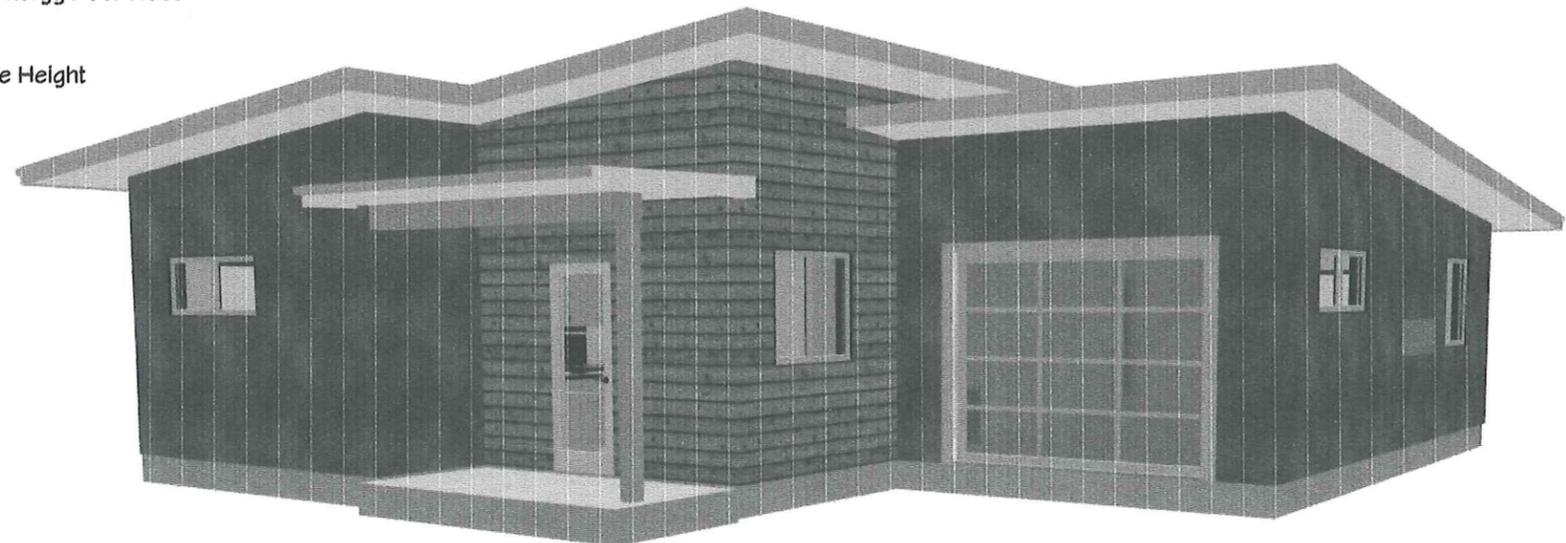
WEST



SOUTH

NOTES:

- 1/12 Pitch all roofs except entry porch .25/12
- All roof overhangs 30" w exception of high side @ 36"
- Standing Seam metal roof



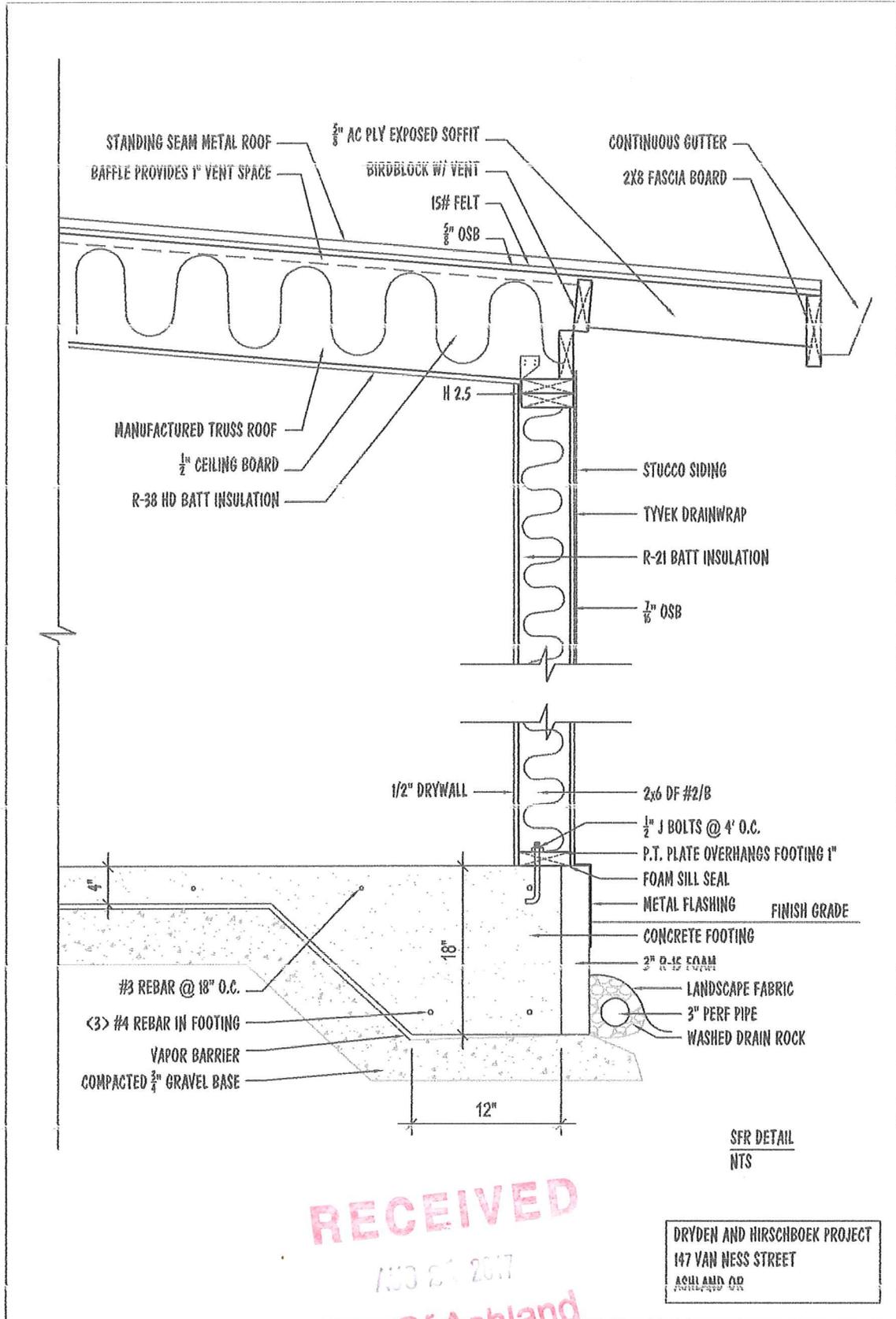
Plans designed by and property of:
 Conscious Construction Inc.
 PO Box 3205 Ashland, OR 97520
 (541) 973.8889
 jason@designbuildashland.com

Hirschboeck/Dryden
 147 Vanness Ave
 Ashland, OR

1/8"=1'

8/18/2017

ELEVATIONS



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City Of Ashland

DRYDEN AND HIRSCHBOEK PROJECT
147 VAN NESS STREET
ASHLAND, OR

- ① EXISTING STRUCTURAL
- ② MATCH EXISTING SIDING $\frac{5}{8}$ " T&H
- ③ OPEN TO STORAGE LFT (SEE PLAN)
- ④ 4EI KNEE WALL FOR DORMER
- ⑤ $5\frac{1}{2}$ " OVERHANG ON BARGE RAFTER
- ⑫ SIMPSON HURRICANE CLIPS.

CEIVED

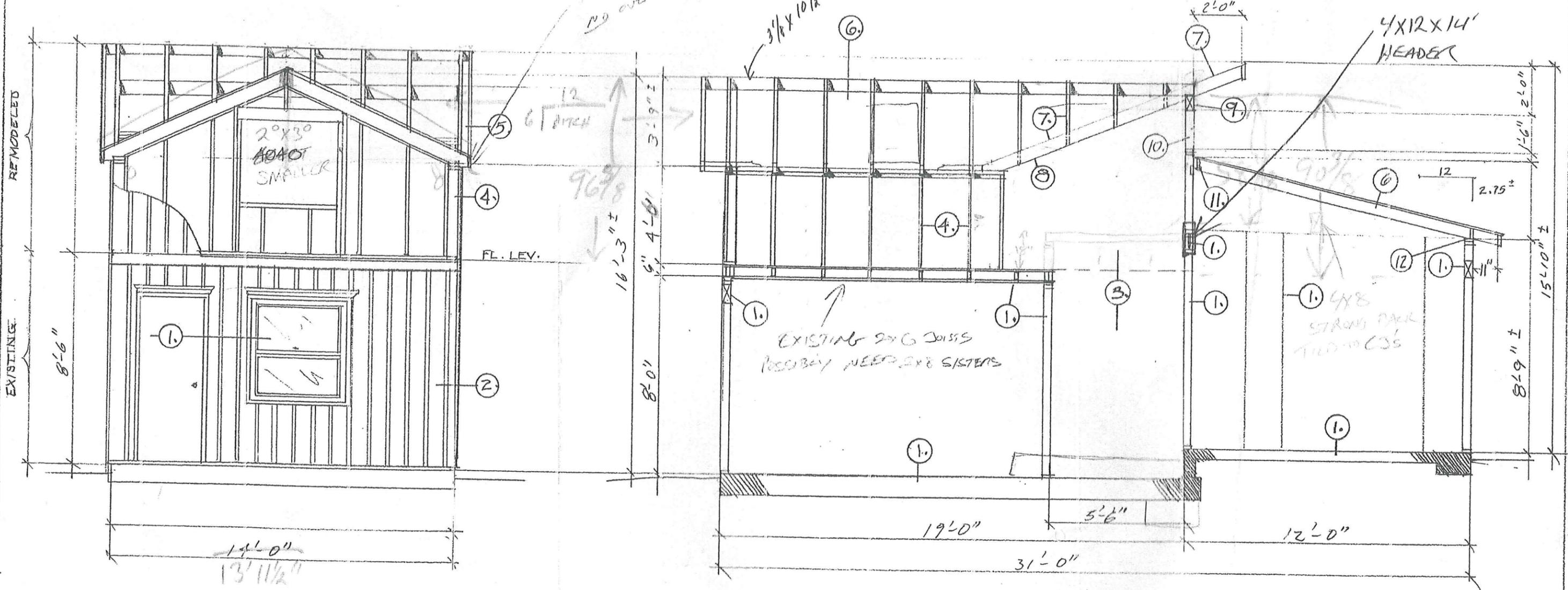
03 21 2017

Of Ashland

- ⑥ DORMER RAFTER FRAMING (SEE: DETAIL A)
- ⑦ JACK RAFTERS (2X6) 24" OC.
- ⑧ (2X8) VALLEY RAFTER (SEE: ROOF PLAN)
- ⑨ (4X6) HEADER OVER VENT WINDOWS
- ⑩ #2-(18"X48") AWNING VINYL WINDOWS w/(OPERATING POLE)
- ⑪ (2X6) RAFTER W/ SIMPSON HANGERS

FLOOR FRAMES TO WALL NO OVERHANG

3/4" X 10 1/2" G/L LAM



FRONT ELEVATION
NORTH

X-SECTION A-A
EAST

SCALE: $\frac{1}{4}$ " = 1'-0"

(E) STUDIO

ROB HIRSCHBOECH
147 1/2 VANNESS AVE

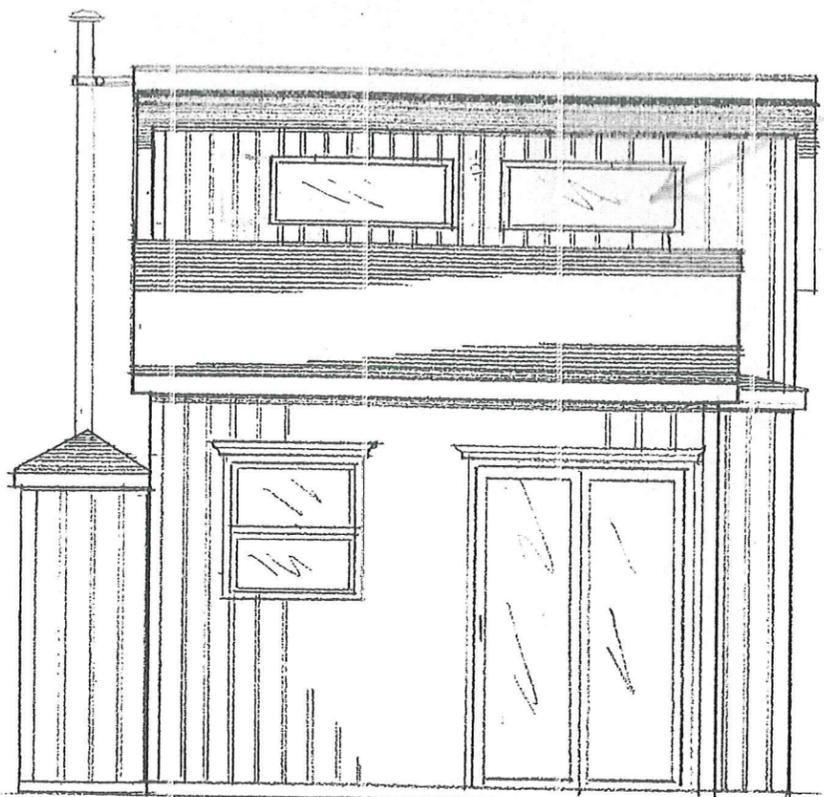
RECEIVED

MAY 21 2017

City Of Ashland

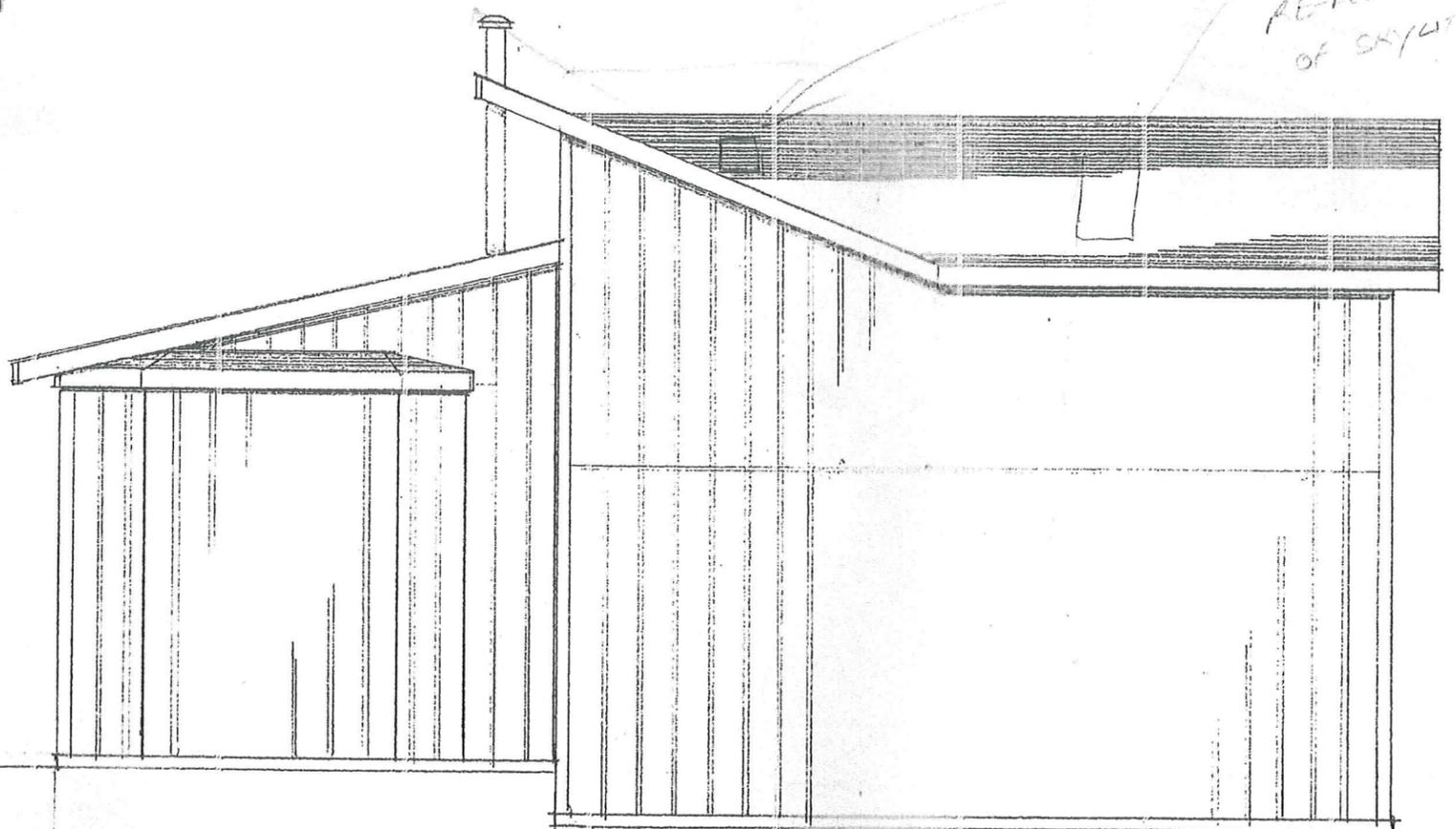
Project
ADDRESS
of SITE

4° x 2°



11'-8"
14'-0"
2'-4"

SOUTH ELEVATION



12'-0"
19'-0"
31'-0"

EAST ELEVATION

ROB HIRSCHBOECH REMODEL

147 1/2 VANNESS AVE.

SCALE: 1/4" = 1'-0"

(E) STUDIO



(E) 944 SF RESIDENCE

PROJECT ASSIGNMENTS FOR PLANNING ACTIONS

September 2017

PA-2014-00710	143 Nutley	Swink & Whitford
PA-2014-02206	485 A Street	Whitford
PA-2015-00878	35 S. Pioneer	Leonard
PA-2015-01695	399 Beach	Skibby
PA-2016-00847	252 B Street	Whitford
PA-2016-01027	276 B Street	Shostrom & Leonard
PA-2016-02103	133 Alida	Swink
PA-2016-02095	563 Rock St.	Whitford
PA-2016-02114	556 B	Von Chamier
PA-2017-00013	15, 35, 44 & 51 S. Pioneer Street	ALL
PA-2017-00235	114 Granite	Leonard
PA-2017-00325	746 C Street	Von Chamier
PA-2017-00200	165 Water	ALL
PA-2017-00707	550 E. Main	Skibby
PA-2017-00838	250 Alta Ave	Emery
PA- 2017-01054	220 Hargadine	Shostrom & Whitford
PA-2017-00969	244 Hargadine	Shostrom & Whitford
PA-2017-01279	692 B Street	Leonard
PA-2017-01294	128 Central	Emery & Swink
PA-2017-01417	228 B Street	Whitford
PA-2017-01310	981 Siskiyou Blvd	Shostrom
PA-2017-01649	84 Dewey Street	Von Chamier

October 2017



Ashland Historic Review Board Schedule Meet at 3:00pm, Lithia Room*

October 5th	Terry, Keith, Sam
October 12th	Terry
October 19th	Terry
October 26th	Terry
November 2nd	Terry

*Call 541-488-5305 to verify there are items on the agenda to review



ASHLAND HISTORIC COMMISSION Membership List

Commissioner's Name	Term Expiration	Mailing Address	Home Phone	Work Phone	E-Mail Address
Dale Shostrom Chairman	4-30-2018				shobro@jeffnet.org
Keith Swink	4-30-2018				kswink@mind.net
Sam Whitford	4-30-2018				skwhippet@mind.net
Terry Skibby	4-30-2018				terryskibby321@msn.com
Tom Giordano	4-30-2017				tomgiordanoarch@gmail.com
Bill Emery	4-30-2017				bill@ashlandhome.net
Taylor Leonard	4-30-2019				tttaylor1@yahoo.com
Piper Von Chamier	4-30-2019				piper@kencairnlandscape.com
Rich Rosenthal Council Liaison					rich@council.ashland.or.us
Maria Harris		City of Ashland Planning Dept.		552-2045	maria.harris@ashland.or.us
Regan Trapp Admin. Staff		City of Ashland Planning Dept.		552-2233	regan.trapp@ashland.or.us