

Note: Anyone wishing to speak at any Planning Commission meeting is encouraged to do so. If you wish to speak, please fill out a Speaker Request Form and place it in the Speaker Request Box by staff. You will then be allowed to speak. Please note that the public testimony may be limited by the Chair and normally is not allowed after the Public Hearing is closed.

**ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
June 11, 2019  
AGENDA**

- I. **CALL TO ORDER:** 7:00 PM, Civic Center Council Chambers, 1175 E. Main Street
  
- II. **ANNOUNCEMENTS**
  
- III. **AD-HOC COMMITTEE UPDATES**
  
- IV. **CONSENT AGENDA**
  - A. **Approval of Minutes**
    - 1. May 14, 2019 Regular Meeting
    - 2. May 28, 2019 Study Session
  
- V. **PUBLIC FORUM**
  
- VI. **TYPE II PUBLIC HEARINGS**
  - A. **PLANNING ACTION: PA-T2-2019-00009**

**SUBJECT PROPERTY:** 158, 160, 166 and 166 ½ North Laurel Street  
**OWNER/APPLICANT:** Laurel Cottages, LLC/Kim Locklin & Vadim Agakhanov  
**DESCRIPTION:** A request for Site Design Review approval to allow the construction of a 924 square foot duplex at the rear of the property, the conversion of 372 square feet of a 704 square foot garage into an apartment, and the creation of a duplex from the existing single-family residence for the property located at 158, 160, 166 and 166 ½ North Laurel Street. There are currently four units on the property including one studio; the proposal would add four units including one new unit in the existing house fronting on Laurel, a 372 square foot apartment in the garage, and two units in the duplex. The application also includes a request for a Conditional Use Permit to exceed the maximum permitted floor area (MPFA) in a historic district by 24.8 percent. (The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area.) **COMPREHENSIVE PLAN DESIGNATION:** High Density, Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 3400.
  
  - B. **PLANNING ACTION: PA-T2-2019-00008**

**SUBJECT PROPERTY:** Map 39 1E 11C Tax Lot #2504 on Engle St/Map 39 1E 11C Tax Lot #2505 on Villard St.  
**OWNER/APPLICANT:** Housing Authority of Jackson County/HAJC Development/Dan Horton, Architect  
**DESCRIPTION:** A request for Site Design Review approval to allow the construction of a for 60-unit multi-family development on two tax lots (#2504 & #2505) along Villard

**CITY OF  
ASHLAND**



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Community Development office at 541-488-5305 (TTY phone is 1-800-735-2900). Notification 48 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1).

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**and Engle Streets as Phase II of the existing 'Snowberry Brook' development. The proposal consists of four two-story eight-plex apartment buildings and seven two-story townhouse four-plexes. Units will consist of ten one-bedroom flats, 12 two-bedroom flats, ten three-bedroom flats, and 28 two-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar (cedrus deodara) which the project arborist describes as posing a hazard. COMPREHENSIVE PLAN DESIGNATION: Low Density, Multi-Family Residential; ZONING: R-2; ASSESSOR'S MAP: 39 1E 11C; TAX LOT: 2504 & 2505.**

**VII. ADJOURNMENT**



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**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
REGULAR MEETING  
**MINUTES - Draft**  
March 14, 2019

**CALL TO ORDER**

Chair Roger Pearce called the meeting to order at 7:04 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Troy Brown, Jr.  
Michael Dawkins  
Alan Harper  
Melanie Mindlin  
Haywood Norton  
Lynn Thompson

**Staff Present:**

Maria Harris, Planning Manager  
Dana Smith, Executive Assistant

**Absent Members:**

Roger Pearce

**Council Liaison:**

Stefani Seffinger, absent

**ANNOUNCEMENTS**

Planning Manager Maria Harris explained the Public Hearing for the Housing Element would go before City Council at their meeting June 4, 2019. The Commission would discuss the election of officers at their meeting May 28, 2019.

**AD-HOC COMMITTEE UPDATES** - None

**CONSENT AGENDA**

**1. Approval of Minutes**

1. March 12, 2019 Regular Meeting
2. March 26, Study Session
3. April 23, 2019 Joint Study Session

**Commissioners Thompson/Harper m/s to approve the minutes of March 12, 2019 and March 26, 2019.**

Commissioner Brown abstained from the vote regarding the minutes of March 12, 2019. **Voice Vote: all AYES. Motion passed.**

Commissioner Thompson offered the following changes to the minutes of April 23, 2019:

- Page 2, last set of bullet points, change the first bullet under **“Other items the Commissions discussed were,”** from **“Public opposition to multi-family rentals,”** to **“Public opposition to high density developments in developed Ashland neighborhoods, particularly parking and traffic impacts.”**
- Page 2, add the following two statements after the second set of bullet points and before the third set and **“Other items the Commissions discussed were:”**
  - “Regarding recommendations to increase allowed density and consider a tax abatement program, the ECONorthwest analysis indicated that such changes would not produce the “rental mix” housing that was highly desired, particularly for families. Rather, it appeared that they would incentivize small units, less than 500 square feet and rent-by-the-bedroom larger units which were what the Planning Commission was currently seeing. Since this is occurring**

anyway, the incremental value of changes in zoning and tax abatements needed to be considered carefully in relation to the impacts and costs of incentivizing such developments.”

And

“The study appeared to indicate that it was not so much a shortage of units as the cost of multi-family rental units that was a problem.”

- Page 3, under “Overall, both Commissions supported the recommendations made by ECONorthwest with several comments concerned with the tax abatement program. Additional comments included,” delete the third bullet, “That the only way to increase housing was either increase density or expand boundaries.”
- Page 3, change the seventh bullet under “Overall, both Commissions supported the recommendations made by ECONorthwest with several comments concerned with the tax abatement program. Additional comments included,” from “Concern on decreasing multi-family parking,” to “Concerns expressed about recommendation to decrease parking requirements for multi-family housing.”
- Page 3, under “Overall, both Commissions supported the recommendations made by ECONorthwest with several comments concerned with the tax abatement program. Additional comments included,” add to list, “Policies that incentivized land annexation within the UGB may be a desirable way to add multi-family including possibly affordable housing.”
- Page 3, “Overall, both Commissions supported the recommendations made by ECONorthwest with several comments concerned with the tax abatement program. Additional comments included,” change to, “Overall, both Commissions supported the recommendations made by ECONorthwest with several comments concerned with the tax abatement program. Commissioners were invited to offer their reactions and the following were expressed by individual Commissioners.”

The Commission discussed the proposed changes.

Commissioners Thompson/Harper m/s to approve the minutes of April 23, 2019, as amended with a further amendment to add a sentence before the bullet points on page 3 (following Overall, both Commissions...) “Commissioners were invited to offer their reactions and the following were expressed by individual Commissioners.” Voice Vote: Commissioners Dawkins, Harper, Norton and Thompson, YES; Commissioners Mindlin and Brown, NO. Motion passed 4-2.

## **PUBLIC FORUM**

Huelz Gutcheon/Ashland/Commented on meeting minutes, climate change and the lack of affordable housing in Ashland.

## **UPDATES**

### **A. Climate and Energy Action Plan (CEAP) Update by Stu Green, Climate and Energy Analyst**

Climate and Energy Analyst Stu Green provided a presentation (attached) on the Climate and Energy Action Plan (CEAP). A slide from 1982 projected greenhouse gas emissions would exceed 400 parts per million carbon dioxide in the atmosphere by 2019 and it had. There were nine actions ascribed to the Community Development Department in the CEAP. The presentation included:

#### **Ashland’s Climate Vision**

- CEAP Timeline.
- Projected Climate Impacts.
- Mitigation and Adaptation Goals.
- Climate Recovery Ordinance for Community and City Operations.
- Climate Goals.
- Status of adopted climate actions.

- Are we making progress?
- Accomplishments.
- Opportunities.

Hydro power was not considered renewable energy. Projections indicated that by 2030 all new energy would be solar power. It was more expensive to purchase a unit of electricity than one unit of natural gas. The City was developing an electrification program but it was budget cycle driven. The master plan for the electric utility would contain a climate policy advisory.

The Commission discussed passive solar in building requirements with Mr. Green. Ms. Harris explained how building specific requirements in the City's code could possibly violate the maximum building code in the International Building Code. Land Use code could not require more than the state required.

Mr. Green would forward the resources listed in the presentation to Ms. Harris.

The Commission suggested sharing information and providing specifics on steps the public could take to minimize their personal impact on the environment. The City could provide residents with multiple categories and a range of actions they could take to make a difference. Additionally, they could create action items that would become cultural norms.

Mr. Green addressed if infill density contributed to the heat island effect. He explained everything was a tradeoff and provided examples. He also commented on eco system restoration versus carbon reduction. Planting more plants would cut down on carbon.

Another Commission suggestion was having the new climate commission incorporate subsidies or incentives for developments that addressed climate items in the CEAP. The Planning Commission could work on zoning to accommodate development.

The Commission liked the report. Ms. Harris noted the Cottage Housing/ARU Ordinance, the Development Standards for Wildfire Lands Ordinance, and the Transit Triangle Overlay Ordinance all addressed action items in the CEAP.

Mr. Green clarified the state did not mandate the Climate and Energy Action Plan.

## **OTHER BUSINESS**

### **A. Technical Advisory Committee Member – “Revitalize Downtown Ashland”**

The Commission agreed that Commissioner Dawkins would participate on the Committee.

### **B. Finalize Planning Commission Retreat Date**

The Commission confirmed the retreat date as June 10, 2019, from 9:30 a.m. to 1:30 p.m.

## **PUBLIC HEARINGS** - None

## **ADJOURNMENT**

Meeting adjourned at 8:45 p.m.

*Submitted by,  
Dana Smith, Executive Assistant*

# Climate and Energy Action Plan

...

Planning Commission Update 2019



## Outline

### Part 1

- Timeline
- Projected climate impacts
- Our goals
- Action status

### Part 2

- Are we making progress?
- Accomplishments
- Opportunities

## Timeline

- 2011 - Regional Renewable Energy Assessment
- 2015 - Climate and Energy Action Planning ad hoc
- 2016 - Greenhouse Gas Inventory (2011-2015)
  - Public Engagement



## Timeline (cont.)

- 2017 - Adopted Climate and Energy Action Plan
  - Climate Recovery Ordinance
- 2018 - CEAP Implementation ad hoc Committee
  - CEAP Progress Report



## Timeline (cont.)

- 2019 - CEAP Progress Report  
- (Climate Policy Advisory Commission)  
- (Greenhouse Gas Inventory update)

- 2020 - (Progress Report)  
- (Climate and Energy Action Plan review)



## Why does the CEAP exist?

To support Ashland's Climate Vision

..for 2050 is to be a resilient community that has zero net greenhouse gas emissions, embraces equity, protects healthy ecosystems, and creates opportunities for future generations.



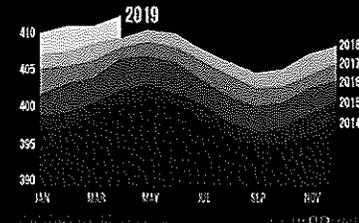
## Climate Impacts



Source: [www.ClimateCentral.org](http://www.ClimateCentral.org)

## Climate Impacts

A NEW HIGH FOR CO<sub>2</sub>  
Monthly Concentration in Parts Per Million

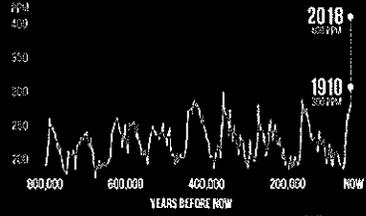


Source: [www.ClimateCentral.org](http://www.ClimateCentral.org)

## Climate Impacts

### CHANGING OUR ATMOSPHERE

800,000 Years of Carbon Dioxide

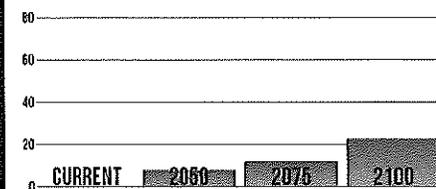


Source: www.ClimateCentral.org

## Climate Impacts

### Ashland, OR

Days above 100°F



If current emission trends continue  
Ashland will have 23 more days above  
100°F by 2100

Source:  
www.Climate  
Central.org

## Climate Impacts

### WESTERN DROUGHT

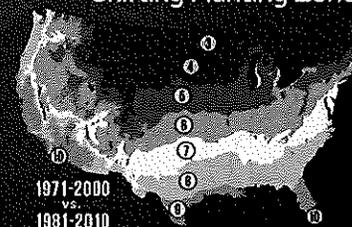
Palmer Hydrological Drought Index



Source: www.ClimateCentral.org

## Climate Impacts

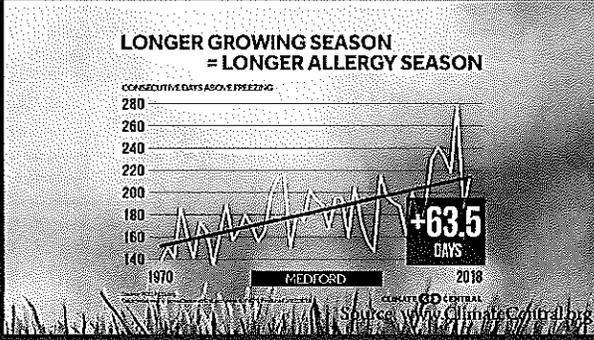
### Shifting Planting Zones



1971-2000  
vs.  
1981-2010

Source: www.ClimateCentral.org

## Climate Impacts



How can we take action on climate change?

How can we take action on climate change?

1.  
Stop making  
it worse

2.  
Get  
ready

How can we take action on climate change?

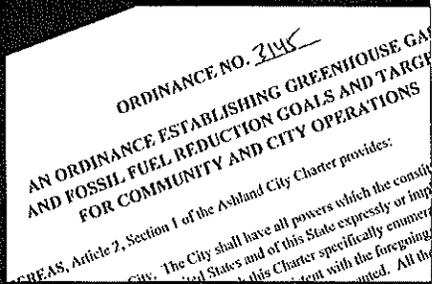
1.  
Stop making  
it worse

(our goals)

2.  
Get  
ready

## Climate Recovery Ordinance

- Creates "AMC 9.40 Climate Recovery"
- Establishes reduction targets for Community and City Operations per current CEAP



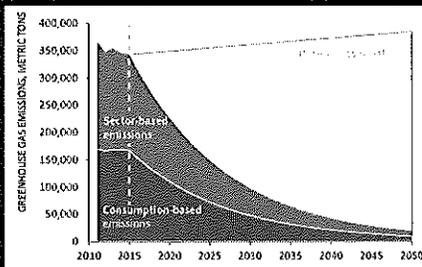
## What are current adopted CEAP goals?

1. Mitigate GHG
  - Community
  - City Operations
2. Adapt for a changing climate

## Goal: Mitigate Community GHG

**-8%**

Average reduction, every year until 2050



## Goal: Mitigate City Operations GHG

Carbon Neutral Operations

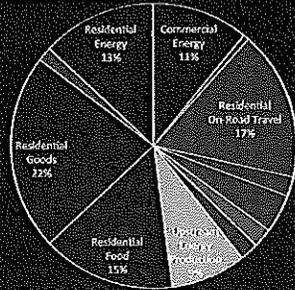
**2030**

Fossil reductions

**50%**  
by 2030

**100%**  
by 2050

## Where do Ashland's GHG come from?



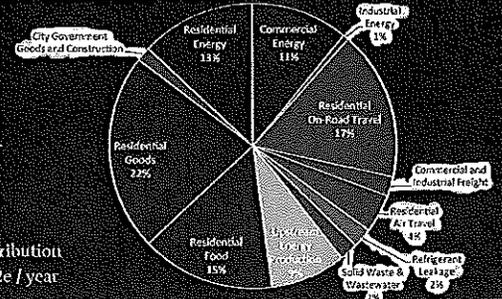
Source: Ashland GHG Inventory 2015

## Where do Ashland's GHG come from?

- 22% Goods
- 17% Driving
- 15% Food
- 13% Home energy
- 4.5% Upstream energy\*
- 4% Air Travel
- 1% Waste\*

76.5% Residential Contribution  
 => 275,000 MT CO<sub>2</sub>e / year

\*for this exercise half of Waste and Energy Production are counted as Residential

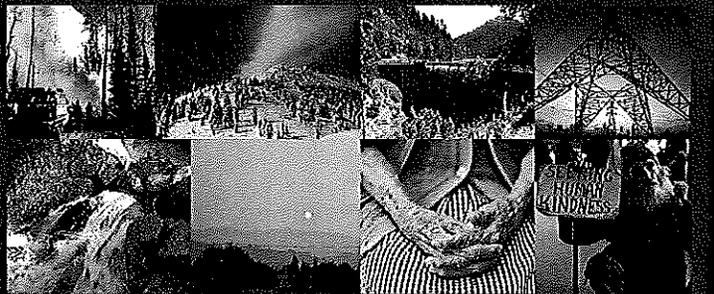


## Goal: Adapt Community

Prepare the city's communities, systems, and resources to be more resilient to climate change impacts.



## Vulnerabilities



**Goal: Adapt Community**

Ecosystems  
Infrastructure  
People & Neighborhoods  
Businesses

**Goal: Adapt Community**

Ecosystems  
Infrastructure  
People & Neighborhoods  
Businesses

**Resilience**  
How prepared are we?  
How quickly can we react?  
How long to get back to normal?

**Status of adopted climate actions**

Status of All CEAP Actions  
2017-2022

Status	Percentage
Complete	14%
In Progress	24%
Not Started	24%
Multiple Commitments	38%

**Are we achieving our climate recovery goals?**

1. It's too soon to assess CEAP impacts in detail.
2. There are some indications of progress.
3. We are not keeping pace with our stated goals.

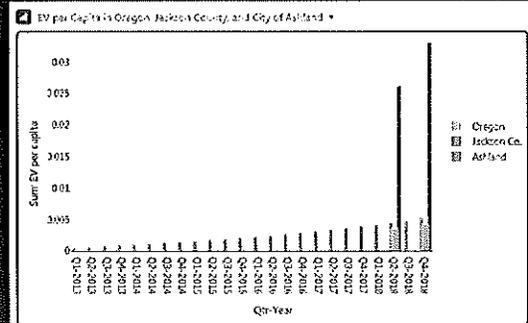
## 1. Too soon to accurately assess impact of CEAP on GHG Emissions

Revised GHG inventories will:

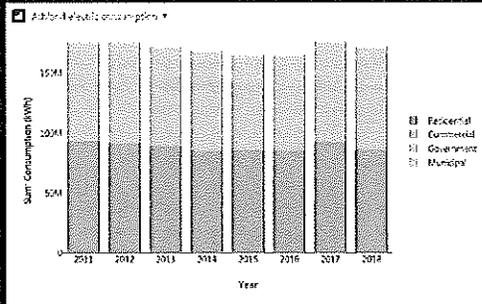
- Be available later in 2019
- Provide updated information and context
- Still contain modeled and uncertain data



## 2. There are indications of progress - EV adoption



## 2. There are indications of progress - Electricity

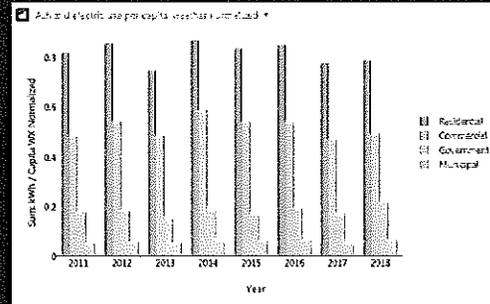


2018 Per capita electricity use increased 1.4% compared to 2015.

Normalizing for population and weather electricity use decreased 2.2%



## 2. There are indications of progress - Electricity

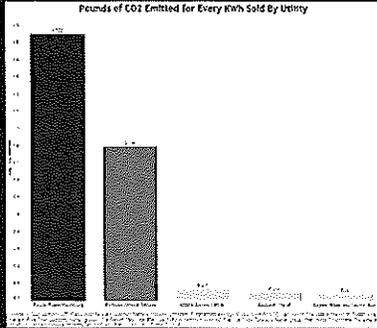


2018 Per capita electricity use increased 1.4% compared to 2015.

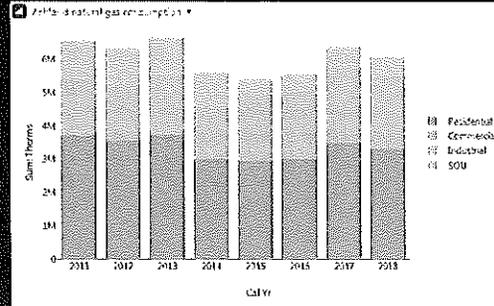
Normalizing for population and weather electricity use decreased 2.2%



## 2. There are indications of progress - Electricity



## 3. Not making enough progress - Natural Gas

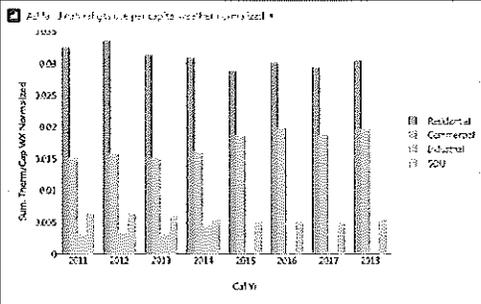


2018 per capita natural gas use increased 9.6% compared to 2015.

Normalizing for population and weather, natural gas use increased 5.8%.



## 3. Not making enough progress - Natural Gas



2018 per capita natural gas use increased 9.6% compared to 2015.

Normalizing for population and weather, natural gas use increased 5.8%.



## Climate Plan Focus Areas

- Building energy use
- Consumption and materials
- Urban form, land use, and transportation
- Natural systems
- Public health, safety, and wellbeing
- Cross-cutting



## High Impact Opportunities

1. Build on existing CEAP
2. Plan for future of Electric Utility
3. Electrify home heating and cooling
4. Electrify transportation, increase mobility options
5. Reduce consumption



## Accomplishments: Making buildings more efficient

- Home Energy Assessment tool ([ashland.eeaudit.com](http://ashland.eeaudit.com))
- City of Ashland Solar Program
- City of Ashland Energy Efficiency Program



## Accomplishments: Planning for the future

- Fossil-free Municipal Facilities and Fleet
- Cottage housing / ARU ordinance
- Transit Triangle / Transit Expansion



## Accomplishments: Improving transportation

- Rogue Bike Share
- Creating EV infrastructure (public chargers)
- Supporting EV adoption (Empower EV, EVA)



## Accomplishments: Making Ecosystems Healthy

- Ashland Forest Resiliency Project
- Saving Water
- Reducing Disposables



## Accomplishments: Protecting Ashland

- Wildfire Ordinance
- Staying informed - Nixle
- CERT / Firewise



## Essential Climate Planning work to be done

Overall:

- GHG emissions reduction by electrification and infill density
- Integrate climate in all Comp Plan Elements
- Special attention to adaptation



## Essential Climate Planning work to be done

For all buildings:

- Emphasize high efficiency
- Adequate electric service capacity
- Require EV/PV ready
- Consider building energy score program



## Essential Climate Planning work to be done

For all transportation:

- Emphasize pedestrian and non-car modes
- Encourage shared, non-fossil options
- Encourage electrification of parking facilities
- Minimize parking requirements



## Urban Form, Land Use, Transportation: Strategies

1. Support better public transit and ridesharing.
2. Make Ashland more bike- and pedestrian-friendly.
3. Support more-efficient vehicles.
4. Support more climate-ready development and land use.



## Planning Commission connection to Climate Plan

- BE-1-2. Promote low-carbon fuels.
- BE-1-3. Facilitate solar energy production.
- BE-2-1. Promote climate-friendly building & construction.
- BE-2-2. Require building energy scores.
- BE-2-4. Minimum energy efficiency standards for affordable housing program.
- BE-5-1. Encourage heat-tolerant building approaches such as cool roofs and passive cooling.



## Planning Commission connection to Climate Plan

- ULT-1-1. Regional coordination of transit, carpooling, and car-sharing.
- ULT-1-3. Develop near transit hubs without displacing disadvantaged populations.
- ULT-1-4. Evaluate expanded local transit options.
- ULT-2-1. Bicycle- and pedestrian-friendly actions in Transportation System and Downtown Parking Plans.
- ULT-2-2. Develop multimodal shared streets.



### Planning Commission connection to Climate Plan

ULT-3-2. EV charging infrastructure at multifamily and commercial developments.

ULT-4-1. Regulate new development in the Wildfire Lands Overlay part of the urban growth boundary.

ULT-4-2. Further revise community development plans to favor walkable neighborhoods and infill density.

ULT-4-3. Modify the WUI code to include construction techniques appropriate for wildfire-prone areas.



### Planning Commission connection to Climate Plan

CC-3-2. Incorporate CEAP goals and actions in future updates of city plans.

CC-3-3. Include climate action goals within the scope of City Advisory Commissions.

CM-2-2. Expand urban agriculture opportunities.

CM-3-3. Strengthen the Demolition Debris and Diversion ordinance for compliance and impact.



### Planning Commission connection to Climate Plan

NS-1-2. Use green infrastructure to reduce flood risk and minimize sediment transport.

PHSW-1-1. Promote the expansion of tree canopy in urban heat islands or areas that need air conditioning such as schools.

PHSW-3-2. Minimize potential urban heat impacts.

PHSW-4-2. Address populations and essential City services that are within the 100-year flood zone.



### A feasible scenario for 2030

- All new energy will be solar
- All new vehicles will be electric and self-driving
- New car market will shrink by 80%
- Up to 80% of parking spaces will be unnecessary
- Individual car ownership will be obsolete



## Selected resources of interest

- “Zoning Practice: Climate Adaptation”, Vol. 34 No. 2 by Megan Susman, American Planning Association, Feb 2017
- “Planning for Solar Energy”, report #575 by David Morley, American Planning Association
- Congress for the New Urbanism (CNU.org)



## Q&A

Thank you!

Stu Green

Climate and Energy Analyst

[stu.green@ashland.or.us](mailto:stu.green@ashland.or.us)



**CITY OF  
ASHLAND**  
ASHLAND PLANNING COMMISSION  
STUDY SESSION  
MINUTES  
May 28, 2019

**CALL TO ORDER**

Chair Roger Pearce called the meeting to order at 7:00 p.m. in the Civic Center Council Chambers, 1175 East Main Street.

**Commissioners Present:**

Troy Brown, Jr.  
Michael Dawkins  
Alan Harper  
Melanie Mindlin  
Haywood Norton  
Roger Pearce  
Lynn Thompson

**Staff Present:**

Bill Molnar, Community Development Director  
Maria Harris, Planning Manager  
April Lucas, Development Services Coordinator

**Absent Members:**

None

**Council Liaison:**

Stefani Seffinger, absent

**ANNOUNCEMENTS**

Community Development Director Bill Molnar announced next Tuesday, June 4, 2019 the City Council will hold a public hearing and first reading of the Housing Element ordinance. He also noted the Planning Commission is scheduled to hold two land use hearings at their June 25 meeting.

**PUBLIC FORUM**

No one came forward to speak.

**SPECIAL PRESENTATION**

**A. Trail Master Plan Update by Michael Black, Director of Ashland Parks & Recreation.**

Michael Black explained the Trails Master Plan amendment was developed by staff and citizens and noted the Master Plan has not been updated for 10 years. He stated the updated plan turned out very nice and noted he would be returning at an upcoming Planning Commission meeting for the public hearing and commission recommendation.

Mr. Black provided a short presentation that addressed the plan background, goals, themes, trail sections, and recommendations (see attachment). He emphasized the Transportation System Plan is the guiding document for the city and this plan is intended to advise that work. Mr. Black commented on maintaining and developing new trails and how the Parks & Recreation Department collaborates with private citizens and developments. He acknowledged in some areas of town people have created their own trails with no easements and stated land owners have the right to exclude people from their property. He stated in some cases the city has successfully negotiated agreements with the property owners, but not every instance turns out this way. Mr. Black explained unless a trail is circular, there is potential for it to be considered as a transportation option. He also commented on the key topics that arose at the public input sessions, which included: pet issues, trail traffic and homeowners' privacy, negative impacts on wildlife and wildlife habitat, and trash left by trail users.

Mr. Black explained the Master Plan addresses each trail as well as regional connections. His presentation highlighted the Bear Creek Greenway and outlined the interim, short term, and permanent alignment options.

#### **Commissioner Questions and Comments**

Mr. Black was asked to explain the process for approving trail plans. He responded that it depends on where the trail is located and noted for the Bear Creek Greenway staff presented the proposal to the city's Transportation Commission and from there it went to the Parks & Recreation Commission and the Planning Commission with final approval issued by the City Council.

Mr. Black was asked to elaborate on the ADA requirements and whether designating trails as a transportation route increased those requirements. He explained natural trails would not be paved over, however ADA crossings would be needed wherever a trail comes in contact with a roadway intersection or mid-block crossing. He clarified not all trails would be designated as transportation routes and stated only the Bear Creek Greenway and the Central Bike Path currently hold this designation.

Mr. Molnar asked about the Hamilton Creek Corridor on the Croman Mill property and whether alternatives to asphalt could be used when it is developed. Mr. Black explained eventually the city will have a north to south transportation route and while pavement is preferred in urban areas, decomposed granite and other materials could be considered.

Comment was made suggesting the plan clearly indicate that some of the trails are outside of the city's jurisdiction and that we will be working cooperatively with Jackson County on those areas.

#### **OTHER BUSINESS**

##### **A. Election of Officers.**

Commissioner Pence explained the municipal code limits officer terms to 3 years, however the commission has generally opted for 2-year terms. General discussion was had on this code section and suggestions were made for potential vice chair and chair nominations.

**Commissioners Dawkins/Harper m/s for Roger Pearce to serve as commissioner chair and Haywood Norton to serve as vice chair for the upcoming year. Voice Vote: all AYES. Motion passed 7-0.**

#### **ADJOURNMENT**

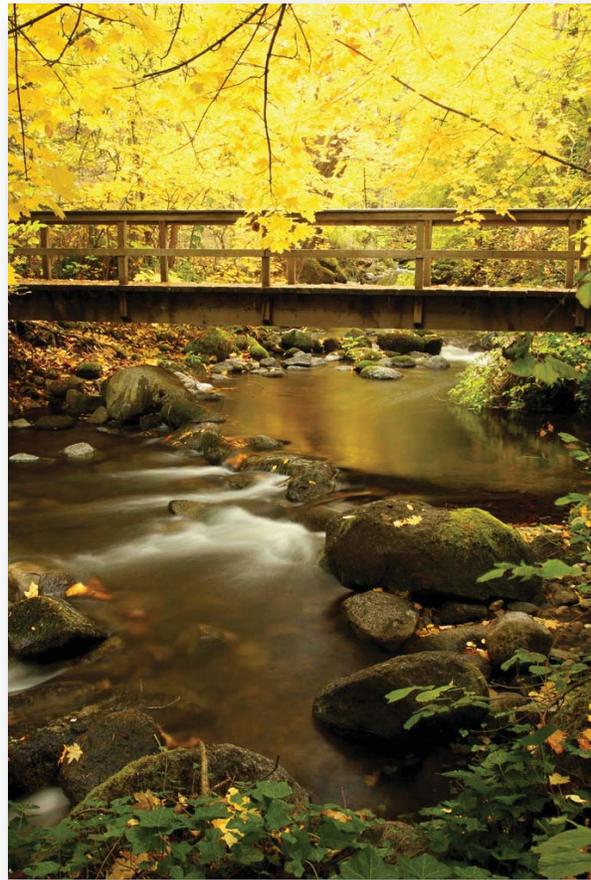
Meeting adjourned at 7:40 p.m.

*Submitted by,  
April Lucas, Development Services Coordinator*

# Trails Master Plan Amendment

May 28, 2019

Michael A. Black, AICP



© Jeffrey McFarland

## Plan Background

- A Technical document meant to advise
- Provided to give consideration to trails in the development process
- Meant to encourage collaboration
- Stresses context in trails
- Emphasizes consistency with other plans



# Master Plan Goals

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- Develop and maintain a system of trails
- Increase trail connectivity
- Enhance the quality of life
- Continue use of accepted trail standards



## Vision

- *Trails connect people to people, people to nature, people to our community and to our region. These vital connections and values are key to the continuing success of our trail system.*

# Trails Master Plan Themes

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- Trails as Transportation
- Trail Access and Connectivity
- Natural Environment and Citizen Rights
- Trail Planning
- Trail Management and Maintenance
- Partners

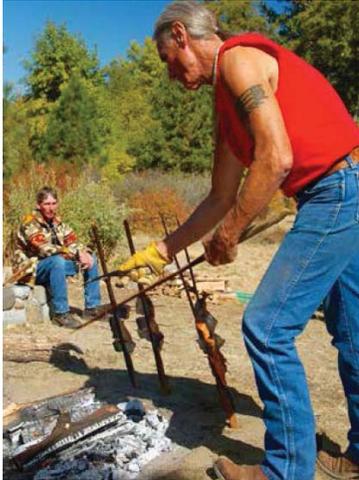
Public Input  
5/2/2018 &  
Public Comment Link

## Public Input:

- Pet issues
- Trail traffic and nearby homeowners' privacy
- Negative impact on wildlife and wildlife habitat within trail corridors;
- Trash left by trail users;
- Interest in a citizen trails volunteer program.

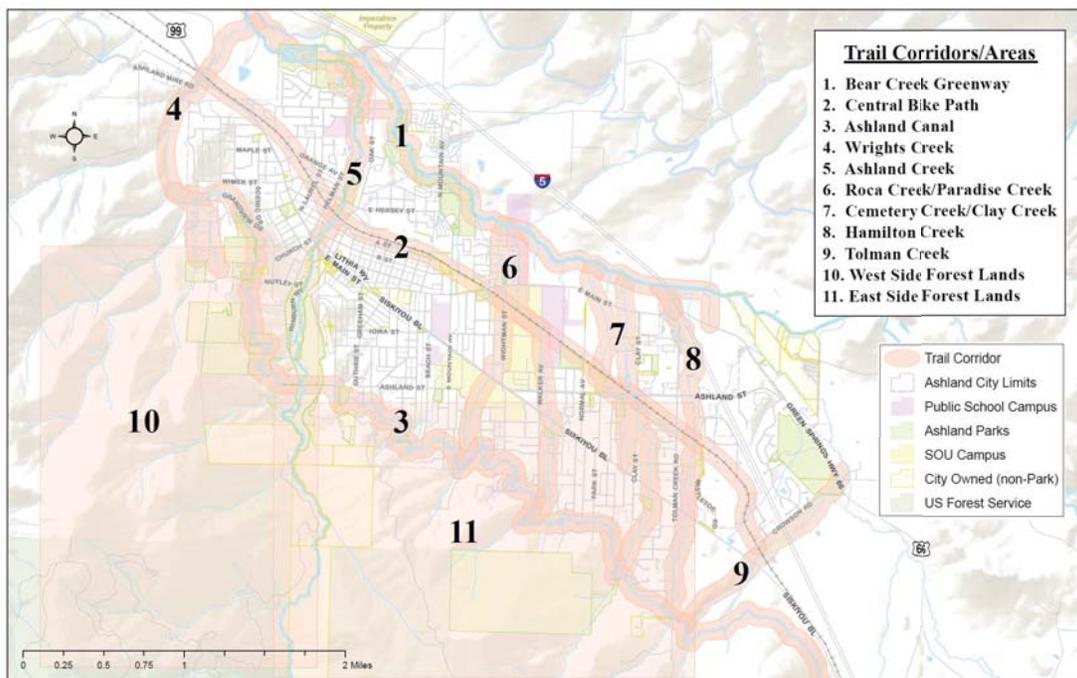


# Chapters

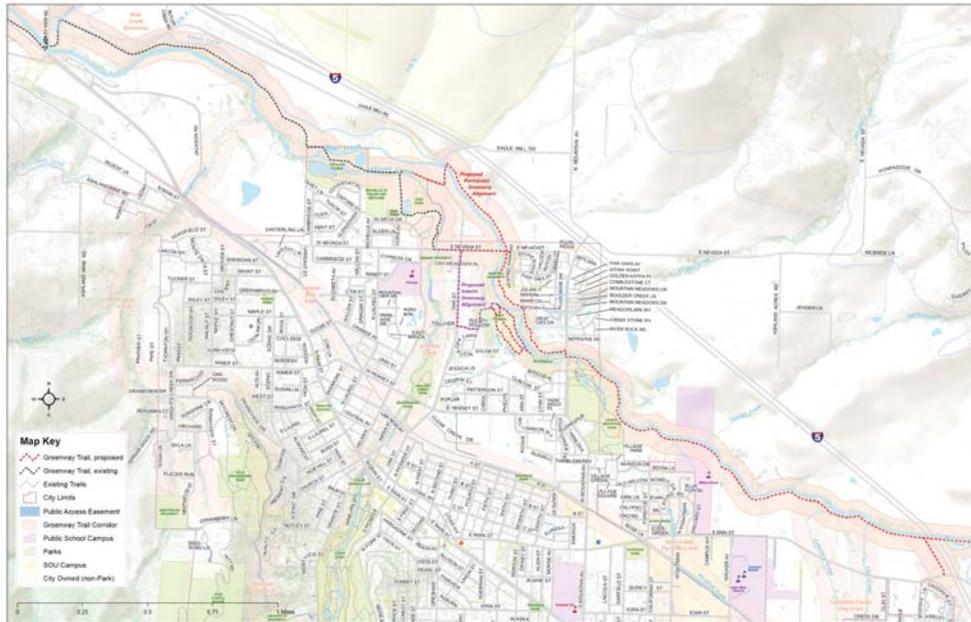


- Trail Standards and Basic Design Elements
- Trail Safety and Protocol
- Indigenous Peoples
- Geology
- Flora and Fauna

## Trails Overview

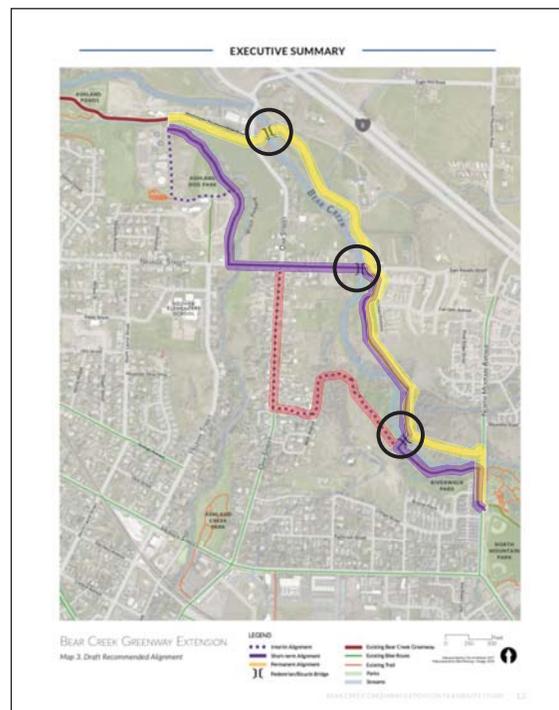


# Trail Sections – Bear Creek Greenway



## BCG Recommendations

- **Permanent Alignment**
  - Highest quality experience, follows most direct route
  - Requires new bridge, private property acquisition
- **Short Term Alignment**
  - Property already in public ownership
- **Interim Alignment**
  - Provides high quality alternative routes
  - Not recommended as part of BCG



**END**

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**PLANNING ACTION:** PA-T2-2019-00009

**SUBJECT PROPERTY:** 158, 160, 166 and 166 ½ North Laurel Street

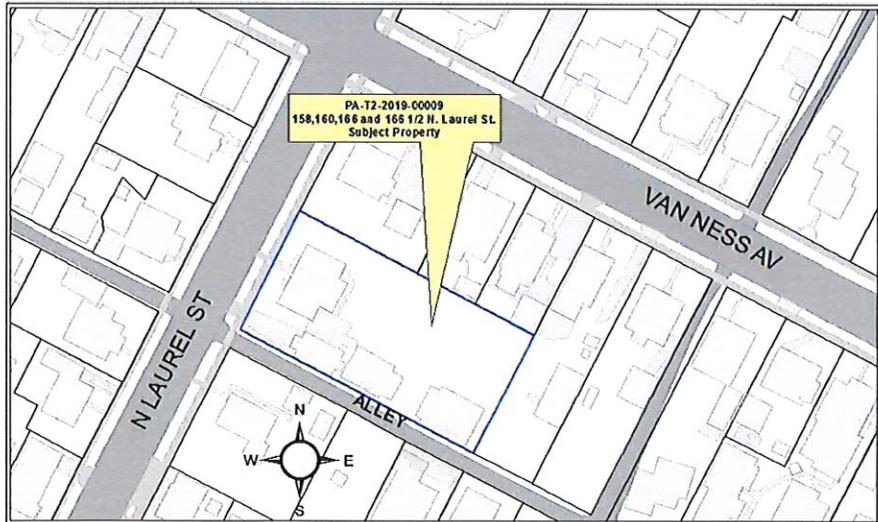
**OWNER/APPLICANT:** Laurel Cottages, LLC/Kim Locklin & Vadim Agakhanov

**DESCRIPTION:** A request for Site Design Review approval to allow the construction of a 924 square foot duplex at the rear of the property, the conversion of 372 square feet of a 704 square foot garage into an apartment, and the creation of a duplex from the existing single-family residence for the property located at 158, 160, 166 and 166 ½ North Laurel Street. There are currently four units on the property including one studio; the proposal would add four units including one new unit in the existing house fronting on Laurel, a 372 square foot apartment in the garage, and two units in the duplex. The application also includes a request for a Conditional Use Permit to exceed the maximum permitted floor area (MPFA) in a historic district by 24.8 percent. (The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area.)

**COMPREHENSIVE PLAN DESIGNATION:** High Density, Multi-Family Residential; **ZONING:** R-3; **ASSESSOR'S MAP:** 39 1E 04CC; **TAX LOT:** 3400.

**NOTE:** The Ashland Historic Commission will also review this Planning Action on **Wednesday, June 5, 2019 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way. The Ashland Tree Commission will also review this Planning Action on **Thursday, June 6, 2019 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** *Tuesday, June 11, 2019 at 7:00 PM, Ashland Civic Center, 1175 East Main St.*



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact Senior Planner Derek Severson at 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### 18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. Underlying Zone: The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. Overlay Zones: The proposal complies with applicable overlay zone requirements (part 18.3).
- C. Site Development and Design Standards: The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. City Facilities: The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

## CONDITIONAL USE PERMITS

### 18.5.4.050.A

A Conditional Use Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.
2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.
3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.
  - a. Similarity in scale, bulk, and coverage.
  - b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.
  - c. Architectural compatibility with the impact area.
  - d. Air quality, including the generation of dust, odors, or other environmental pollutants.
  - e. Generation of noise, light, and glare.
  - f. The development of adjacent properties as envisioned in the Comprehensive Plan.
  - g. Other factors found to be relevant by the approval authority for review of the proposed use.
4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.
5. For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.
  - a. WR and RR. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - b. R-1. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - c. R-2 and R-3. Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.
  - d. C-1. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
  - e. C-1-D. The general retail commercial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 1.00 gross floor to area ratio, complying with all ordinance requirements.
  - f. E-1. The general office uses listed in chapter 18.2.2 Base Zones and Allowed Uses, developed at an intensity of 0.35 floor to area ratio, complying with all ordinance requirements; and within the Detailed Site Review overlay, at an intensity of 0.50 floor to area ratio, complying with all ordinance requirements.
  - g. M-1. The general light industrial uses listed in chapter 18.2.2 Base Zones and Allowed Uses, complying with all ordinance requirements.
  - h. CM-C1. The general light industrial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.50 gross floor to area ratio, complying with all ordinance requirements.
  - i. CM-OE and CM-MU. The general office uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area, complying with all ordinance requirements.
  - k. CM-NC. The retail commercial uses listed in chapter 18.3.2 Croman Mill District, developed at an intensity of 0.60 gross floor to area ratio, complying with all ordinance requirements.
  - l. HC, NM, and SOU. The permitted uses listed in chapters 18.3.3 Health Care Services, 18.3.5 North Mountain Neighborhood, and 18.3.6 Southern Oregon University District, respectively, complying with all ordinance requirements.

**Dana Smith**

---

**Subject:** PA-T2-2019-00009

**From:** Regina Ayars <[reginariley@jeffnet.org](mailto:reginariley@jeffnet.org)>  
**Sent:** Tuesday, June 4, 2019 4:18 PM  
**To:** [avram.biondo@ashland.or.us](mailto:avram.biondo@ashland.or.us); [pearcer22@gmail.com](mailto:pearcer22@gmail.com)  
**Cc:** Regina Ayars <[reginariley@jeffnet.org](mailto:reginariley@jeffnet.org)>  
**Subject:** PA-T2-2019-00009

Attachment available until Jul 4, 2019

Click to Download

IMG\_3822.m4v  
54.8 MB

Avram and Roger

I plan to attend the Planning Commission meeting 6/11/19 to testify for the above mentioned Planning Action. I own property at 164 1/2 Central almost directly across the alley from the existing buildings and the land behind where the new units would be built. I am not opposed to this project nor was I opposed to the project that has yet to be completed at the other end of the alley. We need rental units in town.

I do have several concerns:

1. Huge potholes on gravel alley made my heavy trucks during construction.

Who is responsible for fixing these holes- the city or the construction company? There are always potholes on this alley whether there is construction or not. The city has reluctantly fixed the alley several times but only after I have repeatedly called them. The project approved last year and not yet completed that I referred to above has created huge potholes at the Helman end of the alley. Before this project is approved for occupancy, I am hoping that the potholes will be fixed.

My concern is that with this new project we are going to have the same problem at the Laurel end of the alley. I want the city or the contractor to agree to fix the damage done to the gravel alley.

2. Parking

As you can imagine parking on a single lane gravel alley can be a challenge. I do not know how many parking spaces were approved for the 2018 project but I am assuming that those cars will have off street parking. There is a multi unit complex across the alley and they appear to have sufficient off street parking.

My question will be how much off street parking will PA-T2-2019-00009 have?

3. Traffic Flow

Right now traffic moves both directions and people just pull over to let each other pass. After both these projects are complete there will be over a dozen new units (not sure about this number but at least 10). Should this alley be made one way?

Thank you for your consideration of my questions and concerns

Regina Ayars  
Ashland



# Laurel Cottages - 158-166 ½ N. Laurel St.

**PA-T2-2019-00009** – A request for Site Design Review approval to allow the construction of a 924 square foot duplex at the rear of the property, the conversion of 372 square feet of a 704 square foot garage into an apartment, and the creation of a duplex from the existing single-family residence for the property located at 158, 160, 166 and 166 ½ North Laurel Street. There are currently four units on the property including one studio; the proposal would add four units including one new unit in the existing house fronting on Laurel, a 372 square foot apartment in the garage, and two units in the duplex. The application also includes a request for a Conditional Use Permit to exceed the maximum permitted floor area (MPFA) in a historic district by 24.8 percent. (The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area.)



## Proposal Details

### Site Description

The subject property is an approximately 20,000 square foot, rectangular lot located on the east side of North Laurel Street, between Central and Van Ness Streets. The property is zoned R-3 (High-Density, Multi-Family Residential) and nearby properties are zoned R-2 and R-3. The subject property is located within the Skidmore Academy Historic District, and is considered to be in the district survey document. The site currently contains four residential dwelling units including a 1,060 square foot 3-bedroom residence at 158 North Laurel Street, a 704 square foot 2-bedroom apartment over the garage at 160 North Laurel Street, a 1,534 square foot 3-bedroom residence at 166 North Laurel Street, and a 465 square foot studio unit over the garage at 166½ North Laurel Street.

### Landscaping & Trees

There are two trees on the property: a 24-inch Oak in the front yard of 166 North Laurel, and an 18-inch Maple in the courtyard/patio area between 166 and 166 ½ North Laurel. Both trees are proposed to be preserved and protected, and the application includes a Tree Protection Plan illustrating measures proposed to protect the two existing trees and a Landscape Plan detailing proposed new plantings.

### Proposed Density

The subject property is zoned R-3, which allows a base density of 20 dwelling units per acre. The property is 20,000 square feet in area or 0.459 acres, and as such has a base density of 9.18 units. The proposal would add four new units to the four existing units. One of the existing units and three of the four new units are less than 500 square feet in gross habitable floor area and are considered to be ¾ -units for density purposes, so the proposed density is 7.0 units which is less than the allowed base density. *(Because the property is within a historic district, it is not subject to a minimum density requirement.)*

# Laurel Cottages

**PA-T2-2019-00009** – A request for Site Design Review approval to allow the construction of a 924 square foot duplex at the rear of the property, the conversion of 372 square feet of a 704 square foot garage into an apartment, and the creation of a duplex from the existing single-family residence for the property located at 158, 160, 166 and 166 ½ North Laurel Street. There are currently four units on the property including one studio; the proposal would add four units including one new unit in the existing house fronting on Laurel, a 372 square foot apartment in the garage, and two units in the duplex. The application also includes a request for a Conditional Use Permit to exceed the maximum permitted floor area (MPFA) in a historic district by 24.8 percent. (The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area.)

## Key Issues

### Maximum Permitted Floor Area (MPFA)

Within the historic district, each property has a Maximum Permitted Floor Area (**MPFA**) based on the lot size and number of units proposed. Applicants may request to exceed the MPFA by up to 25 percent with a Conditional Use Permit (**CUP**). The application here includes a request for a CUP to exceed the MPFA by 24.8 percent. The existing development already exceeds the Maximum Permitted Floor Area, and the additional proposed overage here is limited to the addition of a 924 square foot duplex which is to provide two small rental units. *(The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area.)*

### Parking

The applicant has provided calculations showing that a total of 11 off-street parking spaces are required for the proposal. Two spaces are to be provided in the existing garage, one in the existing driveway off of North Laurel, one in a covered carport at 160 North Laurel, and seven spaces are to be provided in a new proposed surface parking lot accessed from the alley. No on-street parking credits are requested. Bicycle parking will be provided in the garages and in a new covered bicycle parking structure.

### Streets

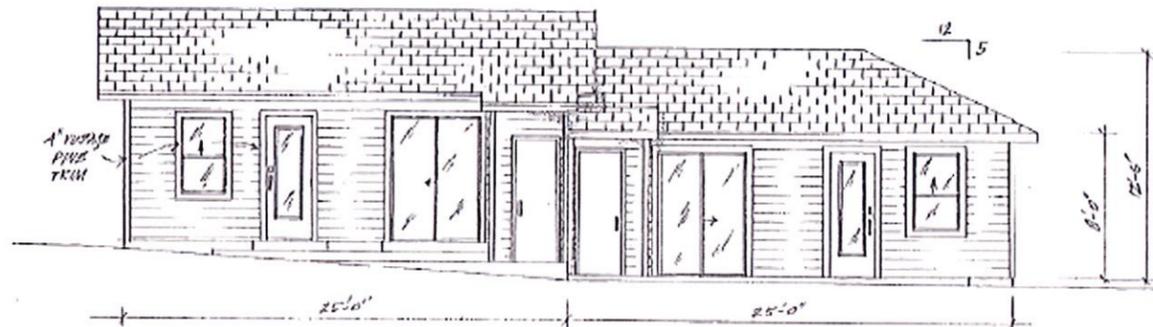
**North Laurel Street** is a residential neighborhood collector street, and city standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb, seven-foot parking bays on each side, and nine- to ten-foot travel lanes on each side. The city-standard cross-section includes a 22- to 34-foot curb-to-curb paved width within a 49- to 61-foot right-of-way, *dependent on the on-street parking configuration*. The existing curb-to-curb paved width along the frontage is 33 feet, and the right-of-way width along the corridor is 60-feet. There are curbs, gutters, paving, six-foot parkrow planting strips and five-foot sidewalks in place along the property frontage, but there are no street trees. As a condition of approval, city standard street trees will need to be planted in the parkrow planting strip along North Laurel Street.

### **Staff Recommendation**

Staff recommends that the application be approved with the conditions detailed in the draft findings included in the Planning Commission's June 11, 2019 meeting packet.

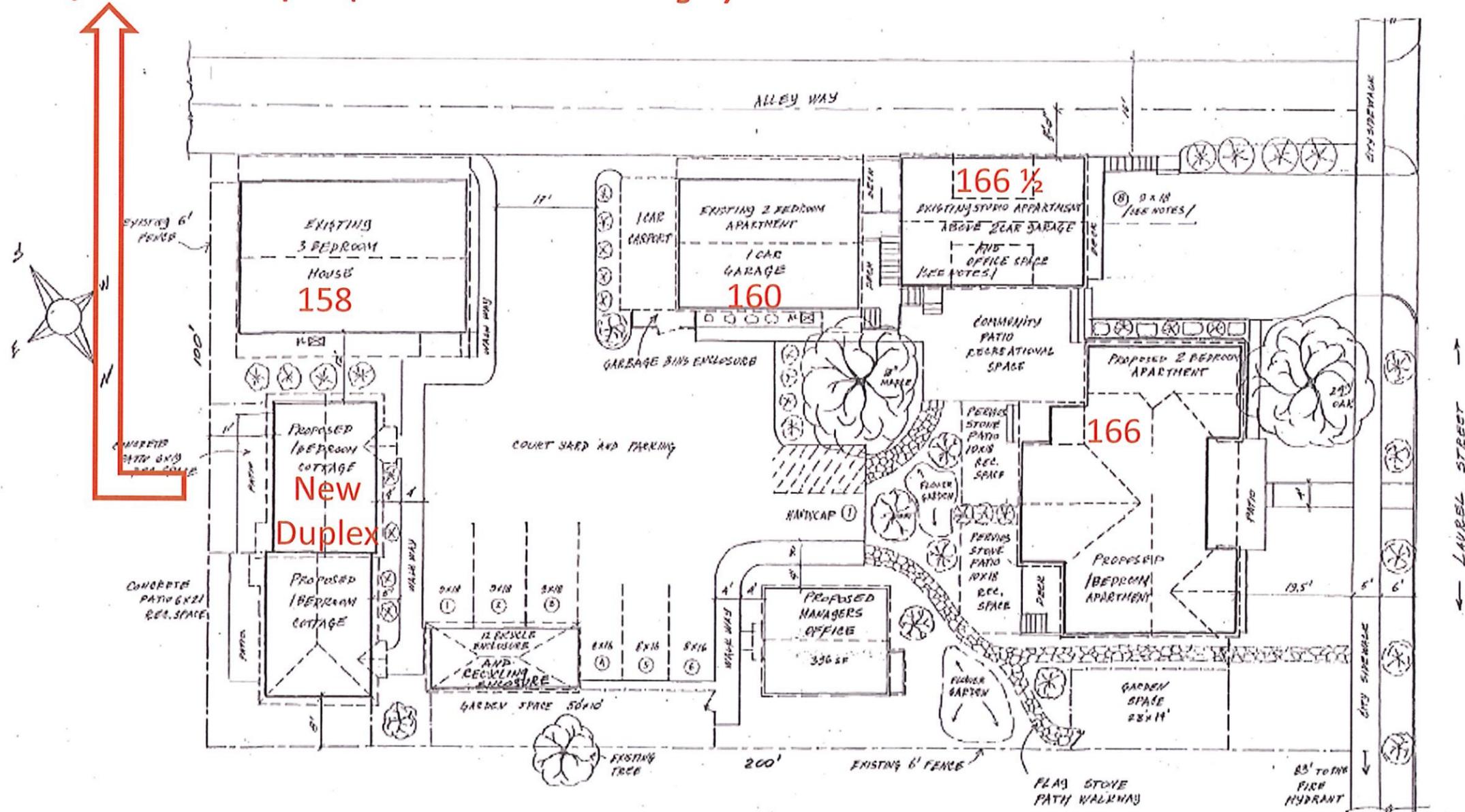
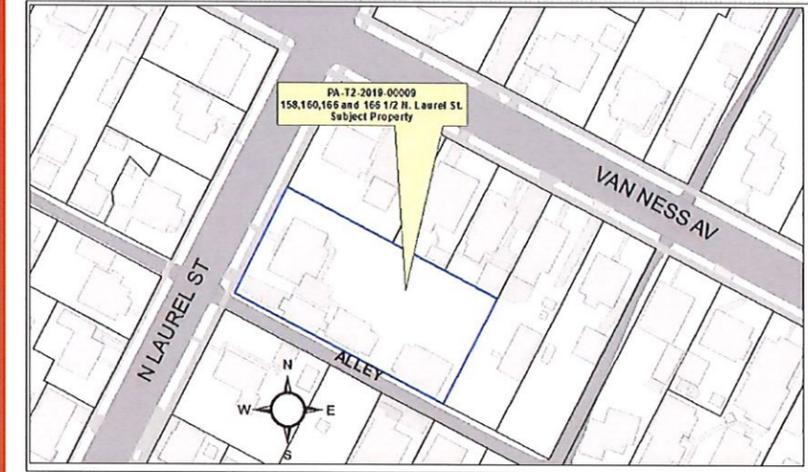
# Laurel Cottages

158-166 1/2 North Laurel Street



Proposed New Duplex (Two 1-Bedroom Cottages)

## Vicinity Map



**BEFORE THE PLANNING COMMISSION**  
**July 9, 2019**

IN THE MATTER OF PLANNING ACTION #PA-T2-2019-00009, A REQUEST FOR )  
SITE DESIGN REVIEW APPROVAL TO ALLOW THE CONSTRUCTION OF A 924 )  
SQUARE FOOT DUPLEX AT THE REAR OF THE PROPERTY, THE CONVERSION )  
OF 372 SQUARE FEET OF A 704 SQUARE FOOT GARAGE INTO AN APARTMENT )  
AND THE CREATION OF A DUPLEX FROM THE EXISTING SINGLE FAMILY )  
RESIDENCE FOR THE PROPERTY LOCATED AT 158, 160, 166 AND 166½ NORTH )  
LAUREL STREET. THERE ARE CURRENTLY FOUR UNITS ON THE PROPERTY )  
INCLUDING ONE STUDIO; THE PROPOSAL WOULD ADD FOUR UNITS INCLUD- )  
ING ONE NEW UNIT IN THE EXISTING HOUSE FRONTING ON LAUREL, A 372 )  
SQUARE FOOT APARTMENT IN THE GARAGE, AND TWO UNITS IN THE DU- )  
PLEX. THE APPLICATION ALSO INCLUDES A REQUEST FOR A CONDITIONAL )  
USE PERMIT TO EXCEED THE MAXIMUM PERMITTED FLOOR AREA (MPFA) IN )  
A HISTORIC DISTRICT BY 24.8 PERCENT. THE MPFA FOR THE PROPERTY IS )  
4,888 SQUARE FEET. THE EXISTING FLOOR AREA ON THE PROPERTY IS AP- )  
PROXIMATELY 5,175 SQUARE FEET, AND AS PROPOSED THE SITE WOULD )  
HAVE 6,099 SQUARE FEET IN FLOOR AREA. )

**DRAFT**  
**FINDINGS,**  
**CONCLUSIONS &**  
**ORDERS**

**OWNER/APPLICANT:** Laurel Cottages, LLC/Kim Locklin & Vadim Agakhanov )

-----  
**RECITALS:**

- 1) Tax lot #3400 of Map 39 1E 04CC is located on the east side of North Laurel Street, between Central and Van Ness Avenues, and is addressed from 158 to 166½ North Laurel Street. The property is zoned R-3 (High-Density Multi-Family Residential).
  
- 2) The applicant is requesting Site Design Review approval to allow the construction of a 924 square foot duplex at the rear of the property, the conversion of 372 square feet of a 704 square foot garage into an apartment, and the creation of a duplex from the existing single-family residence for the property located at 158, 160, 166 and 166 ½ North Laurel Street. There are currently four units on the property including one studio; the proposal would add four units including one new unit in the existing house fronting on Laurel, a 372 square foot apartment in the garage, and two units in the duplex. The application also includes a request for a Conditional Use Permit to exceed the maximum permitted floor area (MPFA) in a historic district by 24.8 percent. The MPFA for the property is 4,888 square feet. The existing floor area on the property is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area. The proposal is outlined in plans on file at the Department of Community Development.

3) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:

- A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.*
- B. **Overlay Zones:** *The proposal complies with applicable overlay zone requirements (part 18.3).*
- C. **Site Development and Design Standards:** *The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*
- D. **City Facilities:** *The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.*
- E. **Exception to the Site Development and Design Standards.** *The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.*
  - 1. *There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or*
  - 2. *There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.*

4) The criteria for a Conditional Use Permit are described in **AMC 18.5.4.050.A** as follows:

- 1. *That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.*
- 2. *That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.*
- 3. *That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.*
  - a. *Similarity in scale, bulk, and coverage.*
  - b. *Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of*

- facilities.*
  - c. Architectural compatibility with the impact area.*
  - d. Air quality, including the generation of dust, odors, or other environmental pollutants.*
  - e. Generation of noise, light, and glare.*
  - f. The development of adjacent properties as envisioned in the Comprehensive Plan.*
  - g. Other factors found to be relevant by the approval authority for review of the proposed use.*
4. *A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.*
  5. *For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses of each zone are as follows.*
    - c. **R-2 and R-3.** Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.*

5) The Planning Commission, following proper public notice, held a public hearing on June 11, 2019 at which time testimony was received and exhibits were presented. Subsequent to the closing of the hearing, the Planning Commission **approved the application subject to conditions pertaining to the appropriate development of the site.**

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

## **SECTION 1. EXHIBITS**

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

## **SECTION 2. FINDINGS & CONCLUSIONS**

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the staff report, public hearing testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Site Design Review and Conditional Use permit approvals meets all applicable criteria for Site Design Review described in AMC 18.5.2.050, and for a Conditional Use Permit as described in AMC 18.5.4.050.A.

2.3 The Planning Commission ***concludes that the proposal satisfies all applicable criteria for Site Design Review approval.***

The first approval criterion addresses the requirements of the underlying zone, requiring that, *“The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.”* The applicant explains that the subject property is zoned R-3, a High-Density Multi-Family Residential zoning, exceeds the minimum lot area and dimension requirements for the zone, and falls within the density allowed.

#### **Density**

The property’s R-3 zoning allows a base density of 20 dwelling units per acre. The property is 20,000 square feet in area or 0.459 acres, and as such has a base density of 9.18 dwelling units [20 du/acre x 0.459 acres = 9.18 du]. The proposal adds four new units to the four units already in place on the property. For the purposes of density calculations, one of the existing units and three of the four new units are less than 500 square feet in gross habitable floor area and are considered to be ¾-units, so the proposed density is actually only 7.0 units [(4 x 1 full unit) + (4 x 0.75 units) = 7.0 units], which is less than the allowed base density. Because the property is within a historic district, it is not subject to a minimum density requirement.

With regard to setbacks, the applicant notes that existing structures on the property – with the exception of the single family residence at 166 North Laurel Street – have non-conforming setbacks along the alley. The applicant further explains that 160 North Laurel Street is less than 12-inches from the alley due to the existing deck at the entry of the residence, and 158 North Laurel Street has a four-foot setback with eaves that encroach into the alley and a five-foot rear yard setback where ten feet per story is required. None of these non-conforming setbacks are proposed to be altered with the proposal, and the new cottages and manager’s building will comply with the current setback requirements and with building separation requirements, and the project as a whole will comply with lot coverage requirements.

The second approval criterion deals with overlay zones, and requires that, *“The proposal complies with applicable overlay zone requirements (part 18.3).”* The applicant explains that the property is within the Skidmore Academy Historic District, and as such is subject to the Historic District Development Standards.

The home at 166 North Laurel Street is designated the A.E. & Vivian Tonner House and is noted as having been built in around 1920. The *"much altered single-story dwelling"* is described as being *"Altered Historic Non-Contributing."* The inventory further notes that each of the other units on the site are *"of uncertain but possibly pre-1949 construction [and] may well have been in place by the end of the period of significance but none currently reflect any presumed historic appearance... they are considered additional historic, non-contributing, elements on the Tonner House site."*

The application explains that the front residence (166) is proposed to be altered from its current, substantially altered state to be more consistent with the historic district design standards through the removal of the "graceful swoop" structure extending from the north side to provide a garden wall, new siding that is historically-consistent, and new windows and doors that have shapes, areas and designs more consistent with the design standards and which will better establish an orientation and sense of entry to North Laurel Street. The new duplex units are also described as historically compatible, and the new manager's office building is noted as being of the same design aesthetic as the proposed duplexes.

The historic district design standards speak to building heights, scales, massing, setbacks, roofs, rhythms of openings, sense of base or platform, building form, entrances, the use of imitative styles and garage placement. The application explains that the structures are similar to average heights in the vicinity, and are shorter than the nearby historic contributing resources, noting that the scale of the duplex is within the range of other dwellings in the neighborhood. The applicant emphasizes that through the incorporation of a stepped gable roof with a hipped gable end, the ridgeline of the duplex is broken up to reduce its mass, and the multiple gable ends and covered porch vary the mass and scale from the existing structures while maintaining a roof pitch that is similar to the vicinity. The applicant also explains that the units have a consistently spaced window pattern that is consistent with the rhythm of the district, and that single hung, vinyl windows are proposed. The windows on the front residence are noted as being more consistent with the historic district design standards to improve the front elevation of the residence, and the doors and siding are to be upgraded. An exposed concrete stem wall foundation is to provide a sense of base and ground the proposed duplex.

The third criterion addresses the Site Development and Design Standards, requiring that *"The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below."* The applicant asserts that the proposal complies with the applicable Site Development and Design Standards, noting that the new floor plan and window, door and siding changes will improve the existing residence's at 166 North Laurel Street's orientation, while the new duplex at the rear of the property is internal to the site. The applicant explains that new parking is proposed to be internal to the property, and with the exception of the existing garage access from Laurel, vehicular access is to be from the alley. Street trees, landscaping and refuse/recycling areas are to be provided in a manner consistent

with standards.

The application notes that 1,600 square feet of open/recreation space is required for the project [20,000 sq ft x 0.08 = 1,600 sq ft]. More than 1,600 square feet is noted as being devoted to open space for recreational use by the tenants in a mix of common patio areas, semi-private outdoor yard areas, lawns and gardens.

### Parking

With regard to the parking requirements in AMC 18.4.3, the applicant has provided calculations showing that a total of 11 off-street parking spaces are required for the proposal. Two spaces are to be provided in the existing garages, one in the existing driveway off of North Laurel, one in a covered carport at 160 North Laurel, and seven spaces are to be provided in a new proposed surface parking lot accessed from the alley. No on-street parking credits are requested. Bicycle parking is to be provided in the garages and in a new covered bicycle parking structure.

The fourth approval criterion addresses city facilities, specifically requiring that, *"The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property."* The applicant explains that city facilities are available at the site, including:

- 4-inch water main in North Laurel Street with two new water meters between Laurel Street driveway and the alley.
- 10-inch sanitary sewer main in North Laurel Street and a 6-inch sanitary sewer main in the alley.
- 12-inch storm sewer main in North Laurel Street.
- Electrical service is available both from the alley and from North Laurel Street.

The application explains that most of the utility services have been upgraded in conjunction with recent remodels of 158 North Laurel Street in anticipation of this application, noting that electrical upgrades were reviewed and approved by the City of Ashland Electric Departments. In discussion with the Electric Department staff, they have noted that conversion of existing units will likely require service upgrades and the installation of two-pack meter bases on each unit, with final placement to be approved by the Electric Department as part of the final electric service plan. A condition to this effect has been included below.

With regard to paved access and adequate transportation, staff would note that North Laurel Street is a residential neighborhood collector street, and city standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb, seven-foot parking bays on each side, and nine- to ten-foot travel lanes on each side. The city-standard cross-section includes a 22- to 34-foot curb-to-curb paved width within a 49- to 61-foot right-of-way, *dependent on the*

*on-street parking configuration.* The existing curb-to-curb paved width along the frontage is approximately 33 feet, and the right-of-way width along the corridor is 60-feet both of which are in keeping with standards. There are curbs, gutters, paving, six-foot parkrow planting strips and five-foot sidewalks in place along the property frontage, but there are no street trees. As a condition of approval, city standard street trees will need to be planted in the parkrow planting strip along North Laurel Street and a condition to that effect is included below.

The Engineering Division has reviewed the proposal and found that a Traffic Impact Analysis (TIA) is not required as the proposal does not reach threshold levels to require a TIA given the number of units and associated average daily trips. The threshold levels to require a TIA are: **Trip Generation Threshold:** 50 newly generated vehicle trips (inbound and outbound) during the adjacent street peak hour; **Mitigation Threshold:** Installation of any traffic control device and/or construction of any geometric improvements that will affect the progression or operation of traffic traveling on, entering, or exiting a highway; and **Heavy Vehicle Trip Generation Threshold:** 20 newly generated heavy vehicle trips (inbound and outbound) during the day.

To the south of the subject property is an alley; city standard alley improvements include a 12-foot paved travel surface with two-foot buffer strips down each side within a 16-foot right-of-way. The existing alley is of a standard width, and is not paved. City policy has long been not to require paving of alleys in historic districts, and in this instance, staff believe that this is particularly appropriate given the limited amount of square footage proposed to be added and the total length of the alley versus the extent that would be paved. Regina Ayars, a property owner in the neighborhood has raised concerns with the impacts of construction to the alley, noting that other projects off of the alley have resulted in "huge potholes" and seeking assurances that the alley surface will be fixed. Ms. Ayars has also question the parking impacts, and asked that the City consider designating the alley for one-way traffic to address increased traffic flow. Conditions are recommended below to require that alley maintenance, including pothole repair, be addressed by the applicant prior to occupancy, and that the project erosion control plan also address potential construction trackout. Public Works has been made aware of concerns about alley traffic flows, and Ms. Ayars informed that this would be an issue for consideration by the Transportation Commission.

The subject property currently takes access both from the alley and from a curb-cut north of the alley to a driveway and garage at 166 ½ North Laurel Street. The application notes that the new central parking area will take access from a new driveway off of the alley, but proposes to retain the driveway off of North Laurel Street to serve a single surface parking space in front of the to-be-converted garage space. A second garage space will continue to be used for parking. Parking and circulation in this location are non-conforming both for the placement between the building and the street and for the driveway's proximity to the alley intersection, and in staff's view, retaining the existing non-conforming driveway and parking in this location is appropriate

only if the existing driveway is not widened from its present width. A condition to this effect has been recommended below.

The final criterion for Site Design Review approval addresses “Exception to the Site Development and Design Standards.” The current request does not include any Exceptions to the Site Development and Design Standards.

Staff believes that the Planning Commission could reasonably conclude that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

2.4 The Planning Commission ***concludes that the proposal satisfies all applicable criteria for Conditional Use Permit approval.***

#### **Maximum Permitted Floor Area (MPFA)**

Within the historic districts, each property has a Maximum Permitted Floor Area (MPFA) based on the lot size and number of units proposed. Applicants may request to exceed the MPFA by up to 25 percent with a Conditional Use Permit (CUP) as allowed in AMC 18.2.5.070.C., which requires that in addition to the approval criteria for a Conditional Use Permit, the Historic District Design Standards must also be addressed. The application here includes a request for a CUP to exceed the MPFA by 24.8 percent. The MPFA for the subject property is 4,888 square feet [**20,000 sq. ft. Lot Area x 0.47 Adj. Factor = 9,400 sq. ft. Adjusted Lot Area x 0.52 Graduated FAR = 4,888 sq. ft. MPFA x 1.25 Maximum CUP allowance = 6,110 sq. ft.**]. The existing floor area on the property now is approximately 5,175 square feet, and as proposed the site would have 6,099 square feet of floor area – the additional floor area requested is for the 924 square foot duplex proposed at the rear of the property.

#### **CUP Criteria (18.5.4.050.A)**

The first criterion for a CUP is, *“That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.”* The applicant asserts here that the use of the site is residential, and is consistent with the relevant Comprehensive Plan policies and does not violate city, state or federal laws. The applicant further suggests that the proposal furthers the purpose and intent of the Land Use Ordinance and provides for an efficient use of land in the high-density, multi-family residential R-3 zoning district. The applicant emphasizes that the proposal will not negatively impact adjacent uses, is compatible and livable, preserves existing historic structures, furthers the design standards of the City and provides needed housing.

The second criterion is that, *“That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can*

and will be provided to the subject property.” This criterion has been thoroughly addressed in the Site Design Review discussion above.

The third criterion is that, *“That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone: a) Similarity in scale, bulk, and coverage; b) Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities; c) Architectural compatibility with the impact area; d) Air quality, including the generation of dust, odors, or other environmental pollutants; e) Generation of noise, light, and glare; f) The development of adjacent properties as envisioned in the Comprehensive Plan; and g) Other factors found to be relevant by the approval authority for review of the proposed use.”* The applicant asserts that the addition 924 square feet will not have a greater adverse material impact that would the target use of the zone, and explains that the existing structures are at the perimeter of the property, adjacent to the alley, providing ample area on site for the proposed improvements while also minimizing their impacts to the surrounding streetscape and neighboring properties. The applicant points out that the proposed structures are similar in height to buildings in the neighborhood, and are limited to single-story construction, and meet required setbacks, building separation and lot coverage providing for a reduced bulk, scale and coverage. The applicant emphasizes that the units are small and have adequate parking provided, and the proposal is an attempt to develop the property according to its envisioned multi-family zoning and allowed density while addressing the Maximum Permitted Floor Area. The application further explains that the units are in a walkable neighborhood, with sidewalks throughout, near a bus route and within walking distance of the downtown, groceries and other retail, restaurants and entertainment, all of which should serve to lessen necessary vehicle trips. The units are noted as having a traditional styling which reflects the character of historic resources in the vicinity and elements of the styles found in the vicinity, and numerous aspects of the previous exterior details on the property are to be upgraded to provide for greater historic compatibility. The applicant concludes that the additional square footage will not impact the generation of noise, light, glare or air quality, nor will it impact the development of surrounding properties as envisioned in the Comprehensive Plan.

The fourth approval criterion is that, *“A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.”* In this instance, the residential uses proposed are outright permitted uses in the zone and are within the allowed base density for the property.

The final CUP criterion is that, *“For the purposes of reviewing conditional use permit applications for conformity with the approval criteria of this subsection, the target uses (for R-2 and R-3 zones is): Residential use complying with all ordinance requirements, developed at the density permitted by chapter 18.2.5 Standards for Residential Zones.”* As noted earlier in this document, the base density for the subject property is 9.18 units, while the density as proposed is 7.0 units. When considered on this

*basis, staff believe it can readily be demonstrated that 7.0 units will have less adverse material impact than would 9.18 units – i.e. fewer average daily trips generated, lesser parking demand, fewer units creating impacts, etc. - and the key consideration in terms of the CUP to exceed the MPFA is in looking at compliance with the Historic District Design Standards.*

#### **Historic District Design Standards (18.4.2.050.H)**

The subject property is located within the Skidmore Academy Historic District. The home at 166 North Laurel Street is designated the A.E. & Vivian Tonner House and is noted as having been built in around 1920. The *“much altered single-story dwelling”* is described as being *“Altered Historic Non-Contributing.”* The inventory further notes that each of the other units on the site are *“of uncertain but possibly pre-1949 construction [and] may well have been in place by the end of the period of significance but none currently reflect any presumed historic appearance... they are considered additional historic, non-contributing, elements on the Tonner House site.”*

The application explains that the front residence is proposed to be altered from its current, substantially altered state to be more consistent with the historic district design standards through the removal of the *“graceful swoop”* structure extending from the north side to provide a garden wall, new siding that is historically-consistent, and new windows and doors that have shapes, areas, designs and placement more consistent with the design standards. The new duplex units are also described as historically compatible, and the new manager’s office building is noted as being of the same design aesthetic as the proposed duplexes.

The historic district design standards speak to building heights, scales, massing, setbacks, roofs, rhythms of openings, sense of base or platform, building form, entrances, the use of imitative styles and garage placement. Compliance with these standards is discussed in the overlay zones discussion in 2.3 above.

AMC 18.4.2.050.A.2.b provides that if a development requires land use approval such as Site Design Review or a Conditional Use Permit and involves new construction, restoration, rehabilitation, or any use greater than a single-family unit, the authority exists in law for the Planning Commission to require modifications in the design to match these standards, and the Historic Commission is specifically identified by code as advising both the applicant and the City decision maker. In this instance, as the staff report and draft findings are being prepared, the Historic Commission has not yet considered the proposal but is scheduled to do so at its regular June meeting. A condition of approval is accordingly recommended that the recommendations of the Historic Commission from its June meeting, where consistent with applicable criteria and standards and with final approval by the Staff Advisor, shall be conditions of approval for the project.

2.5 The Planning Commission finds that there are two trees on the property: a 24-inch Oak in the front yard of 166 North Laurel, and an 18-inch Maple in the courtyard/patio area between 166 and 166 ½ North Laurel. Both trees are proposed to be preserved and protected, and trees on adjacent properties are noted

as being protected by the existing fencing. The application includes a Tree Protection Plan illustrating measures proposed to protect the two existing trees pursuant to AMC 18.4.5 and a Landscape Plan detailing proposed new plantings.

### Trees

The Tree Commission has not yet reviewed the application as these draft findings are being prepared, but is scheduled to do so at its regular meeting on June 6<sup>th</sup>. Staff have recommended conditions requiring that a final Tree Protection Plan incorporating any recommendations of the Tree Commission be provided with building permit submittals, and that a Tree Verification Permit inspection be obtained to verify that required tree protection is in place according to the approved plans prior to any site work. With these conditions, staff believes that the Planning Commission could reasonably conclude that the proposal complies with the requirements for Tree Preservation and Protection from AMC 18.4.5.

## SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, *the Planning Commission concludes that the proposal for Site Design Review for an eight-unit multi-family development and Conditional Use Permit to exceed the maximum permitted floor area in a historic district by 24.8 percent is supported by evidence contained within the whole record.*

In staff's assessment, the Planning Commission could reasonably determine that the proposal, with the conditions recommended complies with all applicable criteria and standards. The existing development already exceeds the Maximum Permitted Floor Area, and the proposed overage is limited to the addition of a 924 square foot duplex which is to provide two small rental units. These units have been designed and placed on site to minimize their impact to the surrounding district. In conjunction with the new project, existing buildings are also being modified to provide additional small rental units while placing the new parking to serve the more efficient redevelopment of the site interior to the property, with access from the alley, and the applicant is fully accommodating required parking on site and requesting no on-street parking credits. The existing historic home along Laurel, which has had substantial modifications impacting its historic character over the years, is to be converted to a duplex and its façade improved by removing ill-suited alterations, replacing the siding, and installing doors and windows to better relate to the streetscape and surrounding historic district.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2019-00009. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #PA-T2-2019-00009 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That new addresses shall be assigned by City of Ashland Engineering Department. The existing “½” address (i.e. 166½ North Laurel Street) will need to be corrected, as “½” addresses are not consistent with approved city addressing conventions.
3. That permits shall be obtained from the Ashland Public Works Department prior to any work in the public right of way, including but not limited to permits for new driveway approaches or any necessary encroachments.
4. That the manager’s office building is an accessory building on site, and shall not have kitchen facilities installed or be used as a rental unit. Prior to the issuance of a certificate of occupancy or final project approvals, a deed restriction shall be recorded to limit the use of this building.
5. Parking and circulation via the driveway from North Laurel Street is non-conforming both for its placement between the building and the street and for its proximity to the alley. The existing driveway shall not be modified from its current width, and parking from this driveway shall be limited to a single garage space and a single space in the driveway. Building permit submittals shall verify compliance with this condition.
6. The recommendations of the Historic Commission from its June 5, 2019 meeting, where consistent with applicable criteria and standards and with final approval by the Staff Advisor, shall be conditions of approval for the project.
7. That the building permit submittals for the new duplex and existing buildings’ conversions shall include:
  - a. That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in conformance with those approved as part of this application, an application to modify the Site Design Review and Conditional Use Permit approval shall be submitted and approved prior to issuance of a building permit.
  - b. All easements including but not limited to public and private utilities and fire apparatus access shall be indicated on the building permit submittal for review by the Planning, Engineering, Building and Fire Departments.
  - c. Solar setback calculations demonstrating that all new construction complies with Solar Setback Standard A in the formula  $[(\text{Height} - 6 / 16) / (0.445 + \text{Slope}) = \text{Required Solar Setback}]$  and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and their height(s) from natural grade.
  - d. Final lot coverage calculations demonstrating how lot coverage is to comply with the applicable coverage allowances of the zoning district (75 percent coverage is allowed in the R-3 district). Lot coverage includes all building footprints, driveways, parking areas and other circulation areas, and any other areas other than natural landscaping.
  - e. Verification that the project as a whole complies with the Maximum Permitted Floor Area requirements of AMC 18.24.040.I. and additional square footage approved herein, including floorplans and ceiling height verification for all living space and potential living space.
  - f. Storm water from all new impervious surfaces and runoff associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb

gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.

- g. Final utility, grading, drainage and erosion control plans.
  - i. The final storm drainage plan shall detail the location and final engineering for all storm drainage improvements associated with the project, and shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions. The storm drainage plan shall demonstrate that post-development peak flows are less than or equal to the pre-development peak flow for the site as a whole, and that storm water quality mitigation has been addressed through the final design.
  - ii. The final utility plan for the parcels shall be submitted for review and approval by the Planning, Engineering, and Building Divisions with the Final Plan application. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer mains and services, manholes and clean-outs, and storm drainage pipes and catch basins. Any required private or public utility easements shall be delineated on the civil plans.
  - iii. The final electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment with the Final Plan application. This plan must be reviewed and approved by the Electric Department prior to permit issuance or installation of equipment. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department. Individual electric services/meters shall be installed underground to serve all units.
  - iv. The erosion control plan shall address trackout prevention pursuant to AMC 9.080.060.
- 8. A final Tree Protection Plan addressing the requirements of AMC 18.4.5.030.B and any recommendations of the Tree Commission from its June 6, 2019 regular meeting, where consistent with applicable standards and criteria and with final approval by the Staff Advisor. The plan shall identify the location and placement of fencing around the drip lines of trees identified for preservation. The amount of fill and grading within the drip lines shall be minimized. Cuts within the drip line shall be noted on the tree protection plan, and shall be executed by handsaw and kept to a minimum. No construction shall occur within the tree protection zone including dumping; storage of materials such as building supplies, soil, waste, equipment, or parked vehicles; and no fill shall be placed around the trunk/root crown.
- 9. A final size- and species-specific landscaping plan including irrigation details and details of the landscape materials to be planted shall be provided for the review and approval of the Staff Advisor. New landscaping shall comply with the General Fuel Modification Area requirements and shall not include plants listed on the Prohibited Flammable Plant List adopted by Resolution

#2018-028.

10. That the requirements of the Ashland Fire Department relating to approved addressing; fire apparatus access; firefighter access pathways and any gates, fences or other obstructions; fire hydrant distance, fire sprinkler and extinguishers as applicable; and wildfire hazard area requirements shall be satisfactorily addressed in the building permit submittals. Fire Department requirements shall be included in the building permit drawings, and a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements of AMC 18.3.10.100.A.2. shall be provided with the building permit submittals. This plan shall be reviewed and approved by the Fire Marshal prior to bringing combustible materials onto the property.
11. That exterior building materials and paint colors shall be compatible with the surrounding area and consistent with those described in the application. Sample exterior building colors shall be provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with 18.4.2.030.E of the Multi-Family Site Design and Use Standards.
12. That prior to the issuance of building permits:
  - a. A Tree Verification Permit shall be applied for and approved by the Ashland Planning Division prior to any site work including excavation, staging or storage of materials, or permit issuance. The Tree Verification Permit is to inspect the installation of tree protection fencing for two trees to be protected. Standard tree protection consists of chain link fencing six feet tall and installed in accordance with the requirements of AMC 18.4.5.030.B. No construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
  - b. That the requirements of the Building Division shall be satisfactorily addressed including but not limited to public way, fire and occupancy separations, and openings relative to property lines.
  - c. That the applicant shall pay for plan review, building permits and all associated fees and charges including system development charges (water, sewer, stormwater, parks and transportation) for all new residential units.
13. That prior to the issuance of a certificate of occupancy or final inspection approvals:
  - a. That the parkrow planting strip along the North Laurel Street frontage shall be planted with irrigated street trees selected from the Recommended Street Trees guide at a spacing of one per 30 feet prior to the issuance of a certificate of occupancy or final inspection approvals for the project.
  - b. Electric services shall be installed underground to serve all proposed units, inspected and approved. A final electric service plan shall be reviewed and approved by the Ashland Electric, Building, Planning and Engineering Divisions prior to installation. Duplex units

will require two-pack meter bases with placement to be approved by the Electric Department, and existing units being converted will require service upgrades.

- c. Utility installations and common area improvements including landscaping, hardscaping, irrigation, bicycle parking, and trash and recycling facilities shall be completed according to approved plans, inspected and approved by the Staff Advisor.
- d. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
- e. That maintenance of the alley, including repair of any damage caused during construction, shall be carried out by the applicant concurrently with completion of construction. The alley shall be graded to the standards required by the Ashland Public Works Department for the full property frontage, and any potholes or other damage caused in the full alley corridor during construction repaired, prior to the issuance of a certificate of occupancy, and shall be maintained to provide at least a 12-foot wide driving surface able to withstand 44,000 lbs. with the full alley width maintained to a vertical clearance of 13-feet, 6-inches.
- f. That bicycle parking facilities to accommodate the ten required bicycle parking spaces shall be installed prior to the issuance of a certificate of occupancy or final project approval. For bicycle parking spaces not located within garages, inverted u-racks shall be used for the bicycle parking. All bicycle parking shall be installed in accordance with design and rack standards in 18.4.3.070.I and J prior to the issuance of the certificate of occupancy, inspected and approved by the Staff Advisor. The building permit submittals shall verify that the bicycle parking spacing and coverage requirements are met in accordance with 18.4.3.070.I.

July 9, 2019

Date

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Planning Commission Approval

158 -166½ North Laurel Street

# Laurel Cottages

Multi-Family Site Design Review and Conditional Use Permit to exceed Maximum Permitted Floor Area to allow for the addition of four dwelling units on the property. The proposal preserves of all the existing historical, structures.



RECEIVED  
MAY 10 2019  
City of Ashland

May 10, 2019

**Multi-Family Site Review for the development of additional dwelling units at the property located at 158 – 166½ Laurel Street. The proposal includes the request to construct a 924-square foot duplex, the conversion of 372-square feet of the 704 square foot garage that is near the front residence to an apartment, and to allow for the duplexing of the front residence.**

**Subject Property**

**Property Address:** 158 – 166½ Laurel Street

**Property Owner:** Laurel Cottages LLC  
Kim Locklin & Vadim Agakhanov  
262 B Street  
Ashland, OR 97520

**Planning Consultant:** Amy Gunter  
Rogue Planning & Development Services  
33 N Central Avenue, Ste. 213  
Medford, OR 97501

**Building Designer:** Vadim Agakhanov  
262 B Street  
Ashland, OR 97520

**Map & Tax Lots:** 39 1E 04CC; Tax Lots: 3400  
**Comprehensive Plan Designation:** High Density, Multi-Family Residential  
**Zoning:** R-3  
**Adjacent Zones:** R-2 and R-3  
**Zoning Overlays:** Skidmore Academy Historic District

**Lot Area:** 20,000 square feet

**Request:**

**RECEIVED**  
**MAY 10 2019**  
City of Ashland

A request for a Multi-Family, Site Design Review approval to add additional dwelling units to the multifamily zoned property. All existing residences on the property, 158, 160, 166 and 166½ are proposed to remain. Some of the existing structures and site improvements are considered pre-existing, non-conforming. The existing structures are all historic in age and have been utilized as residential units for many decades. The setbacks of the existing structures are legal non-conforming.

**Property Details:**

The subject property is on the east side of Laurel Street to the north of the Laurel Street and Central Avenue intersection, and north of North Main Street. The property is to the north of the mid-block public alley that extends between North Laurel Street and Helman Street to the east.

The rectangular lot has 100-feet of frontage North Laurel Street and extends 200-feet to the southeast. The lot is 20,000 square feet in area.

The property and the adjacent properties are zoned R-3, High Density multi-family residential. Other zones in the vicinity include R-2, Low Density multi-family residential, commercial and employment zones.

The subject property appears on the earliest City of Ashland maps and is within the Skidmore Academy Historic District. The site is occupied by four separate residential dwellings as follows:

- 166 North Laurel is a 1,534-square foot, single story, 3-bedroom residence. According to the City of Ashland Historic Resources Inventory, the residence was constructed sometime around the 1920s. Based on aerial photography, the structure appears to have been substantially altered between the late 1930s and the early 1960s leading to an unclear conclusion of the architectural character and significance of this residence.
- 166½ North Laurel is located to the south of 166 North Laurel. It is directly adjacent to the alley. The garage has a footprint of 704-square feet. The studio unit above is 465-square feet in area (attached photos and floor plan). There is less than seven feet of headroom within a large portion of the unit. Though the assessor’s documents indicate this structure is from the 1950s, there is clearly a structure in the same location on the 1939 aerial photography from the area.

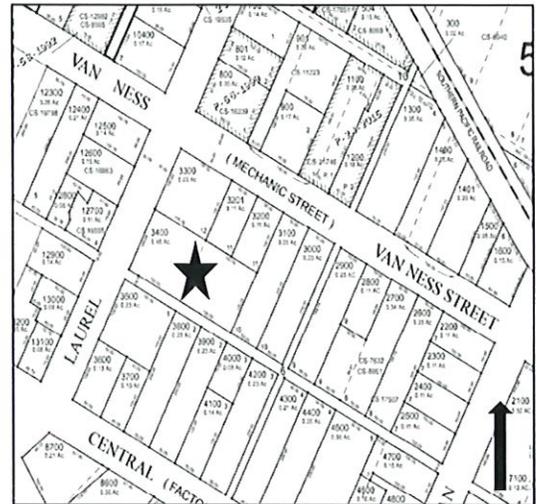


Figure 1: Assessor's Map

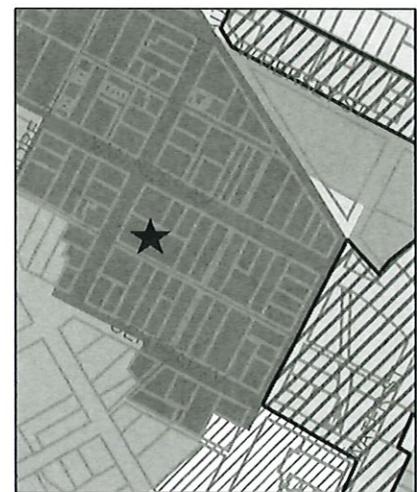


Figure 2: Zoning Map

- 160 North Laurel Street is a two-story structure with a 704 square foot, two-bedroom apartment above an oversized, 704 square foot garage. There is an attached carport on the east side of the structure. The garage is accessed via the alley.
- 158 North Laurel is a single story, three-bedroom, 1,060 square foot residence. This residence at one time had a 200-square foot garage accessed via the alley. Due to grade changes in the alley, access to garage was effectively eliminated. The garage area was converted in 2018 to habitable space. The assessor estimates this structures construction around 1947.

There are two large stature trees greater than six-inches in diameter at breast height on the property. They are:

- A large, 24-inch DBH Oak in the front yard of 166 North Laurel Street and,
- An 18-inch DBH Maple tree in the court yard/patio area between 166 and 166½ North Laurel.

The site has a level grade with approximately three percent slope from the south to north.

<b>Solar Setback Calculation</b>
$1875 - 1871 = 4 / 150 = -.026$
$1873.5 - 1868 = 5.5 / 150 = -.036$
Average: $-.03$

North Laurel Street has a 60-foot wide right-of-way and is improved with pavement, curb and gutter, a seven-foot wide landscaped park row and a five-foot sidewalk along the frontage of the property. The property is accessed via narrow driveway curb cut that is near the south property line. This driveway serves the existing ground level garage with the studio unit above (166½ North Laurel Street). The separation between the driveway and the alley intersection is non-conforming; this proposal retains the driveway.

The public utilities include:

Sanitary Sewer      The property is currently served by a 10-in sanitary sewer main iNorth Laurel Street and a 6-in sanitary sewer main in the alley south of the project.

Water                The property is currently served by a 4-in water main iNorth Laurel Street. There are two, new water meters in the landscape island that is between the Laurel Street driveway and the alley.

Storm Drainage      The property is currently served by a 12-in storm sewer main North Laurel Street.

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Electric

There is electric service available in the alley to the south of the property and from Laurel Street. Most of the utility services were upgrade with the recent remodels of 158 North Laurel Street and in anticipation of this application. The electrical upgrades were approved by the Electric Department of the City of Ashland.

**Proposal:**

A request for a Multi-Family, Site Design Review approval to add additional dwelling units to the multifamily zoned property. The first modification is to duplex the existing front residence (166 North Laurel Street) by creating a 642-square foot, one-bedroom unit on the north side of the residence and a two bedroom, 890-square foot apartment on the south side.

The second unit change is to convert 372 square feet of the 704 square foot ground floor of the North Laurel Street facing garage with studio unit above (166½ North Laurel Street) to a one-bedroom unit on the south side of the structure. The north side of the garage will remain as garage area.

At the rear of the property near the east (rear) property line a two-unit, duplex is proposed. Each unit of the proposed duplex is proposed as a 462 square foot, one-bedroom single story unit.

A detached accessory structure of 396 square feet that will provide a manager’s office and maintenance equipment storage area is proposed to be included in the development.

The proposal conforms to the allowed density in the R-3 zone. The .459 lot area allows for nine (9) dwelling units ( $20000 / 43560 = .459 \times 20 = 9$ ). The proposal is for the equivalent of seven (7) units. There are three (3) existing units, and one (1) studio unit, less than 500 sf (3.75). The proposal adds 3.25 units. The duplexing of 166 North Laurel Street adds one (1) unit, the 372 square foot apartment in the garage, and the proposed new duplex unit are equal to 2.25 units. ( $3 \times .75 = 2.25$ ).

Existing Units	Unit Calculation
Studio unit, less than 500 SF (166½ North Laurel)	.75 unit
158 North Laurel	1.0 unit
160 North Laurel	1.0 unit
166 North Laurel	1.0 unit
<b>Total Existing Units</b>	<b>3.75 units</b>

Proposed Additional Units	Additional Units
Duplex existing home at 166 North Laurel	1 unit
Conversion of Garage at 166½ (372 SF)	.75 unit
New Duplex, each unit to be 462 SF	1.5 units
<b>Total Proposed New Units</b>	<b>3.25 units</b>

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**MPFA**

The proposal requires a Conditional Use Permit to exceed the Maximum Permitted Floor Area (MPFA) in the Historic District to allow the development of the property to the suggested 80 percent minimum density in the multi-family residential zones. Although properties in the historic district are not subject to minimum density, the presence of the existing structures substantially reduces the sites development potential by virtue of the MPFA standards reducing the lot area for development purposes by half.

There is 5,171 square feet of existing structures. The square footage of structures on the property exceeds the maximum allowed of 4,888 square feet. With a Conditional Use Permit, an additional 1,222 square feet of structure is allowed. The presence of two, historical residences above non-conforming garage structures on one parcel is a unique development circumstance in the historic district. The presence of three residential structures, that are well constructed, providing long term housing in the historic district that does not comply with setbacks on one parcel is also a unique and unusual development pattern. The proposed new duplex is 924 square feet which is less than the allowed 25 percent CUP to increase the MPFA.

Though not subject to minimum density standards, the property can support additional dwelling units that are well designed, meet applicable standards, provide parking and retain existing historic housing.

<b>MPFA</b>	20000
<b>Adjusted Lot Area</b>	9400
<b>Graduated FAR</b>	4888
<b>CUP to Increase by 25 percent:</b>	1222
<b>Allowed MPFA w/ CUP</b>	6110
<b>Existing Square footage</b>	5175
<b>Proposed Overage</b>	924

**Parking**

The proposed additional units require parking for the existing site development to be provided. The site development requires a total of eleven (11) vehicle parking spaces. The breakdown is as follows:

<b>Unit</b>	<b>Calculation</b>	<b>Required Parking</b>
(1) 3-bedroom unit	2 x 1	2 spaces
(2) 2-bedroom units	1.75 X 2	3.5 spaces
(1) 1-bedroom > 500 SF	1.5 x 1	1.5 spaces
(4) 1-bedroom units/studio units < 500 SF	1 x 4	4 spaces
<b>Total Required Spaces</b>		<b>11 spaces</b>

Parking requirements will be met as follows:



	<b>Provided Parking</b>
Existing garages	2 spaces
Driveway on North Laurel Street	1 space
Covered carport at 160 North Laurel	1 space
7-vehicle parking lot (proposed)	7 spaces
<b>Total Required Spaces</b>	<b>11 spaces</b>

A seven vehicle-space parking lot is proposed to be developed in the central yard area of the property. A total of eleven parking spaces are provided for on-site. One of the parking spaces is handicap accessible.

**Bicycle Parking**

Bicycle parking is provided for in the garages and under a new covered structure for a total of 11 covered, secured bicycle parking spaces.

**Patio/Yard Areas & Sidewalks**

There are several patio and yard areas for the tenants including a larger open space consisting of lawn area, common patio area and tenant garden areas.

A sidewalk system through the development will be provided in order to provide a safe walking route to the public sidewalk.

**Utilities**

Utilities are proposed to be connected to the existing systems located on North Laurel Street and within the alley to the south of the property.

**Open Space**

The required open space area is 1,600 square feet. A common patio area is to the rear of 166 Laurel Street. There are patio areas and lawn to the rear of the new duplex unit. Along the north property line, there are vegetable garden areas proposed. Other landscape areas are provided that will have shrubbery, bark mulch and other plantings. The landscaping plan provides five new street trees in the landscape park row.

There are two trees greater than six-inches in diameter at breast height (DBH) on the property. Both will be protected throughout the duration of the site disturbance and construction.

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## **Findings of Fact**

### **Ashland Land Use Ordinance**

On the following pages, the criteria from the Ashland Land Use Ordinance as it pertains to Site Design Review and Conditional Use Permit criteria have been addressed. The City of Ashland criteria are in Times New Roman font and the applicant's findings are in Calibri font.

### **Site Development Design Standards Approval Criteria Ashland Municipal Code 18.5.2.050**

**A. Underlying Zone.** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

*The subject property is a pre-existing, R-3, High Density Multi-Family zoned R-3 lot. The parcel is 20,000 square feet and with a 100-foot lot width and 200-foot lot depth, exceeds minimum lot area and minimum lot dimensions in the R-3 zone.*

*The proposal is to have a total eight residential dwelling units (See density discussion on following pages). There are four dwelling units currently on the property. There is a three-bedroom, single story residence at 166 Laurel Street, a small studio apartment above the garage that is to the south of this residence, 166½ Laurel Street. To the East, along the alley is 160 Laurel Street, another two-story structure with an oversized, one vehicle garage on the ground floor and a two-bedroom, apartment above. Further east, along the alley is 158 Laurel Street, a single story, three-bedroom residence.*

#### **Density:**

*The proposed density is less than the maximum allowed in the zone for the 20,000 square foot property. Eight residential units are proposed. Four units exist and four units are proposed. The size of the dwellings is equal to seven units for the purposes of density calculations.*

*The existing studio unit has less than 500 square feet of floor area. The converted garage unit is less than 500 square feet and each side of the duplex is proposed to be less than 500 square feet.*

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AMC Allowed Density 18.2.5.080 Calculation	Unit Calculation
20 DU / ACRE: Lot area= .459 X 20 = 9	
Maximum Allowed Density	9 dwelling units

Proposed Density	Unit Calculation
Existing structures	3.75
Proposed structures (3 units < 500 SF; 1 unit > 500 SF)	3.25
<b>Total Proposed Density</b>	<b>7 dwelling units</b>

**Setbacks:**

*Except for the residence that fronts North Laurel Street, all the other structures on the property have non-conforming setbacks to the public alley that runs along the south property line. The setback of the garage and 166½ North Laurel is zero-feet with eave encroachments. The setback of 160 North Laurel Street is less than 12-inches due to the existing deck that accesses the entrance to the residence. 158 North Laurel Street has a four-foot setback with the eaves that encroach on the south property line (alley). There is a five-foot rear yard setback where ten feet are required.*

*The proposed conversion of 166 North Laurel Street into a duplex does not alter the existing setbacks. Along the rear property line, north of 158 North Laurel Street, two small, attached cottages are proposed.*

*The proposed setback of the duplex structure on the east side is setback ten-feet for the north unit and 11-feet for the south unit, which complies with the required rear yard setback. The structures have a 12-foot separation from 158 North Laurel Street. The proposed structure is setback from the north property line by eight-feet which is greater than the required from the side yard setback.*

*The manager office building is not included in the MPFA calculations. This structure will also comply with the setbacks in the zone.*

(AMC 18.2.5.070.E. **Exceptions.** Basements, detached garages, detached accessory structures, and detached accessory residential units are not counted in the gross floor area for the MPFA calculation if separated from the primary dwelling by six feet or more. Similarly, unenclosed breezeways, and similar open structures connecting an exempt detached structure to the primary dwelling are not counted in the MPFA calculation.)

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**Solar Setback:**

*The setback provided from the north property line also complies with the required solar setback in the zone. The property slopes at three percent downhill to the north. The single story, 5:12 pitch roof that is more than six-feet from the north property line complies with the standards.*

**Orientation:**

*The proposed dwellings have traditional styling that reflects the character of the existing, eclectic historic development in the immediate vicinity. The poorly installed aluminum siding on the front residence is proposed to be removed and the “graceful swoop” that leads to a garden gate on the north side of the residence, exaggerating the roofline is proposed to be removed. The “swoop” facade is rotting, has a minimal foundation, and is in poor condition and of poor architectural taste.*

*The existing front door is not visible from the public right of way. The proposed floor plan modifications to duplex the residence provides two forward facing front doors, improving the orientation to the street. The new units are oriented towards the parking area. There are well-defined entrances on each unit to provide a nice front entry for the tenants.*

*The fixed pane picture window on the front of the residence is proposed to be modified to a fixed pane with two-single hung on either side to provide egress for the now bedroom space. The small fixed pane window on the north side of the residence is also proposed to be replaced with two side by side, single hung windows. The change will improve the rhythm of openings on the structure. No changes are proposed to the other existing residences. Along the alley, where the garage is proposed to be converted to a residence, two single hung windows are proposed. The garage doors are proposed to be retained. Behind the garage door on the south side where the new unit is proposed, a bike storage closet is proposed between the unit and the garage door.*

*The proposed duplex structure has traditional architecture and rhythms of opening with the window and door locations and styles.*

*The required open space area is 1,600 square feet. The proposed open spaces consist of approximately 150 square foot patios/yard space for each of new duplex units. The patios are more than six feet by eight feet within a ten-foot deep yard area. There is substantial lawn and vegetable garden areas proposed that provide more than 2,586 square feet of designated outdoor and recreation areas. More than 1,600 square feet in area has been provided in “functional” open space. Other landscape areas are provided that will have shrubbery, bark mulch and plantings.*

**Lot Coverage:**

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Proposed impervious areas including existing building footprints, proposed building footprints, pathways, driveway and parking area, decks (excluding 200 SF) is 15,000 SF of the 20,000 SF lot for a total lot coverage which is the allowed 75 percent coverage in the zone.

**Parking:**

The proposed development requires eleven (11) vehicle parking spaces.

Unit	Calculation	Required Parking
(1) 3-bedroom unit	2 x 1	2 spaces
(2) 2-bedroom units	1.75 X 2	3.5 spaces
(1) 1-bedroom > 500 SF	1.5 x 1	1.5 spaces
(4) 1-bedroom units/studio units < 500 SF	1 x 4	4 spaces
Total Required Spaces		11 spaces

The two-vehicle garage will be converted to a one vehicle garage. The second vehicle parking space is proposed on the driveway. There is an oversized garage and a carport adjacent to 160 North Laurel Street. The proposed parking lot has seven parking spaces proposed. This is equal to eleven parking spaces.

The new parking area is accessed from the alleyway. The parking area is proposed to have seven surface parking spaces including an ADA accessible space with an off-loading zone. There are no provisions for parking lot bioswales as the code specifies that the design standards for parking lots applies to parking areas of more than seven. Seven parking space are proposed.

Ten bicycle parking spaces are required. There are many secure locations for bicycle parking provided throughout the property. These include the creation of a bicycle closet behind the North Laurel Street facing garage door where the new apartment unit is proposed within ½ of the converted garage. There is a garage below the two-bedroom at 160 North Laurel Street that will accommodate two bicycle spaces. A new structure that will accommodate eight bikes is proposed on the north side of the parking area. All existing and proposed bicycle parking areas are covered.

**Energy Usage:**

All of the units within the proposed development will be constructed to the current energy codes and building code standards. A detailed analysis of the actual energy consumption has not been determined but the square footage, use of ductless heat pump systems, LED lighting all contribute to the low energy consumption anticipated in the duplex units. The new front residence duplexing and the converted garage apartment will be brought up to current building codes as required by the State of Oregon Residential Energy Standards.

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**B. Overlay Zones.** The proposal complies with applicable overlay zone requirements (part 18.3). *The property is in the Skidmore Historic District. Development is subject to AMC 18.4.2.050. The design complies with the applicable overlay zone requirements.*

**18.4.2.050 Historic District Development**  
**B. Historic District Design Standards.**

*The property is occupied by a historic, non-contributing, one-story three-bedroom residence, and three, detached, historic, non-contributing structures.*

*The front residence is proposed to be altered from its current substantially altered state to be more consistent with historic district design standards. Through the removal of the 'graceful swoop' structure extending from the north side to provide a 'garden wall', new siding that is historically consistent, new windows and doors that have a shape and area and design consistent with the design standards.*

*The new units can be historically compatible. The new manager office building is of the same aesthetic as the proposed duplex cottages.*

Height                      *The proposed structures are similar to average heights of structures in the vicinity. The proposed structures are substantially shorter than the historic contributing structures in the vicinity.*

Scale                        *The scale of the proposed duplex is within the range of other multi-family dwellings in the vicinity. The limited square footage of the structures at 924 square feet is in line with single family residence in the vicinity as well.*

Massing                    *Through the incorporation of a stepped gable with a hipped gable end, the ridge line of the duplex has been broken up to reduce the mass. The multiple gable ends and the incorporation of a covered porch, the mass of the duplex has been varied from the mass and scale of the existing structures.*

Setback                    *The setbacks for new construction comply with the standards of the zone.*

Roof                         *The proposed pitch on the new duplex structure is 5:12. The pitch is similar to the various roof pitches in the vicinity. The roof form is broken into a series of masses to reflect the subtle grade changes and to prevent a continuous ridge line. There are smaller gables for the porch roofs that add interest to the roof lines. The roof materials are proposed as composition shingles.*

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<u>Rhythm of Openings</u>	<i>The units have a consistently spaced window pattern that is consistent with the rhythm of openings found in the historic district. The windows are proposed as single hung, vinyl windows. The replaced windows on the front residence are more consistent with historic design standards and will improve the front elevation of the residence substantially.</i>
<u>Base or Platform</u>	<i>The duplex structures concrete foundation stem wall will be exposed for 6" to 18" as the units 'step' down the property. This provides a sense of a base and makes the structure appear grounded.</i>
<u>Form:</u>	<i>The form of the duplexes is consistent with multi-family dwelling development and is sensitive to the Skidmore Academy Historic District. The units are small and single story, reducing visual impacts.</i>
<u>Entrances</u>	<i>The front residence is proposed to have improved orientation to the street through the installation of the two front doors accessed via the front stoop. The other structures retain their existing entrances. Due to the substantial setback of the duplex units from the public streets, there is not a requirement for a visible entrance.</i>
<u>Imitation</u>	<i>The proposed new structure has elements of the existing historic contributing structure on the property, and updated siding, trim, door and window style and the proposed with the remodel to achieve the duplexing of the house bring more historic context and architectural elements that the existing siding and large pane window, and lack of orientation to the street.</i>  <i>The 5-inch cottage lap siding is proposed on the front duplexed residence and on the new duplex structure. The residence along the alley will retain their present, eclectic siding reflective of the period of development of the structures. The color will match or be complementary.</i>  <i>Though there are a variety of housing styles in the vicinity. Vernacular I-home, craftsman and American bungalow cottage style construction are found throughout the Skidmore Academy historic district. The proposed new units have elements of those design styles as well.</i>
<u>Garage Placement</u>	<i>The garage is existing, no changes to placement of garage are proposed.</i>

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### **C. Site Development and Design Standards.**

*The proposed site development complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*

*The proposed layout does not provide for any hidden areas that are not survey able by tenants of the site. The use of low-level lighting, low growing vegetation and open space orientation, the design provides areas of safety for the tenants.*

#### **Building Orientation.**

Building Orientation to Street. Dwelling units shall have their primary orientation toward a street. Where residential buildings are located within 20 feet of a street, they shall have a primary entrance opening toward the street and connected to the right-of-way via an approved walkway.

*The primary residence on the site has its primary orientation towards Laurel Street. The duplexing floor plan and proposed door locations facing Laurel Street improve the orientation to the public street. The duplex at the rear are oriented internally of the site.*

**Limitation on Parking between Primary Entrance and Street.** Automobile circulation or off-street parking is not allowed between the building and the street. Parking areas shall be located behind buildings, or on one or both sides.

*No new parking is proposed between the building and the street. All new parking is located to the internally of the property.*

**Build-to Line.** Where a new building is proposed in a zone that requires a build-to line or maximum front setback yard, except as otherwise required for clear vision at intersections, the building shall comply with the build-to line standard.

*All setbacks are existing for the existing residences and the detached garage. There are no changes to the front yard setback.*

**Garages. Alleys and Shared Drives.** Where a lot abuts a rear or side alley, or a shared driveway, including flag drives, the garage or carport opening(s) for that dwelling shall orient to the alley or shared drive, as applicable, and not a street.

*The garages on-site are existing. New vehicular access to the site is from the alley and not from the street. The property has historically utilized on-street, garage and driveway parking. The existing development will remain generally as is, utilizing the same parking as the historical configuration allowed. The garage facing Laurel Street that is being converted to a residential unit will retain the garage door to provide access to a bicycle parking space storage. The displace parking space will be provided for in the driveway.*

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**Building Materials.** Building materials and paint colors should be compatible with the surrounding area. Very bright primary or neon-type paint colors, which attract attention to the building or use, are unacceptable.

*The building materials are compatible with the surrounding area. The existing structures are proposed to be painted and resided as necessary. The front residence will have the existing siding removed and new, 5-inch cottage lap siding is proposed. Four-inch corner trim and window trim. The units are proposed to have concrete stem wall that forms the base. The units are proposed to have horizontal, cottage lap siding with five-inch exposure, and four-inch corner trim with six-inch fascia. Cottage style doors for the residences and the garage facing Laurel Street are proposed.*

*The roofing is proposed as composite shingles. The structures are proposed to be a shade of grey similar to the existing structures that are in the process of updating.*

**Streetscape.** One street tree chosen from the street tree list shall be placed for each 30 feet of frontage for that portion of the development fronting the street pursuant to subsection 18.4.4.030.E.

*The landscaping plan provides four new street trees. The street trees will be planted in accordance with the street tree standards from 18.4.4.030.E.*

**Landscaping and Recycle/Refuse Disposal Areas.** Landscaping and recycle/refuse disposal areas shall be provided pursuant to chapter 18.4.4.

*A common refuse area will be provided in a screened area adjacent to the covered bicycle parking structure proposed on the north side of the parking area.*

*There is an existing large stature shade tree to the west of the parking areas. A new tree will be planted in the landscape area new the north, new duplex unit.*

**Open Space.**

1. Recreation Area. An area equal to at least eight percent of the lot area shall be dedicated to open space for recreational use by the tenants of the development.

*An area of more than 1,600 square feet in area is devoted to the open space areas available for recreational use by the tenants.*

2. Surfacing. Areas covered by shrubs, bark mulch, and other ground covers that do not provide suitable surface for human use may not be counted towards this requirement.

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MAY 10 2019 Page 14 of 29

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*The areas that account for the open space and recreation areas are surfaced with paver stones, concrete and lawn areas. There designated vegetable garden areas proposed as well.*

3. Decks and Patios. Decks, patios, and similar areas are eligible for open space.

*The open spaces are a mixture of common patio area and semi-private outdoor yard areas and the lawn and garden areas proposed.*

4. Play Areas. Play areas for children are required for projects of greater than 20 units that are designed to include families. Play areas are eligible for open space.

*A play area is not provided in the eight-unit development.*

### **18.4.3.080 Vehicle Area Design**

#### **A. Parking Location**

*The proposed parking is not located between the building and the street. The parking is located internally of the site.*

**B. Parking Area Design.** Required parking areas shall be designed in accordance with the following standards and dimensions as illustrated in 18.4.3.080.B. See also, accessible parking space requirements in section 18.4.3.050 and parking lot and screening standards in subsection 18.4.4.030.F.

1. Parking spaces shall be a minimum of 9 feet by 18 feet.

*Three of the seven proposed parking spaces are 9 feet by 18 feet, one parking space has an ADA off-loading zone adjacent.*

2. Up to 50 percent of the total automobile parking spaces in a parking lot may be designated for compact cars. Minimum dimensions for compact spaces shall be 8 feet by 16 feet. Such spaces shall be signed or the space painted with the words "Compact Car Only."

*Three of the seven proposed parking spaces are 8 feet by 16 feet. They will be painted "compact car only".*

3. Parking spaces shall have a back-up maneuvering space not less than 22 feet, except where parking is angled, and which does not necessitate moving of other vehicles.

*All proposed parking area provides adequate back-up space of 22-feet. The garage parking and the carport parking that exist on site have been working for decades and no changes to the existing parking layout are proposed.*

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4. Parking lots with 50 or more parking spaces, and parking lots where pedestrians must traverse more than 150 feet of parking area...

*Not applicable.*

5. Parking areas shall be designed to minimize the adverse environmental and microclimatic impacts of surface parking through design and material selection as illustrated in Figure 18.4.3.080.B.5. Parking areas of more than seven parking spaces shall meet the following standards.

*The proposed parking area is for seven parking spaces, not more than seven. This code language does not apply.*

Shared Use of Driveways and Curb Cuts.

*The driveway to the existing street facing garage is a non-conforming situation. The new parking is proposed to be accessed via the public alley at the rear of the property.*

b. Developments subject to a planning action shall remove all curb cuts and driveway approaches not shown to be necessary for existing improvements or the proposed development. Curb cuts and approaches shall be replaced with standard curb, gutter, sidewalk, and planter/furnishings strip as appropriate.

*The Laurel Street curb cut is necessary for existing improvements and will be retained.*

c. If the site is served by a shared access or alley, access for motor vehicles must be from the shared access or alley and not from the street frontage.

*All new access is from the alley. The garage access from Laurel Street is pre-existing non-conforming in setback to the alley and that is cannot be removed to achieve parking in an alternative location on site since the structure will be retained as it is presently occupied. The existing upstairs unit has historically utilized on-street parking.*

*The new units will be served by the alley and all new access for motor vehicles will be from the alley and not from the street.*

5. Alley Access. Where a property has alley access, vehicle access shall be taken from the alley and driveway approaches and curb cuts onto adjacent streets are not permitted.

*No new curb cuts are proposed. The existing access from Central is pre-existing non-conforming. The new vehicle access to the parking lot area is from the alley.*

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**E. Parking and Access Construction.** The development and maintenance as provided below, shall apply in all cases, except single-family dwellings.

1. Paving. All required parking areas, aisles, turn-arounds, and driveways shall be paved with concrete, asphaltic, porous solid surface, or comparable surfacing, constructed to standards on file in the office of the City Engineer.

*The parking areas are proposed to be paved with concrete, asphaltic or porous solid surface depending on the storm water detention design.*

2. Drainage. All required parking areas, aisles, and turn-arounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.

*Drainage for the parking area will be provided to prevent sheet flow or drainage of waters into the public rights-of-way or onto abutting private property.*

3. Driveway Approaches. Approaches shall be paved with concrete surfacing constructed to standards on file in the office of the City Engineer.

*The existing driveway approach is pre-existing and is concrete. The driveway approach to the alley for the parking area will be paved to the standards on file in the City Engineer office.*

4. Marking. Parking lots of more than seven spaces shall have all spaces permanently and clearly marked.

*Though the parking area has seven spaces, since one is ADA and to provide better functionality, the parking spaces are proposed to be marked with paint or wheel stops.*

5. Wheel stops. Wheel stops shall be a minimum of four inches in height and width and six feet in length. They shall be firmly attached to the ground and so constructed as to withstand normal wear. Wheel stops shall be provided where appropriate for all spaces abutting property lines, buildings, landscaping, and no vehicle shall overhang a public right-of-way.

*Wheel stops, if provided, will comply with this section.*

6. Walls and Hedges

a. Where a parking facility is adjacent to a street, a decorative masonry wall or evergreen hedge screen between 30 and 42 inches in height and a minimum of 12 inches in width shall be established parallel to and not nearer than two feet from the right-of-way line.

*The parking is not adjacent to a public street.*

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City of Ashland

*A sight-obscuring fence is present on the north side property lines to screen the parking spaces from the adjacent residential property. Additionally, there is a substantial barrier of vegetated area proposed as tenant garden space and the covered bicycle parking and trash recycle structure. This structure is proposed to be setback from the property line not more than six feet, or in accordance with the necessary solar setback.*

#### **18.4.3.090 Pedestrian Access and Circulation**

*A continuous walkway system is proposed to extend throughout the development, and to existing public sidewalks. The walkway provides a safe, reasonably direct, and convenient walkway connection between primary building entrances and Laurel Street. The building entrances are connected to one another to the greatest extent practicable. The walkway connects the on-site parking areas, common areas, and connect to the public sidewalk on North Laurel Street.*

#### **18.4.4.030 Landscaping and Screening**

*The proposed landscaping plan and the irrigation plan that will be submitted with the building permits complies with the Irrigation and Water Conserving Landscaping requirements of the City of Ashland. The conceptual landscaping plan submitted with the application has been designed so that plant coverage of 50 percent after one year, and 90 percent within five years of planting is met. Two-inches of mulch will be provided in all non-turf areas after planting.*

*There are four street trees proposed in the park row. Irrigation will be extended to irrigate the trees.*

*No plant materials are proposed that prevent surveillance of the open space or the porches.*

*The new units will have recessed can light in the porch roof, or a shrouded yard light that provides down-lighting and security for the unit but will not directly illuminate adjacent properties. Any new exterior yard lighting will be shrouded in a manner to not directly illuminate adjacent properties.*

#### **Tree Preservation, Protection, and Removal**

**18.4.5.030 Tree Protection:** *There are two trees greater than six-inches in diameter at breast height. Both trees will have six-foot fencing installed in accordance the Tree Protection Plan installed prior to site disturbance for the new construction.*

*The trees along the property lines of the adjacent properties are protected by a six-foot tall solid panel fence. For the trees on the site, six-foot chain link panels are proposed to be installed at the dripline or in accordance with the tree protection plan that's has been provided with the application.*

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**D. City Facilities.** The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.

*Adequate city facilities exist to service the new units.*

**Sanitary Sewer** - *The property is currently served by a 10-in sanitary sewer main in North Laurel Street and a 6-in sanitary sewer main in the alley south of the project. There is adequate capacity in these lines to serve the property.*

**Water** - *The property is currently served by a 4-in water main in North Laurel Street. There are two, new water meters in the landscape island that is between the Laurel Street driveway and the alley. There are new water meters near the southeast corner of the property.*

**Storm Drainage** - *The property is currently served by a 12-in storm sewer main in North Laurel Street. Storm water collection, detention and treatment as required by the RVSS Storm Water Quality Design Manual with overflow to an approved system will be installed according to the design specifications of the project Civil Engineer.*

**Electrical** – *There is a quad panel electric meter panel on the east side of 158 Laurel Street. This serves 158 Laurel Street, the new duplex structure and an irrigation / common area meter.*

*The meter base on 166 Laurel Street will be converted to a duplex base and the meter for the new duplexing of the residence. The meter base at 166½ Laurel Street will also be converted to a duplex meter to serve the new apartment in the converted garage.*

*Laurel Street is paved with curb, gutter sidewalk and park row along the frontage of the property which provides paved access to the development. Street trees are proposed in the park row. The 16-foot wide alley to the south of the property is decomposed granite. The applicant request to not pave the alley per the request of adjacent neighbors.*

**E. Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.

1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or

2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

*No exceptions to the Site Development and Design Standards are proposed.*

#### **18.5.4.050 Conditional Use Permit Approval Criteria**

*The conditional use permit request is to exceed the Maximum Permitted Floor Area in the historic district by 24-percent. With a Conditional Use Permit up to a 25 percent increase is permitted. With the large lot, for the purposes of calculating MPFA, the property is heavily penalized, the lot area is reduced by more than 50 percent. With respect to development of multiple residential units, it would appear prudent to not penalize a property owner that is trying to create multi-family rental housing development, in the multi-family zone, preserving the historic structures that have long provided dwellings in the Railroad Historic District.*

*Existing structures consist of (areas for MPFA calculations only):*

*166 N Laurel: 1,534*

*166½ N Laurel: 1,169*

*160 N Laurel: 1,408*

*158 N Laurel: 1,060*

*Total: 5,171 square feet*

*Maximum Permitted Floor Area:*

*MPFA:*

*Lot Area: 20000*

*Adjusted Lot Area: 9400*

*Graduated FAR: 4888*

*CUP to Increase by 25 percent: 1222*

*Allowed MPFA w/ CUP: 6110*

*Existing Square footage: 5171*

*Proposed Additional Area: 924*

1. That the use would be in conformance with all standards within the zoning district in which the use is proposed to be located, and in conformance with relevant Comprehensive plan policies that are not implemented by any City, State, or Federal law or program.

*The use of the site is residential which is consistent with the relevant comprehensive plan policies and does not violate and city, state or federal law.*

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*The proposal furthers the purpose and intent of the Land Use Ordinance and provides for an efficient use of land in the high-density, multi-family, (R-3) zoning district. The proposal will not have negative impacts on adjacent uses to their right to open space for light and air. The development of multi-family units in the zone will not have an impact on the value of property. The preservation of the historic structures and addition of new, energy efficient, historically compatible rental units improve the aesthetic and visual qualities of the neighborhood. The proposal to increase the MPFA will not have negative impacts on public health, safety and welfare. The proposal is compatible, and livable, furthers the design standards of the City and provides for a needed housing type.*

*The proposal furthers the purpose and intent of the Historic District Development Standards. The proposal provides for preservation of existing historic structures while achieving recommended base density in the High Density, Multi-Family zoning district through the development of new units that are compatible in scale, mass, openings, materials and finishes as the structures on the subject and adjacent properties.*

*The proposal is supported by Goal I-1 of the Comprehensive Plan which seeks preservation of historically significant structures. Though the structures are not listed as contributing to the overall importance of the Skidmore Academy Historic District, they are historic in age and are to be preserved. The proposed modifications to the 156 Laurel Street are proposed to return the exterior structure to a more historically relevant façade, this is proposed through the removal of the swooping roofline.*

2. That adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the development, and adequate transportation can and will be provided to the subject property.

*As addressed above, there is adequate capacity of City facilities for water, sewer, electricity, urban storm drainage and paved access to and through the development.*

3. That the conditional use will have no greater adverse material effect on the livability of the impact area when compared to the development of the subject lot with the target use of the zone, pursuant with subsection 18.5.4.050.A.5, below. When evaluating the effect of the proposed use on the impact area, the following factors of livability of the impact area shall be considered in relation to the target use of the zone.

*The additional 924 square feet will not have a greater adverse material effect on the livability of the impact area when compared to the development of the site as eight residential units. The proposal complies with the target use of the zone, the proposal complies with the site design standards applicable in the zone.*

*The existing structures are at the perimeter of the property, adjacent to the alley. The physical locations of the structure provide ample lot area for additional dwelling units*

that comply with setbacks, on-site parking space requirements and adequate area for recreational open spaces.

*The detached accessory structure proposed is 396 square feet in area. According to the code, detached accessory structures when separated from residences by six-feet or more are not included in the MPFA.*

a. Similarity in scale, bulk, and coverage.

*The proposed duplexes are similar in scale, bulk and coverage as other multifamily developments in the zone. The proposed single-story structures are similar in height as the multi-family development near the alleys in the vicinity. All of the separations between buildings including setbacks and lot coverages are met which provides for a reduced bulk and coverage. The other structures exist on the site and retain their scale, bulk and coverage.*

b. Generation of traffic and effects on surrounding streets. Increases in pedestrian, bicycle, and mass transit use are considered beneficial regardless of capacity of facilities.

*The square footage proposed to exceed the maximum permitted floor area will not have an impact on the generation of traffic. The use of the property is multi-family as envisioned in the code. Due to the restrictive MPFA calculation, the density of the property is less than allowed by code. The proposal seeks to to achieve the allowed density on the property. The new units are small and have adequate parking proposed. The property is within walking distance of downtown, grocery and other retail stores, restaurants and bar. The employment center of town and near the bus route on N Main Street. The units are in a walkable neighborhood that has sidewalks throughout.*

c. Architectural compatibility with the impact area.

*The proposed new dwellings have traditional styling that reflects the character of the historic contributing structures in the vicinity. None of the structures on the subject property are historic contributing. Alternatively, numerous aspects of the previous exterior details including doors, windows and siding material are proposed to be modified as part of this proposal to improve the historic compatibility of the structures.*

*There are varying roof forms, with the stepping down the slight grade to break up the gable line and the mass of the structures. The proposed porches add architectural interest and add variation to the horizontal plane. There are numerous windows to allow for ample natural light into the units. The homes in the impact area are a mixture of vernacular, ranch style, bungalow and craftsman. The proposed new construction has elements of each of those design styles.*

d. Air quality, including the generation of dust, odors, or other environmental pollutants.

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*The additional square footage for the development of the site will not impact the air quality in the impact area any more than the allowed multi-family development of the site at seven units.*

e. Generation of noise, light, and glare.

*The proposed increase in square footage will not impact the generation of light, glare or noise any more than the allowed multi-family development of the site with seven units would.*

f. The development of adjacent properties as envisioned in the Comprehensive Plan.

*The additional 924 square feet in excess of the MPFA will not prevent the multi-family development of the adjacent properties.*

g. Other factors found to be relevant by the approval authority for review of the proposed use.

*We believe that all of the standards for the development of the site have been met.*

4. A conditional use permit shall not allow a use that is prohibited or one that is not permitted pursuant to this ordinance.

*The conditional use to increase the MPFA by 924 square feet is an allowable use in the zone.*

**Conclusion:**

In conclusion, the property owner finds that the proposed development complies with the standards for multi-family development and the permitted uses in the R-3 zone. The units proposed are a desirable size for individuals, and some couples comfortable with the small unit size. The property is near downtown, and there is an array of amenities provided within walking distance of the neighborhood. According to Walkscore, the property is Very Walkable and has a score of 85. <https://www.walkscore.com/score/156-n-laurel-st-ashland-or-97520>

Overall, the applicants find the preservation of the existing structures and the future development of an eight-unit, multi-family residential development with historically compatible new construction is a great redevelopment of an under density, multi-family lot.

The proposed units are small in area and footprint. The units are clearly secondary to the primary residence. The units are proposed to have minor details reflective of historic contributing structures

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with historically appropriate rhythms of openings and windows and door areas. With the number of units proposed within the allowed density and the historically appropriate proposed design, the request for a minor provision of additional floor area is reasonable. The proposed development is similar in bulk, scale and coverage as other multi-family developments in the vicinity.

The applicant finds that all of the applicable City of Ashland requirements have been met or can be met through the imposition of conditions of approval.

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**Attachments**

1. Historic Resources Inventory
2. Structure Photographs
3. Site Plan
4. Elevations
5. Tree Plan
6. Landscaping Plan

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**National Register of Historic Places Skidmore Academy Historic District Inventory**  
**28.0**

**TONNER, A E & VIVIAN HOUSE c 1920**  
**166 LAUREL ST N 391E04CC 3400**  
**Other: Altered Historic Non-Contributing**

Although not entirely clear, this much altered single-story dwelling is apparently based upon an early structure that was owned by A. E. and Vivian Tonner as early as 1928. (JCD 173:397). County Assessors files date construction at 1920. The Tonners retained possession at least through 1934 and the house was standing, though no occupant was listed, in the 1948 directory. While the original configuration is unclear, a central gable volume (with internal chimney) has been augmented by two gable wings creating a basic "U-shaped plan. Several window modifications and the application of aluminum siding detract from the structures original character. A separate one and one-half story rental unit is located to the rear, accessed from the alleyway, and was probably built during the post-WWII era.

**28.0 [B]:** Several additional structures are located to the rear of the Tonner House, along the alley that divides this block perpendicular to Laurel Street. The two-story rental volume immediately behind the Tonner House is visible in the photograph of the main dwelling while a third, single-story gable, volume identified as "158 North Laurel" is documented in a second image under this identification number.

Each of these resources, of uncertain but possibly pre-1949 construction, may well have been in place by the end of the period of significance but none currently reflect any presumed historic appearance. Not counted for the purposes of Section 5, they are considered additional historic, non-contributing, elements on the Tonner House site.

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Figure 3: 166 North Laurel Street

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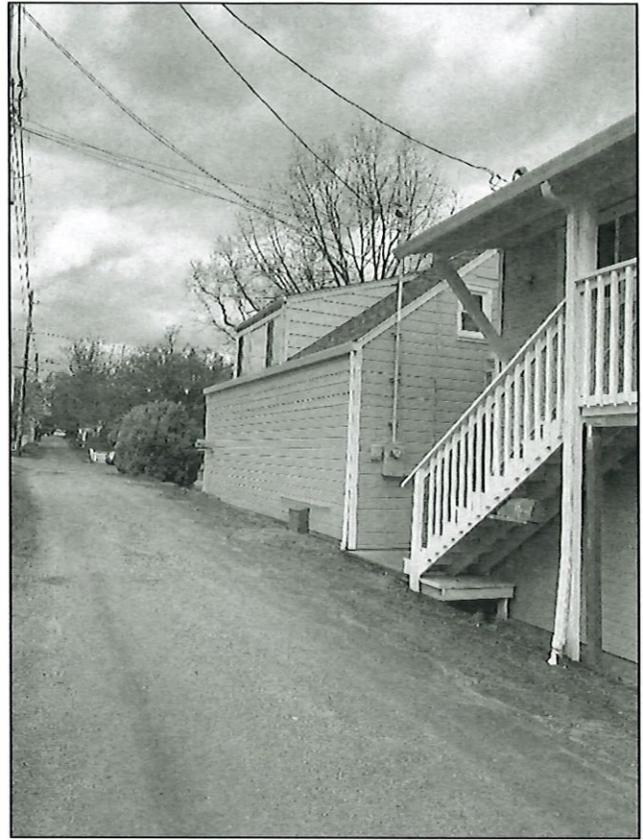
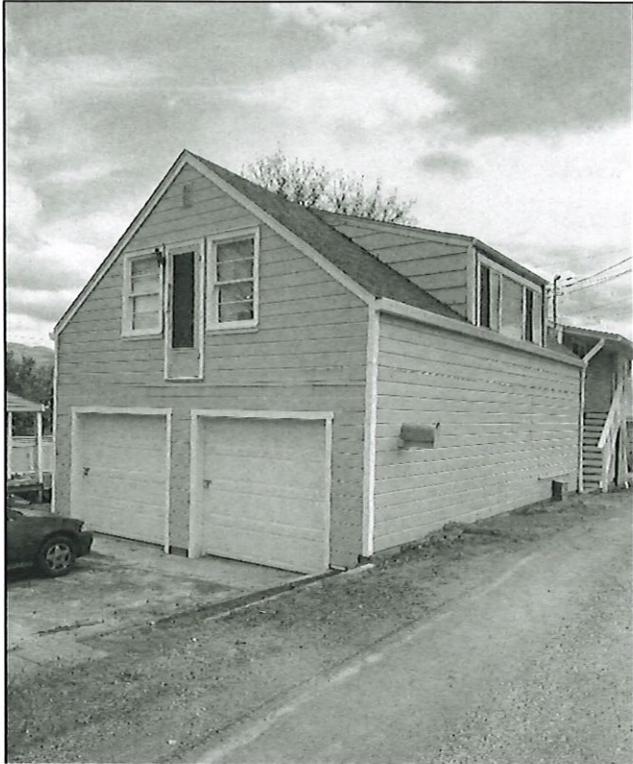


Figure 4: 166 1/2 North Laurel Street

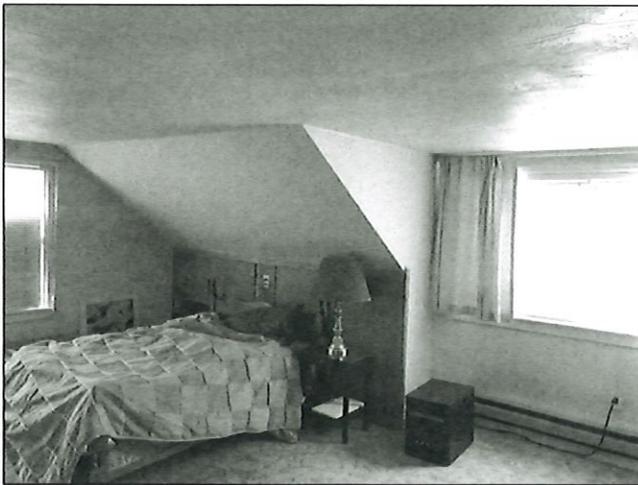


Figure 5: Interior photos of 166 1/2 North Laurel Street

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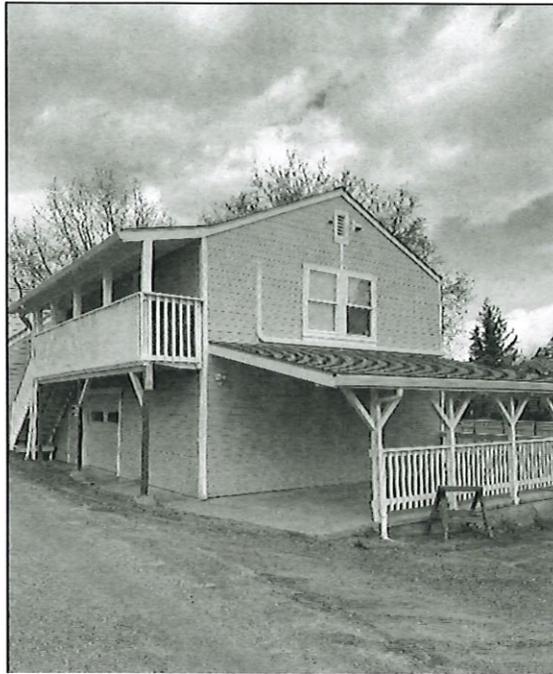


Figure 6: 160 North Laurel Street

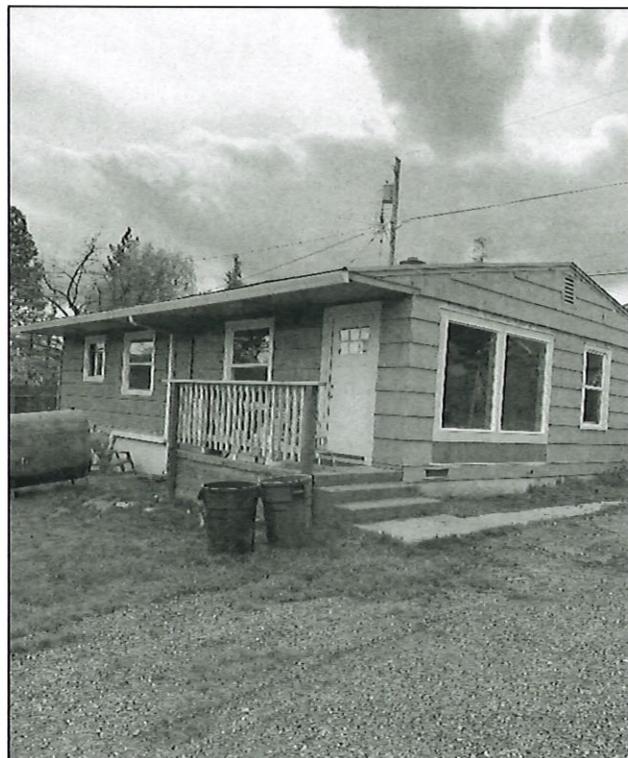
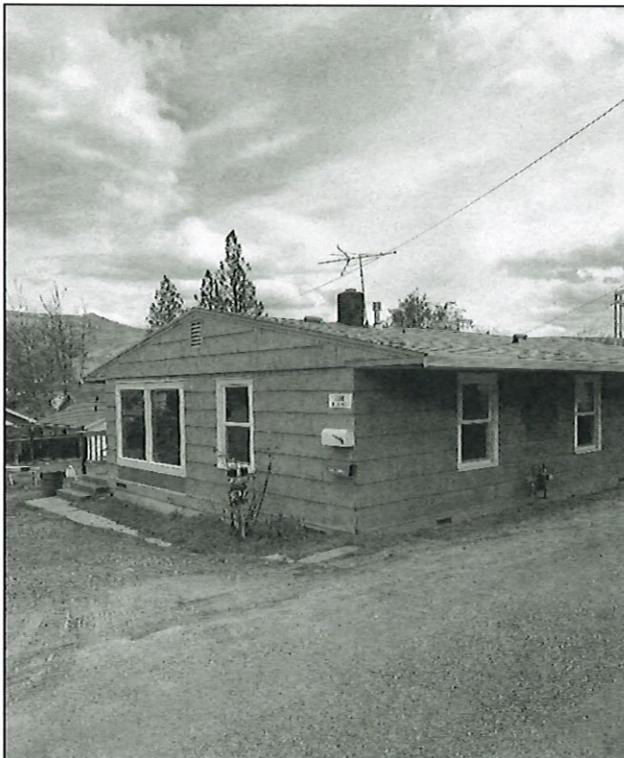


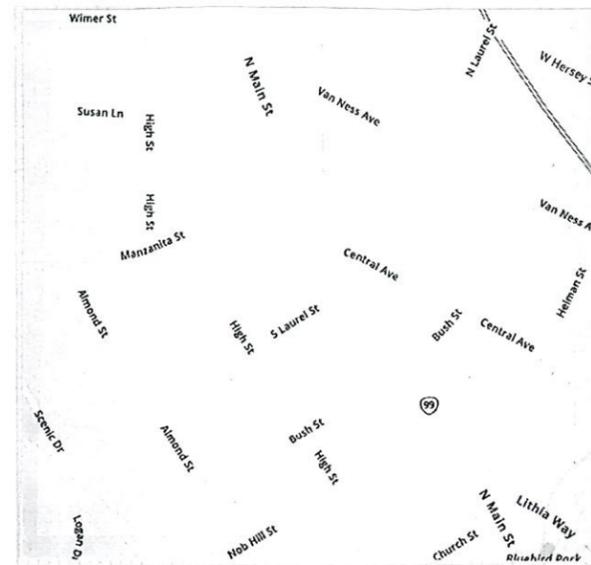
Figure 7: 158 North Laurel Street

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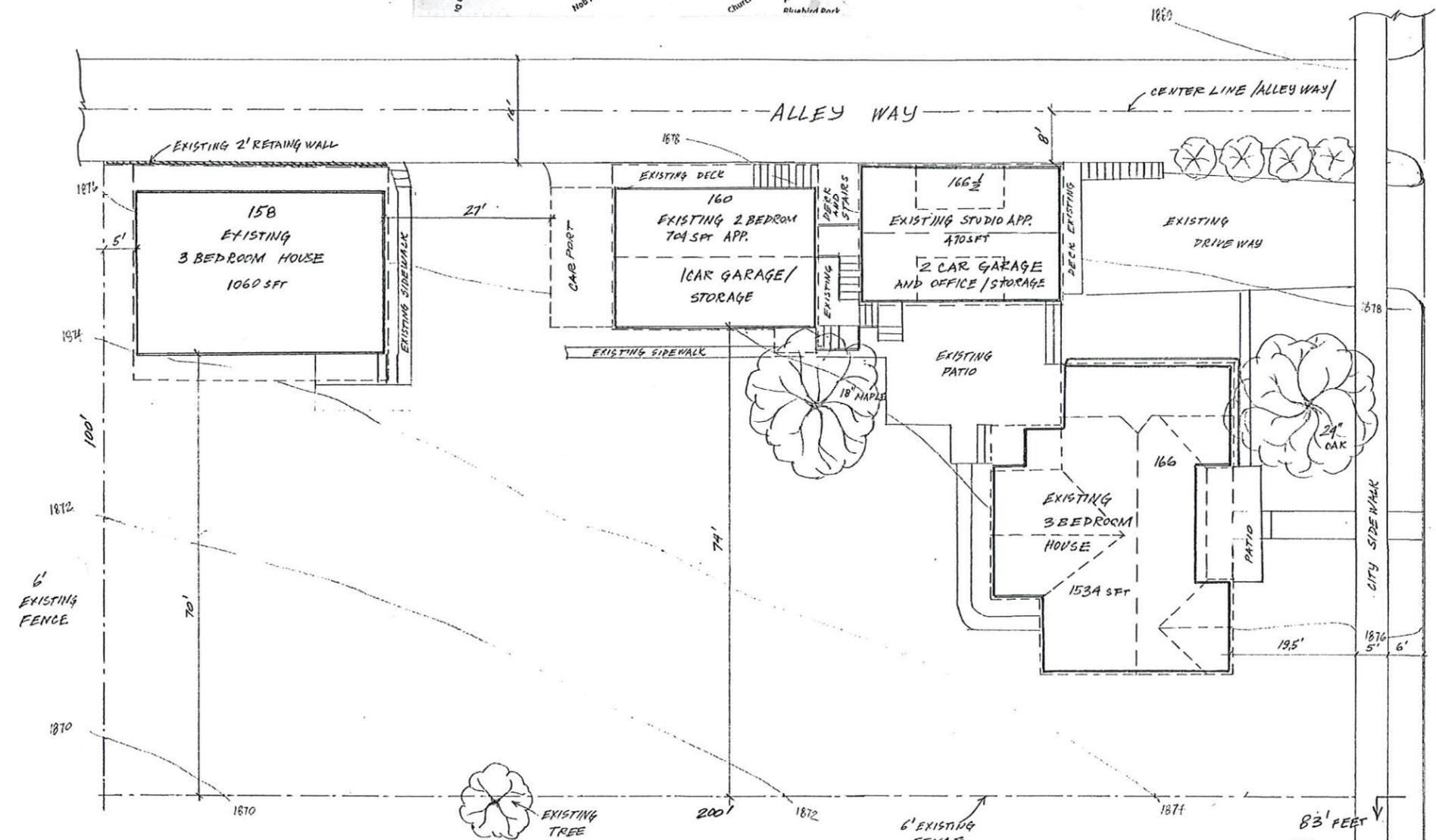
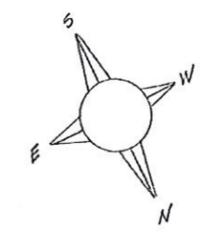
OWNER: K. LOCKLIN ; V. AGARHAWA

166; 166 1/2; 160; 158 LAUREL ST ASHLAND OR 97520

Date	4/13/19
Scale	1"=1'-0"
Drawn	VA
Job	LRL
Sheet	1
Of	Sheets



VICINITY MAP



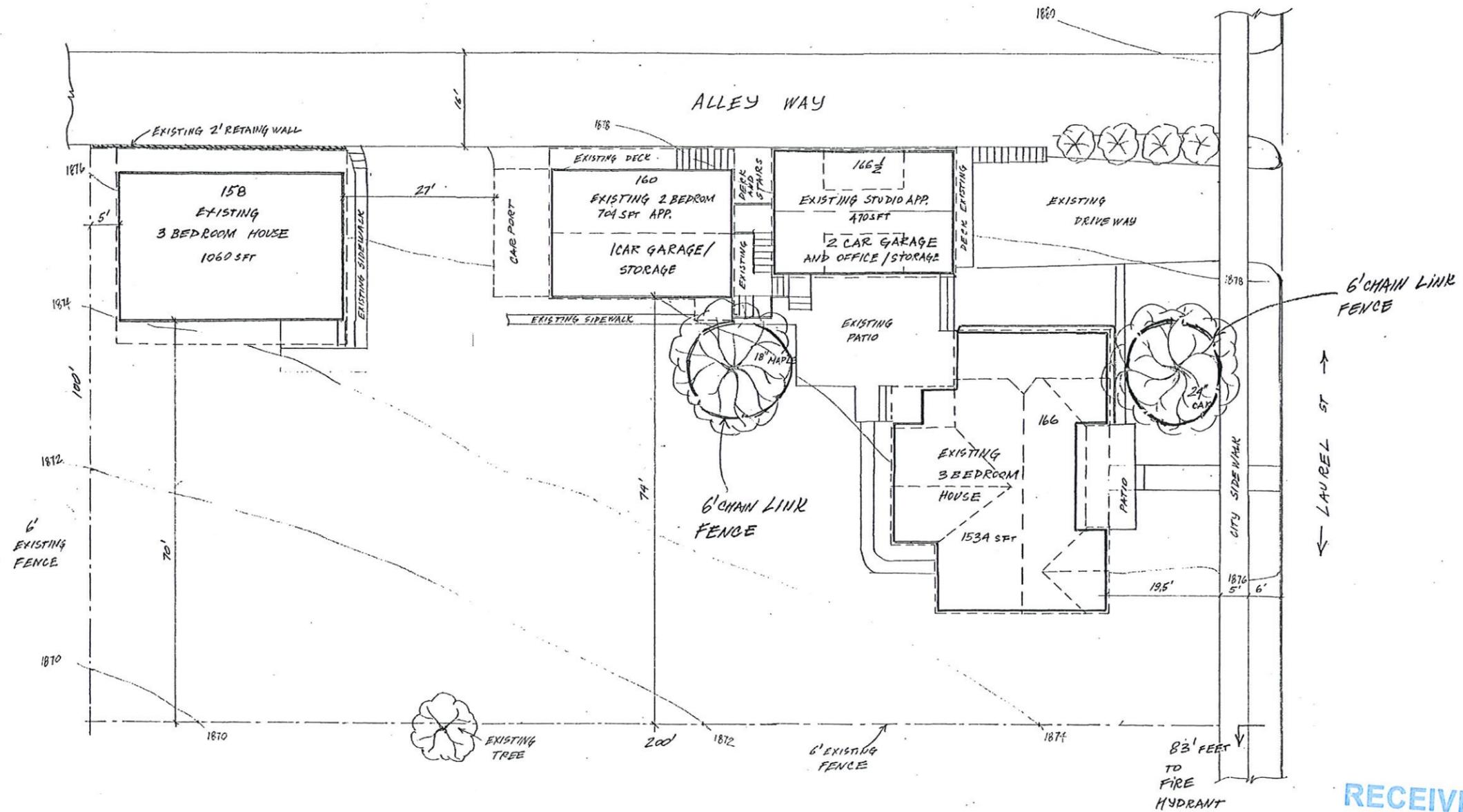
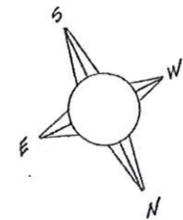
PLOT PLAN  
EXISTING STRUCTURES

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**MAY 10 2019**  
City of Ashland  
SCALE: 1"=1'-0"

83' FEET TO FIRE HYDRANT

# TREE PROTECTION PLAN

SCALE: 1" = 1'-0"



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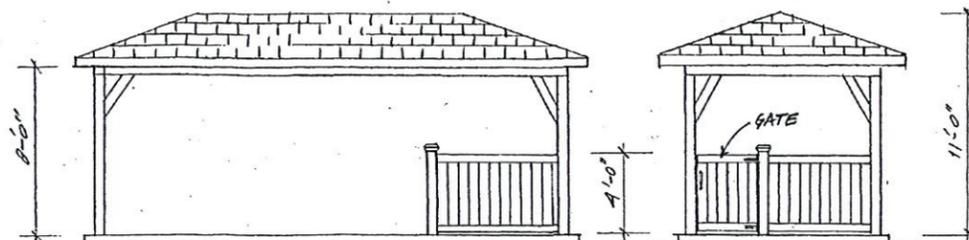
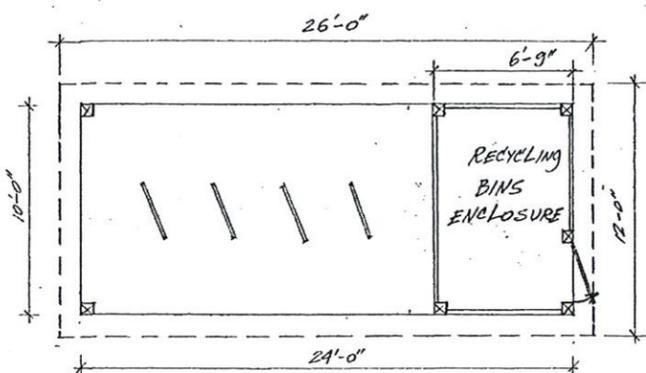
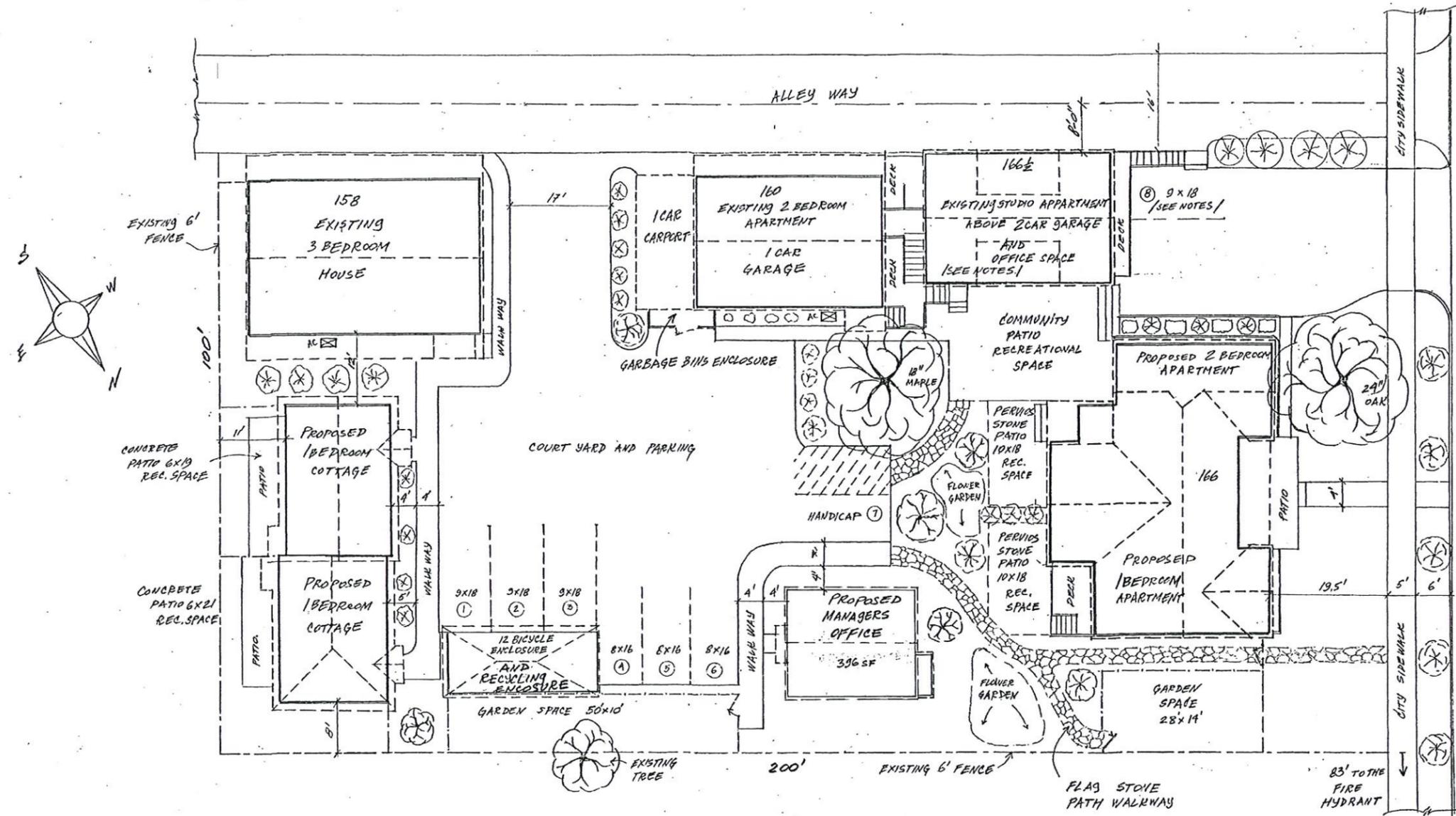
REVISIONS	BY

166; 166 1/2; 160; 158 LAUREL ST ASHLAND OR 97520  
 OWNER: K. LOCKLIN, V. ASATHANAY

Date	4/14/18
Scale	
Drawn	
Job	
Sheet	2
Of	Sheets

NOTE: 166½ EXISTING DOWN STAIRS 2 CAR GARAGE AND STORAGE/OFFICE SPACE TO BE CONVERTED TO 372 SF ONE(1) BEDROOM, ONE (1) BATH APARTMENT AND BICYCLE STORAGE SPACE 45SF AND ONE (1) CAR GARAGE SEE PROPOSED FLOOR PLAN.

SOUTH SIDE PORTION OF THE DRIVEWAY IN FRONT OF 166½ BECOME A PARKING SPACE #B



RECYCLING AND 1. BICYCLE ENCLOSURE / PLAN AND SIDE VIEWS/

SCALE: 1/4" = 1'-0"

SITE PLAN  
(PROPOSED)  
SCALE: 1" = 1'-0"

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OWNER: V. AGARHANEY  
166, 166½, 160, 158 LAUREL STREET ASHLAND

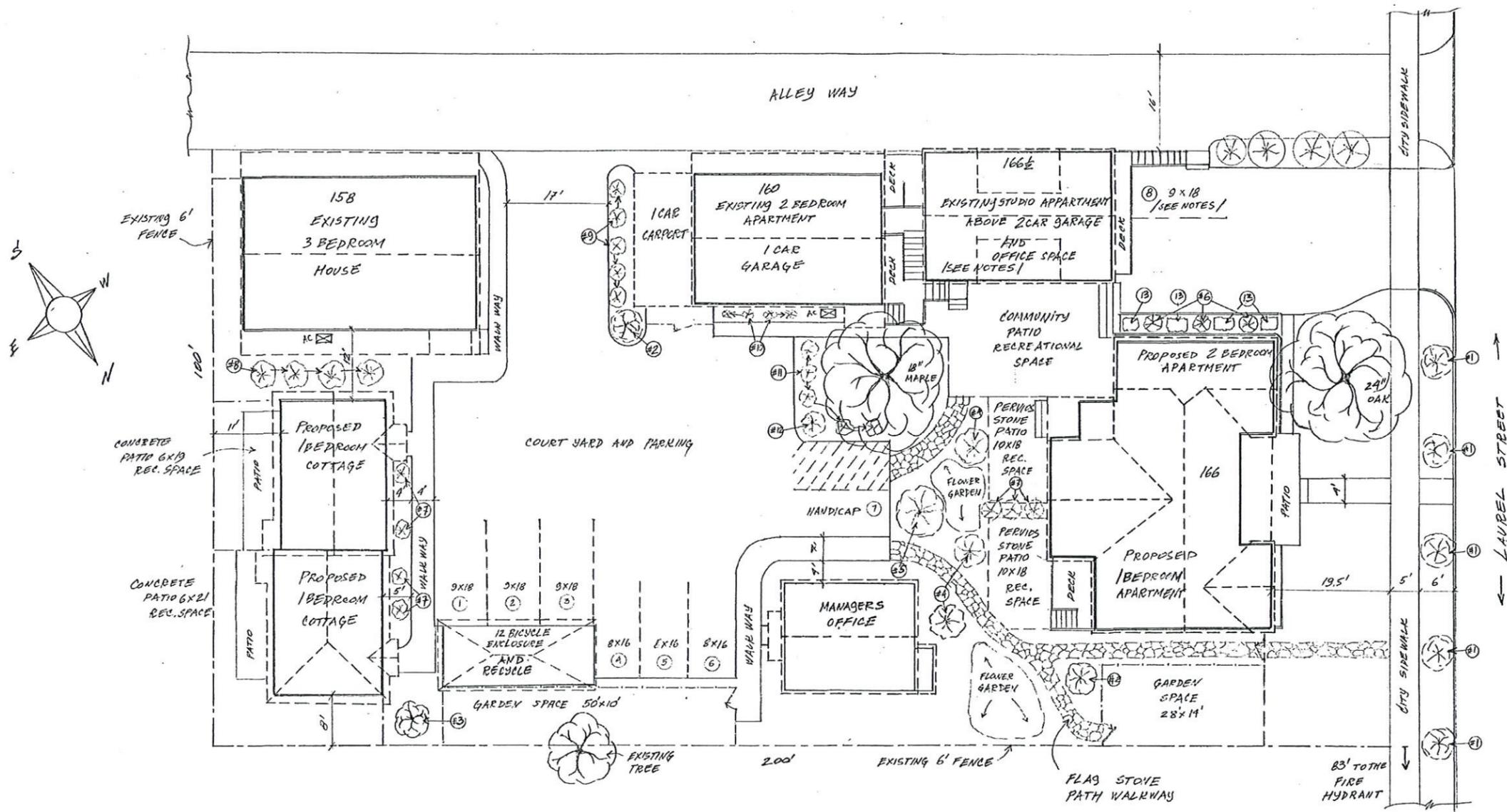
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Of	Sheets

REVISIONS	BY

OWNER: V. AGARHANGI

166; 166½; 160; 158 LAUREL STREET ASHLAND

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Of	Sheets



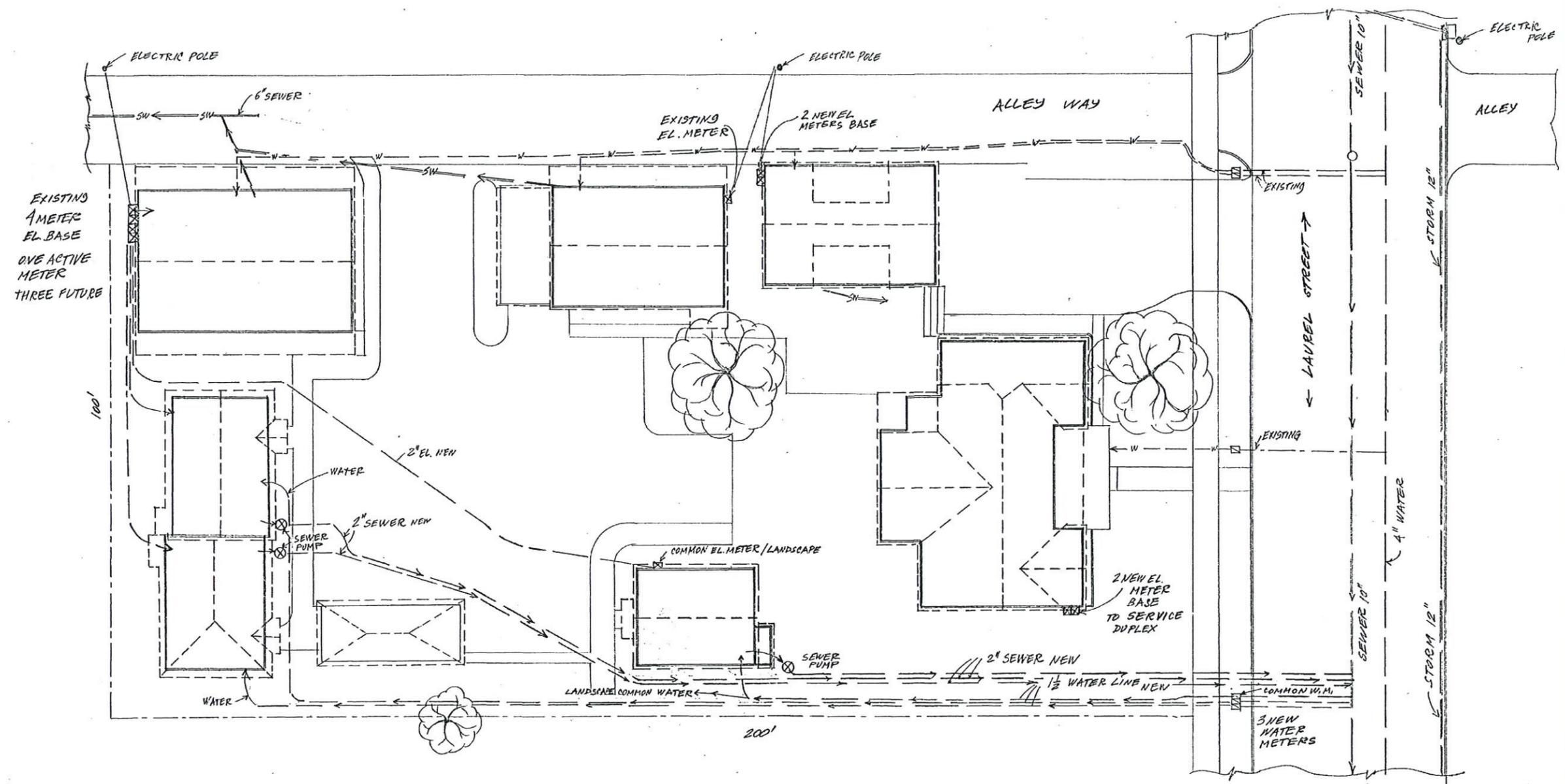
TREES AND SHRUBS

- |  |                               |
|--|-------------------------------|
| # 1 ROYAL BURGUNDY CHERRY /PRUNUS SERRULATA/ | # 8 RODODENDRON MACROPHYLLUM  |
| # 2 APOLLO MAPLE /ACER SACCHARUM/            | # 9 LAVENDER /HIDCOTE/        |
| # 3 RUGGED CHARM MAPLE /ACER TATARICUM/      | # 10 AZALEA                   |
| # 4 FIRST BLUSH CHERRY /PRUNUS/              | # 11 RODODENDRON /SEMI DWARF/ |
| # 5 RED ROCKET MAPLE /ACE RUBRUM/            | # 12 DOGWOOD /CHEROKEE CHIEF/ |
| # 6 ITALIAN CYPRESS                          | # 13 BOX WOOD                 |
| # 7 ARBORVITAE /EMERALD GREEN/               |                               |

LANDSCAPE PLAN

SCALE: 1" = 1'-0"

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EXISTING AND PROPOSED UTILITIES

SCALE: 1" = 1'-0"

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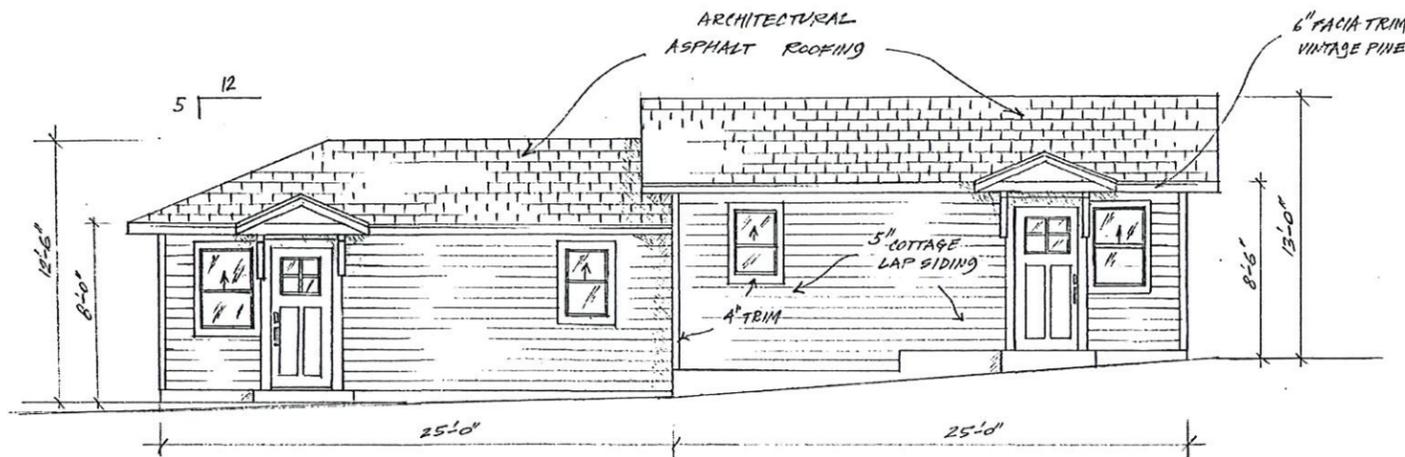
166 LAUREL ST EXISTING AND PROPOSED UTILITIES

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Job	LRL
Sheet	5
Of	Sheets

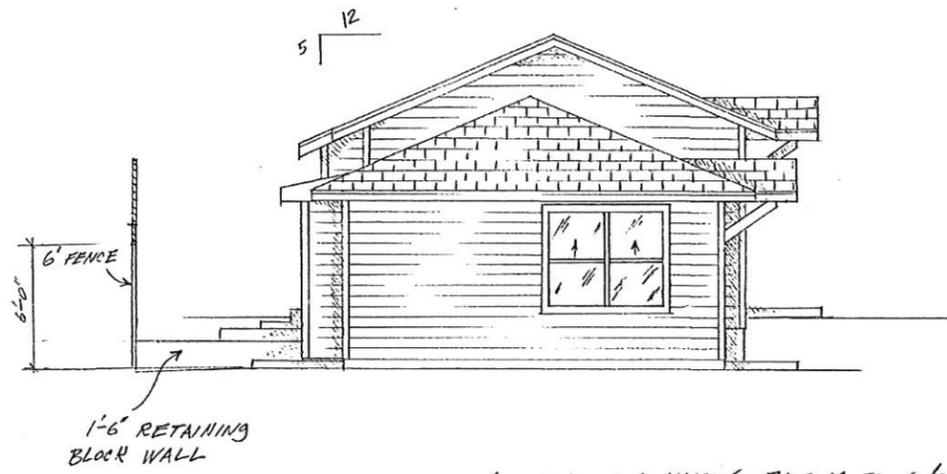




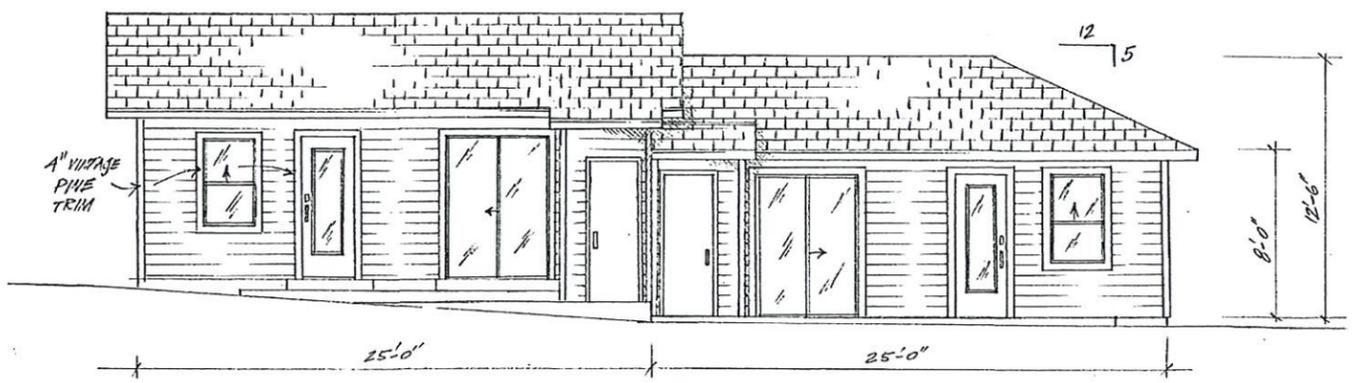
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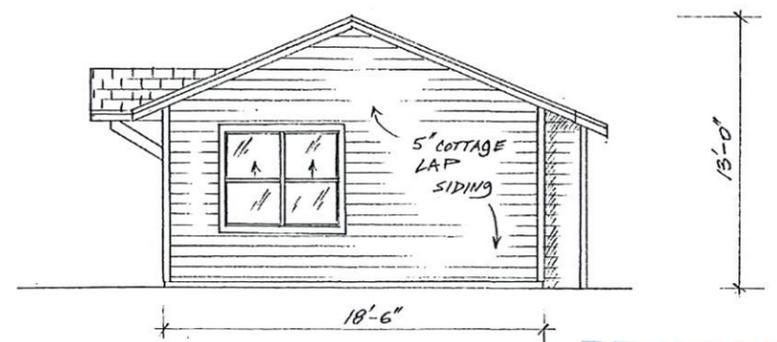
WEST SIDE VIEW ELEVATION /DUPLEX/  
COTTAGES # 1 AND #2



NORTH SIDE VIEW ELEVATION /DUPLEX/



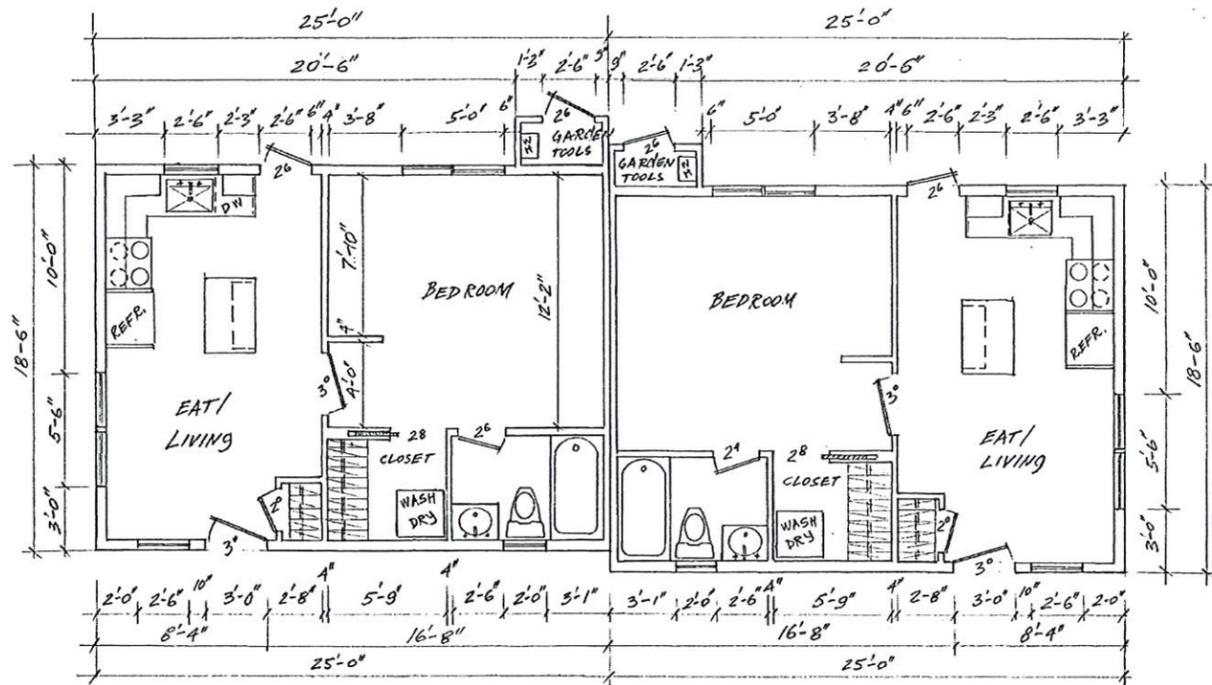
EAST SIDE VIEW ELEVATION /DUPLEX/  
COTTAGES # 1 AND #2



SOUTH SIDE VIEW ELEVATION /DUPLEX/

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SCALE: 1/4" = 1'-0"

166-158 LAUREL ST ASHLAND PROPOSED DUPLEX  
 Date 4/6/19  
 Scale 1/4" = 1'-0"  
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 Job LRL  
 Sheet 8  
 Of 8 Sheets



MAIN FLOOR PLAN / DUPLEX /

COTTAGE # 1 462 SF

COTTAGE # 2 462 SF

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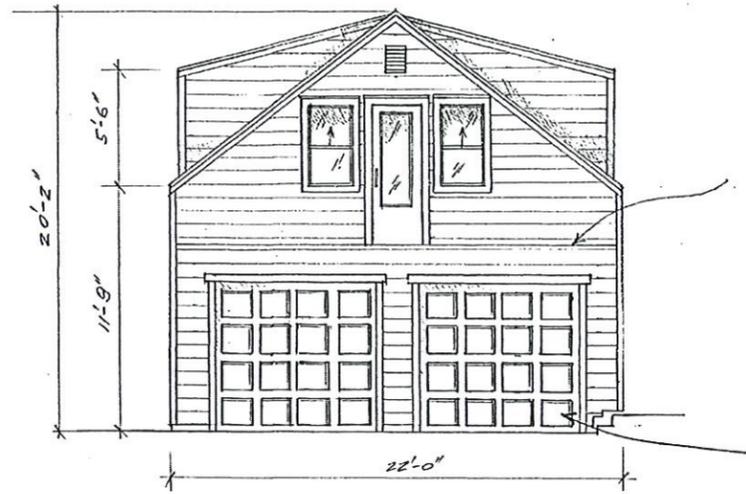
PROPOSED DUPLEX FLOOR PLAN

160-158  
LAUREL ST ASHLAND

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MAY 10 2019  
City of Ashland

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Of	Sheets

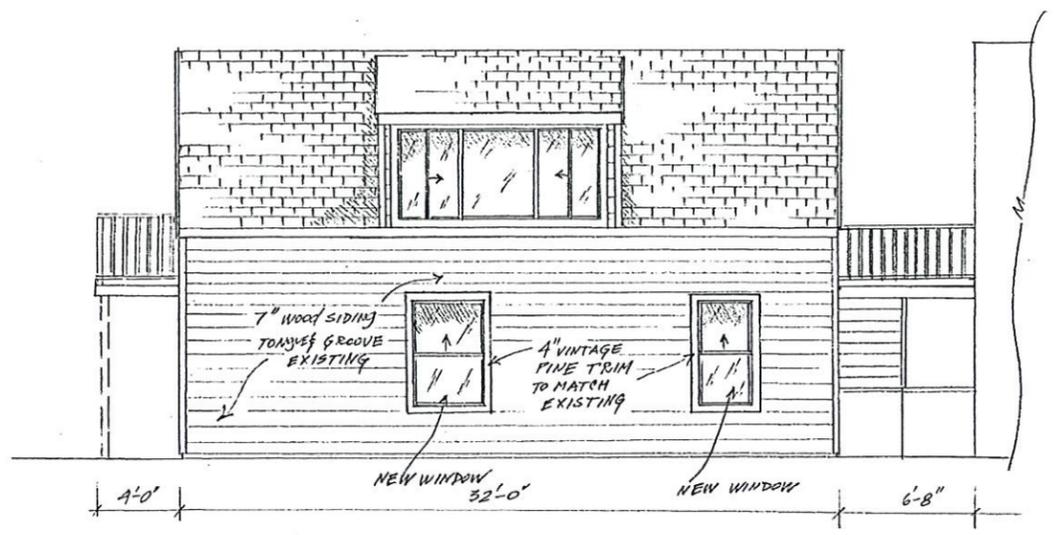
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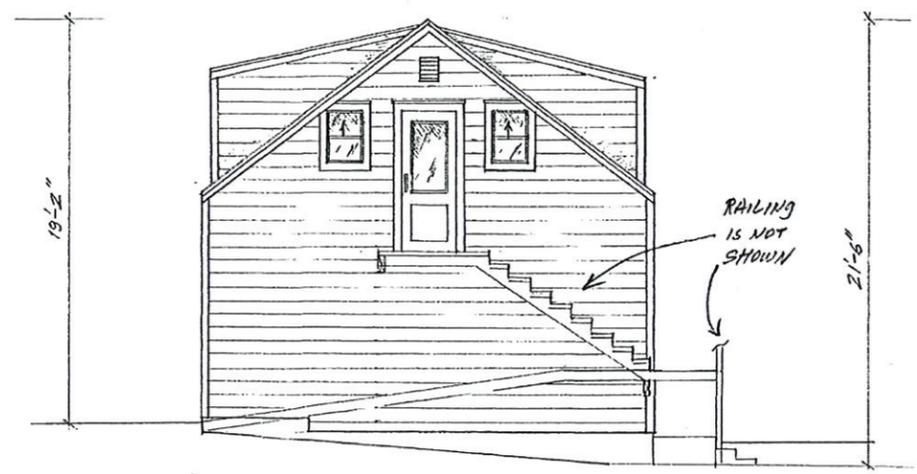
WEST SIDE ELEVATION /PROPOSED/

NOTE:  
REPLACEMENT DECK  
/IS NOT SHOWN/  
IN PLACE OF ORIGINAL  
OLD DECK.  
ORIGINAL OLD DECK  
WAS REMOVED IN SEP 2018  
DUE TO STRUCTURAL  
INSTABILITY AND WAS  
IN DANGER OF COLLAPSE

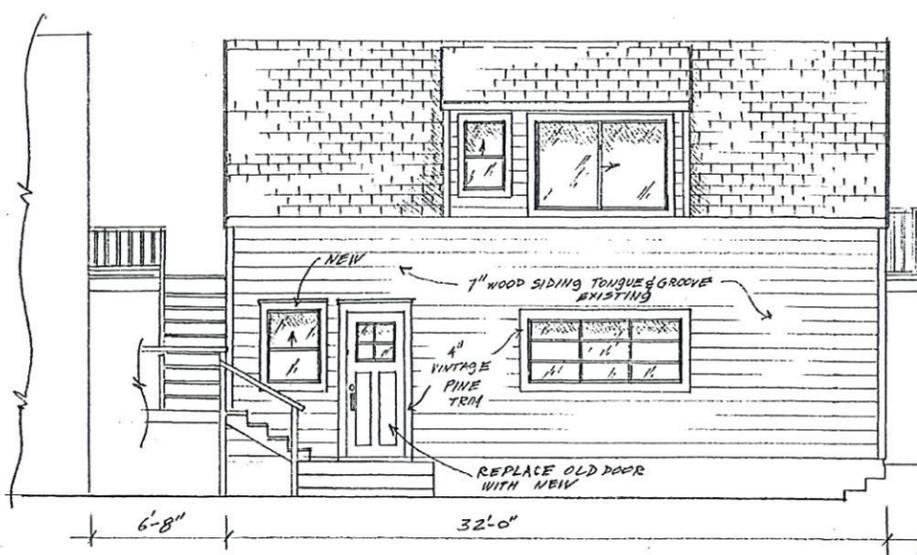
BICYCLE STORAGE WITH  
CAR PARKING ON THE DRIVEWAY  
IN FRONT OF IT.



SOUTH SIDE ELEVATION /PROPOSED/



EAST SIDE ELEVATION /PROPOSED/



NORTH SIDE ELEVATION /PROPOSED/

NOTE:  
REPLACEMENT DECK  
IN PLACE OF ORIGINAL  
OLD DECK.  
ORIGINAL DECK  
WAS REMOVED  
OCTOBER 2018  
DUE TO STRUCTURAL  
INSTABILITY AND  
WAS IN DANGER OF  
COLLAPSE.

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MAY 10 2019

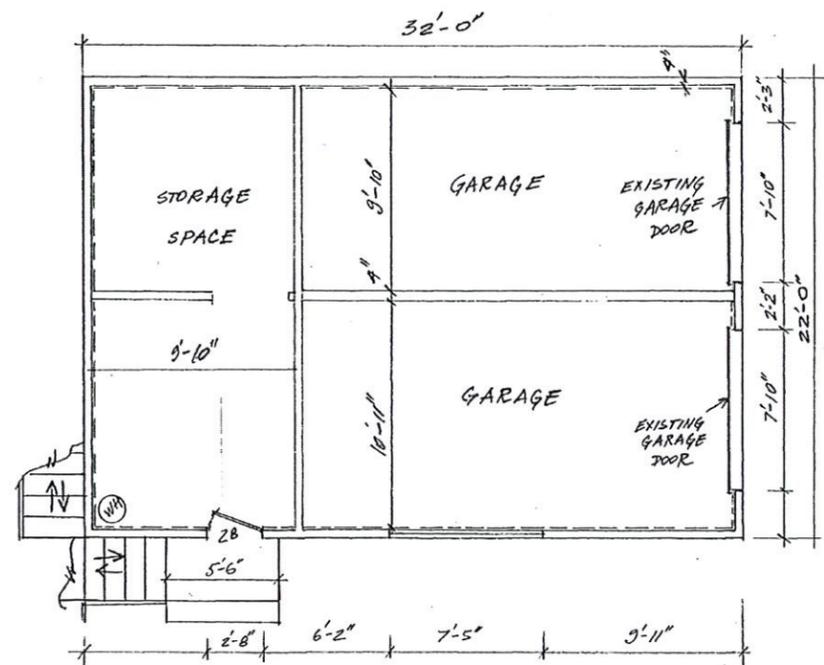
SCALE: 1/4" = 1'-0" City of Ashland

PROPOSED PARTIAL CONVERSION OF EXISTING  
2 CAR GARAGE INTO LIVING SPACE /APARTMENT/

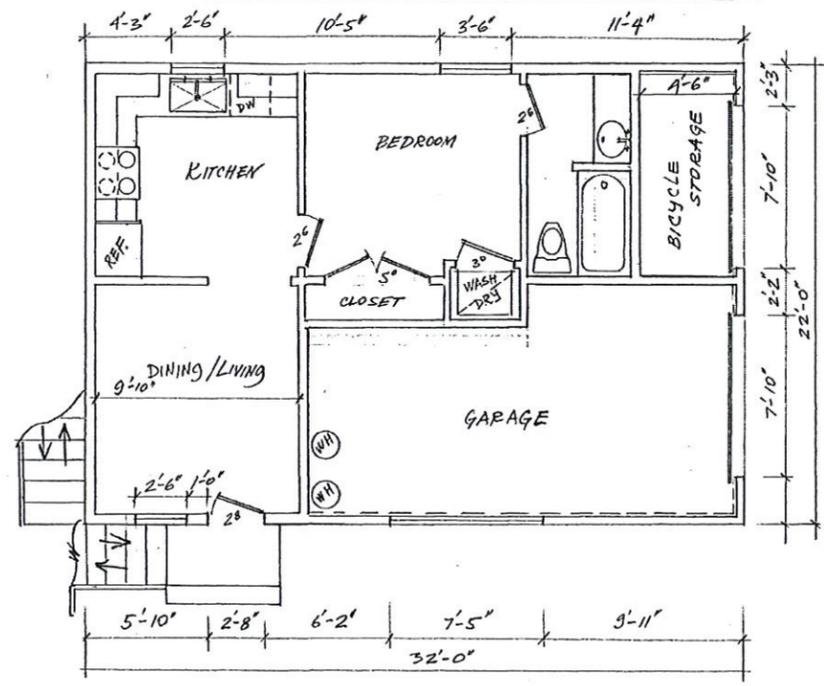
166 1/2 LAUREL ST ASHLAND

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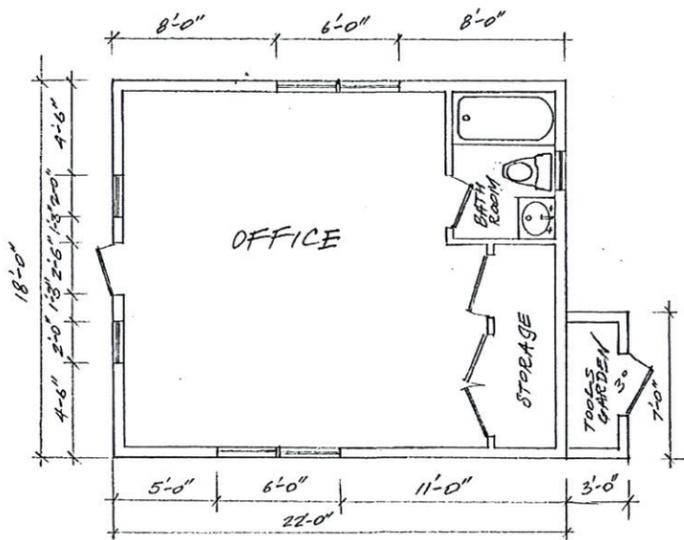
LOWER LEVEL EXISTING GARAGE STRUCTURE



LOWER LEVEL PROPOSED LIVING SPACE /APARTMENT/ AND GARAGE  
 LIVING SPACE 372SF  
 SCALE: 1/4" = 1'-0"

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 City of Ashland

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Sheet	//
Of	Sheets

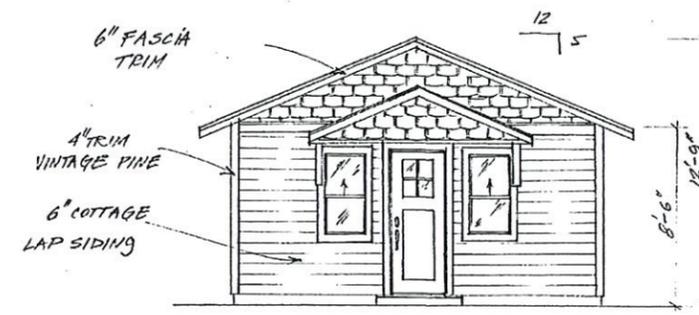


MANAGERS OFFICE FLOOR PLAN

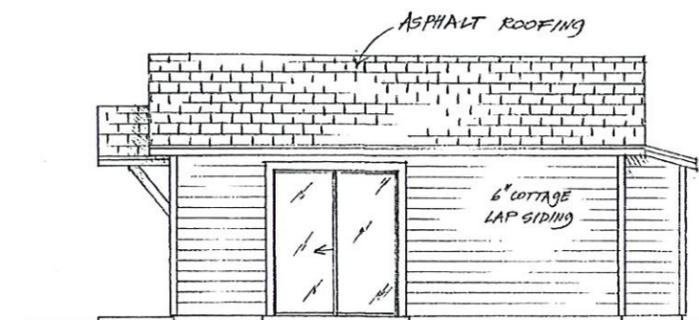
396 SFT OFFICE

SCALE: 1/4" = 1'-0"

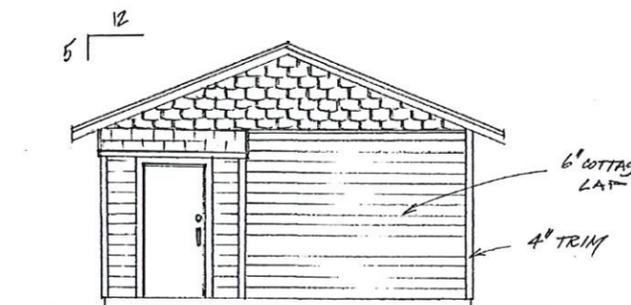
21 SFT TOOL SHED



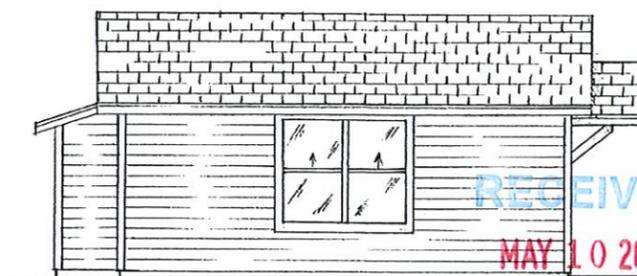
EAST VIEW ELEVATION



NORTH VIEW ELEVATION



WEST VIEW ELEVATION



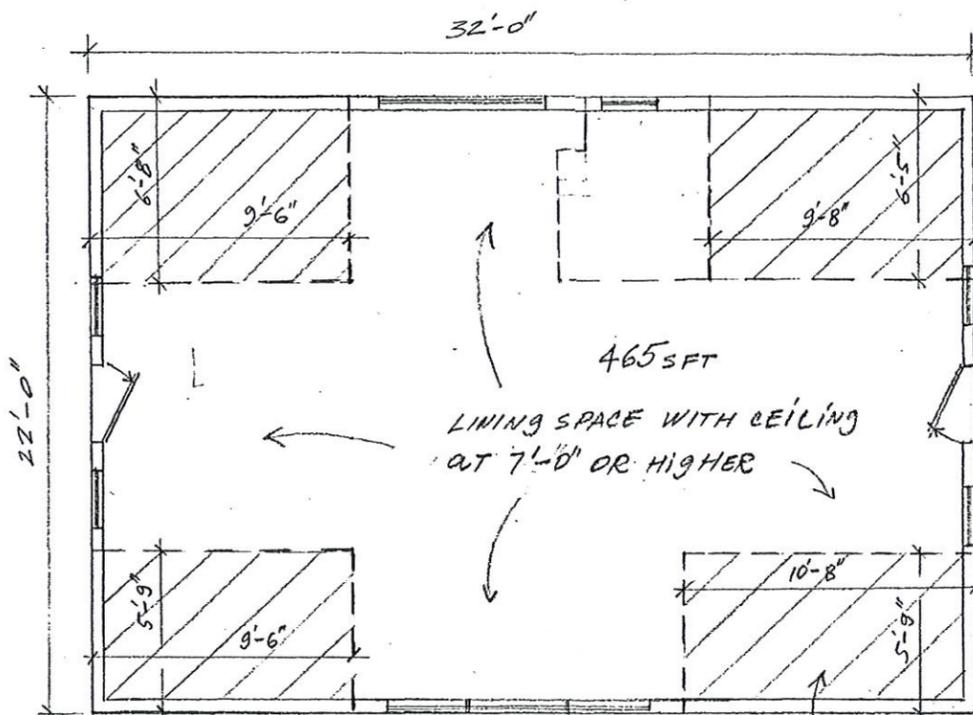
SCALE: 1/4" = 1'-0"

SOUTH VIEW ELEVATION

REVISIONS	BY

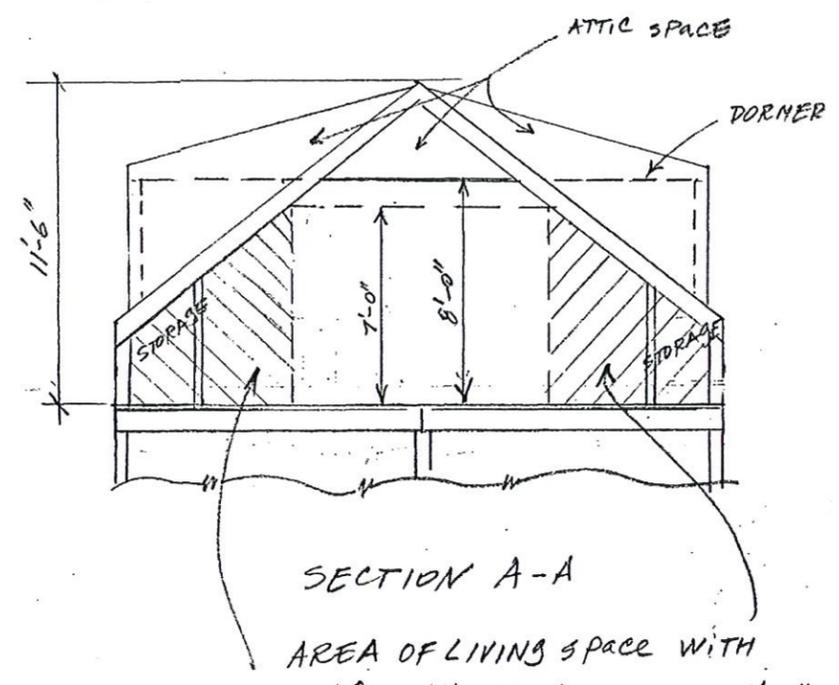
166 - 158 LAUREL ST ASHLAND  
MANAGERS COTTAGE / OFFICE

Date	
Scale	
Drawn	
Job	
Sheet	12
Of	Sheets



CEILING PLAN

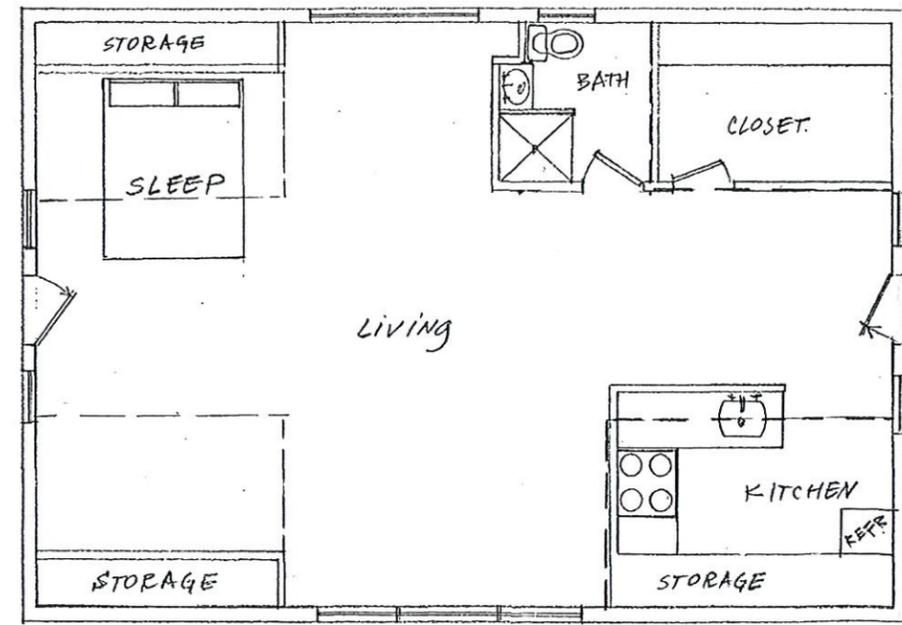
465 SF OF LIVING SPACE WITH CEILING HEIGHTS @ 7'-0" OR HIGHER



SECTION A-A

AREA OF LIVING SPACE WITH CEILING HEIGHTS LESS THAN 7'-0"

SCALE: 1/4" = 1'-0"



FLOOR PLAN

SCALE: 1/4" = 1'-0"

REVISIONS	BY

FLOOR PLAN  
 CEILING HEIGHTS PLAN  
 166 1/2 LAUREL ST ASHLAND

Date	
Scale	
Drawn	
Job	
Sheet	
Of	
Sheets	

**RECEIVED**  
 MAY 10 2019  
 City of Ashland



**PLANNING ACTION:** PA-T2-2019-00008

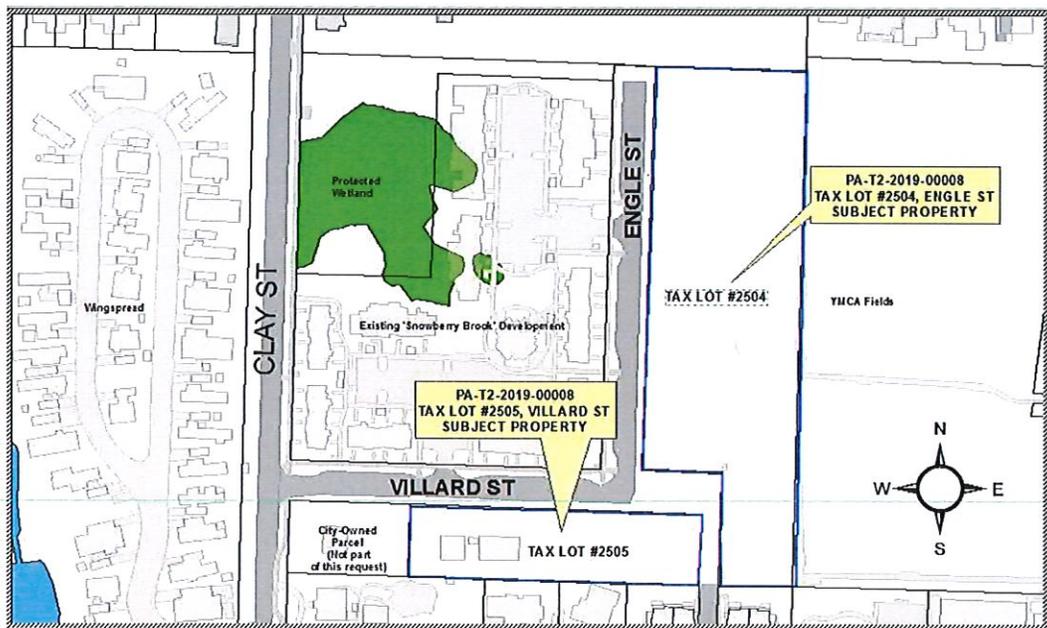
**SUBJECT PROPERTY:** Map 39 1E 11C Tax Lot #2504 on Engle St/Map 39 1E 11C Tax Lot #2505 on Villard St.

**OWNER/APPLICANT:** Housing Authority of Jackson County/HAJC Development/Dan Horton, Architect

**DESCRIPTION:** A request for Site Design Review approval to allow the construction of a for 60-unit multi-family development on two tax lots (#2504 & #2505) along Villard and Engle Streets as Phase II of the existing 'Snowberry Brook' development. The proposal consists of four two-story eight-plex apartment buildings and seven two-story townhouse four-plexes. Units will consist of ten one-bedroom flats, 12 two-bedroom flats, ten three-bedroom flats, and 28 two-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar (*cedrus deodara*) which the project arborist describes as posing a hazard. **COMPREHENSIVE PLAN DESIGNATION:** Low Density, Multi-Family Residential; **ZONING:** R-2; **ASSESSOR'S MAP:** 39 1E 11C; **TAX LOT:** 2504 & 2505.

**NOTE:** The Ashland Tree Commission will also review this Planning Action on **Thursday, June 6, 2019 at 6:00 PM** in the Community Development and Engineering Services building (Siskiyou Room), located at 51 Winburn Way.

**ASHLAND PLANNING COMMISSION MEETING:** *Tuesday, June 11, 2019 at 7:00 PM, Ashland Civic Center, 1175 East Main Street*



Notice is hereby given that a PUBLIC HEARING on the following request with respect to the ASHLAND LAND USE ORDINANCE will be held before the ASHLAND PLANNING COMMISSION on meeting date shown above. The meeting will be at the ASHLAND CIVIC CENTER, 1175 East Main Street, Ashland, Oregon.

The ordinance criteria applicable to this application are attached to this notice. Oregon law states that failure to raise an objection concerning this application, either in person or by letter, or failure to provide sufficient specificity to afford the decision maker an opportunity to respond to the issue, precludes your right of appeal to the Land Use Board of Appeals (LUBA) on that issue. Failure to specify which ordinance criterion the objection is based on also precludes your right of appeal to LUBA on that criterion. Failure of the applicant to raise constitutional or other issues relating to proposed conditions of approval with sufficient specificity to allow this Commission to respond to the issue precludes an action for damages in circuit court.

A copy of the application, all documents and evidence relied upon by the applicant and applicable criteria are available for inspection at no cost and will be provided at reasonable cost, if requested. A copy of the Staff Report will be available for inspection seven days prior to the hearing and will be provided at reasonable cost, if requested. All materials are available at the Ashland Planning Department, Community Development and Engineering Services, 51 Winburn Way, Ashland, Oregon 97520.

During the Public Hearing, the Chair shall allow testimony from the applicant and those in attendance concerning this request. The Chair shall have the right to limit the length of testimony and require that comments be restricted to the applicable criteria. Unless there is a continuance, if a participant so requests before the conclusion of the hearing, the record shall remain open for at least seven days after the hearing.

In compliance with the American with Disabilities Act, if you need special assistance to participate in this meeting, please contact the City Administrator's office at 541-488-6002 (TTY phone number 1-800-735-2900). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting. (28 CFR 35.102.-35.104 ADA Title I).

If you have questions or comments concerning this request, please feel free to contact the Ashland Planning Division, 541-488-5305.

## SITE DESIGN AND USE STANDARDS

### 18.5.2.050

The following criteria shall be used to approve or deny an application:

- A. **Underlying Zone:** The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.
- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

## TREE REMOVAL PERMIT

### 18.5.7.040.B

1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
2. **Tree That is Not a Hazard.** A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.
  - b. Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.
  - c. Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.
  - d. Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures of alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.
  - e. The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.

# Snowberry Brook II

**PA-T2-2019-00008** – A request for Site Design Review approval to allow the construction of a for 60-unit multi-family development along Villard and Engle Streets as Phase II of the existing ‘Snowberry Brook’ development. The proposal consists of four 2-story eight-plex apartment buildings and seven 2-story townhouse four-plexes comprised of ten 1-bedroom flats, 12 2-bedroom flats, ten 3-bedroom flats, and 28 2-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar which is described as posing a hazard, and two additional Almond trees due to their condition and location.



## Proposal Details

### Proposal

The application proposes density bonuses both for providing affordable housing and for constructing all of the proposed units to Earth Advantage® Gold standards.

### Site Description

The subject property consists of two tax lots: Tax Lot #2504 is a generally rectangular, 2.57 acre parcel along Engle Street, and Tax Lot #2505 is a rectangular 0.78 acre parcel along Villard Street. The parcels are separated by a narrow, 24- to 26-foot wide, L-shaped section of right-of-way for the future extension of McCall Drive, an alley, from its current terminus to connect with Villard Street. Until recently, there were two structures along Villard: a 930 square foot garage and a 1,785 square foot barn. The Building Division approved a demolition request based on the buildings’ poor condition, and both buildings were recently demolished.

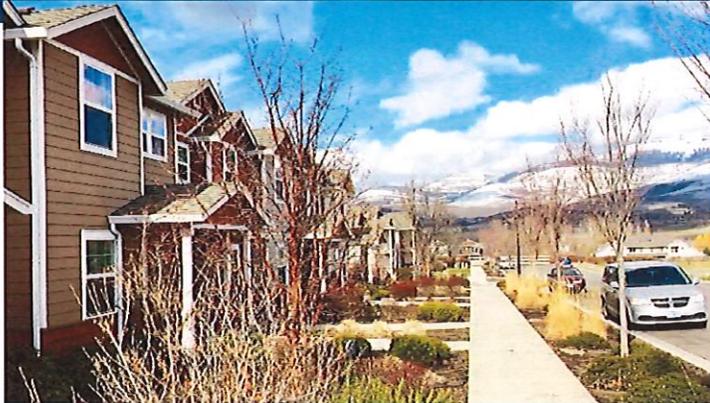
### Landscaping & Trees

The application identifies five trees on the subject property: a 20-inch diameter Oak behind proposed Building H, a 24-inch Deodar Cedar near the proposed parking lot at the southeast corner, two scrub Almonds in the 13-14 inch range near the Deodar, and a 12-inch diameter Cedar near the southeast corner of the property along Villard.) In addition, five trees are noted on adjacent properties within 15 feet of the property line. The remainder of the property is generally devoid of natural features other than native grasses.

The application proposes to remove the 24-inch d.b.h. Deodar Cedar as a hazard tree. The project arborist notes that the tree is in poor condition, with massive historical damage including multiple damaged leaders, no dominant leader and severe damage to large lower branches, and an 18-inch deep trench cutting through the root zone within three feet of the trunk. There are two smaller trees immediately adjacent to this tree, described as scrub Almonds on the Tree Protection Plan, that are also proposed for removal. The project arborist notes that these trees are not in good shape, with dead branches and evidence of prior severe pruning. Both Almonds are located in the path of the proposed new sidewalk along Villard Street.

# Snowberry Brook II

**PA-T2-2019-00008** – A request for Site Design Review approval to allow the construction of a for 60-unit multi-family development along Villard and Engle Streets as Phase II of the existing ‘Snowberry Brook’ development. The proposal consists of four 2-story eight-plex apartment buildings and seven 2-story townhouse four-plexes comprised of ten 1-bedroom flats, 12 2-bedroom flats, ten 3-bedroom flats, and 28 2-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar which is described as posing a hazard, and two additional Almond trees due to their condition and location.



## Key Issues

### Landscaping & Trees (cont'd)

Ashland’s Water Resources map identifies a small “possible wetland” just northeast of the current terminus of McCall Drive. The applicant has retained the services of Schott & Associates who prepared a wetland delineation report that determined there were no jurisdictional wetlands or other waters of the state within the study area. The Oregon Department of State Lands (DSL) has concurred with this determination. AMC 18.3.11.020 provides that no plan amendment is required to correct the Water Resources map where DSL has concurred with the findings, and as such staff will initiate correction of the Water Resources map to remove the “Possible Wetland” identified here.

### Streets

**Clay Street** is a residential neighborhood collector street, and is a County road in this vicinity. City standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb, seven-foot parking bays on each side, and nine- to ten-foot travel lanes on each side. The city-standard cross-section includes a 22- to 34-foot curb-to-curb paved width within a 49- to 61-foot right-of-way, *dependent on the on-street parking configuration*. Frontage improvements including additional paving, curbs, gutters, a bio-swale parkrow planting strip and sidewalk were installed to meet these standards along the Snowberry I frontage, and the property to the north, with the initial Snowberry development.

**Engle and Villard Streets** are residential neighborhood streets, and city standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb and seven-foot parking bays on each side, with an 11- to 14-foot queuing travel lane. The city standard cross-section includes a 25- to 28-foot curb-to-curb paved width in a 50- to 55-foot right-of-way. The existing improvements consist of paving, curb, gutter, sidewalks and parkrow on the west side of Engle and the north side of Villard along Snowberry I, and the applicant proposes to complete the corridors by installing sidewalks and parkrow planting strips along the Snowberry II frontages.

# Snowberry Brook II

**PA-T2-2019-00008** – A request for Site Design Review approval to allow the construction of a for 60-unit multi-family development along Villard and Engle Streets as Phase II of the existing ‘Snowberry Brook’ development. The proposal consists of four 2-story eight-plex apartment buildings and seven 2-story townhouse four-plexes comprised of ten 1-bedroom flats, 12 2-bedroom flats, ten 3-bedroom flats, and 28 2-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar which is described as posing a hazard.

## Streets (cont’d)

**McCall Drive** is an alley as envisioned with its creation in PA #2013-00104, and right-of-way has already been dedicated to complete connect the existing terminus of McCall to Villard. The alley connection will be completed with the current application, and at a minimum pedestrian, bicycle and emergency vehicle connectivity provided. The applicant will work with the Transportation Commission and Public Works Department to determine whether vehicular connectivity will be required.

## Traffic

A Traffic Impact Analysis (TIA) prepared by S.O. Transportation Engineering, LLC concludes that the proposed 60-unit development can be approved without adverse impacts to the transportation system. The TIA recommends limiting McCall Drive to bicycles, pedestrians and emergency vehicles, rather than regular vehicles, to avoid cut-through traffic.

## Parking

The application as proposed requires 105 off-street parking spaces. The applicant proposes to provide 86 off-street parking spaces in surface parking lots, and to utilize 19 on-street parking credits for the remaining required parking. 90 bicycle parking spaces are required, and the applicant proposes to provide 90 covered bicycle parking spaces in shelters on site.

## Density

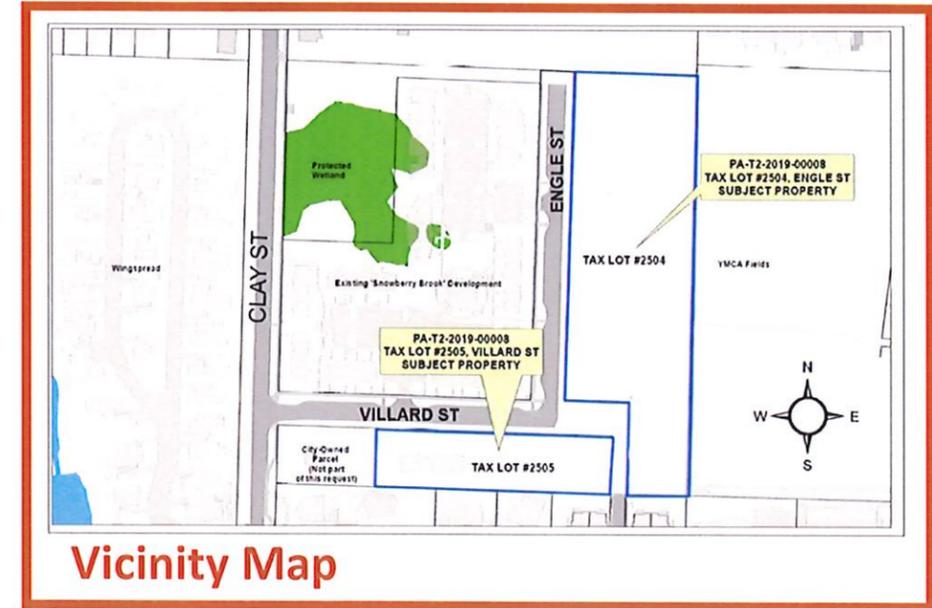
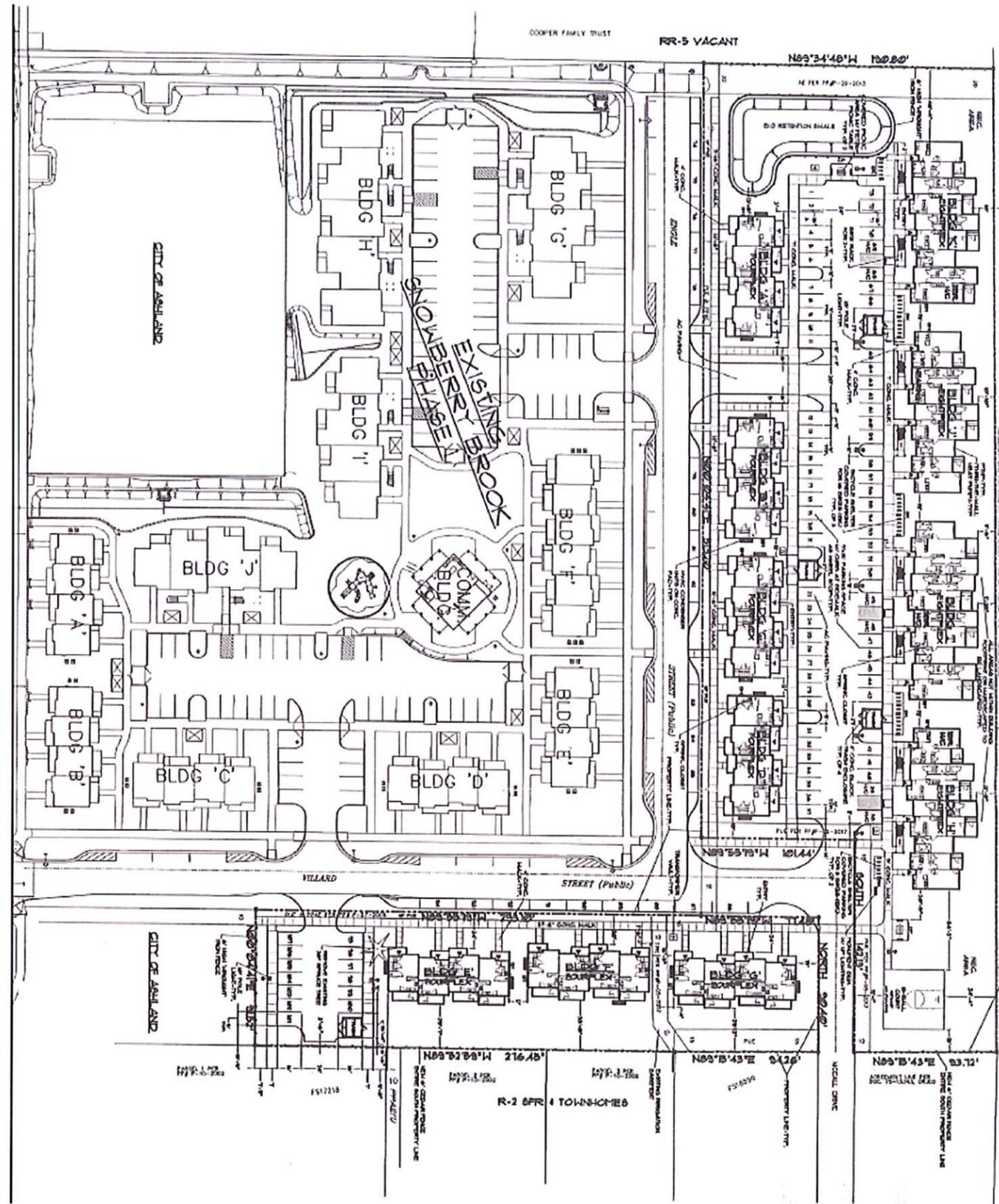
The base density in the R-2 zoning district allows for 13½ dwelling units per acre. The subject properties’ 3.35 acres allow a base density of 45.2 units [ $3.35 \text{ acres} \times 13.5 \text{ du/acre} = 45.225 \text{ du}$ ], and the applicant is proposing density bonuses for providing affordable housing (35 percent) and for conservation housing (15 percent) allowing a 50 percent increase in the base density or 67.8 units [ $45.225 \text{ du} \times 1.50 = 67.8375$ ]. 60 units are proposed, and conditions have been included to require that all units be deed restricted as affordable and certified as Earth Advantage® Gold to qualify for the requested density bonuses.

## **Staff Recommendation**

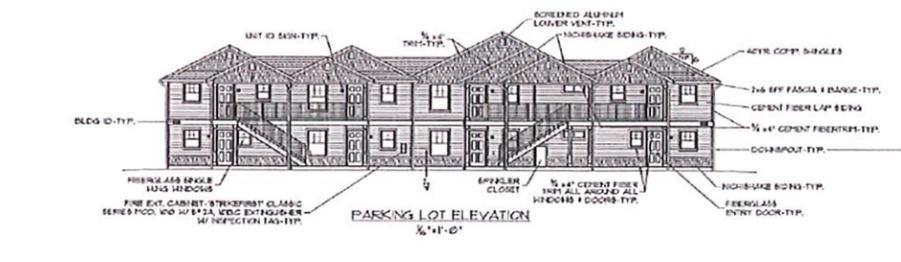
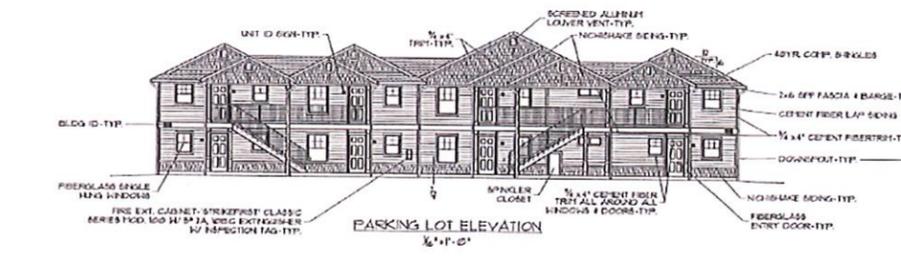
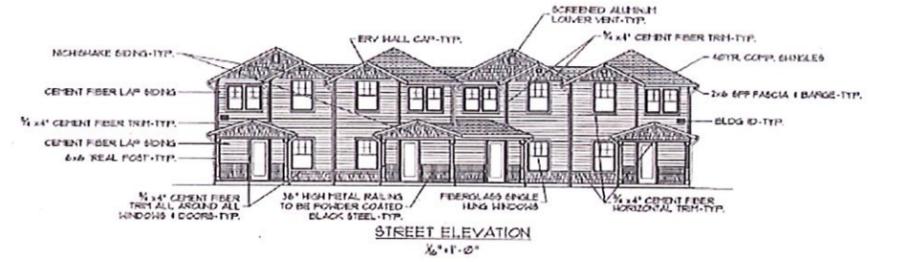
Staff recommends that the application be approved with the conditions detailed in the draft findings included in the Planning Commission’s June 11, 2019 meeting packet.

# Snowberry Brook II

Engle & Villard Streets



Vicinity Map



**BEFORE THE PLANNING COMMISSION**  
**July 9, 2019**

IN THE MATTER OF PLANNING ACTION #PA-T2-2019-00008, A REQUEST FOR )  
SITE DESIGN REVIEW APPROVAL TO ALLOW THE CONSTRUCTION OF A 60- )  
UNIT MULTI-FAMILY DEVELOPMENT ON TWO TAX LOTS (#2504 & #2505) )  
ALONG VILLARD AND ENGLE STREETS AS PHASE II OF THE EXISTING )  
SNOWBERRY BROOK DEVELOPMENT. THE PROPOSAL CONSISTS OF FOUR )  
2-STORY EIGHT-PLEX APARTMENT BUILDINGS AND SEVEN 2-STORY TOWN- )  
HOUSE FOUR-PLEXES. UNITS WILL CONSIST OF TEN 1-BEDROOM FLATS, 12 )  
2-BEDROOM FLATS, TEN 3-BEDROOM FLATS, AND 28 2-BEDROOM TOWN- )  
HOMES. THE APPLICATION INCLUDES A REQUEST FOR A TREE REMOVAL )  
PERMIT TO REMOVE ONE-TREE, AN APPROXIMATELY 24-INCH DIAMETER )  
DEODAR CEDAR (*CEDRUS DEODARA*) WHICH THE PROJECT ARBORIST )  
DESCRIBES AS POSING A HAZARD. )

**DRAFT**

**FINDINGS,  
CONCLUSIONS &  
ORDERS**

**OWNER/APPLICANT:** HOUSING AUTHORITY OF JACKSON COUNTY/ )  
HAJC DEVELOPMENT/DAN HORTON, ARCHITECT )  
)

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**RECITALS:**

- 1) Tax lot #2504 and #2505 of Map 39 1E 11C are located along Engle and Villard Streets, south and east of the Snowberry Brook development at 380 Clay Street and are zoned R-2 (Low-Density Multi-Family Residential). With the first phase of the development, these properties were initially designated as Parks/Open Space, but prior to sale of the properties to the applicant by the City, this designation was removed and alternative property for a neighborhood park was acquired nearby.
  
- 2) The applicant is requesting Site Design Review approval to allow the construction of a for 60-unit multi-family development on two tax lots (#2504 & #2505) along Villard and Engle Streets as Phase II of the existing 'Snowberry Brook' development. The proposal consists of four two-story eight-plex apartment buildings and seven two-story townhouse four-plexes. Units will consist of ten one-bedroom flats, 12 two-bedroom flats, ten three-bedroom flats, and 28 two-bedroom townhomes. The application includes a request for a Tree Removal Permit to remove one tree, an approximately 24-inch diameter Deodar Cedar (*cedrus deodara*) which the project arborist describes as posing a hazard. The application also includes proposals for density bonuses because all units are to be built to Earth Advantage® Gold standards and all units are to be deed-restricted as affordable housing. The proposal is outlined in plans on file at the Department of Community Development.
  
- 3) The criteria for Site Design Review approval are detailed in **AMC 18.5.2.050** as follows:
  - A. **Underlying Zone:** *The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density*

and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards.

- B. **Overlay Zones:** The proposal complies with applicable overlay zone requirements (part 18.3).
- C. **Site Development and Design Standards:** The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.
- D. **City Facilities:** The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property.
- E. **Exception to the Site Development and Design Standards.** The approval authority may approve exceptions to the Site Development and Design Standards of part 18.4 if the circumstances in either subsection 1 or 2, below, are found to exist.
  - 1. There is a demonstrable difficulty meeting the specific requirements of the Site Development and Design Standards due to a unique or unusual aspect of an existing structure or the proposed use of a site; and approval of the exception will not substantially negatively impact adjacent properties; and approval of the exception is consistent with the stated purpose of the Site Development and Design; and the exception requested is the minimum which would alleviate the difficulty.; or
  - 2. There is no demonstrable difficulty in meeting the specific requirements, but granting the exception will result in a design that equally or better achieves the stated purpose of the Site Development and Design Standards.

4) The criteria for a Tree Removal Permit are described in **AMC 18.5.7.040.B** as follows:

- 1. **Hazard Tree.** A Hazard Tree Removal Permit shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.
  - a. The applicant must demonstrate that the condition or location of the tree presents a clear public safety hazard (i.e., likely to fall and injure persons or property) or a foreseeable danger of property damage to an existing structure or facility, and such hazard or danger cannot reasonably be alleviated by treatment, relocation, or pruning. See definition of hazard tree in part 18.6.
  - b. The City may require the applicant to mitigate for the removal of each hazard tree pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.
- 2. **Tree That is Not a Hazard.** A Tree Removal Permit for a tree that is not a hazard shall be granted if the approval authority finds that the application meets all of the following criteria, or can be made to conform through the imposition of conditions.

- a. *The tree is proposed for removal in order to permit the application to be consistent with other applicable Land Use Ordinance requirements and standards, including but not limited to applicable Site Development and Design Standards in part 18.4 and Physical and Environmental Constraints in part 18.10.*
- b. *Removal of the tree will not have a significant negative impact on erosion, soil stability, flow of surface waters, protection of adjacent trees, or existing windbreaks.*
- c. *Removal of the tree will not have a significant negative impact on the tree densities, sizes, canopies, and species diversity within 200 feet of the subject property. The City shall grant an exception to this criterion when alternatives to the tree removal have been considered and no reasonable alternative exists to allow the property to be used as permitted in the zone.*
- d. *Nothing in this section shall require that the residential density to be reduced below the permitted density allowed by the zone. In making this determination, the City may consider alternative site plans or placement of structures or alternate landscaping designs that would lessen the impact on trees, so long as the alternatives continue to comply with the other provisions of this ordinance.*
- e. *The City shall require the applicant to mitigate for the removal of each tree granted approval pursuant to section 18.5.7.050. Such mitigation requirements shall be a condition of approval of the permit.*

5) The Planning Commission, following proper public notice, held a public hearing on June 11, 2019 at which time testimony was received and exhibits were presented. Subsequent to the closing of the hearing, the Planning Commission **approved the application subject to conditions pertaining to the appropriate development of the site.**

Now, therefore, the Planning Commission of the City of Ashland finds, concludes and recommends as follows:

## **SECTION 1. EXHIBITS**

For the purposes of reference to these Findings, the attached index of exhibits, data, and testimony will be used.

Staff Exhibits lettered with an "S"

Proponent's Exhibits, lettered with a "P"

Opponent's Exhibits, lettered with an "O"

Hearing Minutes, Notices, Miscellaneous Exhibits lettered with an "M"

## SECTION 2. FINDINGS & CONCLUSIONS

2.1 The Planning Commission finds that it has received all information necessary to make a decision based on the staff report, public hearing testimony and the exhibits received.

2.2 The Planning Commission finds that the proposal for Site Design Review and Tree Removal permit approvals meets all applicable criteria for Site Design Review described in AMC 18.5.2.050, and for a Tree Removal Permit as described in AMC 18.5.7.040.B.

2.3 The Planning Commission ***concludes that the proposal satisfies all applicable criteria for Site Design Review approval.***

The first approval criterion addresses the requirements of the underlying zone, requiring that, *"The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards."* The applicant explains that the subject property is zoned R-2, a Low-Density Multi-Family Residential zoning, and asserts that all building and yard setback and other applicable standards including building separation, height, lot coverage, landscaping, solar access and density have been addressed in the applicant's designs.

### **Density**

The base density in the R-2 zoning district allows for 13½ dwelling units per acre. The subject properties' 3.35 acres allow a base density of 45.2 units [3.35 acres x 13.5 du/acre = 45.225 du], and the applicant is proposing density bonuses for providing affordable housing (35 percent) and for conservation housing (15 percent) allowing a 50 percent increase in the base density for a total potential density of 67.8 units [45.225 du x 1.50 = 67.8375]. 60 units are proposed, and conditions have been included to require that all units be deed restricted as affordable and certified as Earth Advantage® Gold to qualify for the requested density bonuses.

The second approval criterion deals with overlay zones, and requires that, *"The proposal complies with applicable overlay zone requirements (part 18.3)."* The property has no applicable overlay zones and as such complies with this standard.

The third criterion addresses the Site Development and Design Standards, requiring that *"The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below."* The applicant asserts that the proposal has been designed to address the standards of part 18.4, noting that the buildings are oriented to the streets with individual porches, patios and entries from the street where possible, parking placed behind street-facing building on Engle and to the side on Villard, with no parking or circulation between the buildings and the street. Street trees are to be planted along both

frontages. The proposed buildings are to use durable materials similar to those in the first phase Snowberry Brook development, and will be treated in similar earth tone colors. Walls and roof lines are articulated to break up the building massing and add interest to facades. The community building and play area from the first phase are to be made available to residents of Phase II, and new open space/recreation areas including a basketball court are to be incorporated with Phase II here.

### Parking

The application as proposed requires 105 off-street parking spaces. The applicant proposes to provide 86 off-street parking spaces in the surface parking lots proposed, and to utilize 19 on-street parking credits for the remaining required parking. This amounts to an approximately 18 percent reduction in required off-street parking.

When the Commission considered the Columbia Care "Rogue Ridge" project at 1661 Ashland Street, the applicant there had provided an Affordable Housing Parking Study from the City of San Diego which looked broadly at parking generation for affordable developments and concluded that approximately 47.5 percent of affordable housing residents do not own vehicles. This study had provided the basis for a reduced parking requirement in San Diego and was based on a survey of residents of 34 affordable housing developments around the country about the number of vehicles available to each household, their vehicle use and travel patterns, the number of persons and demographics for each household, and neighborhood characteristics including land use, availability of transit and characteristics of each development including on-site and off-site parking conditions. This study looked at parking requirements and parking demand strategies for affordable housing developments in cities including San Leandro, Santa Barbara, Pasadena, Boulder, Denver, Eugene, Berkeley, Los Angeles, Portland, and Seattle. In that application, the applicants also noted that in assessing numerous Rogue Valley projects with low income residents, they typically have fewer than 50 percent of tenants who own or operate a motor vehicle.

The Planning Commission found that a lower parking ratio for proposed affordable units was appropriate based on the Affordable Housing Parking Study provided which asserts that affordable housing developments require about one-half of the parking typically required of market rate rental developments and the anecdotal observations by the applicants and by Planning staff that affordable housing developments locally tend to generate less off-street parking demand than market rate developments. The Planning Commission further found that determining the minimum parking required based on these considerations was an allowed exercise of the Commission's discretion supported by AMC 18.4.3.030.

In the current application, the applicant has **not** proposed a reduction in required parking and has instead simply proposed to utilize a parking management strategy which is allowed in the municipal code to off-set some of the parking requirement based on available on-street parking. In staff's view, this request can be found to be an appropriate use of an allowed parking

management strategy, but it can also be found that given the nature of the proposal the likely parking demand may be substantially less than calculated.

90 bicycle parking spaces are required, and the applicant proposes to provide the 90 covered bicycle parking spaces required in shelters on site.

The fourth approval criterion addresses city facilities, specifically requiring that, *"The proposal complies with the applicable standards in section 18.4.6 Public Facilities and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property and adequate transportation can and will be provided to the subject property."* The applicant explains that city facilities are available at the site, including: 8-inch Water Mains in Villard & Engle Streets; 8-inch Sanitary Sewer Mains in Villard & Engle Streets; and 12-inch storm drains in Villard & Engle Streets. The applicant explains that stormwater facilities will be provided to meet city, Rogue Valley Stormwater Quality Design Manual, State of Oregon as well as other agency requirements. Electrical service is available to serve the property, and the applicant has met with the Ashland Electric Department; a conceptual electric service plan from the Electric Department is included with the application materials.

In discussions with Public Works/Engineering staff, they have noted that the property along Engle Street is currently served by an 8-inch sanitary sewer main along the southern property line, however they point out that existing grades are such that it may make not be feasible for the entire property along Engle Street to use this main and additional sanitary sewer main/s may need to be installed. A condition that the applicant's civil engineer work to address this issue in the final civil drawings to be approved before building permit issuance has been recommended below.

In terms of paved access and adequate transportation:

- **Clay Street** is a residential neighborhood collector street, and is a County road in this vicinity. City standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb, seven-foot parking bays on each side, and nine- to ten-foot travel lanes on each side. The city-standard cross-section includes a 22- to 34-foot curb-to-curb paved width within a 49- to 61-foot right-of-way, *dependent on the on-street parking configuration*. Frontage improvements including additional paving, curbs, gutters, a bio-swale parkrow planting strip and sidewalks were installed to meet these standards along the Snowberry I frontage, and along the frontage of the property to the north, with the initial Snowberry development.
- **Engle and Villard Streets** are residential neighborhood streets, and city standards envision five-foot sidewalks, seven-foot parkrow planting strips, a six-inch curb and

seven-foot parking bays on each side, with an 11- to 14-foot queuing travel lane. The city standard cross-section includes a 25- to 28-foot curb-to-curb paved width in a 50- to 55-foot right-of-way. The existing "half-street" improvements consist of full paving to accommodate travel lanes and parking, curb, gutter, sidewalks and parkrow on the west side of Engle and the north side of Villard along Snowberry I, and the applicant proposes to complete the corridors by installing sidewalks and parkrow planting strips along the Snowberry II frontages of both streets.

- **McCall Drive** is an alley as envisioned with its creation in PA #2013-00104, and right-of-way has already been dedicated to the city to connect the existing terminus of McCall to Villard. The alley connection will be completed with the current application, and at a minimum pedestrian, bicycle and emergency vehicle connectivity provided. The applicant will work with the city's Transportation Commission and Public Works Department to determine whether vehicular connectivity will be required.

The application also has preserved a 20-foot wide corridor along the north edge of the site where an access easement is in place. A similar corridor was preserved on the north side of the wetland on Phase I, and these two segments would ultimately combine with a similar dedication when the property to the north is annexed to provide for a future vehicular connection between Tolman Creek Road and Clay Street.

A Traffic Impact Analysis (TIA) prepared by S.O. Transportation Engineering, LLC concludes that the proposed 60-unit development can be approved without adverse impacts to the transportation system. The TIA recommends limiting McCall Drive to bicycles, pedestrians and emergency vehicles, but restricting access for non-emergency motor vehicles, to avoid cut-through traffic.

The final criterion for Site Design Review approval addresses "Exception to the Site Development and Design Standards." The current request does not include any Exceptions to the Site Development and Design Standards.

Staff believes that the Planning Commission could reasonably conclude that as detailed above and with the conditions discussed, the proposal complies with the requirements for Site Design Review approval.

2.5 The Planning Commission finds that there are five trees on the subject property: a 20-inch diameter Oak behind proposed Building H, a 24-inch Deodar Cedar near the proposed parking lot at the southeast corner, two scrub Almonds in the 13- to 14-inch range near the Deodar, and a 12-inch diameter Cedar near the southeast corner of the property along Villard. In addition, five trees are noted on adjacent properties within 15 feet of the property line. The remainder of the property is generally devoid of natural features other than native grasses.

### Trees

The application proposes to remove the 24-inch d.b.h. Deodar Cedar as a hazard tree. The project arborist notes that the tree is in poor condition, with massive historical damage including multiple damaged leaders, no dominant leader and severe damage to large lower branches, and an 18-inch deep trench cutting through the root zone within three feet of the trunk.

There are two smaller trees immediately adjacent to this tree, described as scrub Almonds on the Tree Protection Plan, that are also proposed for removal. The project arborist notes that these trees are not in good shape, with dead branches and evidence of prior severe pruning, and both Almonds are located in the path of the proposed new sidewalk along Villard Street.

The application narrative identifies trees on adjacent properties within 15 feet of the property lines and indicates that these trees will be protected.

As the staff report and draft findings are being prepared, the Tree Commission has not yet reviewed the application but is scheduled to do so on June 6, 2019. A condition of approval has accordingly been recommended to require that the recommendations of the Tree Commission become conditions of approval, where consistent with applicable standards and with final approval by the Staff Advisor. A condition is also recommended to require a Tree Verification Permit be obtained to verify the identification of trees to be removed and the installation of any requisite tree protection for trees to be preserved and protected.

Staff believes that the Planning Commission could reasonably conclude that as detailed above and with the conditions discussed, the proposal complies with the requirements for Tree Protection and for Tree Removal Permits to remove three trees.

## SECTION 3. DECISION

3.1 Based on the record of the Public Hearing on this matter, the Planning Commission concludes that the proposal for Site Design Review approval to allow construction of a for 60-unit multi-family development and the removal of three trees is supported by evidence contained within the whole record.

In the originally Snowberry Brook development's staff report, staff noted:

*From staff's perspective, the Housing Authority and their design team merit recognition not only for their willingness to partner with the City to bring this project forward, but also for their willingness to incorporate Ashland's site design standards into the project and to integrate the adjacent wetlands with the open space and landscaping of the project to greatly benefit the livability of the Snowberry Brook development. The partnership with the Housing Authority and the proposed modifications to the (previous) ... subdivision's approval represent a true win-win situation for the City in addressing the transportation infrastructure needs and minimum*

*density requirements of the annexation while enhancing and protecting a locally-significant wetland and providing 60-units of much needed affordable housing where only 17 units were previously to have been provided. With this in mind, staff would strongly recommend approval of the application...*

Here staff would add that the opportunity to add an additional 60 units of affordable housing to a project that has fit well into the community seems to advance on the prior "win-win situation" in providing much-needed affordable rental housing in a mix of one-, two- and three-bedroom configurations immediately adjacent to the previous development, allowing for more efficient management by the Housing Authority. Planning staff are very supportive of the application, believe that it meets applicable criteria and strongly recommend approval.

Therefore, based on our overall conclusions, and upon the proposal being subject to each of the following conditions, we approve Planning Action #PA-T2-2019-00008. Further, if any one or more of the conditions below are found to be invalid, for any reason whatsoever, then Planning Action #PA-T2-2019-00008 is denied. The following are the conditions and they are attached to the approval:

1. That all proposals of the applicant shall be conditions of approval unless otherwise modified herein.
2. That any new addresses shall be assigned by City of Ashland Engineering Department.
3. That permits and associated inspections shall be obtained from the Ashland Public Works Department prior to any work in the public right-of-way, including but not limited to permits for curbs, parkrows, sidewalks and driveway approaches, or for any necessary encroachments. A 1200-C permit will need to be secured by the applicant if required by Oregon State DEQ, and the City of Ashland Engineering Division must receive a copy of this permit prior to any construction activity.
4. That the building permit submittals shall include:
  - a. That the plans submitted for the building permit shall be in substantial conformance with those approved as part of this application. If the plans submitted for the building permit are not in conformance with those approved as part of this application, an application to modify the Site Design Review approval shall be submitted and approved prior to issuance of a building permit.
  - b. All easements including but not limited to public and private utilities, irrigation, access, public pedestrian access and fire apparatus access shall be indicated on the building permit submittal for review by the Planning, Engineering, Building and Fire Departments.
  - c. Solar setback calculations demonstrating that all new construction complies with Solar Setback Standard A in the formula  $[(\text{Height} - 6/16)/(0.445 + \text{Slope}) = \text{Required Solar Setback}]$  and elevations or cross section drawings clearly identifying the highest shadow producing point(s) and their height(s) from natural grade.
  - d. Final lot coverage calculations demonstrating how lot coverage is to comply with the applicable coverage allowances of the zoning district (65 percent coverage is allowed in

the R-R district). Lot coverage includes all building footprints, driveways, parking areas and other circulation areas, and any other areas other than natural landscaping.

- e. Storm water from all new impervious surfaces and run-off associated with peak rainfalls must be collected on site and channeled to the City storm water collection system (i.e., curb gutter at public street, public storm pipe or public drainage way) or through an approved alternative in accordance with Ashland Building Division policy BD-PP-0029. On-site collection systems shall be detailed on the building permit submittals.
- f. The engineered construction drawings for the proposed street improvements along Engle and Villard Streets including city standard seven-foot parkrow planting strips with irrigated street trees and five-foot sidewalks shall be submitted for review and approval of the Ashland Planning and Engineering Divisions with the building permit submittal, prior to work in the street right-of-way or installation of improvements in the pedestrian corridor. Sidewalk designs will need to include handicap access ramps meeting current United States Access Board Guidelines (PROWAG) and shall be designed in accordance with the current ODOT design guidelines. Civil drawings shall consider existing fill and grades in the vicinity of Buildings A and K and the proposed detention pond, and grades and fill will need to be planned to enable future street extensions to the north, east and west, with any associated retaining walls, to relate to the existing Engle Street elevation.
- g. All frontage improvements, including but not limited to the sidewalk, parkrow planting strips and irrigated street trees shall be constructed across the entire Engle and Villard frontages of the subject properties, and if additional right-of-way is necessary to accommodate city standard frontage improvements the applicant shall dedicate additional right-of-way or necessary public pedestrian access easements prior to the issuance of a certificate of occupancy or final project approval.
- h. The engineered construction drawings for the proposed alley improvements connecting the McCall Drive alley from its existing terminus to Villard Street shall be submitted for review and approval of the Ashland Planning and Engineering Divisions with the building permit submittal, prior to work in the right-of-way. Alley improvements shall be constructed to city standard which requires a minimum 12-foot width paved driving surface with two-foot buffers on each side, and shall at a minimum provide bicycle, pedestrian and emergency vehicle access through to McCall Drive to the south. Should the applicant's wish to limit regular motor vehicle access with the installation of removable bollards, approvals would need to be obtained through the Public Works Department and Transportation Commission prior to bollard installation.
- i. Final utility, grading, drainage and erosion control plans.
- i. The final storm drainage plan shall detail the location and final engineering for all storm drainage improvements associated with the project, and shall be submitted for review and approval by the Departments of Public Works, Planning and Building Divisions. The storm drainage plan shall demonstrate that post-development peak flows are less than or equal to the pre-development peak flow for the site as a whole, and that storm water quality mitigation has been addressed through the final design.

- ii. The final utility plan for the parcels shall be submitted for review and approval by the Planning, Engineering, and Building Divisions with the Final Plan application. The utility plan shall include the location of connections to all public facilities including the locations of water lines and meter sizes, fire hydrants, sanitary sewer mains and services, manholes and clean-outs, and storm drainage pipes and catch basins, and shall address the sewer mains on Engle Street relative to grades and any additional mains needed to serve the Engle parcel. Any required private or public utility easements shall be delineated on the civil plans.
  - iii. The final electric design and distribution plan including load calculations and locations of all primary and secondary services including transformers, cabinets and all other necessary equipment with the Final Plan application. This plan must be reviewed and approved by the Electric Department prior to permit issuance or installation of equipment. Transformers and cabinets shall be located in areas least visible from streets and outside of vision clearance areas, while considering the access needs of the Electric Department. Individual electric services/meters shall be installed underground to serve all units.
- j. A final Tree Protection Plan addressing the requirements of AMC 18.4.5.030.B and any recommendations of the Tree Commission from its June 6, 2019 regular meeting, where consistent with applicable standards and criteria and with final approval by the Staff Advisor. The plan shall identify the location and placement of fencing around the drip lines of all trees identified for preservation. The amount of fill and grading within the drip lines shall be minimized. Cuts within the drip line shall be noted on the tree protection plan, and shall be executed by handsaw and kept to a minimum. No construction shall occur within the tree protection zone including dumping; storage of materials such as building supplies, soil, waste, equipment, or parked vehicles; and no fill shall be placed around the trunk/root crown.
  - k. A final size- and species-specific landscaping plan including irrigation details and details of the landscape materials to be planted shall be provided for the review and approval of the Staff Advisor. New landscaping shall comply with the General Fuel Modification Area requirements and shall not include plants listed on the Prohibited Flammable Plant List adopted by Resolution #2018-028.
  - l. That the requirements of the Ashland Fire Department relating to approved addressing; fire apparatus access and approach; firefighter access pathways and any gates, fences or other obstructions; fire flow; fire hydrant spacing, distance and clearance; fire department work area; fire sprinkler, extinguishers and fire department connection, as applicable; and wildfire hazard area requirements shall be satisfactorily addressed in the building permit submittals. Fire Department requirements shall be included in the building permit drawings, and a Fire Prevention and Control Plan addressing the General Fuel Modification Area requirements of AMC 18.3.10.100.A.2. shall be provided with the building permit submittals. This plan shall be reviewed and approved by the Fire Marshal prior to bringing combustible materials onto the property.
  - m. That exterior building materials and paint colors shall be compatible with the surrounding area and consistent with those described in the application. Sample exterior building colors shall be

provided with the building permit submittals for review and approval of the Staff Advisor. Very bright or neon paint colors shall not be used in accordance with 18.4.2.030.E of the Multi-Family Site Design and Use Standards.

- n. That the final plans shall demonstrate that the strategies for “Parking Design to Reduce Environmental Impacts” in AMC 18.4.3.080.B.5 are addressed in the parking lot designs.
5. That prior to building permit issuance:
    - a. A Tree Verification Permit shall be applied for and approved by the Ashland Planning Division prior to any site work including excavation, staging or storage of materials, or building or excavation permit issuance. The Tree Verification Permit is to inspect the identification of the trees to be removed and the installation of tree protection fencing for trees to be protected. Standard tree protection consists of chain link fencing six feet tall and installed in accordance with the requirements of AMC 18.4.5.030.B. No construction shall occur within the tree protection zone including dumping or storage of materials such as building supplies, soil, waste, equipment, or parked vehicles.
    - b. That the requirements of the Building Division shall be satisfactorily addressed.
    - c. That the applicant shall pay all applicable plan review & building permit fees and all associated fees and charges.
  6. That prior to the issuance of a certificate of occupancy or final project approval:
    - a. That the sidewalks shall be installed according to the approved plan, and parkrow planting strips along the Villard and Engle street frontages planted with irrigated street trees selected from the Recommended Street Trees guide at a spacing of one per 30 feet prior to the issuance of a certificate of occupancy or final inspection approvals for the project.
    - b. That the McCall Drive alley improvements shall be completed according to the approved plan.
    - c. Electric services shall be installed underground to serve all proposed units, inspected and approved. A final electric service plan shall be reviewed and approved by the Ashland Electric, Building, Planning and Engineering Divisions prior to installation.
    - d. Utility installations and common area improvements including landscaping, open space/recreational areas, hardscaping, irrigation, automobile and bicycle parking, and trash and recycling facilities shall be completed according to approved plans, inspected and approved by the Staff Advisor.
    - e. That all exterior lighting shall be directed on the property and shall not directly illuminate adjacent properties.
    - f. That bicycle parking facilities to accommodate the 90 required bicycle parking spaces shall be installed prior to the issuance of a certificate of occupancy or final project approval. Inverted u-racks shall be used for the bicycle parking. All bicycle parking shall be installed in accordance with design and rack standards in 18.4.3.070.I and J prior to the issuance of

the certificate of occupancy, inspected and approved by the Staff Advisor. The building permit submittals shall verify that the bicycle parking rack design, spacing and coverage requirements are met in accordance with 18.4.3.070.I.

- g. The applicant shall record deed restrictions in accordance with the City of Ashland's Affordable Housing Standards to satisfy the requirements for the affordable housing density bonus requested.
- h. The applicant shall provide evidence of Earth Advantage certifications necessary to satisfy the requirements for the conservation housing density bonus requested.

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Planning Commission Approval

July 9, 2019  
Date

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*18.5.2.040 Application Submission Requirements*

*A. General Submission Requirements. Information required for a Type I or Type II review, as applicable (see sections 18.5.1.050 and 18.5.1.60) including but not limited to a written statement or letter explaining how the application satisfies each and all of the relevant criteria and standards.*

*B. Site Design Review Information. In addition to the general information required for Site Design Review, the applicant shall provide the following information.*

- 1. Basic Plan Information*
- 2. Site Analysis Map*
- 3. Proposed Site Plan*
- 4. Architectural Drawings*
- 5. Preliminary Grading and Drainage Plan*
- 6. Erosion Control Plan*
- 7. Landscape and Irrigation Plans*
- 8. Narrative*

*b. For residential developments:*

- i The total square footage of the development.*
- ii The number of dwelling units in the development.*
- iii Percentage of lot coverage by structures, streets, roads, or drives; public use areas, common area/private recreation areas, landscaping, and parking areas.*

NARRATIVE RESPONSE:

Snowberry Brook II is a proposed 60 unit multiple family project located off of Engle and Villard Streets in Ashland, Oregon. The proposed project will consist of four - two story wood framed eight-plex apartment buildings and seven - two story townhouse fourplexes. The square footage of the area proposed to be developed is 145,852 or 3.38 acres contained in two existing tax lots.

The apartment distribution will be ten one bedroom flats, twelve two bedroom flats, twenty eight two bedroom townhouses, and ten three bedroom flats all with individual exterior entries and covered patios and decks.

The percentages of the site proposed to be covered by structures is 23%, other impervious surfaces are 20% paved parking and driveways, 11% sidewalks, leaving 46% of the site pervious for landscaping and recreation areas. Site amenities include 8% of the site landscaped outdoor recreation areas and a small basketball court. All buildings are designed



for durability and low maintenance as well as meeting the standards for Earth Advantage 'Gold' certification. All applicable local, State, and Federal codes will be met including accessibility.

Snowberry Brook II will be accessed off of the existing Villard and Engle streets with new sidewalks and planter strips as required by Ashland Land Use Ordinance. Adjacent properties are zoned R-2 to the South with existing residences and townhomes, R-2 to the East containing YMCA ballfields, RR-5 to the North currently vacant pasture, and R-2 to the West containing the existing first phase of Snowberry Brook. All ALUO requirements will be met with no exceptions requested.

*18.5.2.050 Approval Criteria*

*An application for Site Design Review shall be approved if the proposal meets the criteria in subsections A, B, C, and D below. The approval authority may, in approving the application, impose conditions of approval, consistent with the applicable criteria.*

- A. *Underlying Zone. The proposal complies with all of the applicable provisions of the underlying zone (part 18.2), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture and other applicable standards.*

RESPONSE:

The proposed Snowberry Brook II has been designed to comply with all applicable provisions of the underlying R-2 zone.

Tax lot 2504 building setbacks are Front, 15' (north property line adjacent to the extension of Villard Street); Side, 10' (east property line adjacent to YMCA), Side, 14'-8" ( west property line adjacent to Engle Street); and Rear 48'-8" ( north property line). All setbacks meet or exceed those required by ALUO.

Tax lot 2505 building setbacks are Front, 15' (east property line adjacent to McCall Drive); Side 20' (north adjacent to Villard Street); Side, 29' (south property line adjacent to existing development). Rear, 87' (west property line adjacent to City owned vacant lot). All setbacks meet or exceed those required by ALUO.

Building separations all meet the 1/2 building height of the tallest building except that the maximum separation required is 12'. All buildings are separated by a minimum of 12'. Building heights are 27' to the highest ridge which is less than the 35' height allowed. Proposed lot coverage is 54% with up to 65% allowed. Landscaped areas total 38% of the site with not less than 35% allowed and open recreation areas are at the code required 8%.

Residential density in the R-2 zone is a base density of 13.5 units per acre. 3.35 acres times 13.5 equals 42.2 units per acre. Residential bonus for affordable housing (35%) and conservation measures (15%) allow a density increase of 50%. Allowable density with bonus is 63 units, 60 units are proposed.



Solar Setback Standard A requirements are met using the formula  $SSB = H - 6 \text{ over } 0.445 + S$ . H is 24 feet to the highest shadow producing ridge on Building 'K', S is a negative slope of 3% thus the calculation is  $24 - 6 = 18 \text{ over } 0.445 - .03 = 0.415$ . 18 divided by 0.415 = 43.37 feet. Building 'K', the furthest North building, is setback from the property line 48 feet thus the shadow from the building will not project past the property line.

A thorough environmental study has been performed on the site and it has been found that there are no significant environmental issues existing. An analysis of a potential wetland near the southeast corner of the property was performed by a professional wetlands consultant with no findings of wetlands. Attached is a Letter of Concurrence from Oregon Department of State Lands.

*B. Overlay Zones. The proposal complies with applicable overlay zone requirements (part 18.3)*

RESPONSE:

The site is not in an overlay zone.

*C. Site Development and Design Standards. The proposal complies with the applicable Site Development and Design Standards of part 18.4, except as provided by subsection E, below.*

RESPONSE:

The project has been designed to meet the design standards of part 18.4. The buildings have been oriented to the streets with individual porches, patios, and entries from the street where possible. Parking is located behind street facing buildings on Engle Street and to the side of the street facing buildings on Villard Street with no parking or circulation between buildings and street. Street frontage landscaping is proposed in accordance with ALUO to include street trees in an irrigated parkrow at a minimum spacing of one tree per 30' of frontage. The developer is proposing, and will apply for a permit for removal of the singular existing Deodor Cedar tree on tax lot 2505 due to its poor condition and conclusion of the Landscape Architect/Arborist that the tree is a hazard tree. See attached correspondence with the Arborist and photos of the tree.

The buildings are designed with durable materials similar to the original Snowberry Brook development with cement fiber siding, fiberglass windows, and architectural grade composition roofing. Wall and roof lines are articulated to break up massing and add interest to the facades. Colors will be similar if not the same as the previous development with earthtone colors on shingle and lap siding.

The community building and play area with structure of the previous development will be shared and utilized by tenants of this current phase of development.

Parking has been calculated at the code required 1.5 spaces for one bedroom units, 1.75 spaces for two bedroom units, and 2.0 spaces for three bedroom units. Total required parking spaces is 105 spaces, 107 spaces are proposed with 86 spaces in offstreet parking areas and 21 onstreet parking spaces. Landscaping of offstreet parking areas will slightly exceed the code required minimum of 7% parking landscaping with 8% being provided.

Bicycle parking has been calculated at the code required 1.0 space for one bedroom units, 1.5 spaces for two bedroom units, and 2.0 spaces for three bedroom units. Total bicycle parking required is 90 spaces, 90 spaces are proposed at covered bike shelters.

Snowberry Brook II



D. *City Facilities. The proposal complies with the applicable standards in section 18.4.6 Public Facilities, and that adequate capacity of City facilities for water, sewer, electricity, urban storm drainage, paved access to and throughout the property, and adequate transportation can and will be provided to the subject property.*

RESPONSE:

City facilities are available at the site including 8 inch water mains in Villard and Engle Streets, 8 inch sanitary sewer mains in Villard and Engle Streets, and 12 inch storm drain lines in both streets. Storm water facilities will be engineered to meet all city requirements, Rogue Valley Storm Water Quality Design Manual, as well as other agency requirements.

Electrical service is available at the site and meetings have been held with Ashland Electric Department to assist engineers in design development. Attached are conceptual electrical layout plans from Ashland Electric Department.

Streets from which Snowberry Brook II will take access are existing residential neighborhood streets. Villard and Engle Streets were developed in 2009 at the time of construction of the existing first phase of Snowberry Brook. Both streets have an existing curb to curb width of 34.30 feet which allows two ten foot travel lanes and two seven foot parking lanes. The current project will include construction of seven foot parkstrips and five foot concrete walks on the currently undeveloped sides of the streets. McCall Drive located at the Southeast portion of the project is a 24 foot wide dedication connecting to driveways to the development to the south. It is anticipated that the section of McCall Drive up to the Project's south property line be developed and used for emergency and pedestrian access only.

A Traffic Impact Analysis has been prepared by Southern Oregon Transportation Engineering for submittal for Site Design Review and is attached. The report finds the proposed development will have no adverse impacts to area transportation.

ADDITIONAL APPROVAL CRITERIA - NEEDED HOUSING:

This application for Site Design Review approval is an application for development of needed housing. The following is an excerpt from a previous application in another jurisdiction written by attorneys as an explanation of Oregon law as it applies to needed housing:

ORS 197.303(1) defines "needed housing" as "housing types determined to meet the need shown for housing within an urban growth boundary at particular price ranges and rent levels" and includes both "multiple family housing for both owner and renter occupancy" and "government assisted housing." ORS 197.303(1) Local governments must permit needed housing in one or more zoning districts and "with sufficient buildable land to satisfy that need." ORS 197.307(3). Local governments may only impose "clear and objective standards, conditions and procedures regulating the development of needed housing." ORS 197.307(4).

The local government's standards applicable to needed housing must be clear and objective on the face of the ordinance. ORS 227.173(2). Furthermore, the local government bears the



burden of demonstrating that the standards, conditions, and procedures are capable of being imposed only in a clear and objective manner. ORS 197.831. Finally, these "clear and objective" standards, conditions, and procedures must not have the effect of "discouraging needed housing through unreasonable cost or delay." ORS 197.307(4).

The legislature's policy goal in enacting the "needed housing" statutes was to prevent local governments from using their land use regulations to exclude certain housing types, particularly manufactured housing, which the legislature believed was needed to satisfy low and moderate-income housing demand." *Rogue Valley Association of Realtors v. City of Ashland*, 35 Or LUBA 139, 156 (1998) *af'd* 158 Or App 1, 970 P2d 685, rev den 328 Or 594 (1999). In fact, ORS 197.307(1) states that the "availability of affordable, decent, safe and sanitary housing opportunities for persons of lower, middle and fixed income \*\*\*\*\* is a matter of statewide concern."

LUBA has interpreted these statutes to preclude local governments from applying subjective, discretionary criteria to applications for needed housing:

"'Needed housing' is not to be subjected to standards, conditions or procedures that involve subjective, value- laden analyses that are designed to balance or mitigate impacts of the development on (1) the property to be developed or (2) the adjoining properties or community. Such standards, conditions or procedures are not clear and objective and could have the effect of discouraging needed housing through unreasonable costs or delay."

*Rogue Valley Association of Realtors*, 35 Or LUBA at 158 (Emphasis supplied.). Further, LUBA has specifically held that a standard requiring that a development be "compatible with surrounding development" was not clear and objective. *Rogue Valley Association of Realtors*, 35 Or LUBA at 157 (citing Land Conservation and Development Commission policy that served as basis for legislative enactment of needed housing statute). See also *Clark v. Coos County*, 53 Or LUBA 325 (2007) (local code approval standard that requires that proposed uses be compatible with surrounding uses is subjective). The Court of Appeals has also held that a local code standard is not "clear and objective" when it can "plausibly be interpreted in more than one way." *Tirumali v. City of Portland*, 169 Or App 241, 246, 7 P3d 761 (2000).

The City has determined in the 'Housing Element' of the Comprehensive Plan and a 'Housing Needs Analysis', a need for lower income, affordable housing. Comprehensive Plan Chart VI-4 shows a 'Historic Housing' mix of 21% of all housing available in Ashland as multifamily. Chart VI-5 'Future Housing' mix indicates 32% of all future housing demand to be multifamily. As can be seen in comparing Charts VI-4 and VI-5, the percentage of multifamily housing stock needs to be increased significantly to meet the demands of the City's residents for a low to moderate cost housing option.

Therefore, this application which proposes 60 units of affordable multiple-family housing in the Multi Family Residential district constitutes an application for development of "needed housing" for purposes of ORS 197.307. As a result, the City must only apply clear and objective standards, conditions and procedures to the approval of this application. Furthermore, even these clear and objective standards, conditions and procedures cannot have the effect of discouraging needed housing through unreasonable cost or delay.



Signed: 

Daniel Horton, Agent

Attachments:

- Architectural Site Plan
- Building Elevations
- Landscape Plan
- Preliminary Grading and Utility Plans
- Site Electrical Plans from Ashland Electric Dept
- Traffic Impact Analysis
- Wetlands Letter of Concurrence from DSL
- Statement and Photos of existing Cedar tree condition



# Memo

---

DATE: June 11, 2019  
TO: Planning Commissioners  
FROM: Derek Severson, Senior Planner  
RE: Traffic Impact Analysis  
Snowberry Brook Development, Phase 2

The Executive Summary for Phase 2 of the Snowberry Brook Development's Traffic Impact Analysis (TIA) is included here with your packets. The full TIA will be posted with the on-line packet at <http://www.ashland.or.us/Page.asp?NavID=17709>.



# Snowberry Brook Phase 2 Development

## Traffic Impact Analysis

May 2, 2019

Prepared By:

*SOUTHERN OREGON TRANSPORTATION ENGINEERING, LLC*



## **I. EXECUTIVE SUMMARY**

### **Summary**

Southern Oregon Transportation Engineering, LLC prepared a traffic analysis for a proposed Snowberry Brook Phase 2 residential development for the Housing Authority of Jackson County in Ashland, Oregon. The subject property is located along the east side of Engle Street and south side of Villard Street. Proposed development includes 60 multi-family residential units, similar to what was constructed on the northwest corner of Villard Street / Engle Street in Phase 1 development of the Snowberry Brook Subdivision.

Access to the site is proposed on Engle Street and Villard Street, with an emergency access through McCall Drive. Proposed development is estimated to generate 493 average daily trips (ADT) with 28 trips occurring during the a.m. peak hour and 34 trips during the p.m. peak hour. Four study area intersections and internal subdivision driveways were evaluated under existing year 2019 no-build, design year 2021 no-build, and design year 2021 build conditions during the a.m. and p.m. peak hours to determine what impacts the proposed development may have on the transportation system.

### **Conclusions**

The findings of the traffic impact analysis conclude that the proposed 60-unit residential subdivision (Phase 2 of Snowberry Brook) can be approved on the transportation system without creating adverse impacts. Results of the analysis are as follows:

1. All study area intersections are shown to operate within performance standards under existing year 2019 and design year 2021 no-build and build conditions during both the a.m. and p.m. peak hours.
2. Study area intersection 95<sup>th</sup> percentile queue lengths were shown to stay within available link distances for all analysis scenarios.
3. Sight distance is estimated to be adequate from Villard Street at Clay Street, but limited from McCall Drive at Clay Street to the west. No action is shown to be necessary at this time.
4. Left and right turn lane criterion is not shown to be met on Clay Street at Villard Street or on Clay Street at McCall Drive under design year 2021 build conditions.

### **Recommendations**

1. A McCall Drive vehicular extension to Villard Street is not shown to make much difference operationally at the Villard Street / Clay Street and McCall Drive / Clay Street intersections, but the connection will encourage cut-through traffic and could have an adverse impact for both the Snowberry Brook Subdivision and neighboring subdivision to the south. Our recommendation is to include an extension of McCall Drive for pedestrian connectivity and emergency response, but no regular vehicular traffic.



# Oregon

Kate Brown, Governor

## Department of State Lands

775 Summer Street NE, Suite 100

Salem, OR 97301-1279

(503) 986-5200

FAX (503) 378-4844

[www.oregon.gov/dsl](http://www.oregon.gov/dsl)

### State Land Board

October 3, 2018

Housing Authority of Jackson County  
Attn: Ryan D. Haynes  
2251 Table Rock Rd  
Medford, OR 97501

Kate Brown  
Governor

Re: WD # 2018-0390 Wetland Delineation Report for Residential Housing;  
Jackson County; T39S R1E Sec. 11C, Tax Lots 2504 & 2505

Dennis Richardson  
Secretary of State

Dear Mr. Haynes:

Tobias Read  
State Treasurer

The Department of State Lands has reviewed the wetland delineation report prepared by Schott and Associates for the site referenced above. Based upon the information presented in the report, we concur that there are no jurisdictional wetlands or other waters of the state within the study area, as indicated on the attached Figure 6. Please replace all copies of the preliminary wetland map with this final Department-approved map. This concurrence is based on information provided to the agency and is for purposes of the state Removal-Fill Law only. Federal or local permit requirements may apply as well.

The jurisdictional determination is valid for five years from the date of this letter unless new information necessitates a revision. Circumstances under which the Department may change a determination are found in OAR 141-090-0045 (available on our web site or upon request). In addition, laws enacted by the legislature and/or rules adopted by the Department may result in a change in jurisdiction; individuals and applicants are subject to the regulations that are in effect at the time of the removal-fill activity or complete permit application. The applicant, landowner, or agent may submit a request for reconsideration of this determination in writing within six months of the date of this letter.

Thank you for having the site evaluated. If you have any questions, please phone me at 503-986-5218.

Sincerely,

Lauren Brown  
Jurisdiction Coordinator

Approved by

Peter Ryan, PWS  
Aquatic Resource Specialist

Enclosures

ec: Jodi Reed, Schott and Associates  
City of Ashland Planning Department (Maps enclosed for updating LWI)  
Benny Dean, Corps of Engineers  
Melody Rudenko, DSL

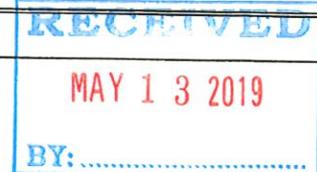


## WETLAND DELINEATION / DETERMINATION REPORT COVER FORM

Fully completed and signed report cover forms and applicable fees are required before report review timelines are initiated by the Department of State Lands. Make checks payable to the Oregon Department of State Lands. To pay fees by credit card, go online at: <https://apps.oregon.gov/DSL/EPS/program?key=4>.

Attach this completed and signed form to the front of an unbound report or include a hard copy with a digital version (single PDF file of the report cover form and report, minimum 300 dpi resolution) and submit to: Oregon Department of State Lands, 775 Summer Street NE, Suite 100, Salem, OR 97301-1279. A single PDF of the completed cover form and report may be e-mailed to: [Wetland\\_Delineation@dsl.state.or.us](mailto:Wetland_Delineation@dsl.state.or.us). For submittal of PDF files larger than 10 MB, e-mail DSL instructions on how to access the file from your ftp or other file sharing website.

Contact and Authorization Information	
<input checked="" type="checkbox"/> Applicant <input type="checkbox"/> Owner Name, Firm and Address: Ryan D. Haynes Housing Authority of Jackson County 2251 Table Rock Road Medford, Oregon 97501	Business phone # (541) 779-5785 Mobile phone # (optional) E-mail: ryan@hajc.net
<input type="checkbox"/> Authorized Legal Agent, Name and Address (if different):	Business phone # Mobile phone # (optional) E-mail:
I either own the property described below or I have legal authority to allow access to the property. I authorize the Department to access the property for the purpose of confirming the information in the report, after prior notification to the primary contact.	
Typed/Printed Name: <u>Ryan D. Haynes</u> Signature: <u>[Signature]</u> Date: <u>6/28/18</u> Special instructions regarding site access: _____	
Project and Site Information	
Project Name: Villard & Engle	Latitude: 42.188256 Longitude: -122.674376 decimal degree - centroid of site or start & end points of linear project
Proposed Use: Residential Housing	Tax Map # 391E11C Tax Lot(s) 2504, 2505 Tax Map # Tax Lot(s)
Project Street Address (or other descriptive location): Corner of Villard & Engle Street	Township 39S Range 1E Section 11C QQ Use separate sheet for additional tax and location information
City: Ashland County: Jackson	Waterway: _____ River Mile: _____
Wetland Delineation Information	
Wetland Consultant Name, Firm and Address: Schott & Associates, Inc. Attn: Jodi Reed PO BOX 589 Aurora, Oregon 97002	Phone # (503) 678-6007 Mobile phone # (if applicable) E-mail: Jodi@schottandassociates.com
The information and conclusions on this form and in the attached report are true and correct to the best of my knowledge.	
Consultant Signature: <u>Jodi Reed</u>	Date: 06/28/2018
Primary Contact for report review and site access is <input checked="" type="checkbox"/> Consultant <input type="checkbox"/> Applicant/Owner <input type="checkbox"/> Authorized Agent	
Wetland/Waters Present? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No Study Area size: 3.36 ac Total Wetland Acreage: 0.0000	
Check Applicable Boxes Below	
<input type="checkbox"/> R-F permit application submitted <input type="checkbox"/> Mitigation bank site <input type="checkbox"/> Industrial Land Certification Program Site <input type="checkbox"/> Wetland restoration/enhancement project (not mitigation) <input type="checkbox"/> Previous delineation/application on parcel If known, previous DSL # _____	<input type="checkbox"/> Fee payment submitted \$ _____ <input type="checkbox"/> Fee (\$100) for resubmittal of rejected report <input type="checkbox"/> Request for Reissuance. See eligibility criteria. (no fee) DSL # _____ Expiration date _____ <input type="checkbox"/> LWI shows wetlands or waters on parcel Wetland ID code _____
For Office Use Only	
DSL Reviewer: <u>LM</u> Fee Paid Date: ____/____/____	DSL WD # <u>2018-0390</u>
Date Delineation Received: <u>7/2/18</u> Scanned: <input type="checkbox"/> Electronic: <input checked="" type="checkbox"/>	DSL App.# _____





Date: 6/15/2018

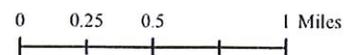
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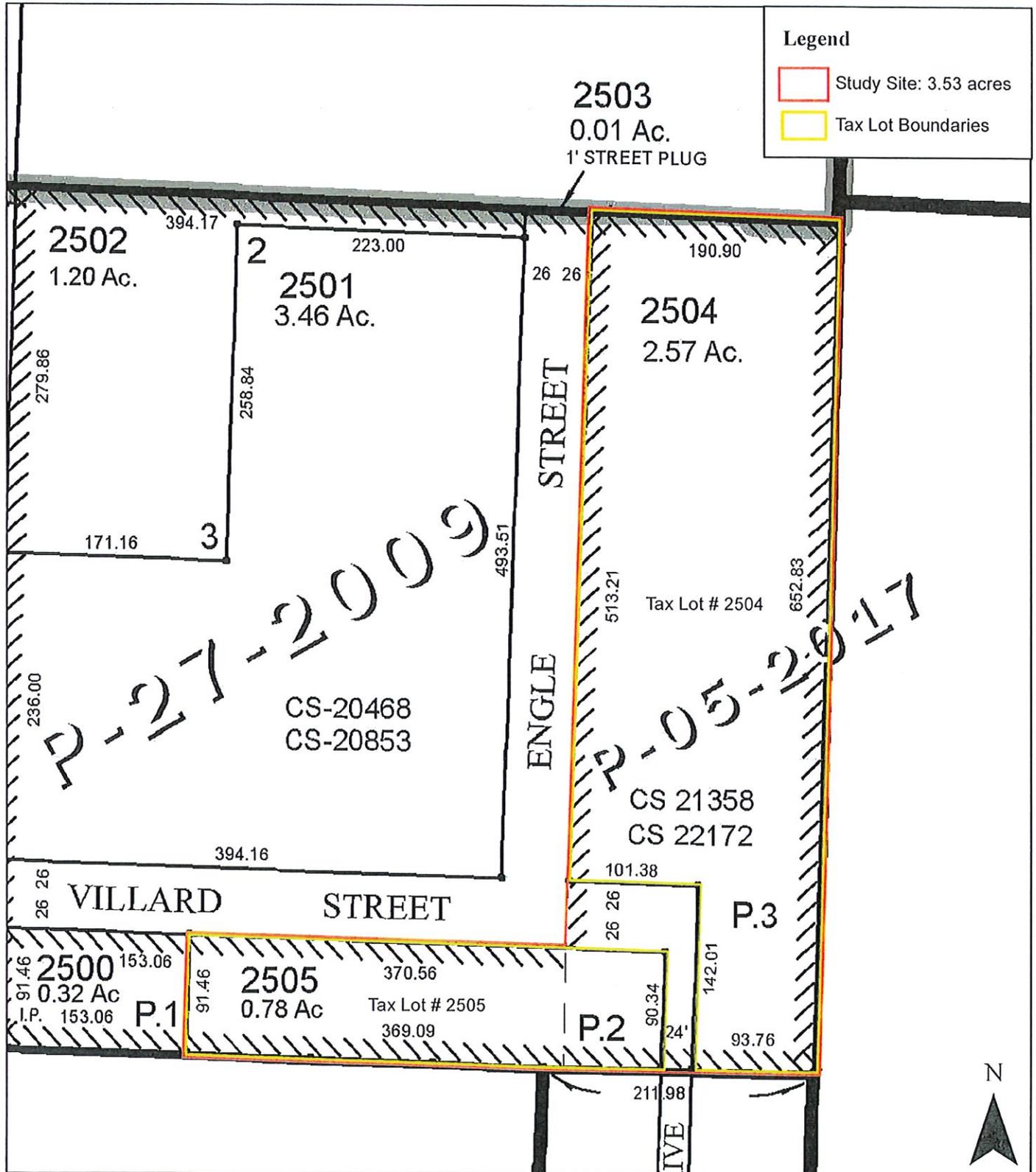
Data Source: ESRI, 2018; Jackson County GIS, 2018

Figure 1. Vicinity Map  
Villard & Engle Street Project Site



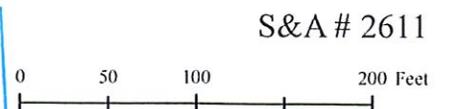
S&A # 2611





Date: 7/2/2018  
 1 inch = 100 feet  
 Data Source: ORMAP (www.ormap.net);  
 Jackson County GIS, 2018

Figure 2. Jackson County Tax Map: 391E11C  
 Villard & Engle Street Project Site



**Mapping Method and Precision Statement:** The mapped areas were based on soils, vegetation, and hydrology data gathered in the field by Schott & Associates. The sample plots and wetland boundaries were flagged and recorded utilizing a Trimble Geo XT hand-held unit to a +/- 3 foot accuracy. The GPS data were then imported into ArcGIS software to produce maps.

**Legend**

- Study Site: 3.53 acres
- Tax Lot Boundaries
- Fill
- Ditch Centerline
- X Sample Plots
- Photo Points

Contour: 2 Ft. Interval



Date: 7/2/2018

1 inch = 125 feet

Data Source: ESRI, 2018; USGS, National Elevation Data, 2013

DSL WD # 2018-0390

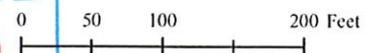
Approval Issued 10-03-2018

Approval Expires 10-03-2023

Figure 6. Wetland Delineation Map  
Villard & Engle Street Project Site

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 BY: .....

S&A # 2611



Subject **Re: Snowberry II**  
From tom madara <madaradesign@yahoo.com>  
To <dan@hortonarchitecture.com>  
Date 2019-03-23 19:29



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  - P1020530 - Copy.jpg (~825 KB)
  - P1020533.jpg (~966 KB)
  - P1020536.jpg (~519 KB)
  - P1020537.jpg (~524 KB)
- 

Thank you Dan, I will.

I was by the site this week and reviewed the Deodore Cedar on Villard Street. Here is what I found. I am attaching some pictures and have more if appropriate.

The condition of the tree is Poor. There is massive historical damage that happened many years ago. The tree continued growing with multiple leaders, a number of which were further damaged over time. The original dominant leader was destroyed at about 18' if elevation and the tree is currently 30-35' tall. There is currently no dominant leader and the natural structure of the tree no longer exists.

There are many large lower lateral branches some of which are severely damaged. I believe the tree was never appropriately cared for.

As a further sign of damage more recently, there is a trench 18" deep approximately 3' from the East side of the trunk that cuts through the entire root zone. This further goes to the conclusion I would draw and that is that this tree is a Hazard Tree and should be removed.

Thanks,

Tom Madara

Madara Design Inc

Off: 541-664-7055

Cell: 541-944-4287

[www.madaradesigninc.com/](http://www.madaradesigninc.com/)



## TREE PROTECTION NOTES

- A. LANDSCAPE ADJACENT TO THE PROJECT AREA SHALL BE PROTECTED FROM DAMAGE. NO STORAGE OF EQUIPMENT OR MATERIALS SHALL OCCUR WITHIN DRIP LINES OF TREES TO BE PRESERVED, AS IDENTIFIED ON THIS PLAN. ALL DAMAGE CAUSED BY CONSTRUCTION TO EXISTING TREES SHALL BE COMPENSATED FOR, BEFORE THE PROJECT WILL BE CONSIDERED COMPLETED.
- B. TREES THAT ARE SHOWN TO REMAIN SHALL BE PROTECTED WITH FENCING AS SHOWN ON PLANS WHERE REQUIRED. FENCING SHALL BE 6' TALL TEMPORARY CHAIN LINK PANELS INSTALLED WITH METAL CONNECTIONS SO THAT ALL PANELS ARE INTEGRATED, THESE FENCES SHALL BE INSTALLED TO PROHIBIT PASSAGE OF PEDESTRIANS AND/OR VEHICLES THROUGH IT.
- C. A CERTIFIED ARBORIST SHALL BE CONSULTED IF ANY PRUNING IS NECESSARY DURING CONSTRUCTION, ON TREES TO REMAIN.
- D. DISTURBANCE OF TREE PROTECTION FENCES PRIOR TO COMPLETION OF PROJECT, SHALL BE PERMITTED ONLY TO COMPLETE REQUIRED WORK WHICH OCCURS WITHIN THE DRIPLINE OF EXISTING TREES.
- E. IF TREE ROOTS OVER 2" ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL CONTACT A CERTIFIED ARBORIST TO ASSESS AND ROOT PRUNE AS NECESSARY.
- F. CONTRACTOR SHALL NOT DISTURB ROOTS OF TREES WHEN REMOVING SOIL OR PLANT MATERIAL.
- G. CONTRACTOR SHALL NOT RAISE THE SOIL LEVEL WITHIN THE DRIP LINES OF EXISTING TREES TO ACHIEVE POSITIVE DRAINAGE, EXCEPT TO MATCH GRADES WITH SIDEWALKS AND CURBS, AND IN THOSE AREAS, FEATHER THE ADDED TOPSOIL BACK TO EXISTING GRADE AT AN APPROXIMATELY 3:1 SLOPE.
- H. INSPECTION SCHEDULE:
  - 1) TREE PROTECTION FENCING PLACEMENT SHALL BE APPROVED BY OWNER'S REPRESENTATIVE BEFORE DEMOLITION BEGINS.
  - 2) ROUTINE INSPECTIONS OF FENCING AND SITE CONDITIONS WILL OCCUR DURING THE COURSE OF CONSTRUCTION, WORK SHALL CEASE IF FENCING IS DAMAGED OR MOVED WITHOUT PRIOR APPROVAL OR AS OUTLINED ABOVE.
  - 3) FINAL INSPECTION AT COMPLETION OF PROJECT TO DETERMINE CONDITION OF TREES.
- I. IRRIGATION OF TREES TO REMAIN:
  - 1) ALL EXISTING TREES TO REMAIN THAT HAVE BEEN IRRIGATED PRIOR TO CONSTRUCTION SHALL BE DEEP WATERED ONCE A MONTH FOR 8 HOURS THROUGHOUT THE DRY SEASON.
  - 2) DO NOT IRRIGATE TREES THAT HAVE NOT RECEIVED IRRIGATION PRIOR TO CONSTRUCTION UNLESS DIRECTED BY ARBORIST OR LANDSCAPE ARCHITECT.
  - 3) USE SOAKER HOSE PER DIAGRAM.



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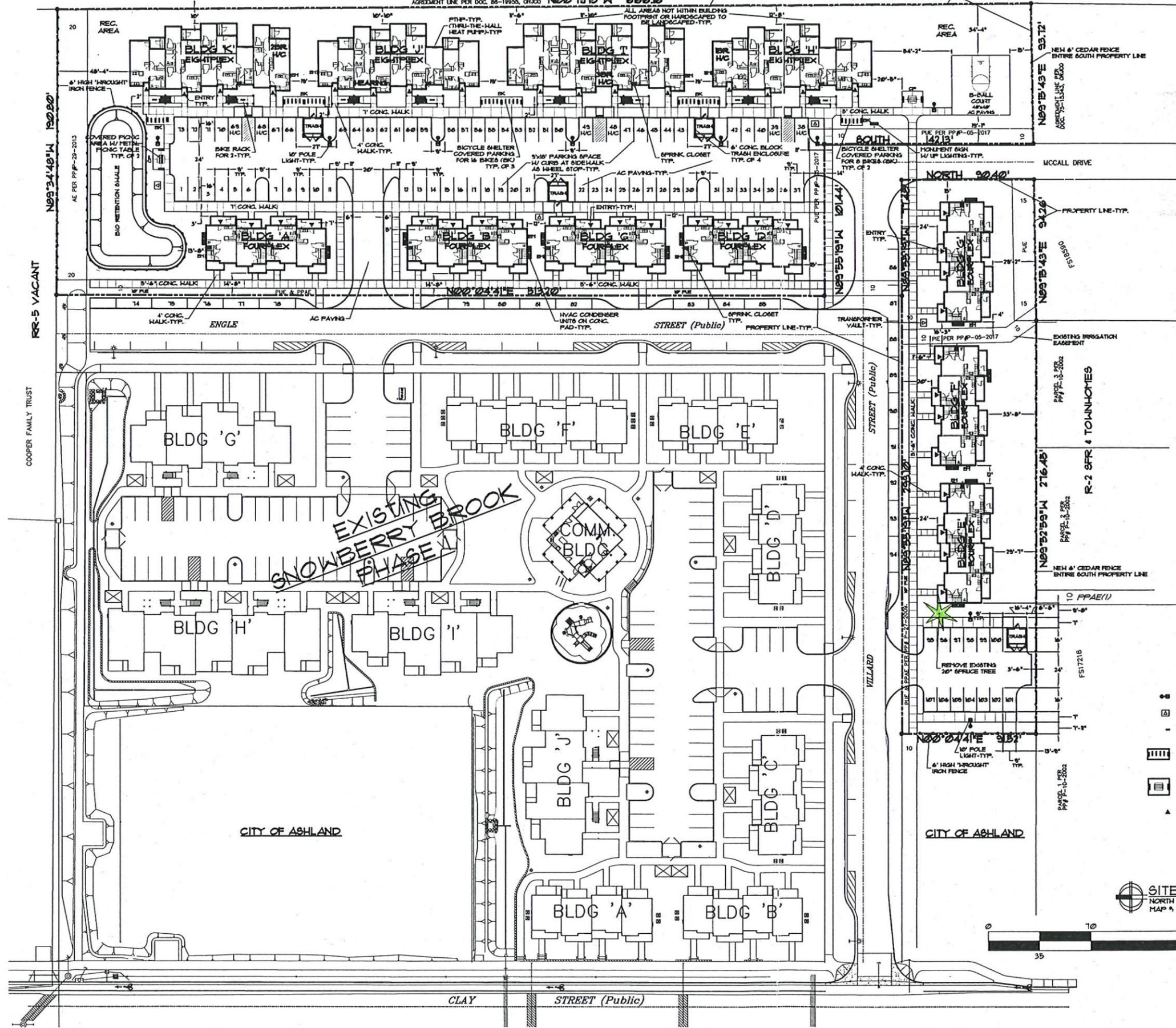
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MAY 13 2019

BY: .....

R-2 YMCA BALL FIELD - PARK OVERLAY  
 AGREEMENT LINE PER DOC. 88-19955, 09100 N00°19'15"W 666.16'



**SITE TABULATIONS**

ZONING: R-2

AREA OF SITE:  
 GROSS PARCEL 2: 11,957.1  
 GROSS PARCEL 2: 23,222.8  
 145,852.1, 3.35 ACRES TOTAL

DENSITY:  
 13.5 UNITS / AC BASE ALLOWABLE  
 BONUS FOR AFFORDABLE HOUSING + CONSERVATION = 50%  
 214.9 UNITS / AC ALLOWABLE WITH BONUS = 63 UNITS MAX ALLOWABLE  
 119.1 UNITS / AC PROPOSED X 3.35 ACRES = 60 UNITS PROPOSED

SITE COVERAGE:  
 BUILDING, COV. BICYCLE, & SHELTER FOOTPRINT: 33,866.4 / 145,852.1 = 23%  
 PAVED PARKING/DRIVEWAYS: 28,946.4 / 145,852.1 = 20%  
 SIDEWALKS: 5,867.1 / 145,852.1 = 4%  
 PERVIOUS: 61,153.4 / 145,852.1 = 42%  
 TOTAL 100%

RECREATION AREA: 12,265.9 / 145,852.1 = 8% OF LOT  
 PARKING LANDSCAPING: 2,301.1 / 28,946.4 = 8% OF PARKING AREA  
 SITE LANDSCAPING AREA: 53,965.4 / 145,852.1 = 37%

PARKING:  
 10 ONE BEDROOM UNITS x 15 = 150 SPACES  
 40 TWO BEDROOM UNITS x 15 = 600 SPACES  
 10 THREE BEDROOM UNITS x 20 = 200 SPACES  
 105 SPACES REQ. & 101 SPACES PROVIDED

BICYCLE PARKING:  
 LOCATED AT COVERED BIKE SHELTERS  
 10 ONE BEDROOM x 10 = 100  
 40 TWO BEDROOM x 15 = 600  
 10 THREE BEDROOM x 20 = 200  
 900 TOTAL REQ. & PROVIDED

UNITS	GROSS LIVING AREA
28 2BR TOWNHOUSES	• 9571 = 26,756.8
17 1BR FLATS	• 6324 = 6,324.0
17 2BR FLATS	• 5071 = 10,084.0
10 3BR FLATS	• 11,022 = 11,022.0
60 UNITS	TOTAL = 54,186.8

ALL RESIDENTIAL BUILDINGS ARE TWO STORY WITH BUILDING HEIGHT OF 21 FT.

**BUILDING TYPES:**  
 BLDG 'A' - FOURPLEX TOWNHOUSES  
 BLDG 'B' - FOURPLEX TOWNHOUSES  
 BLDG 'C' - FOURPLEX TOWNHOUSES  
 BLDG 'D' - FOURPLEX TOWNHOUSES  
 BLDG 'E' - FOURPLEX TOWNHOUSES  
 BLDG 'F' - FOURPLEX TOWNHOUSES  
 BLDG 'G' - FOURPLEX TOWNHOUSES  
 BLDG 'H' - EIGHTPLEX TWO STORY FLATS  
 BLDG 'I' - EIGHTPLEX TWO STORY FLATS  
 BLDG 'J' - EIGHTPLEX TWO STORY FLATS  
 BLDG 'K' - EIGHTPLEX TWO STORY FLATS

**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
 ASHLAND, OR

AN AFFORDABLE HOUSING DEVELOPMENT FOR HOUSING AUTHORITY OF JACKSON COUNTY  
 2251 TABLEROCK RD.  
 MEDFORD, OR  
 PROJECT NO: 18-129



**LEGEND**

- 10' POLE LIGHT
- ⊠ TRANSFORMER VAULT
- ELECTRICAL METER - (EM)
- ||||| BICYCLE SHELTER-COVERED PARKING FOR 8 BIKES - (BK) EIGHT SHELTERS IN ALL
- ▭ COVERED PICNIC AREA W/ METAL PICNIC TABLE - (CP) TWO PICNIC AREAS IN ALL
- ▲ ENTRY

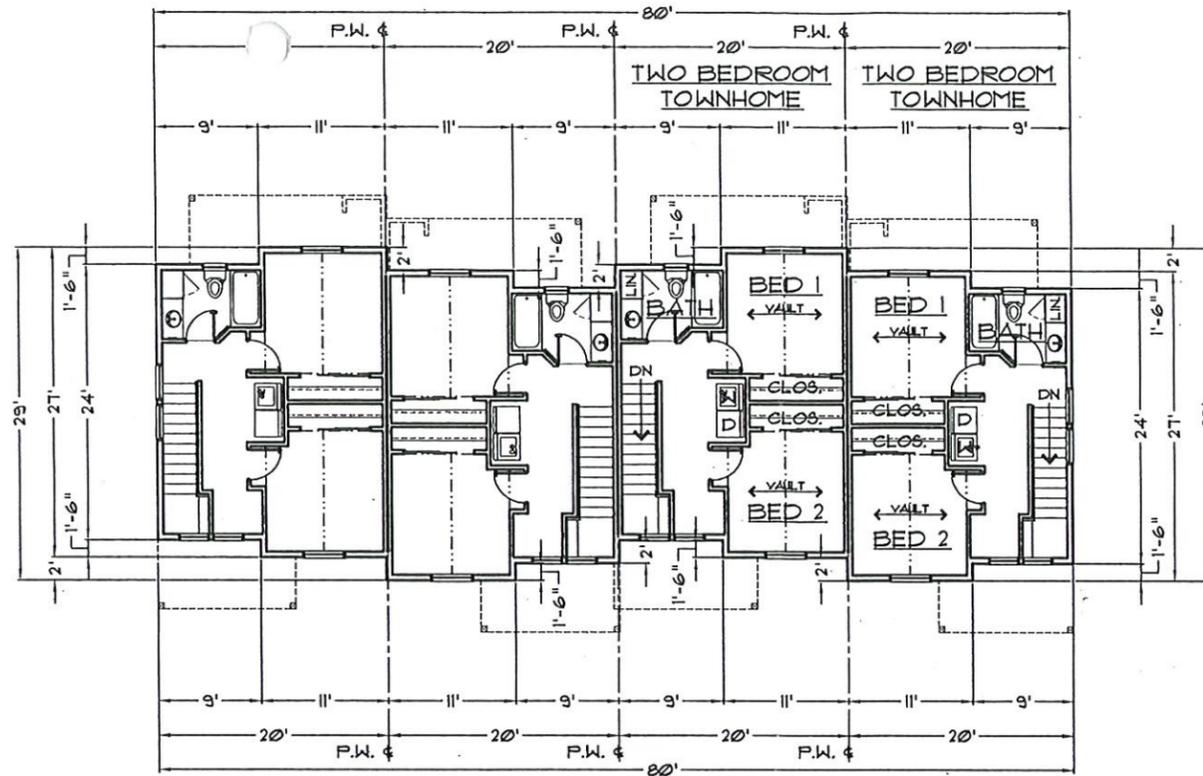
**SITE PLAN** 1"=10'  
 NORTH  
 MAP 9, 39 E 11C TAX LOT 2504 & 2505



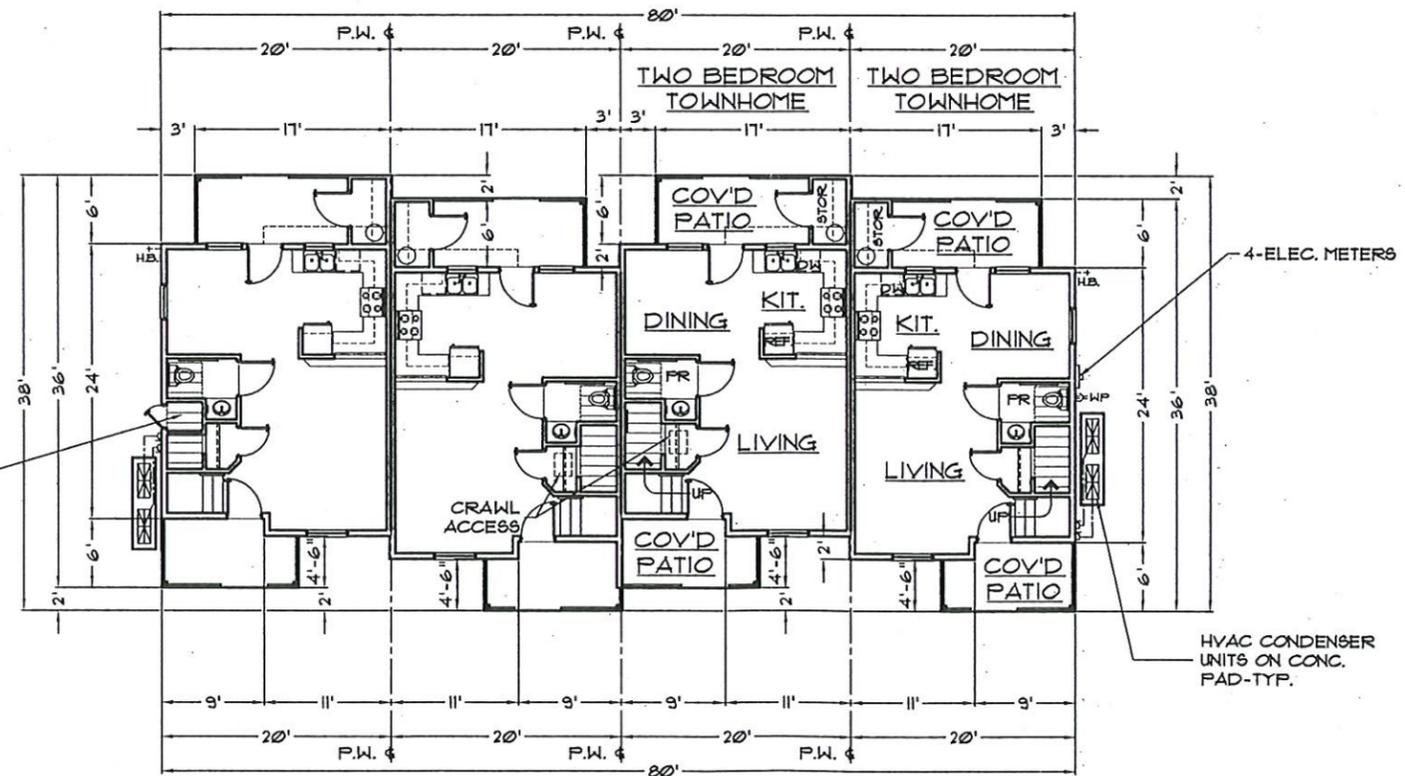
**DRH**  
 © HORTON ARCHITECTURE, INC. 2018  
 163 W. MAIN ST., P.O. BOX 682  
 EAGLE POINT, OR 97124  
 PHONE: 541-830-1014

The Drawings, Specifications, and other Documents prepared by the Architect for this project are to be used solely with respect to this project. The Architect shall retain ownership and all common law, statutory and other reserved rights to the Documents, including Copyright.

DATE: 9 MAY 2019  
 DRAWING NUMBER: 1 OF 11



UPPER FLOOR PLAN  
 1/16" = 1'-0" - 4 PLEX



LOWER FLOOR PLAN  
 1/16" = 1'-0" - 4 PLEX

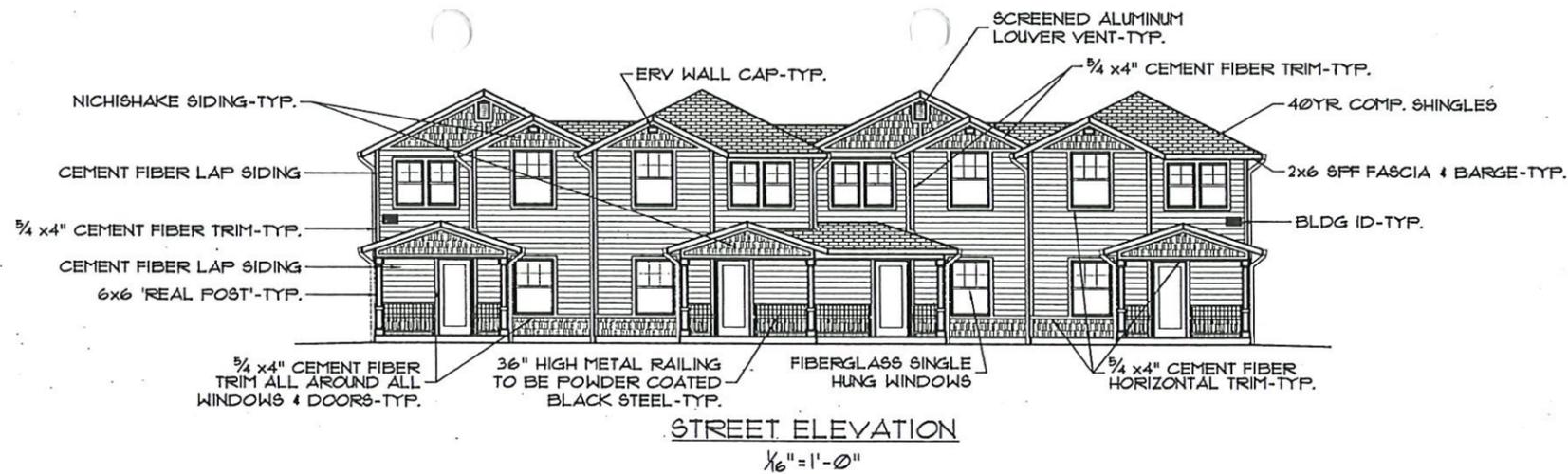
BUILDINGS 'A'-'G'  
 SNOWBERRY BROOK 2  
 ENGLE & VILLARD STREET,  
 ASHLAND, OR  
 AN AFFORDABLE  
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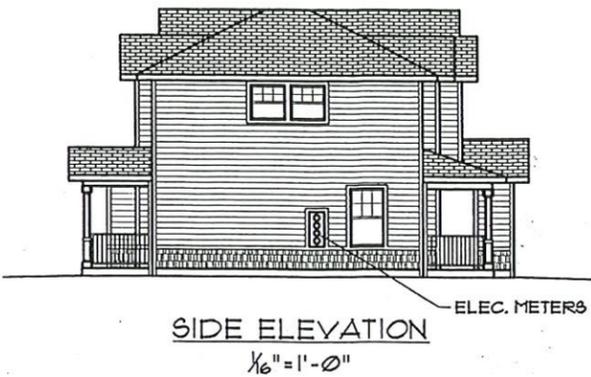


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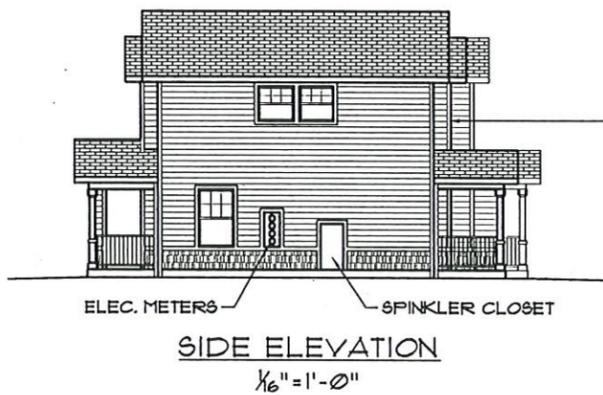
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 2 OF 11



**STREET ELEVATION**  
1/16" = 1'-0"



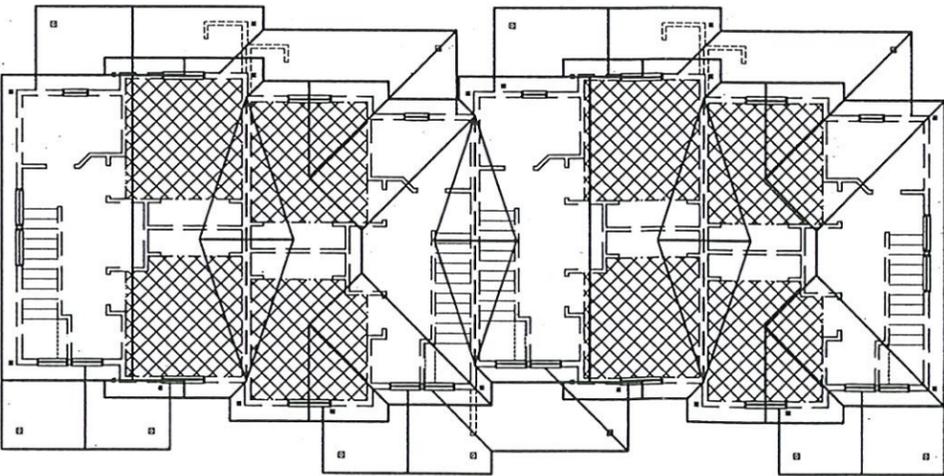
**SIDE ELEVATION**  
1/16" = 1'-0"



**SIDE ELEVATION**  
1/16" = 1'-0"



**REAR ELEVATION**  
1/16" = 1'-0"



**ROOF PLAN**  
1/16" = 1'-0" - 4 FLEX

**BUILDINGS 'A'-'G'**  
**SNOWBERRY BROOK 2**  
ENGLE & VILLARD STREET  
ASHLAND, OR

AN AFFORDABLE  
HOUSING DEVELOPMENT FOR  
**HOUSING AUTHORITY OF  
JACKSON COUNTY**  
2251 TABLEROCK RD.  
MEDFORD, OR  
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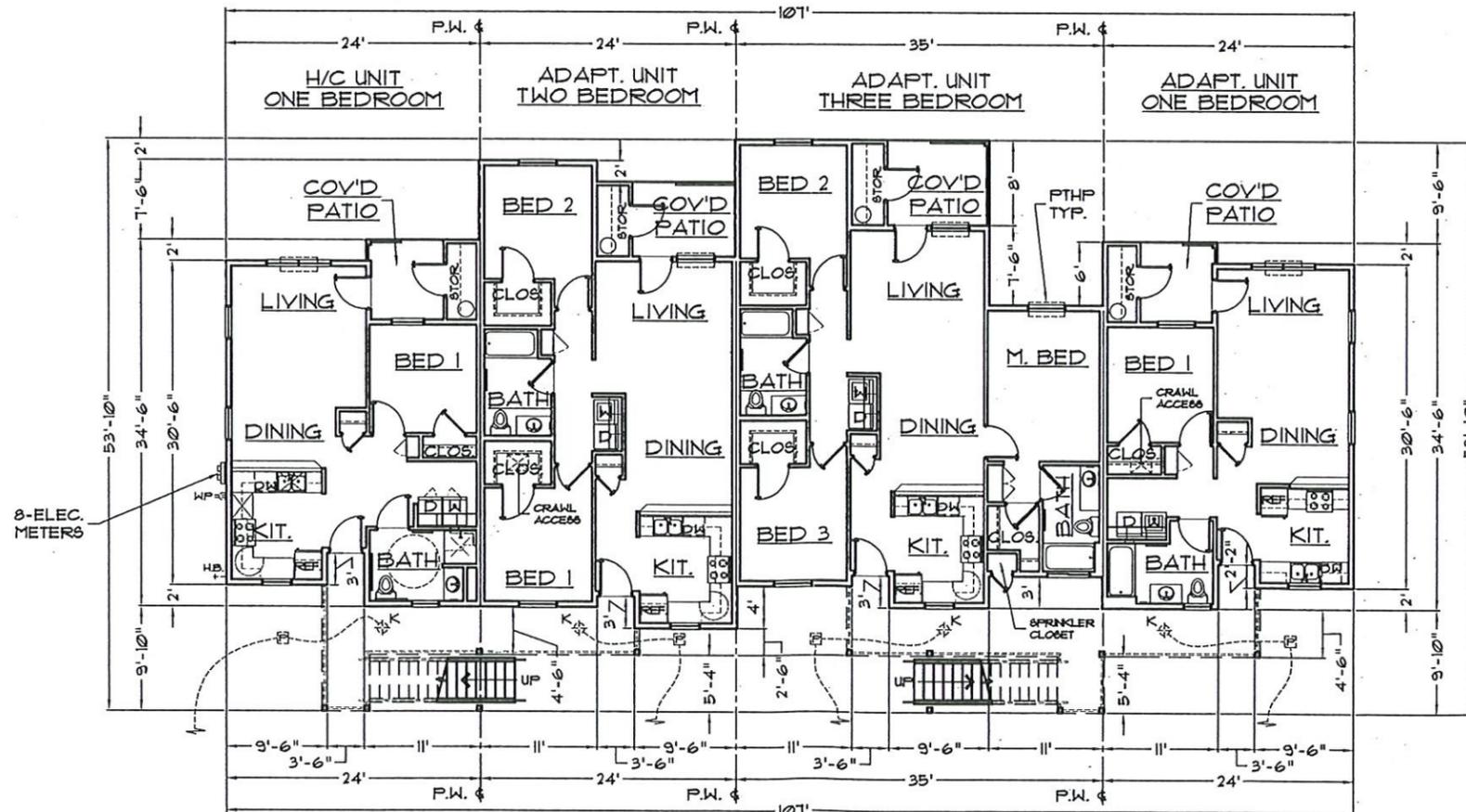
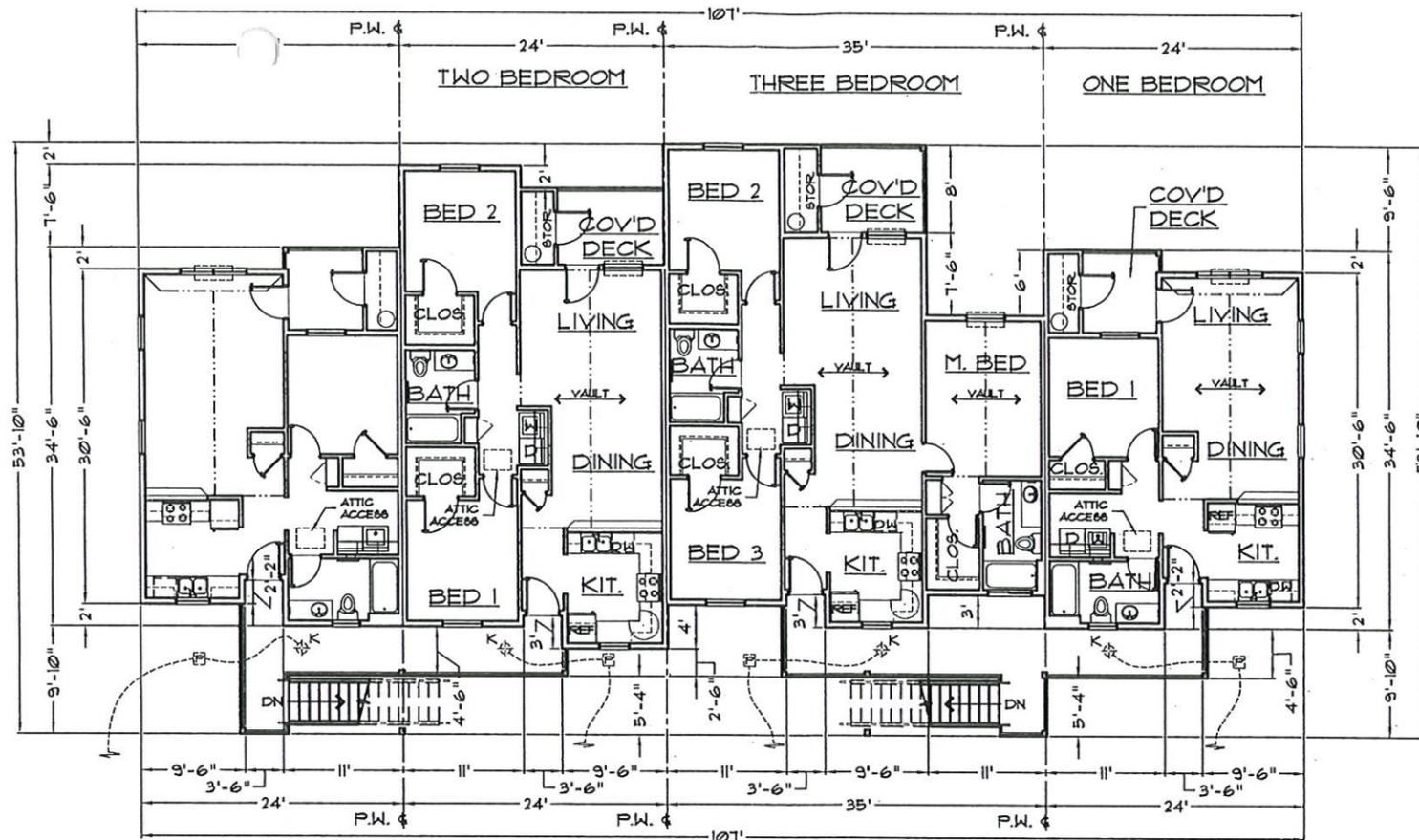
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DATE:  
9 MAY 2019

DRAWING NUMBER:  
3 OF 11



**BUILDING 'H'**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
 ASHLAND, OR

AN AFFORDABLE  
 HOUSING DEVELOPMENT FOR  
**HOUSING AUTHORITY OF  
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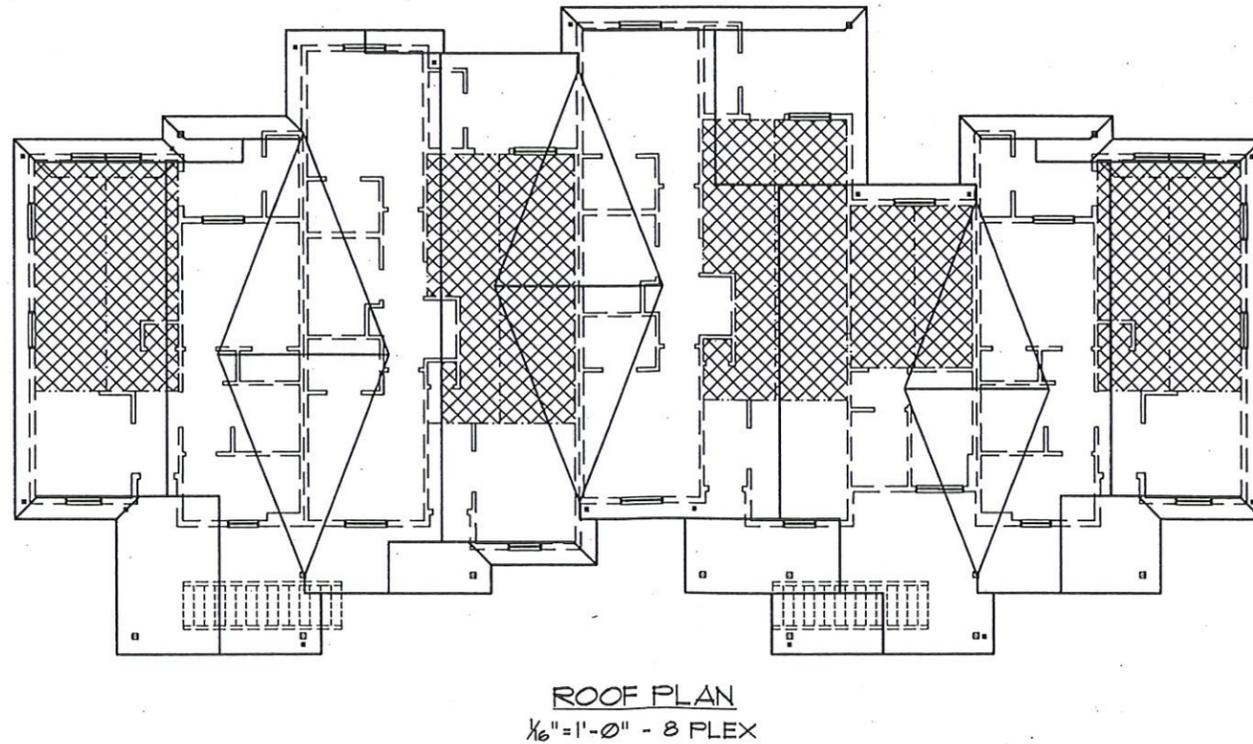
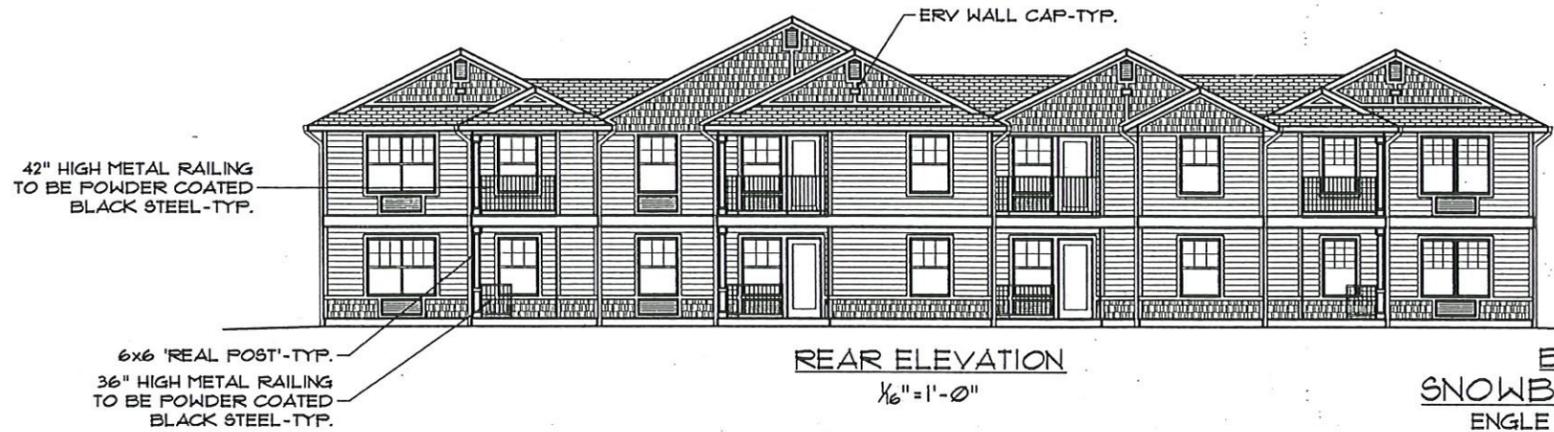
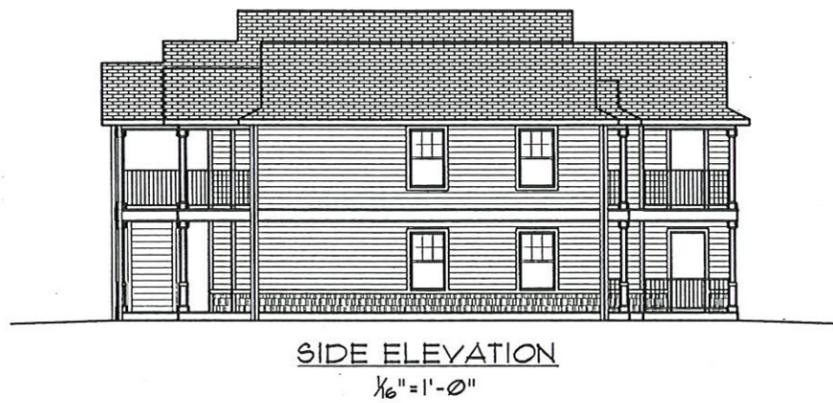
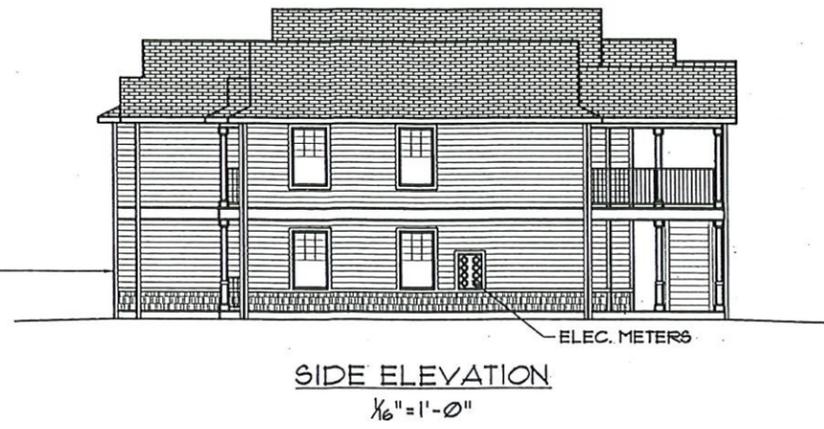
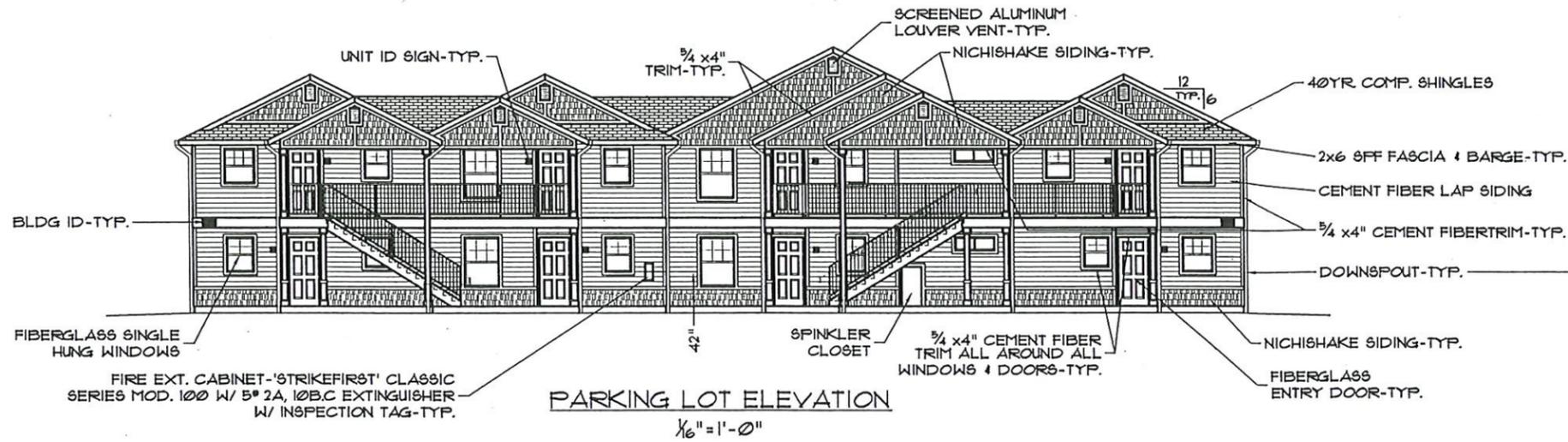
PROJECT NO: 18-129

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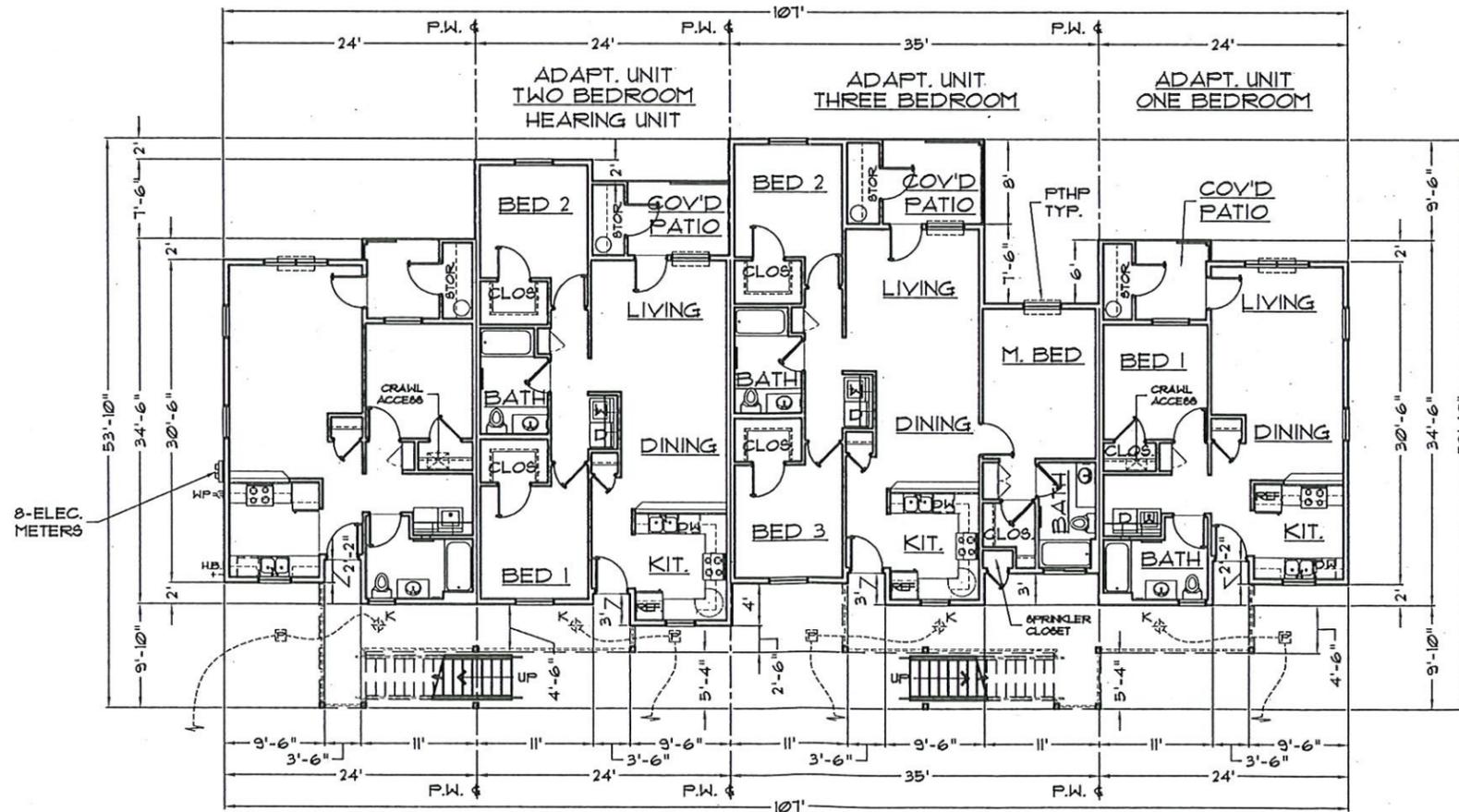
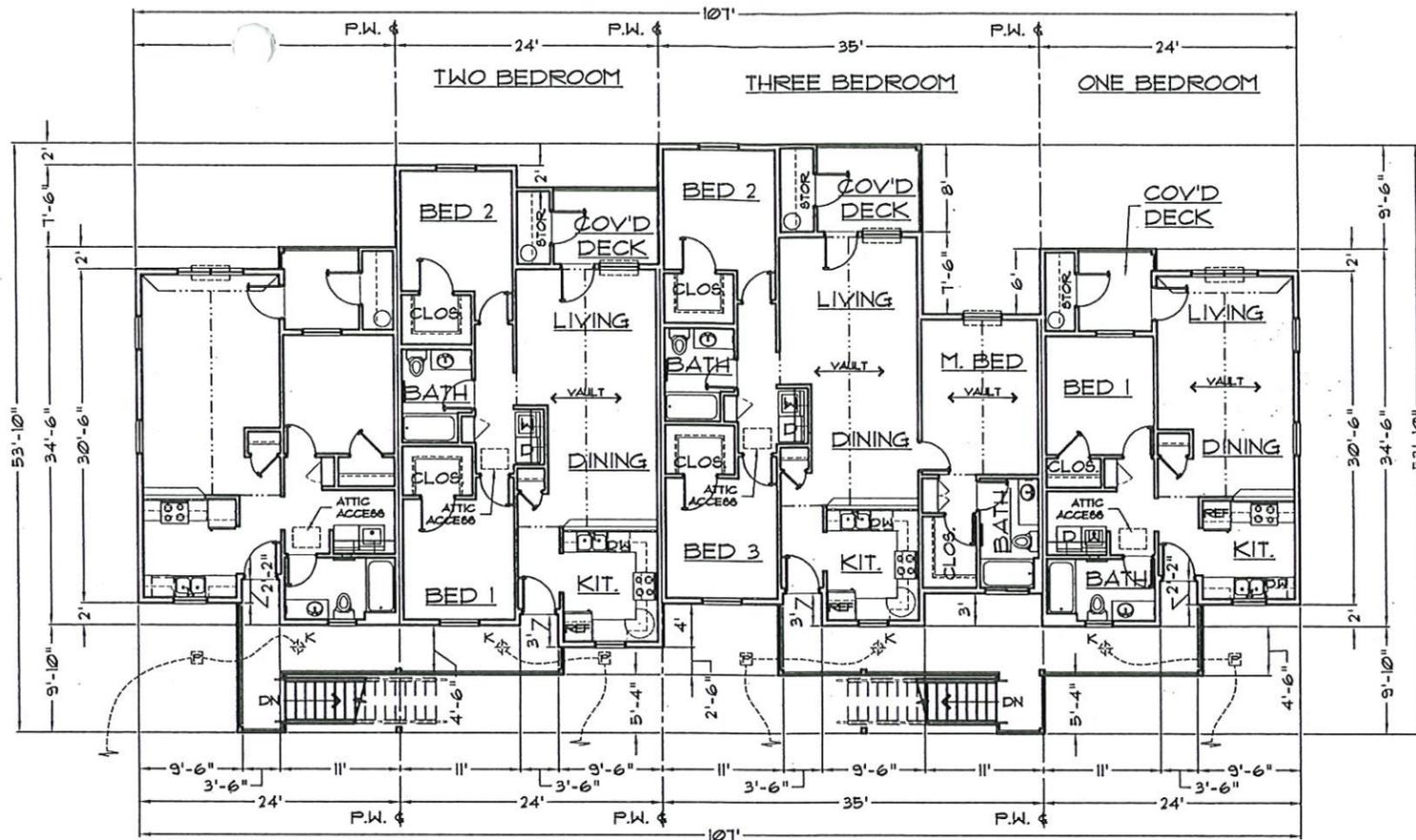
DATE:  
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 DRAWING NUMBER:  
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**BUILDING 'H'**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
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**BUILDING J'**  
**SNOWBERRY BROOK 2**  
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**HOUSING AUTHORITY OF  
 JACKSON COUNTY**  
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 MEDFORD, OR  
 PROJECT NO: 18-129



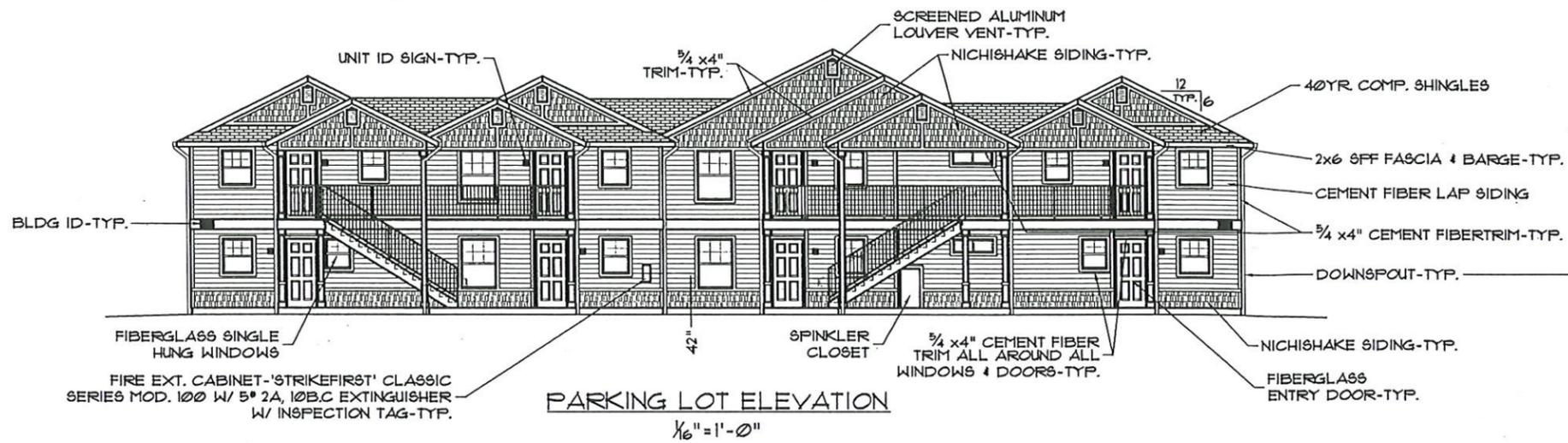
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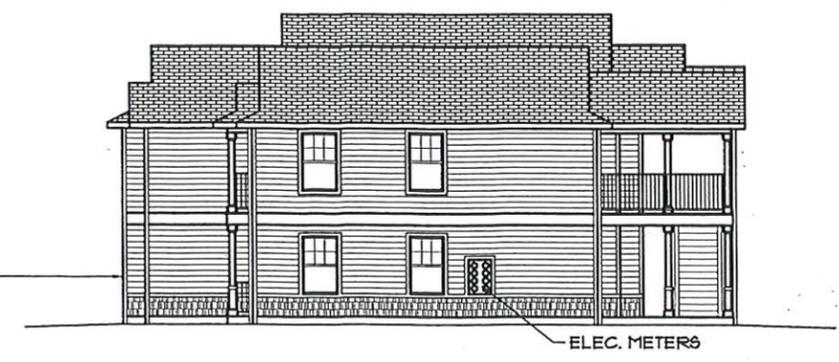
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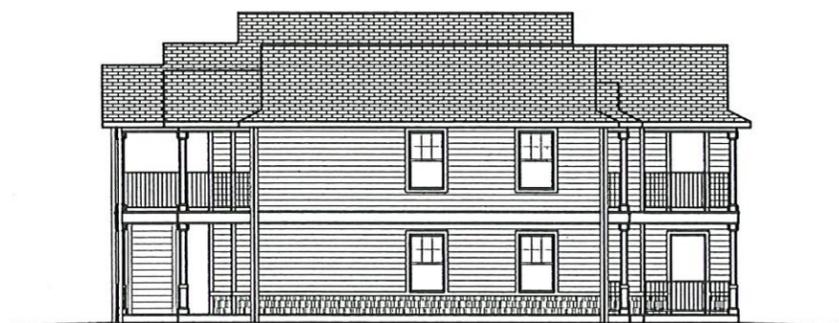
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**PARKING LOT ELEVATION**  
1/16" = 1'-0"



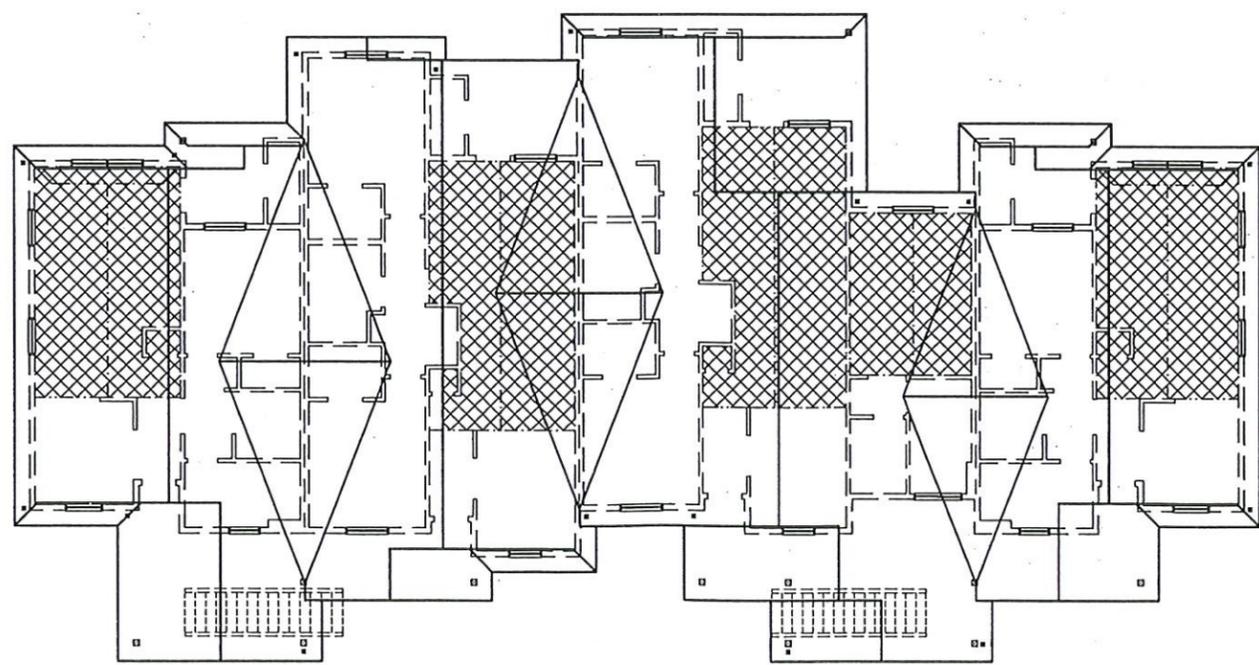
**SIDE ELEVATION**  
1/16" = 1'-0"



**SIDE ELEVATION**  
1/16" = 1'-0"



**REAR ELEVATION**  
1/16" = 1'-0"

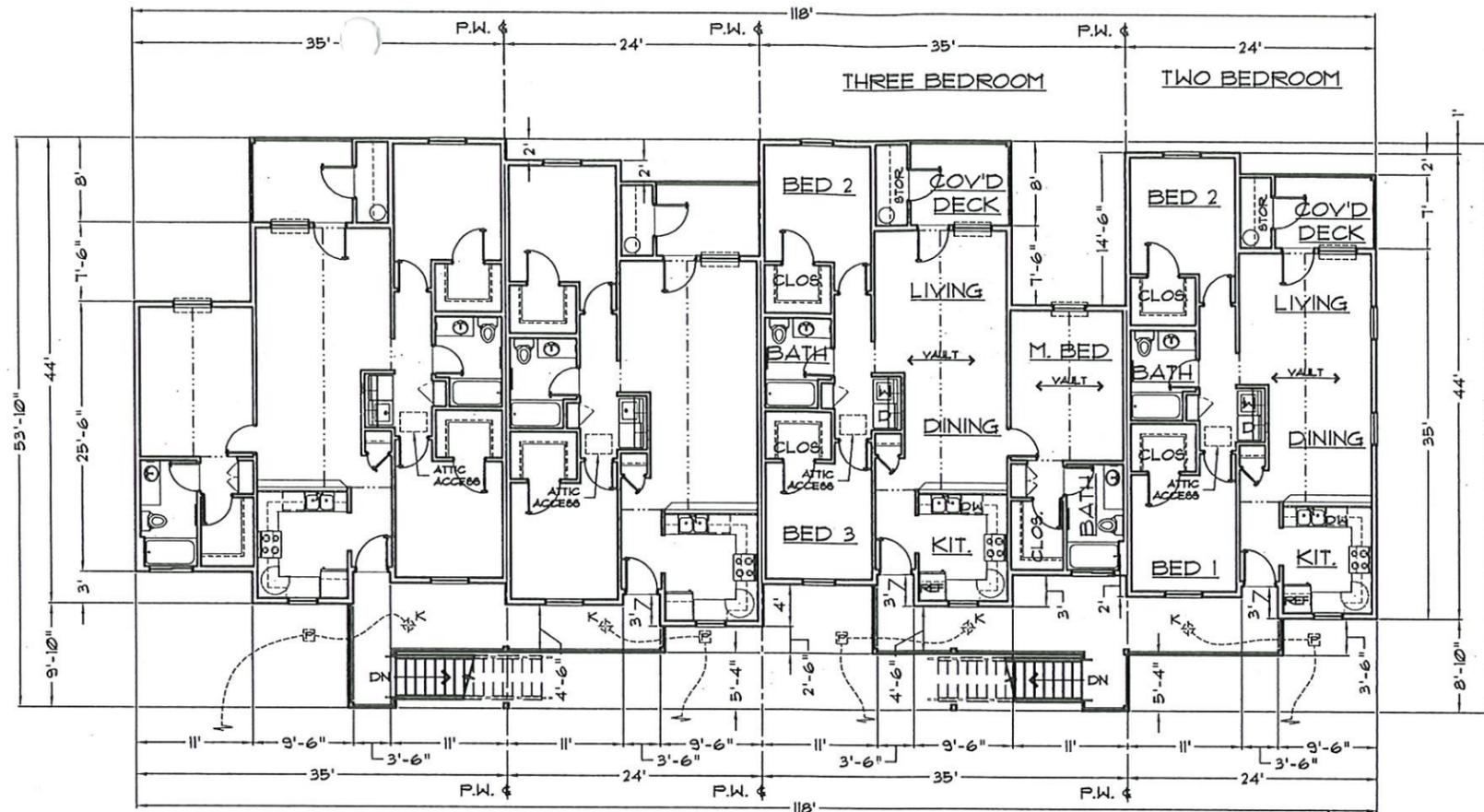


**ROOF PLAN**  
1/16" = 1'-0" - 8 PLEX

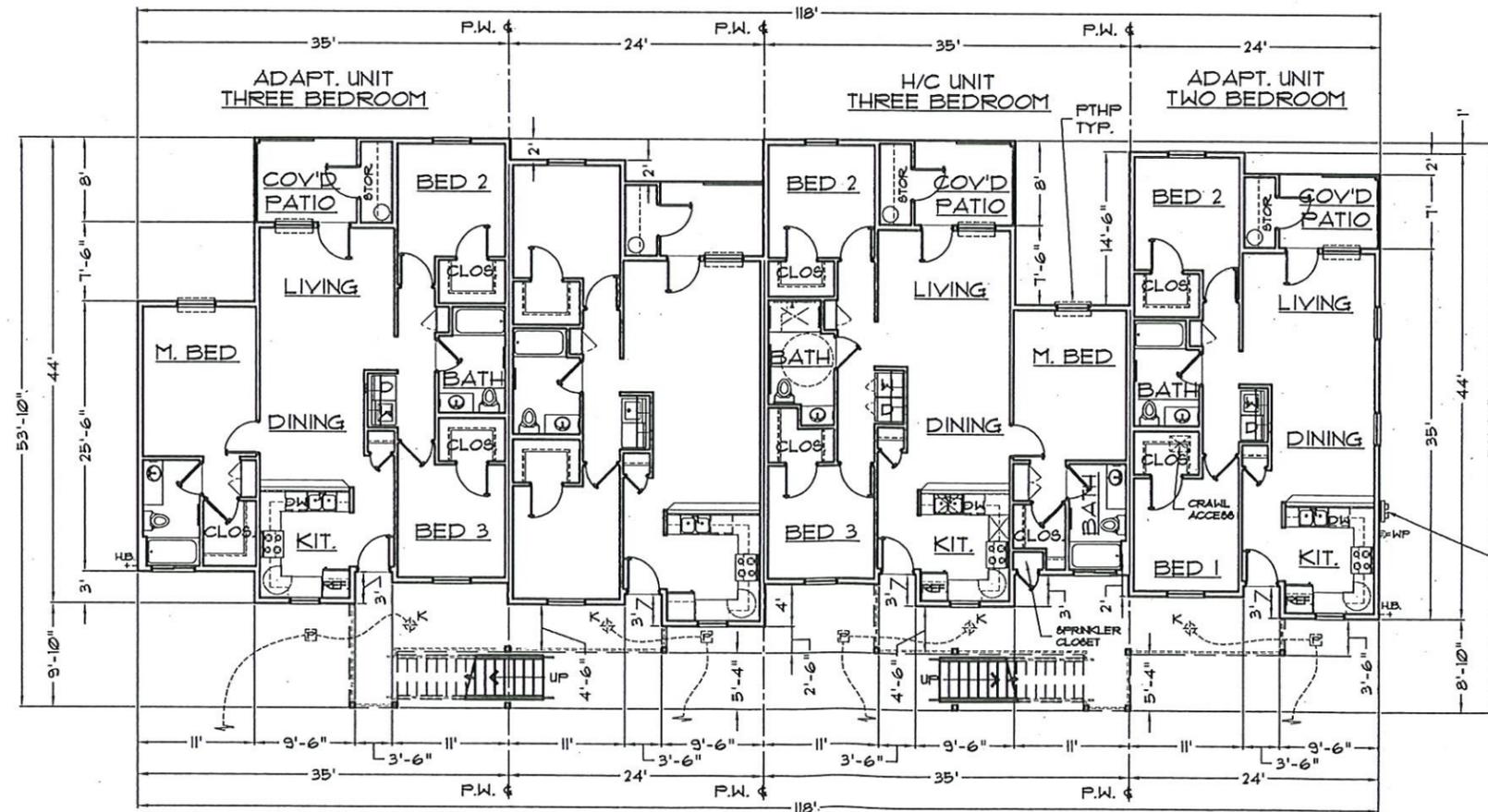
**BUILDING 'J'**  
**SNOWBERRY BROOK 2**  
ENGLE & VILLARD STREET  
ASHLAND, OR  
  
AN AFFORDABLE  
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UPPER FLOOR PLAN  
 1/16" = 1'-0" - 8 FLEX



LOWER FLOOR PLAN  
 1/16" = 1'-0" - 8 FLEX

**BUILDING 1**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
 ASHLAND, OR

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 HOUSING DEVELOPMENT FOR  
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**JACKSON COUNTY**  
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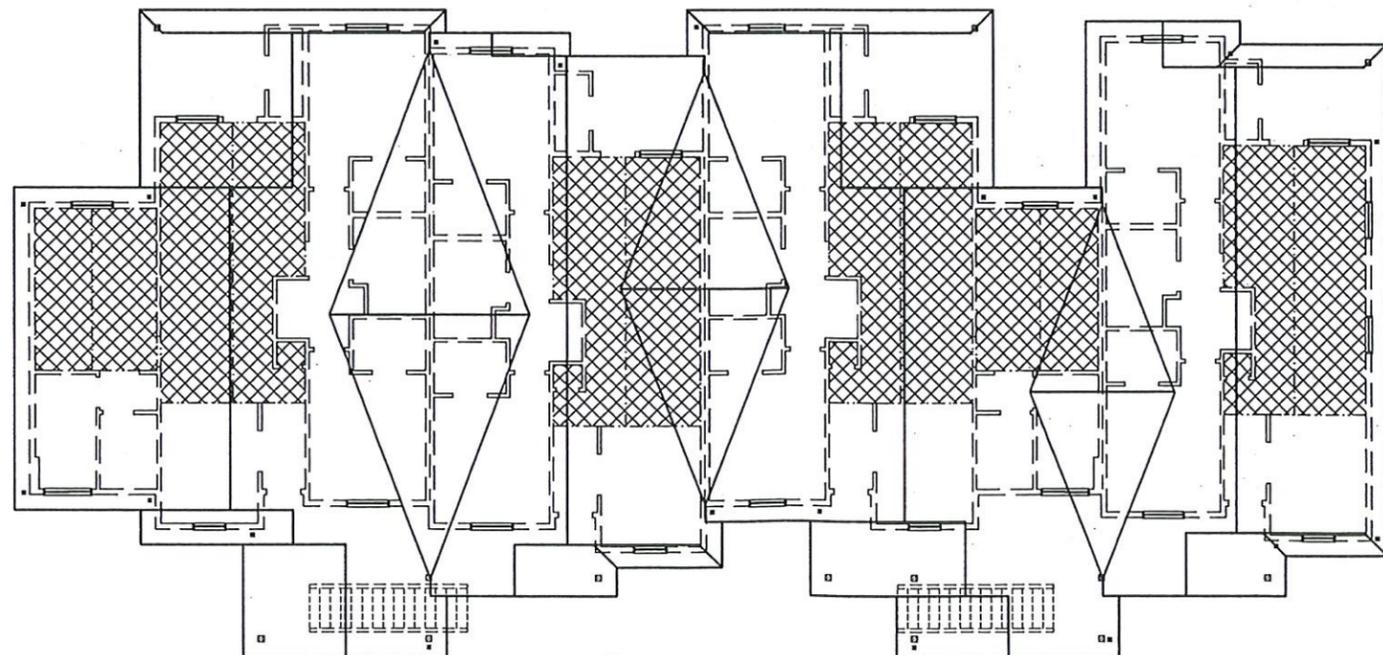
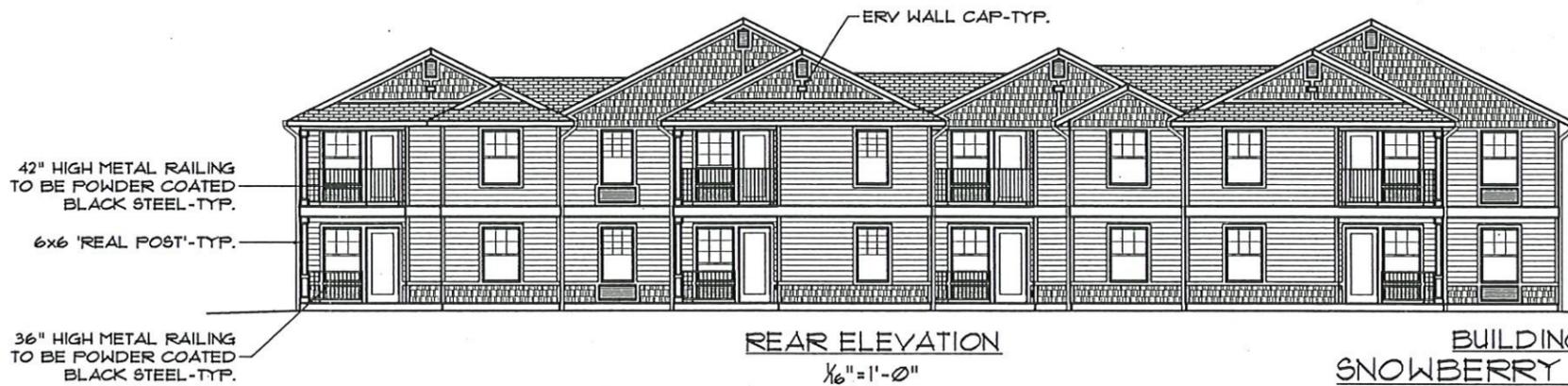
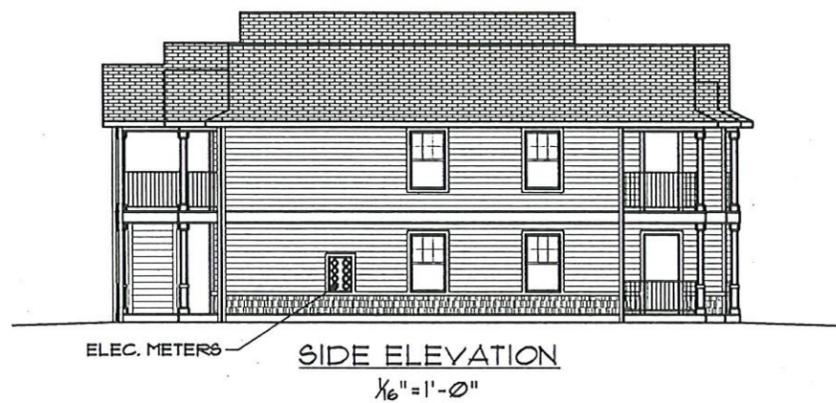
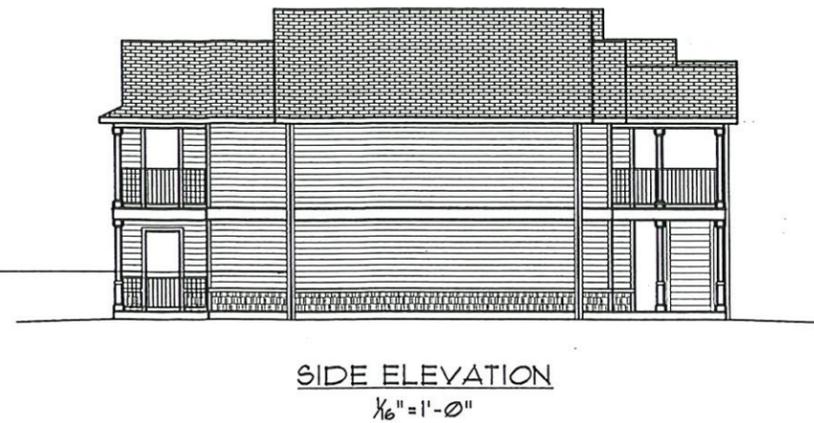
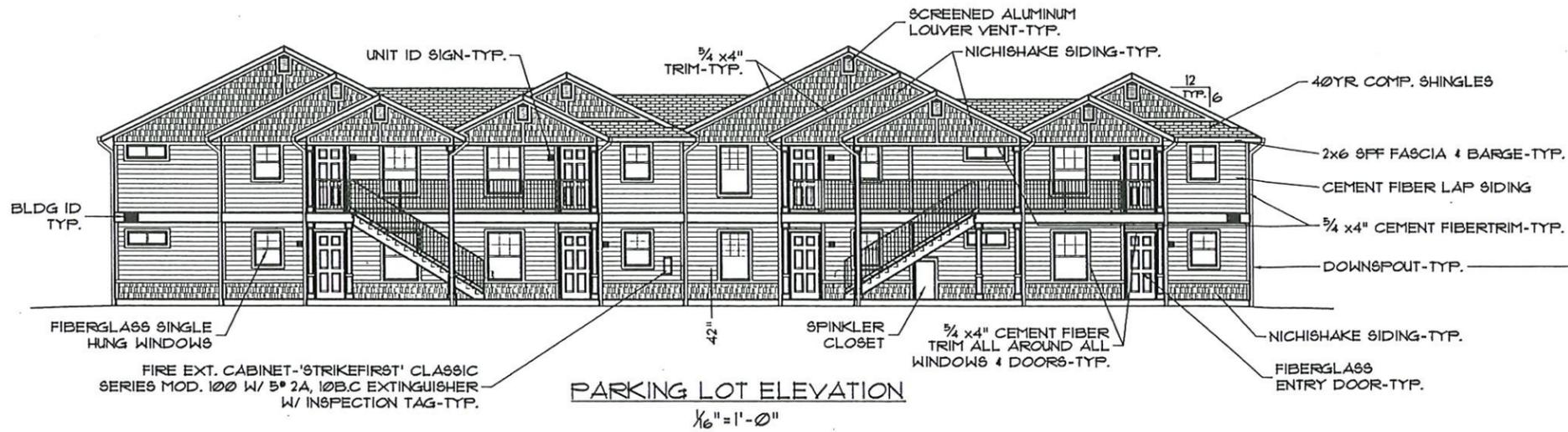
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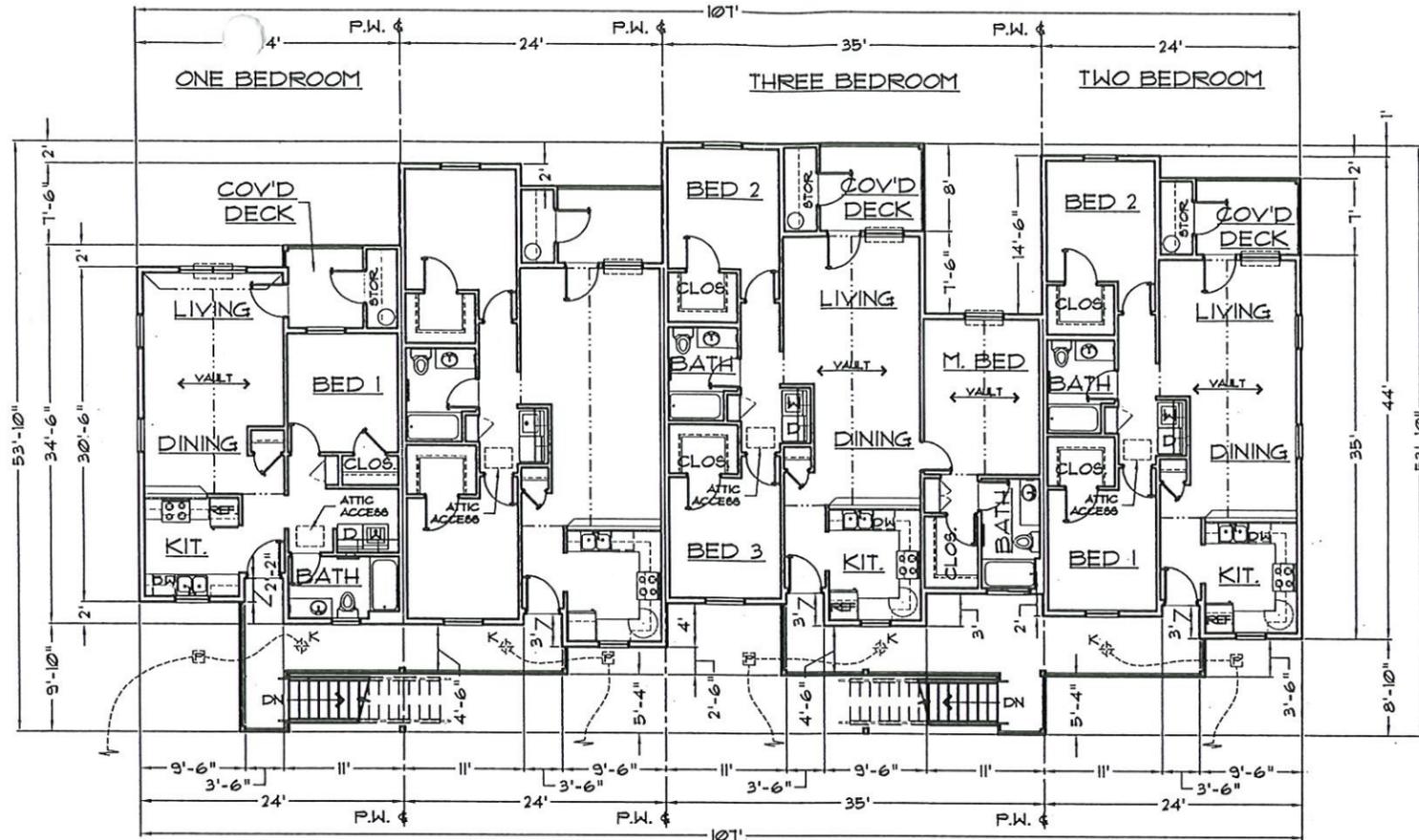




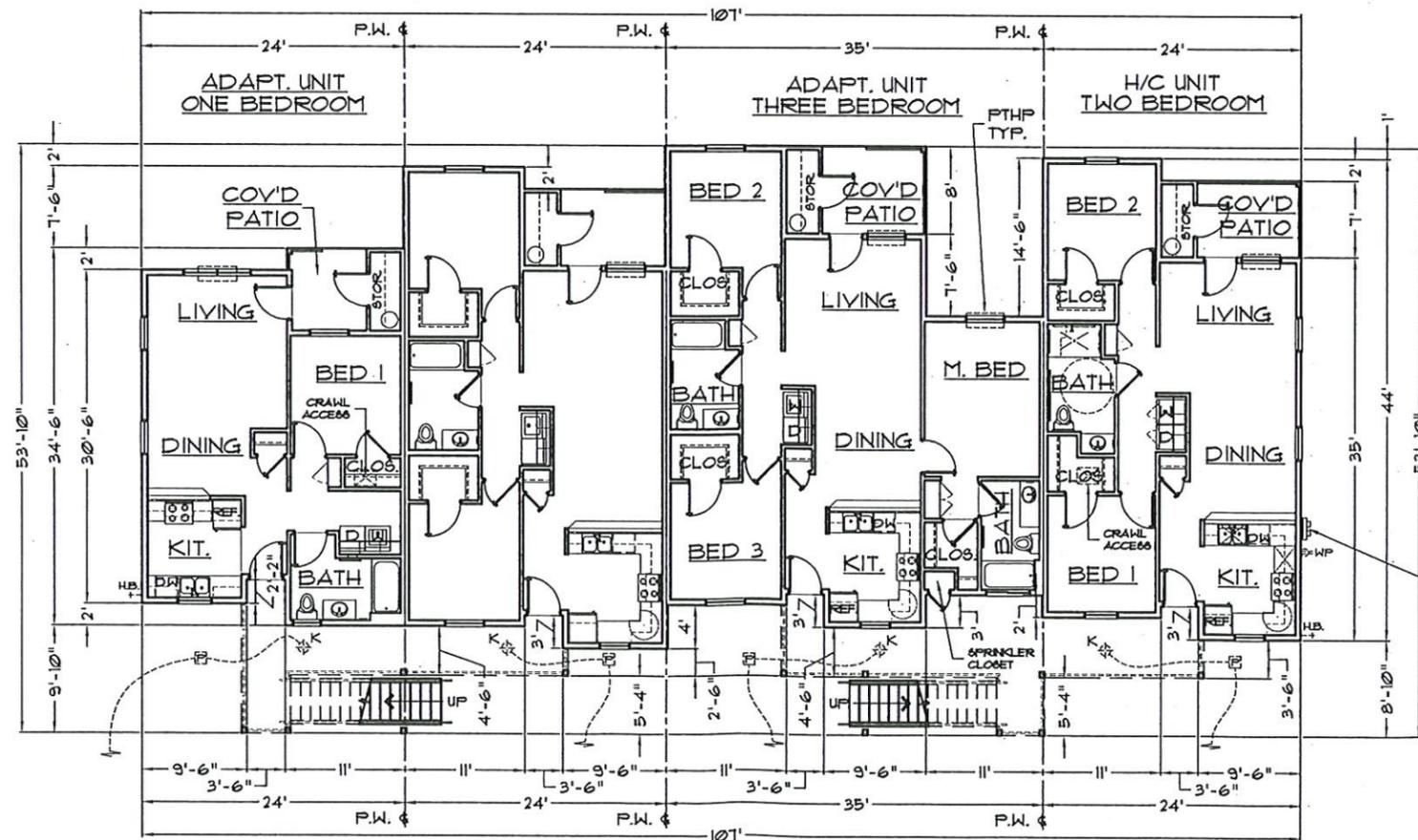
**BUILDING 'I'**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
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UPPER FLOOR PLAN  
 1/8" = 1'-0" - 8 FLEX



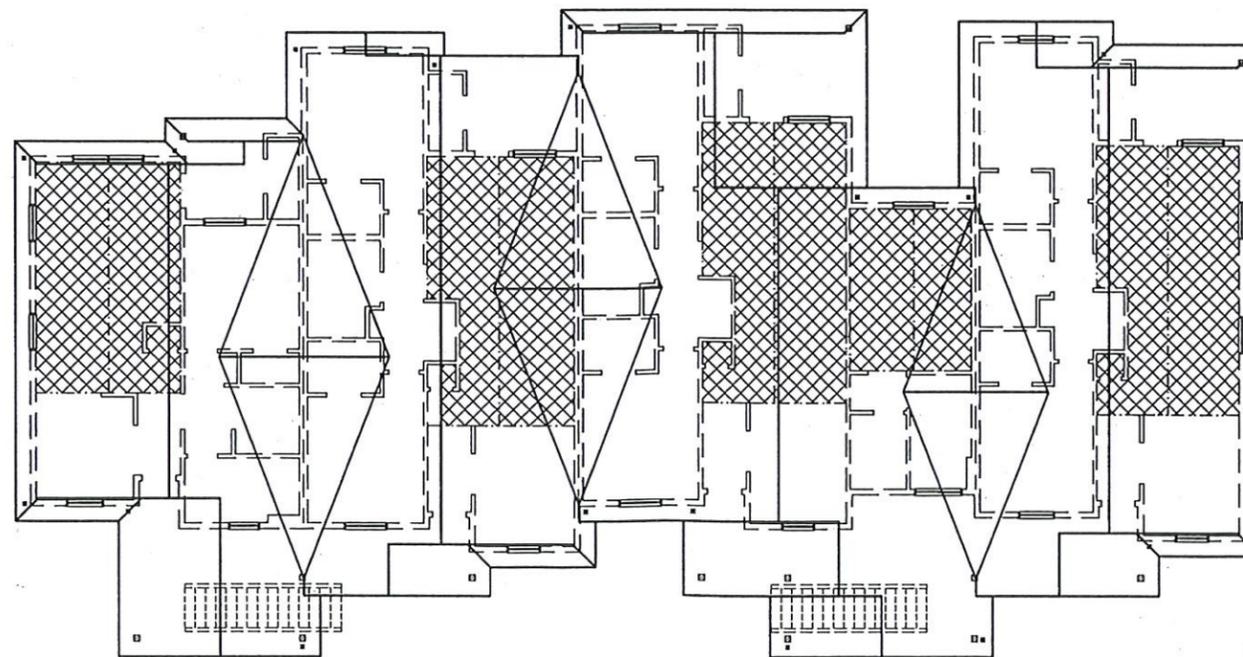
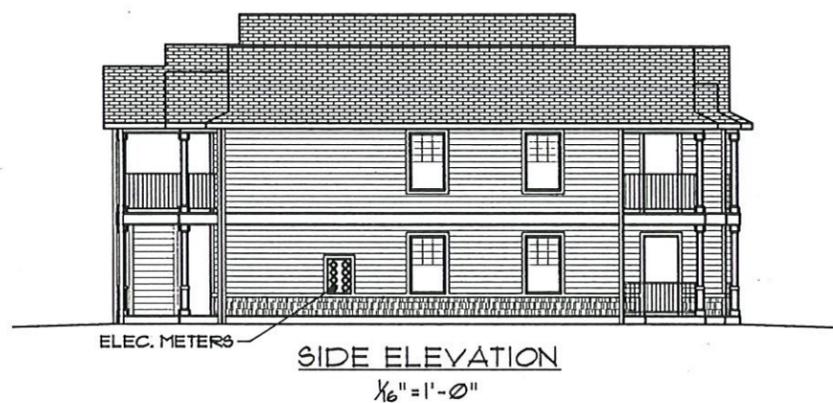
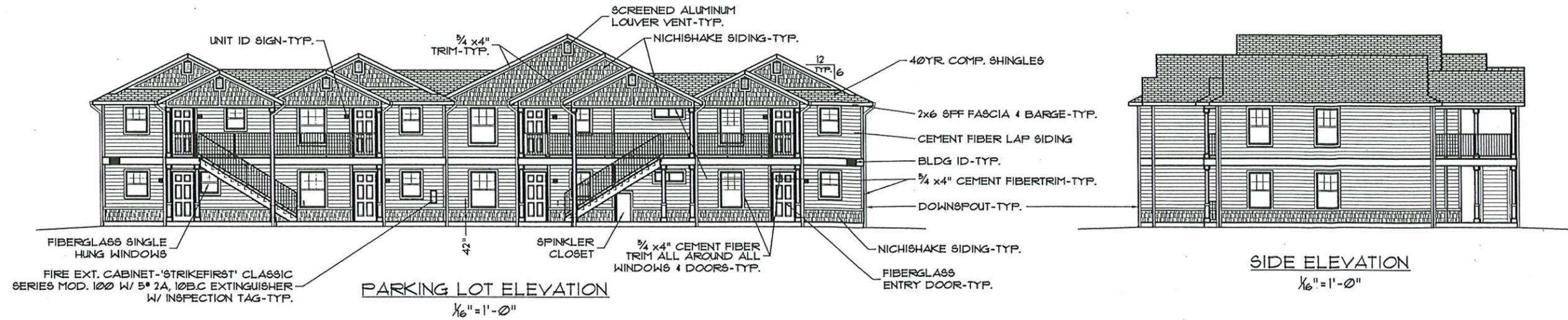
LOWER FLOOR PLAN  
 1/8" = 1'-0" - 8 FLEX

**BUILDING 'K'**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
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8-ELEC.  
 METERS

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**BUILDING 'K'**  
**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
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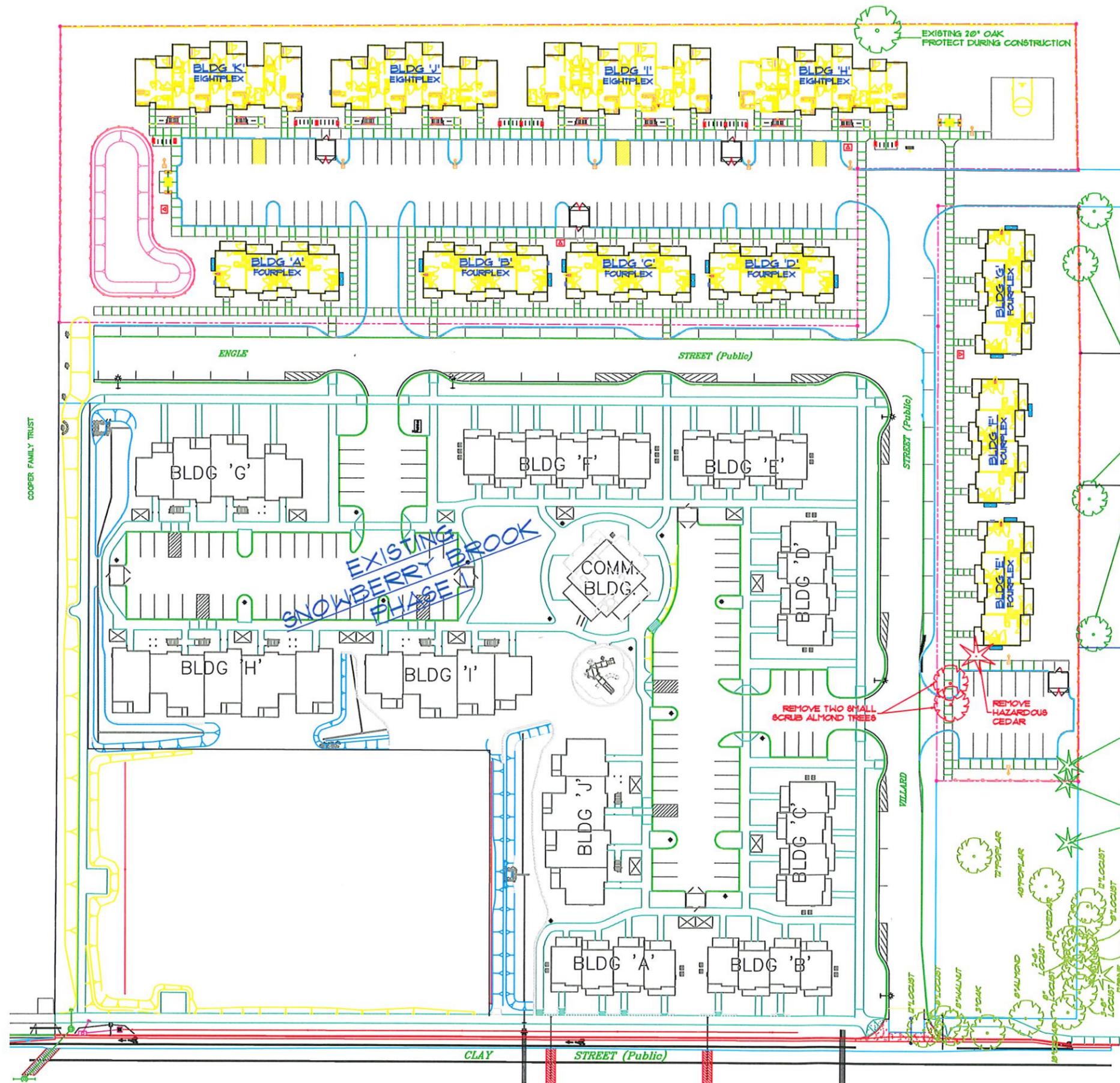


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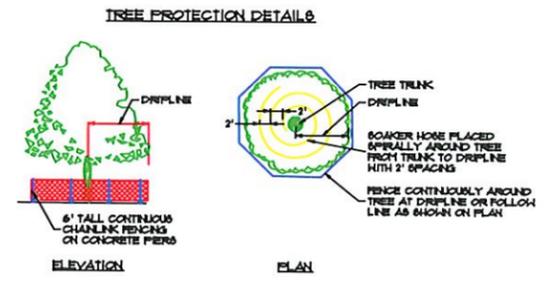
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- TREE PROTECTION NOTES**
- A. LANDSCAPE ADJACENT TO THE PROJECT AREA SHALL BE PROTECTED FROM DAMAGE. NO STORAGE OF EQUIPMENT OR MATERIALS SHALL OCCUR WITHIN DRIP LINES OF TREES TO BE PRESERVED, AS IDENTIFIED ON THIS PLAN. ALL DAMAGE CAUSED BY CONSTRUCTION TO EXISTING TREES SHALL BE COMPENSATED FOR, BEFORE THE PROJECT WILL BE CONSIDERED COMPLETED.
  - B. TREES THAT ARE SHOWN TO REMAIN SHALL BE PROTECTED WITH FENCING AS SHOWN ON PLANS WHERE REQUIRED. FENCING SHALL BE 6' TALL TEMPORARY CHAIN LINK PANELS INSTALLED WITH METAL CONNECTIONS SO THAT ALL PANELS ARE INTERLOCKED. THESE FENCES SHALL BE INSTALLED TO PREVENT PASSAGE OF PEDESTRIANS AND/OR VEHICLES THROUGH IT.
  - C. A CERTIFIED ARBORIST SHALL BE CONSULTED IF ANY PRUNING IS NECESSARY DURING CONSTRUCTION, ON TREES TO REMAIN.
  - D. DISTURBANCE OF TREE PROTECTION FENCES PRIOR TO COMPLETION OF PROJECT, SHALL BE PERMITTED ONLY TO COMPLETE REQUIRED WORK WHICH OCCURS WITHIN THE DRIP LINE OF EXISTING TREES.
  - E. IF TREE ROOTS OVER 2" ARE ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL CONTACT A CERTIFIED ARBORIST TO ASSESS AND ROOT PRUNE AS NECESSARY.
  - F. CONTRACTOR SHALL NOT DISTURB ROOTS OF TREES WHEN REMOVING SOIL OR PLANT MATERIAL.
  - G. CONTRACTOR SHALL NOT RAISE THE SOIL LEVEL WITHIN THE DRIP LINES OF EXISTING TREES TO ACHIEVE POSITIVE DRAINAGE, EXCEPT TO MATCH GRADES WITH SIDEWALKS AND CURBS, AND IN THOSE AREAS, FEATHER THE ADDED TOPSOIL BACK TO EXISTING GRADE AT AN APPROXIMATELY 3:1 SLOPE.
  - H. INSPECTION SCHEDULE:
    - 1) TREE PROTECTION FENCING PLACEMENT SHALL BE APPROVED BY OWNER'S REPRESENTATIVE BEFORE DEMOLITION BEGINS.
    - 2) ROUTINE INSPECTIONS OF FENCING AND SITE CONDITIONS WILL OCCUR DURING THE COURSE OF CONSTRUCTION. WORK SHALL CEASE IF FENCING IS DAMAGED OR MOVED WITHOUT PRIOR APPROVAL OR AS OUTLINED ABOVE.
    - 3) FINAL INSPECTION AT COMPLETION OF PROJECT TO DETERMINE CONDITION OF TREES.
  - I. IRRIGATION OF TREES TO REMAIN:
    - 1) ALL EXISTING TREES TO REMAIN THAT HAVE BEEN IRRIGATED PRIOR TO CONSTRUCTION SHALL BE DEEP WATERED ONCE A MONTH FOR 8 HOURS THROUGHOUT THE DRY SEASON.
    - 2) DO NOT IRRIGATE TREES THAT HAVE NOT RECEIVED IRRIGATION PRIOR TO CONSTRUCTION UNLESS DIRECTED BY ARBORIST OR LANDSCAPE ARCHITECT.
    - 3) USE SOAKER HOSE PER DIAGRAM.



**SNOWBERRY BROOK 2**  
 ENGLE & VILLARD STREET  
 ASHLAND, OR

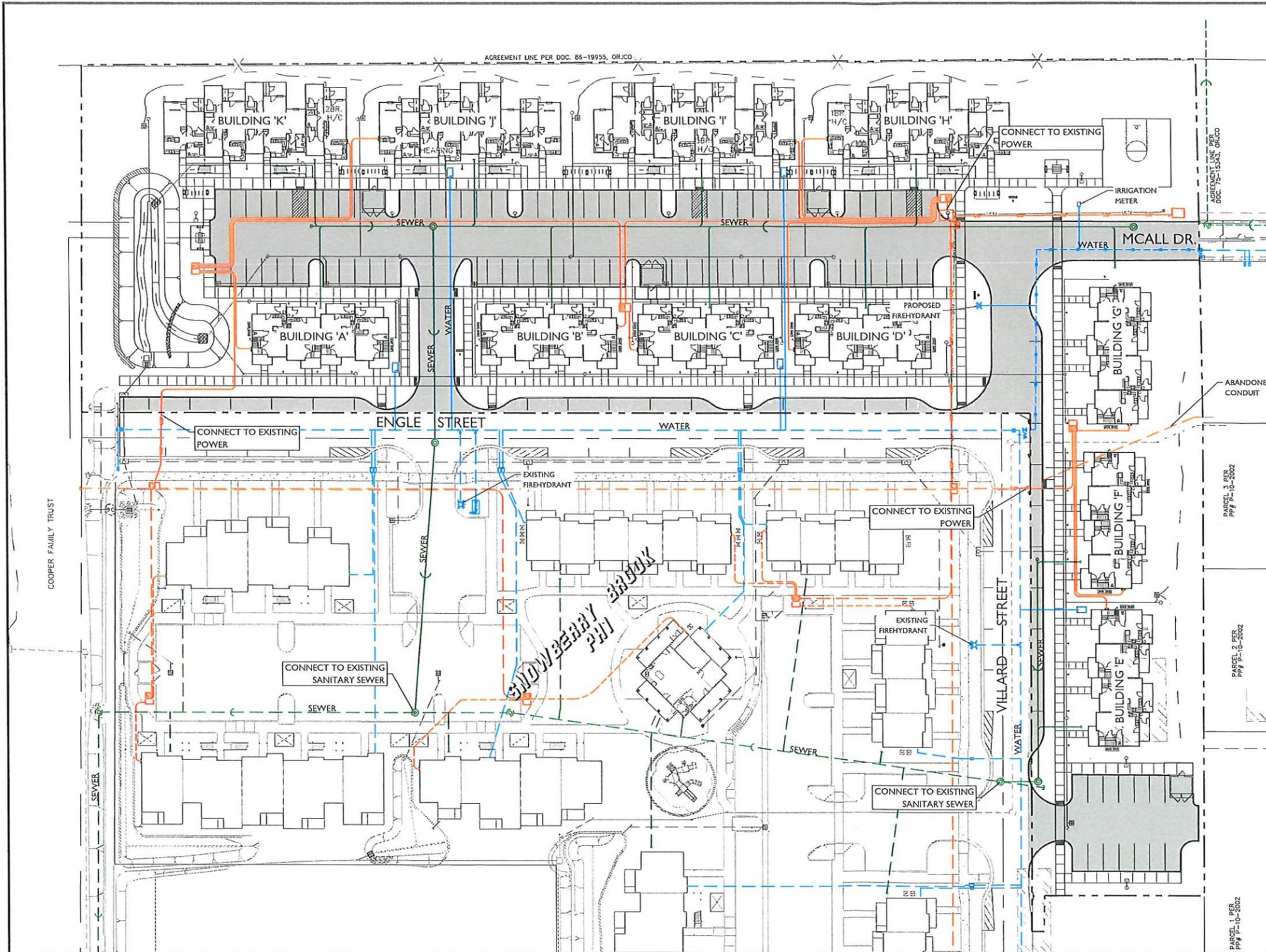
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 MEDFORD, OR  
 PROJECT NO: 18-129

**TREE PROTECTION PLAN**  
 NORTH  
 MAP & 39 E IC TAX LOT 2504 & 2505

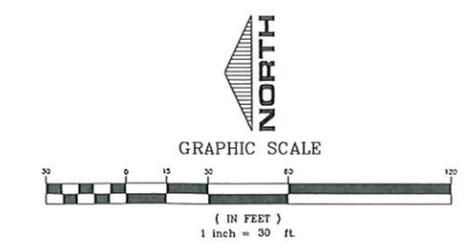
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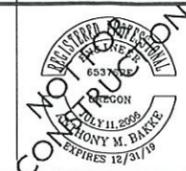
- LEGEND**
- EXISTING WATER MAIN
  - EXISTING FIRE HYDRANT
  - PROPOSED WATER MAIN
  - PROPOSED FIRE HYDRANT
  - PROPOSED SEWER MANHOLE OR CLEANOUT
  - PROPOSED SANITARY SEWER MAIN
  - EXISTING SANITARY SEWER MAIN
  - EXISTING ELECTRICAL
  - PROPOSED ELECTRICAL CONDUIT
  - PROPOSED ELECTRICAL VAULT



**EXHIBIT C.1**

CITY OF ASHLAND  
 SNOWBERRY BROOK II  
 CONCEPTUAL  
 UTILITY PLAN

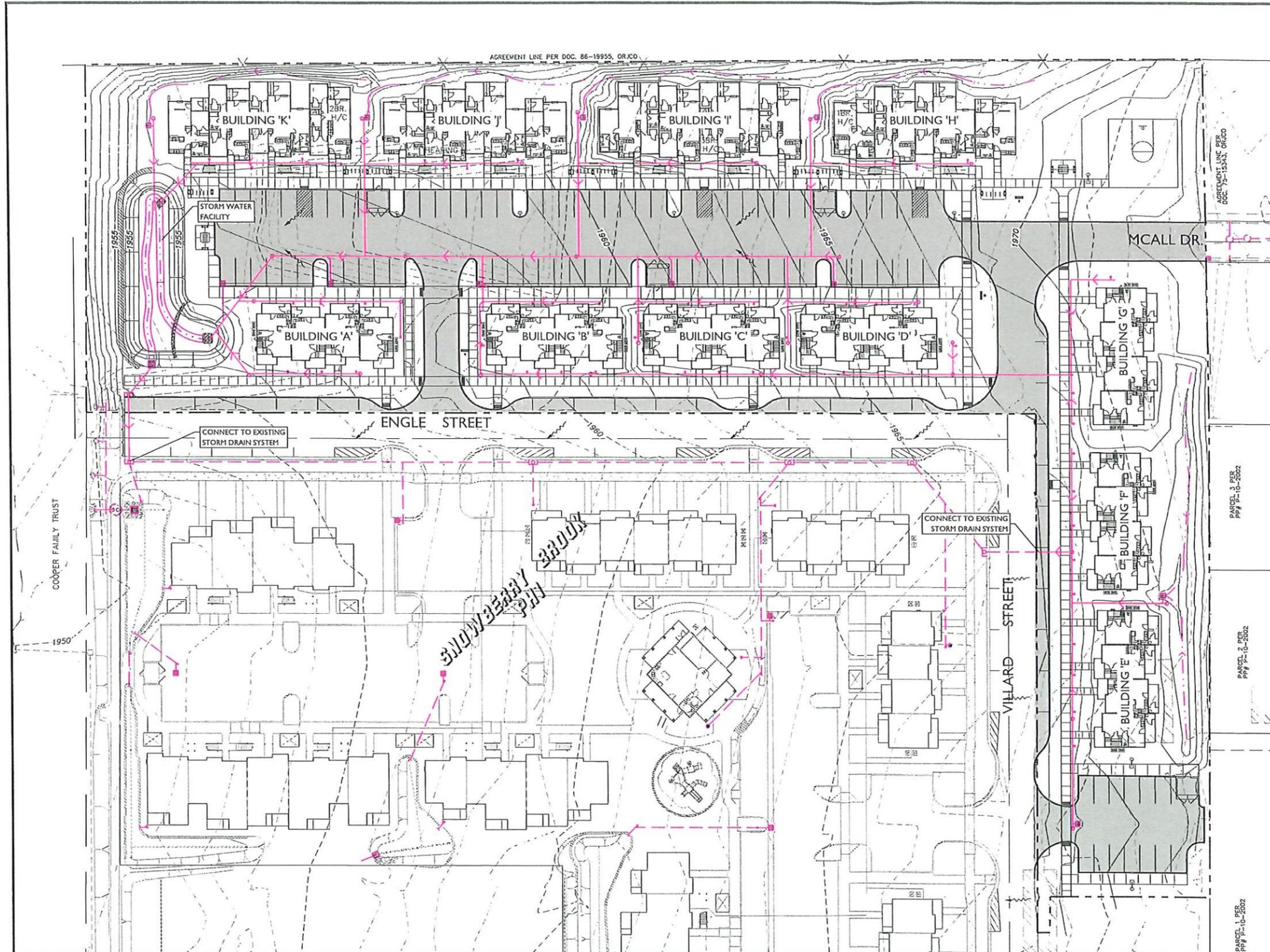
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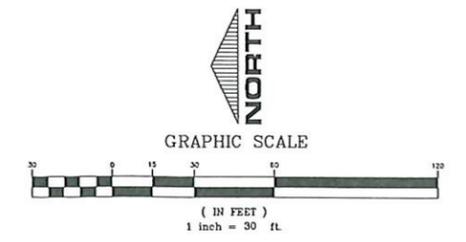
DRAWN BY: J.J.L.	DATE: 04/19	NO. REVISION	DATE	BY
CHECKED BY: A.M.B.	DATE: 04/19			
	DATE:			
	DATE:			
	DATE:			



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- LEGEND**
- PROPOSED STORM MANHOLE OR CLEANOUT
  - PROPOSED CURB INLET
  - PROPOSED CATCH BASIN
  - PROPOSED DITCH INLET
  - PROPOSED STORM MAIN
  - EXISTING STORM MAIN
  - DRAINAGE DIRECTION



**EXHIBIT C.2**

CITY OF ASHLAND  
 SNOWBERRY BROOK II  
 CONCEPTUAL  
 GRADING & DRAINAGE PLAN



DRAWING NAME: SVO-CONCEPT.dwg PROJ. NO: 19-18 PLOT DATE: 5/9/19

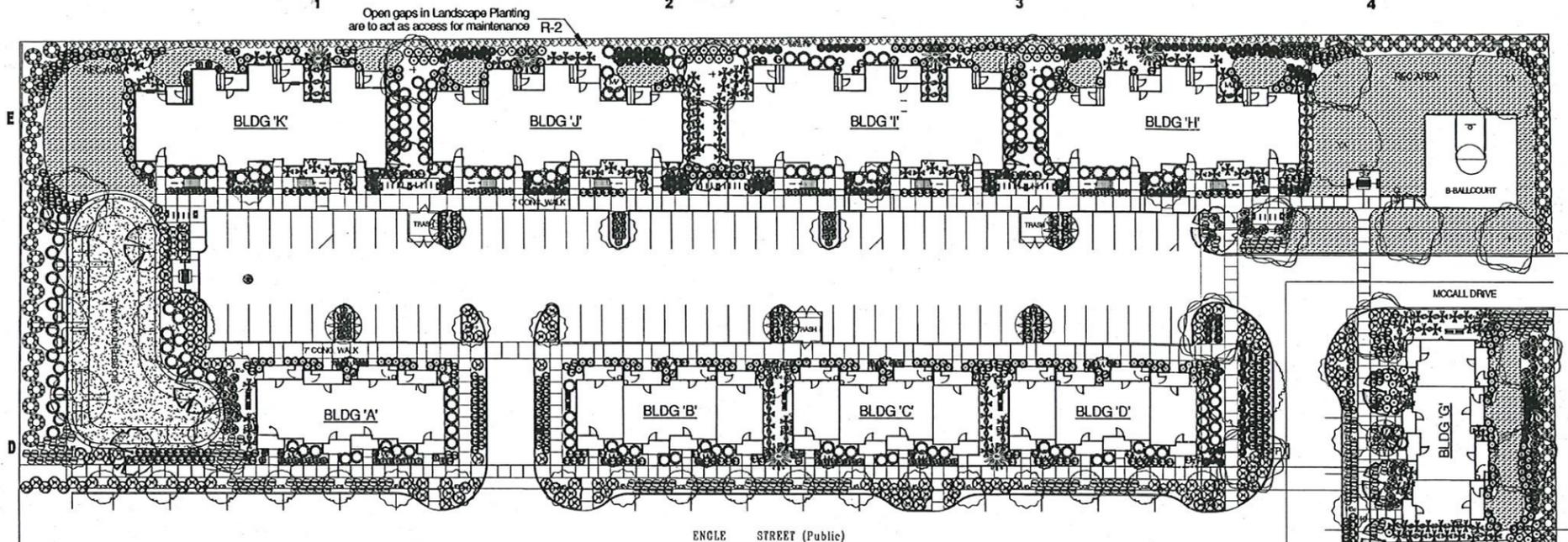


**CONSTRUCTION  
 ENGINEERING  
 CONSULTANTS  
 INC.**  
 P.O. BOX 1724 - MEDFORD, OREGON 97501  
 PH. (541) 779-5268

DRAWN BY:	JUL	DATE:	04/19
CHECKED BY:	AMB	DATE:	04/19
		DATE:	
		DATE:	
		DATE:	

NO.	REVISION	DATE	BY

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Open gaps in Landscape Planting are to act as access for maintenance R-2

Open gaps in Landscape Planting are to act as access for maintenance

**PLANT LIST**

Quantity	Common Name	Botanical Name	Size
<b>Trees</b>			
9	Cherry, Royal Burgundy	<i>Prunus somnata</i> 'Royal Burgundy'	2"
14	Chinese Pistache	<i>Pistacia chinensis</i>	2"
3	Gum, Sour	<i>Nyssa sylvatica</i>	2"
17	Hopler, Bowhall	<i>Acer rubrum</i> 'Bowhall'	2"
12	Oak, Crimson Spire	<i>Quercus robur</i> 'Crimschmidt'	2"
7	Redbud, Western	<i>Cercis occidentalis</i>	15g
7	Yellowwood, American	<i>Cladrestis kentuckea</i>	2g
219	Abelia, Kaleidoscope	<i>Abelia 'Kaleidoscope'</i>	2g
97	Dogwood, Katsuyi	<i>Cornus sericea</i> 'Katsuyi'	2g
122	Heavenly Bamboo, Dwarf Purple	<i>Nandina domestica</i> 'Dwarf Purple'	2g
295	Holly, Japanese Hofferi	<i>Ilex crenata</i> 'Hofferi'	1g
153	Holly, Sky Pencil	<i>Ilex crenata</i> 'Sky Pencil'	3g
219	Laurel, Otto Luyken	<i>Prunus laurocerasus</i> 'Otto Luyken'	2g
86	Orange, Mexican	<i>Choisya ternata</i>	5g
250	Viburnum, David	<i>Viburnum davidii</i>	2g
<b>Ground Cover / Grasses</b>			
283	Grass, Hameln Dwarf	<i>Pennisetum alopecuroides</i> 'Hameln'	1g
227	Grass, Little Kitten	<i>Miscanthus sinensis</i> 'Little Kitten'	1g
201	Kinnikinnick, Emerald Carpet	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	1g
277	Rose, Red Drift	<i>Rosa 'Meigaipei'</i>	1g

**PLANTING**

- Plant material to be provided in accordance with species, sizes and quantities indicated below. Substitutions to be made with the approval of landscape architect.
- No planting to proceed until irrigation system is fully functioning in the area to be planted.
- All plant holes to be dug 2 times the volume of their root ball size. Backfill shall consist of 1/3 organic mulch, 2/3 top soil, micronutrient supplement and 16-16-16 fertilizer as follows:  
1gal 1oz  
3-Sgal 2oz  
larger 4oz
- Plant upright and face to give best appearance or relationship to plants, structures and predominant viewing angle. Trees are to be planted so as to be straight up and down without the assistance of staking. Staking is solely for support against outside forces.
- Loosen and remove wire binding and burlap from around top of each root ball. Soak root balls of plants exhibiting a root bound condition, being careful not to damage the root balls integrity. Stake and guy trees immediately after this work.
- Place and compact backfill soil mixture carefully to avoid injury to roots, and fill all voids.
- When hole is 2/3 filled with soil, completely soak and allow water to soak away at least two times or more, as necessary to completely water individual plants.
- Guarantee plant materials and related workmanship of installation, beginning after written acceptance of work, for one year.  
A. Replace plant material not surviving or in poor condition during guarantee period.  
B. Perform all replacement work in accordance with original specifications at no additional cost to Owner.  
C. Damage or loss of plant materials due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility.

- Turf Areas install sod with Perennial Ryegrass blend
- 18" Round Concrete Stepping Stones set level on 3" of 3/4" compacted gravel

**GRADING**

- All planting beds to be excavated to a minimum depth of 12" or deeper as determined by surrounding concrete and hardscape. Excavation to continue until contacting native soil. Non-native soil to include gravel, rock and debris greater than 1 1/2" in size, and heavy clay soil. Final excavation not to exceed 18" below surrounding hardscapes.
- Fill planters with available reserved top soil harvested from site that is determined to be satisfactory for superior plant growth by testing done at Soil & Plant Laboratory Inc of Anaheim, CA. If available backfill material does not meet specifications fill all planters spaces with Tom White Trucking topsoil or equal. Fill with sufficient excess to allow for 25% compaction. Fill planters so as to achieve a final grade within 3" of the top of the nearest hard surface or concrete.
- Placement of any soil to be done in coordination with suitable weather condition so as to prevent damage to soil structure.
- Sub-grading and final grade to consist of a smooth even grade, no undulation greater than plus or minus 1" within any 10 linear feet of distance.
- SEE CIVIL ENGINEERS DRAWINGS FOR GRADES WITHIN DRAINAGE SWALES
- All sub-grades to be adequately firm without being overly compacted.
- Once subgrade is established the Landscape Contractor is to add mixture compost at a rate of 3 cu yds per 1,000 sq ft, and then rip to blend with top soil to a depth of 8"
- Some areas adjacent to parking lot trees will require structural soil extending into paved areas. Those areas will be determined based on the expected mature size of the adjacent tree. This is based on City of Medford Municipal Code 10.780-G-10a.
- Finish grade in shrub areas to be a smooth even grade rounded 3" high in the middle of beds and ending 3" below surrounding areas. All finish grading to promote positive drainage away from structures and to be done in such a way as to eliminate puddling or collection of water.
- Landscape contractor responsible for addressing any drainage problems encountered during the course of construction, with Landscape Architect.

**FIRE PREVENTION AND CONTROL PLAN NOTES**

- The Whole Parcel noted on this sheet is considered covered as General Fuel Modification Area and is intended to follow Ashland LDC Subsection 18.3.10.100.B
- All standing dead and dying vegetation shall be removed from the property and disposed of appropriately at the time of initial site development. Once done there will be no re-use existing materials remaining.
- All new plantings, including trees, shrubs and ground cover throughout the site are devoid of any plant materials listed on the Prohibited Flammable Plant List.
- There are no planned structures, including fencing, that are considered to be of flammable material within 5' of any planned building. Any site fencing will be of vinyl construction.

NOTE: IF THIS SHEET IS LESS THAN 24" x 36" IT HAS BEEN REDUCED AND IS NOT TO SCALE.

**GENERAL CONSTRUCTION NOTES**

- SITE OBSERVATION VISITS**
  - The Landscape Architect shall be notified by the Landscape Contractor 48 hours in advance of all site observation visits required by this document or requested by the Landscape Contractor.
  - The Landscape Contractor shall be present at each site observation visit.
  - All work that is to be viewed by the Landscape Architect shall be ready and in place. The Landscape Architect has the right to have changes made to any or all of the work.
  - Site observation visits by the Landscape Architect are required for:
    - Pre-construction site meeting
    - Sub grading
    - Preliminary irrigation layout, trench locations, P.O.C and vault sizes.
    - Finish grading and soil preparation
    - Placement of plant materials prior to planting
    - Final installation checklist
    - Periodic review of completed job during maintenance period.
    - Final checklist.
- Landscape Architect may comment and report on any other work being performed as part of any visit.
- Additional site observation visits may be required by the Landscape Architect at any time. If more than one site observation visit is required for a particular portion of work because of excessive deficiencies (as determined by Landscape Architect), the Landscape Contractor shall be charged for additional observations including during the maintenance period.
- General preparation of site to include:
  - Eradication of weeds through the certified application of herbicides, allowing adequate time for kill.
  - Removal, from site, of all existing surface rock and/or debris in planting beds.
  - All shrub beds to be finish graded to a smooth condition prior to mulching.
  - Medium coarse mulch to be placed in all shrub beds to a depth of 3"
  - Treatment Swale preparation:
    - Swale grading and preparation to be per Civil Engineers Specifications.
    - Final grade to include bring areas to an even, smooth grade removing hard or soft areas that might impede drainage or cause puddling.
    - Drainage swales to be hydro-seeded with per
 

Drainage Swale blend grass seed	10lb per 1000 sq ft
Wood Fiber Mulch	45lbs. per 1000 sq ft
Guar Mulch	1 1/2lbs. per 1000 sq ft
Sawdust/Granular Fertilizer 16-16-16	10lbs. per 1000 sq ft
Subdue MAXX	Per Manufacturer Specifications
    - See Hatch Detail for locations and blend
  - Sod lawn to be Perennial Ryegrass Blend or equal as determined by Landscape Architect.
  - Sod lawn preparation:
    - Bring areas to an even, smooth grade removing hard or soft areas that might impede drainage or cause puddling.
    - Install as per growers recommendations.
- INCLUDE 95 DAYS OF MAINTENANCE** from the day of acceptance, including but not limited to:
  - Mow lawns weekly to manufacturer recommended height. Fertilize to manufactures recommendation.
  - Maintain planting areas in a healthy, weed free condition through a minimum of 14 weekly visits.
  - Replace any material showing signs of stress.
  - Monitor irrigation for correct timing.
  - Provide owner with complete list of instructions for continued care.
- When in disagreement and measurements should be confirmed on-site. Any changes are the responsibility of the contractor to co-ordinate with the owners representative.

SEE CIVIL ENGINEERS DRAWINGS FOR GRADES WITHIN DRAINAGE SWALE

**IRRIGATION DETAILS**

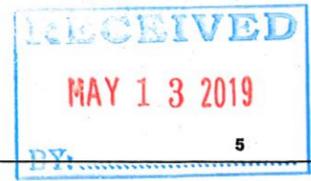
- An automatic irrigation system to be provided for all plant materials areas in accordance with industry standards. System is intended to perform at 20 gpm and 50 psi. Confirm on-site before proceeding depending on the available water source.
- All materials are to be new and in original condition.
- No zone shall exceed 30gpm.
- Place manual drain valves as needed at low points in mainline
- Mainline should be located in area with least conflict with surrounding utilities. Mainline location on plan for ease of interpretation
- Hunter PROS-18 emitter heads in lawn and drainage swale areas. Install Toro Precision Series Spray nozzles in narrow areas, less than 15', and Hunter MP rotators in larger areas. Use the appropriate radius for the area being watered and separate zones by nozzle type. Heads are to be located 2-3" from any concrete, hard surface or transitional area between lawn and shrub. Heads are to be 10' from any building or structure. ALL SPRAY SPRINKLER HEADS TO BE PLACED BASED ON HEAD TO HEAD COVERAGE FROM THE PERIMETER WORKING INTO THE MIDDLE. \*\*\*\*\*NO EXCEPTIONS\*\*\*\*\*
- All drip zones to use PVC laterals to locate a point of connection in each individual planting bed terminating in a Drip Filter.
- Shrub areas to be irrigated by drip irrigation
  - All surface drip tubing to be 1/2" poly tubing. Tubing ends to have removable caps. Tubing to be buried a minimum of 3-5" and held down every 5" with stakes.
  - Rain Bird XB-10 Emitters to be placed at the outside edge of root zones of plants at the following rate:  
1-2g plants 2- 1GPH emitters placed on opposite sides of root ball  
3-5g plants 3- 1GPH emitters placed on opposite sides of root ball  
Larger material 5- 1GPH emitters spaced equally around perimeter of root ball
  - All Drip zones to include a 200 mesh filter and 30psi pressure regulator
- All trenching to be a minimum of 15" deep. Backfill is to be clean and free of any material larger than 1 1/2" in diameter. Backfill shall be adequately compacted and guaranteed against further settling.
- Mainline of 2" SC40 PVC
- All lateral pipe shall be PVC SC40 and 1" minimum.
- Electronic control clock located outside structure at the direction of owner.
- Control wires are to be a minimum of 14ga and applied with water proof connections only. Place all wiring below piping in trenches.
- Steering to be provided under all hardscapes by general contractor for irrigation purposes.
- Irrigation system to be guaranteed against defective material or workmanship for one year from the date of final acceptance. Damage or loss due to vandalism, freezing or acts of neglect by others, is exempt from Contractor's replacement responsibility.
- Provide owner with an accurate as-built locating all valves, wire splices, main line and any steering.
- Provide owner with preliminary watering schedule for the established landscape.
- Provide owner with complete set of written instructions for operation of sprinkler system including spring start up, clock operation, and winterization.
- Walk owner through the entire system describing the operating instructions.

**TREATMENT SWALE HYDRO-SEEDING**

Moderate to Drier Plantings as Hydro-seed Mix	20 pounds per acre
Blue Wildrye	<i>Elymus glaucus</i> 20%
Idaho Fescue	<i>Festuca idahoensis</i> 20%
California Brome	<i>Bromus californicus</i> 20%
California Oatgrass	<i>Dactyloctenium aegyptium</i> 20%
Prairie Junegrass	<i>Koeleria macrantha</i> 20%

Additional Seed Added for Erosion Control in Hydro-seeding

Sterile Wheelgrass	Sterile Trifoliate	20 pounds per acre
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**LANDSCAPE PLAN**  
Scale 1" = 30'



**Madara Design Inc**  
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541-664-7055  
madaradesign@yahoo.com

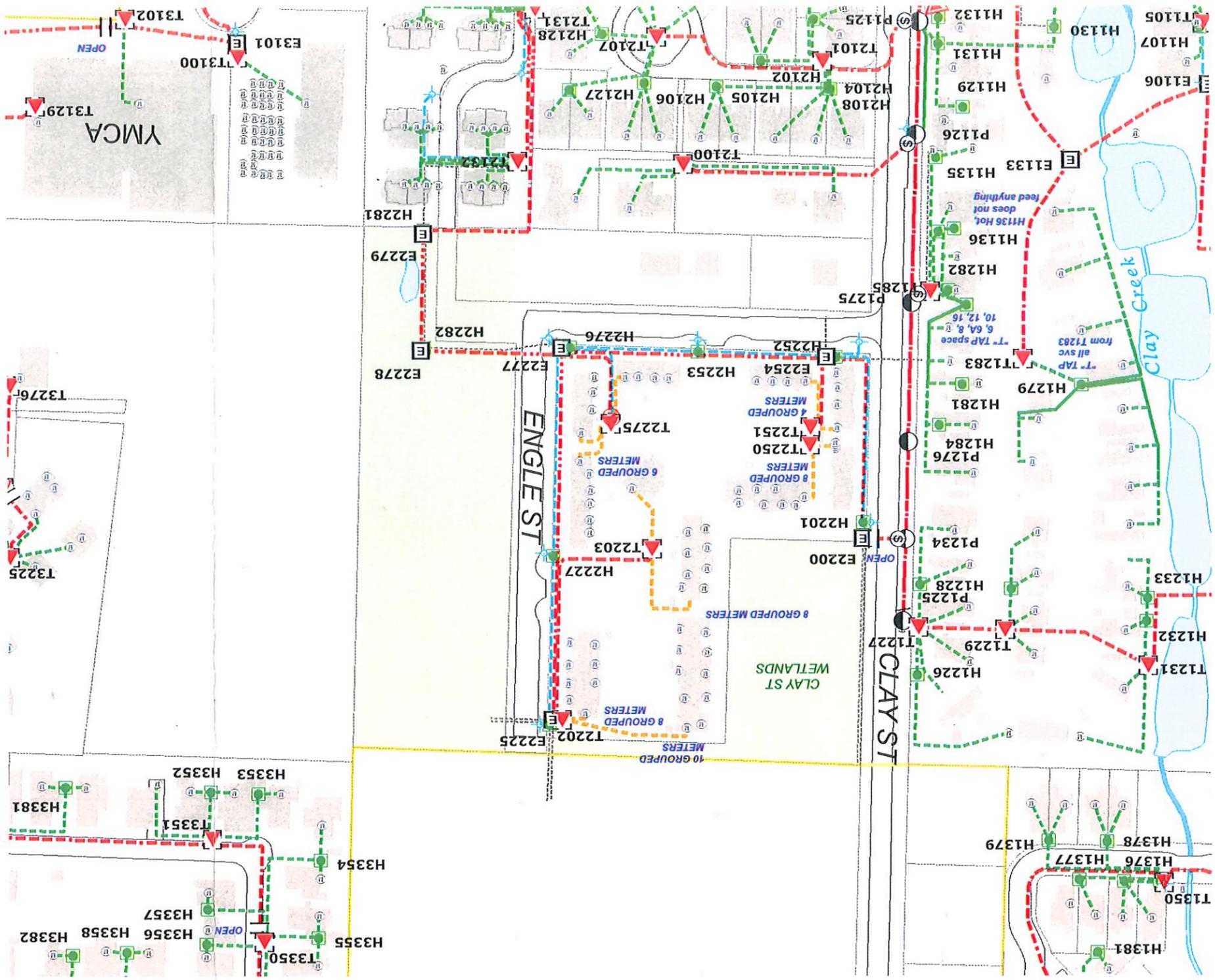
**SNOWBERRY BROOK 2**  
ENGLE & VILLARD STREET  
ASHLAND, OREGON

REVISION	DATE	DESCRIPTION

PROJECT OWNER:  
**HOUSING AUTHORITY OF JACKSON COUNTY**  
2251 TABLECROCK RD.  
MEDFORD, OR  
PROJECT NO: 18-129

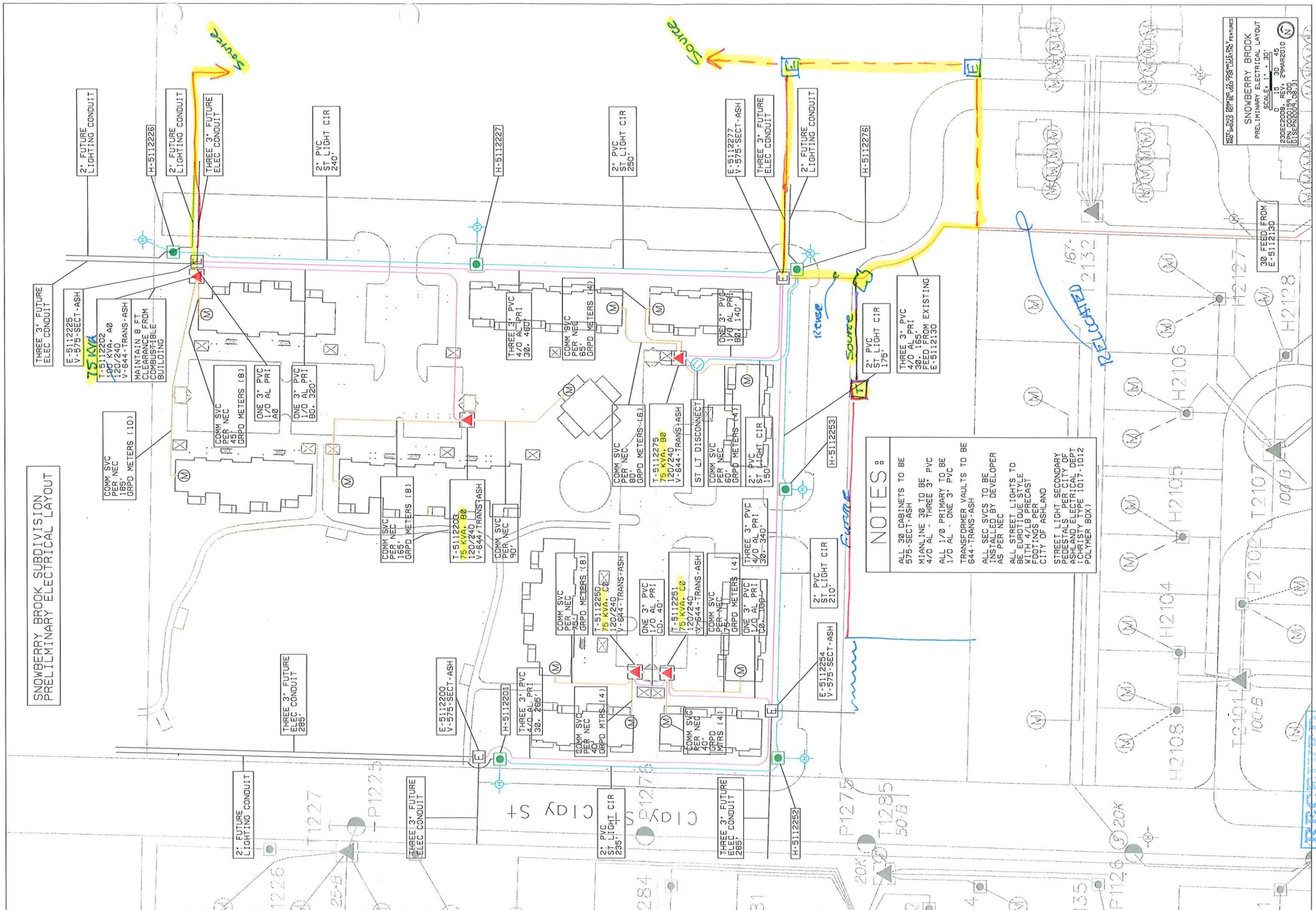
SHEET TITLE:  
**LANDSCAPE PLAN**

SHEET NO:  
**L-1**



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SNOWBERRY BROOK SUBDIVISION  
PRELIMINARY ELECTRICAL LAYOUT



**NOTES :**

- ALL 3Ø CABINETS TO BE 575-SECT-ASH
- MIANLINE 3Ø TO BE 4/0 AL - THREE 3" PVC
- ALL 1/Ø PRIMARY TO BE 1/0 AL - ONE 3" PVC
- TRANSFORMER VAULTS TO BE 644-TRANS-ASH
- ALL SEC SVCS TO BE INSTALLED BY DEVELOPER AS PER NEC
- ALL STREET LIGHTS TO BE EUROLOUE STYLE WITH 4' Ø PRECAST FOOTINGS PER CITY OF ASHLAND
- STREET LIGHT SECONDARY PEDESTALS PER CITY OF ASHLAND ELECTRICAL DEPT (CURBTYPE 1017-1012 POLYMER BOX)

NOTE: THIS DRAWING IS FOR PERMITS ONLY. FEATURES NOT SHOWN ARE TO BE USED FOR PERMITS ONLY. FEATURES

SNOWBERRY BROOK  
PRELIMINARY ELECTRICAL LAYOUT  
SCALE: 1" = 30'  
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EPN:0000159 300  
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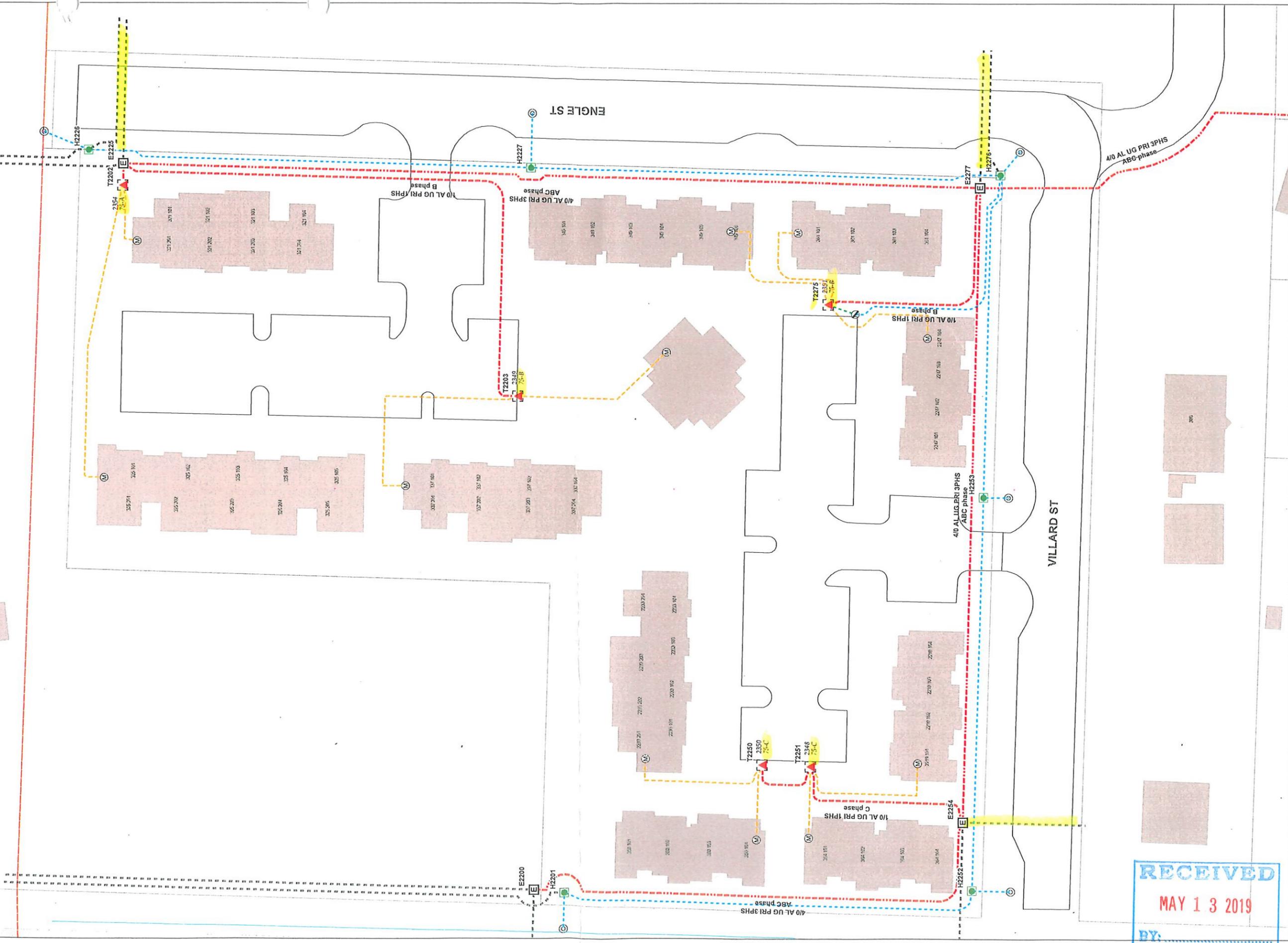
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